



DESIGN REVIEW BOARD DECISION

DATE: March 9, 2023
FILE NUMBER: DRV22-00569
PROJECT NAME: Polaris at Totem Lake
APPLICANT: Dane Knudson, with Olson Projects
PROJECT PLANNER: Jennifer Anderer, Associate Planner

I. SUMMARY OF DECISION

Dane Knudson, with Olson Projects, applied for a Design Response Conference for a new mixed-use development located at 12335 120th Avenue NE (see Attachment 1). The project consists of two eight-story buildings with approximately 442 housing units, 15,000 square feet of interior amenity space, 4,000 square feet of commercial space, and a parking garage with one story below grade. Residential vehicular access is proposed from 120th Avenue NE via an access easement shared with the Totem Lake Plaza, a commercial property south of the subject property.

On November 7, 2022, the Design Review Board (DRB) approved the proposed project subject to the following conditions:

- A. This application is subject to the applicable requirements contained in the Kirkland Municipal Code, Zoning Code, and Building and Fire Code. It is the responsibility of the applicant to ensure compliance with the various provisions contained in these ordinances. Attachment 2, Development Standards, is intended to familiarize the applicant with some of the additional development regulations. This attachment does not include all of the additional regulations.
- B. As part of the application for a building permit, the applicant shall submit the following:
 1. Construction plans demonstrating compliance with the project plans approved by the DRB based on the applicable design guidelines and all zoning regulations (see Attachment 3). Compliance with zoning regulations will be confirmed as part of the permit review process.
 2. Construction plans that are consistent with an emphasis on the landscape plans, exterior elevations, and materials and incorporate the following design details as approved by the DRB:
 - a. Decorative screening panels along the public parking garage as proposed; and

- b. Lighting for the ground floor public pedestrian plaza located on 120th Avenue NE as proposed; and
 - c. A public walkway located within the butterfly garden along 120th Avenue NE; and
 - d. Enhanced landscaping treatments surrounding the garage entrance located on the south side of the project.
 3. A summary of any project changes that are not consistent with the DRB approval indexed to the permit drawings. Based on the scope of the changes, staff may require a minor modification application (see Section VI below).
- C. Prior to the final inspection of a building permit by the Planning Official, the project architect shall submit a letter stating that they have evaluated the project to ensure it is consistent with the plans approved through Design Board Review and that no modifications have been made that were not previously approved by the City.

II. DESIGN RESPONSE CONFERENCE MEETINGS

A. Background Summary

The DRB held two Design Response Conference meetings for the project. Staff reports and associated materials for the meetings referenced below can be found online at the DRB's meeting webpage:

<https://www.kirklandwa.gov/Government/Departments/Planning-and-Building/Design-Review-Board/DRB-Meeting-Materials-Archive>

To prepare for the Design Response Conferences, the DRB held two Conceptual Design Conferences on May 2, 2022 and June 6, 2022 on the proposed project. At the meetings, the DRB provided direction to the applicant in preparation for the Design Response Conference.

Below is a summary of the Board's discussions at the October 3, 2022 and November 7, 2022 Design Response Conferences.

October 3, 2022 Design Response Conference: The Design Review Board reviewed the plans submitted by the applicant dated July 27, 2022. Staff provided an overview of the Zoning Code and the key design issues for the project. Staff's memo dated September 25, 2022, provides an analysis of project consistency with the Zoning regulations and Design Guidelines for Totem Lake Business Districts. After receiving public comment on the project and deliberating, the Board requested the applicant to return for a second meeting to respond to the following DRB comments:

- Increase the overall modulation, particularly along 120th Avenue NE, and to create variation in the parapet design.
- Refine the public plaza to improve access, incorporate additional seating, and enhance the connection between the public sidewalk and the plaza.

- Provide a more detailed landscape plan focusing on seasonal plantings along 120th Avenue NE and the landscape buffer along the north property line adjacent to the Evergreen Montessori property.
- Expand the “anchor corner” so that it functions as a focal point to the building’s overall design.
- Provide additional treatments to any blank walls.

November 7, 2022 Design Response Conference: The DRB reviewed the plans submitted by the applicant dated November 7, 2022. Staff’s memo dated, also November 7, 2022, provided an overview of the items discussed at the October 3rd conference and how the applicant has addressed them. See Section III below for further information regarding the DRB’s discussion and conclusions.

B. Public Comment

All public comment letters and e-mails received during the Design Response Conference meetings were forwarded to the Board for consideration in addition to the oral comment from the public meetings (see Attachment 4). All comments are contained in the City’s official file. Below is a summary of the general public comment themes that emerged through the design review process:

Tree Retention

A row of mature trees along the north property line provides a buffer between the subject property and an existing school. Both the applicant and the property owners to the north are interested in preserving the trees. The Kirkland Zoning Code (KZC) Chapter 95 requires the applicant to maintain a perimeter landscape buffer for driving and parking areas. Since the row of trees along the north property line are also adjacent to a proposed driveway and within the required perimeter landscape buffer area, they should be retained and be maintained in perpetuity. From a design perspective, the applicants have revised their grading plans to preserve the root zones for these trees, worked closely with a certified arborist to assess the health of the trees, and prepared a shadow analysis to confirm the massing of the building will not impact the trees.

Lines of Sight

The property owners of the existing school north of the project were concerned about the lines of sight between the proposed residential units along the north side of the building and the outdoor play space used by school children throughout the day. The applicant has worked with the neighboring property and provided a revised design wherein only 8 of the 442 residential units will have a direct line of sight to the play space. This accounts for less than 2% of the total units.

Commercial Space

Representatives from the Lake Washington School District reached out to staff to note their support of additional affordable housing units in Kirkland and requested that some of the commercial space be used in a way that provides school space, such as a preschool. The school district has been in contact with multiple affordable housing developers with active projects in Kirkland and plans to continue to work with the Polaris applicants to explore opportunities that would support the school district.

III. DESIGN REVIEW BOARD DISCUSSION AND CONCLUSIONS

Below is a summary of the key issues and conclusions reached by the Design Review Board during the design review process. For more background on these issues and an evaluation of how the project meets the Zoning Code and design guidelines, see staff advisory reports from the design response conferences contained in File DRV22-00569.

A. BUILDING HEIGHT, ARCHITECTURAL AND HUMAN SCALE

DRB Discussion: The DRB agreed with the applicant's preferred two-building massing model with complimentary massing treatments and requested additional modulation, particularly along 120th Avenue NE due to the expansive façade and development of the "corner anchor". Vertical modulation was achieved through upper story terraces, building separation, and an "H" shaped design which provides visual relief. Horizontal modulation was achieved through strong street activation, engaging pedestrian commercial space on the ground floor, and material articulation. Roofline parapet modulation and material variation were added to the "corner anchor", creating a focal point for the building.

DRB Conclusions: The DRB concluded that the proposed building massing, architectural scale, and human scale are consistent with the applicable design guidelines found in the Design Guidelines for Totem Lake Business Districts.

B. VEHICULAR AND PEDESTRIAN ACCESS

DRB discussion: The DRB reviewed the vehicular access, pedestrian access, and street activation along 120th Avenue NE for the project. Residential vehicular access is proposed from 120th Avenue NE via an access easement shared with the Totem Lake Plaza, a commercial property south of the subject property. The site contains multiple pedestrian access points with leasing and amenity spaces in both towers. The DRB discussed the following pedestrian-oriented elements of the project with a focus on street activation, which were then incorporated into the design:

- 120th Avenue NE Public Plaza:
The DRB requested that the applicant look at ways to activate the plaza space by providing a more seamless connection to the public sidewalk along 120th Avenue NE and incorporating additional lighting, decorative pavers, and seating opportunities. The applicant revised the pedestrian circulation and access scheme by providing stair access at the primary entrance to the plaza and a wider access ramp that allows the gathering space to spill over from the plaza toward the public parking garage and butterfly garden. The revised design also incorporates additional seating, decorative paving, a lighting plan for public spaces, and reduced planter beds to increase the pedestrian circulation space.

- Butterfly Garden:
The DRB appreciated that the design incorporated a butterfly garden along 120th Avenue NE and asked the applicant to explore ways to increase public activation of this space. The applicant has agreed to incorporate a walkway through the garden to draw pedestrians into the garden and create a more interactive public space.
- Public Parking Screening:
The DRB requested the applicant explore artwork opportunities within the enhanced landscape areas, particularly near the public parking garage. The applicant revised the design to include decorative metal panels as a backdrop to the landscaping and butterfly garden, which doubles as screening for the public parking garage.

