

128TH AVE NE GREENWAY

KIRKLAND, WASHINGTON

PROJECT# 25-18-PW

128TH AVE NE FROM NE 75TH ST TO NE 112TH PL
NE 112TH ST FROM 128TH AVE NE TO SLATER AVE NE
NE 75TH ST AND 120TH AVE NE INTERSECTION
LENGTH 12,400 FEET = 2.35 MILES



VICINITY MAP
N.T.S.

CITY OFFICIALS

| | |
|--------------------|--------------------------|
| PENNY SWEET | MAYOR |
| JAY ARNOLD | DEPUTY MAYOR |
| TOM NEIR | COUNCIL MEMBER |
| KELLI CURTIS | COUNCIL MEMBER |
| TOBY NIXON | COUNCIL MEMBER |
| DAVE ASHER | COUNCIL MEMBER |
| JON PASCAL | COUNCIL MEMBER |
| KURT TRIPLETT | CITY MANAGER |
| KATHY BROWN | PUBLIC WORKS DIRECTOR |
| ROD STEITZER, P.E. | CAPITAL PROJECTS MANAGER |

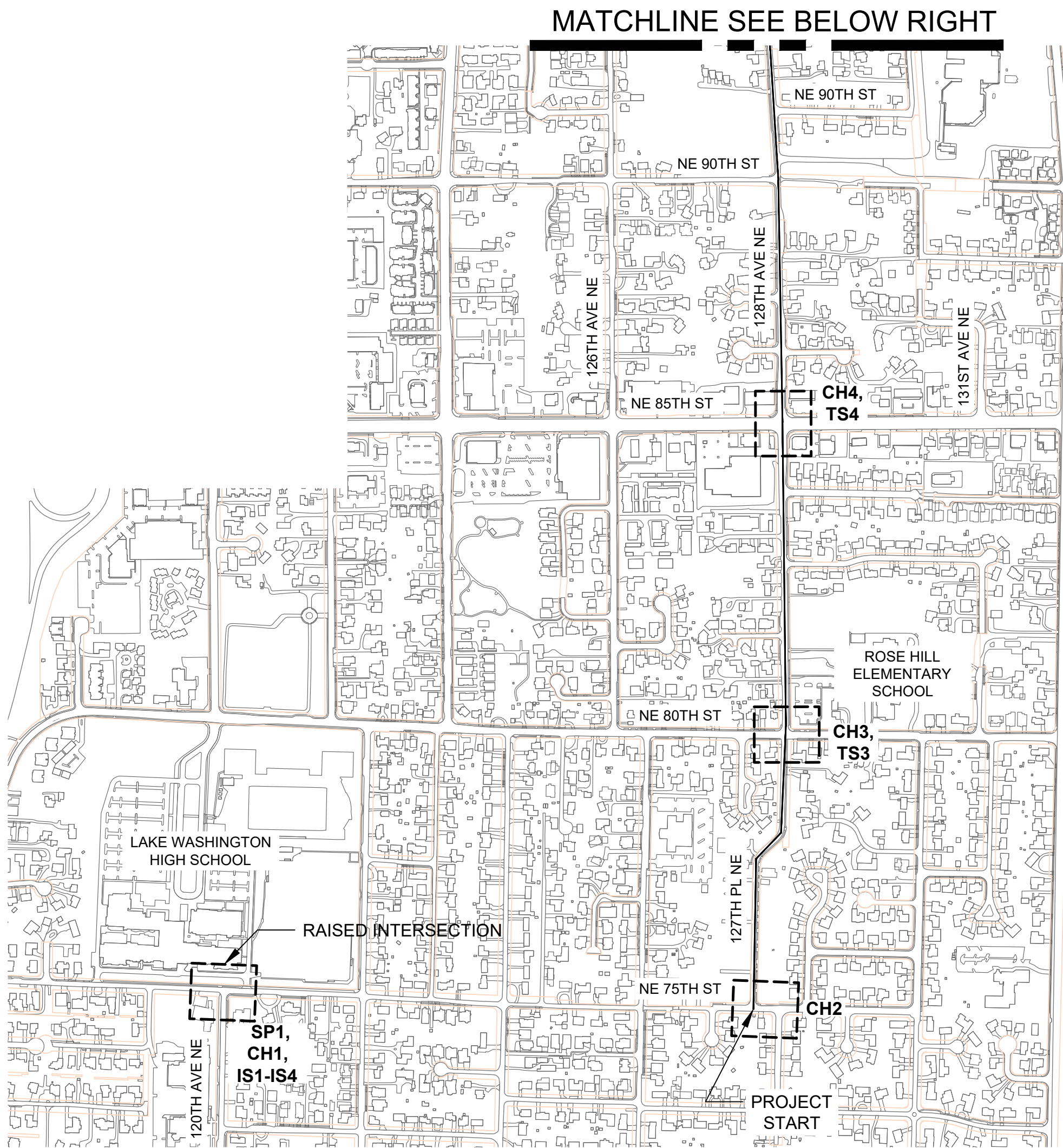
CONTACT PERSONNEL

| | | |
|----------------------|------------------------------|--------------|
| MARIUS EUGENIO, P.E. | PROJECT ENGINEER | 425-587-3872 |
| RIK MAYER | COK INSPECTOR | 206-496-4265 |
| NATHEN HOWER | COK STREET DEPARTMENT | 425-587-3900 |
| JASON OSBORN | COK STORMWATER | 425-587-3900 |
| TOM CHRIEST | COK WATER & WASTEWATER DEPT. | 425-587-3900 |
| DISPATCH | COK POLICE DEPARTMENT | 425-577-5656 |
| DISPATCH | COK FIRE DEPARTMENT | 425-587-3650 |
| JOSH PANTSKE | COK UTILITY MANAGER | 425-587-3917 |
| JEANNE COLEMAN | PUGET SOUND ENERGY | 425-449-7410 |
| JAY SCHWAB | FRONTIER COMMUNICATIONS | 425-263-4035 |
| JOHN NICHOLSON | INTEGRA TELECOM | 425-970-7000 |
| RAYMOND PILKENTON | COMCAST CABLE | 425-263-5332 |
| CONST. COORDINATOR | KING COUNTY METRO | 206-684-2732 |
| BETH BROOK | LAKE WASH. SCHOOL DIST. | 425-882-5147 |
| JEFF MILES | LAKE WASH. SCHOOL DIST. | 425-882-5120 |
| GREG MATOTE | NORTHSHORE UTILITY DIST. | 425-398-4400 |
| | EMERGENCY | 911 |
| | ONE CALL | 800-424-5555 |

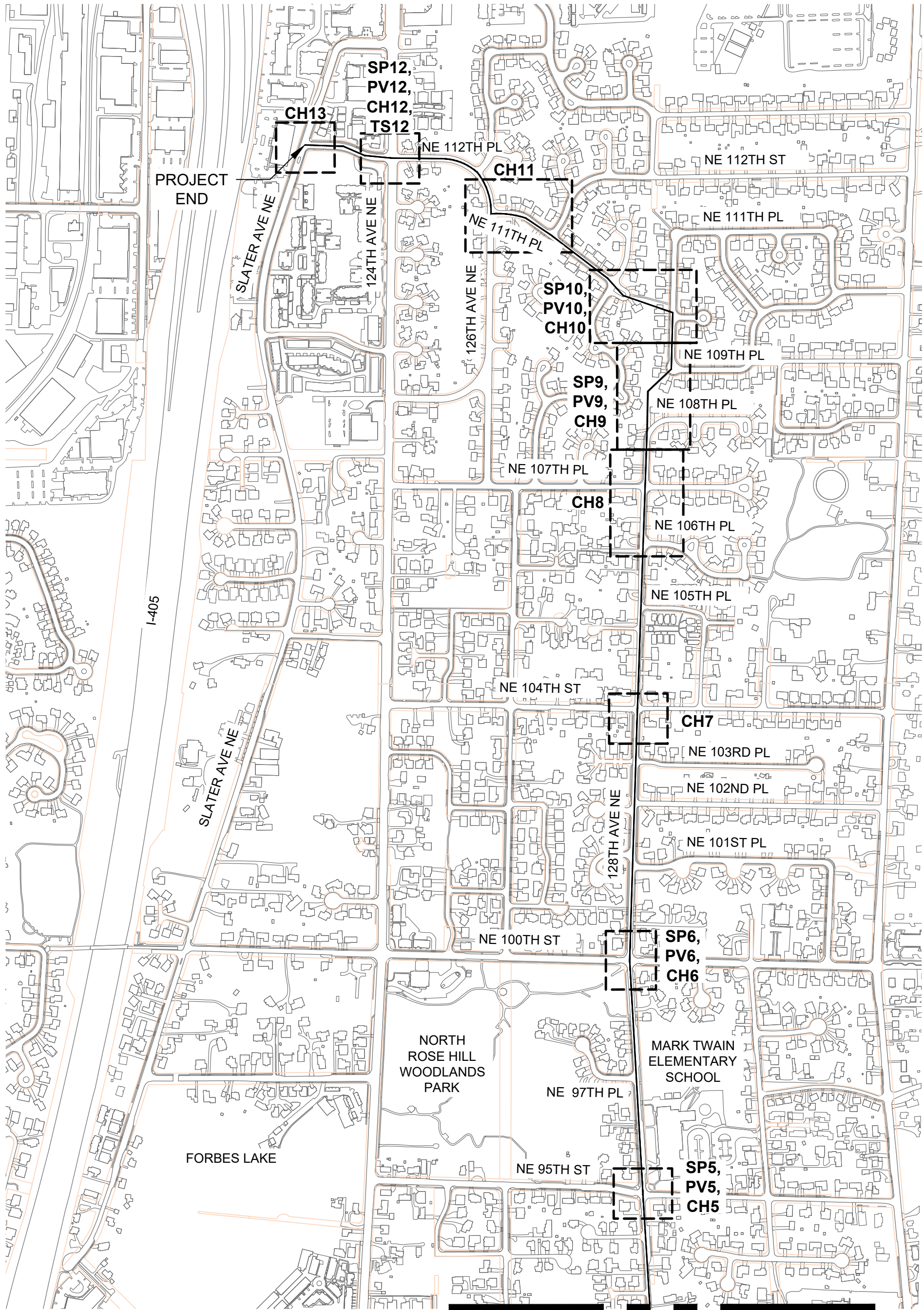
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| 2 | NT1 | SHEET INDEX & LOCATION MAP |
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BASE MAP NOTES:

1. SEE ALIGNMENT PLANS FOR SURVEY CONTROL NOTES. THE ALIGNMENT PLANS INDICATE WHICH ROAD SEGMENTS AND INTERSECTIONS WERE SURVEYED BY LIN AND ASSOCIATES.
2. THE BASE MAPS FOR NON-SURVEYED ROAD SEGMENTS AND INTERSECTIONS WERE CREATED FROM GIS PROVIDED BY THE CITY OF KIRKLAND AND DO NOT REFLECT SURVEY ACCURACY.
3. THE CONTRACTOR SHALL CONFIRM ACTUAL FIELD CONDITIONS AND SHALL NOTIFY ENGINEER OR ANY CONFLICTS OR DISCREPANCIES PRIOR TO CONSTRUCTION.



LOCATION MAP
1"=500'



MATCHLINE SEE ABOVE LEFT



100% SUBMITTAL - NOT FOR CONSTRUCTION



128TH AVE NE GREENWAY
KIRKLAND GREENWAYS
PROJECT# 25-18-PW
KIRKLAND, WA

PREPARED: AL/TD
CHECKED: CS/KL
DATE
DECEMBER 2019
REV. 1
REV. 2
REV. 3
SHEET NAME
SHEET INDEX AND
LOCATION MAP
DRAWING NUMBER
NT1
SHEET NUMBER
2 OF 49

TOOLE
DESIGN
720 3RD AVE SUITE 2020
SEATTLE, WA 98104
PHONE: (206) 297-1601
FAX: (301) 927-2800
WWW.TOOLEDESIGN.COM

GENERAL NOTES

1.

A PRE-CONSTRUCTION CONFERENCE SHALL BE HELD PRIOR TO THE START OF CONSTRUCTION.
2.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR SECURING ALL NECESSARY PERMITS PRIOR TO CONSTRUCTION.
3.

ALL ROADWAY WORK AND MATERIAL SHALL BE IN ACCORDANCE WITH THE CURRENT APWA AND CITY OF KIRKLAND STANDARDS AND SPECIFICATIONS.
4.

ALL PUBLIC ROADWAYS SHALL BE CONSTRUCTED OF 2" CLASS "B" AC PAVING ON 4" ASPHALT-TREATED BASE (ATB), UNLESS OTHERWISE APPROVED BY THE PUBLIC WORKS DEPARTMENT.
5.

A COPY OF THE APPROVED ROADWAY PLANS MUST BE ON THE JOB SITE WHENEVER CONSTRUCTION IS IN PROGRESS.
6.

DENSITY TEST REPORTS WILL BE REQUIRED FOR ALL PUBLIC ROADWAYS AND ALL PRIVATE ROADWAYS WITHIN PLATS. ALL TRENCH BACKFILL SHALL BE COMPACTED TO 95 PERCENT DENSITY IN ROADWAYS, ROADWAY SHOULDERS, ROADWAY PRISM AND DRIVEWAYS, AND 85 PERCENT DENSITY IN UNPAVED AREAS. ALL PIPE ZONE COMPACTION SHALL BE 95 PERCENT.
7.

ALL COMMERCIAL AND RESIDENTIAL DRIVEWAYS MUST CONFORM TO THE CITY OF KIRKLAND DEPARTMENT OF PUBLIC WORKS DRIVEWAY POLICY.
8.

ALL CONCRETE FOR SIDEWALKS AND CURB AND GUTTER MUST BE 4,000 PSI MINIMUM. (5-3/4 SACK MIX.)
9.

IN THE CASE OF NEW ROAD CONSTRUCTION OR RECONSTRUCTION REQUIRING MAILBOXES TO BE MOVED OR REARRANGED, THE CONTRACTOR SHALL COORDINATE WITH THE U.S. POSTAL SERVICE FOR THE NEW LOCATION OF THE MAILBOX STRUCTURE.
10.

ANY ROADWAY SIGNAGE OR STRIPING REMOVED OR TEMPORARILY MOVED BY THE CONTRACTOR SHALL BE RESTORED SO AS TO MEET THE CURRENT CITY OF KIRKLAND STANDARDS.
11.

IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO PROVIDE ADEQUATE TEMPORARY TRAFFIC CONTROL TO ENSURE TRAFFIC SAFETY DURING CONSTRUCTION ACTIVITIES. THEREFORE, THE CONTRACTOR SHALL SUBMIT A TRAFFIC CONTROL PLAN TO THE PUBLIC WORKS DEPARTMENT AT LEAST 48 HOURS PRIOR TO STARTING ANY WORK IN THE RIGHT-OF-WAY. ALL TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (MUTCD) OR AS MODIFIED BY THE TRAFFIC ENGINEER.
12.

WHERE A SIDEWALK IS TO BE CONSTRUCTED ABOVE A SLOPE OR ADJACENT TO A ROCKERY OR RETAINING WALL WHERE THE LOWEST FINISHED ELEVATION OF THE SLOPE, ROCKERY, OR RETAINING WALL IS TO BE THIRTY (30") OR MORE BELOW THE FINISHED ELEVATION OF THE SIDEWALK, A SAFETY RAILING SHALL BE REQUIRED WHEN: (A) THE PLANE OF THE WALL FACE IS LESS THAN 4' IN HORIZONTAL DISTANCE FROM THE OUTSIDE EDGE OF THE SIDEWALK; (B) THE SLOPES ADJACENT TO THE SIDEWALK AVERAGE GREATER THAN TWO TO ONE.
13.

THE MAXIMUM GRADE FOR PRIVATE ROADWAYS SHALL BE TWENTY PERCENT (20%), OR FIFTEEN PERCENT (15%) IF USED FOR FIRE ACCESS. FOR PUBLIC ROADWAYS, THE MAXIMUM GRADE SHALL BE FIFTEEN PERCENT (15%).
14.

DEAD-END STREETS SHALL BE APPROPRIATELY SIGNED AND BARRICADED. SEE MOST CURRENT EDITION OF THE MUTCD.
15.

SIDEWALK AND CURB AND GUTTER CANNOT BE POURED MONOLITHICALLY. THERE MUST BE A COLD JOINT OR FULL-DEPTH EXPANSION JOINT BETWEEN THEM.
16.

MEASURES SHALL BE TAKEN BY THE CONTRACTOR TO PROVIDE GROUND COVER IN AREAS WITHIN THE RIGHT-OF-WAY WHICH HAVE BEEN STRIPPED OF NATURAL VEGETATION OR HAVE A POTENTIAL FOR EROSION.
17.

THE CONTRACTOR SHALL COORDINATE WITH PUGET POWER FOR THE DESIGN AND INSTALLATION OF STREET LIGHTS ON ALL NEWLY-CREATED PUBLIC ROADWAYS AND EXISTING ROADWAYS.
18.

WHEN AN EXISTING ROADWAY IS TO RECEIVE A HALF-STREET OVERLAY, THE EXISTING ROADWAY MUST BE COLD PLANED AT THE EDGE OF THE GUTTER AND CENTERLINE. WHEN THE EXISTING ROADWAY IS TO RECEIVE A FULL-STREET OVERLAY, IT MUST BE COLD PLANED AT THE EDGE OF BOTH GUTTERS. SEE CITY OF KIRKLAND STANDARD DETAIL NO. R.13.
19.

THE TEMPORARY EROSION AND SEDIMENTATION CONTROL (TESC) MEASURES SHOWN IN THESE PLANS SHALL BE CONSIDERED A MINIMUM. THE CONTRACTOR SHALL PROVIDE ANY REASONABLE ADDITIONAL MEASURES AS MAY BE REQUIRED TO FACILITATE ACTUAL SITE RUNOFF CONDITIONS AT THE TIME OF CONSTRUCTION. ALL NECESSARY TEMPORARY EROSION AND SEDIMENTATION CONTROL MEASURES SHALL BE IN-PLACE PRIOR TO ANY DEMOLITION OR CONSTRUCTION ACTIVITIES.
20.

WHEN INSTALLING NEW SIDEWALK, THE AREA BEHIND THE SIDEWALK MUST BE GRADED SO THAT THE YARD DRAINAGE DOES NOT DRAIN OVER THE SIDEWALK.
21.

ANY EXISTING PUBLIC IMPROVEMENTS DAMAGED DURING CONSTRUCTION SHALL BE REPLACED PRIOR TO FINAL INSPECTION.
22.

THE CONTRACTOR IS RESPONSIBLE FOR KEEPING ALL PUBLIC STREETS FREE FROM MUD AND DEBRIS AT ALL TIMES. THE CONTRACTOR SHALL BE PREPARED TO USE POWER SWEEPERS OR OTHER PIECES OF EQUIPMENT NECESSARY TO KEEP THE ROADWAYS CLEAN.
23.

BACKFILL IN ALL STREET CUTS ON ARTERIALS WILL BE CONTROL DENSITY FILL (CDF). CONTRACTOR MUST PROVIDE STEEL PLATING NECESSARY TO ALLOW THE CDF TO CURE.
24.

WHEN CONSTRUCTING NEW CURB AND GUTTER WHICH DOES NOT ALIGN WITH THE EXISTING EDGE OF PAVEMENT, THE ROADWAY MUST BE TAPERED FROM THE ENDS OF THE NEW CURB AND GUTTER TO MATCH THE EXISTING PAVEMENT. THE ENTRY TAPER INTO THE NEW IMPROVEMENTS SHALL BE 5:1, AND LEAVING THE NEW IMPROVEMENTS SHALL BE 10:1.
25.

WHEN AN EXISTING ROADWAY IS TO BE WIDENED, THE EXISTING PAVEMENT MUST BE SAW CUT AT LEAST ONE FOOT FROM THE EDGE TO PROVIDE A PROPER MATCH BETWEEN NEW AND EXISTING ASPHALT. HOWEVER, WHEN THE EXISTING PAVEMENT CONTAINS ALLIGATORED AREAS, THOSE AREAS MUST BE REMOVED PRIOR TO WIDENING. ALL SAW CUTS SHALL BE PARALLEL OR PERPENDICULAR TO THE RIGHT-OF-WAY CENTERLINE.
26.

ALL ROCKERIES MUST BE CONSTRUCTED IN ACCORDANCE WITH THE MOST CURRENT GUIDELINES OF THE ASSOCIATION.

CITY OF KIRKLAND STANDARD
EROSION AND SEDIMENT CONTROL (ESC) NOTES

1.

THE APPROVED CONSTRUCTION SEQUENCE SHALL BE AS FOLLOWS:

a.

CONDUCT PRE-CONSTRUCTION MEETING.

b.

FLAG OR FENCE CLEARING LIMITS AS DIRECTED BY ENGINEER.

c.

POST SIGN WITH NAME AND PHONE NUMBER OF TESC SUPERVISOR.

d.

INSTALL CATCH BASIN PROTECTION IF REQUIRED.

e.

GRADE AND INSTALL CONSTRUCTION ENTRANCE(S).

f.

INSTALL PERIMETER PROTECTION (SILT FENCE, BRUSH BARRIER, ETC.).

g.

CONSTRUCT SEDIMENT PONDS AND TRAPS.

h.

GRADE AND STABILIZE CONSTRUCTION ROADS.

i.

CONSTRUCT SURFACE WATER CONTROLS (INTERCEPTOR DIKES, PIPE SLOPE DRAINS, ETC.) SIMULTANEOUSLY WITH CLEARING AND GRADING FOR PROJECT DEVELOPMENT.

j.

MAINTAIN EROSION CONTROL MEASURE IN ACCORDANCE WITH CITY OF KIRKLAND STANDARDS AND MANUFACTURER'S RECOMMENDATIONS.

k.

RELOCATE EROSION CONTROL MEASURES OR INSTALL NEW MEASURES SO THAT AS SITE CONDITIONS CHANGE, THE EROSION AND SEDIMENT CONTROL IS ALWAYS IN ACCORDANCE WITH THE CITY TESC MINIMUM REQUIREMENTS.

l.

COVER ALL AREAS WITHIN THE SPECIFIED TIME FRAME WITH STRAW, WOOD FIBER MULCH, COMPOST, PLASTIC SHEETING, CRUSHED ROCK OR EQUIVALENT.

m.

STABILIZE ALL AREAS THAT REACH FINAL GRADE WITHIN 7 DAYS.

n.

SEED OR SOD ANY AREAS TO REMAIN UNWORKED FOR MORE THAN 30 DAYS.

o.

UPON COMPLETION OF THE PROJECT, ALL DISTURBED AREAS MUST BE STABILIZED AND BEST MANAGEMENT PRACTICES REMOVED IF APPROPRIATE.

2.

CONTRACTOR IS RESPONSIBLE FOR KEEPING STREETS CLEAN AND FREE OF CONTAMINANTS AT ALL TIMES AND FOR PREVENTING AN ILLICIT DISCHARGE (KMC 15.52) INTO THE MUNICIPAL STORM DRAIN. IF YOUR CONSTRUCTION PROJECT CAUSES AN ILLICIT DISCHARGE TO THE MUNICIPAL STORM DRAIN SYSTEM, THE CITY OF KIRKLAND STORM MAINTENANCE DIVISION WILL BE CALLED TO CLEAN THE PUBLIC STORM SYSTEM, AND OTHER AFFECTED PUBLIC INFRASTRUCTURE. THE CONTRACTOR(S), PROPERTY OWNER, AND ANY OTHER RESPONSIBLE PARTY MAY BE CHARGED ALL COSTS ASSOCIATED WITH THE CLEAN-UP AND MAY ALSO BE ASSESSED MONETARY PENALTIES (KMC 1.12.200). THE MINIMUM PENALTY IS \$250. A FINAL INSPECTION OF YOUR PROJECT WILL NOT BE GRANTED UNTIL ALL COSTS ASSOCIATED WITH THE CLEAN-UP, AND PENALTIES, ARE PAID TO THE CITY OF KIRKLAND.

3.

CONSTRUCTION DEWATERING DISCHARGES SHALL ALWAYS MEET WATER QUALITY GUIDELINES LISTED IN COK POLICY E-1. SPECIFICALLY, DISCHARGES TO THE PUBLIC STORMWATER DRAINAGE SYSTEM MUST BE BELOW 25NTU, AND NOT CONSIDERED AN ILLICIT DISCHARGE (PER KMC 15.52.090). TEMPORARY DISCHARGES TO SANITARY SEWER REQUIRE PRIOR AUTHORIZATION AND PERMIT FROM KING COUNTY INDUSTRIAL WASTE PROGRAM (206-263-3000) AND NOTIFICATION TO THE PUBLIC WORKS CONSTRUCTION INSPECTOR.

4.

ALL WORK AND MATERIALS SHALL BE IN ACCORDANCE WITH CITY OF KIRKLAND STANDARDS AND SPECIFICATIONS.

5.

THE BOUNDARIES OF THE CLEARING LIMITS AS DIRECTED BY THE ENGINEER SHALL BE CLEARLY FLAGGED IN THE FIELD BY A CLEARING CONTROL FENCE PRIOR TO CONSTRUCTION. DURING THE CONSTRUCTION PERIOD, NO DISTURBANCE OR REMOVAL OF ANY GROUND COVER BEYOND THE FLAGGED CLEARING LIMITS SHALL BE PERMITTED. THE FLAGGING SHALL BE MAINTAINED BY THE PERMITTEE/CONTRACTOR FOR THE DURATION OF CONSTRUCTION.

6.

APPROVAL OF THIS EROSION/SEDIMENTATION CONTROL (ESC) PLAN DOES NOT CONSTITUTE AN APPROVAL OF PERMANENT ROAD OR DRAINAGE DESIGN (E.G., SIZE AND LOCATION OF ROADS, PIPES, RESTRICTORS, CHANNELS, RETENTION FACILITIES, UTILITIES, ETC.).

7.

THE IMPLEMENTATION OF THIS ESC PLAN AND THE CONSTRUCTION, MAINTENANCE, REPLACEMENT, AND UPGRADING OF THESE ESC FACILITIES IS THE RESPONSIBILITY OF THE PERMITTEE/CONTRACTOR UNTIL ALL CONSTRUCTION IS APPROVED.

8.

A COPY OF THE APPROVED ESC PLANS MUST BE ON THE JOB SITE WHENEVER CONSTRUCTION IS IN PROGRESS.

9.

THE ESC FACILITIES SHOWN ON THIS PLAN MUST BE CONSTRUCTED PRIOR TO OR IN CONJUNCTION WITH ALL CLEARING AND GRADING ACTIVITIES IN SUCH A MANNER AS TO ENSURE THAT SEDIMENT-LADEN WATER DOES NOT ENTER THE DRAINAGE SYSTEM OR VIOLATE APPLICABLE WATER STANDARDS. WHEREVER POSSIBLE, MAINTAIN NATURAL VEGETATION FOR SILT CONTROL.

10.

THE ESC FACILITIES SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE DETAILS ON THE APPROVED SYSTEM. LOCATIONS MAY BE MOVED TO SUIT FIELD CONDITIONS, SUBJECT TO APPROVAL BY THE ENGINEER AND THE CITY OF KIRKLAND INSPECTOR.

11.

THE ESC FACILITIES SHOWN ON THIS PLAN ARE THE MINIMUM REQUIREMENTS FOR ANTICIPATED SITE CONDITIONS. DURING THE CONSTRUCTION PERIOD, THESE ESC FACILITIES SHALL BE UPGRADED (E.G., ADDITIONAL SUMPS, RELOCATION OF DITCHES AND SILT FENCES, ETC.) AS NEEDED FOR UNEXPECTED STORM EVENTS. ADDITIONALLY, MORE ESC FACILITIES MAY BE REQUIRED TO ENSURE COMPLETE SILTATION CONTROL. THEREFORE, DURING THE COURSE OF CONSTRUCTION IT SHALL BE THE OBLIGATION AND RESPONSIBILITY OF THE CONTRACTOR TO ADDRESS ANY NEW CONDITIONS THAT MAY BE CREATED BY HIS ACTIVITIES AND TO PROVIDE ADDITIONAL FACILITIES OVER AND ABOVE THE MINIMUM REQUIREMENTS AS MAY BE NEEDED.

12.

THE ESC FACILITIES SHALL BE INSPECTED BY THE PERMITTEE/CONTRACTOR DAILY DURING NON-RAINFALL PERIODS, EVERY HOUR (DAYLIGHT) DURING A RAINFALL EVENT, AND AT THE END OF EVERY RAINFALL, AND MAINTAINED AS NECESSARY TO ENSURE THEIR CONTINUED FUNCTIONING. IN ADDITION, TEMPORARY SILTATION PONDS AND ALL TEMPORARY SILTATION CONTROLS SHALL BE MAINTAINED IN A SATISFACTORY CONDITION UNTIL SUCH TIME THAT CLEARING AND/OR CONSTRUCTION IS COMPLETED, PERMANENT DRAINAGE FACILITIES ARE OPERATIONAL, AND THE POTENTIAL FOR EROSION HAS PASSED. WRITTEN RECORDS SHALL BE KEPT DOCUMENTING THE REVIEWS OF THE ESC FACILITIES.

13.

THE ESC FACILITIES ON INACTIVE SITES SHALL BE INSPECTED AND MAINTAINED A MINIMUM OF ONCE A MONTH OR WITHIN 48 HOURS FOLLOWING A STORM EVENT.

14.

STABILIZED CONSTRUCTION ENTRANCES SHALL BE INSTALLED AT THE BEGINNING OF CONSTRUCTION AND MAINTAINED FOR THE DURATION OF THE PROJECT. ADDITIONAL MEASURES, SUCH AS WASH PADS, MAY BE REQUIRED TO ENSURE THAT ALL PAVED AREAS ARE KEPT CLEAN FOR THE DURATION OF THE PROJECT.

15.

ALL DENUDED SOILS MUST BE STABILIZED WITH AN APPROVED TEMPORARY ESC METHOD (E.G. SEEDING, MULCHING, PLASTIC COVERING, CRUSHED ROCK) WITHIN THE FOLLOWING TIMELINES:

•

MAY 1 TO SEPTEMBER 30 – SOILS MUST BE STABILIZED WITHIN 7 DAYS OF GRADING.

•

OCTOBER 1 TO APRIL 30 – SOILS MUST BE STABILIZED WITHIN 2 DAYS OF GRADING.

•

STABILIZE SOILS AT THE END OF THE WORKDAY PRIOR TO A WEEKEND, HOLIDAY, OR PREDICTED RAIN EVENT.

CITY OF KIRKLAND STANDARD
EROSION AND SEDIMENT CONTROL NOTES (CONT)

16.

WHERE SEEDING FOR TEMPORARY EROSION CONTROL IS REQUIRED, FAST GERMINATING GRASSES SHALL BE APPLIED AT AN APPROPRIATE RATE (EXAMPLE: ANNUAL OR PERENNIAL RYE APPLIED AT APPROXIMATELY 80 POUNDS PER ACRE).
17.

WHERE STRAW MULCH IS REQUIRED FOR TEMPORARY EROSION CONTROL, IT SHALL BE APPLIED AT A MINIMUM THICKNESS OF 2".
18.

ALL LOTS ADJOINING OR HAVING ANY NATIVE GROWTH PROTECTION EASEMENTS (NGPE) SHALL HAVE A 6" HIGH TEMPORARY CONSTRUCTION FENCE (CHAIN LINK WITH PIER BLOCKS) SEPARATING THE LOT (OR BUILDABLE PORTIONS OF THE LOT) FROM THE AREA RESTRICTED BY THE NGPE AND SHALL BE INSTALLED PRIOR TO ANY GRADING OR CLEARING AND REMAIN IN PLACE UNTIL THE PLANNING DEPARTMENT AUTHORIZES REMOVAL.
19.

CLEARING LIMITS SHALL BE DELINEATED WITH A CLEARING CONTROL FENCE. THE CLEARING CONTROL FENCE SHALL CONSIST OF A 6-FT. HIGH CHAIN LINK FENCE ADJACENT THE DRIP LINE OF TREES TO BE SAVED, WETLAND OR STREAM BUFFERS, AND SENSITIVE SLOPES. CLEARING CONTROL FENCES ALONG WETLAND OR STREAM BUFFERS OR UPSLOPE OF SENSITIVE SLOPES SHALL BE ACCOMPANIED BY AN EROSION CONTROL FENCE. IF APPROVED BY THE CITY, A FOUR-FOOT HIGH ORANGE MESH CLEARING CONTROL FENCE MAY BE USED TO DELINEATE CLEARING LIMITS IN ALL OTHER AREAS.
20.

OFF-SITE STREETS MUST BE KEPT CLEAN AT ALL TIMES. IF DIRT IS DEPOSITED ON THE PUBLIC STREET SYSTEM, THE STREET SHALL BE IMMEDIATELY CLEANED WITH POWER SWEEPER OR OTHER EQUIPMENT. ALL VEHICLES SHALL LEAVE THE SITE BY WAY OF THE CONSTRUCTION ENTRANCE AND SHALL BE CLEANED OF ALL DIRT THAT WOULD BE DEPOSITED ON THE PUBLIC STREETS.
21.

ROCK FOR EROSION PROTECTION OF ROADWAY DITCHES, WHERE REQUIRED, MUST BE OF SOUND QUARRY ROCK, PLACED TO A DEPTH OF 1' AND MUST MEET THE FOLLOWING SPECIFICATIONS: 4"-8" ROCK/40%-70% PASSING; 2"-4" ROCK/30%-40% PASSING; AND 1"-2" ROCK/10%-20% PASSING. RECYCLED CONCRETE SHALL NOT BE USED FOR EROSION PROTECTION, INCLUDING CONSTRUCTION ENTRANCE OR TEMPORARY STABILIZATION ELSEWHERE ON THE SITE.
22.

IF ANY PART(S) OF THE CLEARING LIMIT BOUNDARY OR TEMPORARY EROSION/SEDIMENTATION CONTROL PLAN IS/ARE DAMAGED, IT SHALL BE REPAIRED IMMEDIATELY.
23.

ALL PROPERTIES ADJACENT TO THE PROJECT SITE SHALL BE PROTECTED FROM SEDIMENT DEPOSITION AND RUNOFF.
24.

AT NO TIME SHALL MORE THAN 1' OF SEDIMENT BE ALLOWED TO ACCUMULATE WITHIN A CATCH BASIN. ALL CATCH BASINS AND CONVEYANCE LINES SHALL BE CLEANED IMMEDIATELY FOLLOWING REMOVAL OF EROSION CONTROL BMPs. THE CLEANING OPERATION SHALL NOT FLUSH SEDIMENT-LADEN WATER INTO THE DOWNSTREAM SYSTEM.
25.

ANY PERMANENT RETENTION/DETENTION FACILITY USED AS A TEMPORARY SETTLING BASIN SHALL BE MODIFIED WITH THE NECESSARY EROSION CONTROL MEASURES AND SHALL PROVIDE ADEQUATE STORAGE CAPACITY. IF THE PERMANENT FACILITY IS TO FUNCTION ULTIMATELY AS AN INFILTRATION OR DISPERSION SYSTEM, THE FACILITY SHALL NOT BE USED AS A TEMPORARY SETTLING BASIN. NO UNDERGROUND DETENTION TANK, DETENTION VAULT, OR SYSTEM WHICH BACKS UNDER OR INTO A POND SHALL BE USED AS A TEMPORARY SETTLING BASIN.
26.

ALL EROSION/SEDIMENTATION CONTROL PONDS WITH A DEAD STORAGE DEPTH EXCEEDING 6" MUST HAVE A PERIMETER FENCE WITH A MINIMUM HEIGHT OF 3'.
27.

THE WASHED GRAVEL BACKFILL ADJACENT TO THE FILTER FABRIC FENCE SHALL BE REPLACED AND THE FILTER FABRIC CLEANED IF IT IS NONFUNCTIONAL BY EXCESSIVE SILT ACCUMULATION AS DETERMINED BY THE CITY OF KIRKLAND. ALSO, ALL INTERCEPTOR SWALES SHALL BE CLEANED IF SILT ACCUMULATION EXCEEDS ONE-QUARTER DEPTH.
28.

PRIOR TO THE OCTOBER 1 OF EACH YEAR (THE BEGINNING OF THE WET SEASON), ALL DISTURBED AREAS SHALL BE REVIEWED TO IDENTIFY WHICH ONES CAN BE SEEDED IN PREPARATION FOR THE WINTER RAINS. THE IDENTIFIED DISTURBED AREA SHALL BE SEEDED WITHIN ONE WEEK AFTER OCTOBER 1. A SITE PLAN DEPICTING THE AREAS TO BE SEEDED AND THE AREAS TO REMAIN UNCOVERED SHALL BE SUBMITTED TO THE PUBLIC WORKS CONSTRUCTION INSPECTOR. THE INSPECTOR CAN REQUIRE SEEDING OF ADDITIONAL AREAS IN ORDER TO PROTECT SURFACE WATERS, ADJACENT PROPERTIES, OR DRAINAGE FACILITIES.
29.

ANY AREA TO BE USED FOR INFILTRATION OR PERVIOUS PAVEMENT (INCLUDING A 5-FOOT BUFFER) MUST BE SURROUNDED BY SILT FENCE PRIOR TO CONSTRUCTION AND UNTIL FINAL STABILIZATION OF THE SITE TO PREVENT SOIL COMPACTION AND SILTATION BY CONSTRUCTION ACTIVITIES.
30.

IF THE TEMPORARY CONSTRUCTION ENTRANCE OR ANY OTHER AREA WITH HEAVY VEHICLE LOADING IS LOCATED IN THE SAME AREA TO BE USED FOR INFILTRATION OR PERVIOUS PAVEMENT, 6" OF SEDIMENT BELOW THE GRAVEL SHALL BE REMOVED PRIOR TO INSTALLATION OF THE INFILTRATION FACILITY OR PERVIOUS PAVEMENT (TO REMOVE FINES ACCUMULATED DURING CONSTRUCTION).
31.

ANY CATCH BASINS COLLECTING RUNOFF FROM THE SITE, WHETHER THEY ARE ON OR OFF THE SITE, SHALL HAVE ADEQUATE PROTECTION FROM SEDIMENT. CATCH BASINS DIRECTLY DOWNSTREAM OF THE CONSTRUCTION ENTRANCE OR ANY OTHER CATCH BASIN AS DETERMINED BY THE CITY INSPECTOR SHALL BE PROTECTED WITH A "STORM DRAIN PROTECTION INSERT" OR EQUIVALENT.
32.

IF A SEDIMENT POND IS NOT PROPOSED, A BAKER TANK OR OTHER TEMPORARY GROUND AND/OR SURFACE WATER STORAGE TANK MAY BE REQUIRED DURING CONSTRUCTION, DEPENDING ON WEATHER CONDITIONS.
33.

DO NOT FLUSH CONCRETE BY-PRODUCTS OR TRUCKS NEAR OR INTO THE STORM DRAINAGE SYSTEM. IF EXPOSED AGGREGATE IS FLUSHED INTO THE STORM SYSTEM, IT COULD MEAN RE-CLEANING THE ENTIRE DOWNSTREAM STORM SYSTEM, OR POSSIBLY RE-LAYING THE STORM LINE.
34.

RECYCLED CONCRETE SHALL NOT BE STOCKPILED ON SITE, UNLESS FULLY COVERED WITH NO POTENTIAL FOR RELEASE OF RUNOFF.



128TH AVE NE GREENWAY
KIRKLAND GREENWAYS
PROJECT# 25-18-PW
KIRKLAND, WA

PREPARED: AL/TD
CHECKED: CS/KL

DATE
DECEMBER 2019

REV. 1
REV. 2
REV. 3

SHEET NAME
GENERAL NOTES
AND LEGEND

DRAWING NUMBER
NT2

SHEET NUMBER
3 OF 49



CITY OF KIRKLAND STANDARD
STORM DRAINAGE NOTES

1.

A PRE-CONSTRUCTION CONFERENCE SHALL BE HELD PRIOR TO THE START OF CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR SECURING ALL NECESSARY PERMITS PRIOR TO CONSTRUCTION.
2.

BEFORE ANY CONSTRUCTION MAY OCCUR, THE CONTRACTOR SHALL HAVE PLANS WHICH HAVE BEEN SIGNED AND APPROVED BY THE CITY OF KIRKLAND PUBLIC WORKS DEPARTMENT, OBTAINED ALL CITY, COUNTY, STATE, FEDERAL AND OTHER REQUIRED PERMITS, AND HAVE POSTED ALL REQUIRED BONDS.
3.

ALL STORM DRAINAGE IMPROVEMENTS SHALL BE DESIGNED AND CONSTRUCTED IN ACCORDANCE WITH THE LATEST EDITION OF THE CITY OF KIRKLAND PUBLIC WORKS PRE-APPROVED PLANS AND POLICIES AND THE STANDARD SPECIFICATIONS FOR ROAD, BRIDGE AND MUNICIPAL CONSTRUCTION, PREPARED BY WSDOT AND THE AMERICAN PUBLIC WORKS ASSOCIATION (APWA).
4.

ANY DEVIATION FROM THE APPROVED PLANS WILL REQUIRE WRITTEN APPROVAL. ALL CHANGES SHALL BE SUBMITTED TO THE CITY.
5.

A COPY OF THE APPROVED STORM WATER PLANS MUST BE ON THE JOB SITE WHENEVER CONSTRUCTION IS IN PROGRESS.
6.

ALL DISTURBED AREAS SHALL BE SEEDED AND MULCHED OR SIMILARLY STABILIZED TO THE SATISFACTION OF THE CITY OF KIRKLAND DEPARTMENT OF PUBLIC WORKS FOR THE PREVENTION OF ON-SITE EROSION AFTER THE COMPLETION OF CONSTRUCTION.
7.

MINIMUM COVER OVER STORM DRAINAGE PIPES IN ROW OR VEHICULAR PATH SHALL BE 18 INCHES, UNLESS OTHER DESIGN IS APPROVED.
8.

STEEL PIPE SHALL HAVE ASPHALT TREATMENT #1 OR BETTER INSIDE AND OUTSIDE.
9.

ALL CATCH BASINS SHALL BE TYPE I UNLESS OTHERWISE NOTED. CATCH BASINS WITH A DEPTH OF OVER FIVE FEET (5') TO THE PIPE INVERT SHALL BE A TYPE II CATCH BASIN. TYPE II CATCH BASINS EXCEEDING FIVE FEET (5') IN DEPTH SHALL HAVE A STANDARD LADDER INSTALLED.
10.

ALL STORM DRAINAGE MAIN EXTENSIONS WITHIN THE PUBLIC RIGHT-OF-WAY OR IN EASEMENTS MUST BE STAKED FOR LINE AND GRADE PRIOR TO STARTING CONSTRUCTION.
11.

ROCK FOR EROSION PROTECTION OF ROADWAY DITCHES, WHERE REQUIRED, MUST BE OF SOUND QUARRY ROCK, PLACED TO A DEPTH OF ONE FOOT (1') AND MUST MEET THE FOLLOWING SPECIFICATIONS: 4"-8" ROCK/40%-70% PASSING; 2"-4" ROCK/30%-40% PASSING; 2"-MINUS ROCK/10%-20% PASSING. RECYCLED CONCRETE SHALL NOT BE USED FOR EROSION PROTECTION, INCLUDING FOR CONSTRUCTION ENTRANCE OR TEMPORARY STABILIZATION ELSEWHERE ON SITE.
12.

ALL PIPE, MANHOLES, CATCH BASINS, AND APPURTENANCES SHALL BE LAID ON A PROPERLY PREPARED FOUNDATION IN ACCORDANCE WITH THE CURRENT STATE OF WASHINGTON STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION (WSDOT). THIS SHALL INCLUDE NECESSARY LEVELING OF THE TRENCH BOTTOM OR THE TOP OF THE FOUNDATION MATERIAL AS WELL AS PLACEMENT AND COMPACTION OF REQUIRED BEDDING MATERIAL TO UNIFORM GRADE SO THAT THE ENTIRE LENGTH OF THE PIPE WILL BE SUPPORTED ON A UNIFORMLY DENSE, UNYIELDING BASE. IF THE NATIVE MATERIAL IN THE BOTTOM OF THE TRENCH MEETS THE REQUIREMENTS FOR "GRAVEL BACKFILL FOR PIPE BEDDING," THE FIRST LIFT OF PIPE BEDDING MAY BE OMITTED PROVIDED THE MATERIAL IN THE BOTTOM OF THE TRENCH IS LOOSENEED, REGRADED, AND COMPACTED TO FORM A DENSE UNYIELDING BASE. ALL PIPE BEDDING SHALL BE APWA CLASS B, TYPE I, OR BETTER. PIPE SHALL NOT BE INSTALLED ON SOD, FROZEN EARTH, LARGE BOULDERS, OR ROCK. PIPE BEDDING FOR FLEXIBLE PIPES SHALL BE PEA GRAVEL TO THE SPRINGLINE OF THE PIPE.
13.

CONSTRUCTION OF DEWATERING DISCHARGES SHALL ALWAYS MEET WATER QUALITY GUIDELINES LISTED IN COK POLICY E-1. SPECIFICALLY, DISCHARGES TO THE PUBLIC STORMWATER DRAINAGE SYSTEM MUST BE BELOW 25NTU, AND NOT CONSIDERED A PROHIBITED DISCHARGE (PER KMC 15.52.090). TEMPORARY DISCHARGES TO SANITARY SEWER REQUIRE PRIOR AUTHORIZATION AND PERMIT FROM KING COUNTY INDUSTRIAL WASTE PROGRAM (206-263-3000) AND NOTIFICATION TO THE PUBLIC WORKS CONSTRUCTION INSPECTOR.
14.

ISSUANCE OF A BUILDING OR LAND SURFACE MODIFICATION PERMIT BY THE CITY OF KIRKLAND DOES NOT RELIEVE THE OWNER OF THE CONTINUING LEGAL OBLIGATION AND/OR LIABILITY CONNECTED WITH STORM SURFACE WATER DISPOSITION. FURTHER, THE CITY OF KIRKLAND DOES NOT ACCEPT ANY OBLIGATION FOR THE PROPER FUNCTIONING AND MAINTENANCE OF THE SYSTEM DURING OR FOLLOWING CONSTRUCTION EXCEPT AS OUTLINED IN THE CITY OF KIRKLAND PUBLIC WORKS STANDARDS.
15.

ALL TRENCH BACKFILL SHALL BE COMPACTED TO 95 PERCENT DENSITY IN ROADWAYS, ROADWAY SHOULDERS, ROADWAY PRISM AND DRIVEWAYS, AND 85 PERCENT DENSITY IN UNPAVED AREAS. ALL PIPE ZONE COMPACTION SHALL BE 95 PERCENT.
16.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING ADEQUATE SAFEGUARDS, SAFETY DEVICES, PROTECTIVE EQUIPMENT, CONFINED SPACE PROTECTION, FLAGGERS, AND ANY OTHER NEEDED ACTIONS TO PROTECT THE LIFE, HEALTH, AND SAFETY OF THE PUBLIC, AND TO PROTECT PROPERTY IN CONNECTION WITH THE PERFORMANCE OF WORK COVERED BY THE CONTRACT. ANY WORK WITHIN THE TRAVELED RIGHT-OF-WAY THAT MAY INTERRUPT NORMAL TRAFFIC FLOW SHALL REQUIRE A TRAFFIC CONTROL PLAN APPROVED BY THE CITY OF KIRKLAND. ALL SECTIONS OF THE WSDOT STANDARD SPECIFICATIONS, TRAFFIC CONTROL, AND THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) SHALL APPLY.
17.

NO FINAL CUT OR FILL SLOPE SHALL EXCEED SLOPES OF TWO (2) HORIZONTAL TO ONE (1) VERTICAL WITHOUT STABILIZATION BY ROCKERY OR BY A STRUCTURAL RETAINING WALL.
18.

ALL MANHOLE LADDERS SHALL BE FIRMLY ATTACHED AND EXTEND TO WITHIN 1' OF THE BOTTOM OF THE STRUCTURE.
19.

APPROXIMATE LOCATIONS OF EXISTING UTILITIES HAVE BEEN OBTAINED FROM AVAILABLE RECORDS AND ARE SHOWN FOR CONVENIENCE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFICATION OF EXISTING UTILITY LOCATIONS WHETHER OR NOT THESE UTILITIES ARE SHOWN ON THE PLANS. THE CONTRACTOR SHALL EXERCISE ALL CARE TO AVOID DAMAGE TO ANY UTILITY. IF CONFLICTS WITH EXISTING UTILITIES ARISE DURING CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE CITY CONSTRUCTION INSPECTOR AND ANY CHANGES REQUIRED SHALL BE APPROVED BY THE DEVELOPMENT ENGINEER PRIOR TO COMMENCEMENT OF RELATED CONSTRUCTION ON THE PROJECT.
20.

THE UNDERGROUND UTILITY LOCATION SERVICE SHALL BE CONTACTED FOR FIELD LOCATION OF EXISTING UTILITIES PRIOR TO ANY CONSTRUCTION. THE OWNER OR HIS REPRESENTATIVE SHALL BE CONTACTED IF A UTILITY CONFLICT EXISTS. FOR UTILITY LOCATION IN KING COUNTY, CALL 1-800-424-5555. THE CONTRACTOR IS RESPONSIBLE TO ENSURE THAT UTILITY LOCATES ARE MAINTAINED THROUGHOUT THE LIFE OF THE PROJECT.

CITY OF KIRKLAND STANDARD
STORM DRAINAGE NOTES (CONT)

21.

THE CONTRACTOR SHALL VERIFY THE LOCATIONS, WIDTHS, THICKNESSES, AND ELEVATIONS OF ALL EXISTING PAVEMENTS AND STRUCTURES THAT ARE TO INTERFACE WITH NEW WORK. PROVIDE ALL TRIMMING, CUTTING, SAW CUTTING, GRADING, LEVELING, SLOPING, COATING, AND OTHER WORK, INCLUDING MATERIALS AS NECESSARY, TO CAUSE THE INTERFACE WITH EXISTING WORKS TO BE PROPER, ACCEPTABLE TO THE ENGINEER AND THE CITY OF KIRKLAND, COMPLETE IN PLACE AND READY TO USE.
22.

ALL INLET, MANHOLE, AND CATCH BASIN FRAMES AND GRATES SHALL NOT BE ADJUSTED TO GRADE UNTIL IMMEDIATELY PRIOR TO FINAL PAVING. ALL CATCH BASIN GRATES SHALL BE SET 0.10' BELOW PAVEMENT LEVEL.
23.

OPEN CUT ROAD CROSSINGS FOR UTILITY TRENCHES ON EXISTING TRAVELED ROADWAY SHALL BE BACKFILLED ONLY WITH 5/8" MINUS CRUSHED ROCK AND MECHANICALLY COMPACTED (UNLESS OTHERWISE APPROVED BY THE CITY). FOR STREETS CLASSIFIED AS ARTERIALS OR COLLECTORS, BACKFILL FOR CROSSINGS SHALL BE CONTROL DENSITY FILL (CDF). CUTS INTO THE EXISTING ASPHALT SHALL BE NEAT LINE CUT WITH SAW OR JACKHAMMER IN A CONTINUOUS LINE. A TEMPORARY COLD MIX PATCH MUST BE PLACED IMMEDIATELY AFTER BACKFILL AND COMPACTION. A PERMANENT HOT MIX PATCH SHALL BE PLACED WITHIN 30 DAYS AND SHALL BE A MINIMUM OF 1" THICKER THAN THE ORIGINAL ASPHALT WITH A MINIMUM THICKNESS OF 2". SEE STANDARD D.02.
24.

ALL DAMAGES INCURRED TO PUBLIC AND/OR PRIVATE PROPERTY BY THE CONTRACTOR DURING THE COURSE OF CONSTRUCTION SHALL BE PROMPTLY REPAIRED TO THE SATISFACTION OF THE CITY CONSTRUCTION INSPECTOR BEFORE PROJECT APPROVAL AND/OR THE RELEASE OF THE PROJECT'S PERFORMANCE BOND.
25.

GROUT ALL SEAMS AND OPENINGS IN ALL INLETS, CATCH BASINS, AND MANHOLES. JETSET GROUT IS NOT ALLOWED.
26.

WHEN WIDENING AN EXISTING ROADWAY WHERE AN EXISTING TYPE I CATCH BASIN WILL REMAIN IN THE TRAVEL LANE, THE EXISTING FRAME AND COVER SHALL BE REPLACED WITH A ROUND, LOCKING FRAME AND COVER.
27.

FOR OTHER THAN SINGLE-FAMILY DWELLINGS, ALL EXPOSED OR READILY EXPOSED INDOOR STORM DRAINAGE PIPING/PLUMBING SHALL BE LABELED WITH THE WORDS "STORM DRAIN" WITH MINIMUM 2 INCH HIGH LETTERS.
28.

RECYCLED CONCRETE SHALL NOT BE USED AROUND STORMWATER FACILITIES.

CHANNELIZATION AND SIGNING NOTES

1.

THE CONTRACTOR SHALL FURNISH AND INSTALL ALL SIGNAGE UNLESS NOTED OTHERWISE ON THE PLANS.
2.

THE D11-1 SIGNS ARE PLACEHOLDERS AND WILL BE REPLACED WITH CUSTOM GREENWAY SIGNS. COORDINATE THE GREENWAY SIGNS WITH CITY ENGINEER.
3.

ALL STRIPING LINES SHALL BE PAINT. ALL OTHER PAVEMENT MARKINGS SHALL BE PLASTIC.
4.

SIGN LOCATIONS SHALL BE VERIFIED BY THE ENGINEER PRIOR TO INSTALLATION. SIGN LOCATIONS SHOWN ARE APPROXIMATE ONLY. ACTUAL LOCATIONS SHALL BE DETERMINED IN THE FIELD BASED ON CONDITIONS. SIGNS SHALL NOT BE PLACED TO CONFLICT WITH DRIVEWAYS, UTILITIES, OR VEGETATION. SIGNS SHALL NOT BE PLACED WHERE SIGHT LINES TO THE SIGN IS BLOCKED BY TREES.
5.

CONTRACTOR SHALL REMOVE EXISTING PAVEMENT STRIPING, MARKING AND SIGNAGE IN CONFLICT WITH NEW CHANNELIZATION.
6.

ALL CHANNELIZATION DIMENSIONS ADJACENT TO CURB ARE MEASURED TO THE FACE OF CURB.
7.

SOLID BICYCLE LANE LINE STRIPES ARE WHITE, 6 INCHES WIDE UNLESS OTHERWISE NOTED.

LANDSCAPE AND RESTORATION NOTES

1.

PROTECT EXISTING TREES PER SITE PREPARATION NOTES AND PLANS. PROTECT ALL VEGETATION ON PRIVATE PROPERTY.
2.

TRIM AND REMOVE VEGETATION AND TREES PER SITE PREPARATION NOTES AND PLANS.
3.

RESTORE EXISTING LANDSCAPE IMPACTED BY CONSTRUCTION TO EXISTING CONDITIONS OR BETTER PER SPECIFICATIONS. RESTORED LANDSCAPE TO MEET AND MATCH EXISTING LANDSCAPE GRADES AND PAVEMENT EDGES.

CHANNELIZATION AND SIGNING LEGEND

| EXISTING | PROPOSED | DETAIL |
|----------|----------|--|
| | 4" SYL | 4" SOLID YELLOW LINE |
| | 4" DYL | 4" DASHED YELLOW LINE |
| | | 4" YELLOW TWO-WAY LEFT TURN LANE LINES |
| | 6" SWL | 6" SOLID WHITE LINE |
| | 6" DWL | 6" DASHED WHITE LINE |
| | 6" DYL | 6" DASHED YELLOW LINE |
| | 6" DSYCL | 6" DOUBLE SOLID YELLOW CENTER LINE |
| | 12" MMC | 12" MOUNTABLE MEDIAN CURB |
| | | SIGN AND POST (POST HT. PER PLANS) |
| | | SIGN |
| | | BICYCLE LANE SYMBOL |
| | | SHARED LANE MARKING (SHARROW) |
| | | BI-DIRECTIONAL SHARROW |
| | | MODIFIED BI-DIRECTIONAL SHARROW |
| | | BIKE DOT |
| | | WHITE PAVEMENT MARKING |
| | | BICYCLE CROSSING PAVEMENT MARKING |
| | | FLEXIBLE DELINEATOR POST |
| | | 10' CROSSWALK |
| | | 18" STOP BAR |
| | | YIELD SYMBOL PAVEMENT MARKING |

ABBREVIATIONS

| | |
|--|--|
| ±/- = PLUS OR MINUS ASPH = ASPHALT ASTM = AMERICAN STANDARD TEST METHOD APS = ACCESSIBLE PEDESTRIAN SIGNAL AVE = AVENUE BC = BOTTOM OF CURB BLDG = BUILDING BP = BEGINNING POINT BW = BOTTOM OF WALL CB = CATCH BASIN CL = CENTERLINE COK = CITY OF KIRKLAND CONC = CONCRETE CRZ = CRITICAL ROOT ZONE CW = CROSSWALK CY = CUBIC YARD DIA = DIAMETER DSYCL = DOUBLE SOLID YELLOW CENTERLINE DW = DRIVEWAY DWG = DRAWING DWL = DASHED WHITE LINE DYL = DASHED YELLOW LINE E = EAST ELEV = ELEVATION EP = END POINT EQ. = EQUAL ESC = EROSION AND SEDIMENT CONTROL EX/EXIST. = EXISTING FT = FOOT GIS = GEOGRAPHIC INFORMATION SYSTEMS HMA = HOT MIX ASPHALT H-V = HORIZONTAL TO VERTICAL IN. = INCH LF = LINEAR FOOT LS = LUMP SUM LT = LEFT MAX = MAXIMUM | MIN = MINIMUM MMC = MOUNTABLE MEDIAN CURB MPH = MILE PER HOUR MUTCD = MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES N = NORTH NO. = NUMBER N.T.S. = NOT TO SCALE O.C. = ON CENTER OFF = OFFSET PC = POINT OF CURVATURE P.E. = PROFESSIONAL ENGINEER PI = POINT OF INTERSECTION PL = PLACE PT = POINT OF TANGENCY R = RADIUS ROW = RIGHT-OF-WAY RRFB = RECTANGULAR RAPID FLASHING BEACON RT = RIGHT S = SOUTH SB = STOP BAR SF = SQUARE FOOT SPEC = SPECIFICATION STA = STATION STD = STANDARD SQ = SQUARE SWL = SOLID WHITE LINE SY = SQUARE YARD TC = TOP OF CURB TCE = TEMPORARY CONSTRUCTION EASEMENT TW = TOP OF WALL TYP = TYPICAL U.N.O. = UNLESS NOTED OTHERWISE W = WEST W/ = WITH WSDOT = WASHINGTON STATE DEPARTMENT OF TRANSPORTATION |
|--|--|

PREPARED: AL/TD

CHECKED: CS/KL

DATE
DECEMBER 2019

REV. 1

REV. 2

REV. 3

SHEET NAME
**GENERAL NOTES
AND LEGEND**

DRAWING NUMBER

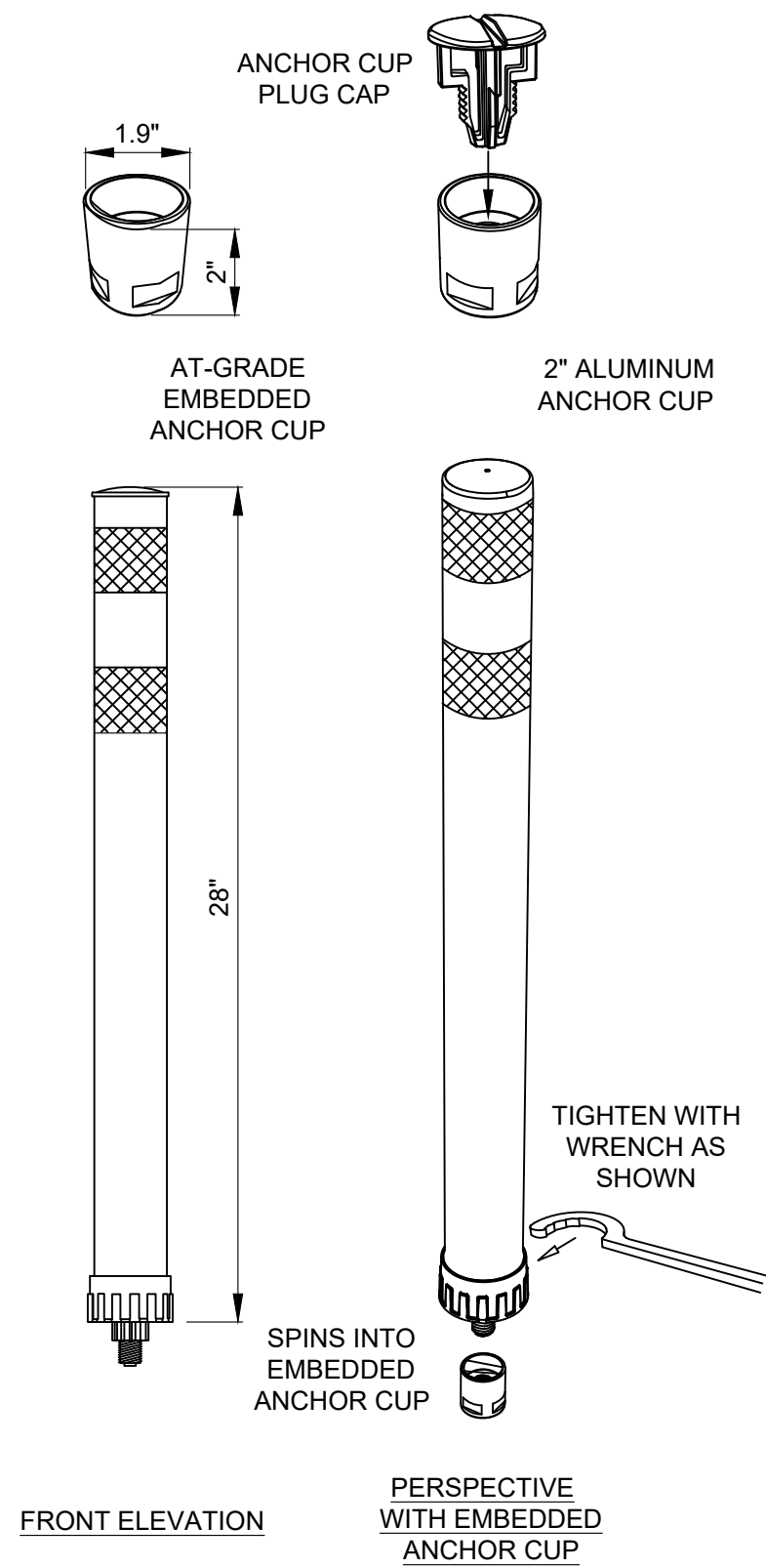
NT3

SHEET NUMBER

4 OF **49**

100% SUBMITTAL - NOT FOR CONSTRUCTION

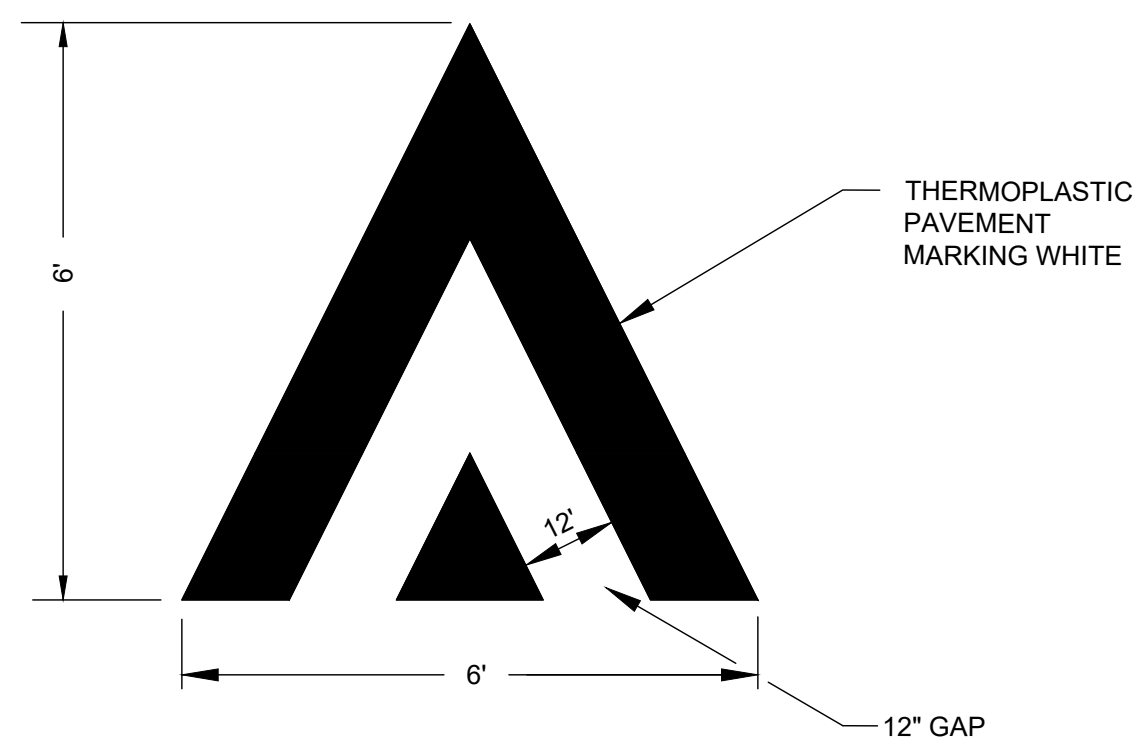




FLEXIBLE DELINEATOR POST

N.T.S.

1
TYP

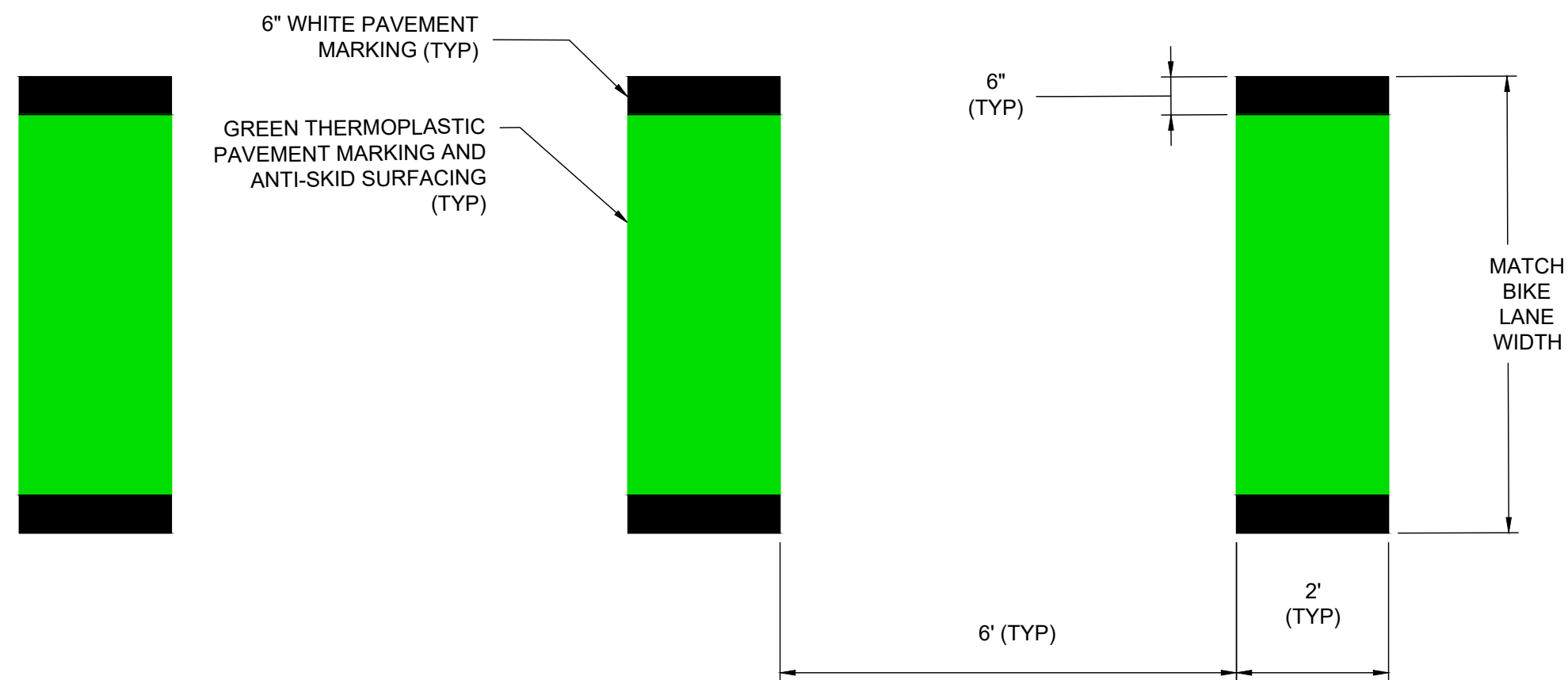


NOTE: THIS CHEVRON DETAIL IS FOR RAISED INTERSECTIONS ONLY. CHEVRON PAVEMENT MARKINGS AT SPEED HUMPS AND SLOTTED SPEED HUMPS SHALL FOLLOW PRE-APPROVED PLAN NO CK-R.67 AND CK-R.67B

RAISED INTERSECTION CHEVRON PAVEMENT MARKING

N.T.S.

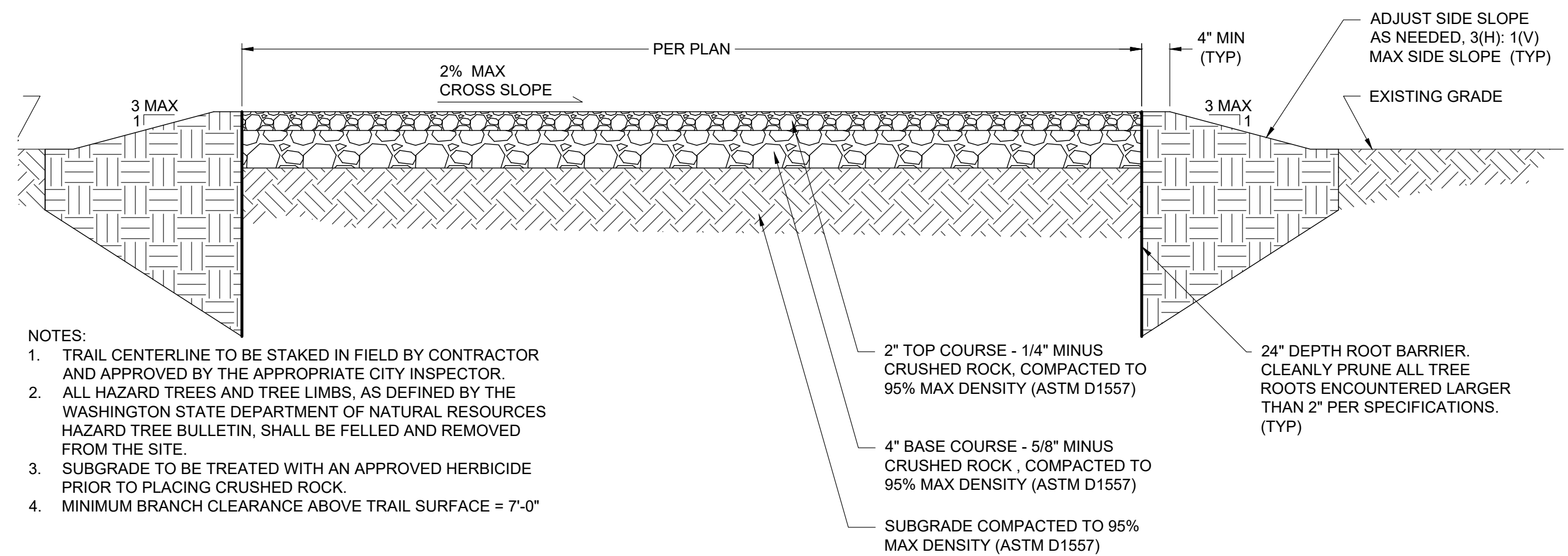
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TYP



TYPICAL BICYCLE CROSSING PAVEMENT MARKING

N.T.S.

2
TYP

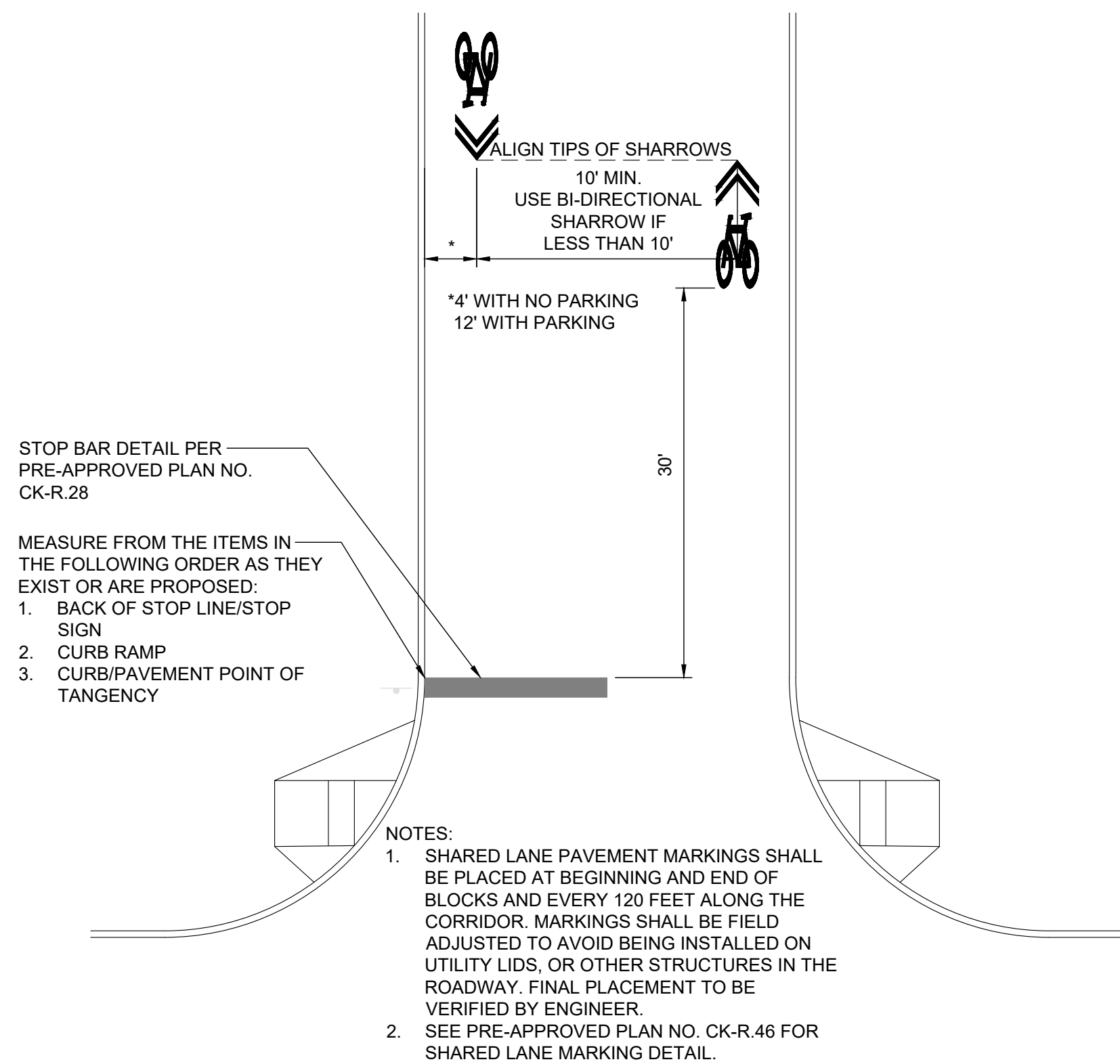


- NOTES:
1. TRAIL CENTERLINE TO BE STAKED IN FIELD BY CONTRACTOR AND APPROVED BY THE APPROPRIATE CITY INSPECTOR.
 2. ALL HAZARD TREES AND TREE LIMBS, AS DEFINED BY THE WASHINGTON STATE DEPARTMENT OF NATURAL RESOURCES HAZARD TREE BULLETIN, SHALL BE FELLED AND REMOVED FROM THE SITE.
 3. SUBGRADE TO BE TREATED WITH AN APPROVED HERBICIDE PRIOR TO PLACING CRUSHED ROCK.
 4. MINIMUM BRANCH CLEARANCE ABOVE TRAIL SURFACE = 7'-0"

CRUSHED SURFACING PATH

N.T.S.

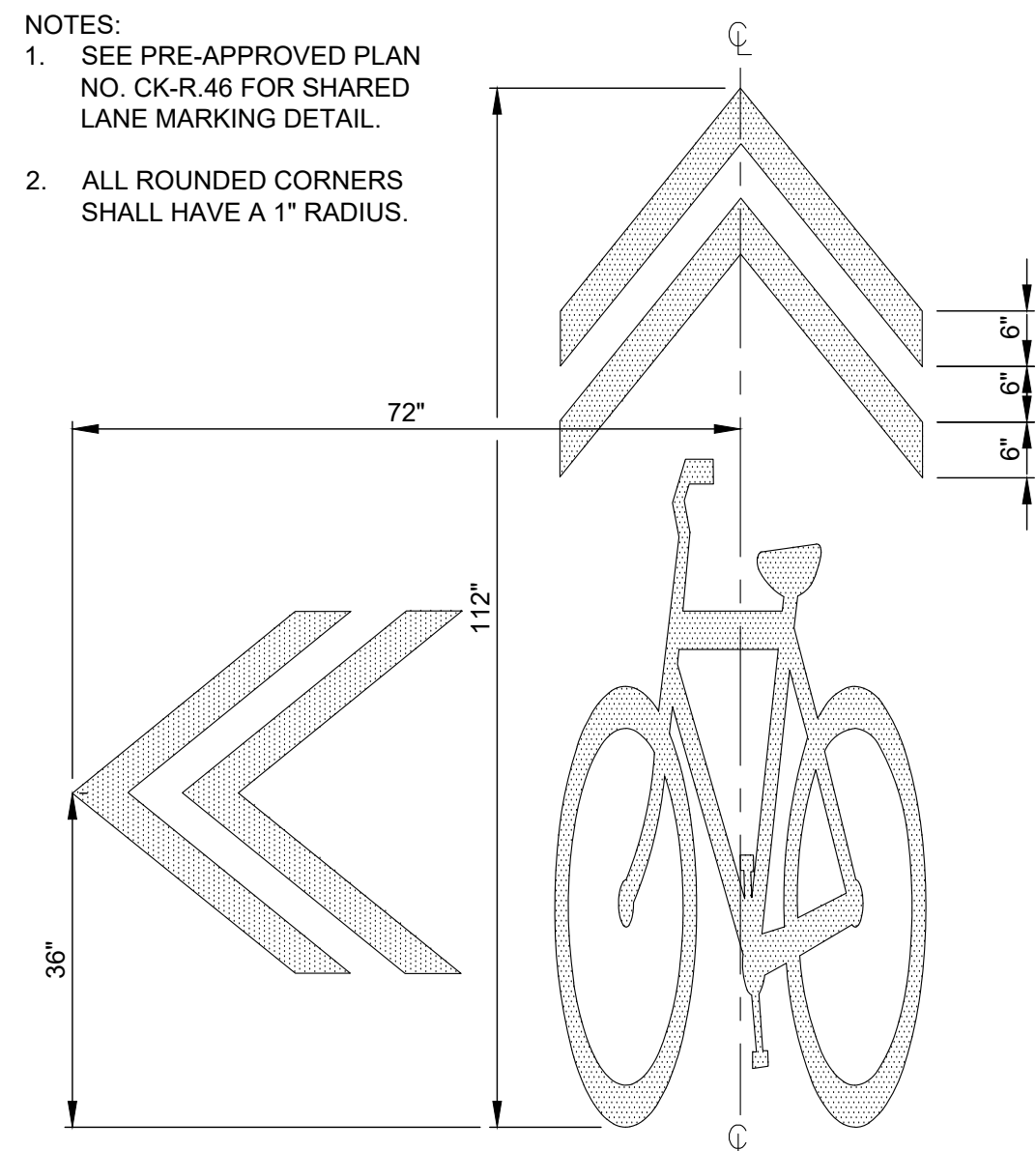
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TYP



SHARED LANE PAVEMENT MARKING (SHARROW) PLACEMENT

1
TYP

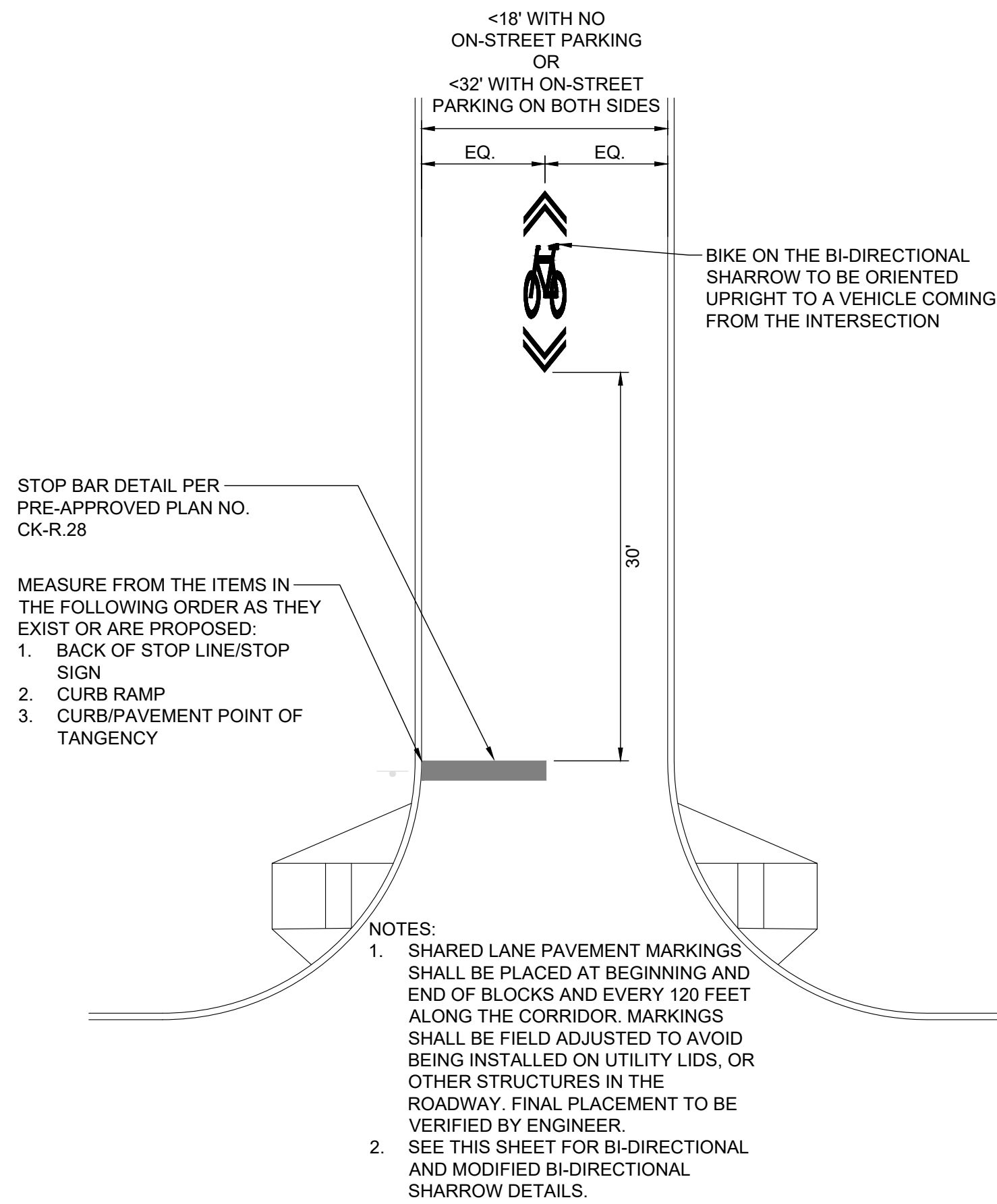
N.T.S.



MODIFIED BI-DIRECTIONAL SHARROW DETAIL

4
TYP

N.T.S.

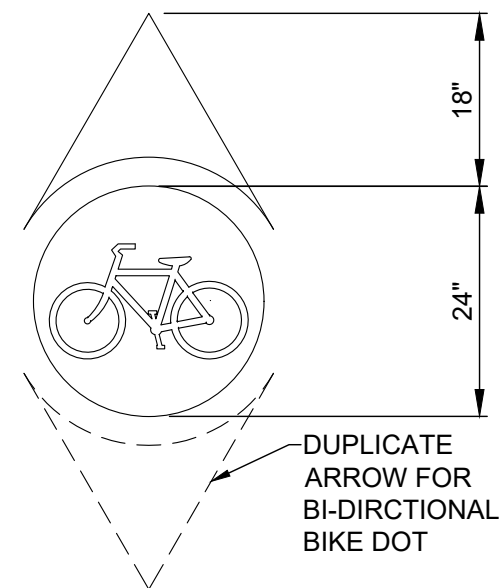


BI-DIRECTIONAL SHARROW PLACEMENT

2
TYP

N.T.S.

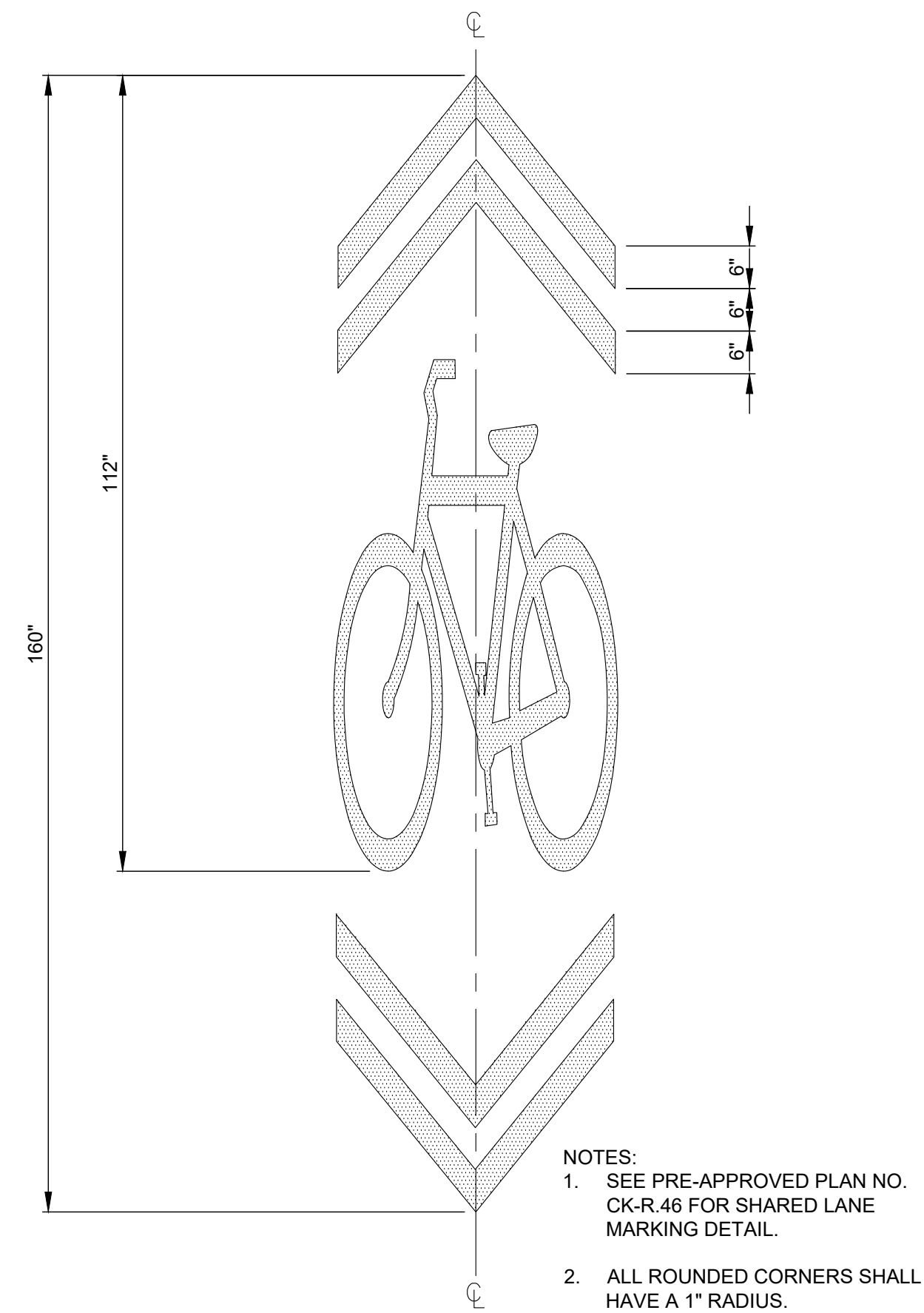
- NOTES:
1. THE BIKE DOT ORIENTATION SHALL BE THE SAME DIRECTION AS ACCOMPANY ARROW UNLESS SPECIFIED.
 2. MARKING SHALL BE THERMOPLASTIC, HEAT FUSED PREFORMED, 90 MIL., OR EQUAL APPROVED BY ENGINEER.



BIKE DOT WITH ARROW DETAIL

5
TYP

N.T.S.



BIDIRECTIONAL BIKE SHARROW DETAIL

3
TYP

N.T.S.



100% SUBMITTAL - NOT FOR CONSTRUCTION

TOOLE
DESIGN
720 3RD AVE SUITE 2020
SEATTLE, WA 98104
PHONE: (206) 297-1601
FAX: (301) 927-2800
www.tooledesign.com

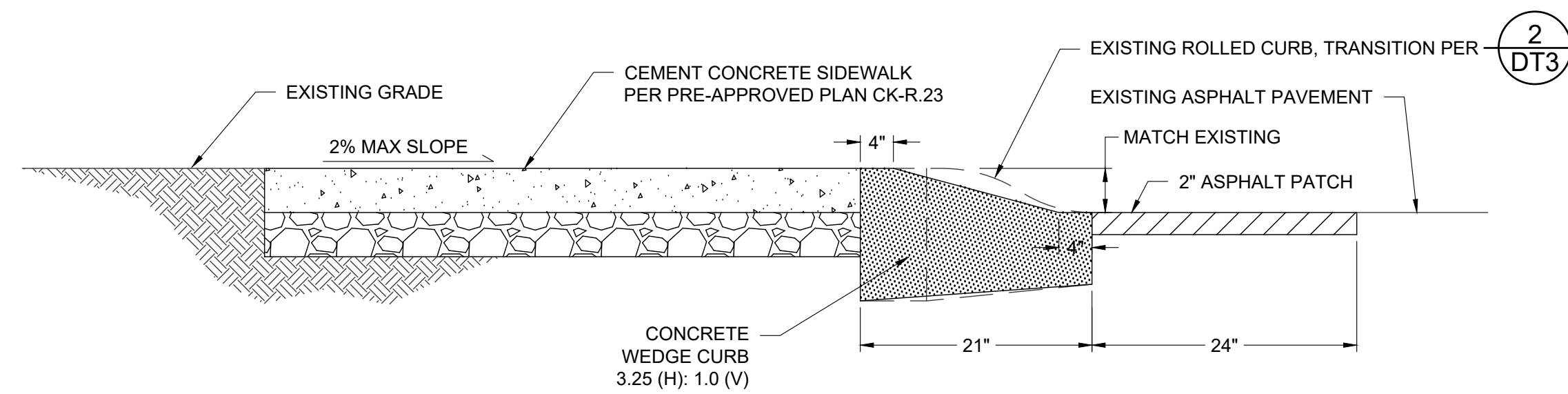


128TH AVE NE GREENWAY
KIRKLAND GREENWAYS
PROJECT# 25-18-PW
KIRKLAND, WA

PREPARED: AL/TD
CHECKED: CS/KL
DATE
DECEMBER 2019
REV. 1
REV. 2
REV. 3
SHEET NAME
DETAILS

DRAWING NUMBER
DT2

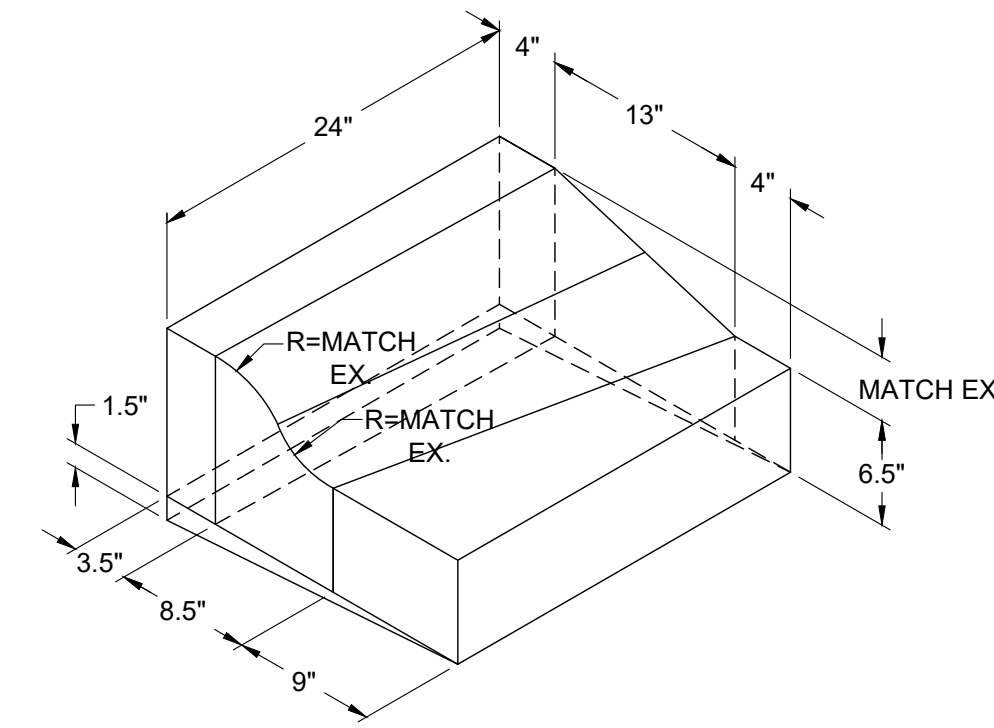
SHEET NUMBER
6 OF **49**



WEDGE CURB DETAIL

N.T.S.

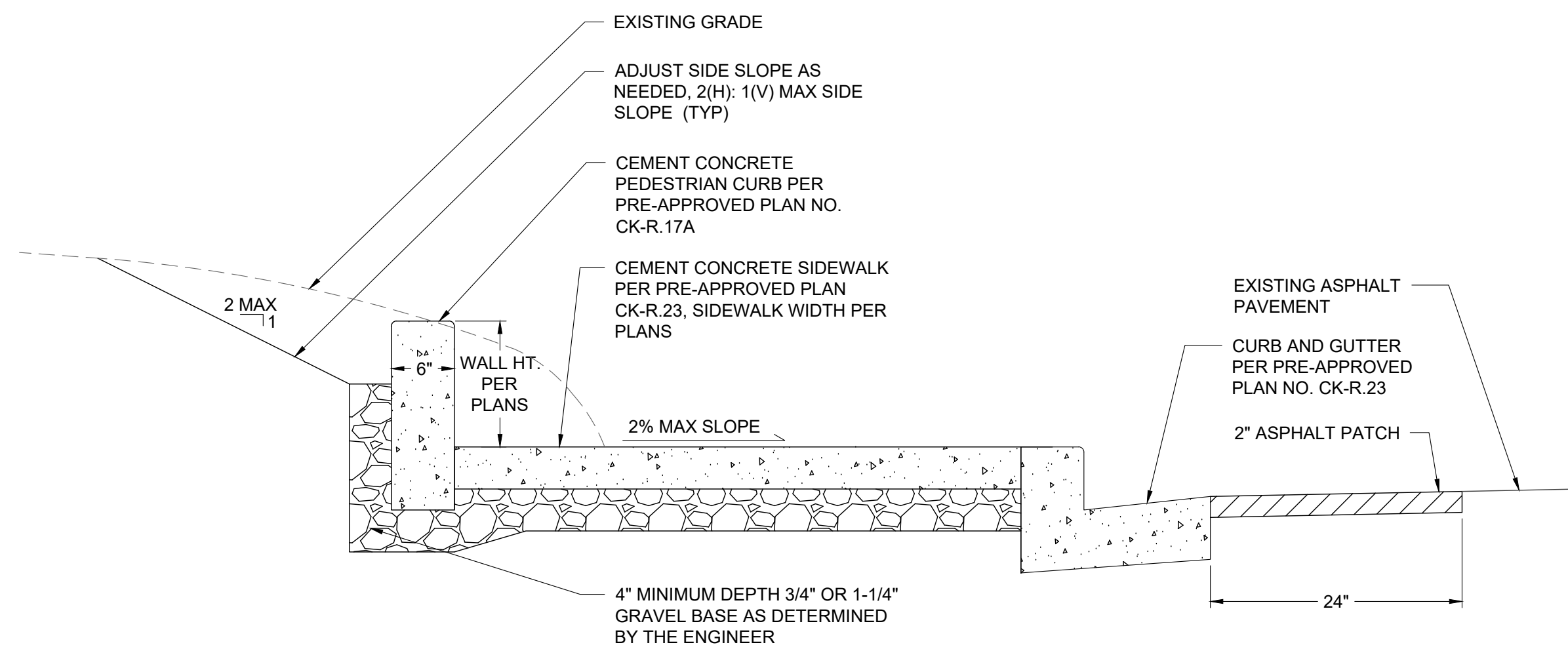
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TYP



EXISTING ROLLED CURB TO PROPOSED WEDGE CURB TRANSITION DETAIL

N.T.S.

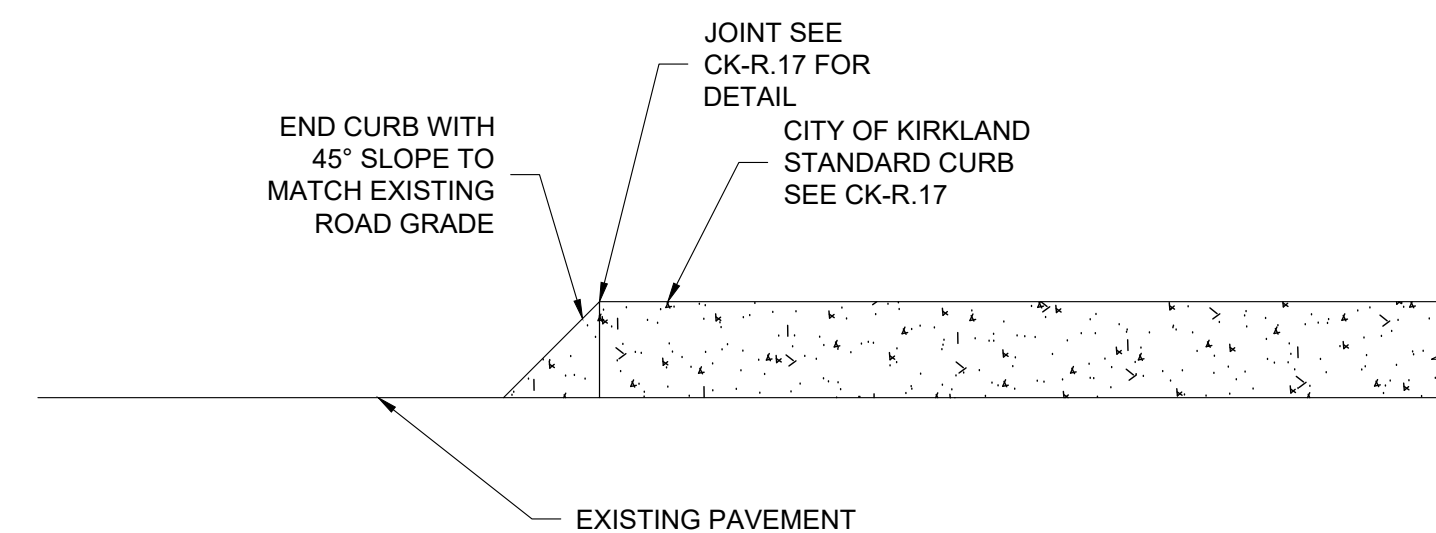
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TYP



CURB WALL DETAIL

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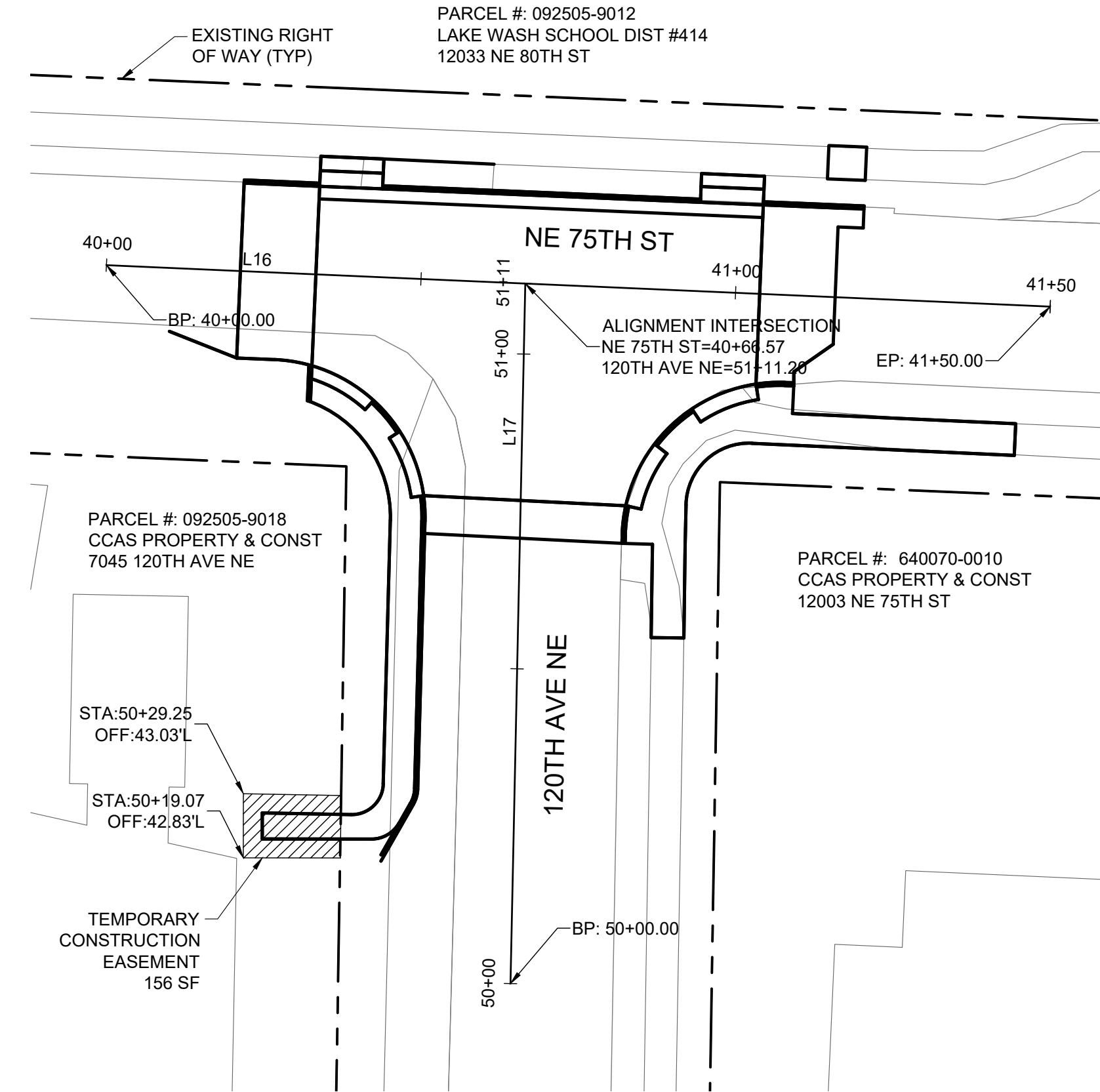
3
TYP



45° END OF CURB CHAMFER DETAIL

N.T.S.

4
TYP



| 75th Street at 120th Avenue 120th Avenue | | | | | | | | |
|---|----------|-------|--------|---------|--------|------------------|-------------|--------------|
| SEGMENT NO. | STATION | DELTA | RADIUS | TANGENT | LENGTH | BEARING | NORTHING | EASTING |
| L16 | 40+00.00 | | | | 150.00 | S87° 29' 07.35"E | 247965.7559 | 1308538.2454 |

| SURVEY CONTROL | | | | | |
|----------------|------------|-----------|-------------|-------------|--------------|
| NORTHING | EASTING | ELEVATION | DESCRIPTION | STREET A | STREET B |
| 249190.04 | 1311304.50 | 429.71 | MON IN CASE | NE 80TH ST | 128TH AVE NE |
| 250685.50 | 1311296.69 | 383.40 | MON IN CASE | NE 85TH ST | 128TH AVE NE |
| 253150.22 | 1311207.35 | 276.68 | MON IN CASE | NE 95TH ST | 128TH AVE NE |
| 255716.40 | 1311176.75 | 309.50 | MON IN CASE | NE 104TH ST | 128TH AVE NE |
| 258659.47 | 1309874.88 | 228.94 | MON IN CASE | NE 112TH PL | 124TH AVE NE |

SURVEY CONTROL NOTES:

HORIZONTAL BASIS OF BEARINGS: NAD 83/91

THE WASHINGTON STATE PLANE COORDINATE SYSTEM, NORTH ZONE, NORTH AMERICAN DATUM OF NAD 83/91, (US SURVEY FEET). MEAN PROJECT COMBINED SCALE FACTOR = 0.999970114

VERTICAL DATUM:

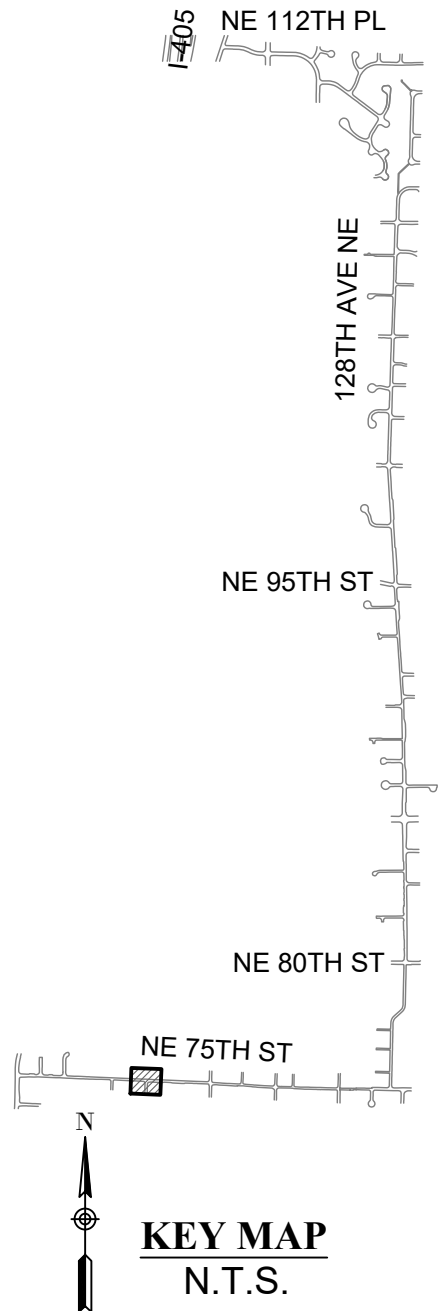
THE VERTICAL DATUM IS NAVD88, GPS DERIVED FROM THE WSRN-CORS USING GEOID12B MODEL.

METHODOLOGY:

3 INDEPENDENT HORIZONTAL-VERTICAL CONTROL POINT POSITIONS PER STREET INTERSECTION WERE DERIVED FROM DIRECT REAL TIME NETWORK (RTN) GPS OBSERVATIONS, UTILIZING THE WASHINGTON STATE REFERENCE NETWORK (WSRN) IN THE REALIZATION OF NAD 83/2011 EPOCH 2010.00.

A MEAN NORTHING AND EASTING COORDINATE SHIFT FROM NAD 83/2011 TO NAD 83/91 WAS DETERMINED FROM 3 PUBLISHED WSDOT DATA SHEETS AND 1 CITY OF BELLEVUE DATA SHEET, LOCAL TO THE PROJECT AREA. A SHIFT OF -0.342 IN NORTHING AND A SHIFT OF -0.354 IN EASTING WERE APPLIED TO THE NAD 83/2011 COORDINATES TO YIELD NAD 83/91 COORDINATES.

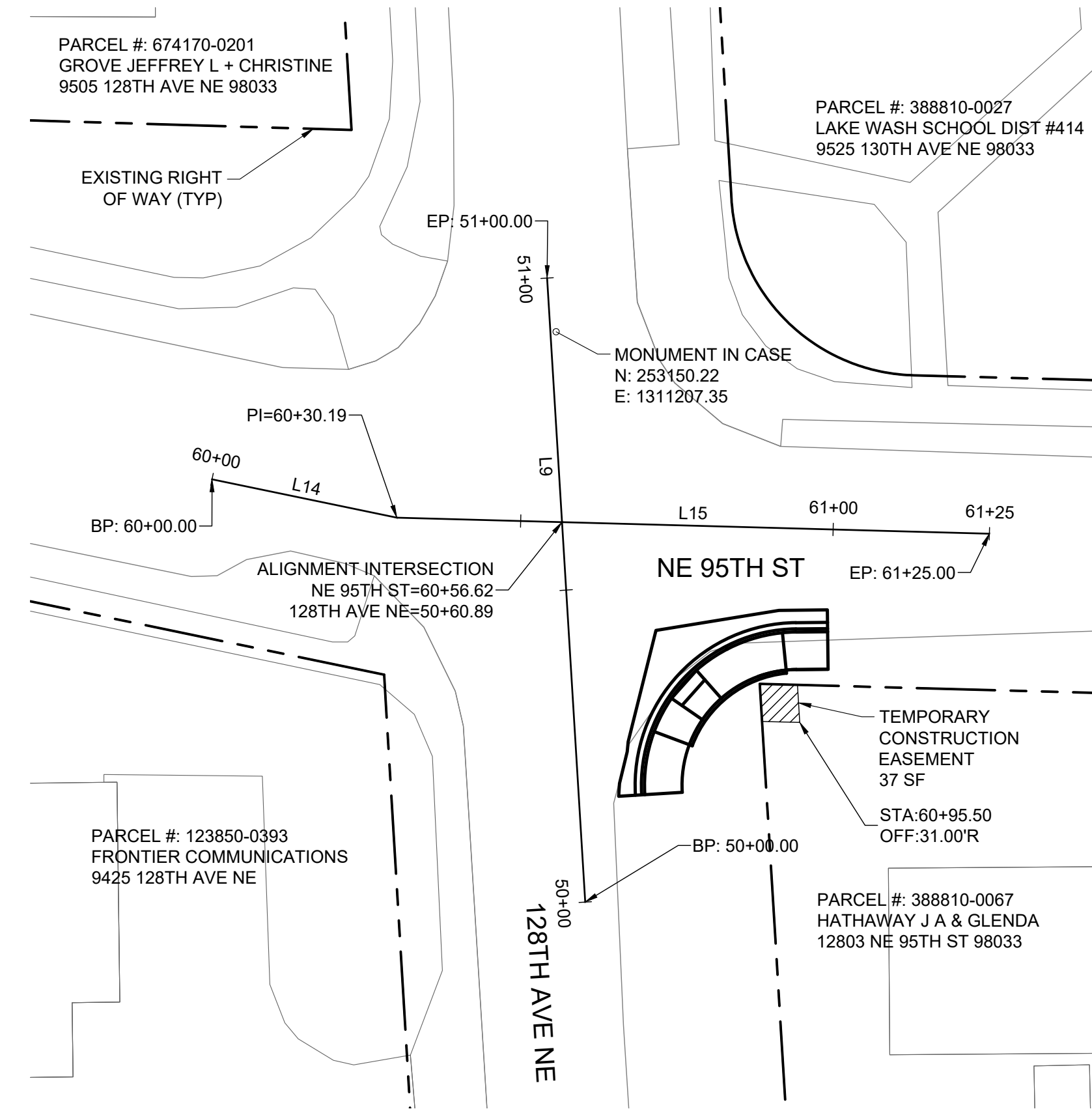
DATE VISITED
BETWEEN DECEMBER 2017, AND JANUARY 2018.