DRB conclusions: The DRB concluded that the proposed project was consistent with the design guidelines found in the Design Guidelines for Totem Lake Business Districts related to vehicular and pedestrian access, provided that the project includes a public walkway through the butterfly garden, decorative screening panels along the public parking garage and the proposed lighting for the ground floor public pedestrian plaza located on 120th Avenue NE. and decorative screening panels along the public parking garage.

C. LANDSCAPING

DRB discussion: The DRB reviewed the pedestrian-oriented elements and landscape plan for the project and supported the applicant's proposed landscaping, particularly along the resident parking garage entrance on the south side of the project and 120th Avenue NE, and tree retention along the north property line of the project site. The Board asked the applicant to provide wide landscape areas and a detailed landscape plan to address privacy, enhance public spaces, and incorporate seasonal plantings. The revised plans incorporated larger landscape areas and a butterfly garden through landscaping techniques, and the applicant agreed to include thoughtful landscaping around the resident parking garage. The Board and applicant also paid particular attention to the existing trees and landscape buffer along the north property line of the site to provide privacy to the neighboring school. This landscape buffer is required to be maintained in perpetuity pursuant to Kirkland Zoning Code Section 95.42.

DRB conclusions: The DRB concluded that the proposed project was consistent with the Design Guidelines for Totem Lake Business Districts related to open space and landscaping, provided additional landscaping beyond what was proposed is incorporated into the design surrounding the resident parking garage.

D. BUILDING MATERIALS, COLOR, AND DETAIL

DRB discussion: The DRB evaluated the proposed materials, colors, and details and supported the applicant's design and use of color. The Board wanted to see thoughtful use of material and color variation to create definition and movement, particularly along 120th Avenue NE. The Board also asked the applicant to explore a screening option for

the surface parking near the north end of the project adjacent to 120th Avenue NE. The revised plans show an increased variation in materials, colored accents, folded scallop infill panels for movements, and decorative metal panels as a backdrop to the butterfly garden that also functions to screen the proposed surface parking.

DRB conclusions: The DRB concluded that the proposed project was consistent with the design guidelines found in the Design Guidelines for Totem Lake Business Districts related to building materials, color, and details.

IV. STATE ENVIRONMENTAL POLICY ACT (SEPA)

The City issued a SEPA Determination of Nonsignificance on February 7, 2023 for the project. No appeals of the determination were filed.

V. DEVELOPMENT REVIEW COMMITTEE

Comments and requirements placed on the project by City departments are found on the Development Standards, Attachment 2.

VI. SUBSEQUENT MODIFICATIONS

Modifications to the approval may be requested and reviewed pursuant to the applicable modification procedures and criteria in effect at the time of the requested modification.

VII. APPEALS OF DESIGN REVIEW BOARD DECISIONS AND LAPSE OF APPROVAL

Appeals

Section 142.40 of the Zoning Code allows the Design Review Board's decision to be appealed to the Hearing Examiner by the applicant or any person who submitted written or oral comments to the Design Review Board. The appeal must be in the form of a letter of appeal and must be delivered, along with any fees set by ordinance, to the Planning and Building Department by 5:00 p.m., April 18, 2023, fourteen (14) calendar days following the postmarked date of distribution of the Design Review Board's decision.

Only those issues under the authority of the Design Review Board as established by Kirkland Zoning Code 142.35(2) are subject to appeal.

Lapse of Approval (KZC 142.55)

The applicant must begin construction or submit to the City a complete building permit application for the development activity, use of land or other actions approved under this chapter within five (5) years after the final approval of the City of Kirkland on the matter, or the decision becomes void.

The applicant must substantially complete construction for the development activity, use of land or other actions approved under this chapter and complete the applicable conditions listed on the notice of decision within seven (7) years after the final approval on the matter or the decision becomes void.

VIII. ATTACHMENTS

1. Vicinity Map
2. Development Standards
3. Applicant Plans
4. Public Comments

IX. PARTIES

The following is a list of parties that have submitted written or oral comments to the DRB:

Joe Tovar of Tovar Planning
540 Dayton Street, #202
Edmonds, WA 98020

David Bedford and Jeff Bedford of Bedford Properties, LLC
1037 NE 65th Street, Ste. 80054
Seattle, WA 98115

Brian Buck of Lake Washington School District
15212 NE 95th Street
Redmond, WA 98052

X. APPROVAL

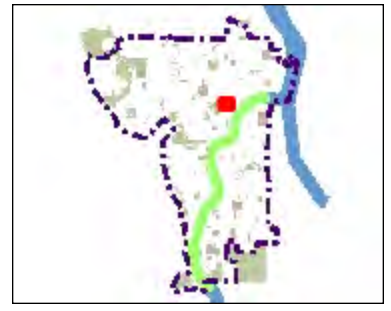
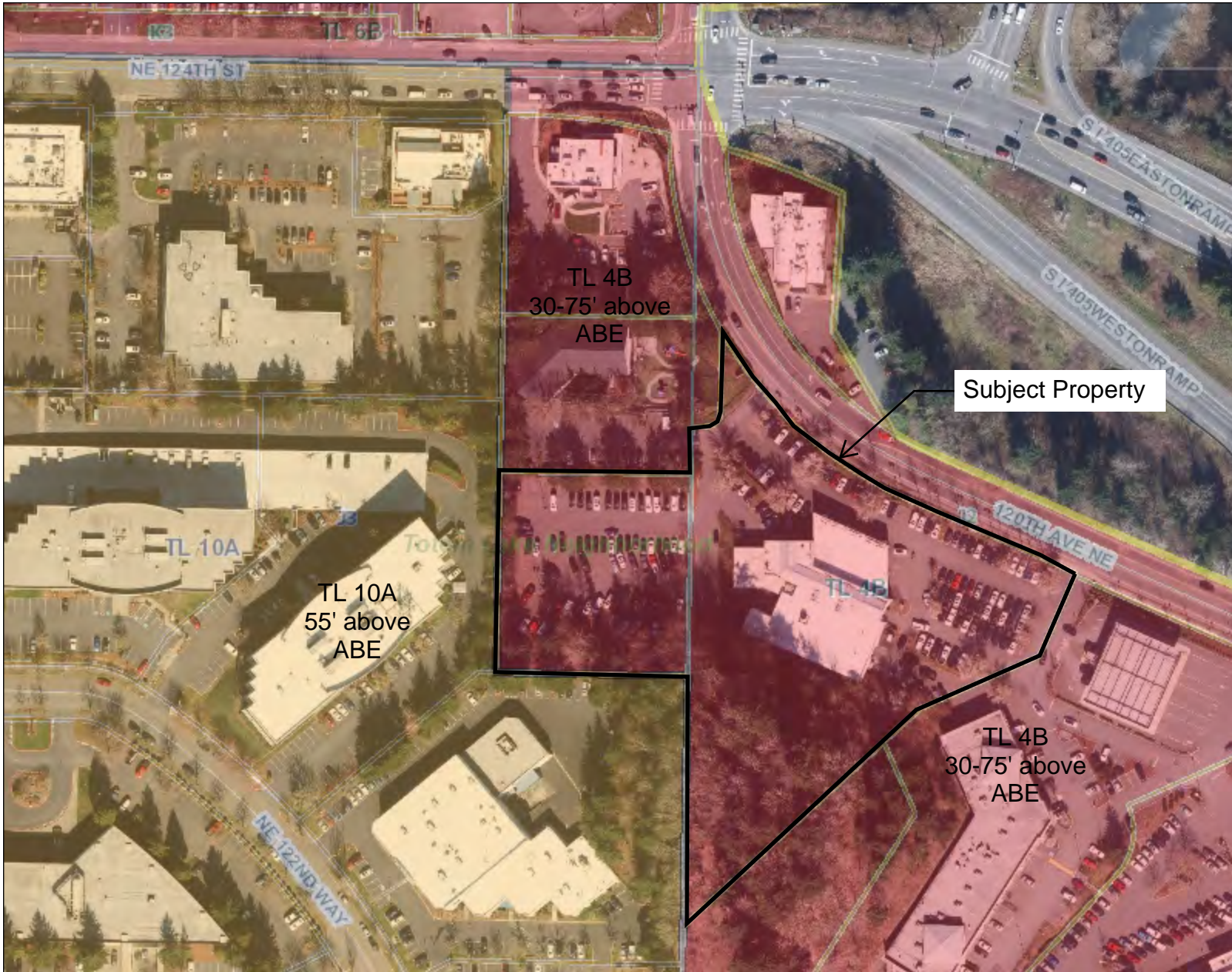


Carlos Castaneda, Design Review Board Chair

Date: 03/27/2023



Vicinity Map



- ### Legend
- City Limits
 - Grid
 - QQ Grid
 - Cross Kirkland Corridor
 - Regional Rail Corridor
 - Streets
 - Parcels
 - Lakes
 - Parks
 - Schools
 - Overlay Zones**
 - (EQ)
 - (HL)
 - (HP)
 - Planned Unit Development
 - Design District
 - City Zoning**
 - Commercial
 - Industrial
 - Transit Oriented Development
 - Office
 - High Density Residential
 - Medium Density Residential
 - Low Density Residential
 - Institutions
 - Park/Open Space

1: 1,920

Notes



NAD_1983_StatePlane_Washington_North_FIPS_4601_Feet

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City of Kirkland
Planning and Building Department
123 5th Avenue, Kirkland, WA 98033
425-587-3600 ~ www.kirklandwa.gov

Development Standards List

File: Polaris at Totem Lake, DRV22-00569

ZONING CODE STANDARDS

85.45 Liability. Prior to issuance of any development permit, the applicant shall enter into an agreement with the City, which runs with the property, in a form acceptable to the City Attorney, indemnifying the City for any damage resulting from development activity on the subject property which is related to the physical condition of the property. The applicant shall record this agreement with the King County Recorder's Office and provide evidence to the City that the agreement has been recorded.

85.50 Notice of Geologic Hazard. Prior to final inspection of any development permit, the applicant shall record (unless legally prohibited from doing so), on the title of the property, a notice stating that the property is potentially located in a geologically hazardous area. This notice will inform future owners that, at the time of the permit's issuance, the property was potentially located in a geologically hazardous area.

95.51.2.a Required Landscaping. All required landscaping shall be maintained throughout the life of the development. The applicant shall submit an agreement to the city to be recorded with King County which will perpetually maintain required landscaping. Prior to issuance of a certificate of occupancy, the proponent shall provide a final as-built landscape plan and an agreement to maintain and replace all landscaping that is required by the City.

95.45 Parking Area Landscape Buffers. Applicant shall buffer all parking areas and driveways from the right-of-way and from adjacent property with a 5-foot wide strip as provided in this section. If located in a design district a low hedge or masonry or concrete wall may be approved as an alternative through design review.

95.50 Tree Installation Standards. Installation of supplemental trees to be planted shall conform to Kirkland Zoning Code Section **95.50**.

95.52 Prohibited Vegetation. Plants listed as prohibited in the Kirkland Plant List shall not be planted in the City. These plants include Himalayan and Evergreen Blackberry, English Holly, Fragrant water lily; Bindweed or Morning Glory, Bird Cherry, English and Atlantic Ivy; Herb Robert; Bohemian, Giant, Himalayan, and Japanese Knotweed; Old man's beard, Poison hemlock, Reed canary grass, Scotch broom, Spurge laurel, Yellow archangel, and Yellow flag iris. Other plants, while not prohibited, are discouraged, including Butterfly bush, Black Locust, European Mountain Ash, Tree-of-Heaven, Common Hawthorn, and English laurel.

100.25 Sign Permits. Separate sign permit(s) are required. In JBD and CBD cabinet signs are prohibited.

105.10.2 Pavement Setbacks. The paved surface in an access easement or tract shall be set back at least 5 feet from any adjacent property which does not receive access from that easement or tract. An access easement or tract that has a paved area greater than 10 feet in width must be screened from any adjacent property that does not receive access from it. Screening standards are outlined in this section.

105.18 Pedestrian Walkways. All uses, except single family dwelling units and duplex structures, must provide pedestrian walkways designed to minimize walking distances from the

building entrance to the right of way and adjacent transit facilities, pedestrian connections to adjacent properties, between primary entrances of all uses on the subject property, through parking lots and parking garages to building entrances. Easements may be required. In design districts through block pathways or other pedestrian improvements may be required. See also Plates 34 in Chapter 180.

105.32 Bicycle Parking. All uses, except single family dwelling units and duplex structures with 6 or more vehicle parking spaces must provide covered bicycle parking within 50 feet of an entrance to the building at a ratio of one bicycle space for each twelve motor vehicle parking spaces. Check with Planner to determine the number of bike racks required and location.

105.18 Entrance Walkways. All uses, except single family dwellings and duplex structures, must provide pedestrian walkways between the principal entrances to all businesses, uses, and/or buildings on the subject property.

105.18 Overhead Weather Protection. All uses, except single family dwellings, multifamily, and industrial uses, must provide overhead weather protection along any portion of the building, which is adjacent to a pedestrian walkway.

105.18.2 Walkway Standards. Pedestrian walkways must be at least 5' wide; must be distinguishable from traffic lanes by pavement texture or elevation; must have adequate lighting for security and safety. Lights must be non-glare and mounted no more than 20' above the ground.

105.18.2 Overhead Weather Protection Standards. Overhead weather protection must be provided along any portion of the building adjacent to a pedestrian walkway or sidewalk; over the primary exterior entrance to all buildings. May be composed of awnings, marquees, canopies or building overhangs; must cover at least 5' of the width of the adjacent walkway; and must be at least 8 feet above the ground immediately below it. In design districts, translucent awnings may not be backlit; see section for the percent of property frontage or building facade.

105.19 Public Pedestrian Walkways. The height of solid (blocking visibility) fences along pedestrian pathways that are not directly adjacent a public or private street right-of-way shall be limited to 42 inches unless otherwise approved by the Planning or Public Works Directors. All new building structures shall be setback a minimum of five feet from any pedestrian access right-of-way, tract, or easement that is not directly adjacent a public or private street right-of-way. If in a design district, see section and Plate 34 for through block pathways standards.

105.58 Parking Lot Locations in Design Districts. See section for standards unique to each district.

105.65 Compact Parking Stalls. Up to 50% of the number of parking spaces may be designated for compact cars.

105.60.2 Parking Area Driveways. Driveways which are not driving aisles within a parking area shall be a minimum width of 20 feet.

105.60.3 Wheelstops. Parking areas must be constructed so that car wheels are kept at least 2' from pedestrian and landscape areas.

105.60.4 Parking Lot Walkways. All parking lots which contain more than 25 stalls must include pedestrian walkways through the parking lot to the main building entrance or a central location. Lots with more than 25,000 sq. ft. of paved area must provide pedestrian routes for every 3 aisles to the main entrance.

105.77 Parking Area Curbing. All parking areas and driveways, for uses other than detached dwelling units must be surrounded by a 6" high vertical concrete curb.

105.96 Drive Through Facilities. See section for design criteria for approving drive through facilities.

110.52 Sidewalks and Public Improvements in Design Districts. See section, Plate 34 and public works approved plans manual for sidewalk standards and decorative lighting design applicable to design districts.

110.60.5 Street Trees. All trees planted in the right-of-way must be approved as to species

by the City. All trees must be two inches in diameter at the time of planting as measured using the standards of the American Association of Nurserymen with a canopy that starts at least six feet above finished grade and does not obstruct any adjoining sidewalks or driving lanes.

115.25 Work Hours. It is a violation of this Code to engage in any development activity or to operate any heavy equipment before 7:00 am. or after 8:00 pm Monday through Friday, or before 9:00 am or after 6:00 pm Saturday. No development activity or use of heavy equipment may occur on Sundays or on the following holidays: New Year's Day, Memorial Day, Independence Day, Labor Day, Thanksgiving, and Christmas Day. The applicant will be required to comply with these regulations and any violation of this section will result in enforcement action, unless written permission is obtained from the Planning official.

115.45 Garbage and Recycling Placement and Screening. For uses other than detached dwelling units, duplexes, moorage facilities, parks, and construction sites, all garbage receptacles and dumpsters must be setback from property lines, located outside landscape buffers, and screened from view from the street, adjacent properties and pedestrian walkways or parks by a solid sight-obscuring enclosure.