128TH AVE NE GREENWAY
KIRKLAND GREENWAYS
PROJECT# 25-18-PW
KIRKLAND, WA



PREPARED: AL/TD
CHECKED: CS/KL
DATE
DECEMBER 2019
REV. 1
REV. 2
REV. 3
SHEET NAME
ALIGNMENT AND
RIGHT-OF-WAY PLAN
DRAWING NUMBER
AL1
SHEET NUMBER
8 OF 49



| SURVEY CONTROL | | | | | |
|----------------|------------|-----------|-------------|-------------|--------------|
| NORTHING | EASTING | ELEVATION | DESCRIPTION | STREET A | STREET B |
| 249190.04 | 1311304.50 | 429.71 | MON IN CASE | NE 80TH ST | 128TH AVE NE |
| 250685.50 | 1311296.69 | 383.40 | MON IN CASE | NE 85TH ST | 128TH AVE NE |
| 253150.22 | 1311207.35 | 276.68 | MON IN CASE | NE 95TH ST | 128TH AVE NE |
| 255716.40 | 1311176.75 | 309.50 | MON IN CASE | NE 104TH ST | 128TH AVE NE |
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VERTICAL DATUM:

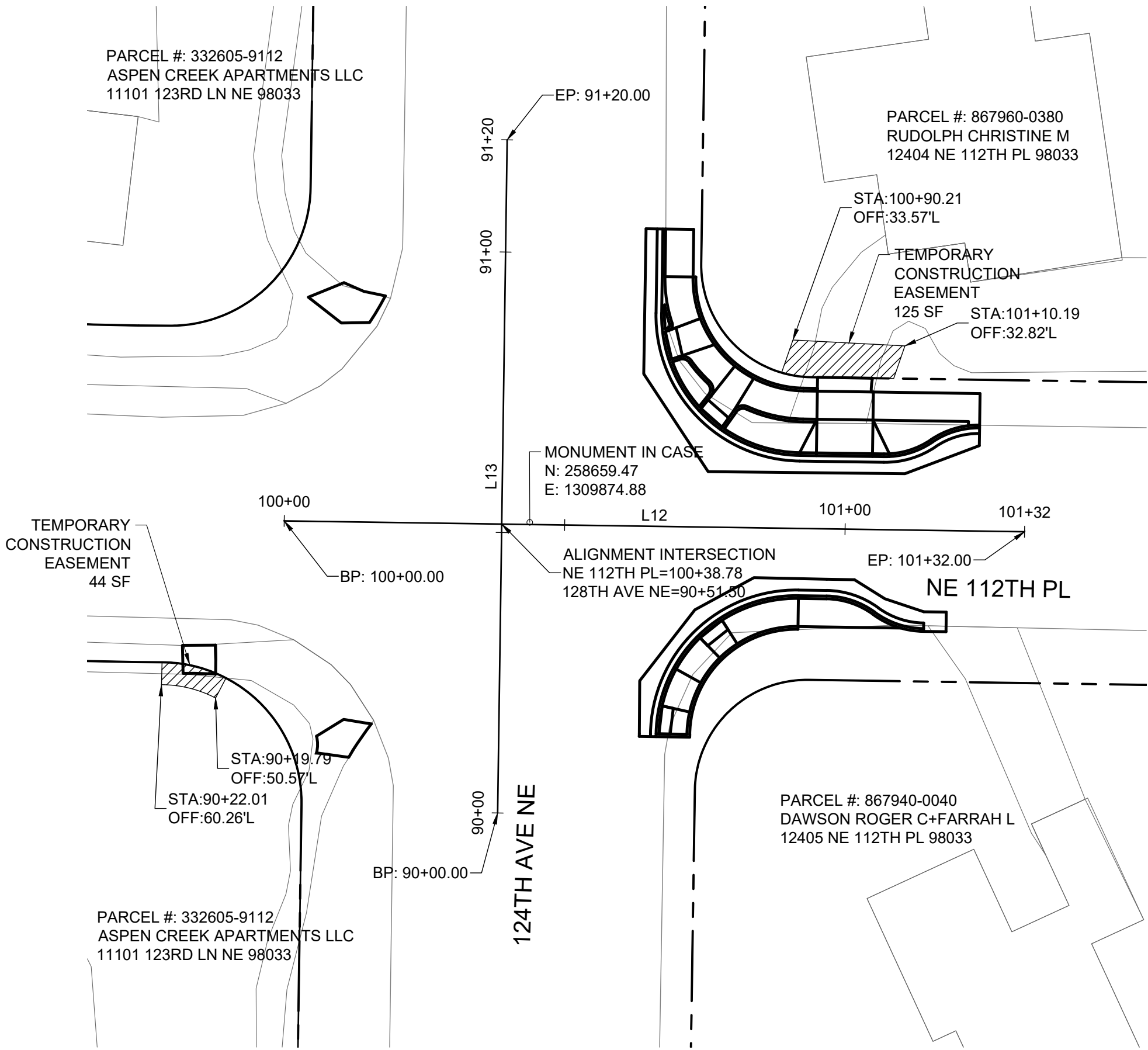
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DATE VISITED
BETWEEN DECEMBER 2017, AND JANUARY 2018.

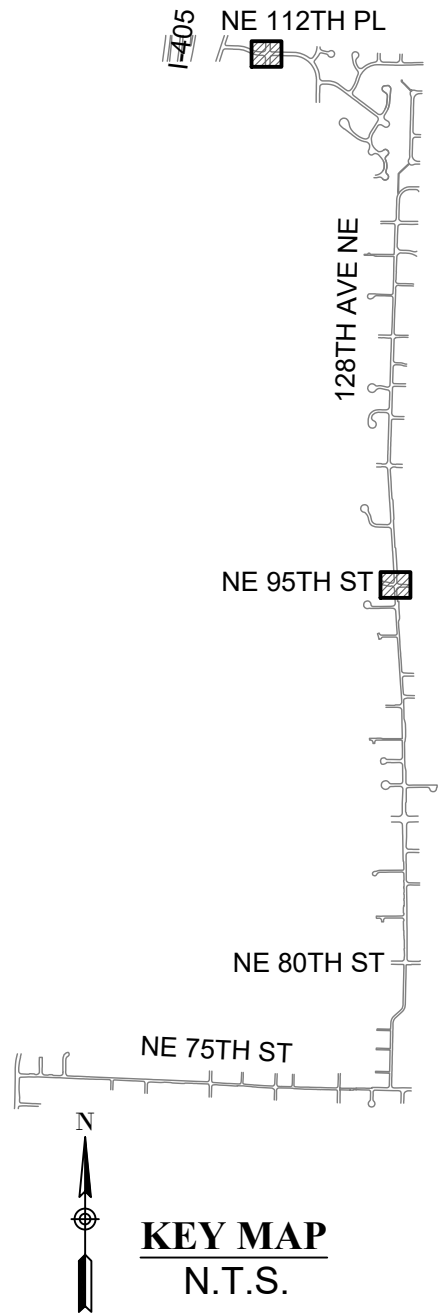


| 128th Avenue @ 95th | | | | | | | | |
|---------------------|----------|-------|--------|---------|--------|-----------------|-------------|--------------|
| SEGMENT NO. | STATION | DELTA | RADIUS | TANGENT | LENGTH | BEARING | NORTHING | EASTING |
| L9 | 50+00.00 | | | | 100.00 | N3° 29' 47.20"W | 253058.9985 | 1311212.0056 |

| NE 112 PI | | | | | | | | |
|-------------|-----------|-------|--------|---------|--------|------------------|-------------|--------------|
| SEGMENT NO. | STATION | DELTA | RADIUS | TANGENT | LENGTH | BEARING | NORTHING | EASTING |
| L12 | 100+00.00 | | | | 132.00 | S89° 09' 44.26"E | 258659.7336 | 1309831.1106 |

| 124th Ave NE | | | | | | | | |
|--------------|----------|-------|--------|---------|--------|-----------------|-------------|--------------|
| SEGMENT NO. | STATION | DELTA | RADIUS | TANGENT | LENGTH | BEARING | NORTHING | EASTING |
| L13 | 90+00.00 | | | | 120.00 | N0° 48' 33.10"E | 258607.6699 | 1309869.1600 |

| NE 95th Street | | | | | | | | |
|----------------|----------|-------|--------|---------|--------|------------------|-------------|--------------|
| SEGMENT NO. | STATION | DELTA | RADIUS | TANGENT | LENGTH | BEARING | NORTHING | EASTING |
| L14 | 60+00.00 | | | | 30.19 | S78° 17' 46.79"E | 253126.6133 | 1311152.3129 |
| L15 | 60+30.19 | | | | 94.82 | S88° 26' 51.36"E | 253120.4903 | 1311181.8706 |



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128TH AVE NE GREENWAY
KIRKLAND GREENWAYS
PROJECT# 25-18-PW
KIRKLAND, WA



PREPARED: AL/TD
CHECKED: CS/KL

DATE
DECEMBER 2019

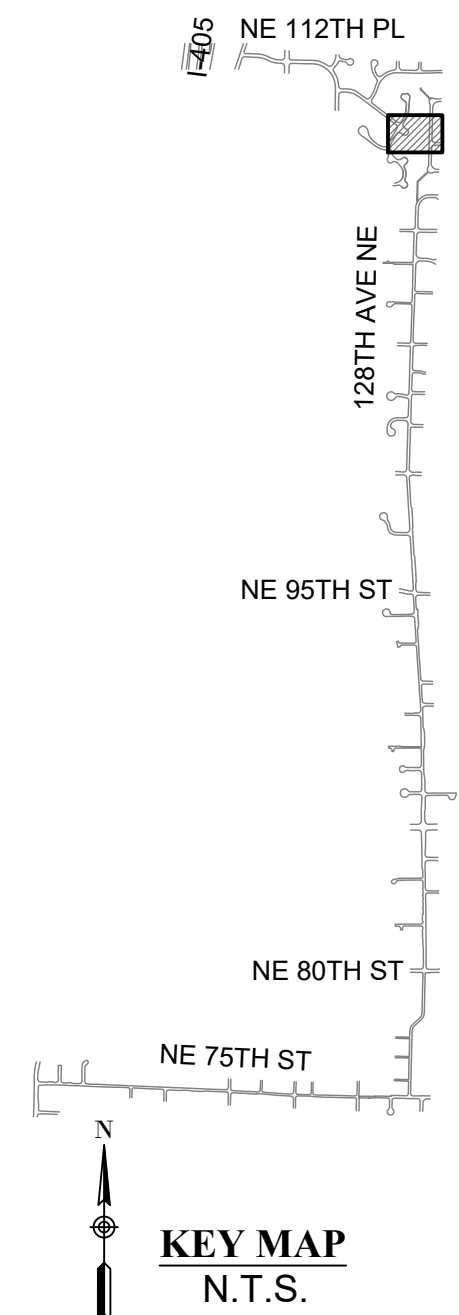
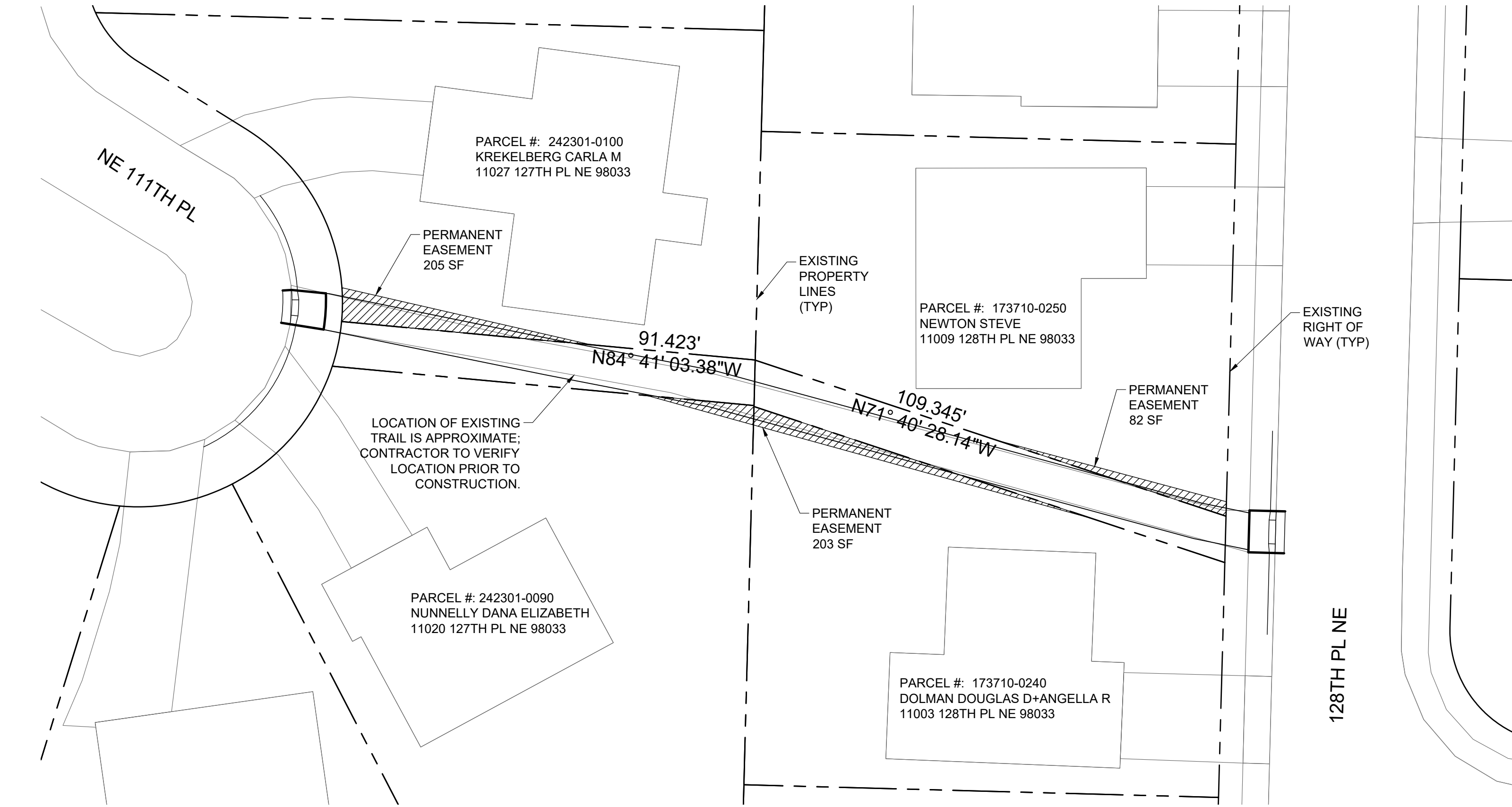
REV. 1
REV. 2
REV. 3

SHEET NAME
**ALIGNMENT AND
RIGHT-OF-WAY PLAN**

DRAWING NUMBER

AL2

SHEET NUMBER
9 OF **49**



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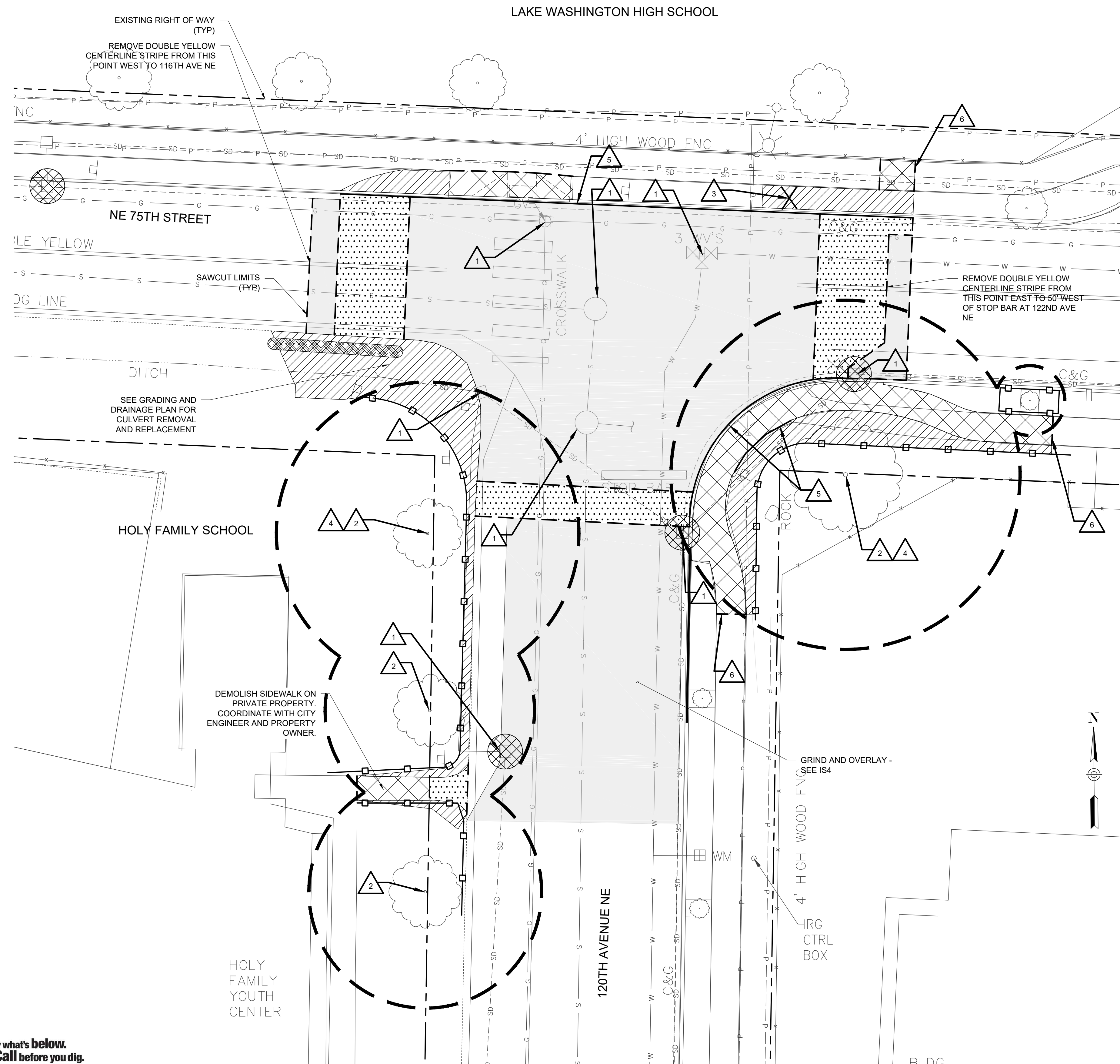
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720 3RD AVE SUITE 2020
SEATTLE, WA 98104
PHONE: (206) 297-1601
FAX: (206) 297-2800
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128TH AVE NE GREENWAY
KIRKLAND GREENWAYS
PROJECT# 25-18-PW
KIRKLAND, WA



PREPARED: AL/TD
CHECKED: CS/KL
DATE
DECEMBER 2019
REV. 1
REV. 2
REV. 3
SHEET NAME
ALIGNMENT AND
RIGHT OF WAY PLAN
DRAWING NUMBER
AL3
SHEET NUMBER
10 OF **49**



SITE PREPARATION NOTES:

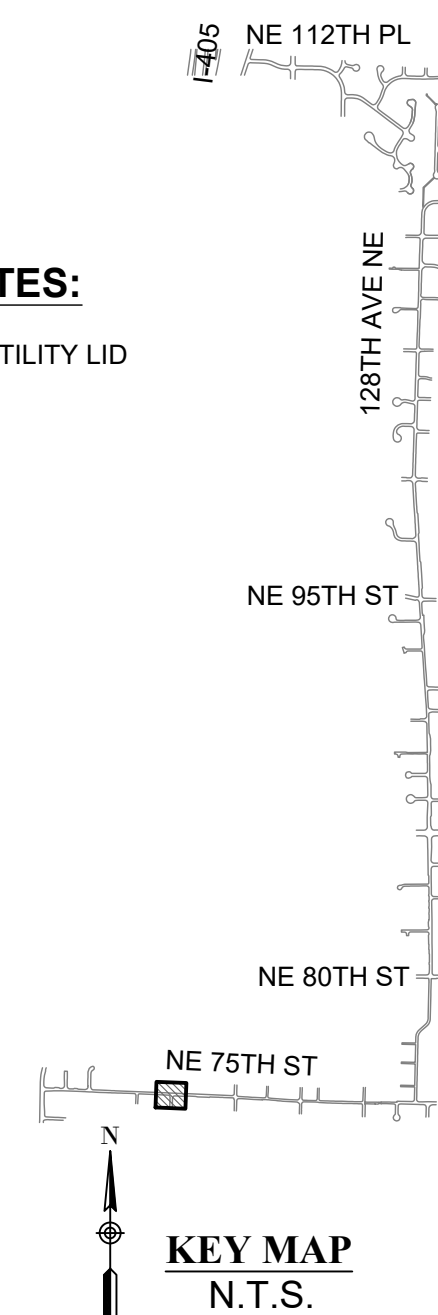
1. SEE AL SHEET SERIES FOR TEMPORARY EASEMENT LIMITS.
2. SEE PV SHEETS SERIES FOR PAVING LIMITS/
3. SEE NT SHEET SERIES FOR GENERAL NOTES AND LEGEND.
4. SEE CH SHEET SERIES FOR SIGN REMOVALS.
5. SEE TS SHEETS FOR TRAFFIC SIGNAL REMOVALS.
6. ALL DIMENSIONS ARE IN FEET, UNLESS OTHERWISE NOTED.
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13. SEE SPECIFICATIONS FOR TREE AND VEGETATION TRIMMING AND PRUNING REQUIREMENTS. ALL PRUNING SHALL BE PERFORMED BY A CERTIFIED ARBORIST AT THE DIRECTION OF THE ENGINEER. PRUNING AND TRIMMING MAY BE REQUIRED BY CITY OF KIRKLAND FOLLOWING SIGN INSTALLATION TO PROVIDE VISIBILITY OF SIGNS.
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15. SEE OP SHEET SERIES FOR OVERVIEW PLANS SHOWING SITE PREPARATION, CHANNELIZATION AND SIGNAGE NOT INCLUDED ON THE SP AND CH PLANS.

SITE PREPARATION LEGEND:

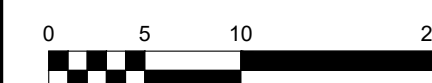
- RIGHT-OF-WAY
- TREE, VEGETATION AND SOIL PROTECTION FENCE PER PRE-APPROVED PLAN NO. CK-R.49
- TREE CRITICAL ROOT ZONE, SEE NOTE 10
- CLEARING AND GRUBBING
- REMOVE FULL DEPTH ASPHALT PAVEMENT
- PLANE / GRIND AND OVERLAY, AS SHOWN ON PLANS
- REMOVE CONCRETE PAVEMENT
- REMOVE CURB
- SAWCUT PAVEMENT
- REMOVE ITEM
- STORM DRAIN PROTECTION INSERT PER PRE-APPROVED PLAN NO. CK-E.11
- STRAW WATTLE PER PRE-APPROVED PLAN NO. CK-E.10

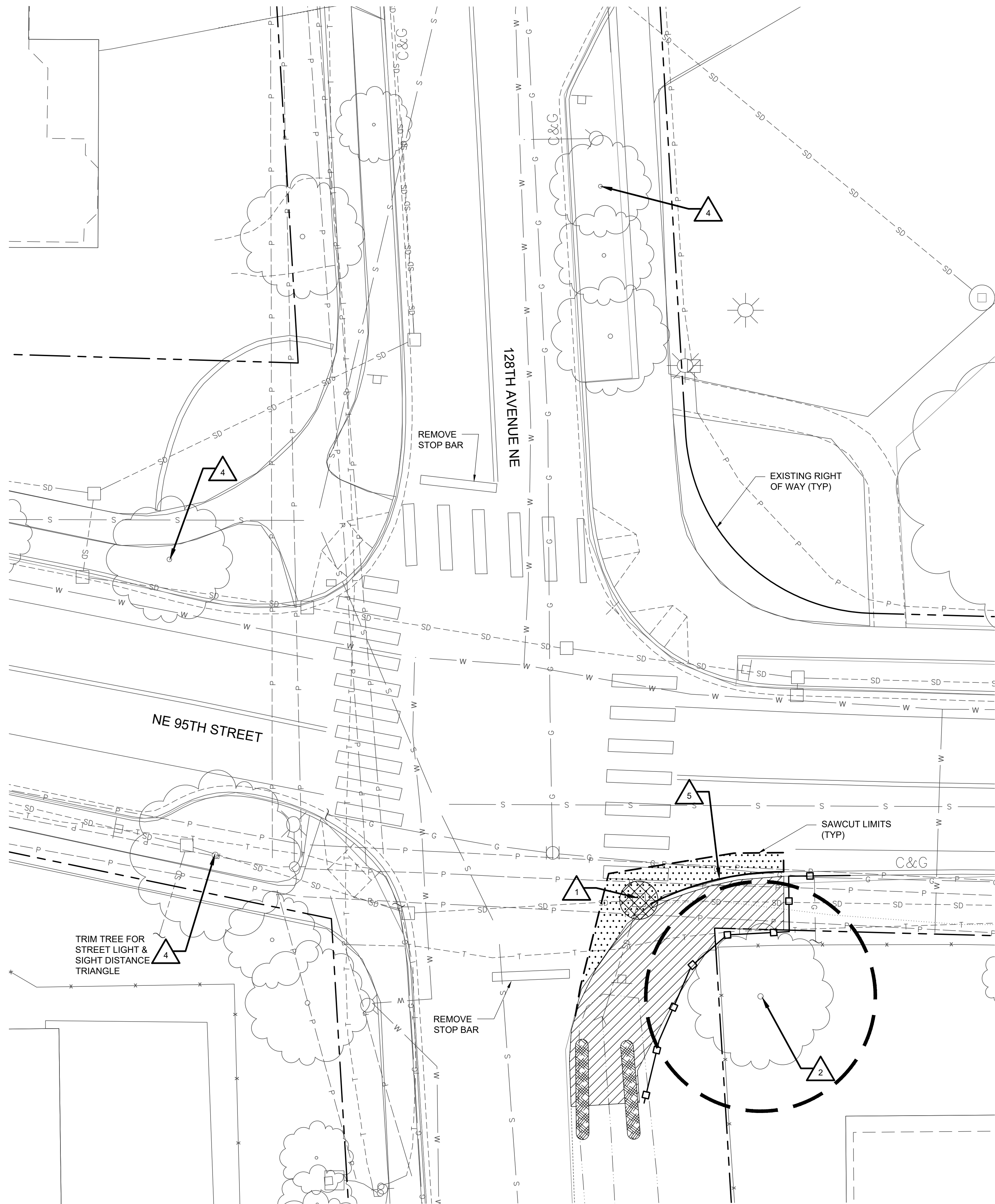
SITE PREPARATION CONSTRUCTION NOTES:

1. PRESERVE AND PROTECT OR ADJUST INLET, MH, CB, UTILITY LID
2. PRESERVE AND PROTECT TREE, SEE NOTES 9 & 10
3. REMOVE TREE, SEE NOTE 12
4. TRIM / PRUNE TREE OR VEGETATION, SEE NOTE 11
5. REMOVE CURB
6. SAWCUT AT NEAREST JOINT



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SITE PREPARATION NOTES:

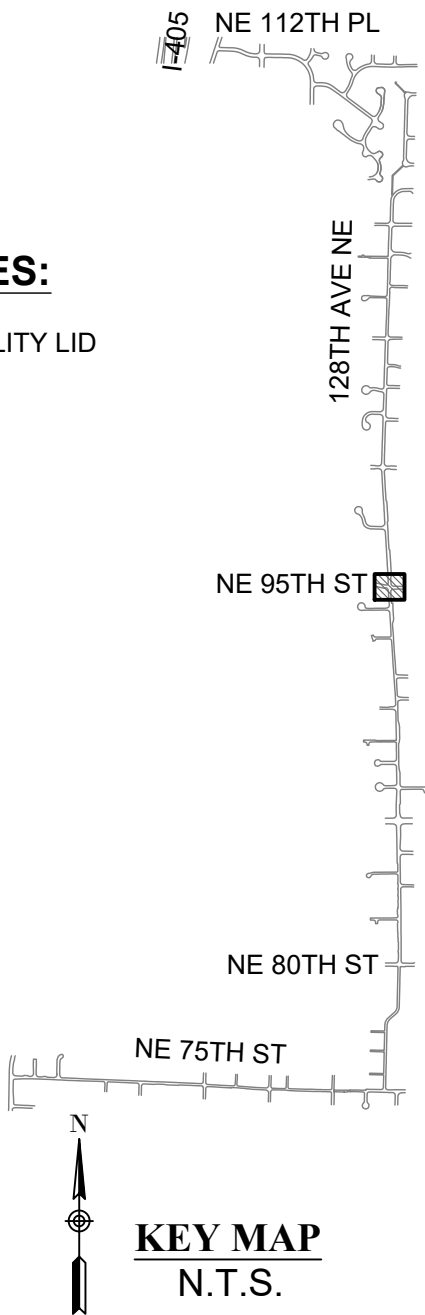
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SITE PREPARATION LEGEND:

- RIGHT-OF-WAY
- TREE, VEGETATION AND SOIL PROTECTION FENCE PER PRE-APPROVED PLAN NO. CK-R.49
- TREE CRITICAL ROOT ZONE, SEE NOTE 10
- CLEARING AND GRUBBING
- REMOVE FULL DEPTH ASPHALT PAVEMENT
- PLANE / GRIND AND OVERLAY, AS SHOWN ON PLANS
- REMOVE CONCRETE PAVEMENT
- REMOVE CURB
- SAWCUT PAVEMENT
- REMOVE ITEM
- STORM DRAIN PROTECTION INSERT PER PRE-APPROVED PLAN NO. CK-E.11
- STRAW WATTLE PER PRE-APPROVED PLAN NO. CK-E.10

SITE PREPARATION CONSTRUCTION NOTES:

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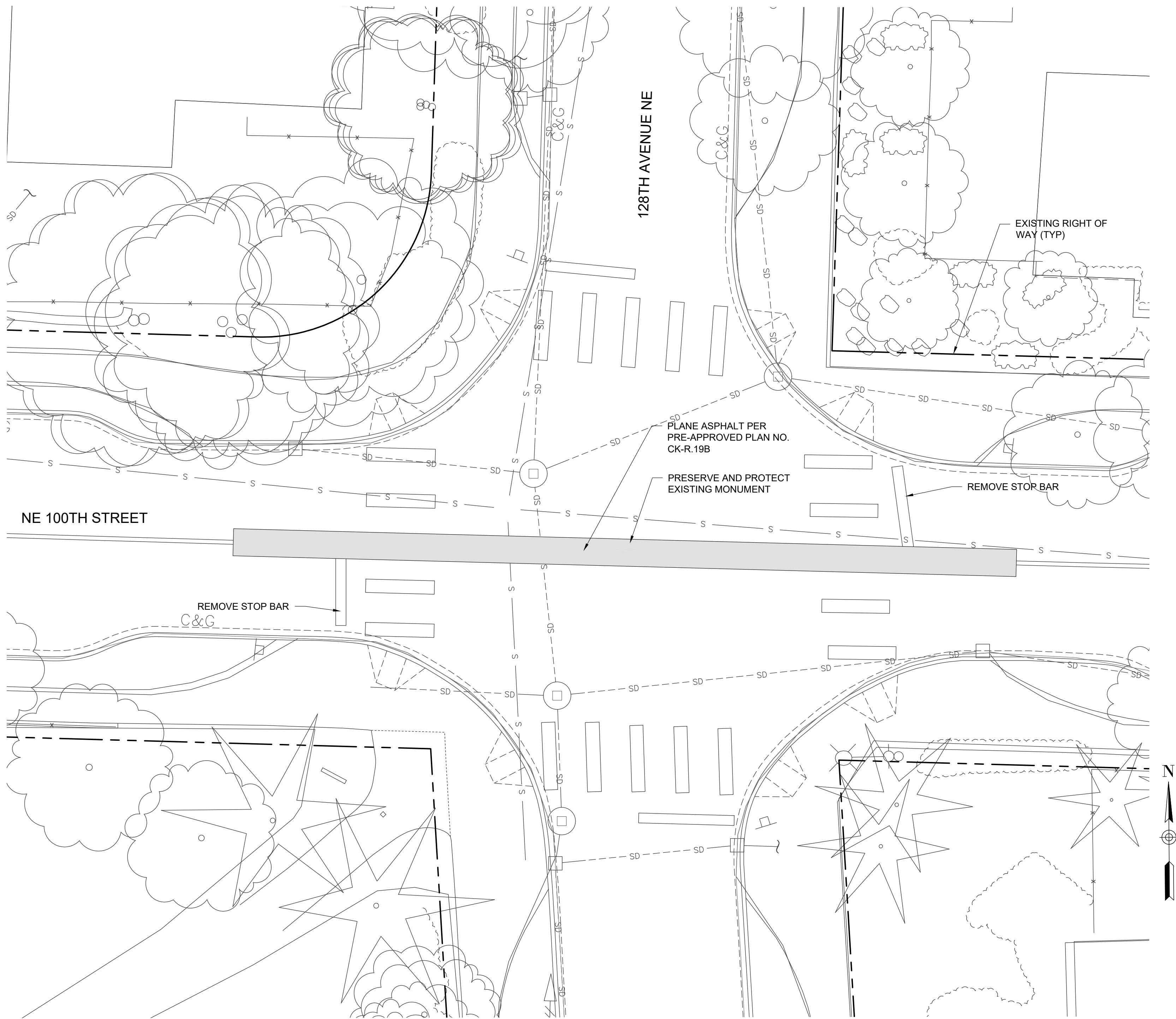
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PREPARED: AL/TD
CHECKED: CS/KL
DATE
DECEMBER 2019
REV. 1
REV. 2
REV. 3
SHEET NAME
SITE PREPARATION

DRAWING NUMBER
SP5

SHEET NUMBER
12 OF **49**



SITE PREPARATION NOTES:

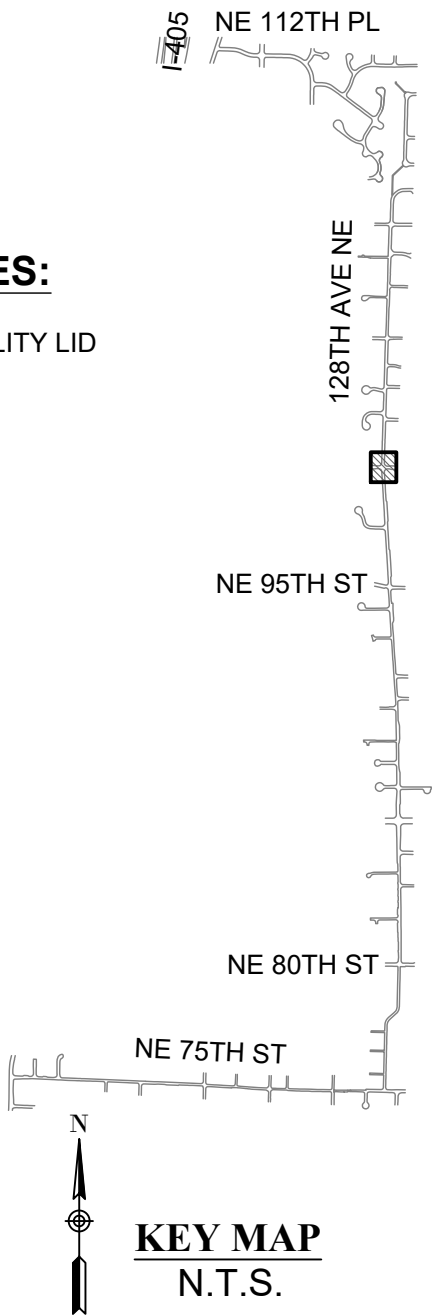
- 1. SEE AL SHEET SERIES FOR TEMPORARY EASEMENT LIMITS.
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SITE PREPARATION LEGEND:


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- TREE, VEGETATION AND SOIL PROTECTION FENCE PER PRE-APPROVED PLAN NO. CK-R.49
- TREE CRITICAL ROOT ZONE, SEE NOTE 10
- CLEARING AND GRUBBING
- REMOVE FULL DEPTH ASPHALT PAVEMENT
- PLANE / GRIND AND OVERLAY, AS SHOWN ON PLANS
- REMOVE CONCRETE PAVEMENT
- REMOVE CURB
- SAWCUT PAVEMENT
- REMOVE ITEM
- STORM DRAIN PROTECTION INSERT PER PRE-APPROVED PLAN NO. CK-E.11
- STRAW WATTLE PER PRE-APPROVED PLAN NO. CK-E.10

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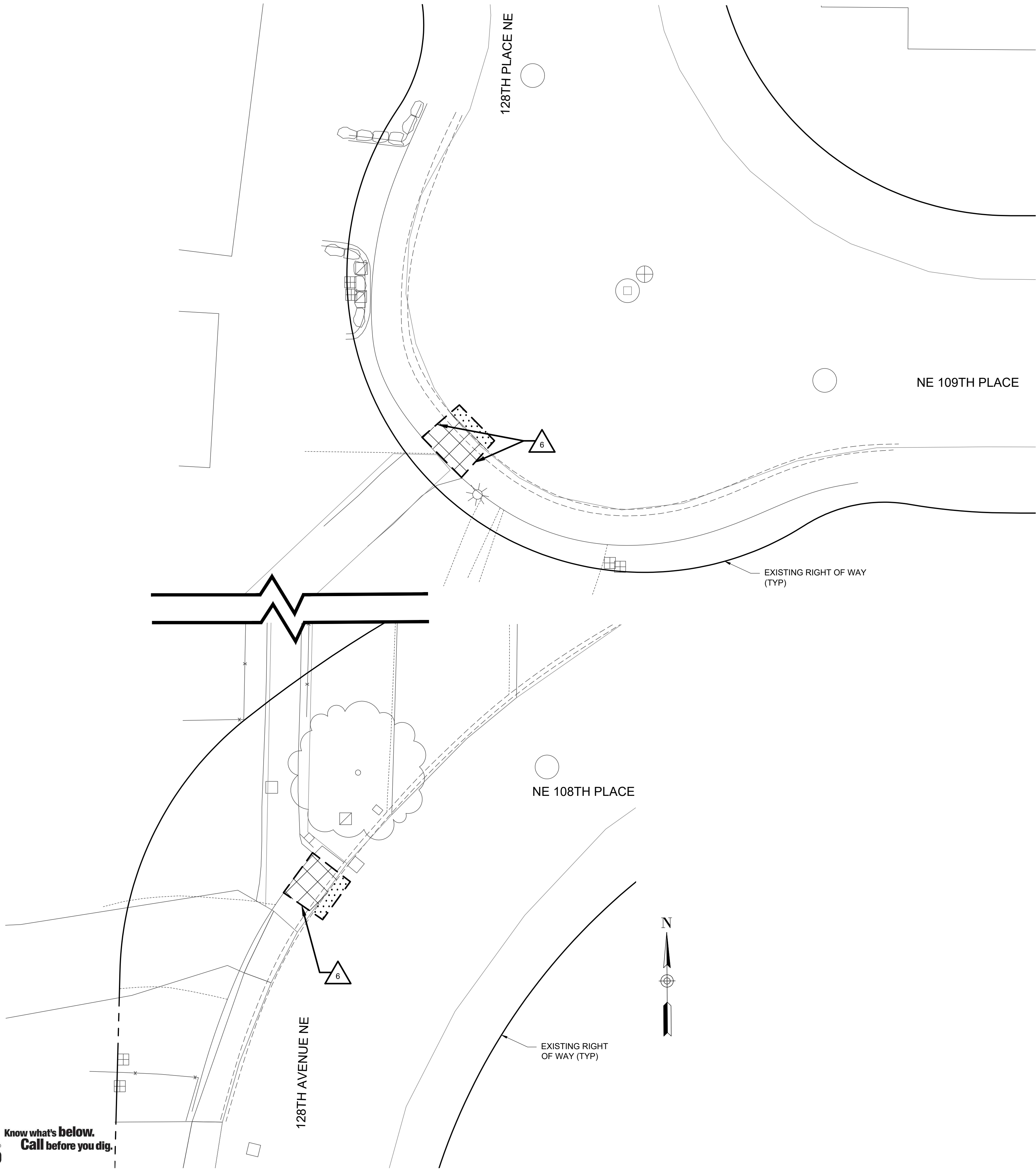
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CITY OF KIRKLAND
123 FIFTH AVENUE
KIRKLAND, WA 98033
PHONE: (425) 887-3000

128TH AVE NE GREENWAY
KIRKLAND GREENWAYS
PROJECT# 25-18-PW
KIRKLAND, WA

PREPARED: AL/TD
CHECKED: CS/KL
DATE
DECEMBER 2019
REV. 1
REV. 2
REV. 3
SHEET NAME
SITE PREPARATION
DRAWING NUMBER
SP6
SHEET NUMBER
13 OF **49**



SITE PREPARATION NOTES:

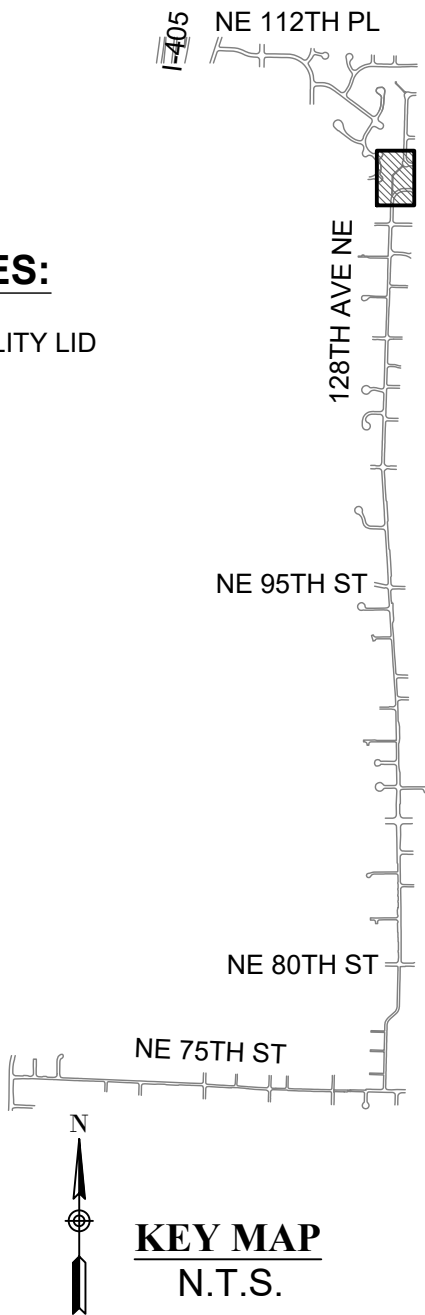
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- REMOVE FULL DEPTH ASPHALT PAVEMENT
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- REMOVE CURB
- SAWCUT PAVEMENT
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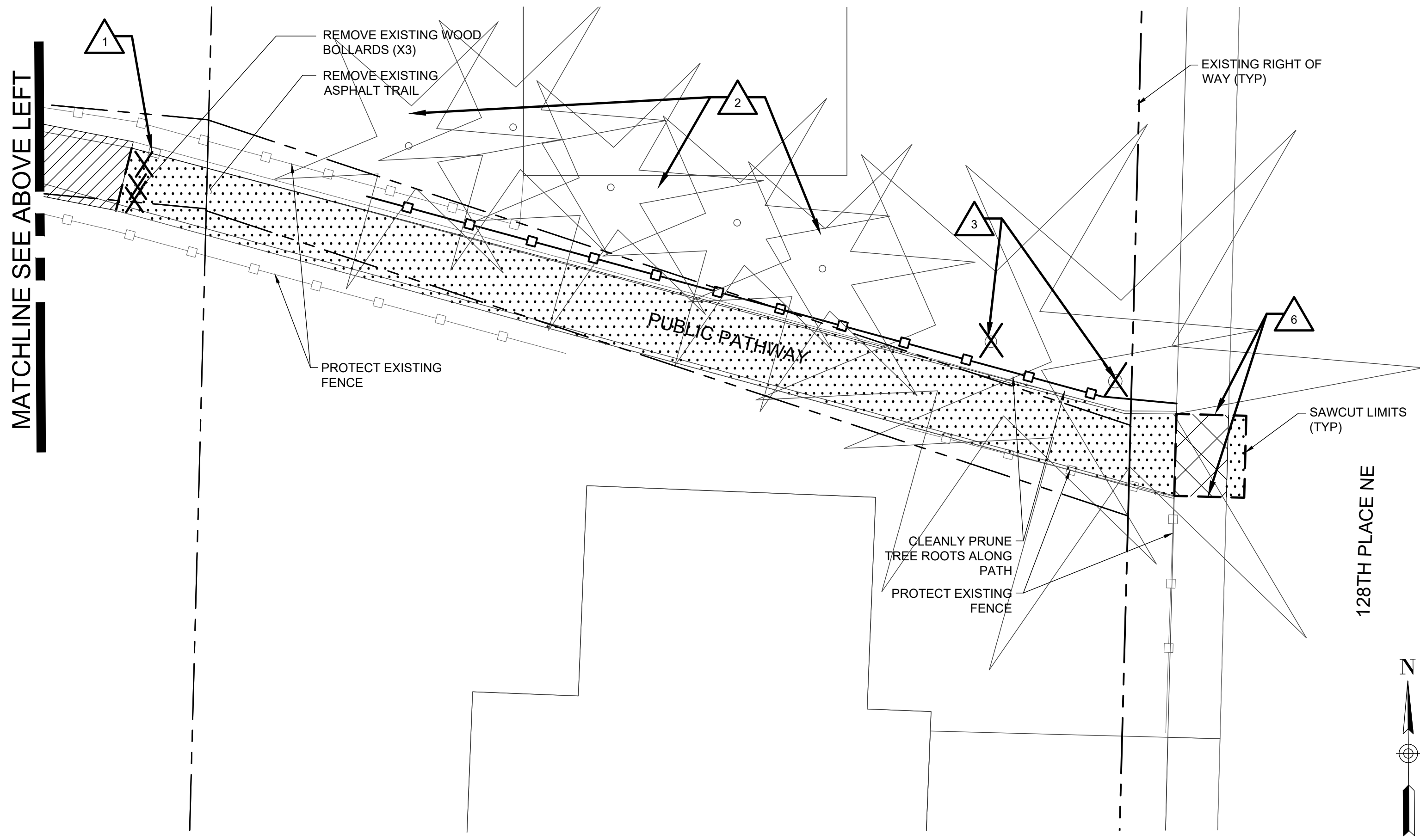
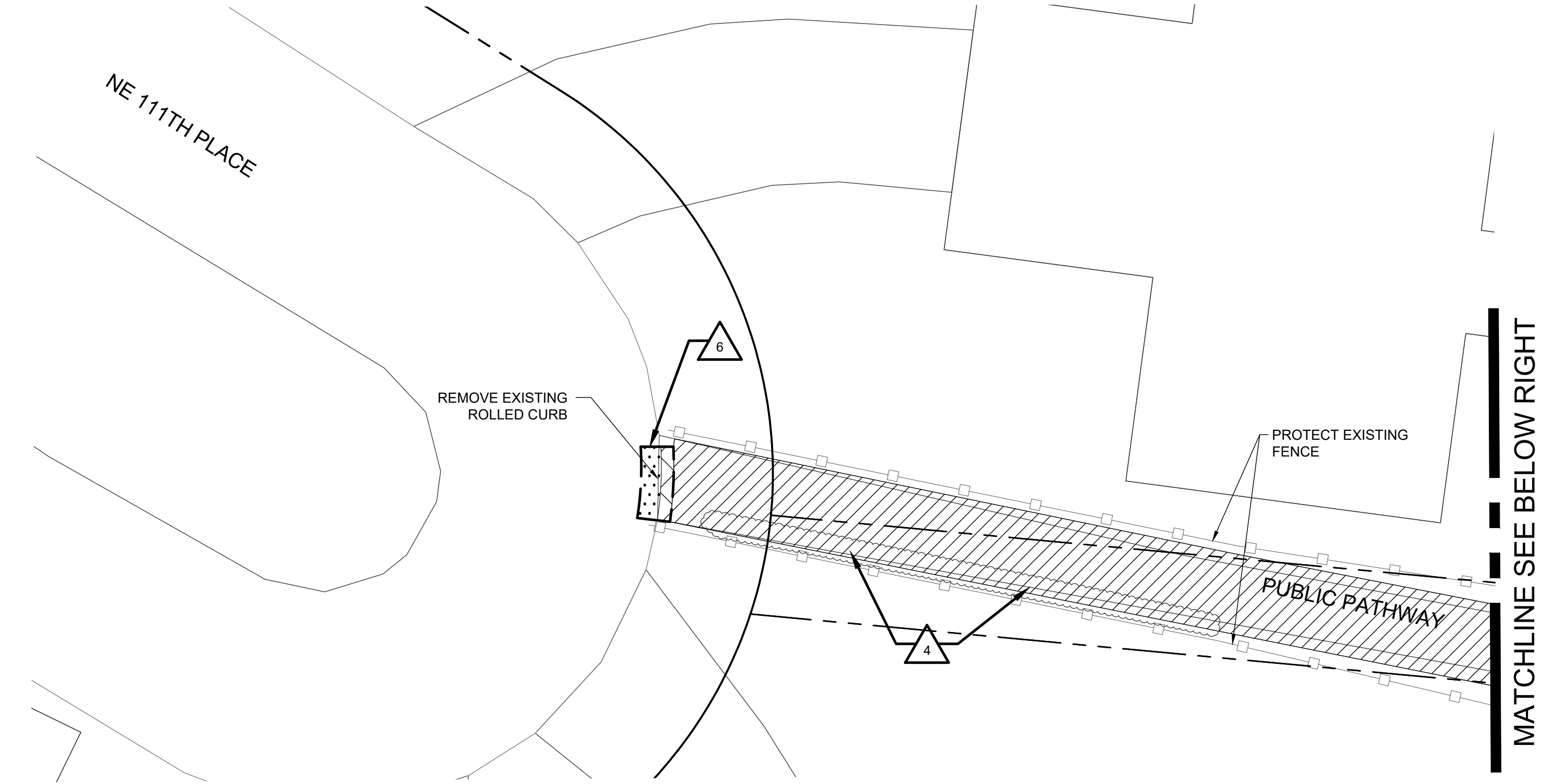
128TH AVE NE GREENWAY
KIRKLAND GREENWAYS
PROJECT# 25-18-PW
KIRKLAND, WA



PREPARED: AL/TD
CHECKED: CS/KL
DATE
DECEMBER 2019
REV. 1
REV. 2
REV. 3
SHEET NAME
SITE PREPARATION

DRAWING NUMBER
SP9
SHEET NUMBER
14 OF 49

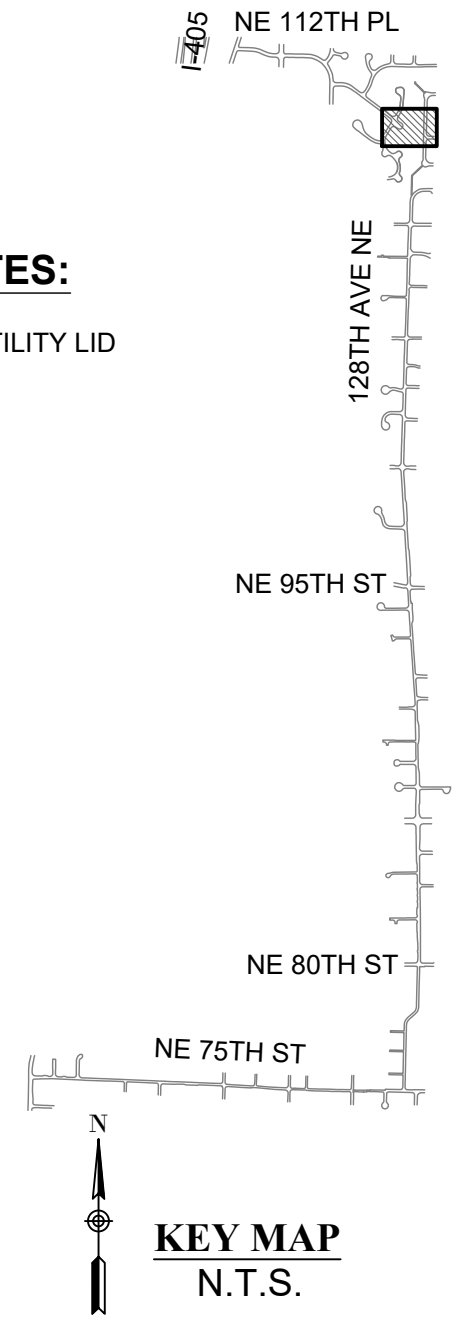
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PHONE: (206) 297-1601
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- SITE PREPARATION LEGEND:**
- RIGHT-OF-WAY
 - TREE, VEGETATION AND SOIL PROTECTION FENCE PER PRE-APPROVED PLAN NO. CK-R.49
 - — — TREE CRITICAL ROOT ZONE, SEE NOTE 10
 - [Hatched Box] CLEARING AND GRUBBING
 - [Dotted Box] REMOVE FULL DEPTH ASPHALT PAVEMENT
 - [Solid Grey Box] PLANE / GRIND AND OVERLAY, AS SHOWN ON PLANS
 - [Cross-hatched Box] REMOVE CONCRETE PAVEMENT
 - — — REMOVE CURB
 - - - SAWCUT PAVEMENT
 - X REMOVE ITEM
 - [Circle with X] STORM DRAIN PROTECTION INSERT PER PRE-APPROVED PLAN NO. CK-E.11
 - [Hatched Box] STRAW WATTLE PER PRE-APPROVED PLAN NO. CK-E.10

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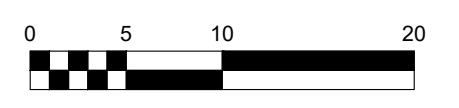


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128TH AVE NE GREENWAY
KIRKLAND GREENWAYS
PROJECT# 25-18-PW
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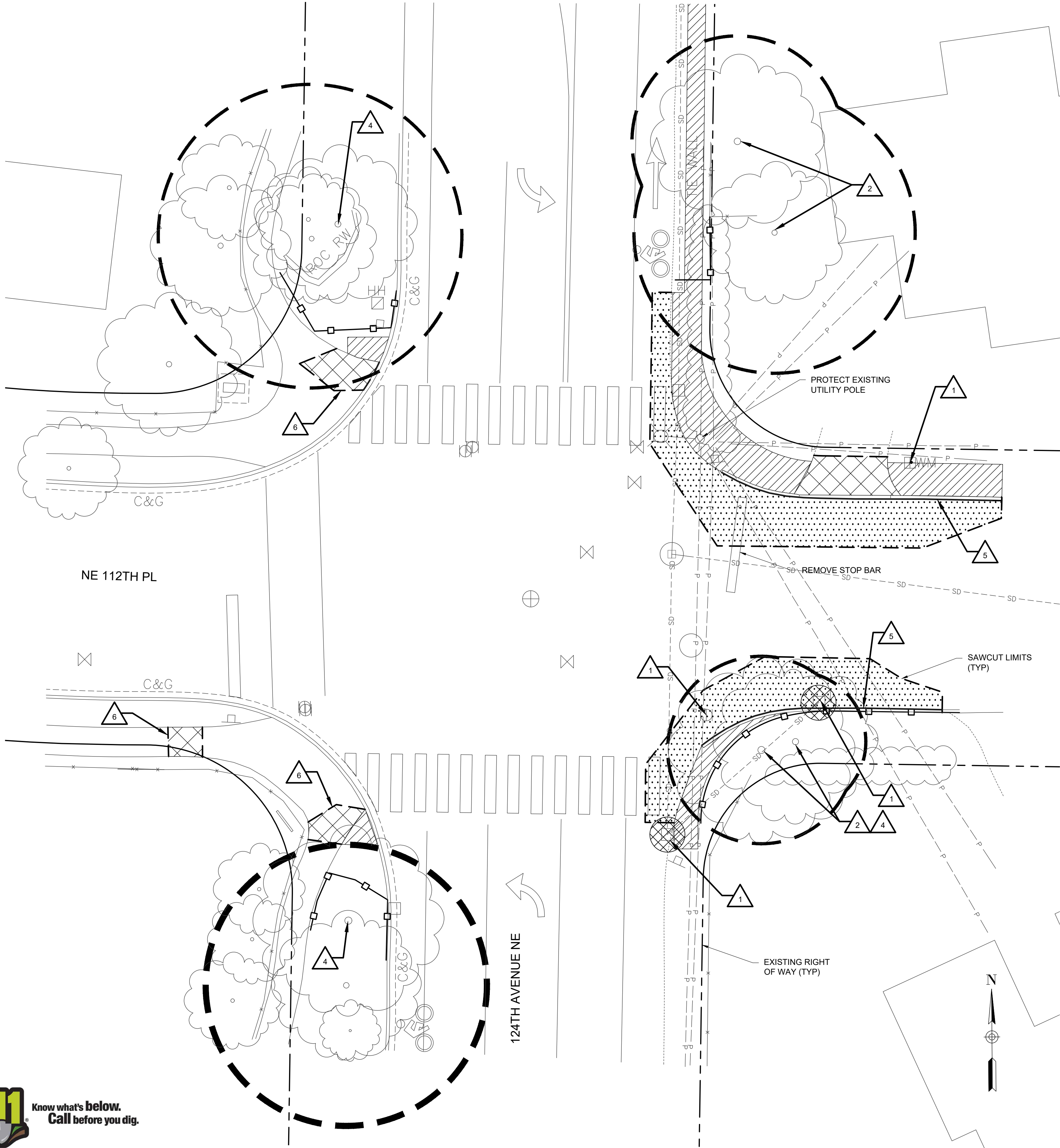
DATE
DECEMBER 2019

REV. 1
REV. 2
REV. 3

SHEET NAME
SITE PREPARATION

DRAWING NUMBER
SP10

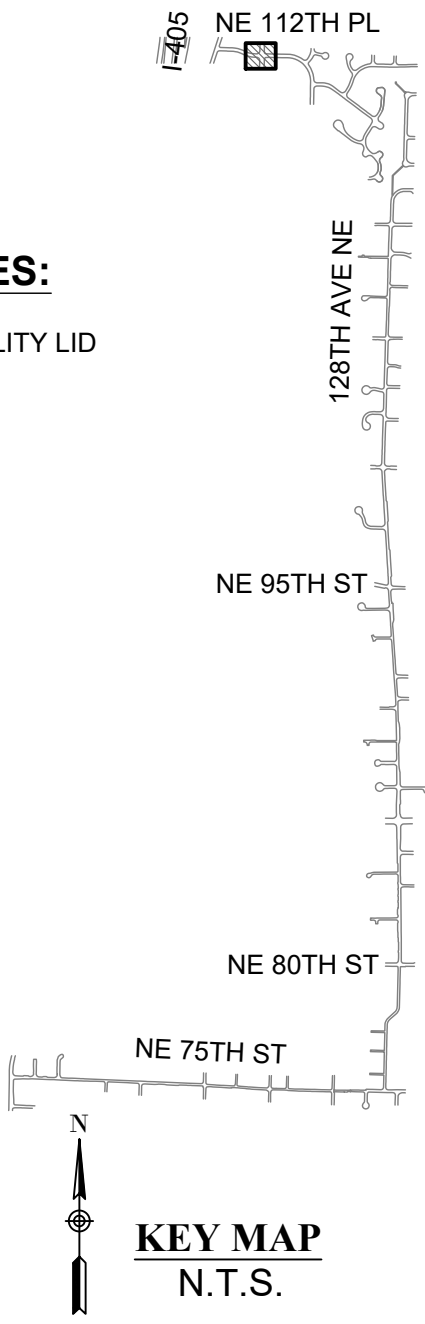
SHEET NUMBER
15 OF **49**



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 13. SEE SPECIFICATIONS FOR TREE AND VEGETATION TRIMMING AND PRUNING REQUIREMENTS. ALL PRUNING SHALL BE PERFORMED BY A CERTIFIED ARBORIST AT THE DIRECTION OF THE ENGINEER. PRUNING AND TRIMMING MAY BE REQUIRED BY CITY OF KIRKLAND FOLLOWING SIGN INSTALLATION TO PROVIDE VISIBILITY OF SIGNS.
 14. FLAG TREES TO BE REMOVED FOR ENGINEER'S APPROVAL PRIOR TO REMOVAL.
 15. SEE OP SHEET SERIES FOR OVERVIEW PLANS SHOWING SITE PREPARATION, CHANNELIZATION AND SIGNAGE NOT INCLUDED ON THE SP AND CH PLANS.