115.47 Service Bay Locations. All uses, except single family dwellings and multifamily structures, must locate service bays away from pedestrian areas. If not feasible must screen from view.

115.75.2 Fill Material. All materials used as fill must be non-dissolving and non-decomposing. Fill material must not contain organic or inorganic material that would be detrimental to the water quality, or existing habitat, or create any other significant adverse impacts to the environment.

115.90 Calculating Lot Coverage. The total area of all structures and pavement and any other impervious surface on the subject property is limited to a maximum percentage of total lot area. See the Use Zone charts for maximum lot coverage percentages allowed. Section 115.90 lists exceptions to total lot coverage calculations See Section 115.90 for a more detailed explanation of these exceptions.

115.95 Noise Standards. The City of Kirkland adopts by reference the Maximum Environmental Noise Levels established pursuant to the Noise Control Act of 1974, RCW 70.107. See Chapter 173-60 WAC. Any noise, which injures, endangers the comfort, repose, health or safety of persons, or in any way renders persons insecure in life, or in the use of property is a violation of this Code.

115.115 Required Setback Yards. This section establishes what structures, improvements and activities may be within required setback yards as established for each use in each zone.

115.115.3.g Rockeries and Retaining Walls. Rockeries and retaining walls are limited to a maximum height of four feet in a required yard unless certain modification criteria in this section are met. The combined height of fences and retaining walls within five feet of each other in a required yard is limited to a maximum height of 6 feet, unless certain modification criteria in this section are met.

115.115.3.p HVAC and Similar Equipment: These may be placed no closer than five feet to a front, side, or rear property line, and may only be located in a required front yard for single-family residential uses pursuant to subsection (3)(p)(2) of this section; provided, that HVAC equipment may be located in a storage shed approved pursuant to subsection (3)(m) of this section or a garage approved pursuant to subsection (3)(o)(2) of this section. All HVAC equipment shall be baffled, shielded, enclosed, or placed on the property in a manner that will ensure compliance with the noise provisions of KZC 115.95.

115.120 Rooftop Appurtenance Screening. New or replacement appurtenances on existing buildings shall be surrounded by a solid screening enclosure equal in height to the appurtenance. New construction shall screen rooftop appurtenances by incorporating them in to the roof form.

115.135 Sight Distance at Intersection. Areas around all intersections, including the entrance of driveways onto streets, must be kept clear of sight obstruction as described in this section.

Prior to issuance of a grading or building permit:

85.25.2 Geotechnical Acknowledgement. Written acknowledgment from the licensed in Washington State geotechnical engineer or licensed in Washington State engineering geologist who prepared the report required by KZC 85.15 that they have reviewed the project plans and that they conform to their recommendations.

85.45 Liability. The applicant shall enter into an agreement with the City, which runs with the property, in a form acceptable to the City Attorney, indemnifying the City for any damage resulting from development activity on the subject property which is related to the physical condition of the property.

95.30.4 Tree Protection Zone (TPZ). A description and location of tree protection measures during construction for trees to be retained must be shown on demolition and grading plans, including the TPZ distance specified in feet from the face of each tree trunk.

95.30.3 Tree Retention Plan Requirements. All required Tree Retention Plan components must be submitted pursuant to KZC 95.30(3), including the identification of Landmark Trees and Groves, planned over-excavation and impacts to retained trees, and individual tree viability ratings pursuant to KZC 95.30(3)(c).

95.32 Tree Protection. Prior to development activity or initiating tree removal on the site, vegetated areas and individual trees to be preserved shall be protected from potentially damaging activities. Protection measures for trees to be retained shall include (1) placing no construction material or equipment within the protected area of any tree to be retained; (2) providing a visible temporary protective chain link fence at least 6 feet in height around the protected area of retained trees or groups of trees until the Planning Official authorizes their removal; (3) installing visible signs spaced no further apart than 15 feet along the protective fence stating "Tree Protection Area, Entrance Prohibited" with the City code enforcement phone number; (4) displaying site plans showing approved tree retention/protection in plain view with general contractor or other responsible party's phone number; (5) prohibiting excavation or compaction of earth or other damaging activities within the barriers unless approved by the Planning Official and supervised by a qualified professional; and (6) ensuring that approved landscaping in a protected zone shall be done with light machinery or by hand.

27.06.030 Park Impact Fees. New residential units are required to pay park impact fees prior to issuance of a building permit. Please see KMC 27.06 for the current rate. Exemptions and/or credits may apply pursuant to KMC 27.06.050 and KMC 27.06.060. If a property contains an existing unit to be removed, a "credit" for that unit shall apply to the first building permit of the subdivision.

Prior to occupancy:

85.50 Notice of Geologic Hazard. Prior to final inspection of any development permit, the applicant shall record (unless legally prohibited from doing so), on the title of the property, a notice stating that the property is potentially located in a geologically hazardous area. This notice will inform future owners that, at the time of the permit's issuance, the property was potentially located in a geologically hazardous area.

95.51.2.a Required Landscaping. All required landscaping shall be maintained throughout the life of the development. The applicant shall submit an agreement to the city to be recorded with King County which will perpetually maintain required landscaping. Prior to issuance of a certificate of occupancy, the proponent shall provide a final as-built landscape plan and an agreement to maintain and replace all landscaping that is required by the City

110.60.5 Landscape Maintenance Agreement. The owner of the subject property shall sign a landscape maintenance agreement, in a form acceptable to the City Attorney, to run with the subject property to maintain landscaping within the landscape strip and landscape island portions of the right-of-way (see Attachment @). It is a violation to pave or cover the landscape

strip with impervious material or to park motor vehicles on this strip.



DEVELOPMENT STANDARDS DRV22-00569

PUBLIC WORKS DEPARTMENT

PUBLIC WORKS CONDITIONS

Permit #: DRV22-00569
Project Name: 467 unit (GMC location)
Project Address: 12335 120th Ave NE
Date: 8/8/2022

Public Works Staff Contacts

Ryan Schauble, Senior Development Engineer
Phone: 425-587-3842 / E-mail: rschauble@kirklandwa.gov

General Conditions:

1. All public improvements associated with this project including street and utility improvements, must meet the City of Kirkland Public Works Pre-Approved Plans and Policies Manual. A Public Works Pre-Approved Plans and Policies manual can be purchased from the Public Works Department, or it may be retrieved from the Public Works Department's page at the City of Kirkland's web site.
 2. This project will be subject to Public Works Permit and Connection Fees. It is the applicant's responsibility to contact the Public Works Department by phone or in person to determine the fees. The applicant should anticipate the following fees:
 - o Water and Surface Water Connection Fees *
 - o Contact Northshore Utility District for Sewer Connection Fees
 - o Water Meter Fee *
 - o Right-of-way Fee
 - o Review and Inspection Fee
 - o Building Permits associated with this proposed project will be subject to the traffic, park, and school impact fees per Chapter 27 of the Kirkland Municipal Code. The impact fees shall be paid prior to issuance of the Building Permit(s). Any existing buildings within this project which are demolished will receive a Traffic Impact Fee credit, Park Impact Fee Credit and School Impact Fee Credit. This credit will be applied to the first Building Permits that are applied for within the project. The credit amount for each demolished building will be equal to the most currently adopted Fee schedule.
- * Fee to be paid with the issuance of a Building Permit.
3. All street and utility improvements shall be permitted by obtaining a Land Surface Modification (LSM) Permit, including the required LSM Checklist.
 4. Review of Building Permits within a detached multi-family project: An Enhance Grading Permit (ELSM) is required if deemed appropriate by the Planning and Building Department. One Building Permit must be submitted prior to issuance of the ELSM. Multiple Building Permits may be reviewed concurrently with the LSM review.
 5. Prior to submittal of a Building or Zoning Permit, the applicant must apply for a Concurrency Test Notice. Contact Thang Nguyen, Transportation Engineer, at 425-587-3869 for more information. A separate Concurrency Permit will be created.
 6. After concurrency has passed, the project will receive a concurrency test notice that allows the applicant to proceed with all development permits. A "Certificate of Concurrency" is established with a development or building permit. It will read as

DRV22-00569

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follows: CERTIFICATE OF CONCURRENCY: This project has been reviewed and approved for water, sewer, and traffic concurrency. Any water and sewer mitigating conditions are listed within the conditions below. Any traffic mitigating conditions will be found in an attached memorandum from the Public Works Traffic Engineering Analyst to the Planning Department Project Planner. Upon issuance of this permit, this project shall have a valid Certificate of Concurrency and concurrency vesting until the permit expires. This condition shall constitute issuance of a Certificate of Concurrency pursuant to chapter 25.12 of the Kirkland Municipal Code.

7. The scope of the transportation impact analysis shall be determined after the project passes transportation concurrency. An analysis shall be required to determine the need to widen the roadway to extend the existing two-way left-turn lane along the rest of the project frontage.

8. All civil engineering plans which are submitted in conjunction with a building, grading, or right-of-way permit must conform to the Public Works Policy G-7, Engineering Plan Requirements. This policy is contained in the Public Works Pre-Approved Plans and Policies manual.

9. All street improvements and underground utility improvements (storm, sewer, and water) must be designed by a Washington State Licensed Engineer; all drawings shall bear the engineers stamp.

10. All plans submitted in conjunction with a building, grading or right-of-way permit must have elevations which are based on the King County datum only (NAVD 88).

11. A completeness check meeting is required prior to submittal of any Building Permit applications.

12. Prior to issuance of any commercial or multifamily Building Permit, the applicant shall provide a plan for garbage, recycling and composting storage and pickup. The plan shall conform to Policy G-9 in the Public Works Pre-approved Plans and be approved by Waste Management and the City. Important feature is to provide enough storage area for recycling and composting; and being able to pick up containers without storing in the ROW overnight. Submit the plan with a cover letter to explain how Policy G-9 requirements will be met. Please contact John MacGillivray, 425.587.3804, if you have questions.

Sanitary Sewer Conditions:

1. Northshore Utility District (NUD) approval required for sanitary. A letter of sewer availability is required. Contact NUD at 425-398-4400.

Water System Conditions:

1. The existing water main in the right-of-way is adequate to serve the domestic needs of the Project. Improvements to the water system will be required as previously discussed, including looping around the building. Provide a 15' public water main easement.

2. See Fire Department conditions for fire flow requirements.

3. In mixed-use projects each use shall have a separate water meter, i.e., the retail use shall have a separate water meter from residential use. If the project requires irrigation a separate water service shall be provided.

4. The existing water service(s) shall be abandoned at the main, unless approved otherwise by Public Works. The property is currently served by the following:

- Existing 2" Domestic meter
- Existing 1 ½" Irrigation meter

Surface Water Conditions:

1. Provide temporary and permanent storm water control in accordance with the 2021 King County Surface Water Design Manual (KCSWDM) and the City of Kirkland Addendum (Policy D-10).

2. To determine the drainage review level required, the target impervious surface area is the maximum allowable lot coverage area for the project, plus any offsite improved impervious areas. See Policies D-2 and D-3 in the Public Works Pre-Approved Plans for drainage review information, or contact Kirkland Surface Water staff at (425) 587-3800 for assistance. The Kirkland Drainage Review Flow Chart is a helpful tool to determine a project's drainage review level. Drainage review levels are summarized below:

- Full Drainage Review
 - o Any non-single-family residential project that creates more than 2,000 sf of new and/or replaced impervious surface, or greater than 7,000 sf of land disturbing activity will trigger a Full Drainage Review.
 - o Single family residential projects that propose improvements greater than the Simplified thresholds explained above will be subject to a Full Drainage Review.

3. Attention to Civil Plan Designers: Kirkland Zoning Code Update and Surface Water Design Policy Update -- Public Works Policy D-10 (City's Addendum to the 2016 KC-SWDM) was updated in July 2019. Follow the new guidelines in D-10 regarding flow control analysis. Effective on July 12, 2019, the City updated KZC Chapter 115.90 – Calculating Lot Coverage. Background: The regulation update allowed conventional (sand set) pavers to be counted as a "Partially Exempt Material", allowed to received 50 percent exemption for the area they cover, and up to 10 percent of the total lot size. Conventional pavers do not have to meet surface water mitigation specifications (e.g. not designed as LID BMP pervious pavers per Public Works Pre-Approved Plan CK-L-09). As a result, lots are allowed 10 percent more runoff generating surface area, and thus have to provide flow control accordingly.

For calculating impervious coverage for proposed residential and commercial development must be estimated for each specific proposal. Impervious coverage for frontage layouts – streets, sidewalks, trails, etc – shall be taken from the layouts of the proposal. House/driveway or building coverage shall be as follows:

- For residential development, the assumed impervious coverage shall be the maximum impervious coverage permitting by the Kirkland Zoning Code (KZC) plus an additional 10%.
- For commercial or multi-family development, the impervious coverage shall either:
 - o Assume the maximum impervious coverage permitted by the KZC plus an additional 10% OR
 - o Estimate impervious coverage from layouts of the proposal. If estimated from the layouts of the proposal, the impervious coverage shall include calculations of all impervious surfaces, including eaves. This option may require a Reduced Impervious Surface Limit to be recorded on the property.

4. This project is in a Level 2 Flow Control Area and is required to comply with core drainage requirements in the KCSWDM. Historic (forested) conditions shall be used as the pre-developed modeling condition for design of the stormwater detention system.

5. The project may qualify for an exception to detention if the target surfaces will generate no more than a 0.15 cfs increase in the historic (forested) conditions 100-year peak flow. The 15-minute time step must be used to perform the flow control analysis. Do not use the 1-hour time step. Approved hydrologic modeling programs are MGS Flood and WWHM 2012.

6. Evaluate the feasibility and applicability of dispersion, infiltration, and other stormwater Low Impact Development (LID) Best Management Practices (BMPs) per the KCSWDM. If feasible, stormwater LID BMPs are required to the maximum extent feasible. If LID BMPs are infeasible, pervious pavement cannot be used to reduce overall impervious lot coverage. The Private Maintenance Agreement will be recorded on all projects that construct a stormwater LID BMP or facility, per Policy D-7.

7. Soil information may be necessary for designing LID BMPs per the KCSWDM, and there are other reasons a soil report is necessary for a project (e.g., steep slopes, sensitive areas, etc.). Refer to Policy D-8 for details.

8. Special inspections may be required for LID BMPs on this project. Provide documentation of inspections by a licensed geotechnical professional that the BMP will function as designed.

9. If the project will create or replace more than 5,000 square feet of pollution generating impervious surface (PGIS), provide water quality treatment in accordance with the KCSWDM; the enhanced treatment level is required for this multi-family

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residential project.

10. Soil Amendment per Pre-Approved Plan E.12 is required for all landscaped areas.

11. If working within an existing ditch, the applicant is hereby given notice that the Army Corps of Engineers (COE) has asserted jurisdiction over upland ditches draining to streams. Either an existing Nationwide COE permit or an Individual COE permit may be necessary for work within ditches, depending on the project activities.

Applicants should obtain the applicable COE permit; information about COE permits can be found at: U.S. Army Corps of Engineers, Seattle District Regulatory Branch
<http://www.usace.army.mil/Missions/CivilWorks/RegulatoryProgramandPermits.aspx>

Specific questions can be directed to: Seattle District, Corps of Engineers, Regulatory Branch, CENWS-OD-RG, Post Office Box 3755, Seattle, WA 98124-3755, Phone: (206) 764-3495

12. Construction Stormwater Pollution Prevention Plan (CSWPPP):

- All proposed projects that will conduct construction activities onsite, or offsite must provide stormwater pollution prevention and spill controls to prevent, reduce, or eliminate the discharge of pollutants (including sediment) to onsite or adjacent stormwater systems or watercourses.
- Refer to Core Requirement No. 5 in the KCSWDM and Policy D-12.
- Provide an erosion control report and plan with the Building or Land Surface Modification Permit application. The plan shall be in accordance with the KCSWDM.
- Construction drainage control shall be maintained by the developer and will be subject to periodic inspections. During the period from May 1 and September 30, all denuded soils must be covered within 7 days; between October 1 and April 30, all denuded soils must be covered within 12 hours. Additional erosion control measures may be required based on site and weather conditions. Exposed soils shall be stabilized at the end of the workday prior to a weekend, holiday, or predicted rain event.