- SITE PREPARATION LEGEND:**
- RIGHT-OF-WAY
 - TREE, VEGETATION AND SOIL PROTECTION FENCE PER PRE-APPROVED PLAN NO. CK-R-49
 - TREE CRITICAL ROOT ZONE, SEE NOTE 10
 - CLEARING AND GRUBBING
 - REMOVE FULL DEPTH ASPHALT PAVEMENT
 - PLANE / GRIND AND OVERLAY, AS SHOWN ON PLANS
 - REMOVE CONCRETE PAVEMENT
 - REMOVE CURB
 - SAWCUT PAVEMENT
 - REMOVE ITEM
 - STORM DRAIN PROTECTION INSERT PER PRE-APPROVED PLAN NO. CK-E.11
 - STRAW WATTLE PER PRE-APPROVED PLAN NO. CK-E.10

- SITE PREPARATION CONSTRUCTION NOTES:**
- 1 PRESERVE AND PROTECT OR ADJUST INLET, MH, CB, UTILITY LID
 - 2 PRESERVE AND PROTECT TREE, SEE NOTES 9 & 10
 - 3 REMOVE TREE, SEE NOTE 12
 - 4 TRIM / PRUNE TREE OR VEGETATION, SEE NOTE 11
 - 5 REMOVE CURB
 - 6 SAWCUT AT NEAREST JOINT



100% SUBMITTAL - NOT FOR CONSTRUCTION





CITY OF KIRKLAND
123 FIFTH AVENUE
KIRKLAND, WA 98033
PHONE: (425) 887-3000

128TH AVE NE GREENWAY
KIRKLAND GREENWAYS
PROJECT# 25-18-PW
KIRKLAND, WA



PREPARED: ~~AMTD~~

CHECKED: ~~CS~~ SKL

DATE
DECEMBER 2019

REV. 1
REV. 2
REV. 3

SHEET NAME
SITE PREPARATION

DRAWING NUMBER
SP12

SHEET NUMBER
16 OF 49

INTERSECTION CURB RETURN CURVE DATA
BOTTOM CURB ELEVATIONS

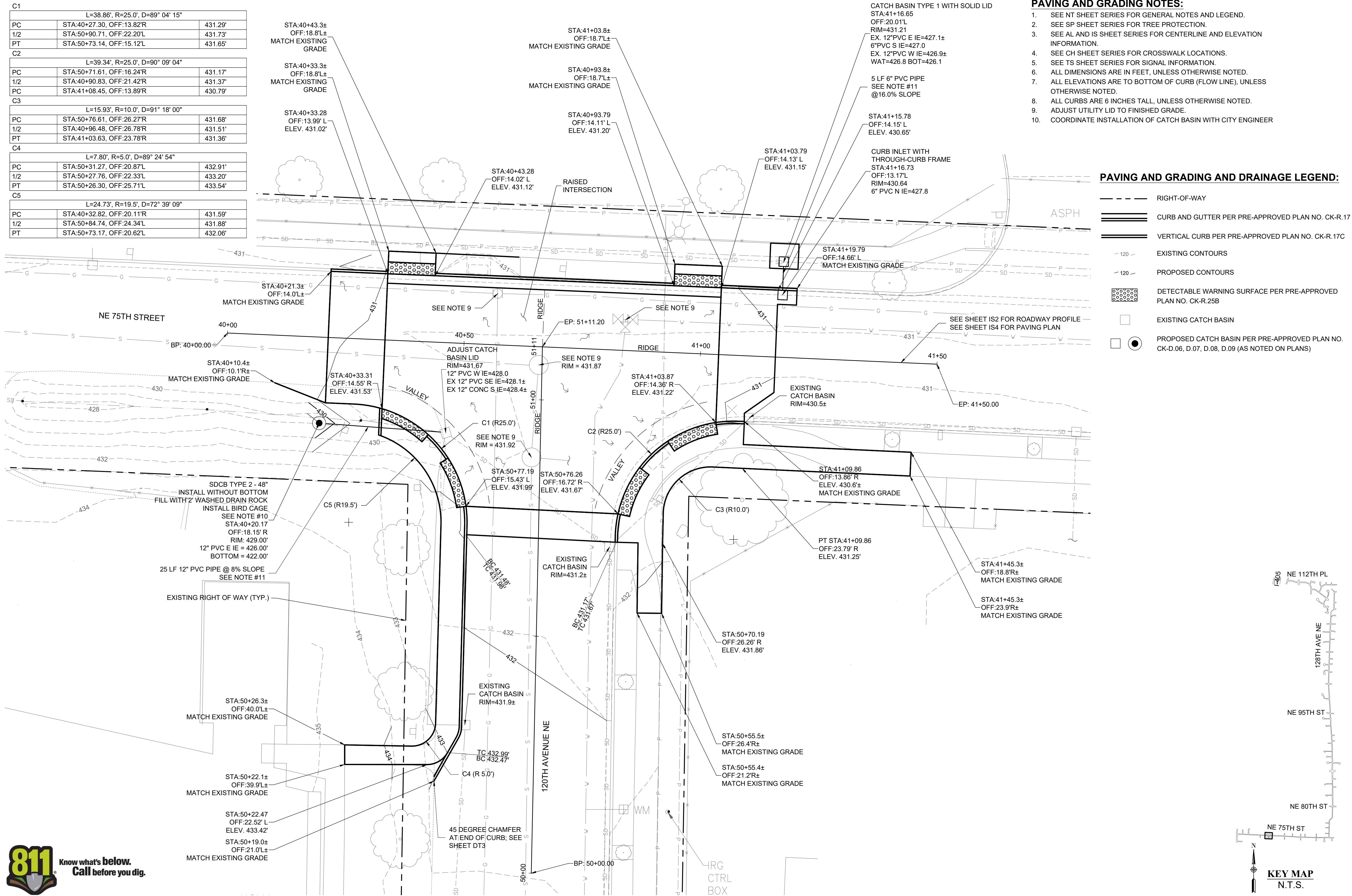
| | | | |
|--|---------------------------|---------|--|
| C1 L=38.86', R=25.0', D=89° 04' 15" | | | |
| PC | STA:40+27.30, OFF:13.82'R | 431.29' | |
| 1/2 | STA:50+90.71, OFF:22.20'L | 431.73' | |
| PT | STA:50+73.14, OFF:15.12'L | 431.65' | |

| | | | |
|--|---------------------------|---------|--|
| C2 L=39.34', R=25.0', D=90° 09' 04" | | | |
| PC | STA:40+71.61, OFF:16.24'R | 431.17' | |
| 1/2 | STA:40+90.83, OFF:21.42'R | 431.37' | |
| PC | STA:41+08.45, OFF:13.89'R | 430.79' | |

| | | | |
|--|---------------------------|---------|--|
| C3 L=15.93', R=10.0', D=91° 18' 00" | | | |
| PC | STA:50+76.61, OFF:26.27'R | 431.68' | |
| 1/2 | STA:40+96.48, OFF:26.78'R | 431.51' | |
| PT | STA:41+03.63, OFF:23.78'R | 431.36' | |

| | | | |
|--------------------------------------|---------------------------|---------|--|
| C4 L=7.80', R=5.0', D=89° 24' 54" | | | |
| PC | STA:50+31.27, OFF:20.87'L | 432.91' | |
| 1/2 | STA:50+27.76, OFF:22.33'L | 433.20' | |
| PT | STA:50+26.30, OFF:25.71'L | 433.54' | |

| | | | |
|--|---------------------------|---------|--|
| C5 L=24.73', R=19.5', D=72° 39' 09" | | | |
| PC | STA:40+32.82, OFF:20.11'R | 431.59' | |
| 1/2 | STA:50+84.74, OFF:24.34'L | 431.88' | |
| PT | STA:50+73.17, OFF:20.62'L | 432.06' | |



PAVING AND GRADING NOTES:

1. SEE NT SHEET SERIES FOR GENERAL NOTES AND LEGEND.
2. SEE SP SHEET SERIES FOR TREE PROTECTION.
3. SEE AL AND IS SHEET SERIES FOR CENTERLINE AND ELEVATION INFORMATION.
4. SEE CH SHEET SERIES FOR CROSSWALK LOCATIONS.
5. SEE TS SHEET SERIES FOR SIGNAL INFORMATION.
6. ALL DIMENSIONS ARE IN FEET, UNLESS OTHERWISE NOTED.
7. ALL ELEVATIONS ARE TO BOTTOM OF CURB (FLOW LINE), UNLESS OTHERWISE NOTED.
8. ALL CURBS ARE 6 INCHES TALL, UNLESS OTHERWISE NOTED.
9. ADJUST UTILITY LID TO FINISHED GRADE.
10. COORDINATE INSTALLATION OF CATCH BASIN WITH CITY ENGINEER

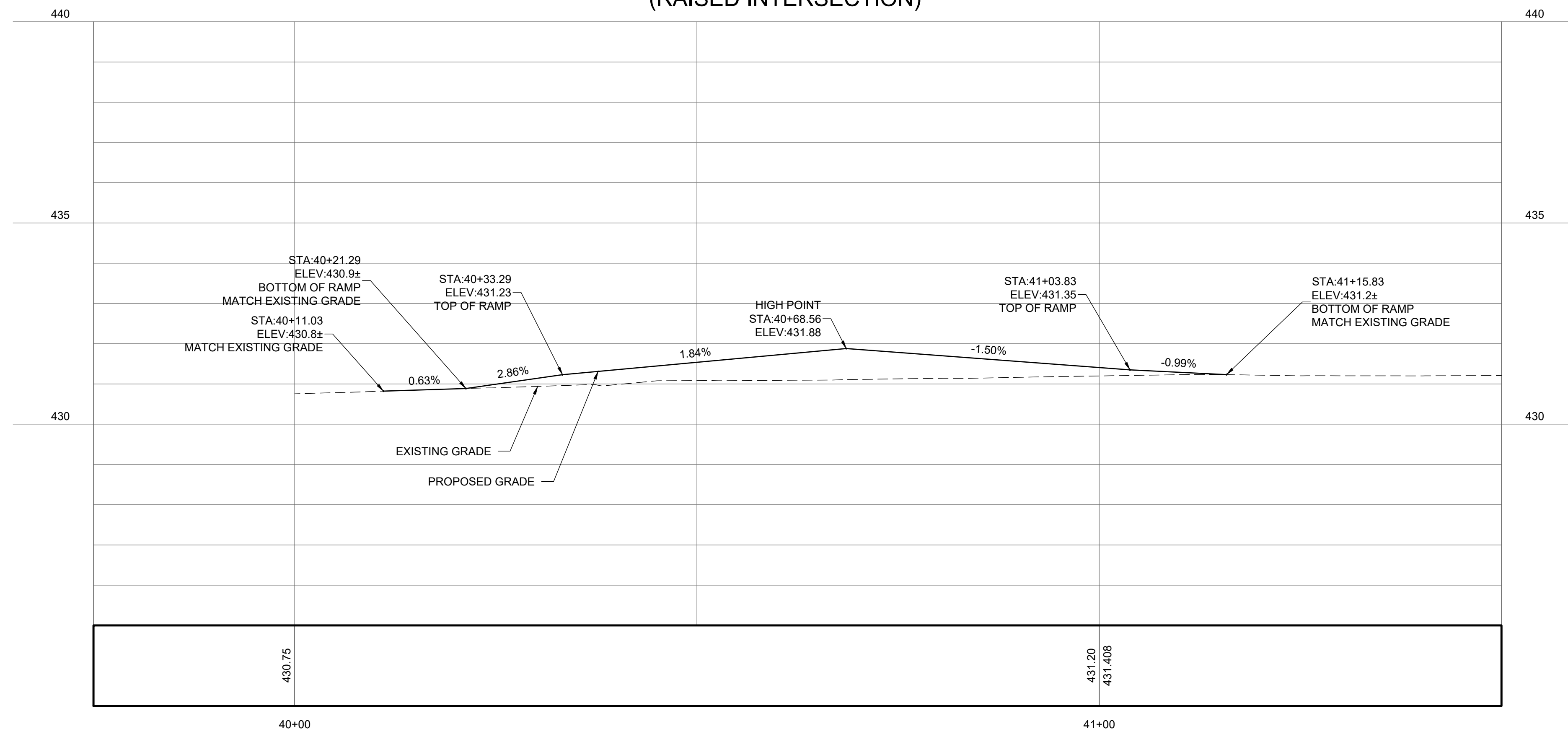
PAVING AND GRADING AND DRAINAGE LEGEND:

- RIGHT-OF-WAY
- === CURB AND GUTTER PER PRE-APPROVED PLAN NO. CK-R.17
- === VERTICAL CURB PER PRE-APPROVED PLAN NO. CK-R.17C
- - - EXISTING CONTOURS
- - - PROPOSED CONTOURS
- [Pattern] DETECTABLE WARNING SURFACE PER PRE-APPROVED PLAN NO. CK-R.25B
- [Square] EXISTING CATCH BASIN
- [Circle] PROPOSED CATCH BASIN PER PRE-APPROVED PLAN NO. CK-D.06, D.07, D.08, D.09 (AS NOTED ON PLANS)

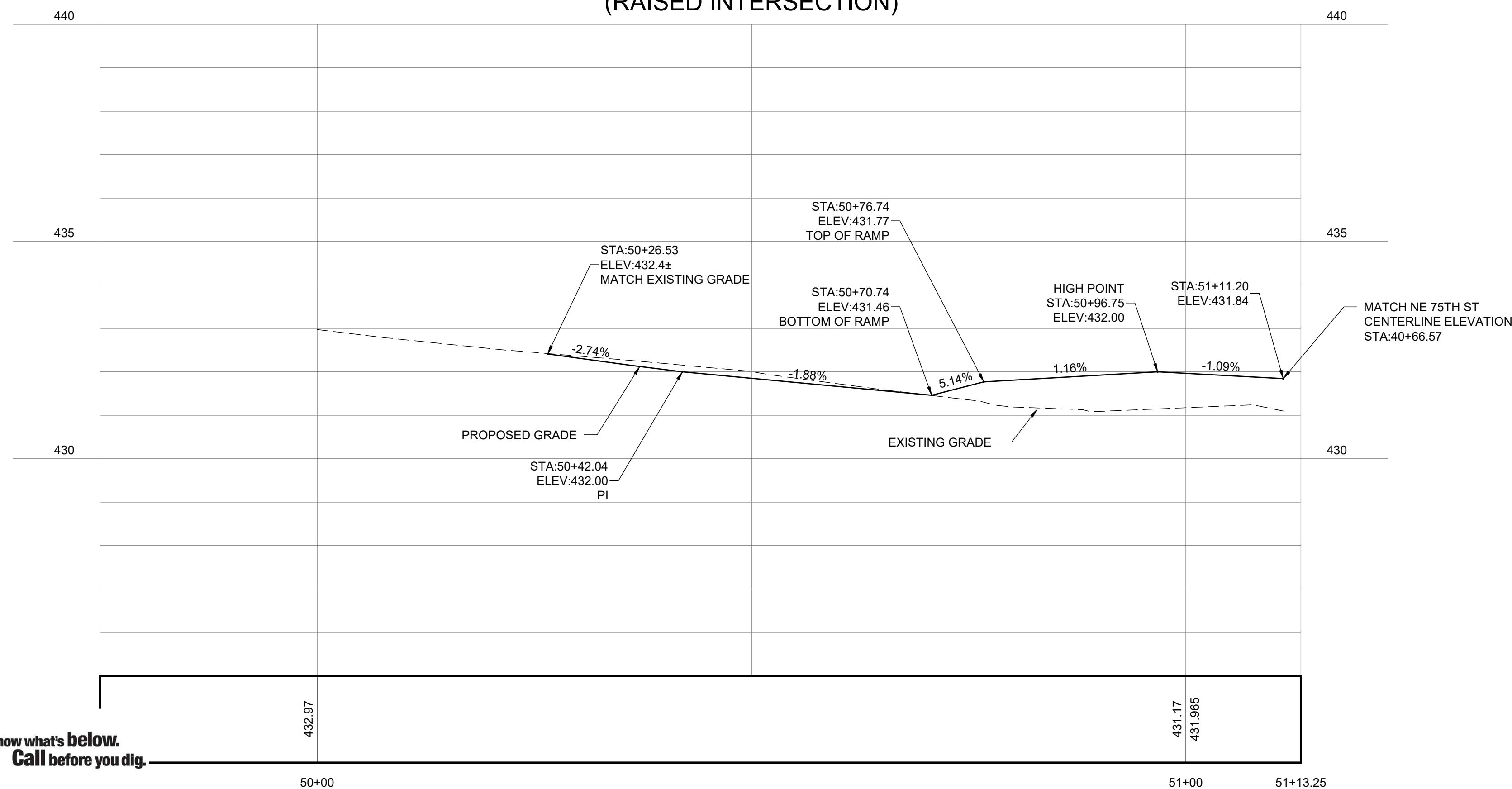
128TH AVE NE GREENWAY
KIRKLAND GREENWAYS
PROJECT# 25-18-PW
KIRKLAND, WA

PREPARED: AL/TD
CHECKED: CS/KL
DATE
DECEMBER 2019
REV. 1
REV. 2
REV. 3
SHEET NAME
ROADWAY
PROFILES
DRAWING NUMBER
IS2
SHEET NUMBER
18 OF 49

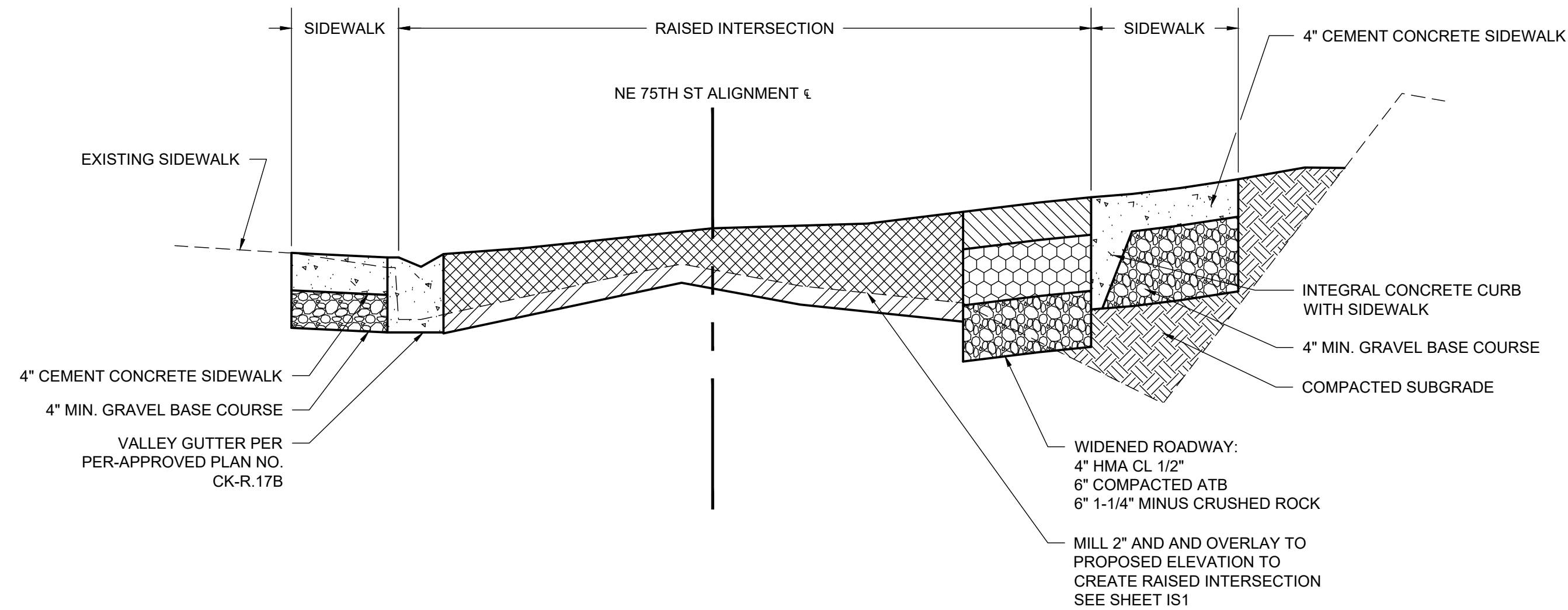
**NE 75TH STREET CENTERLINE PROFILE
(RAISED INTERSECTION)**



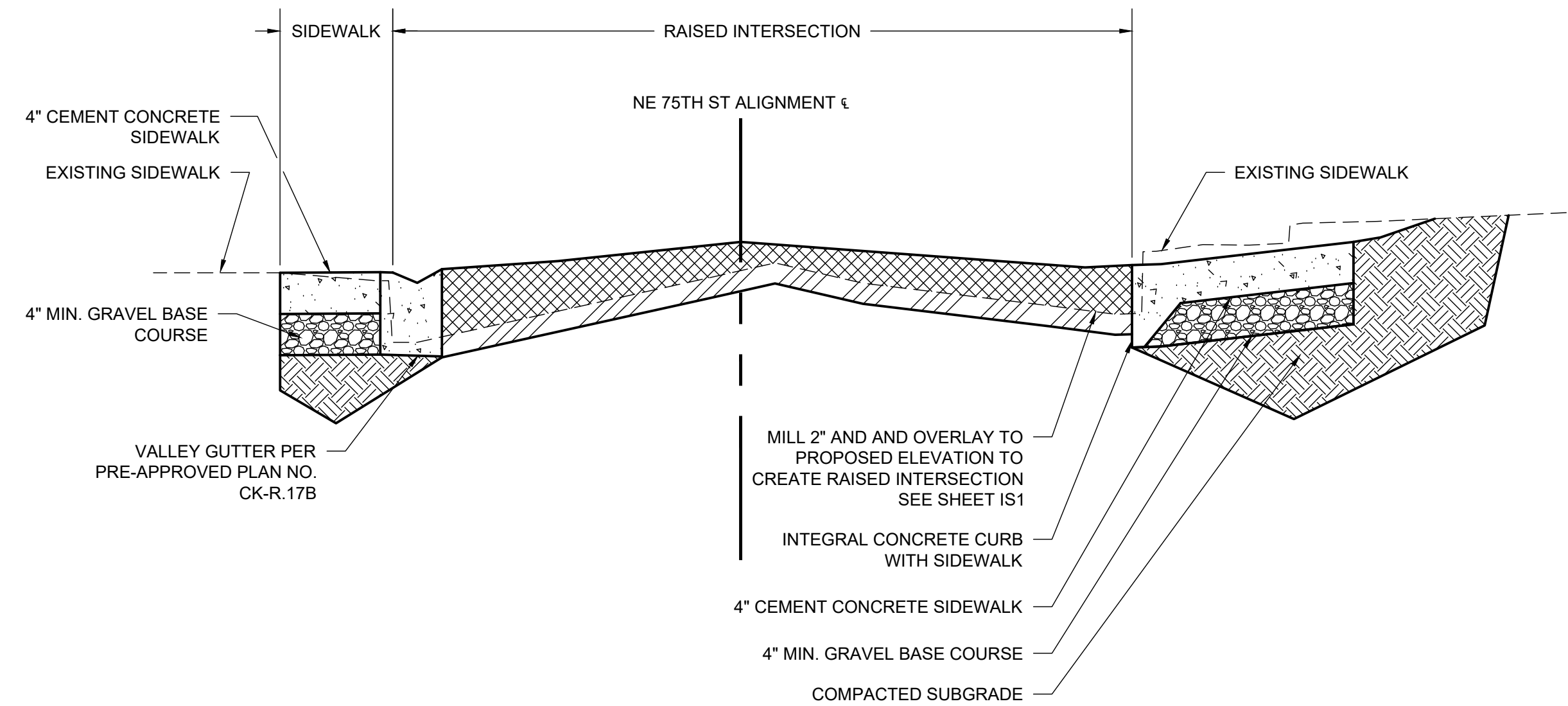
**120TH AVE NE CENTERLINE PROFILE
(RAISED INTERSECTION)**



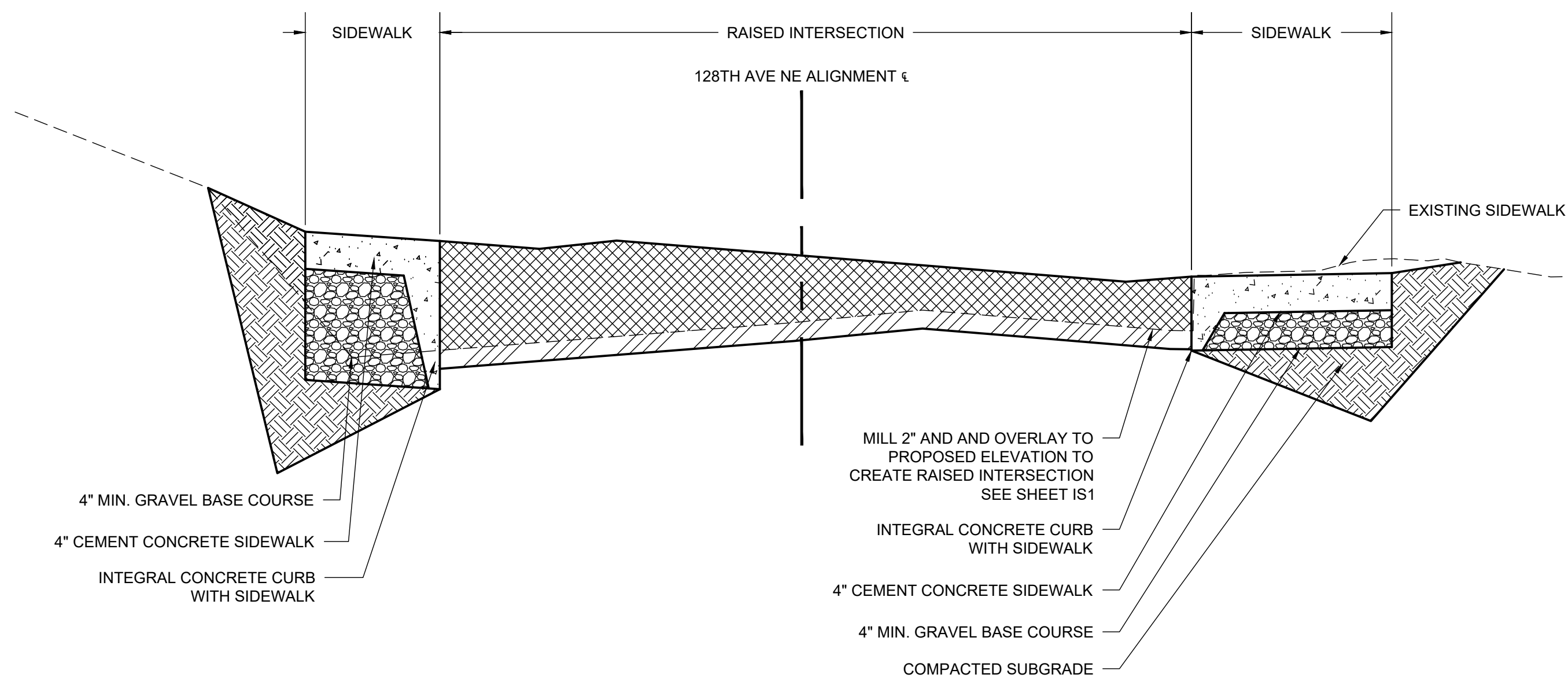
100% SUBMITTAL - NOT FOR CONSTRUCTION



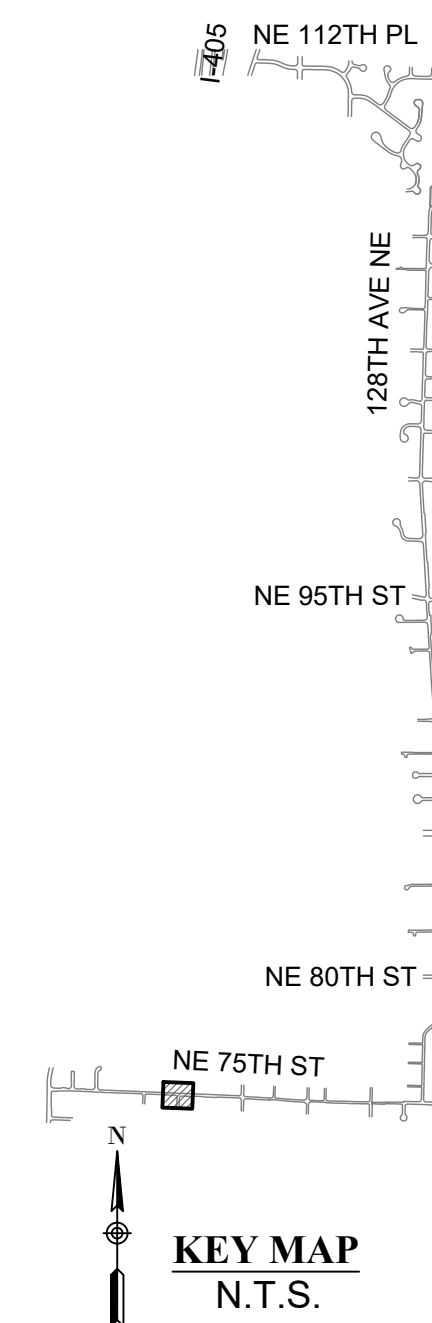
1
IS3
SECTION 40+35
H: 1" = 5'
V: 1" = 1'



2
IS3
SECTION 41+00
H: 1" = 5'
V: 1" = 1'

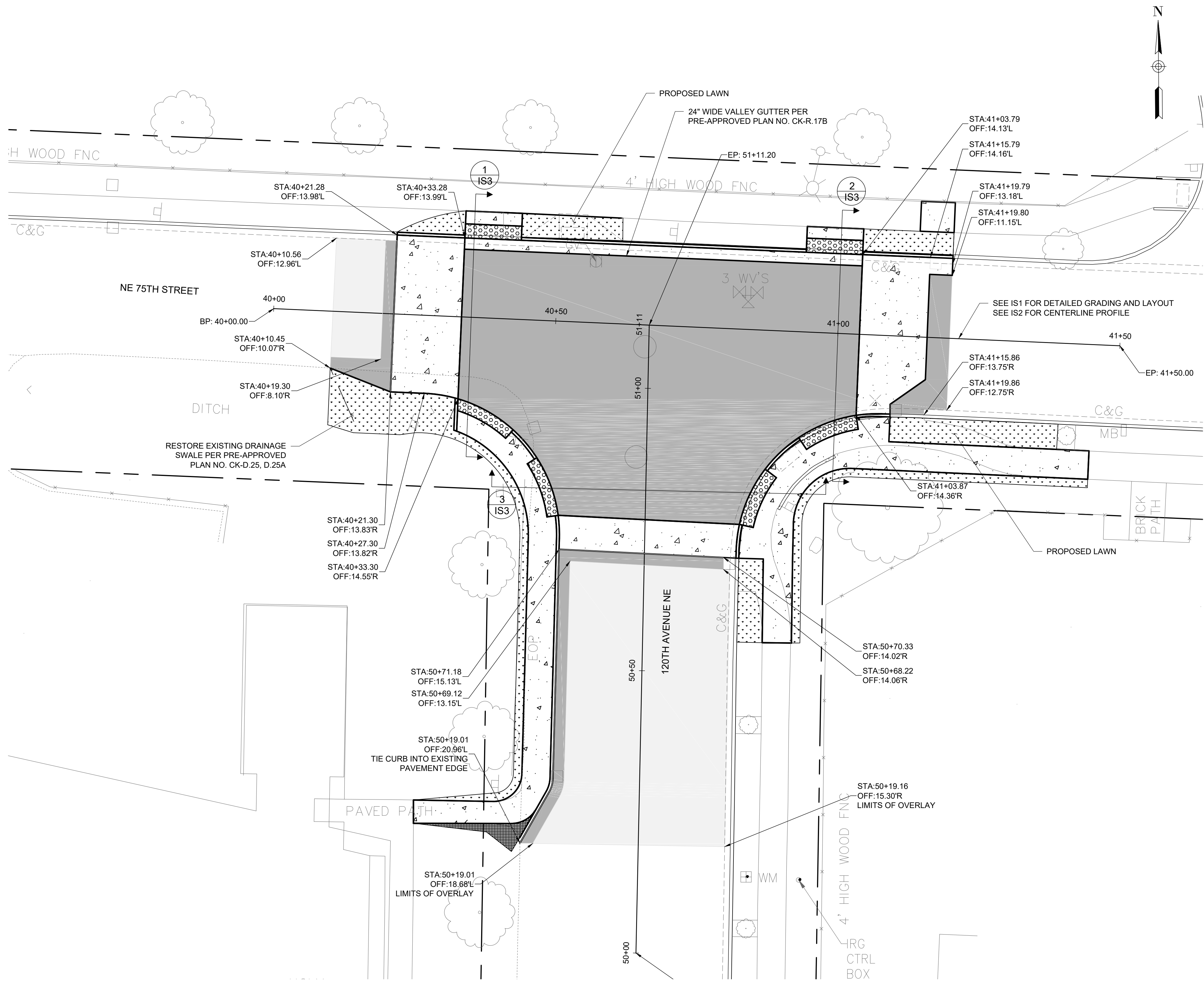


3
IS3
SECTION 50+82
H: 1" = 5'
V: 1" = 1'



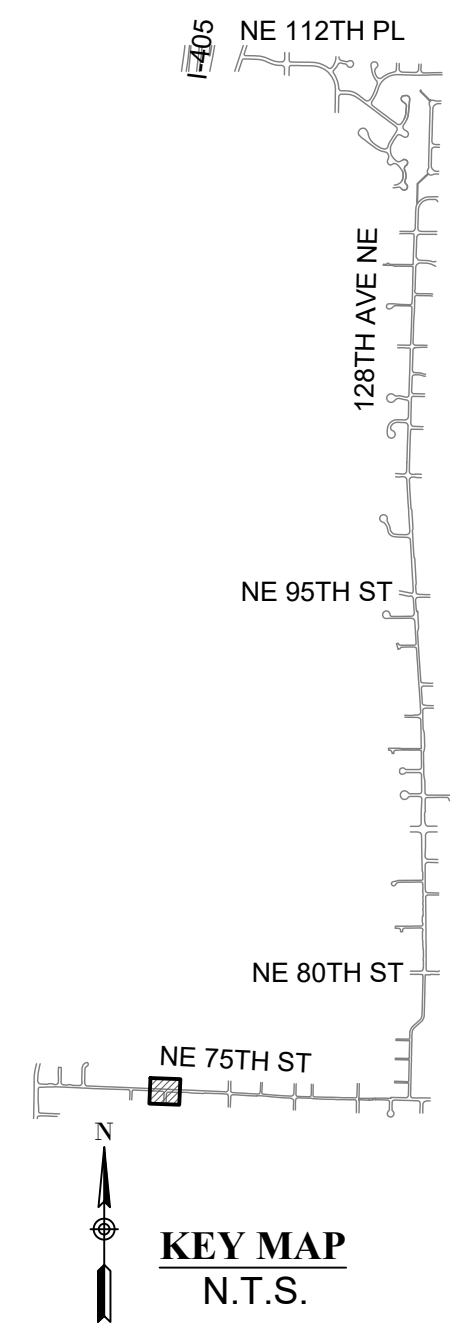
100% SUBMITTAL - NOT FOR CONSTRUCTION



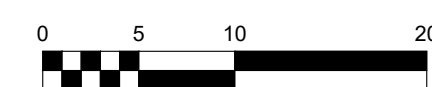


- PAVING AND GRADING NOTES:**
1. SEE NT SHEET SERIES FOR GENERAL NOTES AND LEGEND.
 2. SEE SP SHEET SERIES FOR TREE PROTECTION.
 3. SEE AL AND IS SHEET SERIES FOR CENTERLINE AND ELEVATION INFORMATION.
 4. SEE CH SHEET SERIES FOR CROSSWALK LOCATIONS.
 5. SEE TS SHEET SERIES FOR SIGNAL INFORMATION.
 6. ALL DIMENSIONS ARE IN FEET, UNLESS OTHERWISE NOTED.
 7. ALL ELEVATIONS ARE TO BOTTOM OF CURB (FLOW LINE), UNLESS OTHERWISE NOTED.
 8. ALL CURBS ARE 6 INCHES TALL, UNLESS OTHERWISE NOTED.
 9. ADJUST UTILITY LID TO FINISHED GRADE.
 10. COORDINATE INSTALLATION OF CATCH BASIN WITH CITY ENGINEER
 11. PIPE INSTALLATION SHALL FOLLOW PRE-APPROVED PLAN NO. CK-D.02

- PAVING AND GRADING AND DRAINAGE LEGEND:**
- RIGHT-OF-WAY
 - == CURB AND GUTTER PER PRE-APPROVED PLAN NO. CK-R.17
 - == VERTICAL CURB PER PRE-APPROVED PLAN NO. CK-R.17C
 - FULL DEPTH HMA PER PRE-APPROVED PLAN NO. CK-R.09, PAVEMENT PATCH PER PRE-APPROVED PLAN NO. CK-R.12
 - CEMENT CONCRETE RAMP OR SIDEWALK, PER PRE-APPROVED PLAN NO. CK-R.21, CK-R.23
 - GRIND EXISTING HMA AND OVERLAY (DEPTH PER PLANS)
 - LAWN
 - EXISTING CONTOURS
 - PROPOSED CONTOURS
 - DETECTABLE WARNING SURFACE PER PRE-APPROVED PLAN NO. CK-R.25B
 - EXISTING CATCH BASIN
 - PROPOSED CATCH BASIN PER PRE-APPROVED PLAN NO. CK-D.06, D.07, D.08, D.09 (AS NOTED ON PLANS)



100% SUBMITTAL - NOT FOR CONSTRUCTION

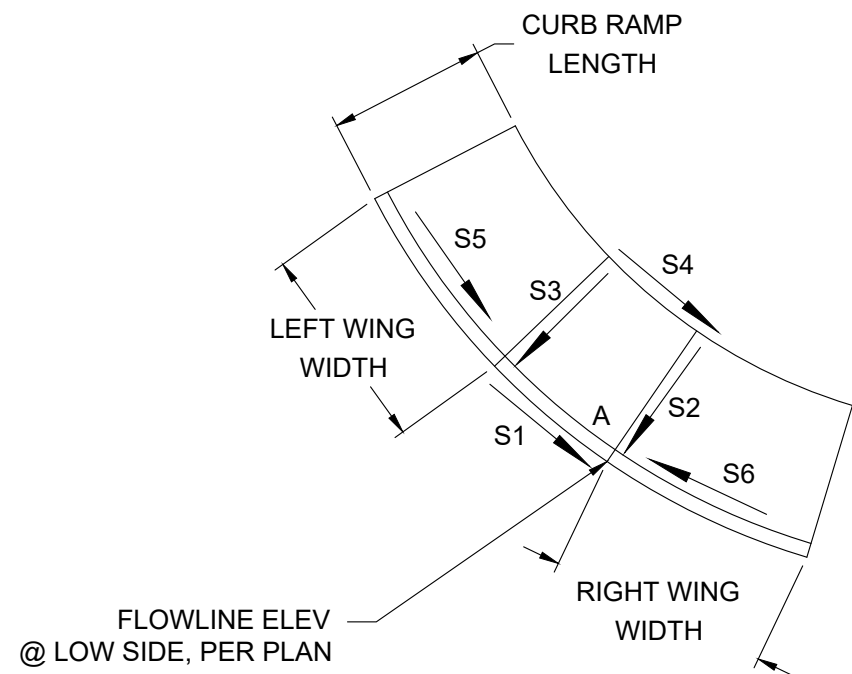


PREPARED: AL/TD
CHECKED: CS/KL
DATE
DECEMBER 2019
REV. 1
REV. 2
REV. 3
SHEET NAME
PAVING PLAN

DRAWING NUMBER
IS4
SHEET NUMBER
20 OF **49**

CURB RAMP TABLE

| # | LEFT | LENGTH | RIGHT | ELEV @ PT A | LOW SIDE | S1 % | S2 % | S3 % | S4 % | S5 % | S6 % |
|-----|------|--------|-------|-------------|----------|------|------|------|------|------|------|
| 4.1 | 15' | 6' | 5' | 278.84' | RIGHT | 1.5 | 2.0 | 2.0 | 2.0 | 13.0 | 0.2 |



INTERSECTION CURB RETURN CURVE DATA
FLOWLINE ELEVATIONS

| | | | | |
|-----|--------------------------------|--|--|---------|
| C1 | L=2.40', R=25.0', D=5° 29' 27" | | | |
| PC | STA:50+16.57, OFF:10.07'R | | | 277.73' |
| 1/2 | STA:50+34.69, OFF:17.83'R | | | 278.88' |
| PT | STA:60+94.49, OFF:16.07'R | | | 280.61' |

ELEVATION TABLE

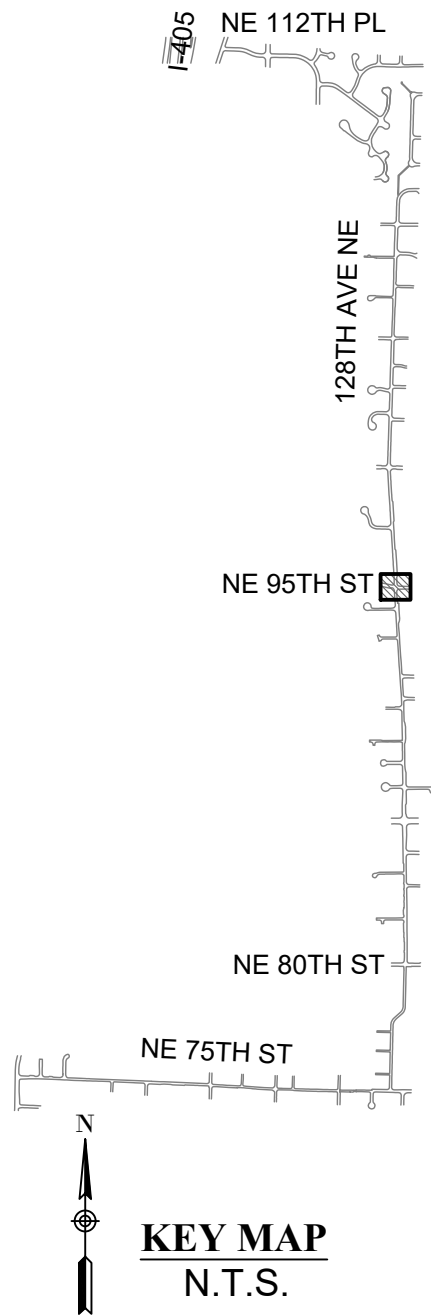
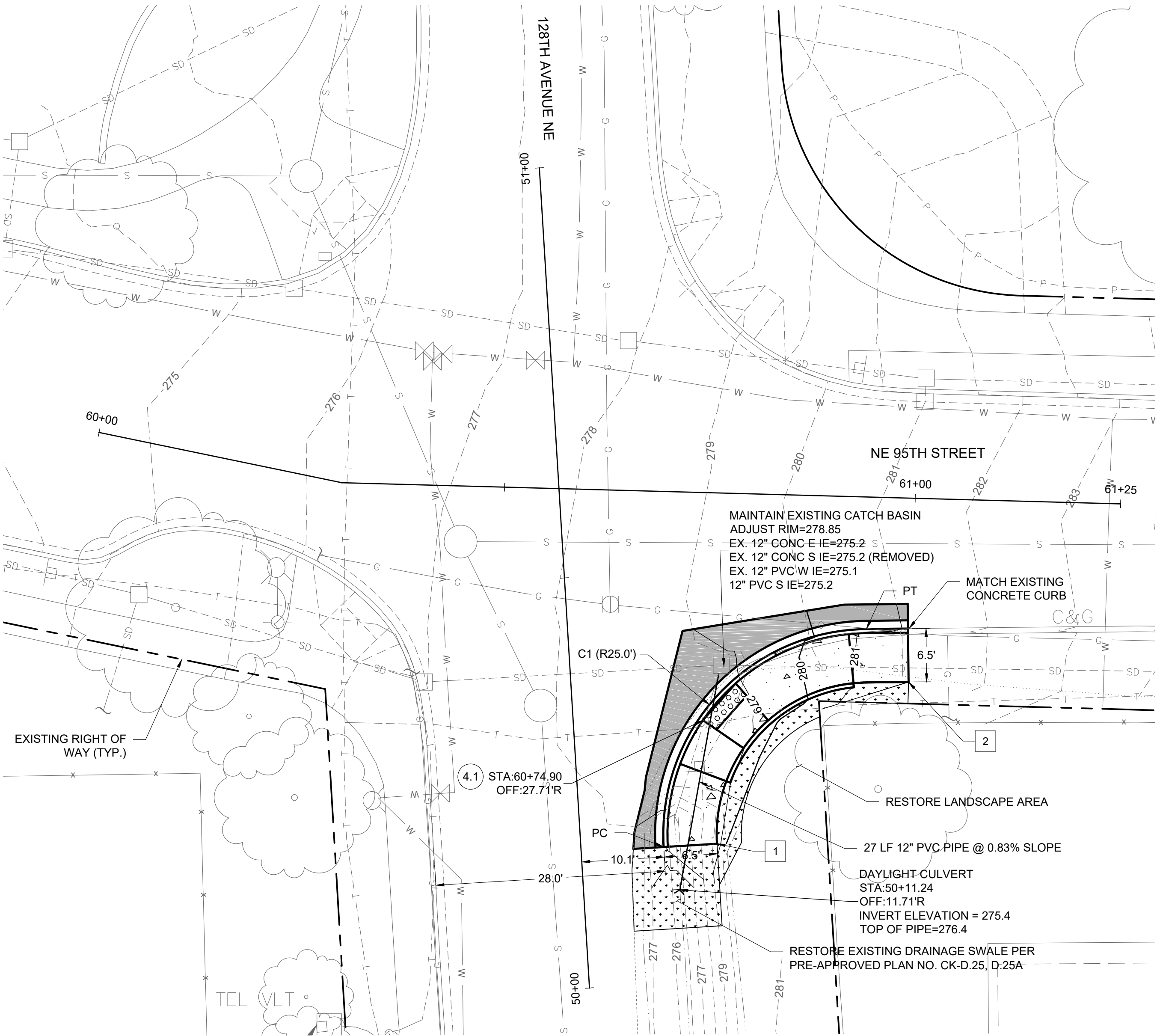
| # | PROP. ELEV | STA. OFF. |
|---|------------|---------------------------|
| 1 | 278.35' | STA:60+77.02, OFF:42.69'R |
| 2 | 281.37' | STA:60+99.79, OFF:22.37'R |

PAVING AND GRADING NOTES:

- SEE NT SHEET SERIES FOR GENERAL NOTES AND LEGEND.
- SEE SP SHEET SERIES FOR TREE PROTECTION.
- SEE AL AND IS SHEET SERIES FOR CENTERLINE AND ELEVATION INFORMATION.
- SEE CH SHEET SERIES FOR CROSSWALK LOCATIONS.
- SEE TS SHEET SERIES FOR SIGNAL INFORMATION.
- ALL DIMENSIONS ARE IN FEET, UNLESS OTHERWISE NOTED.
- ALL ELEVATIONS ARE TO BOTTOM OF CURB (FLOW LINE), UNLESS OTHERWISE NOTED.
- ALL CURBS ARE 6 INCHES TALL, UNLESS OTHERWISE NOTED.
- ADJUST UTILITY LID TO FINISHED GRADE.
- COORDINATE INSTALLATION OF CATCH BASIN WITH CITY ENGINEER
- PIPE INSTALLATION SHALL FOLLOW PRE-APPROVED PLAN NO. CK-D.02

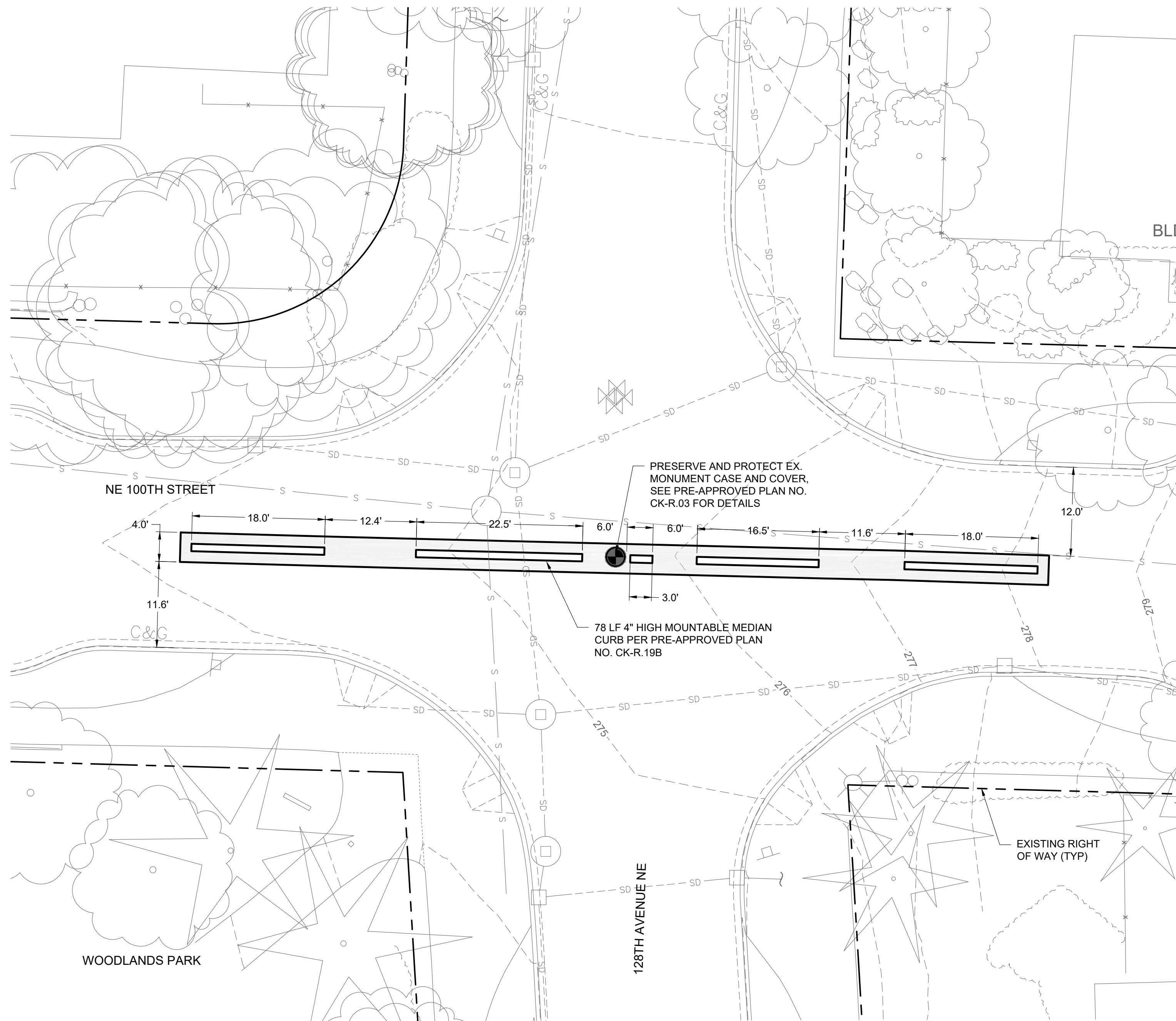
PAVING AND GRADING AND DRAINAGE LEGEND:

- RIGHT-OF-WAY
- CURB AND GUTTER PER PRE-APPROVED PLAN NO. CK-R.17
- VERTICAL CURB PER PRE-APPROVED PLAN NO. CK-R.17C
- FULL DEPTH HMA PER PRE-APPROVED PLAN NO. CK-R.09, PAVEMENT PATCH PER PRE-APPROVED PLAN NO. CK-R.12
- CEMENT CONCRETE RAMP OR SIDEWALK, PER PRE-APPROVED PLAN NO. CK-R.21, CK-R.23
- GRIND EXISTING HMA AND OVERLAY (DEPTH PER PLANS)
- LAWN
- EXISTING CONTOURS
- PROPOSED CONTOURS
- DETECTABLE WARNING SURFACE PER PRE-APPROVED PLAN NO. CK-R.25B
- EXISTING CATCH BASIN
- PROPOSED CATCH BASIN PER PRE-APPROVED PLAN NO. CK-D.06, D.07, D.08, D.09 (AS NOTED ON PLANS)



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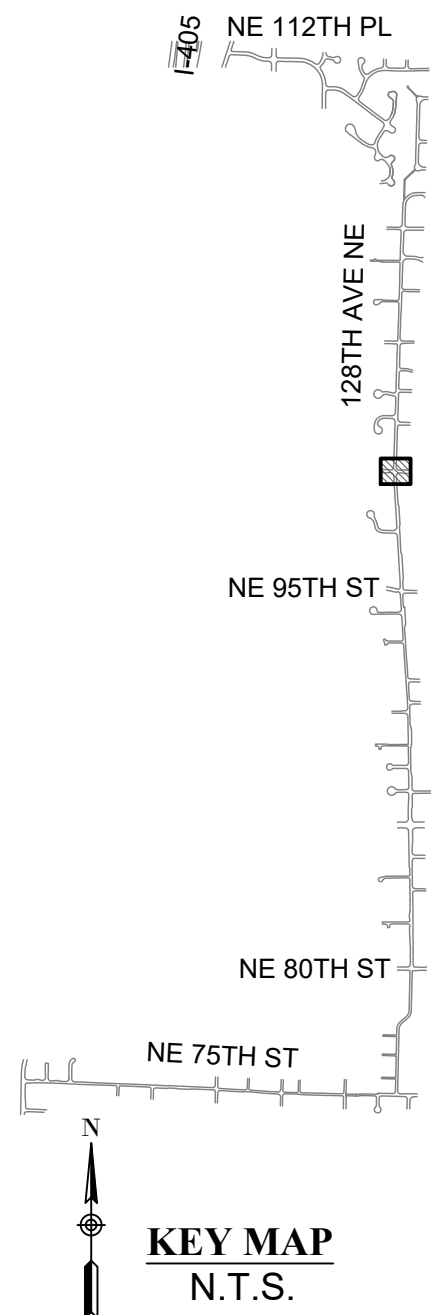


PAVING AND GRADING NOTES:

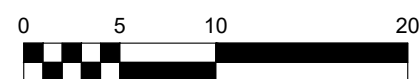
1. SEE NT SHEET SERIES FOR GENERAL NOTES AND LEGEND.
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8. ALL CURBS ARE 6 INCHES TALL, UNLESS OTHERWISE NOTED.
9. ADJUST UTILITY LID TO FINISHED GRADE.
10. COORDINATE INSTALLATION OF CATCH BASIN WITH CITY ENGINEER
11. PIPE INSTALLATION SHALL FOLLOW PRE-APPROVED PLAN NO. CK-D.02

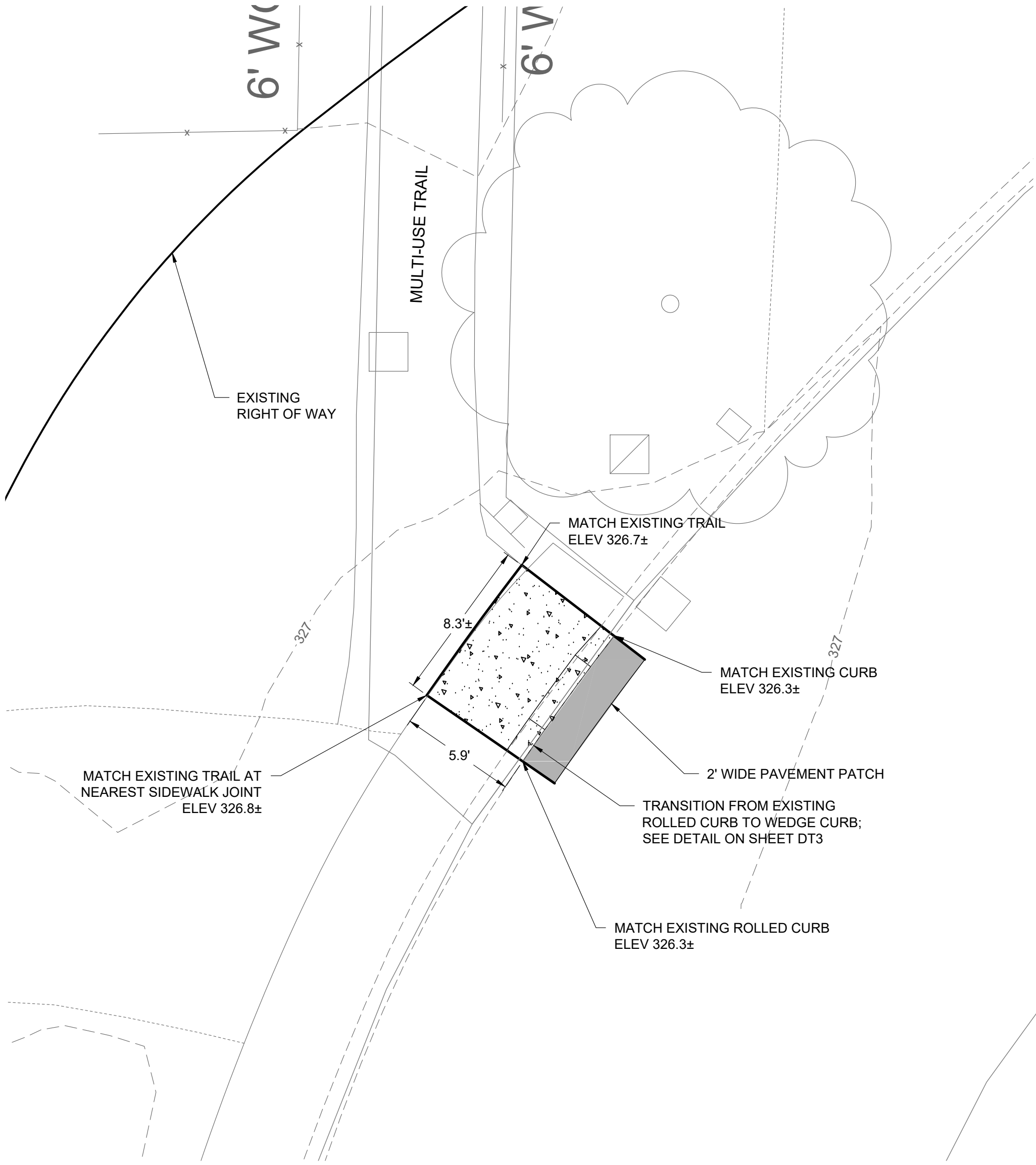
PAVING AND GRADING AND DRAINAGE LEGEND:

- RIGHT-OF-WAY
- === CURB AND GUTTER PER PRE-APPROVED PLAN NO. CK-R.17
- === VERTICAL CURB PER PRE-APPROVED PLAN NO. CK-R.17C
- FULL DEPTH HMA PER PRE-APPROVED PLAN NO. CK-R.09, PAVEMENT PATCH PER PRE-APPROVED PLAN NO. CK-R.12
- 4 CEMENT CONCRETE RAMP OR SIDEWALK, PER PRE-APPROVED PLAN NO. CK-R.21, CK-R.23
- GRIND EXISTING HMA AND OVERLAY (DEPTH PER PLANS)
- LAWN
- 120 - EXISTING CONTOURS
- 120 - PROPOSED CONTOURS
- DETECTABLE WARNING SURFACE PER PRE-APPROVED PLAN NO. CK-R.25B
- EXISTING CATCH BASIN
- ● PROPOSED CATCH BASIN PER PRE-APPROVED PLAN NO. CK-D.06, D.07, D.08, D.09 (AS NOTED ON PLANS)

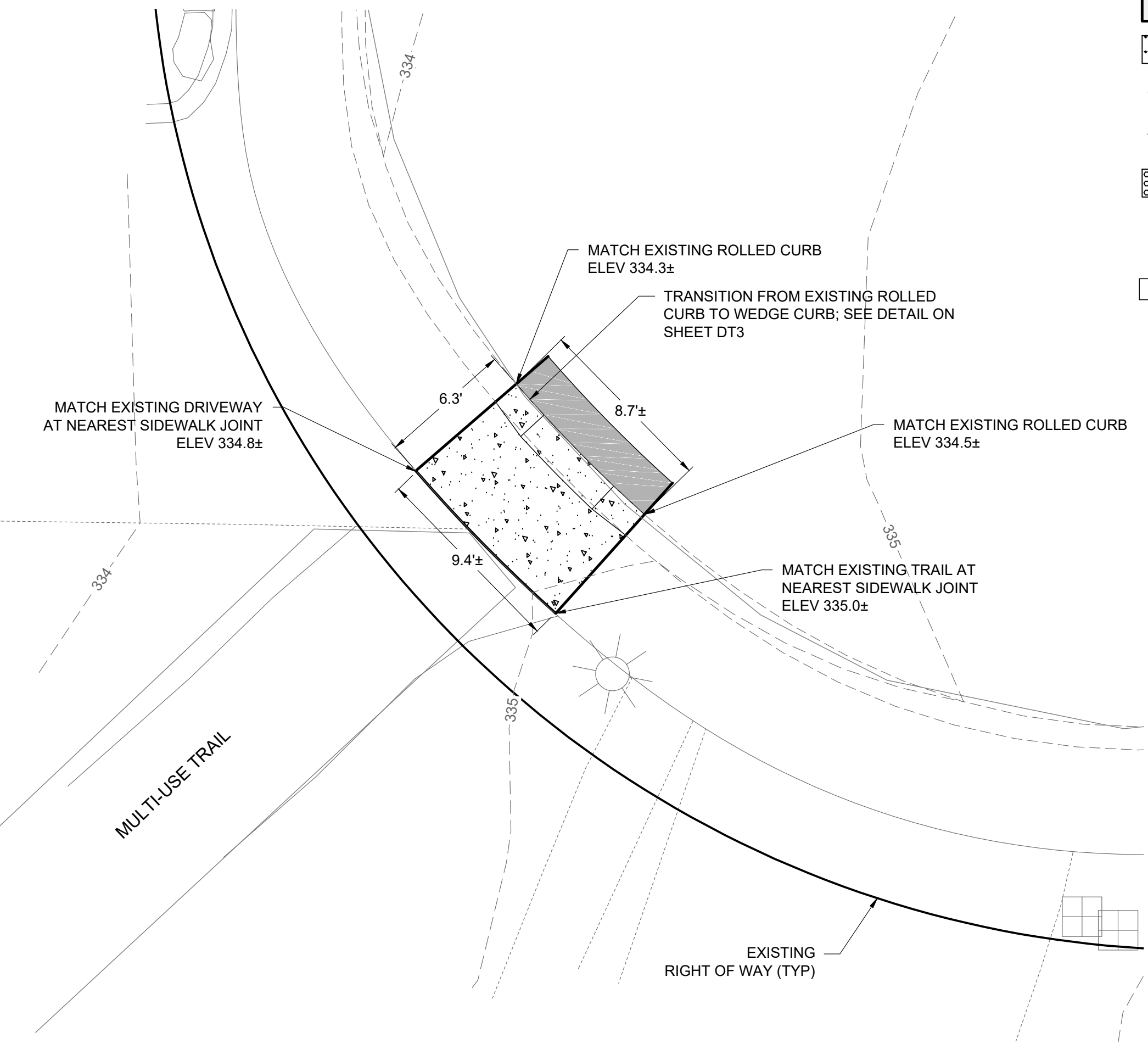


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RAMP DETAIL
NE 108TH PLACE
SCALE 1"=5'



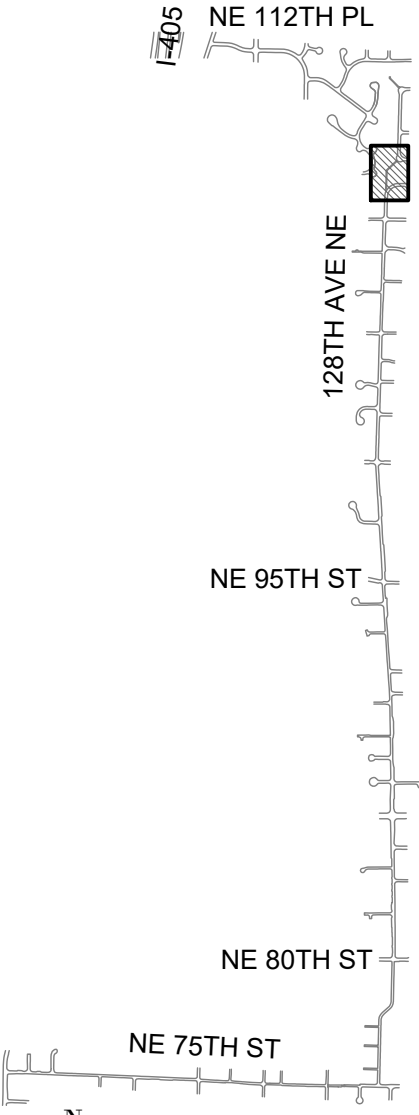
RAMP DETAIL
NE 109TH PLACE
SCALE 1"=5'

PAVING AND GRADING NOTES:

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2. SEE SP SHEET SERIES FOR TREE PROTECTION.
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8. ALL CURBS ARE 6 INCHES TALL, UNLESS OTHERWISE NOTED.
9. ADJUST UTILITY LID TO FINISHED GRADE.
10. COORDINATE INSTALLATION OF CATCH BASIN WITH CITY ENGINEER
11. PIPE INSTALLATION SHALL FOLLOW PRE-APPROVED PLAN NO. CK-D.02

PAVING AND GRADING AND DRAINAGE LEGEND:

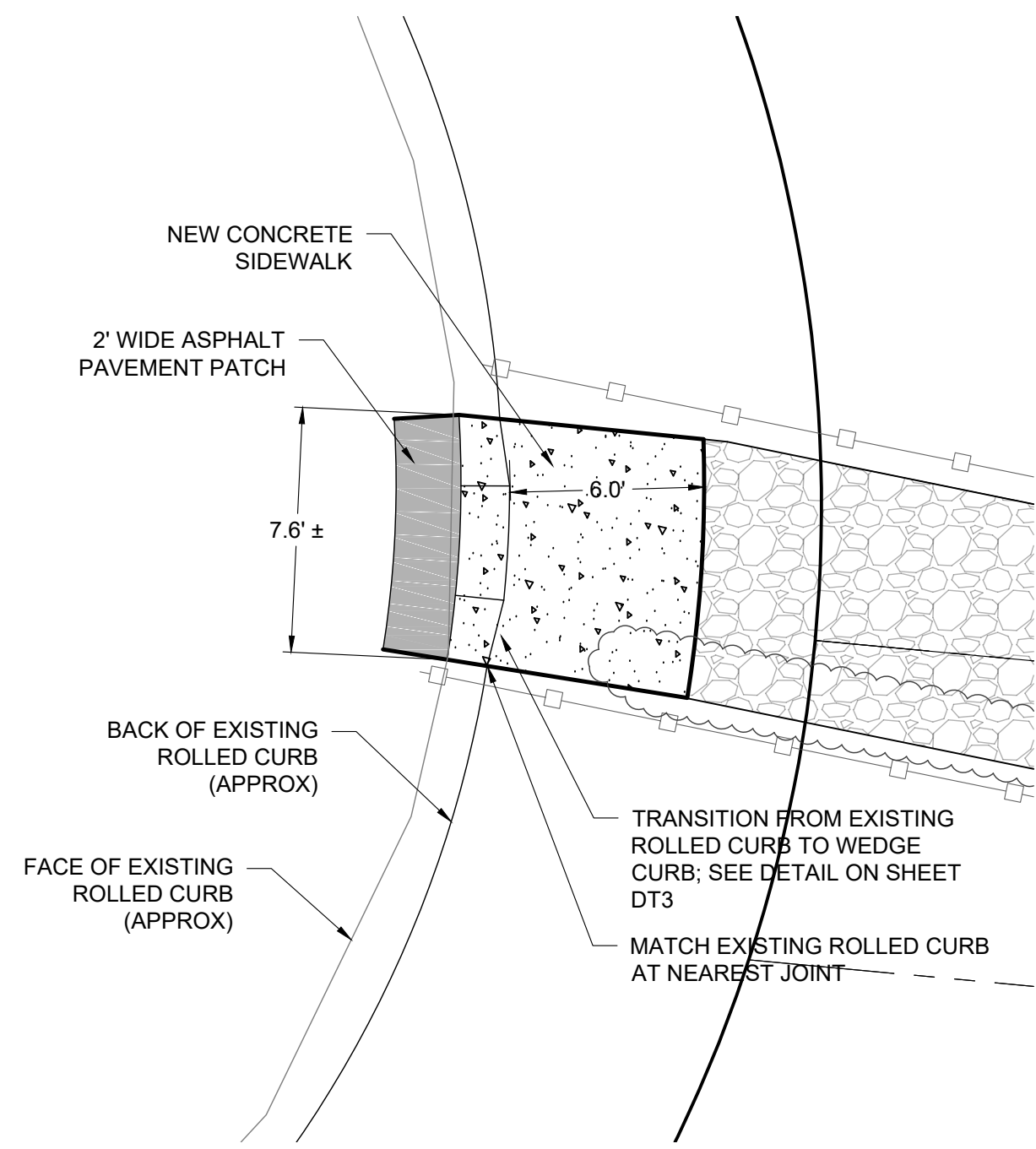
- RIGHT-OF-WAY
- ==== CURB AND GUTTER PER PRE-APPROVED PLAN NO. CK-R.17
- ==== VERTICAL CURB PER PRE-APPROVED PLAN NO. CK-R.17C
- [Hatched Box] FULL DEPTH HMA PER PRE-APPROVED PLAN NO. CK-R.09, PAVEMENT PATCH PER PRE-APPROVED PLAN NO. CK-R.12
- [Dotted Box] CEMENT CONCRETE RAMP OR SIDEWALK, PER PRE-APPROVED PLAN NO. CK-R.21, CK-R.23
- [White Box] GRIND EXISTING HMA AND OVERLAY (DEPTH PER PLANS)
- [Stippled Box] LAWN
- 120- EXISTING CONTOURS
- 120- PROPOSED CONTOURS
- [Cross-hatched Box] DETECTABLE WARNING SURFACE PER PRE-APPROVED PLAN NO. CK-R.25B
- [Square Box] EXISTING CATCH BASIN
- [Square Box with Circle] PROPOSED CATCH BASIN PER PRE-APPROVED PLAN NO. CK-D.07, D.08, D.09 (AS NOTED ON PLANS)



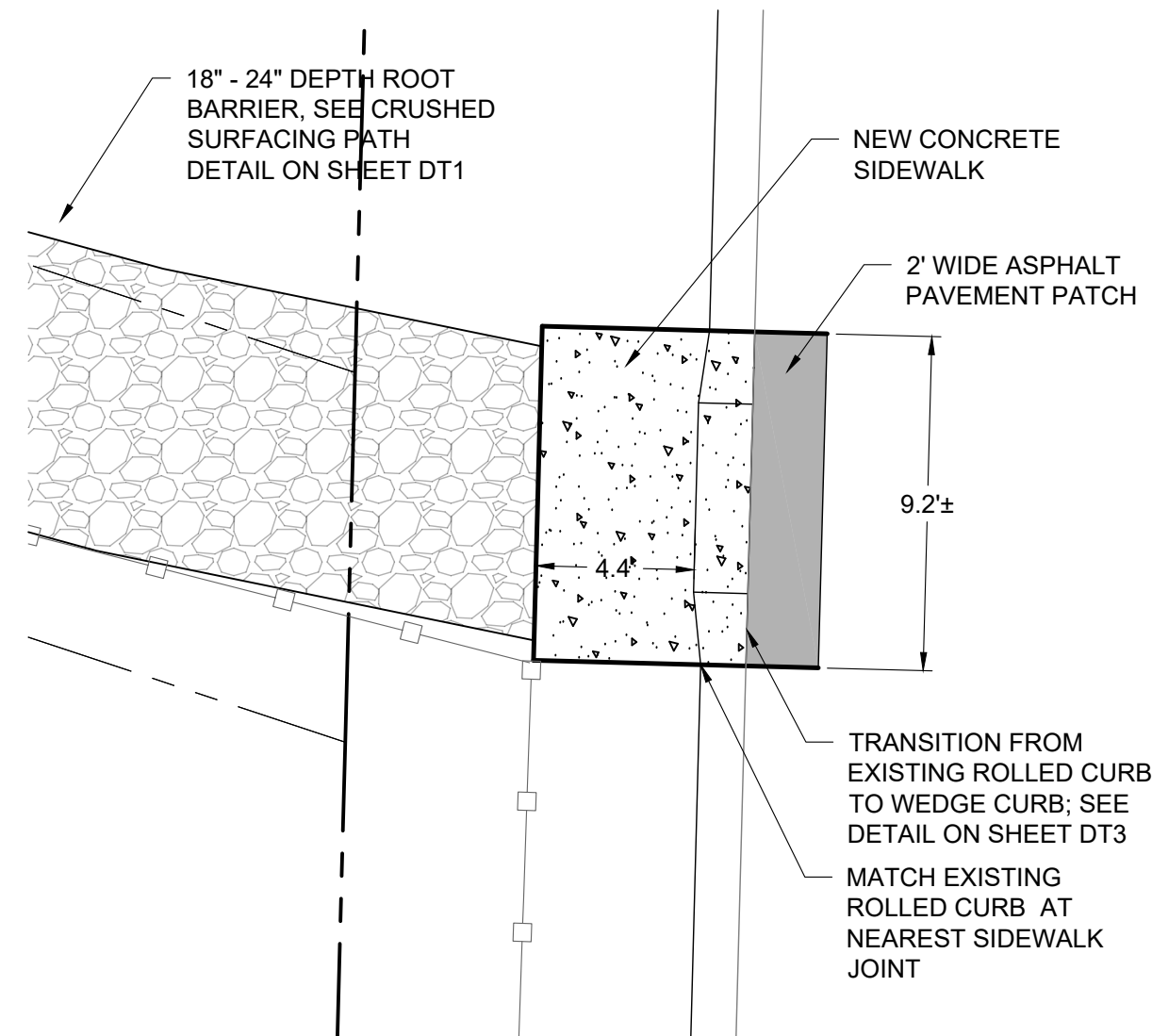
KEY MAP
N.T.S.

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RAMP DETAIL
NE 110TH PLACE
SCALE 1"=5'



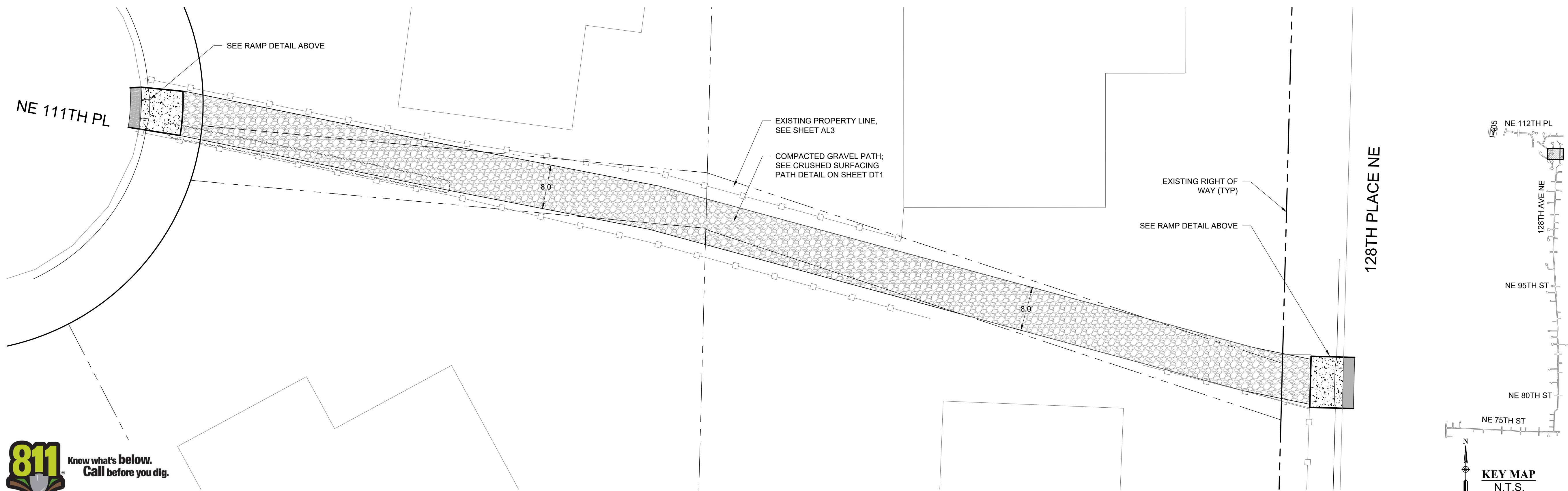
RAMP DETAIL
NE 110TH PLACE
SCALE 1"=5'

PAVING AND GRADING NOTES:

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11. PIPE INSTALLATION SHALL FOLLOW PRE-APPROVED PLAN NO. CK-D.02

PAVING AND GRADING AND DRAINAGE LEGEND:

- RIGHT-OF-WAY
- === CURB AND GUTTER PER PRE-APPROVED PLAN NO. CK-R.17
- === VERTICAL CURB PER PRE-APPROVED PLAN NO. CK-R.17C
- FULL DEPTH HMA PER PRE-APPROVED PLAN NO. CK-R.09, PAVEMENT PATCH PER PRE-APPROVED PLAN NO. CK-R.12
- CEMENT CONCRETE RAMP OR SIDEWALK, PER PRE-APPROVED PLAN NO. CK-R.21, CK-R.23
- GRIND EXISTING HMA AND OVERLAY (DEPTH PER PLANS)
- LAWN
- 120— EXISTING CONTOURS
- 120— PROPOSED CONTOURS
- DETECTABLE WARNING SURFACE PER PRE-APPROVED PLAN NO. CK-R.25B
- EXISTING CATCH BASIN
- PROPOSED CATCH BASIN PER PRE-APPROVED PLAN NO. CK-D.06, D.07, D.08, D.09 (AS NOTED ON PLANS)



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CURB RAMP TABLE

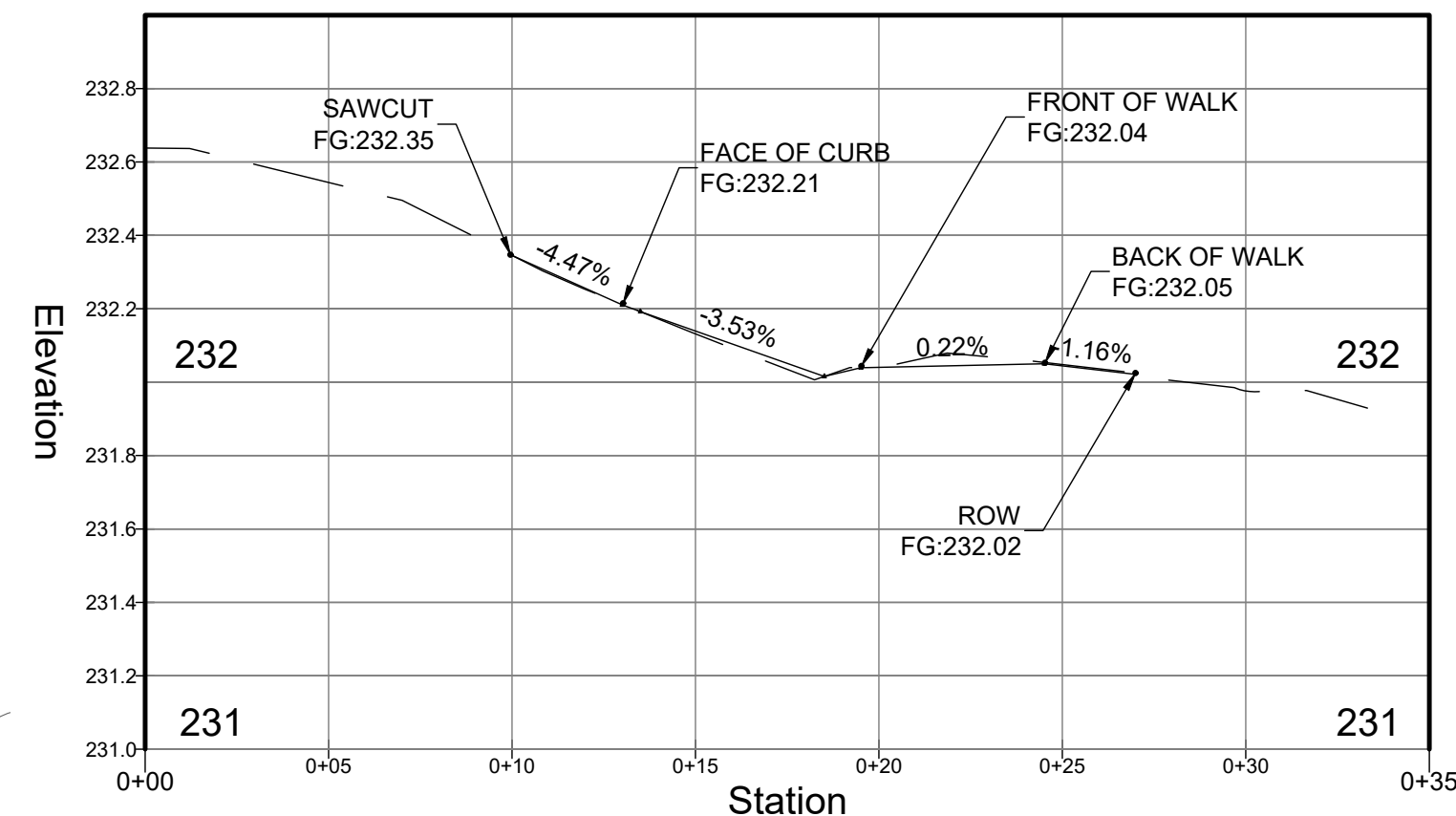
| # | LEFT | LENGTH | RIGHT | ELEV @ PT A | LOW SIDE | S1 % | S2 % | S3 % | S4 % | S5 % | S6 % |
|------|------|--------|-------|-------------|----------|------|------|------|------|------|------|
| 11.1 | 7' | 5' | 0' | 230.31' | RIGHT | 0.7 | 2.0 | 2.0 | 0.9 | 6.0 | N/A |
| 11.2 | 15" | 5' | 7' | 231.12' | RIGHT | 2.0 | 2.0 | 1.5 | 2.0 | 11.0 | -5.0 |
| 11.3 | 6" | 10' | 15" | 230.18' | LEFT | 1.8 | 1.9 | 2.0 | 2.0 | -6.4 | 11.7 |
| 11.4 | 6' | 7' | 6" | 229.58' | LEFT | 2.0 | 1.5 | 1.8 | 2.0 | 2.0 | 6.4 |

*MEF DOCUMENTATION NECESSARY

ELEVATION TABLE

| # | EX. ELEV | PROP. ELEV | STA. OFF. |
|---|----------|------------|----------------------------|
| 1 | 232.0' ± | 232.03' | STA:100+91.69, OFF:13.01'L |
| 2 | 232.2' ± | 232.21' | STA:100+94.69, OFF:13.01'L |
| 3 | 232.0' ± | 232.04' | STA:100+94.70, OFF:19.51'L |
| 4 | 233.0' ± | 232.96' | STA:101+04.69, OFF:13.01'L |
| 5 | 232.7' ± | 232.70' | STA:101+04.70, OFF:19.51'L |
| 6 | 233.2' ± | 233.20' | STA:101+07.69, OFF:13.04'L |
| 7 | 232.1' ± | 232.05' | STA:100+94.70, OFF:24.51'L |
| 8 | 232.5' ± | 232.80' | STA:101+04.70, OFF:24.51'L |

Driveway West Profile



PAVING AND GRADING NOTES:

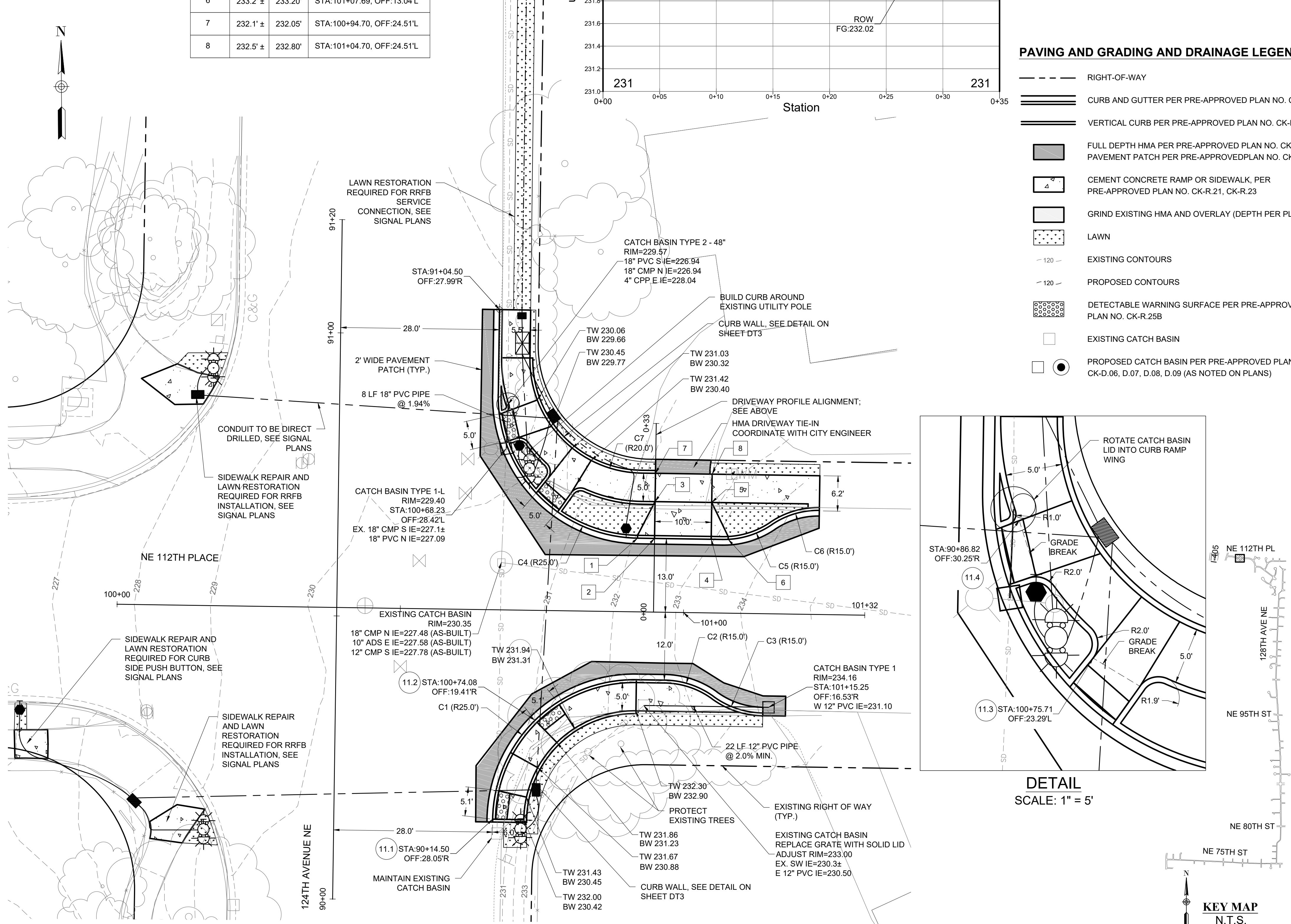
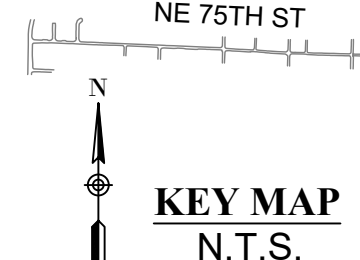
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- ALL ELEVATIONS ARE TO BOTTOM OF CURB (FLOW LINE), UNLESS OTHERWISE NOTED.
- ALL CURBS ARE 6 INCHES TALL, UNLESS OTHERWISE NOTED.
- ADJUST UTILITY LID TO FINISHED GRADE.
- COORDINATE INSTALLATION OF CATCH BASIN WITH CITY ENGINEER.
- PIPE INSTALLATION SHALL FOLLOW PRE-APPROVED PLAN NO. CK-D.02.

PAVING AND GRADING AND DRAINAGE LEGEND:

- RIGHT-OF-WAY
- ==== CURB AND GUTTER PER PRE-APPROVED PLAN NO. CK-R.17
- ==== VERTICAL CURB PER PRE-APPROVED PLAN NO. CK-R.17C
- FULL DEPTH HMA PER PRE-APPROVED PLAN NO. CK-R.09, PAVEMENT PATCH PER PRE-APPROVED PLAN NO. CK-R.12
- CEMENT CONCRETE RAMP OR SIDEWALK, PER PRE-APPROVED PLAN NO. CK-R.21, CK-R.23
- GRIND EXISTING HMA AND OVERLAY (DEPTH PER PLANS)
- LAWN
- EXISTING CONTOURS
- PROPOSED CONTOURS
- DETECTABLE WARNING SURFACE PER PRE-APPROVED PLAN NO. CK-R.25B
- EXISTING CATCH BASIN
- PROPOSED CATCH BASIN PER PRE-APPROVED PLAN NO. CK-D.06, D.07, D.08, D.09 (AS NOTED ON PLANS)

INTERSECTION CURB RETURN CURVE DATA
FLOWLINE ELEVATIONS

| | | | |
|--|----------------------------|---------|--|
| C1 L=39.27', R=25.0', D=90° 00' 14" | | | |
| PC | STA:90+14.50, OFF:28.05'R | 230.31' | |
| 1/2 | STA:100+74.18, OFF:19.30'R | 230.96' | |
| PT | STA:100+91.86, OFF:11.98'R | 232.30' | |
| C2 L=9.25', R=15.0', D=35° 21' 04" | | | |
| PC | STA:100+96.87, OFF:11.98'R | 232.81' | |
| 1/2 | STA:101+02.34, OFF:9.83'R | 233.10' | |
| PRC | STA:101+05.55, OFF:14.75'R | 233.37' | |
| C3 L=9.28', R=15.0', D=35° 27' 55" | | | |
| PRC | STA:101+05.55, OFF:14.75'R | 233.37' | |
| 1/2 | STA:101+10.59, OFF:13.95'R | 234.01' | |
| PT | STA:101+14.26, OFF:17.52'R | 234.06' | |
| C4 L=39.27', R=25.0', D=89° 59' 49" | | | |
| PC | STA:90+89.42, OFF:27.98'R | 229.36' | |
| 1/2 | STA:100+74.06, OFF:20.34'L | 229.81' | |
| PT | STA:100+91.74, OFF:13.02'L | 232.03' | |
| C5 L=4.38', R=15.0', D=16° 44' 26" | | | |
| PC | STA:101+06.74, OFF:13.02'L | 233.17' | |
| 1/2 | STA:101+11.19, OFF:13.69'L | 233.67' | |
| PRC | STA:101+15.24, OFF:15.66'L | 233.80' | |
| C6 L=9.19', R=15.0', D=35° 06' 09" | | | |
| PRC | STA:101+15.24, OFF:15.66'L | 234.13' | |
| 1/2 | STA:101+19.36, OFF:17.65'L | 234.42' | |
| PT | STA:101+23.89, OFF:18.30'L | 234.42' | |
| C7 L=31.41', R=20.0', D=89° 59' 49" | | | |
| PC | STA:90+96.00, OFF:33.48'R | 229.82' | |
| 1/2 | STA:100+78.10, OFF:30.38'L | 230.07' | |
| PT | STA:100+92.23, OFF:24.52'L | 231.69' | |

DETAIL
SCALE: 1" = 5'

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PREPARED: AL/TD

CHECKED: CS/KL

DATE
DECEMBER 2019

REV. 1

REV. 2

REV. 3

SHEET NAME
PAVING, GRADING
AND DRAINAGE
PLAN
DRAWING NUMBER

PV12

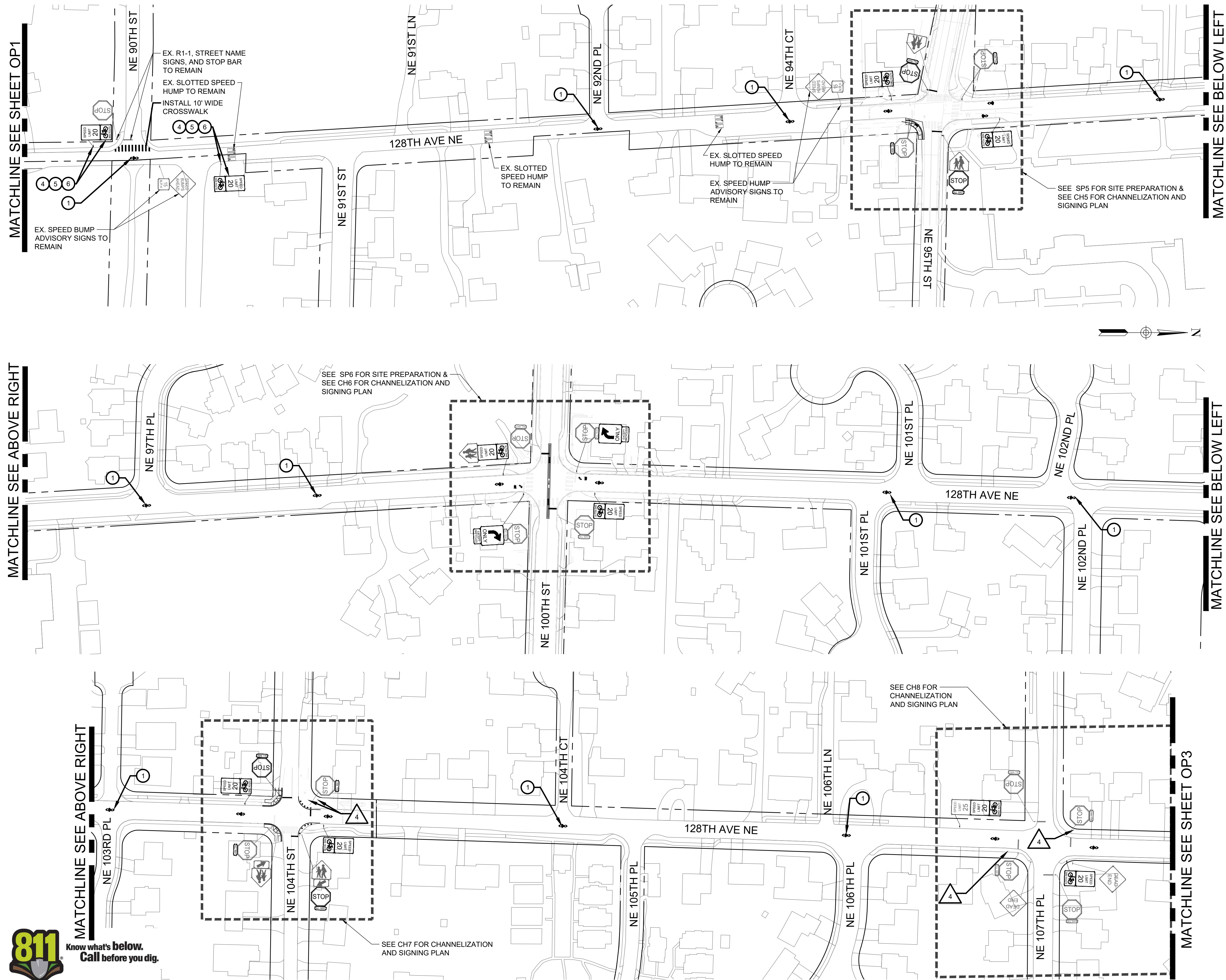
SHEET NUMBER

25 OF 49





Know what's below.
Call before you dig.



SITE PREPARATION NOTES:

1. SEE SP SHEET SERIES FOR ADDITIONAL SITE PREPARATION NOTES, AND DETAILED SITE PREPARATION PLANS.
2. SEE CH SHEET SERIES FOR ADDITIONAL SIGNAGE AND CHANNELIZATION, AND FOR SIGN REMOVALS.
3. SEE TS SHEETS FOR TRAFFIC SIGNAL REMOVALS.
4. SEE SPECIFICATIONS FOR TREE AND VEGETATION TRIMMING AND PRUNING REQUIREMENTS. ALL PRUNING SHALL BE PERFORMED BY A CERTIFIED ARBORIST AT THE DIRECTION OF THE ENGINEER. PRUNING AND TRIMMING MAY BE REQUIRED BY CITY OF KIRKLAND FOLLOWING SIGN INSTALLATION.
5. FLAG TREES TO BE REMOVED FOR ENGINEER'S APPROVAL PRIOR TO REMOVAL.

SITE PREPARATION CONSTRUCTION NOTES:

NOTES:

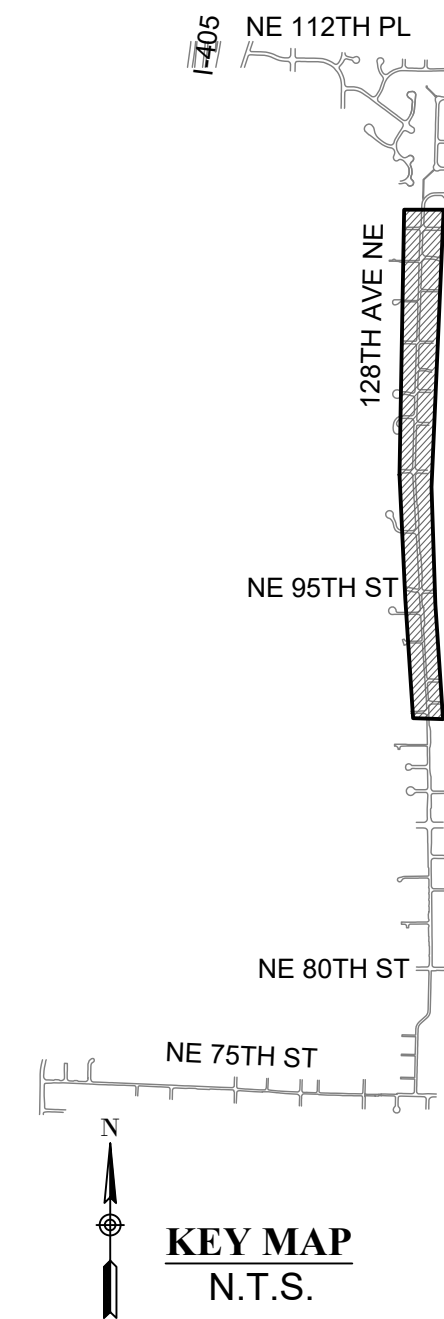
- 1 SAWCUT AND REMOVE EXISTING SPEED HUMP
- 2 REMOVE EXISTING STRIPING
- 3 TRIM/PRUNE TREE OR VEGETATION, SEE SITE PREPARATION NOTE 4
- 4 REMOVE EXISTING TREE, SEE SITE PREPARATION NOTE 5

CHANNELIZATION AND SIGNING NOTES:

1. SEE CH SHEET SERIES FOR ADDITIONAL CHANNELIZATION AND SIGNAGE NOTES, AND DETAILED CHANNELIZATION AND SIGNING PLANS
2. SEE DT SHEET SERIES FOR PAVEMENT MARKING AND SIGNAGE DETAILS.
3. SEE SP SHEET SERIES FOR TREE PROTECTION, TREE AND VEGETATION TRIMMING/PRUNING, AND CHANNELIZATION REMOVALS.
4. SEE PV SHEET SERIES FOR PAVING AND GRADING PLANS.

CHANNELIZATION AND SIGNING CONSTRUCTION NOTES:

- 1 INSTALL BIDIRECTIONAL SHARROW
- 2 INSTALL MODIFIED BIDIRECTIONAL BIKE SHARROW
- 3 INSTALL SLOTTED SPEED HUMP AND/OR SIGNAGE PER PRE-APPROVED PLANS NO. CK-R.67 & CK-R.67B
- 4 INSTALL D11-1 SIGN (24"x18")
- 5 INSTALL R2-20 SIGN (24"x30")
- 6 INSTALL 10' POST PER PRE-APPROVED PLAN NO. CK-R.44
- 7 INSTALL SHARED LANE MARKING PER PRE-APPROVED PLAN NO. CK-R.46
- 8 INSTALL BIKE LANE SYMBOL PER PRE-APPROVED PLAN NO. CK-R.34 & WSDOT STD. PLAN M-9.50-02
- 9 INSTALL R7-9A SIGN (12"x18")



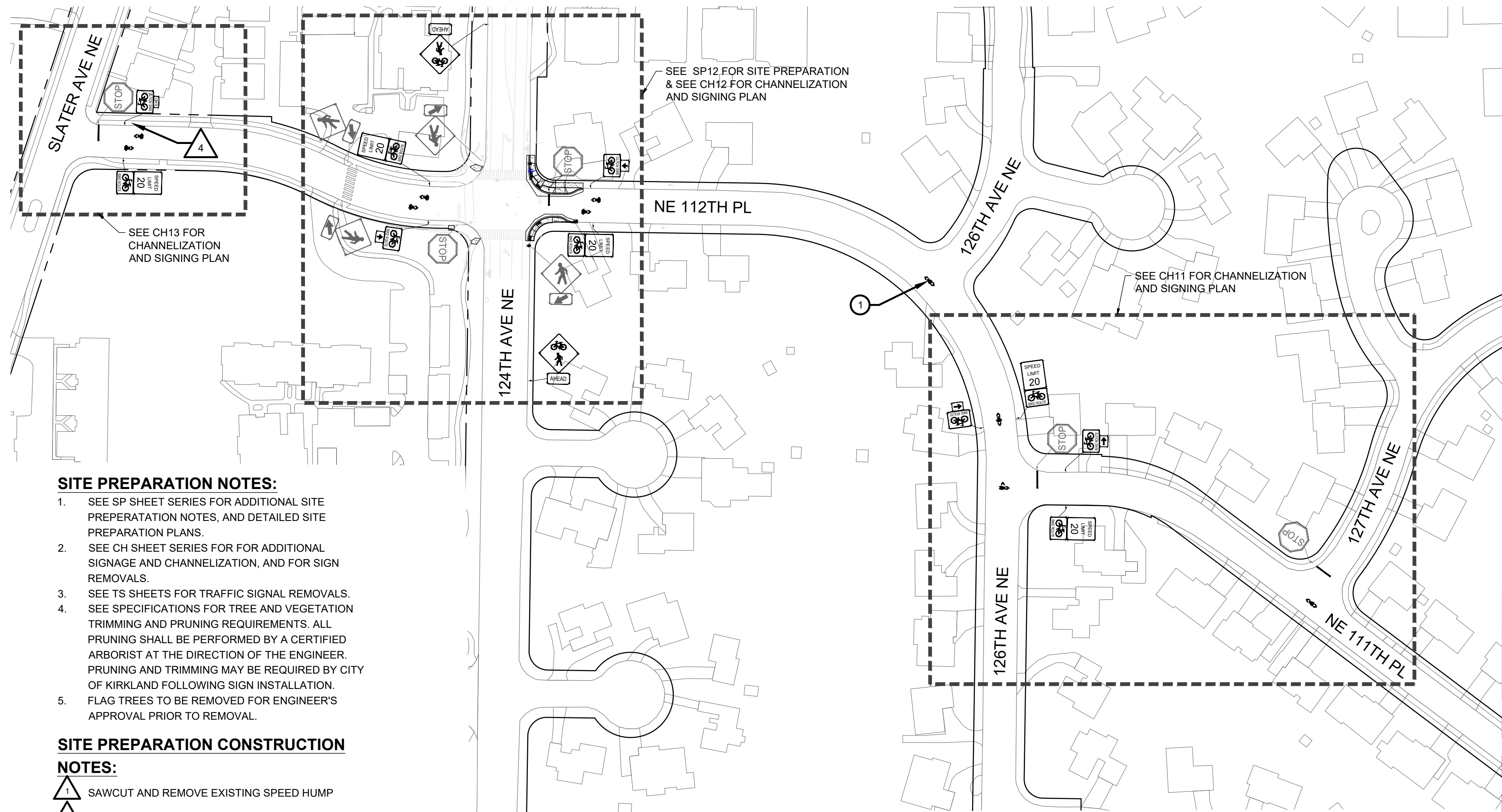
128TH AVE NE GREENWAY
KIRKLAND GREENWAYS
PROJECT# 25-18-PW
KIRKLAND, WA



PREPARED: AL/TD
CHECKED: CS/KL
DATE
DECEMBER 2019
REV. 1
REV. 2
REV. 3
SHEET NAME
OVERVIEW PLAN

DRAWING NUMBER
OP2
SHEET NUMBER
27 OF 49

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SITE PREPARATION NOTES:

1. SEE SP SHEET SERIES FOR ADDITIONAL SITE PREPARATION NOTES, AND DETAILED SITE PREPARATION PLANS.
2. SEE CH SHEET SERIES FOR FOR ADDITIONAL SIGNAGE AND CHANNELIZATION, AND FOR SIGN REMOVALS.
3. SEE TS SHEETS FOR TRAFFIC SIGNAL REMOVALS.
4. SEE SPECIFICATIONS FOR TREE AND VEGETATION TRIMMING AND PRUNING REQUIREMENTS. ALL PRUNING SHALL BE PERFORMED BY A CERTIFIED ARBORIST AT THE DIRECTION OF THE ENGINEER. PRUNING AND TRIMMING MAY BE REQUIRED BY CITY OF KIRKLAND FOLLOWING SIGN INSTALLATION.
5. FLAG TREES TO BE REMOVED FOR ENGINEER'S APPROVAL PRIOR TO REMOVAL.

SITE PREPARATION CONSTRUCTION NOTES:

1. SAWCUT AND REMOVE EXISTING SPEED HUMP
2. REMOVE EXISTING STRIPING
3. TRIM/PRUNE TREE OR VEGETATION. SEE SITE PREPARATION NOTE 4
4. REMOVE EXISTING TREE, SEE SITE PREPARATION NOTE 5

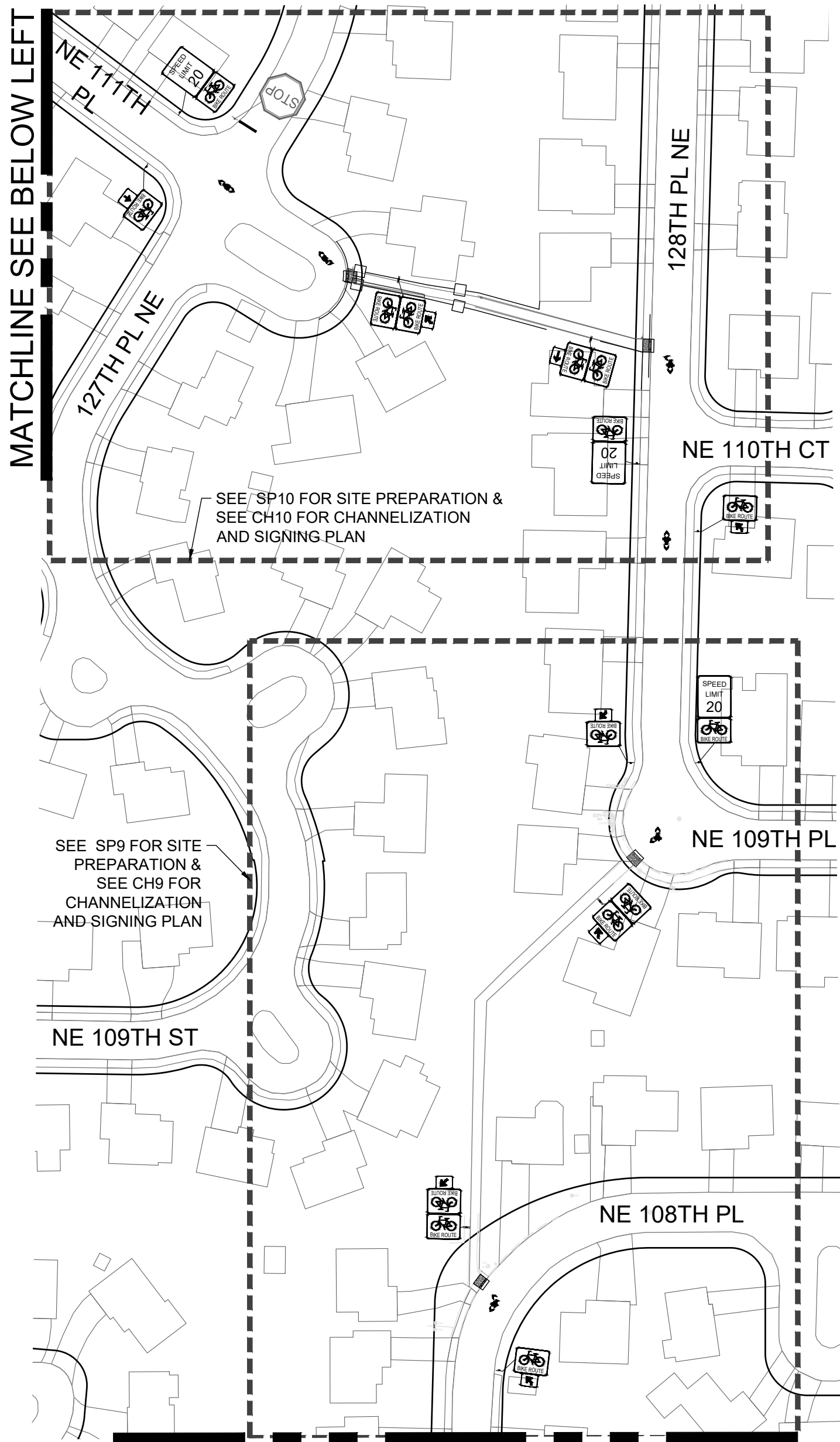
CHANNELIZATION AND SIGNING NOTES:

1. SEE CH SHEET SERIES FOR ADDITIONAL CHANNELIZATION AND SIGNAGE NOTES, AND DETAILED CHANNELIZATION AND SIGNING PLANS
2. SEE DT SHEET SERIES FOR PAVEMENT MARKING AND SIGNAGE DETAILS.
3. SEE SP SHEET SERIES FOR TREE PROTECTION, TREE AND VEGETATION TRIMMING/PRUNING, AND CHANNELIZATION REMOVALS.
4. SEE PV SHEET SERIES FOR PAVING AND GRADING PLANS.