13. If the project site is one acre or greater, the following conditions apply:

- The applicant is responsible to apply for a Construction Stormwater General Permit from Washington State Department of Ecology. Provide the City with a copy of the Notice of Intent for the permit. Permit Information can be found at the following website: <http://www.ecy.wa.gov/programs/wq/stormwater/construction/>
 - o Among other requirements, this permit requires the applicant to prepare a Storm Water Pollution Prevention Plan (SWPPP) and identify a Certified Erosion and Sediment Control Lead (CESCL) prior to the start of construction. The CESCL shall attend the City of Kirkland PW Dept. pre-construction meeting with a completed SWPPP.
- Turbidity monitoring by the developer/contractor is required for any surface water leaving the site.
- A Stormwater Pollution Prevention and Spill (SWPPS) Plan must be kept on site during all phases of construction and shall address construction-related pollution generating activities. Follow the guidelines in the Ecology Pollution Prevention Manual for plan preparation.

Street and Pedestrian Improvement Conditions:

1. The subject property abuts 120th Ave NE. This street is an Arterial type street. Zoning Code sections 110.10 and 110.25 require the applicant to make half-street improvements in rights-of-way abutting the subject property. Section 110.30-110.50 establishes that this street is part of the Totem Lake Business District Circulator with a designation for a Major Pedestrian Sidewalk (MPS) and shall be improved with the following:

- A. Dedicate sufficient right-of-way (ROW) abutting the property to install half-street improvements; a minimum of 5 ft of sidewalk must be in a dedicated as right-of-way with the remainder in a Public Pedestrian Easement.
- B. All tapers needed for the installation of the required improvements shall start outside of the property frontage.
- C. Maintain existing turn and drive lane widths and install an 8-ft wide buffered bike lane (3' buffer plus 5' bike).
- D. Install new Type-A concrete curb and gutter along the entire frontage.
- E. Install a 6 ft wide planter with street trees 30 ft on-center.
- F. Install an 8-ft wide sidewalk and pedestrian Lighting at 60 feet on-center.
- G. Coordinate additional improvements with Kirkland Planning Department for MPS design:

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- a. Pedestrian benches
 - b. Enhanced lighting around building entrances
 - c. Enhances lighting near transit stops
 - d. Public Art
 - H. Install new street lights to accommodate new curb alignments, verify spacing with PSE to ensure street lighting standards are met.
 - I. There are some existing surface mounted utilities that will need to be relocated.
 - J. Coordinate improvements with planned Kirkland street projects, if any.
2. Onsite parking shall meet following conditions:
- No parking within 60 feet of the driveway throat measured from the face of curb.
 - All commercial loading area must be provided onsite.
 - Moving truck loading area must be provided onsite. The truck loading area must be shown on the site plan and be located convenient to both buildings else each building shall have its own truck loading area. Vehicles are not allowed to back out into the street; therefore, trucks must be able to turn around and head out of the project site.
 - Parking for commercial must be accessible during business hours.
 - Parking for residential visitors must not be gated and available at all times.
 - If shared parking is proposed, a parking management plan will be required.
 - If less parking is provided than required, then a parking modification and a transportation demand management plan (TDM) will be required. The scope of the parking analysis for the parking modification shall be discussed and approved by the city transportation engineer. The parking modification can be requested through the planning department.
3. Meet the requirements of the Kirkland Driveway Policy R-4. Spacing Table from R-4, for reference:
4. The driveways and garage entrances/gates must be 24 feet wide at the minimum. The goal is to maintain a narrow driveway crossing the public crosswalk along 120th Avenue NE. All drive aisles into the parking area and within the parking garage must be 24 feet wide. Driveway variance must be requested separately with a variance application.
5. Meet the requirements of the Kirkland Intersection Sight Distance Policy R.13. All street and driveway intersections shall not have any visual obstructions within the sight distance triangle.
6. Transportation impact fee shall be assessed for the development. Credit against the impact fee shall be given for the existing use (car dealership).
7. When three or more utility trench crossings occur within 150 lineal ft. of street length or where utility trenches parallel the street centerline, the street shall be overlaid with new asphalt or the existing asphalt shall be removed and replaced per the City of Kirkland Street Asphalt Overlay Policy R-7.
- Existing streets with 4-inches or more of existing asphalt shall receive a 2-inch (minimum thickness) asphalt overlay. Grinding of the existing asphalt to blend in the overlay will be required along all match lines.
 - Existing streets with 3-inches or less of existing asphalt shall have the existing asphalt removed and replaced with an asphalt thickness equal or greater than the existing asphalt provided however that no asphalt shall be less than 2-inches thick and the subgrade shall be compacted to 95% density.
8. It shall be the responsibility of the applicant to relocate any above-ground or below-ground utilities which conflict with the project, associated street, or utility improvements.
9. Underground all new and existing on-site utility lines and overhead transmission lines. Underground any new off-site transmission lines.
10. New LED street lights may be required per Puget Sound Energy (PSE) design and Public Works approval. Contact PSE to perform lighting analysis. If new lighting or upgrades are necessary, design plans must be submitted for review prior to issuance of an LSM or building permit. Contact:

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Kayla Neckorcuk – Territory Engineer for King County, Intolight, PUGET SOUND ENERGY
Tel 425-577-2392 | Fax 425-462-3149
Email Kayla.Neckorcuk@pse.com | Website: www.intolight.com

11. A striping plan for the street must be submitted with the building or grading permit.



POLARIS AT TOTEM LAKE

12335 120TH AVENUE NE, KIRKLAND, WASHINGTON 98034
DESIGN RESPONSE CONFERENCE - ROUND 2
11.07.2022

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CONCEPTUAL DESIGN CONFERENCE - SUMMARY

Conceptual Design Conference(s):
Design Response Conference:

May 2nd, 2022 & June 6th, 2022
October 3rd, 2022

A. Scale

- 1.) Increase overall modulation and articulation particularly along 120th Ave NE
- 2.) More movement and breaking up of the parapet
- 3.) Explore variation of window size and style
- 4.) Expand the corner towerer so it functions as an accent and anchor of the building

B. Pedestrian Oriented Elements

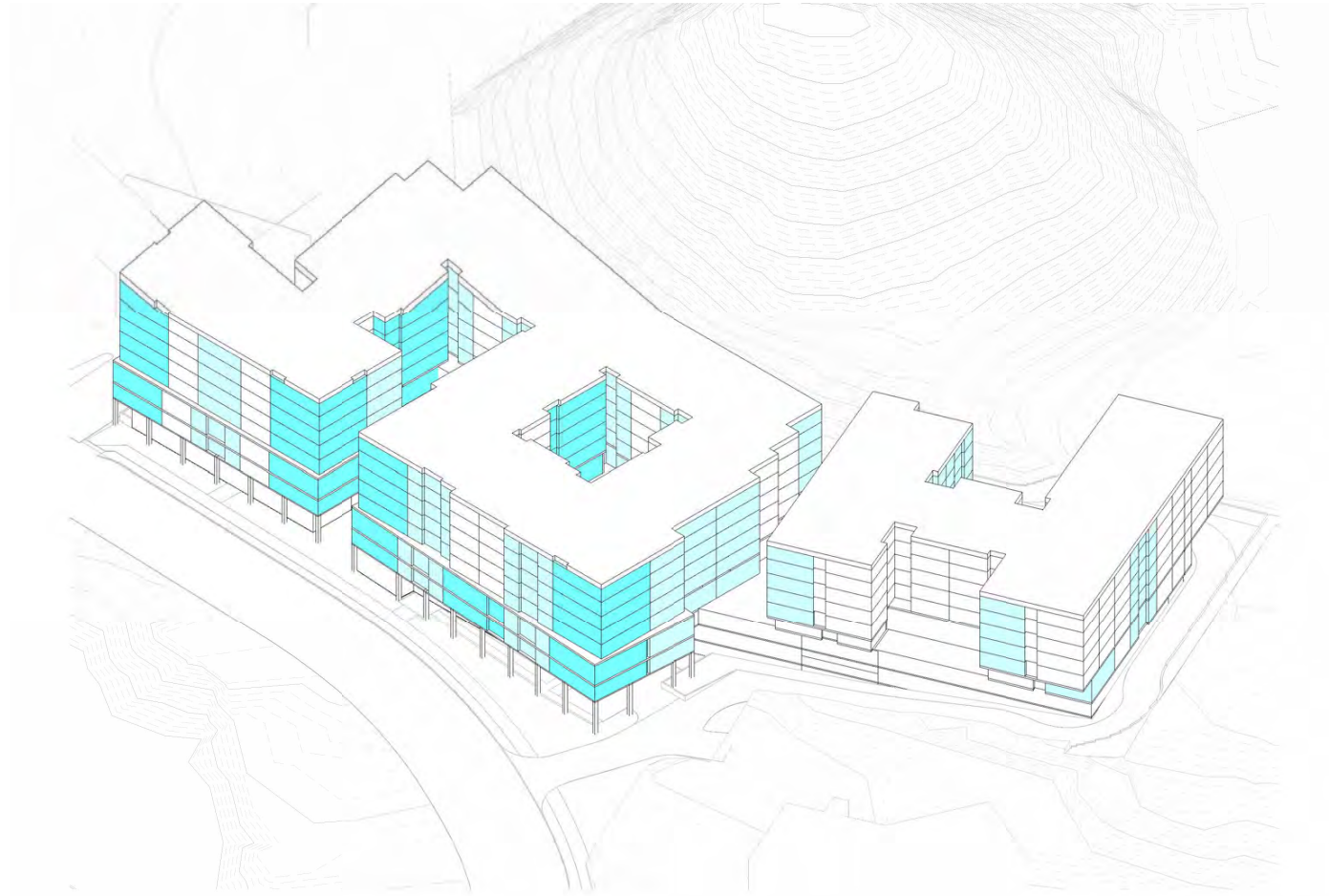
- 1.) Ground floor activation through use of benches, pavers, landscaping
- 2.) Expand the amount of frontage along 120th Ave. NE that engages the public
- 3.) Explore artwork opportunities within enhanced landscaping spaces particularly in and around the parking garage