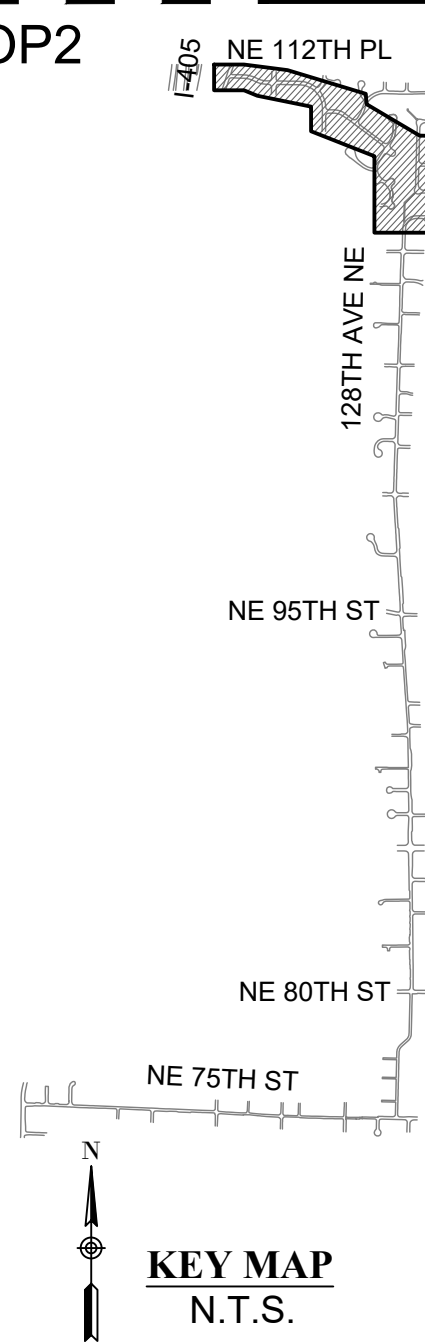
CHANNELIZATION AND SIGNING CONSTRUCTION NOTES:

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2. INSTALL MODIFIED BIDIRECTIONAL BIKE SHARROW
3. INSTALL SLOTTED SPEED HUMP AND/OR SIGNAGE PER PRE-APPROVED PLANS NO. CK-R.67 & CK-R.67B
4. INSTALL D11-1 SIGN (24"x18")
5. INSTALL R2-20 SIGN (24"x30")
6. INSTALL 10' POST PER PRE-APPROVED PLAN NO. CK-R.44
7. INSTALL SHARED LANE MARKING PER PRE-APPROVED PLAN NO. CK-R.46
8. INSTALL BIKE LANE SYMBOL PER PRE-APPROVED PLAN NO. CK-R.34 & WSDOT STD. PLAN M-9.50-02
9. INSTALL R7-9A SIGN (12"x18")

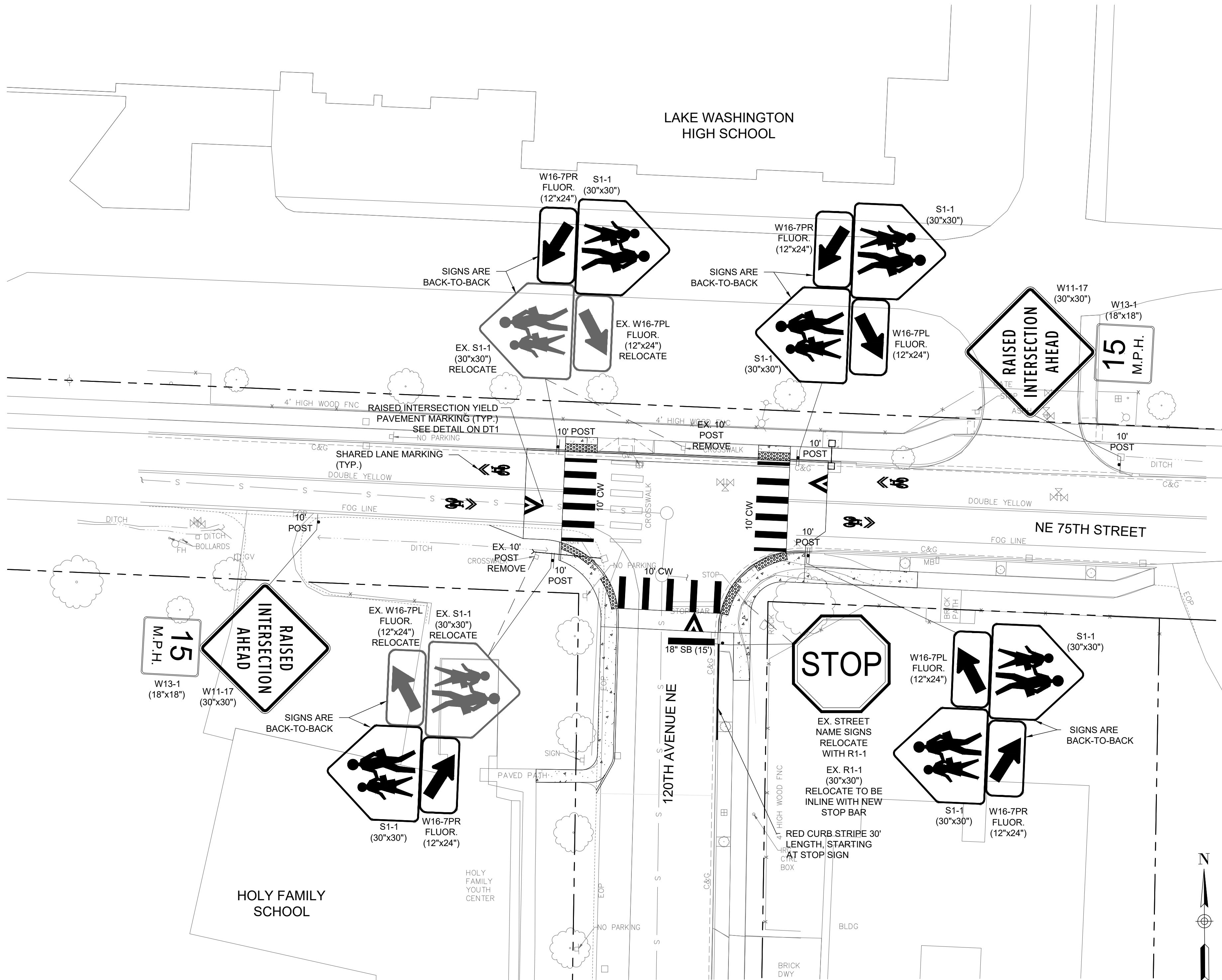
MATCHLINE SEE ABOVE RIGHT



MATCHLINE SEE SHEET OP2



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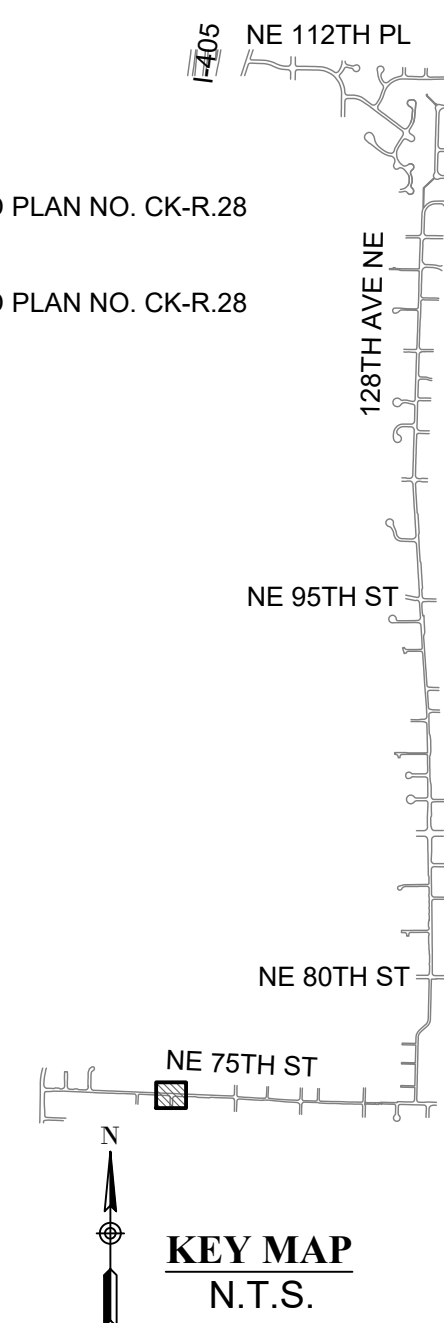


CHANNELIZATION AND SIGNING NOTES:

1. SEE NT SHEET SERIES FOR GENERAL NOTES AND LEGEND.
2. SEE DT SHEET SERIES FOR PAVEMENT MARKING AND SIGNAGE DETAILS.
3. SEE SP SHEET SERIES FOR TREE PROTECTION, TREE AND VEGETATION TRIMMING, AND CHANNELIZATION REMOVALS.
4. SEE PV SHEET SERIES FOR PAVING AND GRADING PLANS.
5. SEE OP SHEET SERIES FOR OVERVIEW PLANS SHOWING SITE PREPARATION, CHANNELIZATION AND SIGNAGE NOT INCLUDED ON THE SP AND CH PLANS.
6. ALL DIMENSIONS ARE IN FEET, UNLESS OTHERWISE NOTED.
7. SIGN AND SYMBOL MARKING LOCATIONS SHALL BE VERIFIED BY THE ENGINEER PRIOR TO INSTALLATION. LOCATIONS SHOWN ARE APPROXIMATE ONLY. ACTUAL LOCATIONS SHALL BE DETERMINED IN THE FIELD BASED ON CONDITIONS. SIGNS SHALL NOT BE PLACED TO CONFLICT WITH DRIVEWAYS, UTILITIES, OR VEGETATION. SIGNS SHALL NOT BE PLACED WHERE SIGHT LINES TO THE SIGN ARE BLOCKED BY TREES.
8. SIGNS NOTED "TO BE REMOVED" SHALL INCLUDE REMOVAL AND SALVAGE OF SIGN AND POST PER SPECIFICATIONS UNLESS OTHERWISE NOTED.
9. REPLACE ALL R2-25 "25 SPEED LIMIT" SIGNS WITH R2-20 "20 SPEED LIMIT" SIGNS ALONG THE KIRKLAND GREENWAY ROUTE.

CHANNELIZATION AND SIGNING LEGEND ITEM DETAIL

| | | |
|----------|--|---|
| 4" SYL | 4" SOLID YELLOW LINE | PRE-APPROVED PLAN NO. CK-R-31 |
| 4" DYL | 4" DASHED YELLOW LINE | PRE-APPROVED PLAN NO. CK-R-31 |
| | 4" YELLOW TWO-WAY LEFT TURN LANE LINES | PRE-APPROVED PLAN NO. CK-R-31 |
| 6" SWL | 6" SOLID WHITE LINE | PRE-APPROVED PLAN NO. CK-R-31 |
| 6" DWL | 6" DASHED WHITE LINE | PRE-APPROVED PLAN NO. CK-R-31 |
| 6" DYL | 6" DASHED YELLOW LINE | PRE-APPROVED PLAN NO. CK-R-31 |
| 6" DSYCL | 6" DOUBLE SOLID YELLOW CENTER LINE | PRE-APPROVED PLAN NO. CK-R-31 |
| 12" MMC | 12" MOUNTABLE MEDIAN CURB | PRE-APPROVED PLAN NO. CK-R-19B |
| | SIGN AND POST (POST HT. PER PLANS) | PRE-APPROVED PLANS NO. CK-R-43 & CK-R-44 |
| | SIGN | |
| | BICYCLE LANE SYMBOL | PRE-APPROVED PLAN NO. CK-R-34 & WSDOT STD. PLAN M-9.50-02 |
| | SHARED LANE MARKING (SHARROW) | PRE-APPROVED PLAN NO. CK-R-46 |
| | BI-DIRECTIONAL SHARROW | 2 DT2, 3 DT2, 1 DT2 |
| | MODIFIED BI-DIRECTIONAL SHARROW | 4 DT2 |
| | BIKE DOT | 5 DT2 |
| | WHITE PAVEMENT MARKING | WSDOT STD. PLANS M-80.10-01 & M-24.40-02 |
| | BICYCLE CROSSING PAVEMENT MARKING | 2 DT1, 1 DT1 |
| | FLEXIBLE DELINEATOR POST | |
| | 10' CROSSWALK | PRE-APPROVED PLAN NO. CK-R-28 |
| | 18" STOP BAR | PRE-APPROVED PLAN NO. CK-R-28 |
| | YIELD SYMBOL PAVEMENT MARKING | 4 DT1 |



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128TH AVE NE GREENWAY KIRKLAND GREENWAYS PROJECT# 25-18-PW KIRKLAND, WA



PREPARED: AL/TD

CHECKED: CS/KL

DATE
DECEMBER 2019

REV. 1

REV. 2

REV. 3

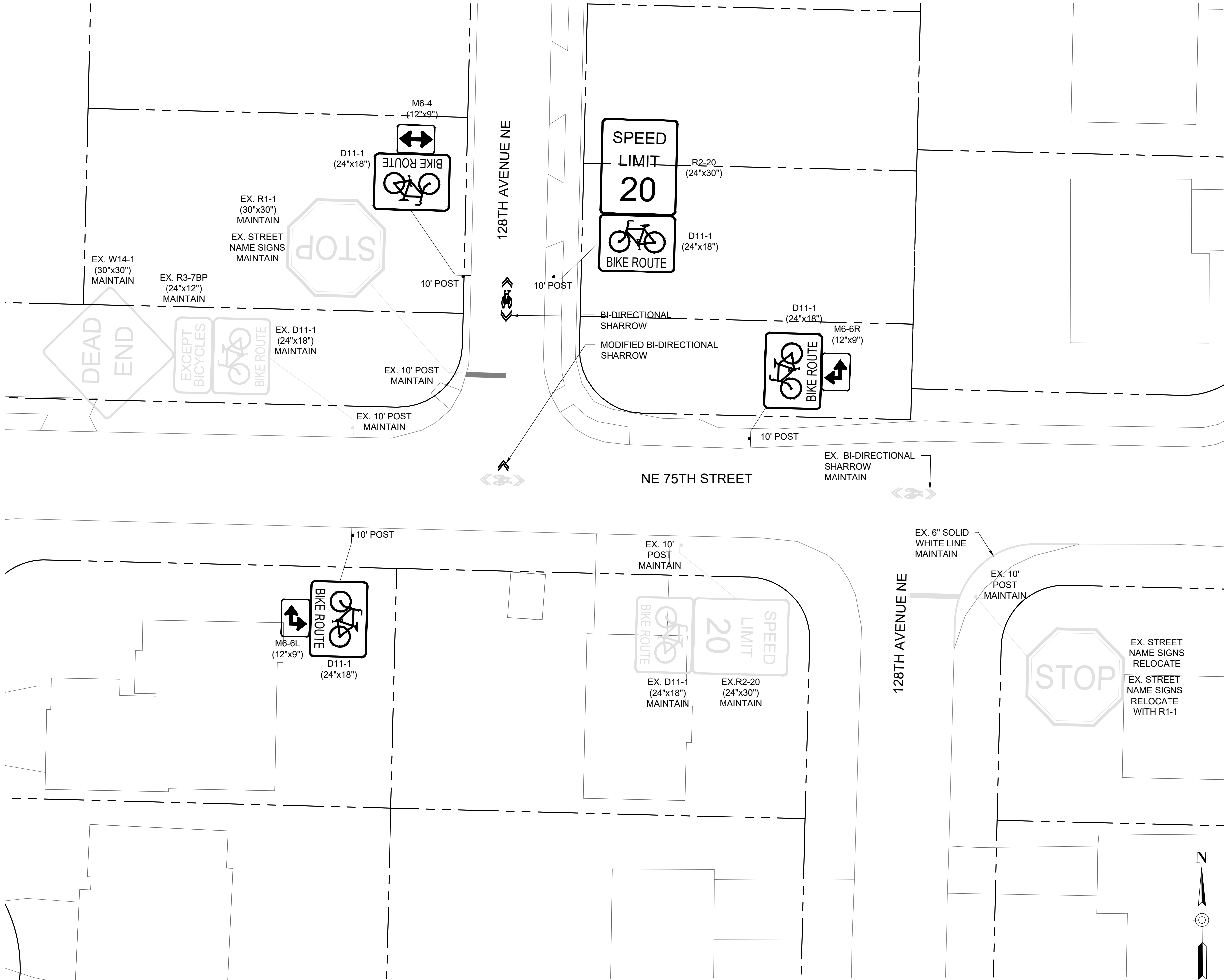
SHEET NAME
CHANNELIZATION
AND SIGNING PLAN

DRAWING NUMBER

CH1

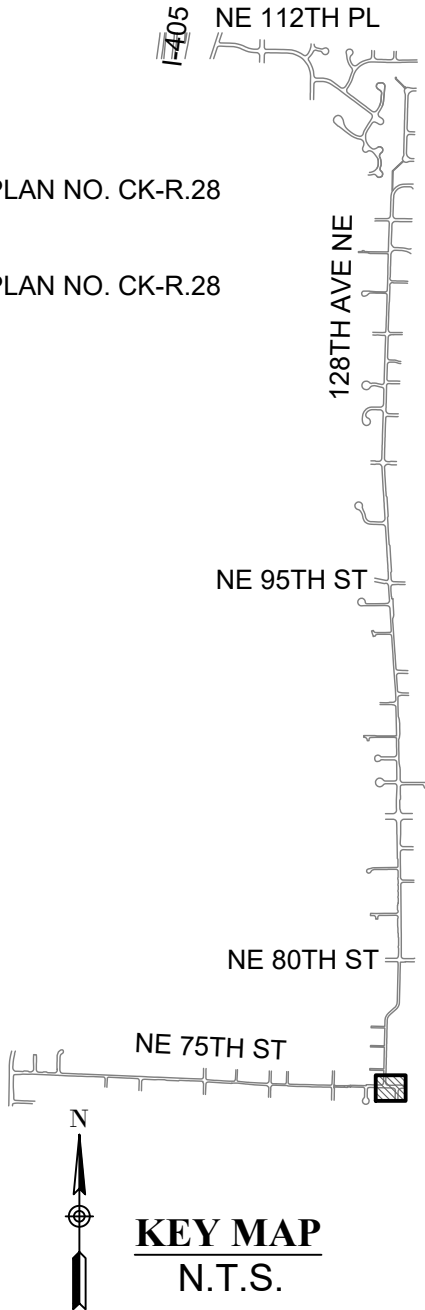
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29 OF 49



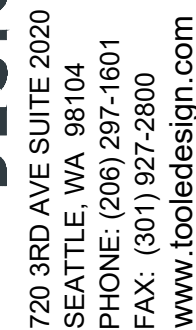
- CHANNELIZATION AND SIGNING NOTES:**
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 7. SIGN AND SYMBOL MARKING LOCATIONS SHALL BE VERIFIED BY THE ENGINEER PRIOR TO INSTALLATION. LOCATIONS SHOWN ARE APPROXIMATE ONLY. ACTUAL LOCATIONS SHALL BE DETERMINED IN THE FIELD BASED ON CONDITIONS. SIGNS SHALL NOT BE PLACED TO CONFLICT WITH DRIVEWAYS, UTILITIES, OR VEGETATION. SIGNS SHALL NOT BE PLACED WHERE SIGHT LINES TO THE SIGN ARE BLOCKED BY TREES.
 8. SIGNS NOTED "TO BE REMOVED" SHALL INCLUDE REMOVAL AND SALVAGE OF SIGN AND POST PER SPECIFICATIONS UNLESS OTHERWISE NOTED.
 9. REPLACE ALL R2-25 "25 SPEED LIMIT" SIGNS WITH R2-20 "20 SPEED LIMIT" SIGNS ALONG THE KIRKLAND GREENWAY ROUTE.

| CHANNELIZATION AND SIGNING LEGEND | |
|-------------------------------------|---|
| ITEM | DETAIL |
| 4" SYL | 4" SOLID YELLOW LINE |
| 4" DYL | 4" DASHED YELLOW LINE |
| 4" YLW TWO-WAY LEFT TURN LANE LINES | PRE-APPROVED PLAN NO. CK-R.31 |
| 6" SWL | 6" SOLID WHITE LINE |
| 6" DWL | 6" DASHED WHITE LINE |
| 6" DYL | 6" DASHED YELLOW LINE |
| 6" DSYCL | 6" DOUBLE SOLID YELLOW CENTER LINE |
| 12" MMC | 12" MOUNTABLE MEDIAN CURB |
| SIGN AND POST (POST HT. PER PLANS) | PRE-APPROVED PLANS NO. CK-R.43 & CK-R.44 |
| STOP | SIGN |
| BICYCLE LANE SYMBOL | PRE-APPROVED PLAN NO. CK-R.34 & WSDOT STD. PLAN M-9.50-02 |
| SHARED LANE MARKING (SHARROW) | PRE-APPROVED PLAN NO. CK-R.46 |
| BI-DIRECTIONAL SHARROW | |
| MODIFIED BI-DIRECTIONAL SHARROW | |
| BIKE DOT | |
| WHITE PAVEMENT MARKING | WSDOT STD. PLANS M-80.10-01 & M-24.40-02 |
| BICYCLE CROSSING PAVEMENT MARKING | |
| FLEXIBLE DELINEATOR POST | |
| 10' CROSSWALK | PRE-APPROVED PLAN NO. CK-R.28 |
| 18" STOP BAR | PRE-APPROVED PLAN NO. CK-R.28 |
| YIELD SYMBOL PAVEMENT MARKING | |



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128TH AVE NE GREENWAY
KIRKLAND GREENWAYS
PROJECT# 25-18-PW
KIRKLAND WA



CHECKED: CS/KL

DATE
DECEMBER 2019

REV. 1
REV. 2
REV. 3

SHEET NAME
CHANNELIZATION
AND SIGNING PLAN

DRAWING NUMBER

CH3














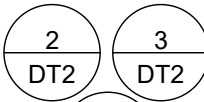
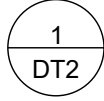














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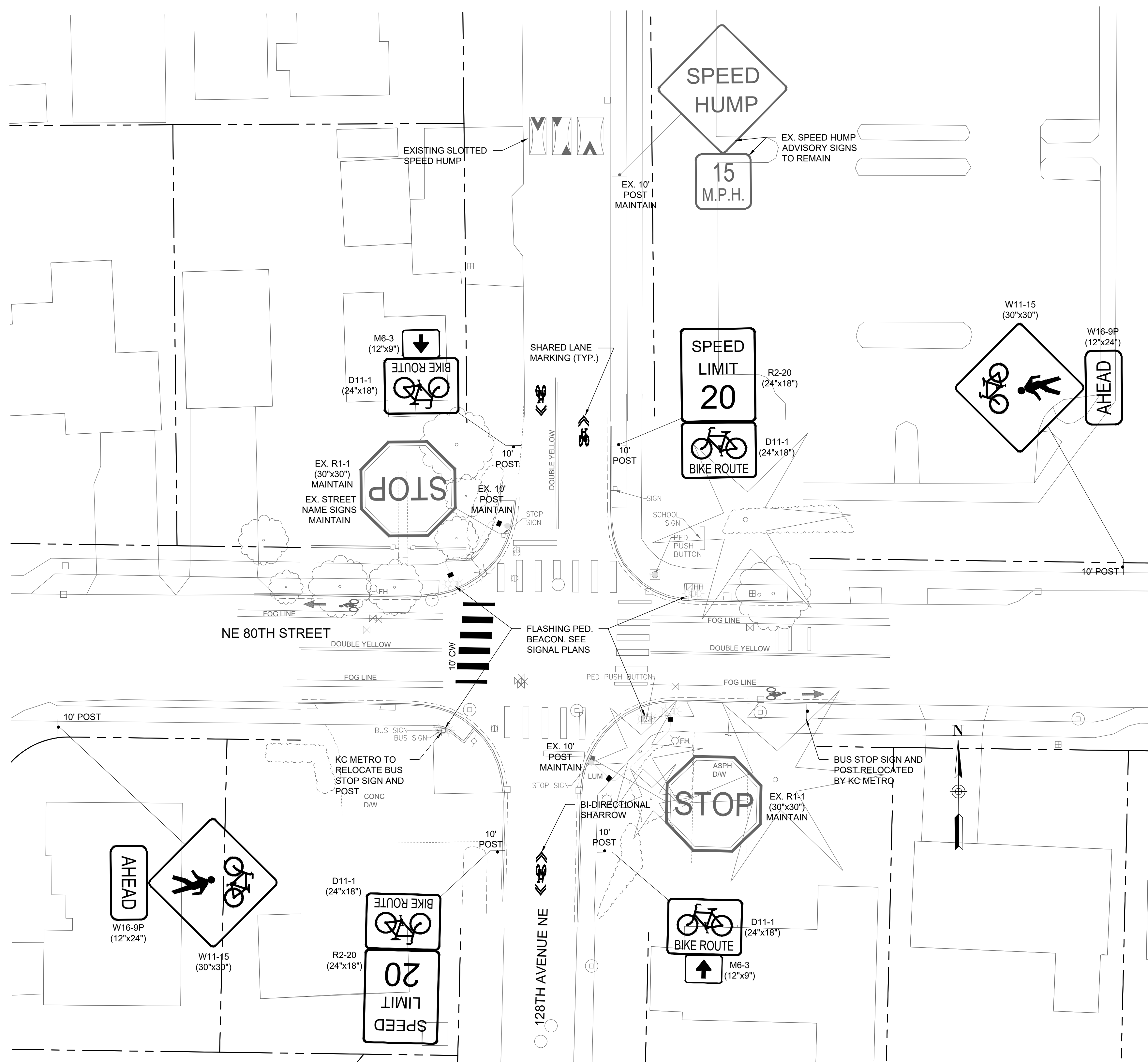
31 OF 49

CHANNELIZATION AND SIGNING NOTES:

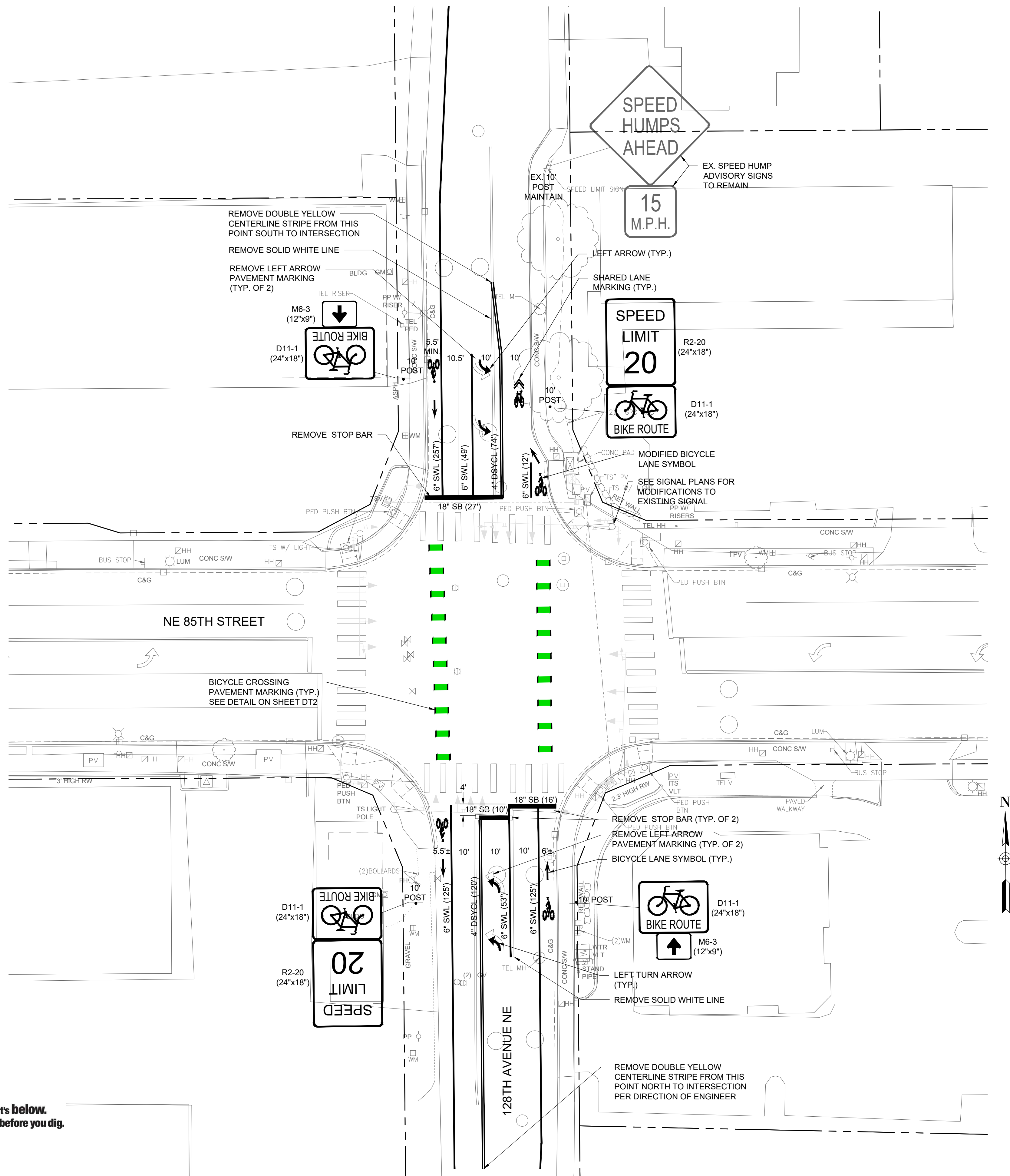
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9. REPLACE ALL R2-25 "25 SPEED LIMIT" SIGNS WITH R2-20 "20 SPEED LIMIT" SIGNS ALONG THE KIRKLAND GREENWAY ROUTE.

| CHANNELIZATION AND SIGNING LEGEND | |
|-----------------------------------|--------|
| ITEM | DETAIL |

| | | | |
|---|--|---|---|
|  | 4" SOLID YELLOW LINE | | |
|  | 4" DASHED YELLOW LINE | | PRE-APPROVED PLAN NO. CK-R.31 |
|  | 4" YELLOW TWO-WAY LEFT TURN LANE LINES | | PRE-APPROVED PLAN NO. CK-R.31 |
|  | 6" SOLID WHITE LINE | | |
|  | 6" DASHED WHITE LINE | | PRE-APPROVED PLAN NO. CK-R.31 |
|  | 6" DASHED YELLOW LINE | | PRE-APPROVED PLAN NO. CK-R.31 |
|  | 6" DOUBLE SOLID YELLOW CENTER LINE | | PRE-APPROVED PLAN NO. CK-R.31 |
|  | 12" MOUNTABLE MEDIAN CURB | | PRE-APPROVED PLAN NO. CK-R.19B |
|  | SIGN AND POST (POST HT. PER PLANS) | | PRE-APPROVED PLANS NO. CK-R.43 & CK-R.44 |
|  | SIGN | | |
|  | BICYCLE LANE SYMBOL | | PRE-APPROVED PLAN NO. CK-R.34 & WSDOT STD. PLAN M-9.50-02 |
|  | SHARED LANE MARKING (SHARROW) | | PRE-APPROVED PLAN NO. CK-R.46 |
|  | BI-DIRECTIONAL SHARROW |  |  |
|  | MODIFIED BI-DIRECTIONAL SHARROW |  | |
|  | BIKE DOT |  | |
|  | WHITE PAVEMENT MARKING |  | |
|  | BICYCLE CROSSING PAVEMENT MARKING | | WSDOT STD. PLANS M-80.10-01 & M-24.40-02 |
|  | FLEXIBLE DELINEATOR POST |  |  |
|  | 10' CROSSWALK | | PRE-APPROVED PLAN NO. CK-R.28 |
|  | 18" STOP BAR | | PRE-APPROVED PLAN NO. CK-R.28 |
|  | YIELD SYMBOL PAVEMENT MARKING |  | |



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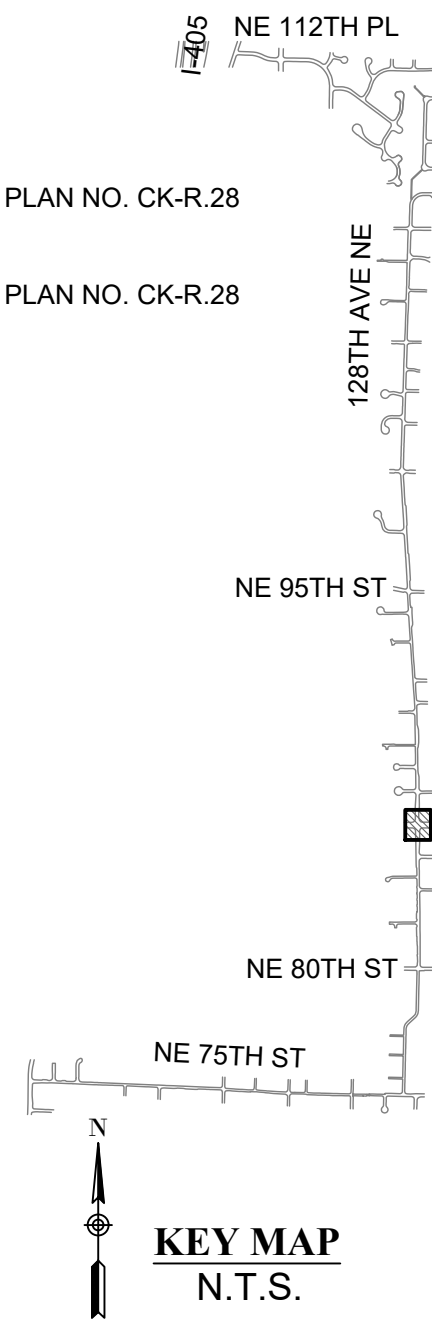


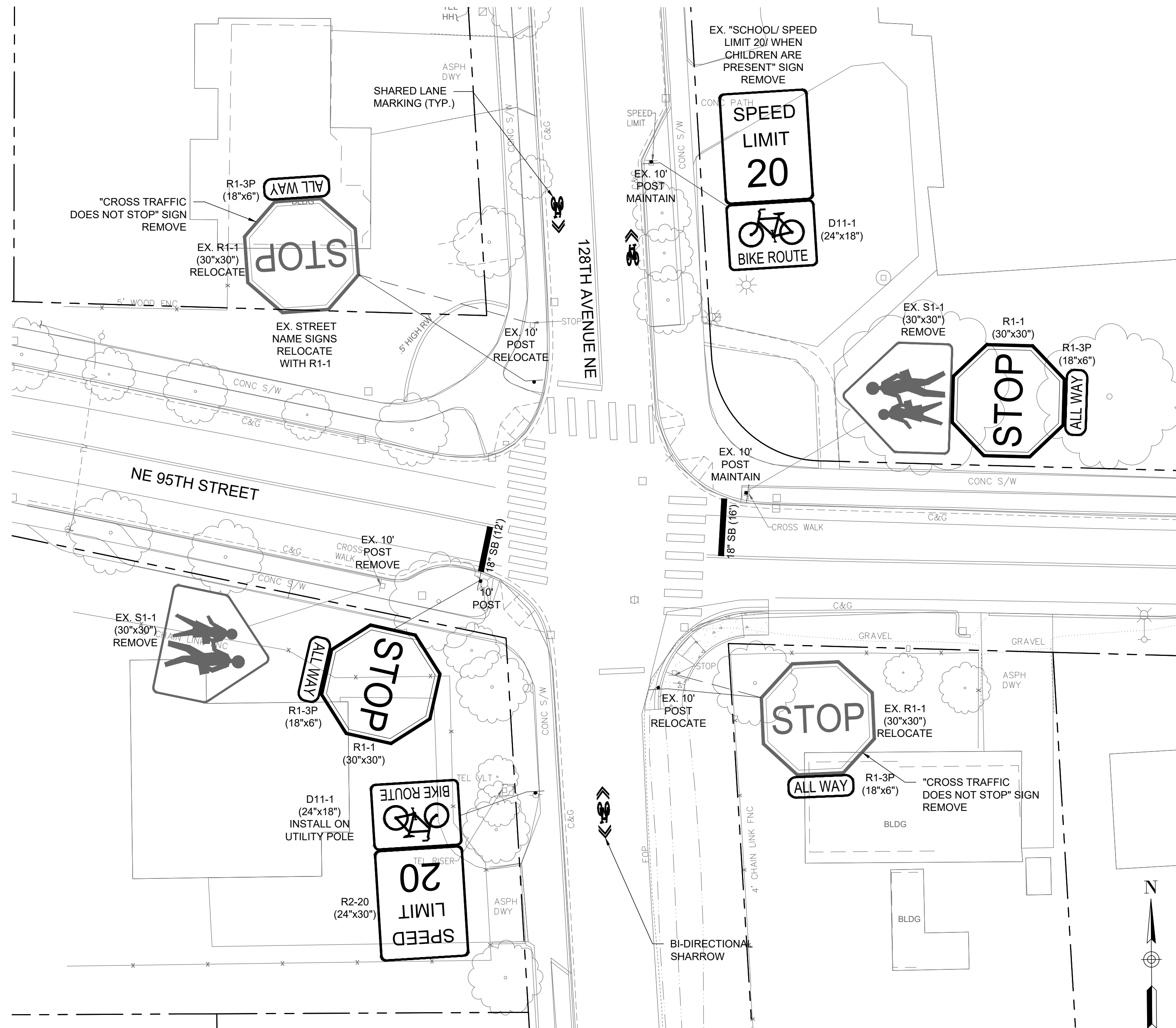
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CHANNELIZATION AND SIGNING LEGEND
ITEM DETAIL

| | | |
|----------|--|---|
| 4" SYL | 4" SOLID YELLOW LINE | |
| 4" DYL | 4" DASHED YELLOW LINE | PRE-APPROVED PLAN NO. CK-R.31 |
| | 4" YELLOW TWO-WAY LEFT TURN LANE LINES | PRE-APPROVED PLAN NO. CK-R.31 |
| 6" SWL | 6" SOLID WHITE LINE | |
| 6" DWL | 6" DASHED WHITE LINE | PRE-APPROVED PLAN NO. CK-R.31 |
| 6" DYL | 6" DASHED YELLOW LINE | PRE-APPROVED PLAN NO. CK-R.31 |
| 6" DSYCL | 6" DOUBLE SOLID YELLOW CENTER LINE | PRE-APPROVED PLAN NO. CK-R.31 |
| 12" MMC | 12" MOUNTABLE MEDIAN CURB | PRE-APPROVED PLAN NO. CK-R.19B |
| | SIGN AND POST (POST HT. PER PLANS) | PRE-APPROVED PLANS NO. CK-R.43 & CK-R.44 |
| | SIGN | |
| | BICYCLE LANE SYMBOL | PRE-APPROVED PLAN NO. CK-R.34 & WSDOT STD. PLAN M-9.50-02 |
| | SHARED LANE MARKING (SHARROW) | PRE-APPROVED PLAN NO. CK-R.46 |
| | BI-DIRECTIONAL SHARROW | 2 DT2, 3 DT2, 1 DT2 |
| | MODIFIED BI-DIRECTIONAL SHARROW | 4 DT2 |
| | BIKE DOT | 5 DT2 |
| | WHITE PAVEMENT MARKING | WSDOT STD. PLANS M-80.10-01 & M-24.40-02 |
| | BICYCLE CROSSING PAVEMENT MARKING | 2 DT1, 1 DT1 |
| | FLEXIBLE DELINEATOR POST | |
| | 10' CROSSWALK | PRE-APPROVED PLAN NO. CK-R.28 |
| | 18" STOP BAR | PRE-APPROVED PLAN NO. CK-R.28 |
| | YIELD SYMBOL PAVEMENT MARKING | 4 DT1 |



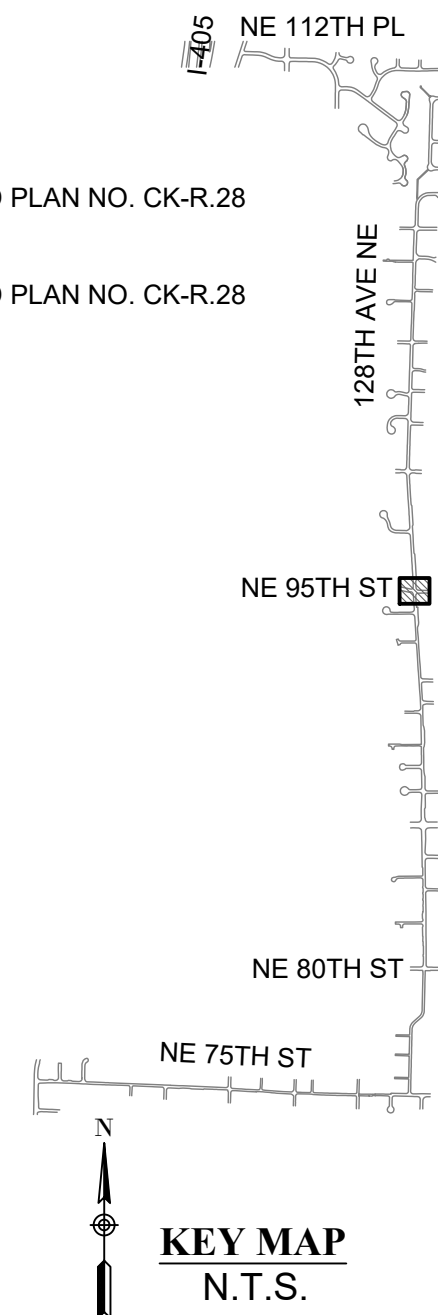


CHANNELIZATION AND SIGNING NOTES:

1. SEE NT SHEET SERIES FOR GENERAL NOTES AND LEGEND.
2. SEE DT SHEET SERIES FOR PAVEMENT MARKING AND SIGNAGE DETAILS.
3. SEE SP SHEET SERIES FOR TREE PROTECTION, TREE AND VEGETATION TRIMMING, AND CHANNELIZATION REMOVALS.
4. SEE PV SHEET SERIES FOR PAVING AND GRADING PLANS.
5. SEE OP SHEET SERIES FOR OVERVIEW PLANS SHOWING SITE PREPARATION, CHANNELIZATION AND SIGNAGE NOT INCLUDED ON THE SP AND CH PLANS.
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9. REPLACE ALL R2-25 "25 SPEED LIMIT" SIGNS WITH R2-20 "20 SPEED LIMIT" SIGNS ALONG THE KIRKLAND GREENWAY ROUTE.

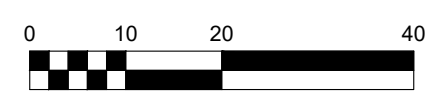
CHANNELIZATION AND SIGNING LEGEND ITEM DETAIL

| | | |
|----------|--|---|
| 4" SYL | 4" SOLID YELLOW LINE | |
| 4" DYL | 4" DASHED YELLOW LINE | PRE-APPROVED PLAN NO. CK-R.31 |
| | 4" YELLOW TWO-WAY LEFT TURN LANE LINES | PRE-APPROVED PLAN NO. CK-R.31 |
| 6" SWL | 6" SOLID WHITE LINE | |
| 6" DWL | 6" DASHED WHITE LINE | PRE-APPROVED PLAN NO. CK-R.31 |
| 6" DYL | 6" DASHED YELLOW LINE | PRE-APPROVED PLAN NO. CK-R.31 |
| 6" DSYCL | 6" DOUBLE SOLID YELLOW CENTER LINE | PRE-APPROVED PLAN NO. CK-R.31 |
| 12" MMC | 12" MOUNTABLE MEDIAN CURB | PRE-APPROVED PLAN NO. CK-R.19B |
| | SIGN AND POST (POST HT. PER PLANS) | PRE-APPROVED PLANS NO. CK-R.43 & CK-R.44 |
| | SIGN | |
| | BIKE LANE SYMBOL | PRE-APPROVED PLAN NO. CK-R.34 & WSDOT STD. PLAN M-9.50-02 |
| | SHARED LANE MARKING (SHARROW) | PRE-APPROVED PLAN NO. CK-R.46 |
| | BI-DIRECTIONAL SHARROW | 2 DT2, 3 DT2, 1 DT2 |
| | MODIFIED BI-DIRECTIONAL SHARROW | 4 DT2 |
| | BIKE DOT | 5 DT2 |
| | WHITE PAVEMENT MARKING | WSDOT STD. PLANS M-80.10-01 & M-24.40-02 |
| | BIKE CROSSING PAVEMENT MARKING | 2 DT1, 1 DT1 |
| | FLEXIBLE DELINEATOR POST | |
| | 10' CROSSWALK | PRE-APPROVED PLAN NO. CK-R.28 |
| | 18" STOP BAR | |
| | YIELD SYMBOL PAVEMENT MARKING | 4 DT1, PRE-APPROVED PLAN NO. CK-R.28 |



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128TH AVE NE GREENWAY
KIRKLAND GREENWAYS
PROJECT# 25-18-PW
KIRKLAND, WA



PREPARED: AL/TD
CHECKED: CS/KL

DATE
DECEMBER 2019

REV. 1
REV. 2
REV. 3

SHEET NAME
CHANNELIZATION AND SIGNING PLAN

DRAWING NUMBER
CH6

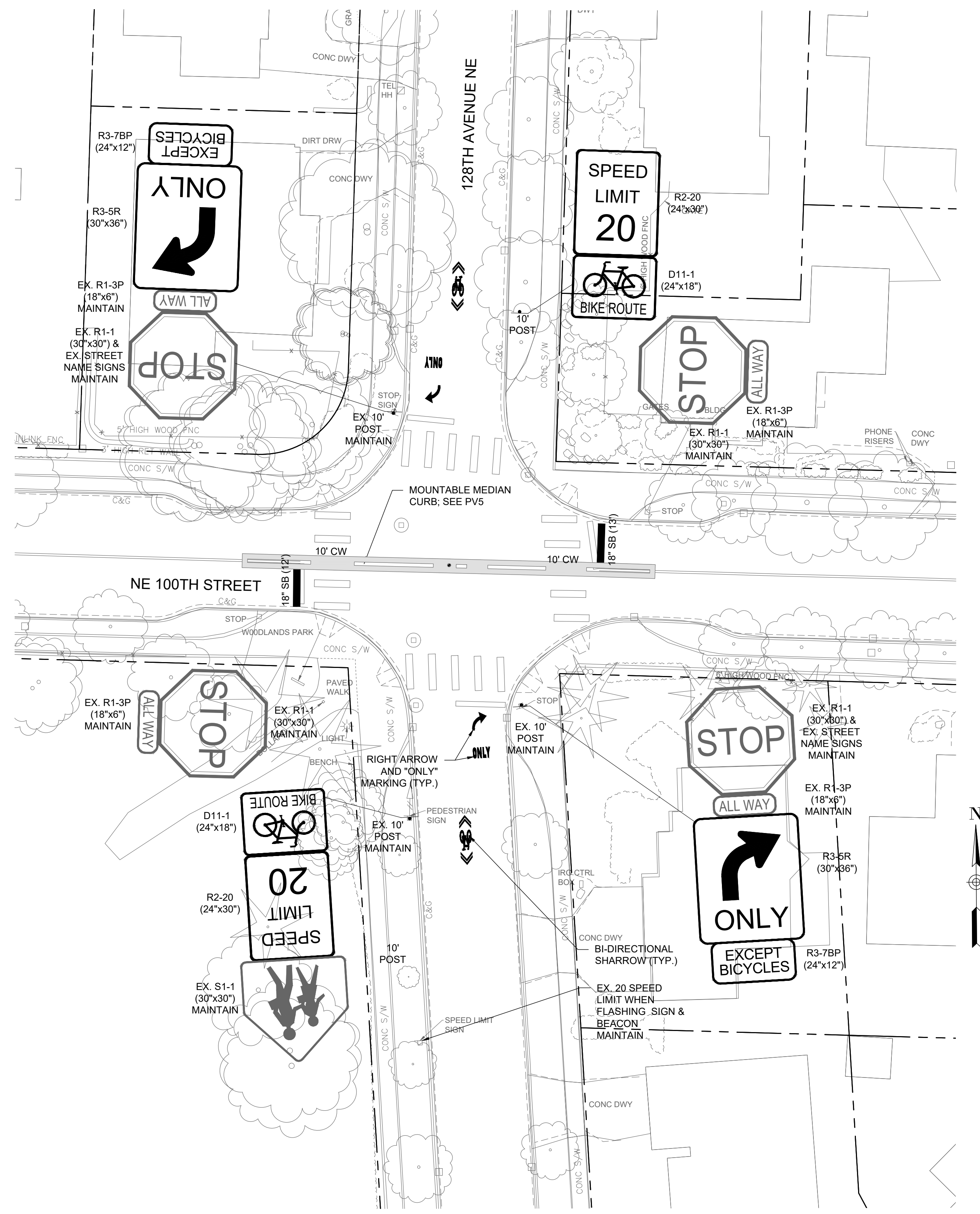
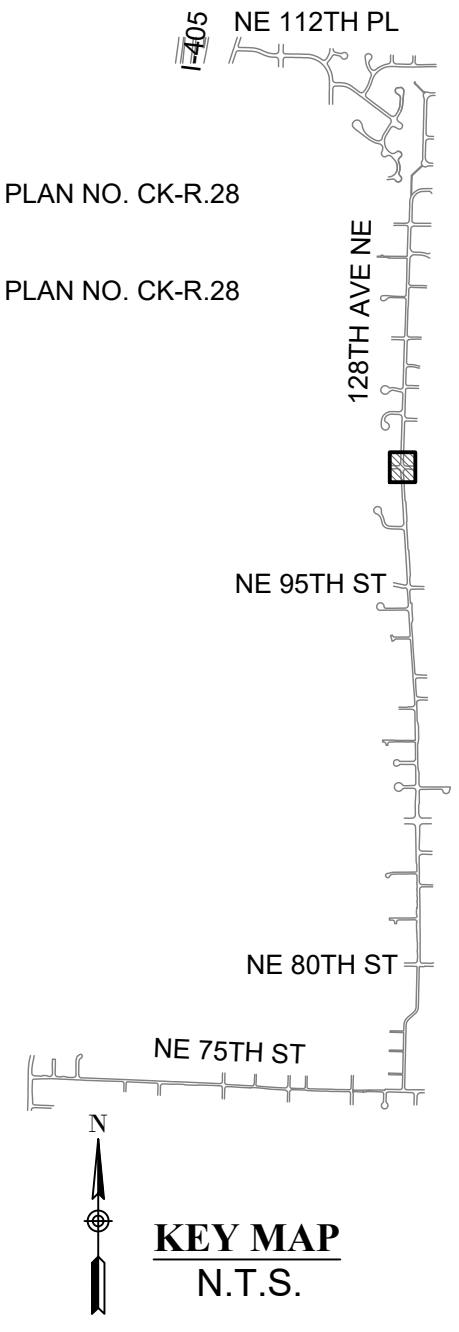
SHEET NUMBER
34 OF **49**

CHANNELIZATION AND SIGNING NOTES:

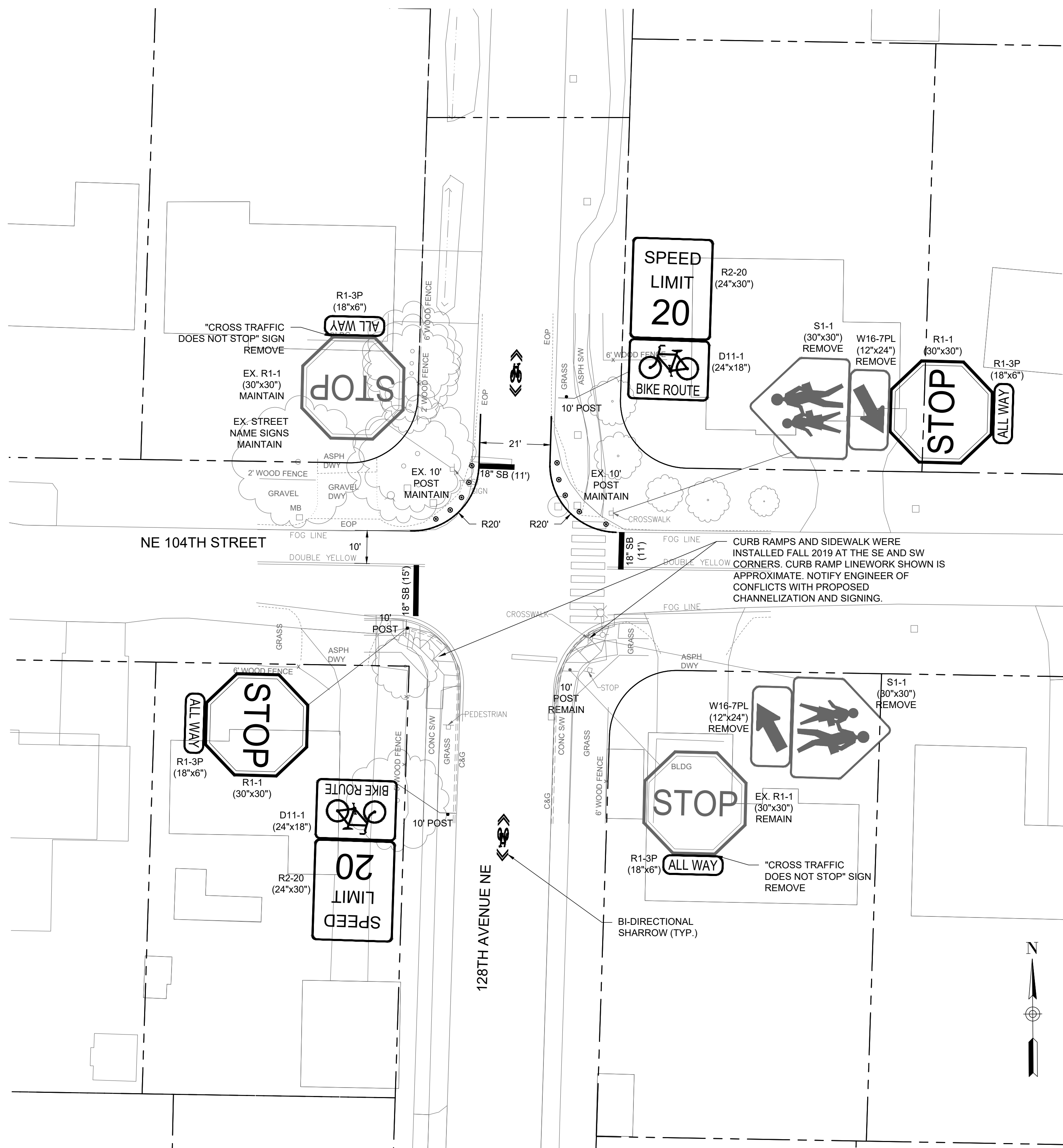
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**CHANNELIZATION AND SIGNING LEGEND
ITEM DETAIL**

| | | |
|----------|--|---|
| 4" SYL | 4" SOLID YELLOW LINE | |
| 4" DYL | 4" DASHED YELLOW LINE | PRE-APPROVED PLAN NO. CK-R.31 |
| | 4" YELLOW TWO-WAY LEFT TURN LANE LINES | PRE-APPROVED PLAN NO. CK-R.31 |
| 6" SWL | 6" SOLID WHITE LINE | |
| 6" DWL | 6" DASHED WHITE LINE | PRE-APPROVED PLAN NO. CK-R.31 |
| 6" DYL | 6" DASHED YELLOW LINE | PRE-APPROVED PLAN NO. CK-R.31 |
| 6" DSYCL | 6" DOUBLE SOLID YELLOW CENTER LINE | PRE-APPROVED PLAN NO. CK-R.31 |
| 12" MMC | 12" MOUNTABLE MEDIAN CURB | PRE-APPROVED PLAN NO. CK-R.19B |
| | SIGN AND POST (POST HT. PER PLANS) | PRE-APPROVED PLANS NO. CK-R.43 & CK-R.44 |
| | SIGN | |
| | BICYCLE LANE SYMBOL | PRE-APPROVED PLAN NO. CK-R.34 & WSDOT STD. PLAN M-9.50-02 |
| | SHARED LANE MARKING (SHARROW) | PRE-APPROVED PLAN NO. CK-R.46 |
| | BI-DIRECTIONAL SHARROW | 2 DT2, 3 DT2, 1 DT2 |
| | MODIFIED BI-DIRECTIONAL SHARROW | 4 DT2 |
| | BIKE DOT | 5 DT2 |
| | WHITE PAVEMENT MARKING | WSDOT STD. PLANS M-80.10-01 & M-24.40-02 |
| | BICYCLE CROSSING PAVEMENT MARKING | 2 DT1, 1 DT1 |
| | FLEXIBLE DELINEATOR POST | |
| | 10' CROSSWALK | PRE-APPROVED PLAN NO. CK-R.28 |
| | 18" STOP BAR | PRE-APPROVED PLAN NO. CK-R.28 |
| | YIELD SYMBOL PAVEMENT MARKING | 4 DT1 |