C. Open Space and Landscaping

- 1.) Consider stair access to the plaza
- 2.) Can the plaza space spill over into the ramp area for a more blended transition from the sidewalk into the plaza space?
- 3.) Look into use of pavers, stamped concrete, benches
- 4.) Provide a more detailed landscape plan (including species) to address privacy, enhancing the public spaces (plaza etc.), ground floor pedestrian engagement, and seasonal plantings
- 5.) Opportunities to explore with the wide landscape areas

D. Other Comments

- 1.) Blank Wall Treatments: Identify any blank walls and how they will be treated.
- 2.) Retaining Wall: Address the views for units facing the retaining wall adjacent to the greenbelt along the west side of the building. Consider stamped paving, more greenery, and wall treatments.



A // SCALE

VERTICAL & HORIZONTAL MODULATION

Board Comment(s) / Summary:

- 1.) Increase overall modulation and articulation particularly along 120th Ave NE

Response:

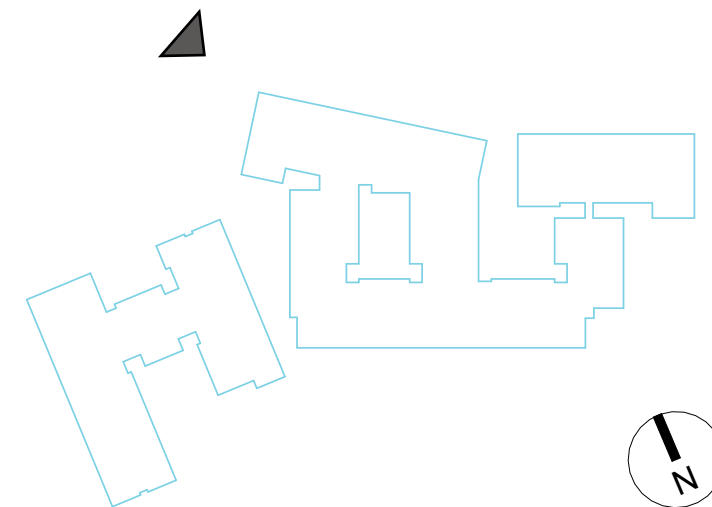
- (A) Material articulation
- (B) Texture variation - changes in fiber cement patterns from smooth to coarse help to add movement in the building facade which can be visible at various directions and times of the day
- (C) Additional modulation at levels 5 and 6 on Tower A - East Wing
- (D) Roof line parapet modulation - See page 7 for more information
- (E) Similar 2/3 Proportional relationship as Tower A - West Wing to create bookend appearance
- (F) Additional fenestration type/style to help further breakdown the scale of the facade and highlight more activated spaces within the building - See page 8 for more information
- (G) Vertical modulation to help break the corner anchor element from the rest of the primary building mass and emphasize verticality along the facade



PREVIOUS



CURRENT



VERTICAL & HORIZONTAL MODULATION

Board Comment(s) / Summary:

- 1.) Increase overall modulation and articulation particularly along 120th Ave NE

Response:

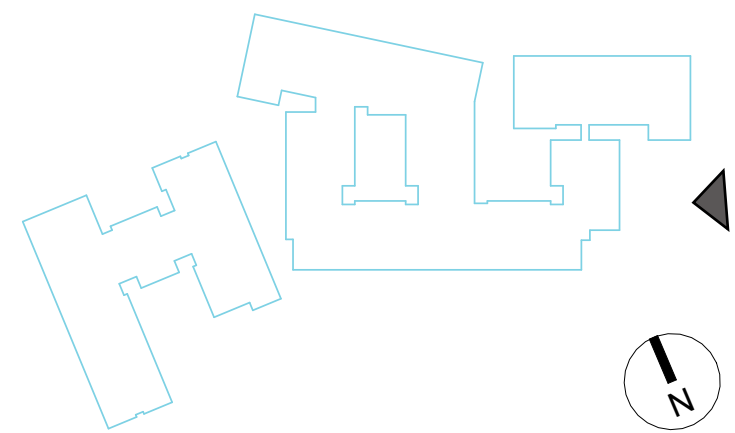
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- (G) Vertical modulation to help break the corner anchor element from the rest of the primary building mass and emphasize verticality along the facade



PREVIOUS



CURRENT



ROOF LINE PARAPET MODULATION

Board Comment(s) / Summary:

2.) More movement and breaking up of the parapet

Response:

We revised the exterior elevations to provide more modulation to the parapets at the roof. Vertical breaks at the roof now occur at each of the recessed wall locations. This move helps to break down the overall building mass into smaller, more vertical elements along the 120th Ave. NE frontage.

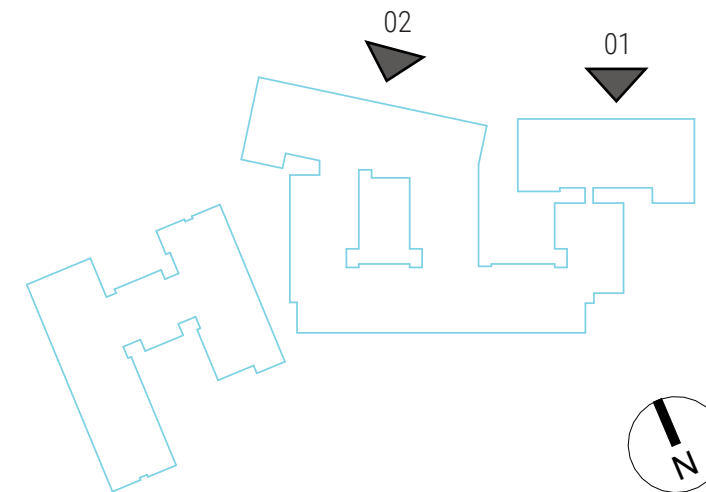
- (A) Increased parapet height by 2'-6" at corner anchor element
- (B) Increased parapet height by 1'-0"
- (C) Added vertical wall modulation
- (D) Omitted roof connection at recessed wall locations



01//PERSPECTIVE



02//PERSPECTIVE



FENESTRATION



01//PERSPECTIVE

Board Comment(s) / Summary:

3.) Explore variation of window size and style

Response:

We have provided an additional window type/style to the project to help further breakdown the overall scale of the building as recommended by the board. By increasing the size of fenestration at certain unit living rooms along 120th Ave. NE. the overall floor to floor relationship is minimized and therefore reduces the vertical scale/percieved height of the building. Larger window openings at these living areas also help to breakdown the horizontal patterning, creating relief points in the building facade. In addition, the board made a comment in regards to the detailing of the scallop infill panels. The comment stated that they are a nice touch but might be more affective if pedestrians at the street level could witness the variation in their faceting. In response, we revised the single slope scallop to provide a folded scallop which still provides minimal surfacing for dirt and debris collection but allows for shape variation which can be seen from below.