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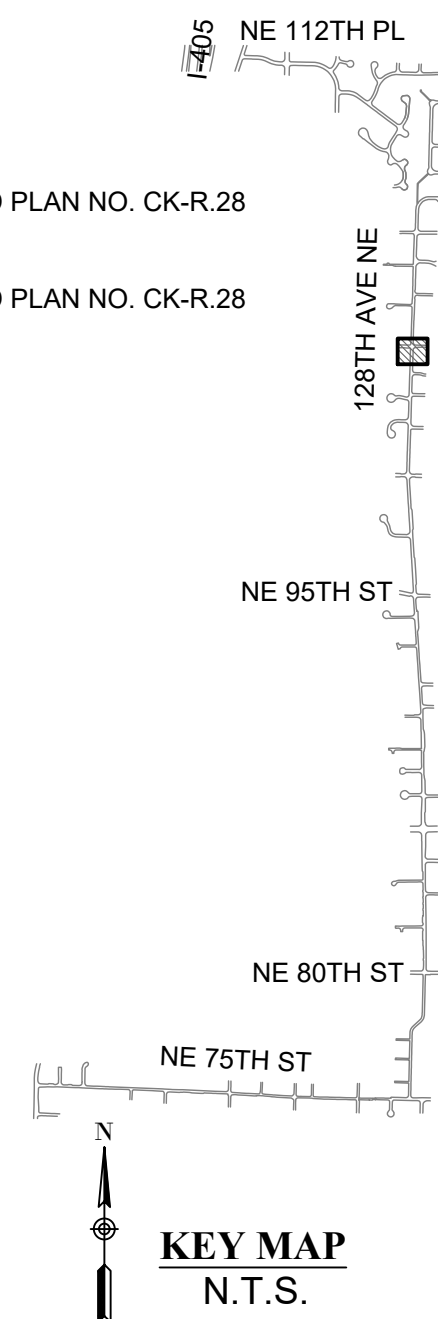


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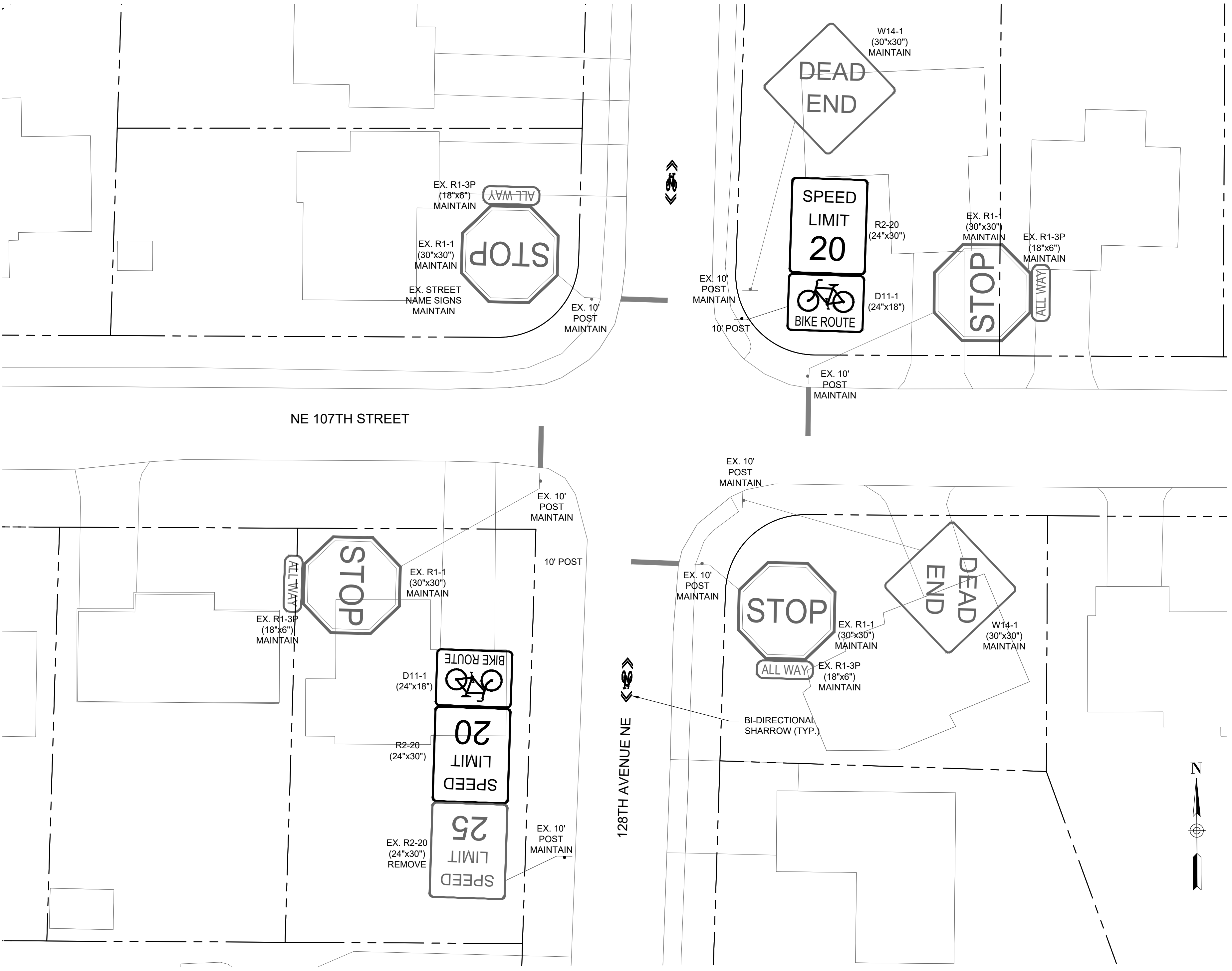
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CHANNELIZATION AND SIGNING LEGEND ITEM DETAIL

| | | |
|----------|--|---|
| 4" SYL | 4" SOLID YELLOW LINE | |
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| 6" SWL | 6" SOLID WHITE LINE | |
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| 6" DYL | 6" DASHED YELLOW LINE | PRE-APPROVED PLAN NO. CK-R.31 |
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| | SIGN | |
| | BIKE LANE SYMBOL | PRE-APPROVED PLAN NO. CK-R.34 & WSDOT STD. PLAN M-9.50-02 |
| | SHARED LANE MARKING (SHARROW) | PRE-APPROVED PLAN NO. CK-R.46 |
| | BI-DIRECTIONAL SHARROW | 2 DT2, 3 DT2, 1 DT2 |
| | MODIFIED BI-DIRECTIONAL SHARROW | 4 DT2 |
| | BIKE DOT | 5 DT2 |
| | WHITE PAVEMENT MARKING | WSDOT STD. PLANS M-80.10-01 & M-24.40-02 |
| | BIKE CROSSING PAVEMENT MARKING | 2 DT1, 1 DT1 |
| | FLEXIBLE DELINEATOR POST | |
| | 10' CROSSWALK | PRE-APPROVED PLAN NO. CK-R.28 |
| | 18" STOP BAR | |
| | YIELD SYMBOL PAVEMENT MARKING | PRE-APPROVED PLAN NO. CK-R.28 |
| | | 4 DT1 |



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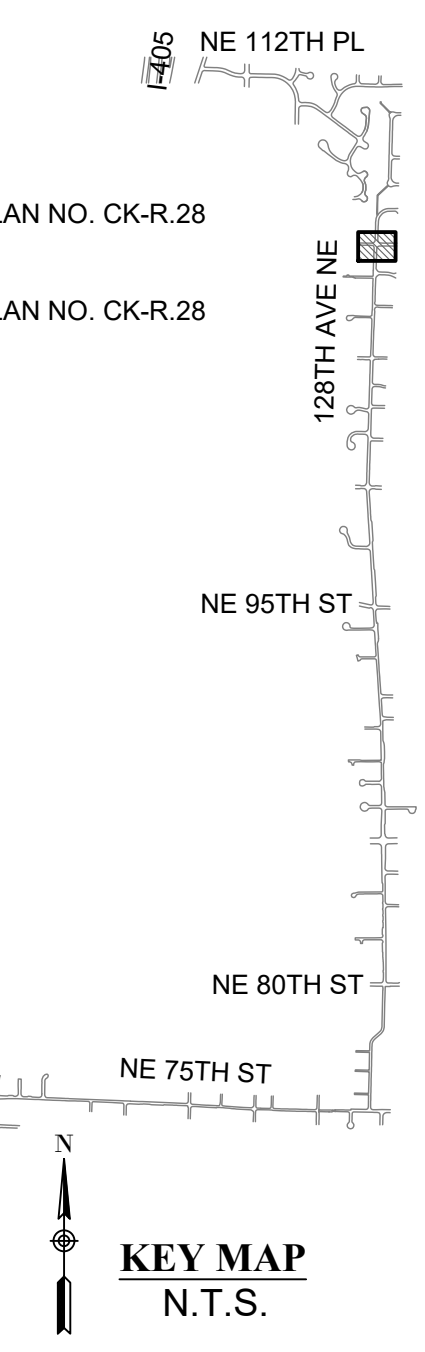


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CHANNELIZATION AND SIGNING LEGEND
ITEM DETAIL

| | | |
|----------|--|---|
| 4" SYL | 4" SOLID YELLOW LINE | |
| 4" DYL | 4" DASHED YELLOW LINE | PRE-APPROVED PLAN NO. CK-R.31 |
| | 4" YELLOW TWO-WAY LEFT TURN LANE LINES | PRE-APPROVED PLAN NO. CK-R.31 |
| 6" SWL | 6" SOLID WHITE LINE | |
| 6" DWL | 6" DASHED WHITE LINE | PRE-APPROVED PLAN NO. CK-R.31 |
| 6" DYL | 6" DASHED YELLOW LINE | PRE-APPROVED PLAN NO. CK-R.31 |
| 6" DSYCL | 6" DOUBLE SOLID YELLOW CENTER LINE | PRE-APPROVED PLAN NO. CK-R.31 |
| 12" MMC | 12" MOUNTABLE MEDIAN CURB | PRE-APPROVED PLAN NO. CK-R.19B |
| | SIGN AND POST (POST HT. PER PLANS) | PRE-APPROVED PLANS NO. CK-R.43 & CK-R.44 |
| | SIGN | |
| | BI-CYCLE LANE SYMBOL | PRE-APPROVED PLAN NO. CK-R.34 & WSDOT STD. PLAN M-9.50-02 |
| | SHARED LANE MARKING (SHARROW) | PRE-APPROVED PLAN NO. CK-R.46 |
| | BI-DIRECTIONAL SHARROW | |
| | MODIFIED BI-DIRECTIONAL SHARROW | |
| | BIKE DOT | |
| | WHITE PAVEMENT MARKING | WSDOT STD. PLANS M-80.10-01 & M-24.40-02 |
| | BI-CYCLE CROSSING PAVEMENT MARKING | |
| | FLEXIBLE DELINEATOR POST | |
| | 10' CROSSWALK | PRE-APPROVED PLAN NO. CK-R.28 |
| | 18" STOP BAR | PRE-APPROVED PLAN NO. CK-R.28 |
| | YIELD SYMBOL PAVEMENT MARKING | |



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TOOLE
DESIGN

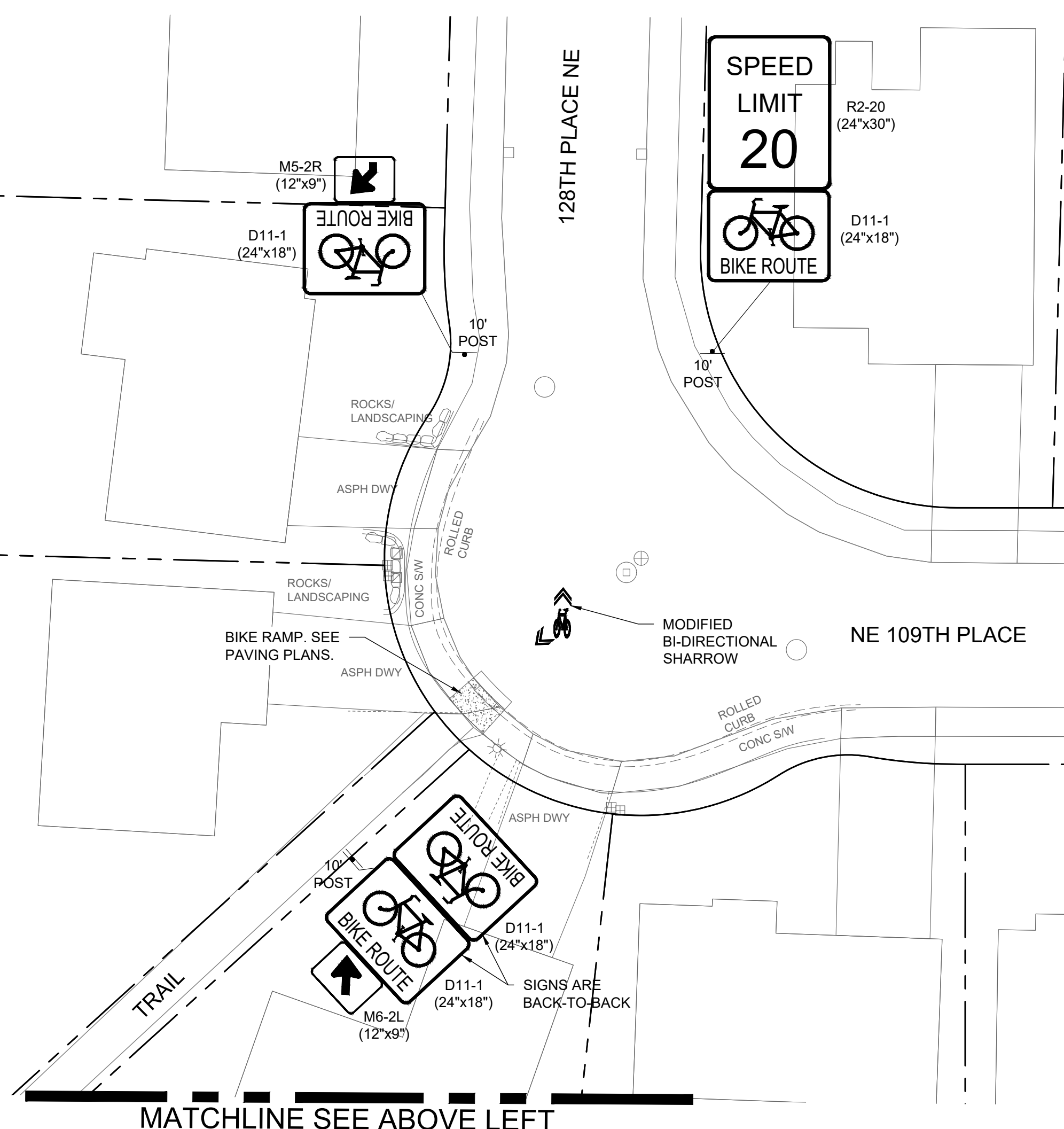
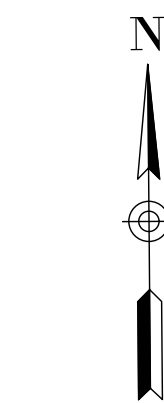
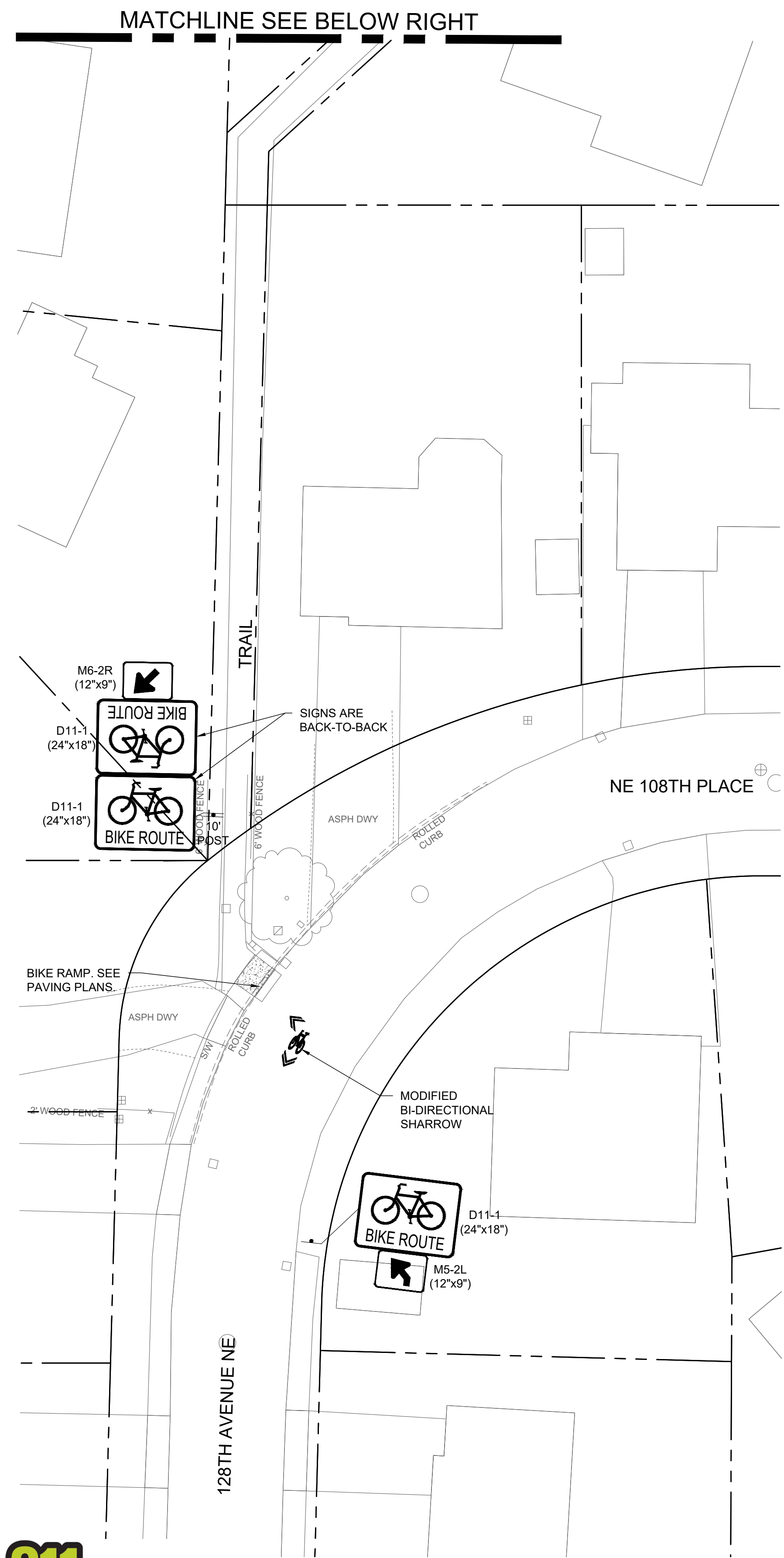
720 3RD AVE SUITE 2020
SEATTLE, WA 98104
PHONE: (206) 297-1601
FAX: (301) 927-2800
www.tooledesign.com

CITY OF KIRKLAND
123 FIFTH AVENUE
KIRKLAND, WA 98033
PHONE: (425) 587-3000

128TH AVE NE GREENWAY
KIRKLAND GREENWAYS
PROJECT# 25-18-PW
KIRKLAND, WA

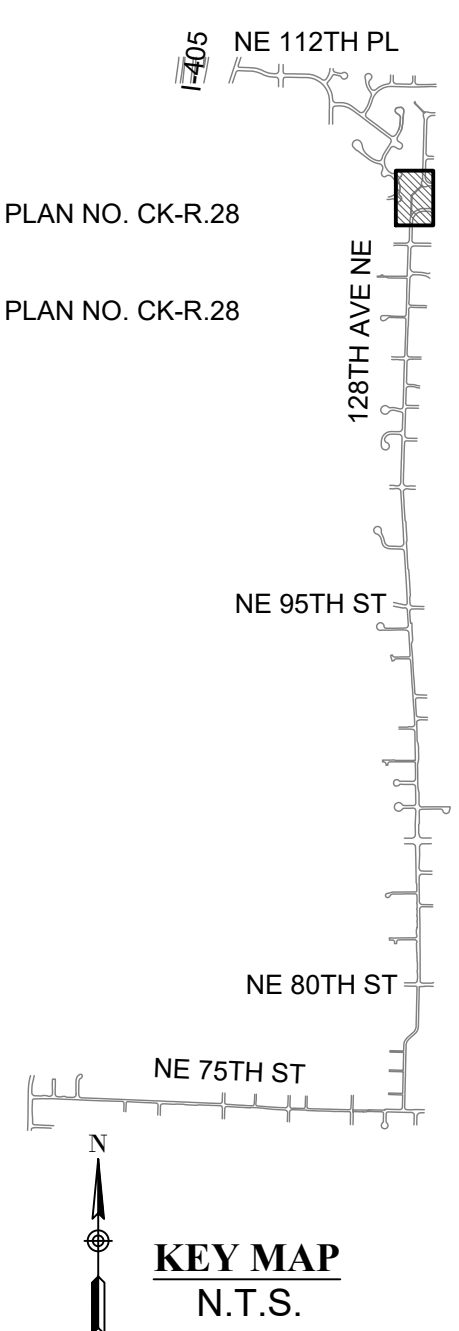
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PREPARED: AL/TD
CHECKED: CS/KL
DATE
DECEMBER 2019
REV. 1
REV. 2
REV. 3
SHEET NAME
CHANNELIZATION
AND SIGNING PLAN
DRAWING NUMBER
CH8
SHEET NUMBER
36 OF 49



- CHANNELIZATION AND SIGNING NOTES:**
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| CHANNELIZATION AND SIGNING LEGEND | |
|-----------------------------------|--|
| ITEM | DETAIL |
| 4" SYL | 4" SOLID YELLOW LINE |
| 4" DYL | 4" DASHED YELLOW LINE |
| | 4" YELLOW TWO-WAY LEFT TURN LANE LINES |
| 6" SWL | 6" SOLID WHITE LINE |
| 6" DWL | 6" DASHED WHITE LINE |
| 6" DYL | 6" DASHED YELLOW LINE |
| 6" DSYCL | 6" DOUBLE SOLID YELLOW CENTER LINE |
| 12" MMC | 12" MOUNTABLE MEDIAN CURB |
| | SIGN AND POST (POST HT. PER PLANS) |
| STOP | SIGN |
| | BICYCLE LANE SYMBOL |
| | SHARED LANE MARKING (SHARROW) |
| | BI-DIRECTIONAL SHARROW |
| | MODIFIED BI-DIRECTIONAL SHARROW |
| | BIKE DOT |
| | WHITE PAVEMENT MARKING |
| | BICYCLE CROSSING PAVEMENT MARKING |
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| | 10' CROSSWALK |
| | 18" STOP BAR |
| | YIELD SYMBOL PAVEMENT MARKING |

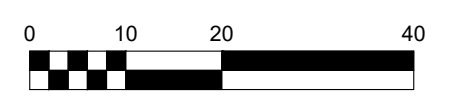


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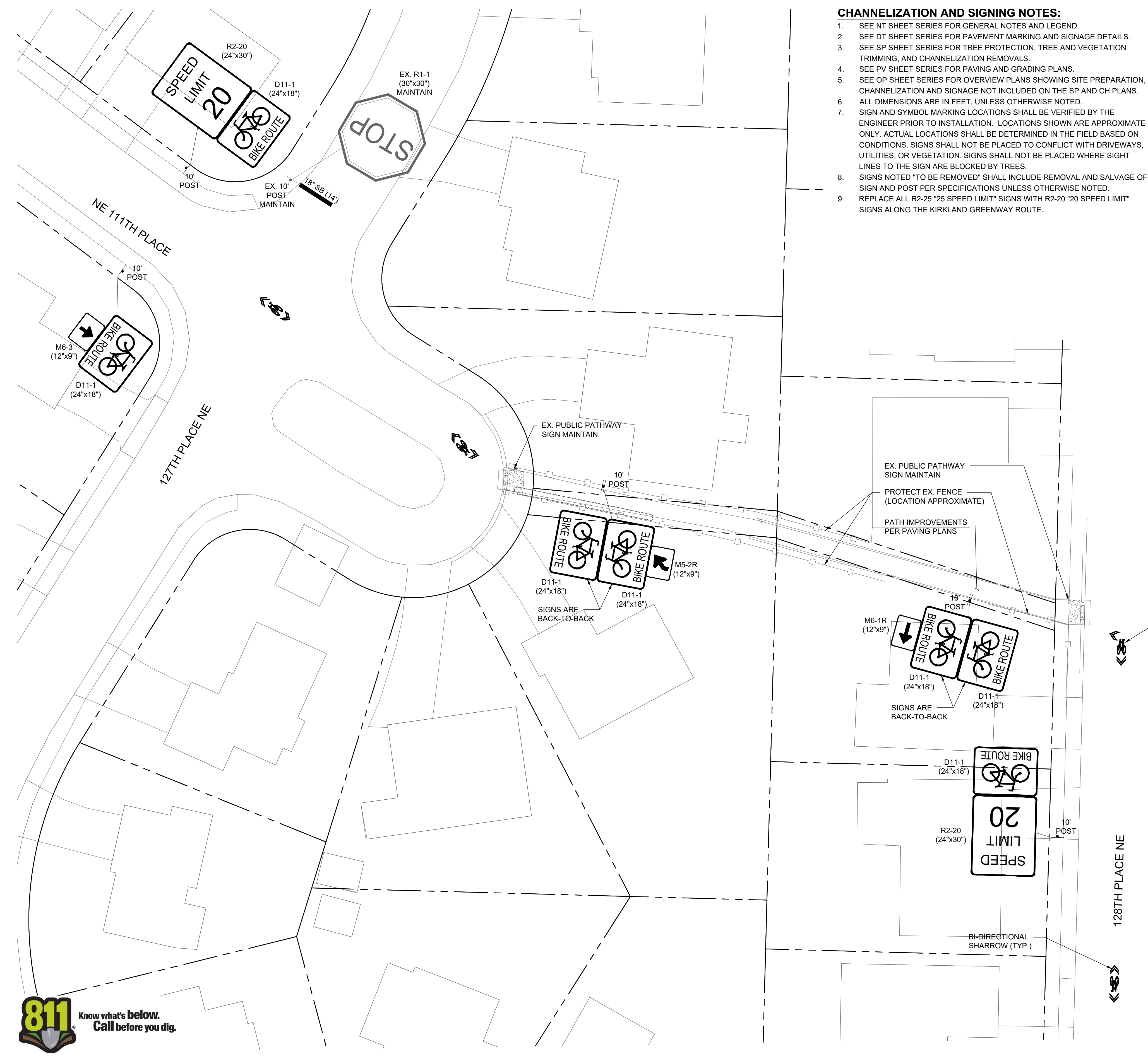
TOOLE DESIGN
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128TH AVE NE GREENWAY
KIRKLAND GREENWAYS
PROJECT# 25-18-PW
KIRKLAND, WA



PREPARED: AL/TD
CHECKED: CS/KL
DATE
DECEMBER 2019
REV. 1
REV. 2
REV. 3
SHEET NAME
CHANNELIZATION
AND SIGNING PLAN
DRAWING NUMBER
CH9
SHEET NUMBER
37 OF **49**

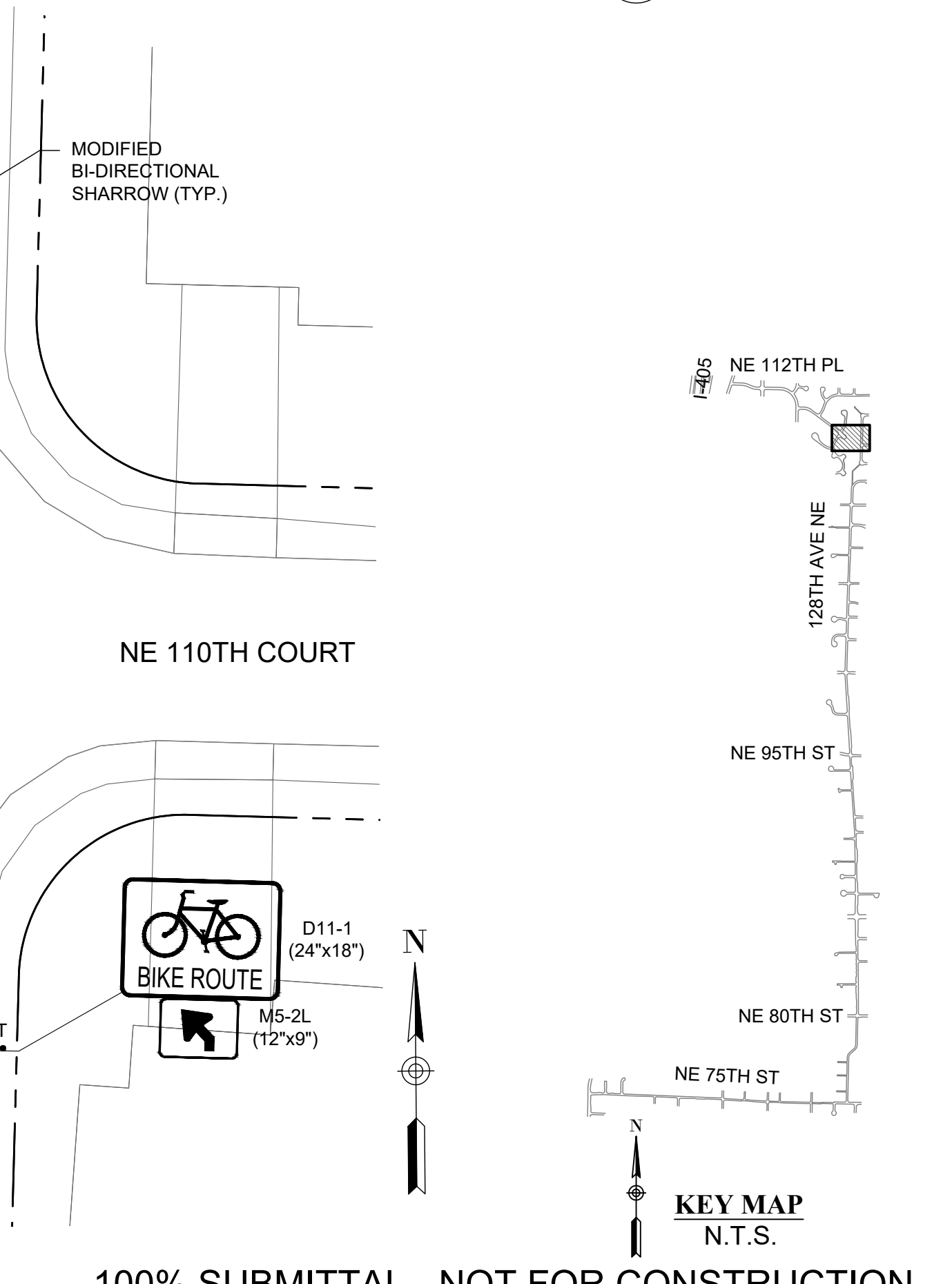


CHANNELIZATION AND SIGNING NOTES:

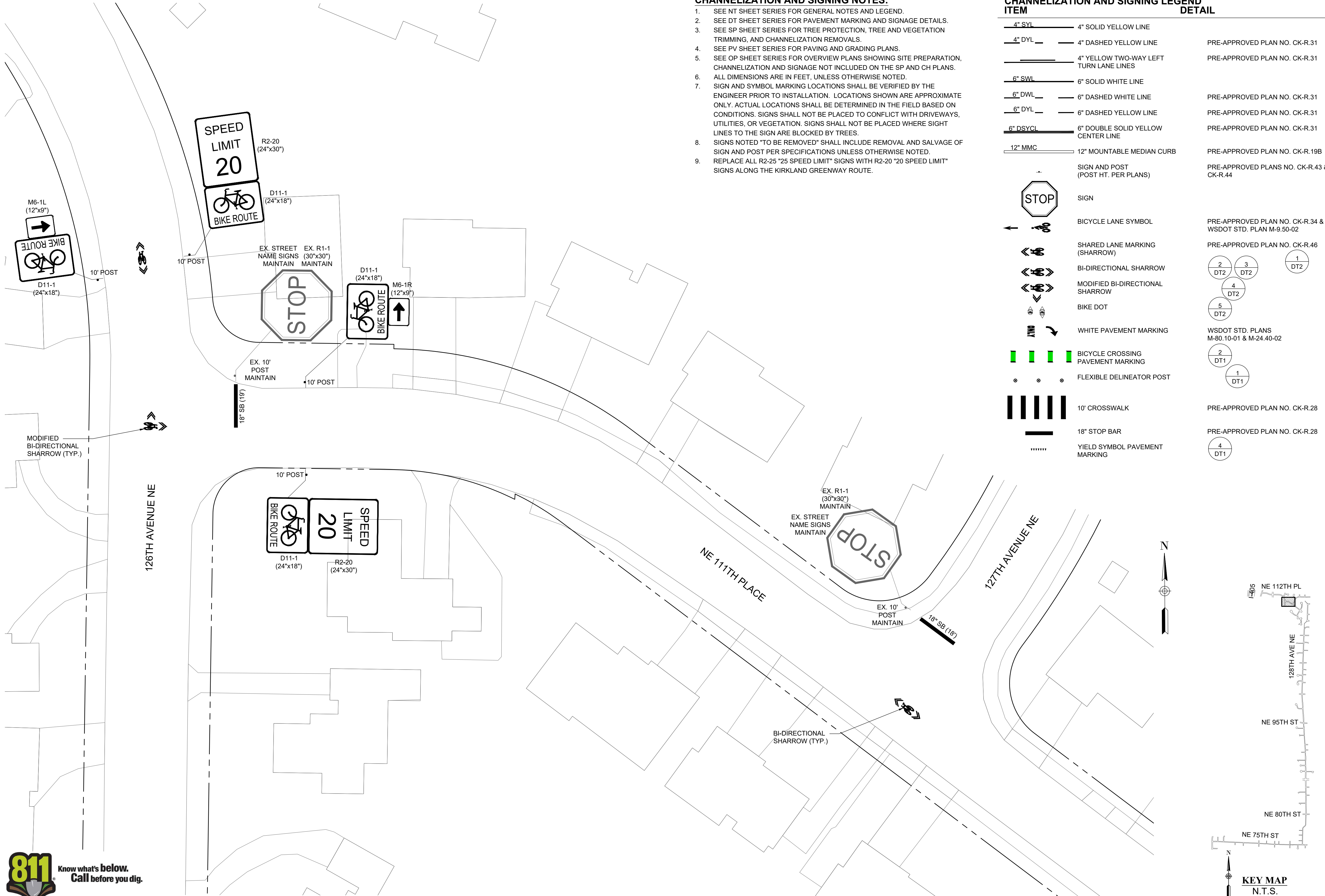
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**CHANNELIZATION AND SIGNING LEGEND
ITEM DETAIL**

| | | |
|-------------------------------------|------------------------------------|---|
| 4" SYL | 4" SOLID YELLOW LINE | PRE-APPROVED PLAN NO. CK-R.31 |
| 4" DYL | 4" DASHED YELLOW LINE | PRE-APPROVED PLAN NO. CK-R.31 |
| 4" YLW TWO-WAY LEFT TURN LANE LINES | | PRE-APPROVED PLAN NO. CK-R.31 |
| 6" SWL | 6" SOLID WHITE LINE | PRE-APPROVED PLAN NO. CK-R.31 |
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| BI-DIRECTIONAL SHARROW | | |
| MODIFIED BI-DIRECTIONAL SHARROW | | |
| BIKE DOT | | |
| WHITE PAVEMENT MARKING | | WSDOT STD. PLANS M-80.10-01 & M-24.40-02 |
| BICYCLE CROSSING PAVEMENT MARKING | | |
| FLEXIBLE DELINEATOR POST | | |
| 10' CROSSWALK | | PRE-APPROVED PLAN NO. CK-R.28 |
| 18" STOP BAR | | PRE-APPROVED PLAN NO. CK-R.28 |
| YIELD SYMBOL PAVEMENT MARKING | | |



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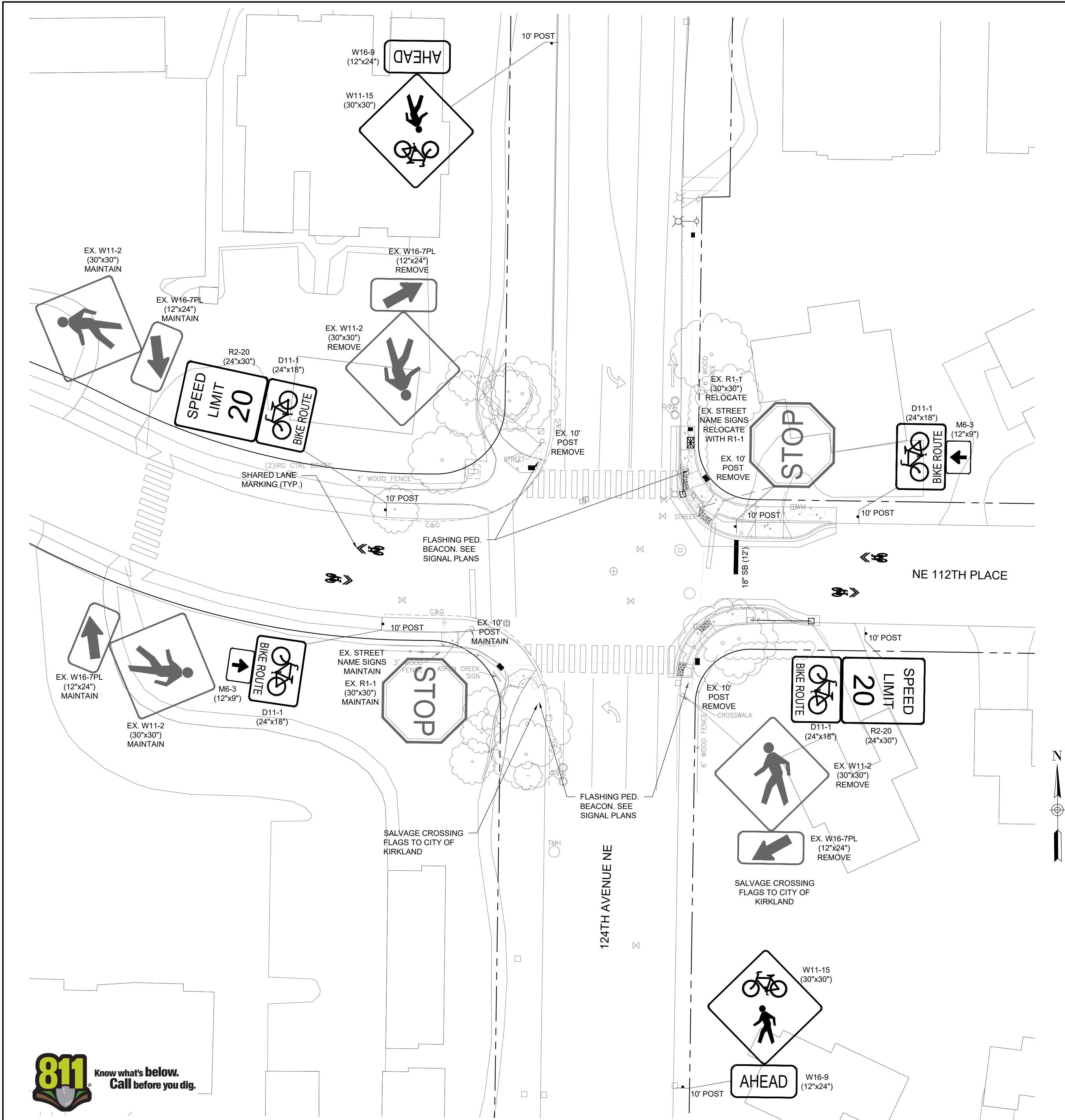


CHANNELIZATION AND SIGNING NOTES:

1. SEE NT SHEET SERIES FOR GENERAL NOTES AND LEGEND.
2. SEE DT SHEET SERIES FOR PAVEMENT MARKING AND SIGNAGE DETAILS.
3. SEE SP SHEET SERIES FOR TREE PROTECTION, TREE AND VEGETATION TRIMMING, AND CHANNELIZATION REMOVALS.
4. SEE PV SHEET SERIES FOR PAVING AND GRADING PLANS.
5. SEE OP SHEET SERIES FOR OVERVIEW PLANS SHOWING SITE PREPARATION, CHANNELIZATION AND SIGNAGE NOT INCLUDED ON THE SP AND CH PLANS.
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9. REPLACE ALL R2-25 "25 SPEED LIMIT" SIGNS WITH R2-20 "20 SPEED LIMIT" SIGNS ALONG THE KIRKLAND GREENWAY ROUTE.

CHANNELIZATION AND SIGNING LEGEND
ITEM DETAIL

| | | |
|----------|--|---|
| 4" SYL | 4" SOLID YELLOW LINE | |
| 4" DYL | 4" DASHED YELLOW LINE | PRE-APPROVED PLAN NO. CK-R.31 |
| | 4" YELLOW TWO-WAY LEFT TURN LANE LINES | PRE-APPROVED PLAN NO. CK-R.31 |
| 6" SWL | 6" SOLID WHITE LINE | |
| 6" DWL | 6" DASHED WHITE LINE | PRE-APPROVED PLAN NO. CK-R.31 |
| 6" DYL | 6" DASHED YELLOW LINE | PRE-APPROVED PLAN NO. CK-R.31 |
| 6" DSYCL | 6" DOUBLE SOLID YELLOW CENTER LINE | PRE-APPROVED PLAN NO. CK-R.31 |
| 12" MMC | 12" MOUNTABLE MEDIAN CURB | PRE-APPROVED PLAN NO. CK-R.19B |
| | SIGN AND POST (POST HT. PER PLANS) | PRE-APPROVED PLANS NO. CK-R.43 & CK-R.44 |
| | SIGN | |
| | BICYCLE LANE SYMBOL | PRE-APPROVED PLAN NO. CK-R.34 & WSDOT STD. PLAN M-9.50-02 |
| | SHARED LANE MARKING (SHARROW) | PRE-APPROVED PLAN NO. CK-R.46 |
| | BI-DIRECTIONAL SHARROW | 2 DT2 3 DT2 1 DT2 |
| | MODIFIED BI-DIRECTIONAL SHARROW | 4 DT2 |
| | BIKE DOT | 5 DT2 |
| | WHITE PAVEMENT MARKING | WSDOT STD. PLANS M-80.10-01 & M-24.40-02 |
| | BICYCLE CROSSING PAVEMENT MARKING | 2 DT1 1 DT1 |
| | FLEXIBLE DELINEATOR POST | |
| | 10' CROSSWALK | PRE-APPROVED PLAN NO. CK-R.28 |
| | 18" STOP BAR | PRE-APPROVED PLAN NO. CK-R.28 |
| | YIELD SYMBOL PAVEMENT MARKING | 4 DT1 |

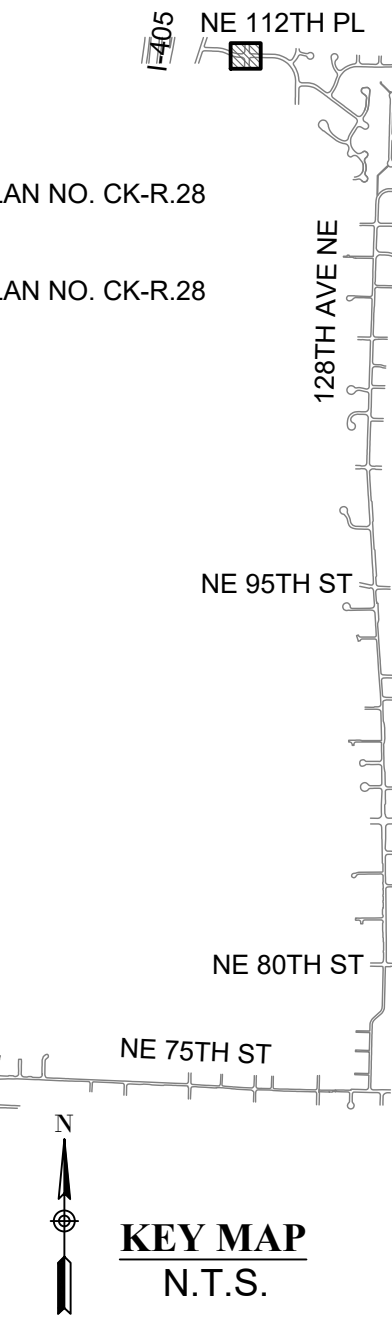


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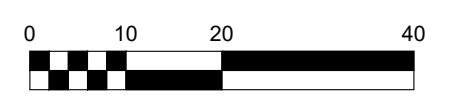
**CHANNELIZATION AND SIGNING LEGEND
ITEM DETAIL**

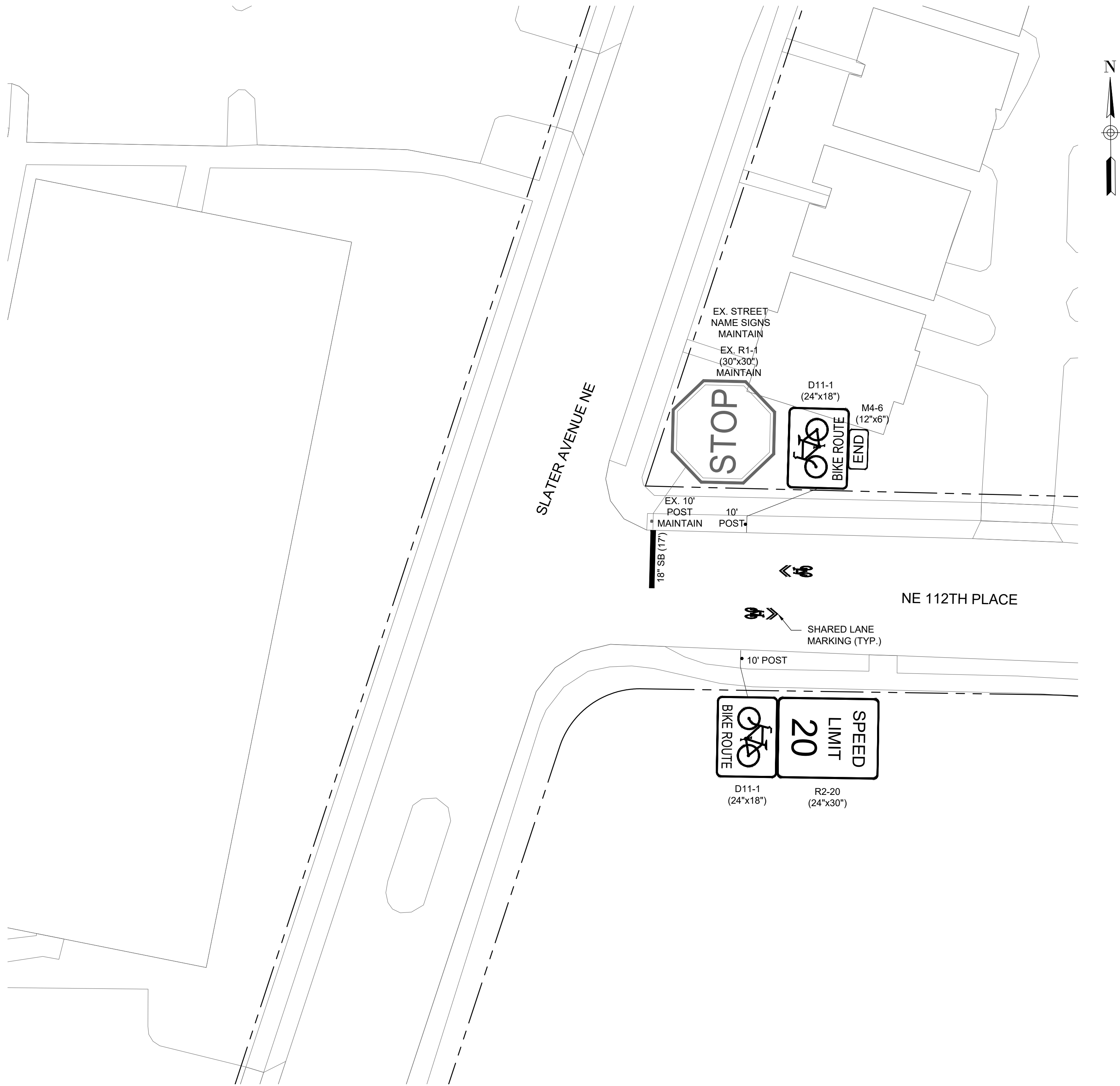
| | | |
|----------|--|---|
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| 4" DYL | 4" DASHED YELLOW LINE | PRE-APPROVED PLAN NO. CK-R.31 |
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| | MODIFIED BI-DIRECTIONAL SHARROW | 4 DT2 |
| | BIKE DOT | 5 DT2 |
| | WHITE PAVEMENT MARKING | WSDOT STD. PLANS M-80.10-01 & M-24.40-02 |
| | BI-CYCLE CROSSING PAVEMENT MARKING | 2 DT1, 1 DT1 |
| | FLEXIBLE DELINEATOR POST | |
| | 10' CROSSWALK | PRE-APPROVED PLAN NO. CK-R.28 |
| | 18" STOP BAR | PRE-APPROVED PLAN NO. CK-R.28 |
| | YIELD SYMBOL PAVEMENT MARKING | 4 DT1 |



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128TH AVE NE GREENWAY
KIRKLAND GREENWAYS
PROJECT# 25-18-PW
KIRKLAND, WA



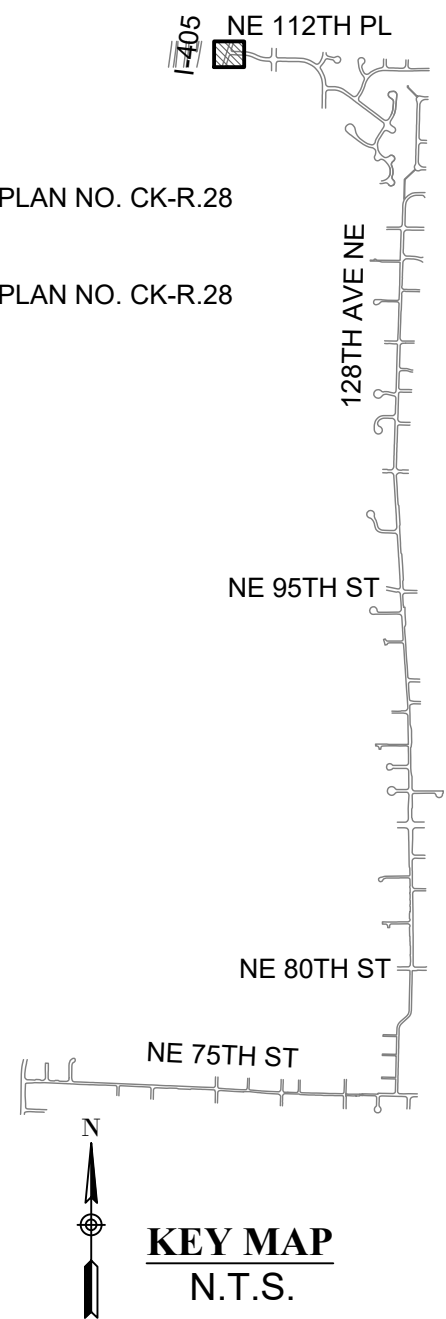


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CHANNELIZATION AND SIGNING LEGEND
ITEM DETAIL

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| 6" DYL | 6" DASHED YELLOW LINE | PRE-APPROVED PLAN NO. CK-R.31 |
| 6" DSYCL | 6" DOUBLE SOLID YELLOW CENTER LINE | PRE-APPROVED PLAN NO. CK-R.31 |
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| | SIGN AND POST (POST HT. PER PLANS) | PRE-APPROVED PLANS NO. CK-R.43 & CK-R.44 |
| | SIGN | |
| | BICYCLE LANE SYMBOL | PRE-APPROVED PLAN NO. CK-R.34 & WSDOT STD. PLAN M-9.50-02 |
| | SHARED LANE MARKING (SHARROW) | PRE-APPROVED PLAN NO. CK-R.46 |
| | BI-DIRECTIONAL SHARROW | |
| | MODIFIED BI-DIRECTIONAL SHARROW | |
| | BIKE DOT | |
| | WHITE PAVEMENT MARKING | WSDOT STD. PLANS M-80.10-01 & M-24.40-02 |
| | BICYCLE CROSSING PAVEMENT MARKING | |
| | FLEXIBLE DELINEATOR POST | |
| | 10' CROSSWALK | PRE-APPROVED PLAN NO. CK-R.28 |
| | 18" STOP BAR | PRE-APPROVED PLAN NO. CK-R.28 |
| | YIELD SYMBOL PAVEMENT MARKING | |



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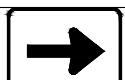



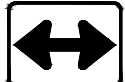


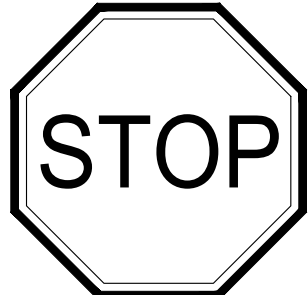






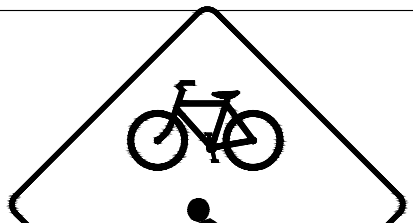




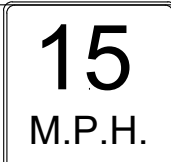



CITY OF KIRKLAND
123 FIFTH AVENUE
KIRKLAND, WA 98033
PHONE: (425) 887-3000

128TH AVE NE GREENWAY
KIRKLAND GREENWAYS
PROJECT# 25-18-PW
KIRKLAND, WA

PREPARED: AL/TD
CHECKED: CS/KL
DATE
DECEMBER 2019
REV. 1
REV. 2
REV. 3
SHEET NAME
CHANNELIZATION
AND SIGNING PLAN
DRAWING NUMBER
CH13
SHEET NUMBER
41 OF 49

SIGN SCHEDULE

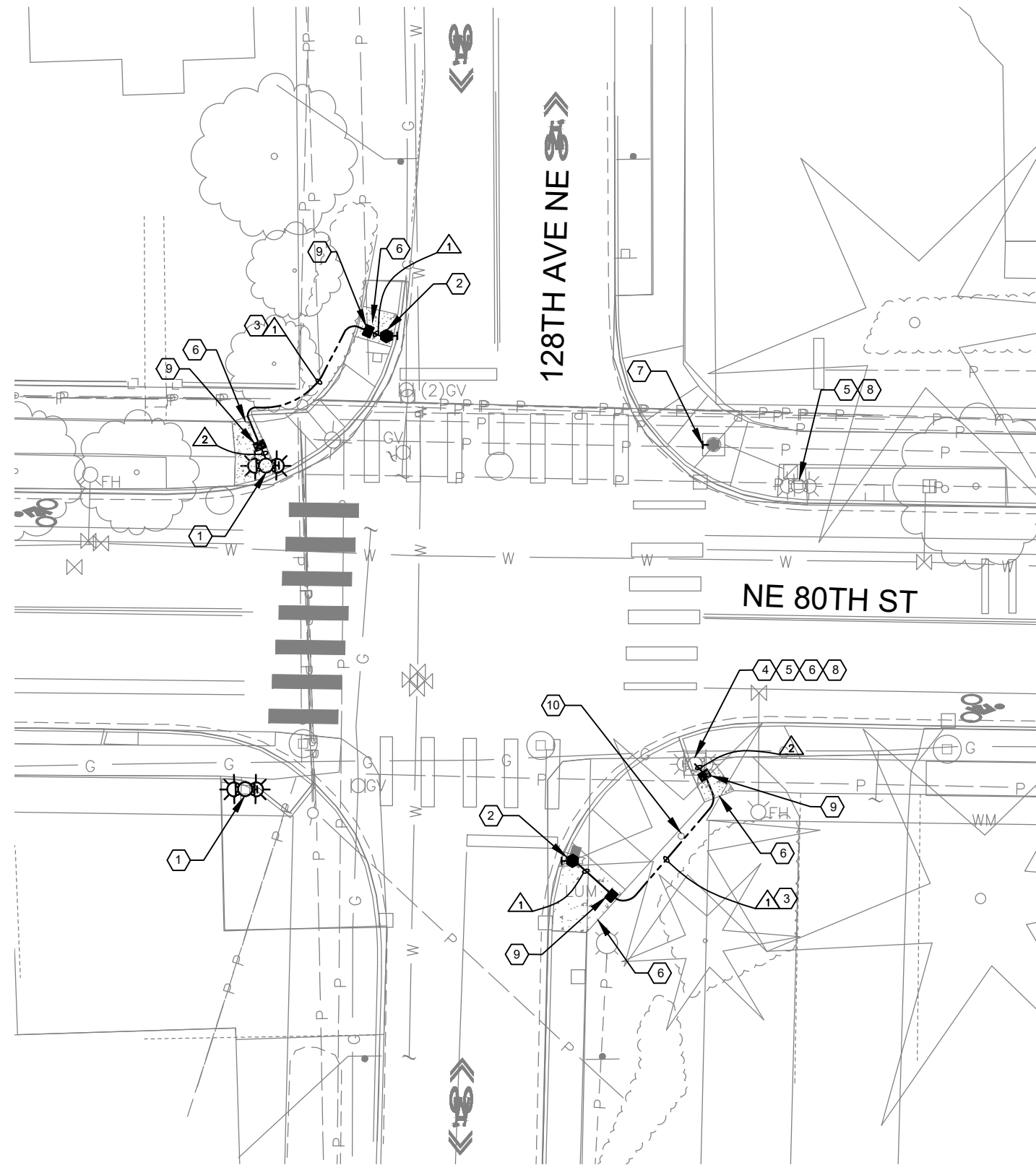
| SIGN CODE | SIGN IMAGE | DESCRIPTION (SIZE) | QUANTITY PER SHEET | | | | | | | | | | | | | TOTAL QTY. | | | |
|-----------|---|--|--------------------|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|------|------------|------|------|------|
| | | | OP1 | OP2 | OP3 | CH1 | CH2 | CH3 | CH4 | CH5 | CH6 | CH7 | CH8 | CH9 | CH10 | | CH11 | CH12 | CH13 |
| D11-1 |  | BIKE ROUTE (24"x18") | 2 | 2 | | | 4 | 4 | 4 | 2 | 2 | 2 | 2 | 7 | 8 | 4 | 4 | 2 | 49 |
| M4-6 |  | END (12"x6") | | | | | | | | | | | | | | | | 1 | 1 |
| M5-2L |  | DIRECTIONAL ARROW (12"x9") | | | | | | | | | | | | 1 | 1 | | | | 2 |
| M5-2R |  | DIRECTIONAL ARROW (12"x9") | | | | | | | | | | | | 1 | 1 | | | | 2 |
| M6-1L |  | LEFT ARROW (12"x9") | | | | | | | | | | | | | | 1 | | | 1 |
| M6-1R |  | RIGHT ARROW (12"x9") | | | | | | | | | | | | | 1 | 1 | | | 2 |
| M6-2L |  | DIAGONAL LEFT ARROW (12"x9") | | | | | | | | | | | | 1 | | | | | 1 |
| M6-2R |  | DIAGONAL RIGHT ARROW (12"x9") | | | | | | | | | | | | 1 | | | | | 1 |
| M6-3 |  | STRAIGHT ARROW (12"x9") | | | | | | 2 | 2 | | | | | | 1 | | 2 | | 7 |
| M6-4 |  | LEFT/RIGHT ARROWS (12"x9") | | | | | 1 | | | | | | | | | | | | 1 |
| M6-6L |  | LEFT/STRAIGHT ARROWS (12"x9") | | | | | 1 | | | | | | | | | | | | 1 |
| M6-6R |  | RIGHT/STRAIGHT ARROWS (12"x9") | | | | | 1 | | | | | | | | | | | | 1 |
| R1-1 |  | STOP (30"x30") | | | | 1 | | | | 2 | | 2 | | | | | | | 5 |
| R1-3P |  | ALL WAY (18"x6") | | | | | | | | 4 | | 4 | | | | | | | 8 |
| R2-20 |  | SPEED LIMIT 20 (24"x30") | 2 | 2 | | | 1 | 2 | 2 | 2 | 2 | 2 | 2 | 1 | 2 | 2 | 2 | 1 | 25 |
| R3-5R |  | RIGHT TURN ONLY - MANDATORY MOVEMENT LANE CONTROL (30"x36") | | | | | | | | | 2 | | | | | | | | 2 |
| R3-7BP |  | EXCEPT BICYCLES (24"x12") | | | | | | | | | 2 | | | | | | | | 2 |
| R7-9A |  | NO PARKING BIKE LANE (12"x18") | 2 | | | | | | | | | | | | | | | | 2 |
| S1-1 |  | SCHOOL ADVANCE WARNING (30"x30") SIGN TO HAVE FLUORESCENT YELLOW-GREEN BACKGROUND | | | | 6 | | | | | | | | | | | | | 6 |
| W11-15 |  | COMBINATION BIKE AND PEDESTRIAN ADVANCE WARNING (30"x30") | | | | | | | 2 | | | | | | | | 2 | | 4 |

| SIGN CODE | SIGN IMAGE | DESCRIPTION (SIZE) | QUANTITY PER SHEET | | | | | | | | | | | | | | TOTAL QTY. | | |
|----------------|---|---|--------------------|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|------|------|------------|------|------|
| | | | OP1 | OP2 | OP3 | CH1 | CH2 | CH3 | CH4 | CH5 | CH6 | CH7 | CH8 | CH9 | CH10 | CH11 | | CH12 | CH13 |
| |  | PED CROSSING (30"x30") | | | | | | | | | | | | | | | | | |
| W11-17 |  | RAISED INTERSECTION (30"x30") | | | | | | | | | | | | | | | | | 2 |
| W13-1 |  | ADVISORY SPEED (18"x18") | | | | | | | | | | | | | | | | | 2 |
| W16-7PL FLUOR. |  | DIAGONAL DOWNWARD ARROW (24"x12") SIGN TO HAVE FLUORESCENT YELLOW-GREEN BACKGROUND | | | | | | | | | | | | | | | | | 2 |
| W16-7PR FLUOR. |  | DIAGONAL DOWNWARD ARROW (24"x12") SIGN TO HAVE FLUORESCENT YELLOW-GREEN BACKGROUND | | | | | | | | | | | | | | | | | 4 |
| W16-9P |  | AHEAD (24"x12") | | | | | | | 2 | | | | | | | | 2 | | 4 |

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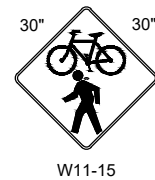




CONSTRUCTION NOTES:

- 1 INSTALL SOLAR POWERED RECTANGULAR RAPID FLASH BEACON (RRFB) SYSTEM BACK-TO-BACK INCLUDING POLE, FOUNDATION, SOLAR PANEL, XAV2 AUDIBLE PUSHBUTTON AND SIGNS ACCORDING TO MANUFACTURER'S REQUIREMENT. SEE DRAWING TSDT1 DETAIL B FOR RRFB DETAILS.
- 2 INSTALL BIKE PUSHBUTTON POST, FOUNDATION, PUSHBUTTON, SIGN R10-25C ACCORDING TO MANUFACTURER'S REQUIREMENT. SEE DRAWING TSDT1 DETAIL C FOR BIKE PUSHBUTTON DETAILS.
- 3 INSTALL 1" CONDUIT FROM RRFB BASE TO BIKE PUSHBUTTON POST BASE.
- 4 REMOVE EXISTING RRFB FOUNDATION AND TEMPORARY REMOVE RRFB. INSTALL NEW FOUNDATION WITH 1" CONDUIT AT THE EXISTING LOCATION, INSTALL EXISTING RRFB ON NEW FOUNDATION AND MAKE WIRE CONNECTION. REPLACE EXISTING PUSHBUTTON WITH XAV2 AUDIBLE PUSHBUTTON.
- 5 REPLACE EXISTING CONTROL UNIT WITH XAVCU2-DC CONTROL UNIT. MAKE WIRE CONNECTION.
- 6 REPLACE FULL SIDEWALK PANEL TO MATCH SURROUNDING AREA.
- 7 REPLACE EXISTING PUSHBUTTON WITH XAV2 AUDIBLE PUSH BUTTON.
- 8 REPLACE EXISTING SIGNS S1-1 ON RRFB WITH NEW SIGNS W11-15.
- 9 INSTALL SLIP-RESISTANT TYPE 1 JUNCTION BOX OUTSIDE OF CURB RAMP AND LANDING AREA ACCORDING TO WSDOT STANDARD PLAN J-40.10-04.
- 10 EXISTING TREE TO BE REMOVED AND SEE SHEET OP1 FOR DETAILS.

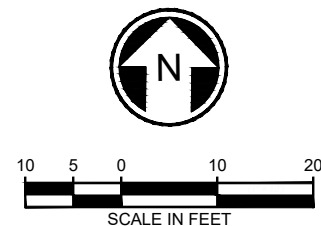
SIGN:



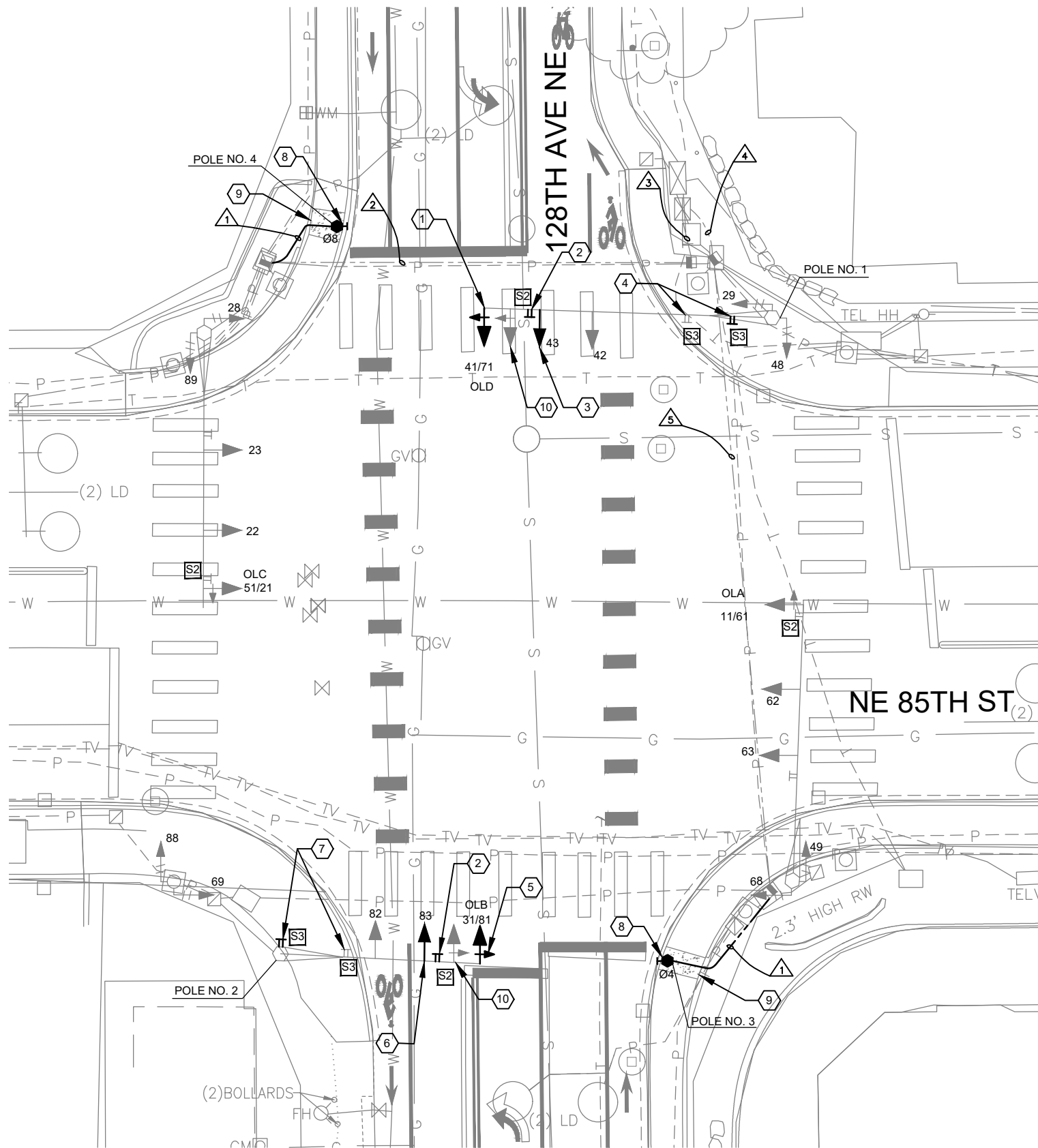
| WIRING SCHEDULE | | | |
|-----------------|-------------------------|----------------|---------|
| RUN NO. # | INCLOSURE OR ATTACHMENT | NEW | REMARKS |
| | | PUSH BUTTON 8C | |
| 1 | 1" PVC | 1 | |
| 2 | 2" PVC | 1 | |

GENERAL NOTES:

- 1. RRFB, PUSHBUTTON POST AND CONDUIT LOCATIONS SHOWN ARE APPROXIMATE. FINAL INSTALLATION LOCATION SHALL AVOID CURB RAMP AND LANDING AREA, AND SHALL BE APPROVED BY THE CITY ENGINEER PRIOR TO CONSTRUCTION.
- 2. ALL WORK SHALL BE PERFORMED PER CITY OF KIRKLAND STANDARD PLANS AND SPECIFICATIONS, UNLESS OTHERWISE NOTED IN THE DRAWING.
- 3. INVESTIGATE EXISTING UNDERGROUND UTILITIES PRIOR TO ANY POLE FOUNDATION EXCAVATION OR CONDUIT TRENCHING TO AVOID DAMAGE TO ANY EXISTING UNDERGROUND UTILITIES.
- 4. CONDUITS SHALL BE SCHEDULE 80 PVC.
- 5. ALL MATERIALS SHALL GO THROUGH THE SUBMITTAL REVIEW PROCESS AND BE APPROVED BY THE CITY ENGINEER.
- 6. REFERENCE TO CITY'S PRE-APPROVED POLICY R-24: RECTANGULAR RAPID FLASH BEACON (RRFB) INSTALLATION POLICY FOR RRFB COMPONENT LIST.



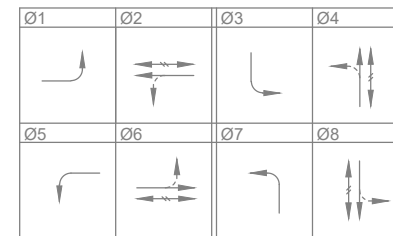
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NOTES:

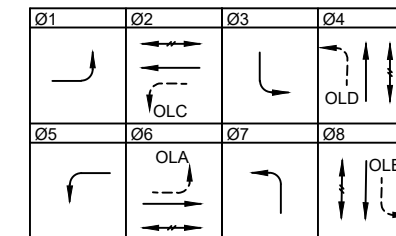
1. SEE DRAWING TS4 FOR GENERAL NOTES, WIRING SCHEDULE, AND FIELD WIRE TERMINATIONS.

EXISTING PHASE DIAGRAM:



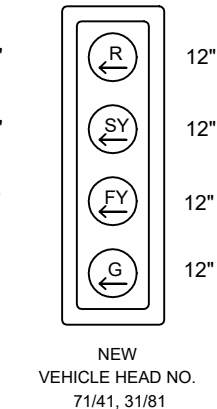
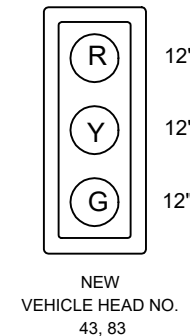
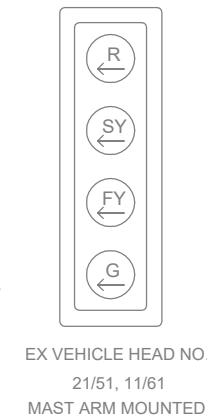
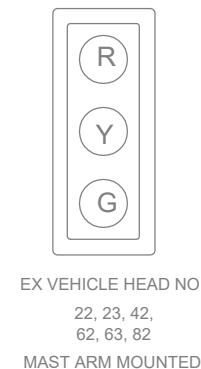
VEHICLE MOVEMENT (PROTECTED) →
VEHICLE MOVEMENT (PERMISSIVE) →
PEDESTRIAN MOVEMENT →

PROPOSED PHASE DIAGRAM :

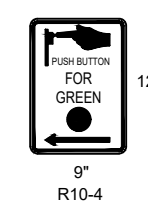
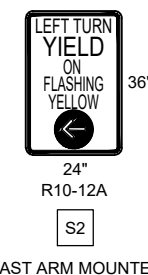


VEHICLE MOVEMENT (PROTECTED) →
VEHICLE MOVEMENT (PERMISSIVE) →
PEDESTRIAN MOVEMENT →
FLYER →

SIGNAL DISPLAYS:

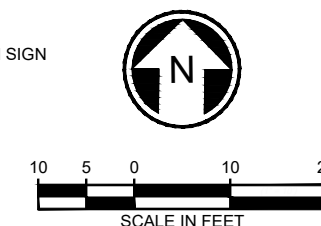


SIGNS:



CONSTRUCTION NOTES:

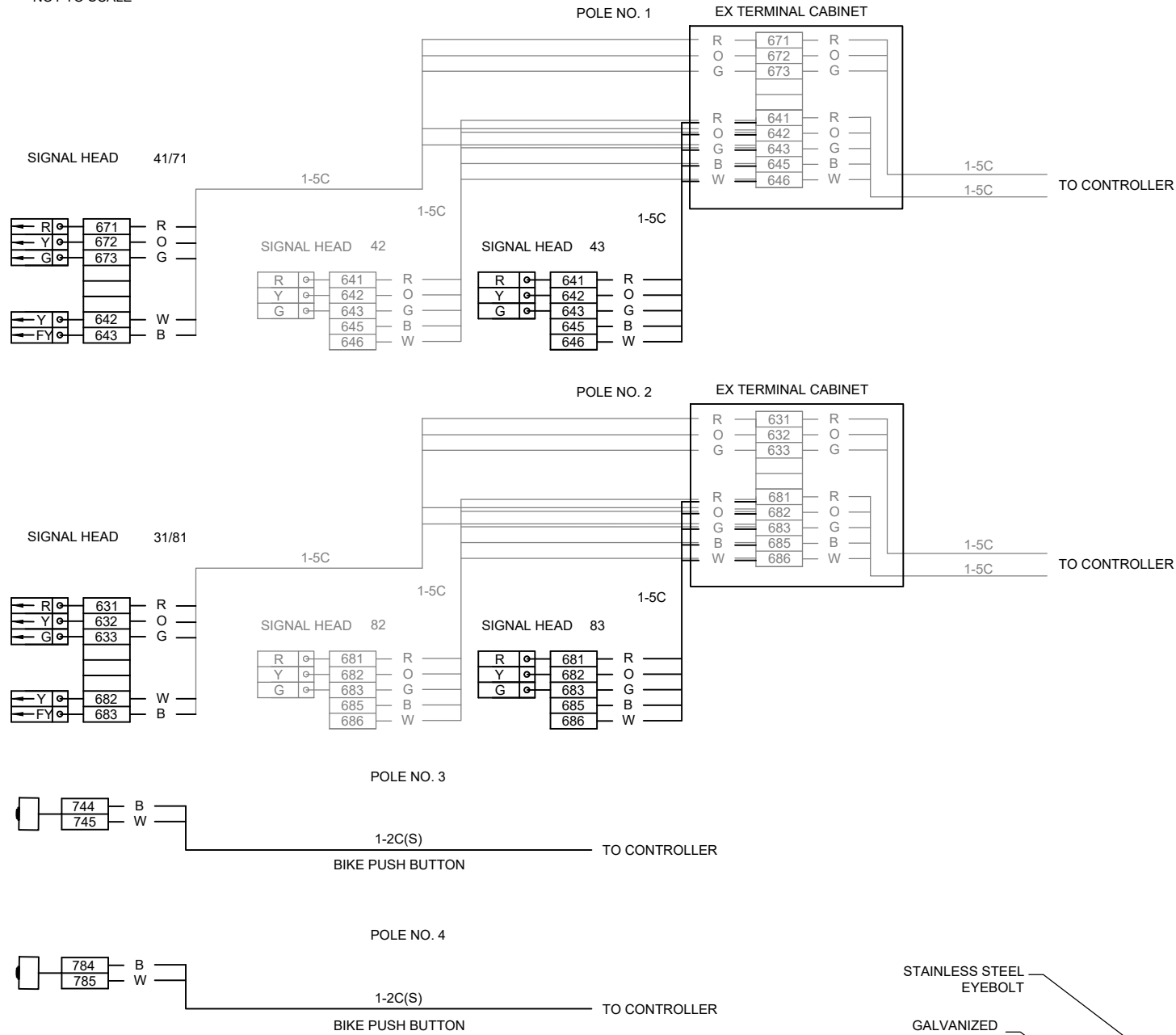
1. USE EXISTING TENON AT THE END OF MAST ARM TO INSTALL NEW 4-SECTION SIGNAL HEAD 71/41.
2. REPLACE EXISTING SIGN WITH NEW SIGN S2.
3. USE EXISTING TENON ON THE MAST ARM TO INSTALL NEW 3-SECTION SIGNAL HEAD 43.
4. RELOCATE EXISTING SIGN S3 FROM 14.5' ON MAST ARM FROM POLE TO 6' ON MAST ARM FROM POLE.
5. INSTALL NEW 4-SECTION SIGNAL HEAD 31/81 USING EXISTING TENON AT THE END OF MAST ARM.
6. USE EXISTING TENON ON MAST ARM TO INSTALL NEW 3-SECTION SIGNAL HEAD 83.
7. RELOCATE EXISTING SIGN S3 FROM MAST ARM TO POLE. SEE DRAWING TS5 DETAIL A FOR INSTALLATION DETAILS.
8. INSTALL BIKE BUSH BUTTON POST, FOUNDATION AND BULLDOG BIKE PUSHBUTTON WITH SIGN R10-4. SEE DRAWING TS5 DETAIL C FOR BIKE PUSHBUTTON DETAILS.
9. REPLACE FULL SIDEWALK PANEL TO MATCH SURROUNDING AREA.
10. REMOVE EXISTING 4-SECTION VEHICLE SIGNAL HEAD (3-BALL, 1-ARROW).



100% SUBMITTAL - NOT FOR CONSTRUCTION

FIELD WIRE TERMINATIONS:

NOT TO SCALE



NOTE:

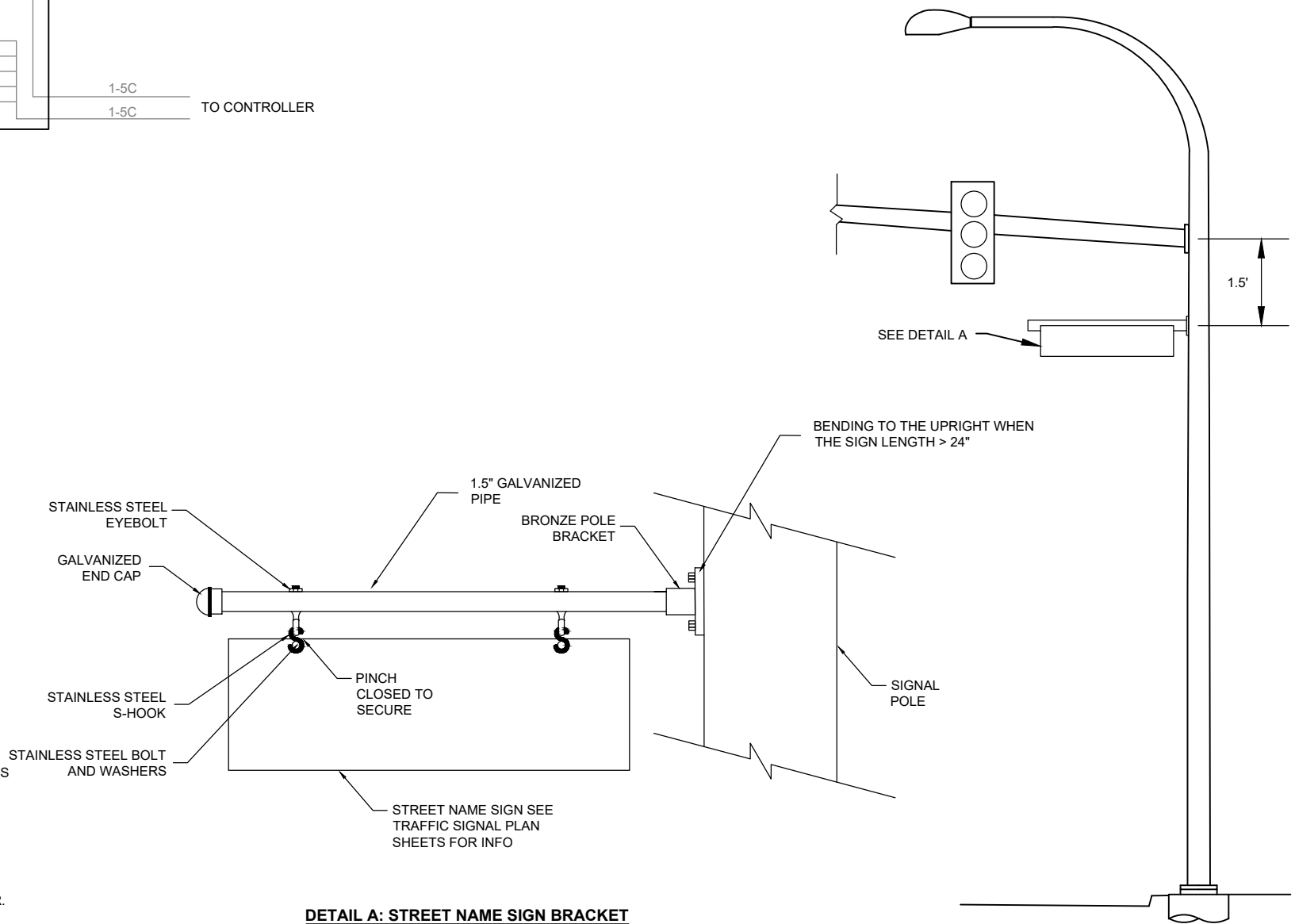
PROVIDE AT LEAST 7 DAYS ADVANCED NOTICE TO CITY OF KIRKLAND SIGNAL TIMING GROUP (425-587-3868) FOR THE CABINET WIRE TERMINATION.

GENERAL NOTES:

- ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH CITY OF KIRKLAND STANDARD PLANS AND SPECIFICATIONS UNLESS OTHERWISE NOTED IN THE DRAWING.
- INVESTIGATE EXISTING UNDERGROUND UTILITIES PRIOR TO ANY POLE FOUNDATION EXCAVATION OR CONDUIT TRENCHING TO AVOID DAMAGE TO ANY EXISTING UNDERGROUND UTILITIES.
- CONDUITS SHALL BE SCHEDULE 80 PVC.
- ALL MATERIALS SHALL GO THROUGH THE SUBMITTAL REVIEW PROCESS AND BE APPROVED BY THE CITY ENGINEER.

WIRING SCHEDULE

| RUN NO. ⚠ | CONDUIT SIZE | EXISTING | | | | | | NEW BIKE PUSH BUTTON 2C(SH) | REMARKS |
|--------------|--------------|-------------------|-------------------|-------------|------------|-------------|----------|-----------------------------|---------|
| | | VEH DETECT 2C(SH) | E V DETECT 3C(SH) | VEH HEAD 5C | PPB 2C(SH) | PED HEAD 5C | FIBER IC | EOP GROUND #8 | |
| 1 | 1" PVC | | | | | | | 1 | |
| 2 | 3" PVC | 6 | 1 | 2 | 2 | 2 | | 1 | |
| | 2" PVC | | | | | | | 1 | |
| 3 | 4" PVC | 6 | 1 | 2 | 2 | 2 | 1 | 1 | |
| | 2" PVC | | | | | | | 1 | |
| 4 | 4" PVC | 22 | 3 | | 3 | | | 1 | |
| | 3" PVC | | | 6 | | 6 | | 1 | |
| | 2" PVC | | | | | | | 1 | |
| 5 | 3" PVC | 8 | | 2 | 1 | 2 | | 1 | |
| | 2" PVC | | | | | | | 1 | |

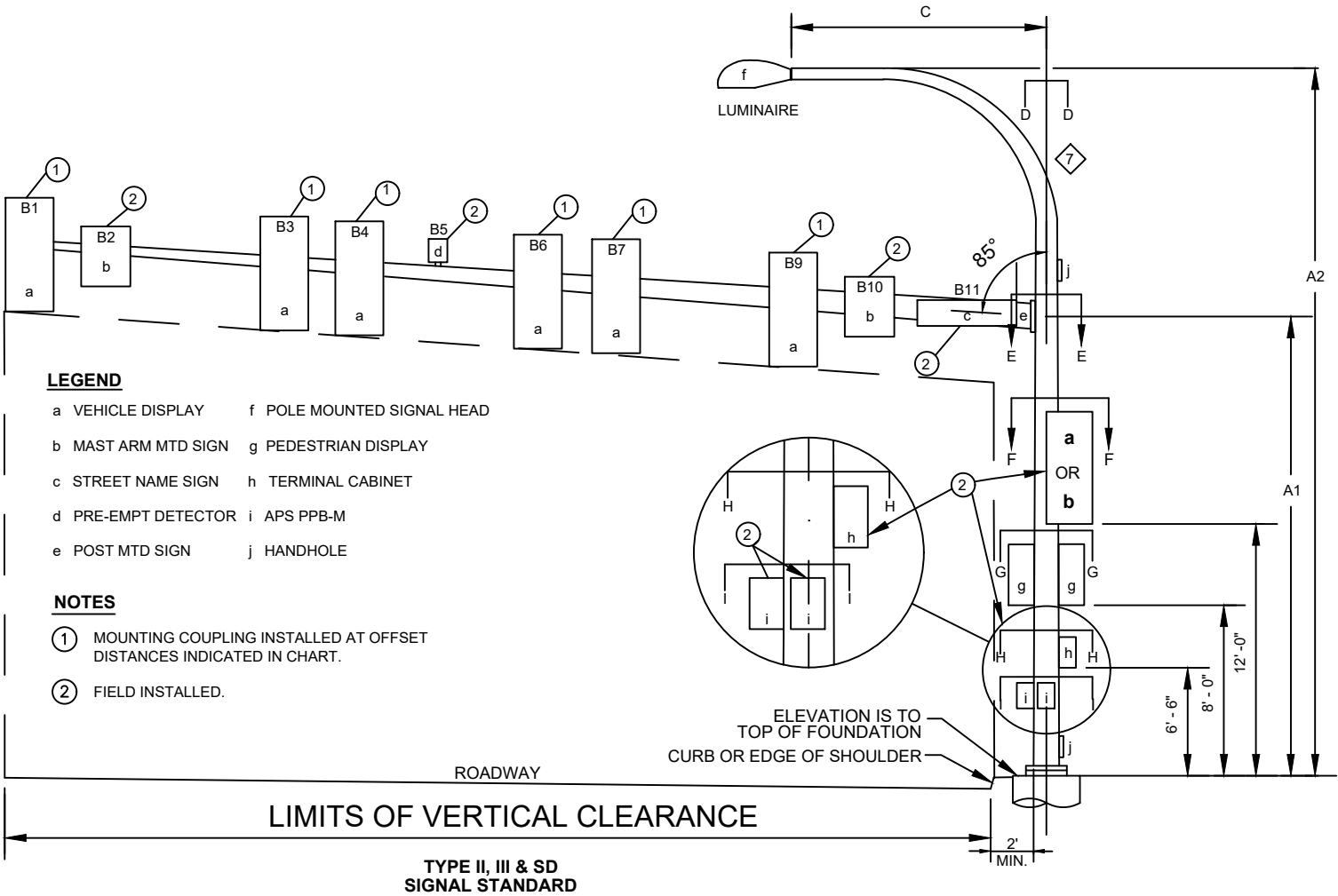
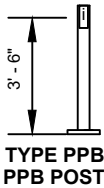


DETAIL A: STREET NAME SIGN BRACKET

NOT TO SCALE

100% SUBMITTAL - NOT FOR CONSTRUCTION

| FOUNDATION DEPTH TABLE | | | | | | | |
|--|-----------------|---|-----|-------|-------|-------|-------|
| ALLOWABLE LATERAL BEARING PRESSURE (psf) | FOUNDATION TYPE | XYZ (CUBIC FEET) TYPE II,III AND SD MAST ARM STANDARDS | | | | | |
| | | 600 | 900 | 1,200 | 1,500 | 1,900 | 2,300 |
| 1,000 | 3' ROUND | 10' | 10' | 11' | 11' | 13' | 15' |
| | 3' SQUARE | 8' | 8' | 9' | 9' | 10' | 11' |
| | 4' ROUND | 8' | 8' | 9' | 9' | 10' | 11' |
| 1,500 | 3' ROUND | 8' | 8' | 9' | 11' | 13' | 15' |
| | 3' SQUARE | 7' | 7' | 7' | 8' | 8' | 9' |
| | 4' ROUND | 7' | 7' | 7' | 8' | 8' | 9' |
| 2,500 | 3' ROUND | 6' | 6' | 7' | 11' | 13' | 15' |
| | 3' SQUARE | 6' | 6' | 6' | 6' | 7' | 7' |
| | 4' ROUND | 6' | 6' | 6' | 6' | 7' | 7' |



- LEGEND**
- a VEHICLE DISPLAY
 - b MAST ARM MTD SIGN
 - c STREET NAME SIGN
 - d PRE-EMPT DETECTOR
 - e POST MTD SIGN
 - f POLE MOUNTED SIGNAL HEAD
 - g PEDESTRIAN DISPLAY
 - h TERMINAL CABINET
 - i APS PPB-M
 - j HANDHOLE

- NOTES**
- ① MOUNTING COUPLING INSTALLED AT OFFSET DISTANCES INDICATED IN CHART.
 - ② FIELD INSTALLED.

MAST ARM DATA DETAIL
NOT TO SCALE

| SIGNAL STANDARD DETAIL CHART | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
|------------------------------|-------------------|-----------|----------------------|---|----|----|----|----|----|----|----|-----|----|----|------------------------------|----|----|----|-----|--------------------|-------------------------------------|----|----|---|----|----|---|----|----|--|--|--|-----------------------------|-----------------------------|-------------------------|--------|--------|---------|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|--|
| STD. No. | REFERENCE ROADWAY | POLE TYPE | MOUNTING HEIGHT (FT) | SIGNAL MAST ARM DATA | | | | | | | | | | | | | | | | LUMINAIRE ARM (FT) | POLE ATTACHMENT POINT ANGLES (deg.) | | | | | | | | | | | | FOUNDATION DESIGN XYZ (FT³) | SOIL BEARING PRESSURE (PSF) | FOUNDATION N DEPTH (FT) | | | REMARKS | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | | | | OFFSETS (FT) (POLE TO ATTACHMENT POINT) | | | | | | | | | | | WINDLOAD AREAS (FT²) (X) (Y) | | | | | | | | | | | | | | | | | | | | 3' RD. | 3' SQ. | 4' RD. | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | | | A1 | A2 | B1 | B2 | B3 | B4 | B5 | B6 | B7 | B11 | B1 | B2 | B3 | B4 | B6 | B7 | B11 | C | D | E1 | E2 | F | G1 | G2 | H | I1 | I2 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |

*SEE WSDOT STANDARD PLAN J-20.10-03 FOR FOUNDATION DETAILS.

TOOLE DESIGN

720 3RD AVE SUITE 2020
SEATTLE, WA 98104
PHONE: (206) 297-1601
FAX: (301) 927-2800
www.tooledesign.com

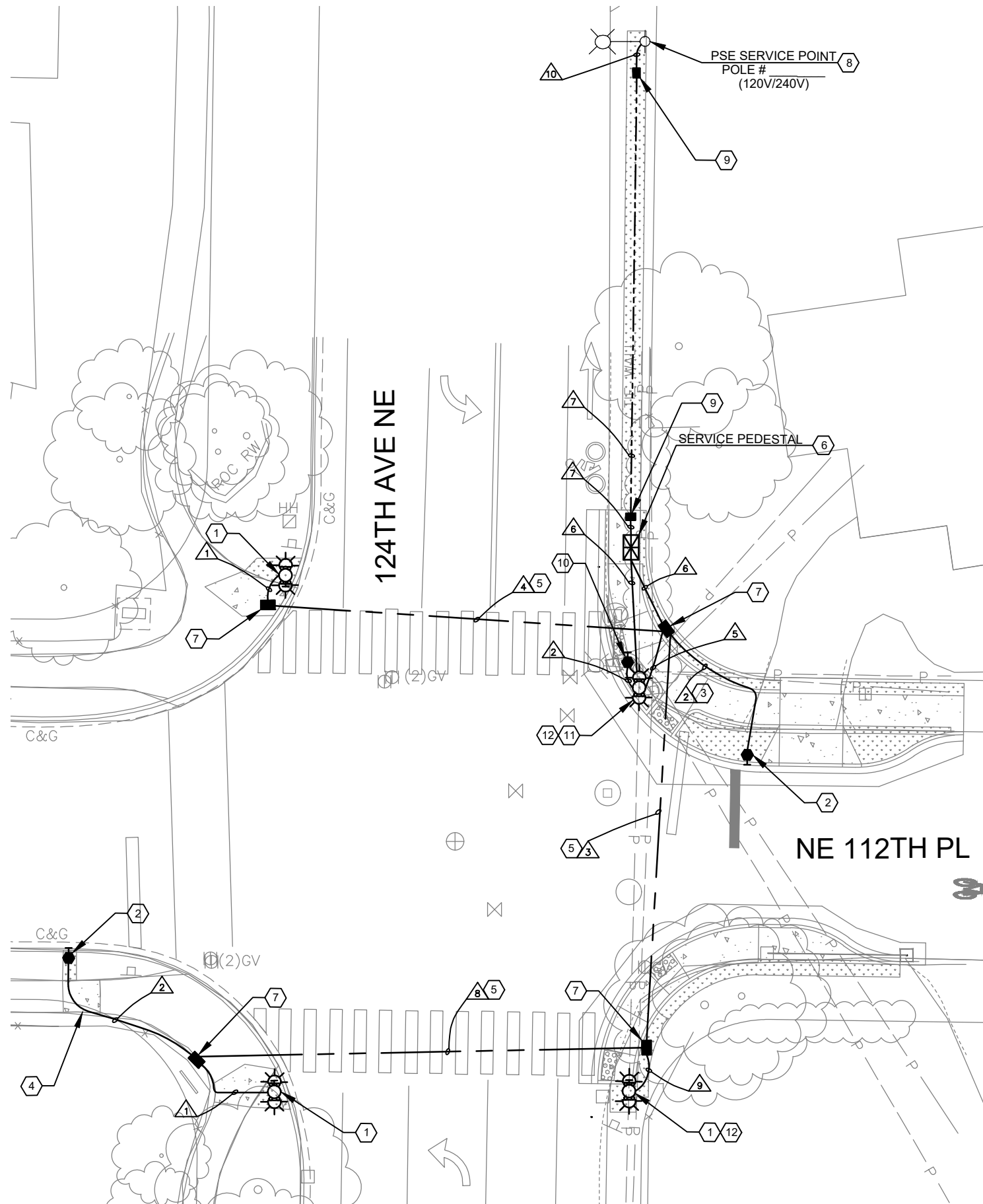
concord ENGINEERING

CITY OF KIRKLAND WASHINGTON

128TH AVE NE GREENWAY
KIRKLAND GREENWAYS
PROJECT# 25-18-PW
KIRKLAND, WA

0 10 20 40
SCALE AS NOTED

PREPARED: AL/TD
CHECKED: CS/KL
DATE
DECEMBER 2019
REV. 1
REV. 2
REV. 3
SHEET NAME
NE 85TH ST
SIGNAL PLAN
DRAWING NUMBER
TS6
SHEET NUMBER
46 OF 49



CONSTRUCTION NOTES:

- 1 INSTALL AC-POWERED RECTANGULAR RAPID FLASH BEACON (RRFB) SYSTEM BACK-TO-BACK INCLUDING POLE, FOUNDATION, XAV2 AUDIBLE PUSH BUTTON AND SIGNS ACCORDING TO MANUFACTURER'S REQUIREMENT. SEE DRAWING TSDT2 DETAIL D FOR RRFB DETAILS.
- 2 INSTALL BREAKAWAY BIKE PUSHBUTTON POST, FOUNDATION, PUSHBUTTON, SIGN R10-25C ACCORDING TO MANUFACTURER'S REQUIREMENT. SEE DRAWING TSDT1 DETAIL C FOR BIKE PUSHBUTTON DETAILS.
- 3 INSTALL 1" CONDUIT FROM RRFB BASE TO BIKE PUSHBUTTON POST BASE.
- 4 REPLACE FULL SIDEWALK PANEL TO MATCH SURROUNDING AREA.
- 5 DIRECT DRILL TO INSTALL 3" CONDUIT CROSSING.
- 6 COORDINATE WITH PSE FOR SERVICE PEDESTAL TYPE AND INSTALLATION.
- 7 INSTALL SLIP-RESISTANT TYPE 2 JUNCTION BOX ACCORDING TO WSDOT STANDARD PLAN J-40.10-04.
- 8 INSTALL ONE 2" CONDUIT RISER ON NORTH SIDE OF POLE FOR RRFB SERVICE. PULL SERVICE CONDUCTORS TO TOP OF RISER AND COIL 15' OF WIRE FOR CONNECTION BY PSE.
- 9 COORDINATE WITH PSE FOR SECONDARY HANDHOLE INSTALLATION.
- 10 INSTALL BREAKAWAY PUSHBUTTON POST, FOUNDATION, PUSHBUTTON, SIGN R10-25 ACCORDING TO MANUFACTURER'S REQUIREMENT.
- 11 INSTALL AC-POWERED RECTANGULAR RAPID FLASH BEACON (RRFB) SYSTEM BACK-TO-BACK INCLUDING POLE, FOUNDATION, AND SIGNS ACCORDING TO MANUFACTURER'S REQUIREMENT. SEE DRAWING TSDT2 DETAIL D FOR RRFB DETAILS.
- 12 INSTALL RRFB CONTROLLER ENCLOSURE UNIT.

GENERAL NOTES:

1. RRFB, PUSHBUTTON POST AND CONDUIT LOCATIONS SHOWN ARE APPROXIMATE. FINAL INSTALLATION LOCATION SHALL AVOID CURB RAMP AND LANDING AREA, AND SHALL BE APPROVED BY THE CITY ENGINEER PRIOR TO CONSTRUCTION.
2. ALL WORK SHALL BE PERFORMED PER CITY OF KIRKLAND STANDARD PLANS AND SPECIFICATIONS, UNLESS OTHERWISE NOTED IN THE DRAWING.
3. INVESTIGATE EXISTING UNDERGROUND UTILITIES PRIOR TO ANY DIRECT DRILLING, POLE FOUNDATION EXCAVATION OR CONDUIT TRENCHING TO AVOID DAMAGE TO ANY EXISTING UNDERGROUND UTILITIES.
4. CONDUITS SHALL BE SCHEDULE 80 PVC.
5. ALL MATERIALS SHALL GO THROUGH THE SUBMITTAL REVIEW PROCESS AND BE APPROVED BY THE CITY ENGINEER.
6. REFERENCE TO CITY'S PRE-APPROVED POLICY R-24: RECTANGULAR RAPID FLASH BEACON (RRFB) INSTALLATION POLICY FOR RRFB COMPONENT LIST.

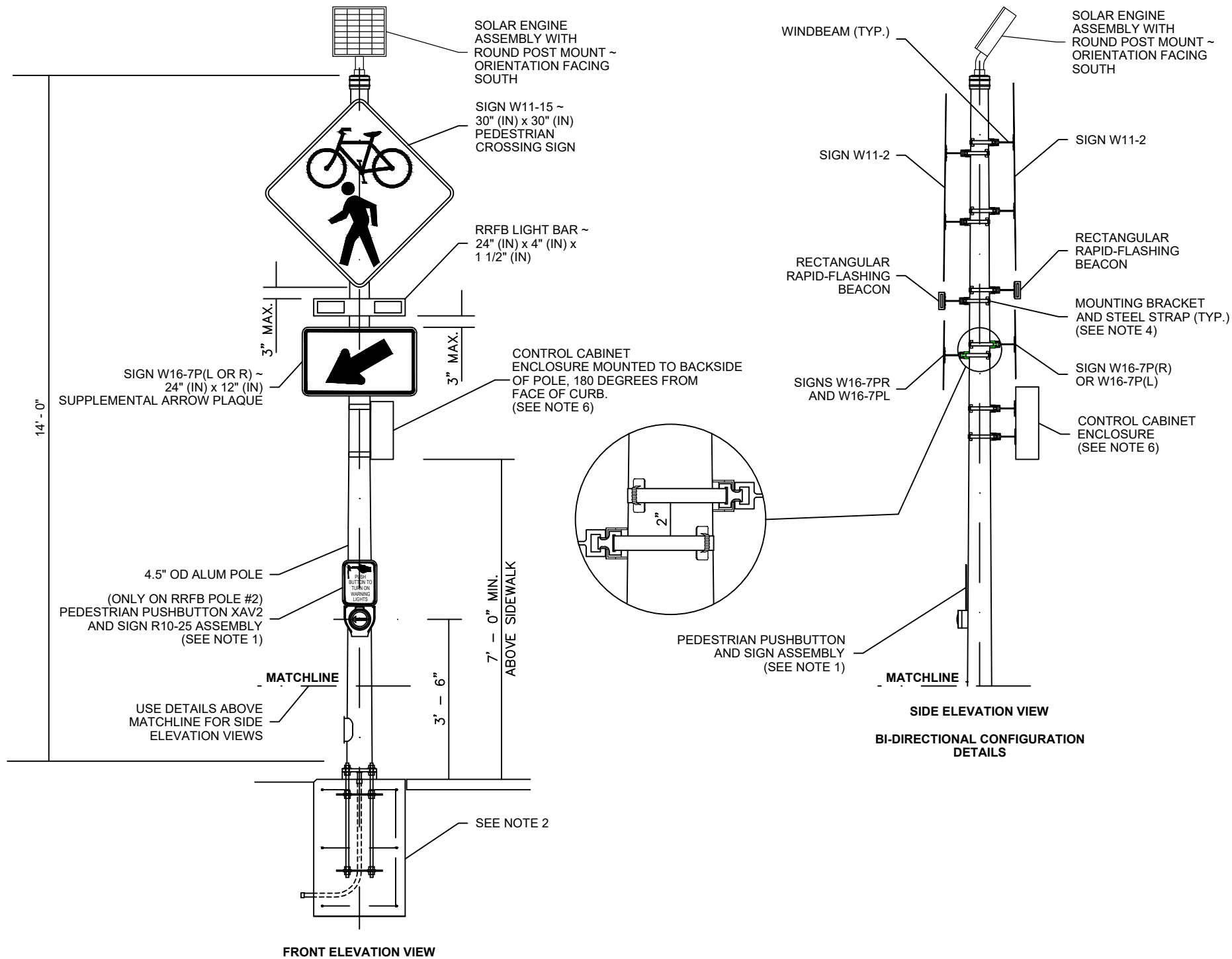
WIRING SCHEDULE

| RUN NO. # | CONDUIT SIZE | SERVICE POWER 2-#2 | CONTROLLER 2-#10 | RRFB 1-4C | PUSH BUTTON 1-8C | GROUND #8 | REMARK |
|--------------|-------------------|-----------------------|---------------------|--------------|---------------------|--------------|--------|
| 1 | 1" PVC | | | 1 | 1 | 1 | |
| 2 | 1" PVC | | | | 1 | 1 | |
| 3 | 3" HDPE | | 1 | | | 1 | |
| 4 | 3" HDPE | | | 1 | 1 | 1 | |
| 5 | 2" PVC | | | 1 | 2 | 1 | |
| 6 | 2" PVC | | 1 | | | 1 | |
| 7 | 2" PVC | 1 | | | | 1 | |
| 8 | 3" HDPE | | | 1 | 2 | 1 | |
| 9 | 2" PVC | | 1 | 1 | 2 | 1 | |
| 10 | 2" PVC WITH RISER | 1* | | | | 1 | |

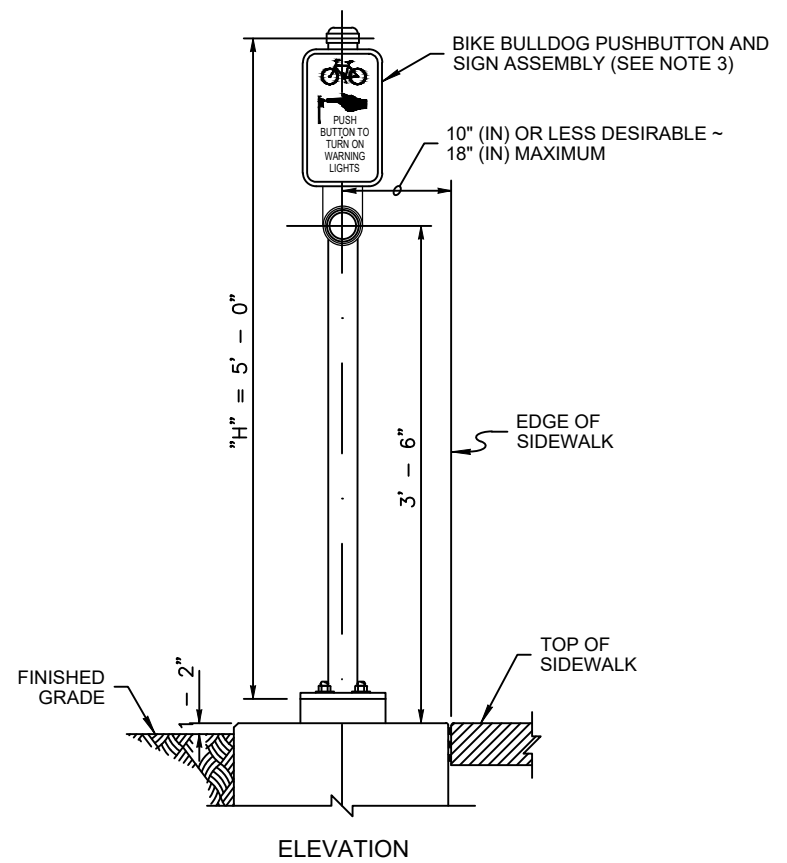
* COIL 15' OF WIRE FOR PSE POWER CONNECTION.



100% SUBMITTAL - NOT FOR CONSTRUCTION



**SOLAR POWERED RECTANGULAR RAPID FLASH BEACON
DETAIL B (NTS)**

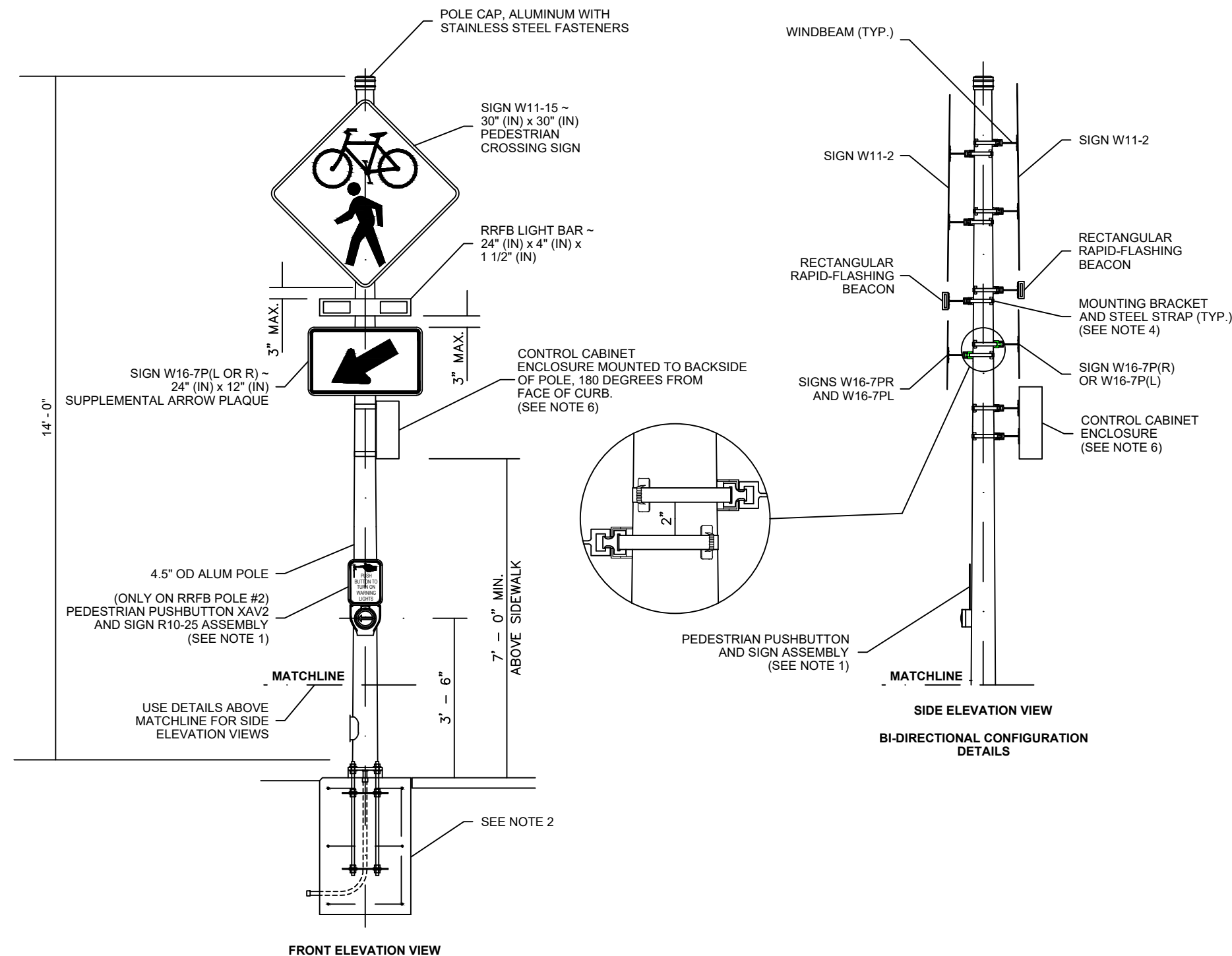


**BIKE PUSHBUTTON POST
DETAIL C (NTS)**

NOTES

1. PEDESTRIAN PUSHBUTTON AND SIGN ASSEMBLY - MAY BE SEPARATE PARTS. USE 9" (IN) X 12" (IN) R10-25 SIGN IN ACCORDANCE WITH 2009 MUTCD. SIGN MAY INCLUDE INTEGRATED WARNING LIGHTS. COORDINATE WITH CITY OF KIRKLAND FOR AUDIBLE MESSAGE.
2. SEE WSDOT STANDARD PLAN J-21.10-04 FOR SIGNAL STANDARD FOUNDATION WITH FIXED BASE DETAILS.
3. BIKE PUSHBUTTON AND SIGN ASSEMBLY - MAY BE SEPARATE PARTS. USE 9" (IN) X 12" (IN) R10-25C SIGN IN ACCORDANCE WITH 2009 MUTCD. SIGN MAY INCLUDE INTEGRATED WARNING LIGHTS. COORDINATE WITH CITY OF KIRKLAND FOR AUDIBLE MESSAGE. SEE WSDOT STANDARD PLAN J-20.10-04 FOR INSTALLATION DETAILS.
4. SEE WSDOT STANDARD PLAN G-30.10 FOR SIGN INSTALLATION ON SIGNAL STANDARD DETAILS.
5. TERMINATE RRFB WIRE CONNECTIONS PER MANUFACTURER'S RECOMMENDATION.
6. CONTROL CABINET ENCLOSURE SHALL BE SIZED BY THE RRFB MANUFACTURER. THE CONTROL CABINET SHALL BE MANUFACTURED PER TERMINAL CABINET REQUIREMENTS OF WSDOT STANDARD SPECIFICATION SECTION 9-29.25.

100% SUBMITTAL - NOT FOR CONSTRUCTION



**AC POWERED RECTANGULAR RAPID FLASH BEACON
DETAIL D (NTS)**

NOTES

1. PEDESTRIAN PUSHBUTTON AND SIGN ASSEMBLY - MAY BE SEPARATE PARTS. USE 9" (IN) X 12" (IN) R10-25 SIGN IN ACCORDANCE WITH 2009 MUTCD. SIGN MAY INCLUDE INTEGRATED WARNING LIGHTS. COORDINATE WITH CITY OF KIRKLAND FOR AUDIBLE MESSAGE.
2. SEE WSDOT STANDARD PLAN J-21.10-04 FOR SIGNAL STANDARD FOUNDATION WITH FIXED BASE DETAILS.
3. NOT USED.
4. SEE WSDOT STANDARD PLAN G-30.10 FOR SIGN INSTALLATION ON SIGNAL STANDARD DETAILS.
5. TERMINATE RRFB WIRE CONNECTIONS PER MANUFACTURER'S RECOMMENDATION.
6. CONTROL CABINET ENCLOSURE SHALL BE SIZED BY THE RRFB MANUFACTURER. THE CONTROL CABINET SHALL BE MANUFACTURED PER TERMINAL CABINET REQUIREMENTS OF WSDOT STANDARD SPECIFICATION SECTION 9-29.25.