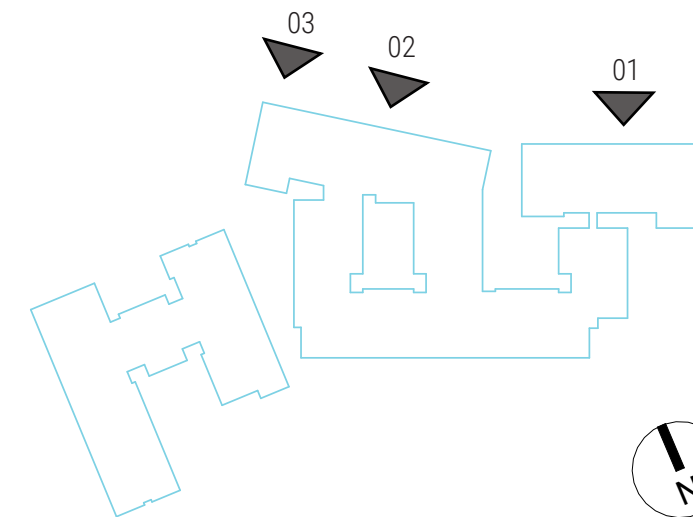
- (A) Added window type / style
- (B) Two window types/ styles are grouped together to provide the appearance of a larger opening
- (C) Deep recessed windows
- (D) Folded scallop infill panels



02//PERSPECTIVE



03//PERSPECTIVE



CORNER ANCHOR

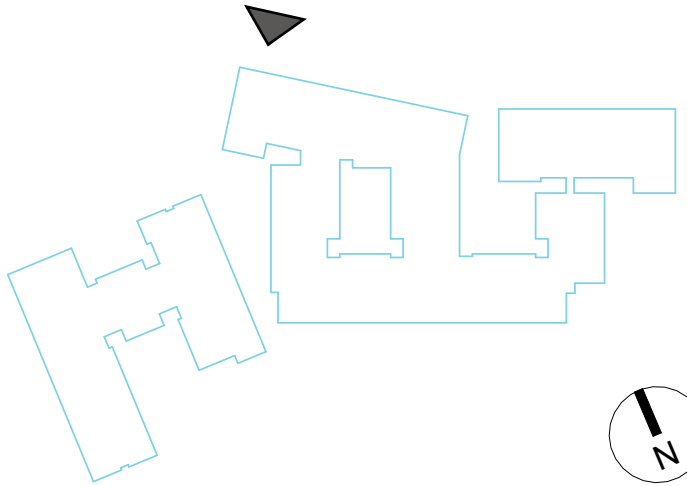
Board Comment(s) / Summary:

- 4.) Expand the corner tower so it functions as an accent and anchor of the building

Response:

We have revised numerous aspects of the corner anchor element from our last DRC meeting on October 3rd. Based on the boards feedback there was just something intangible about this portion of the building that didn't correlate to the rest of project and furthermore lacked definition to further promote an "anchor element". The following points represent the revisions that have taken place:

- (A) We revised the column layout to better align with the perimeter of the corner anchor and help ground this element
- (B) Vertical modulation to help break the corner anchor element from the rest of the primary building mass and emphasize verticality along the facade
- (C) Raised the parapet to help emphasize the priority of this corner anchor element
- (D) Removed the parapet extension to isolate this element and once again promote vertically
- (E) We revised the fenestration on both facades to provide larger openings and therefore create a stronger more visually interesting aspect of the project



B // PEDESTRIAN ORIENTED ELEMENTS

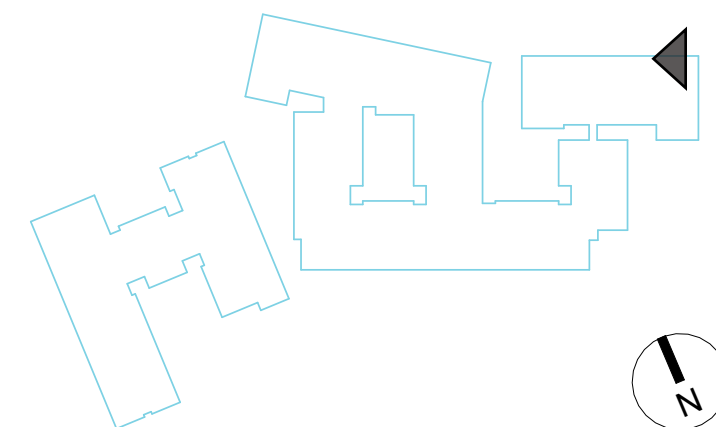
PEDESTRIAN-FRIENDLY BUILDING FRONTS

Board Comment(s) / Summary:

- 1.) Ground floor activation through use of benches, pavers, landscaping.
- 2.) Expand the amount of frontage along 124th Ave NE that engages the public.
- 3.) Explore artwork opportunities within enhanced landscaping spaces particularly in and around the parking garage.

Response:

- (A) By providing more seating nodes along the frontage we have created more opportunities of reprieve, interaction as well as reflection for patrons and the public alike
- (B) The intent is to provide a decorative paving pattern either through the use of linear concrete pavers in multiple gray tones or through saw cut concrete slab on grade
- (C) By reducing the square footage of certain raised planters we have revised the proposed open space layout to provide more area for pedestrian circulation and gathering
- (D) We have enhanced the pedestrian experience along 120th Ave. NE by taking the board's advice and incorporating a "butterfly garden" through the use of specific landscaping techniques
- (E) We have also provided decorative metal panels as a backdrop to the landscaping which doubles as screening for the proposed surface parking lot in this region of the project



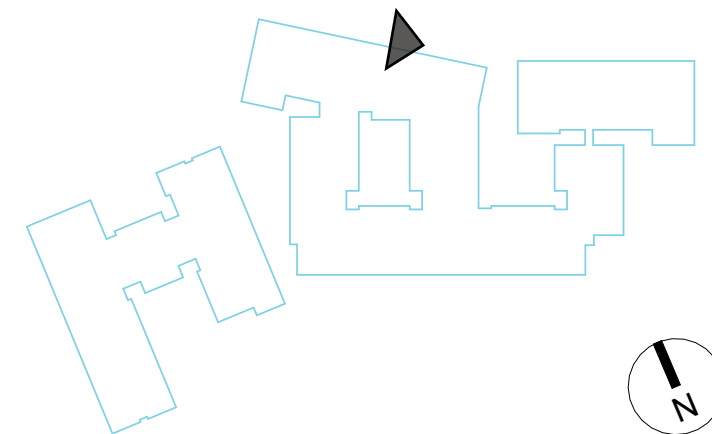
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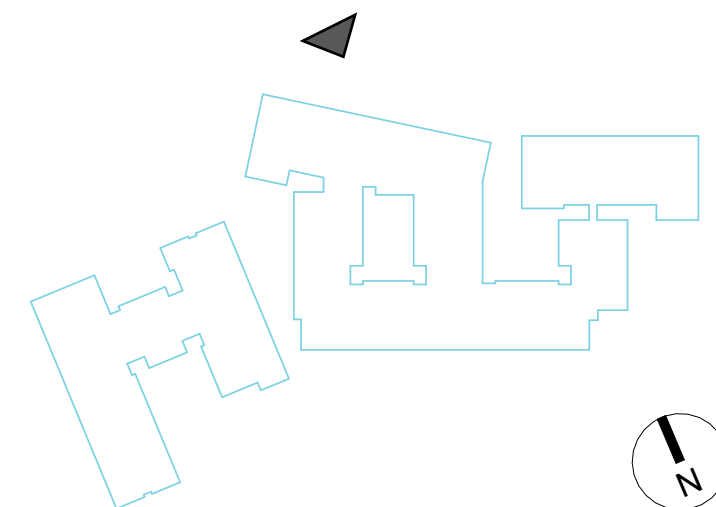
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C // OPEN SPACE AND LANDSCAPING

PEDESTRIAN PLAZAS



Board Comment(s) / Summary:

- 1.) Consider stair access to the plaza
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- 4.) Provide a more detailed landscape plan (including species) to address privacy, enhancing the public spaces (plaza etc.), ground floor pedestrian engagement, and seasonal plantings
- 5.) Opportunities to explore with the wide landscape areas

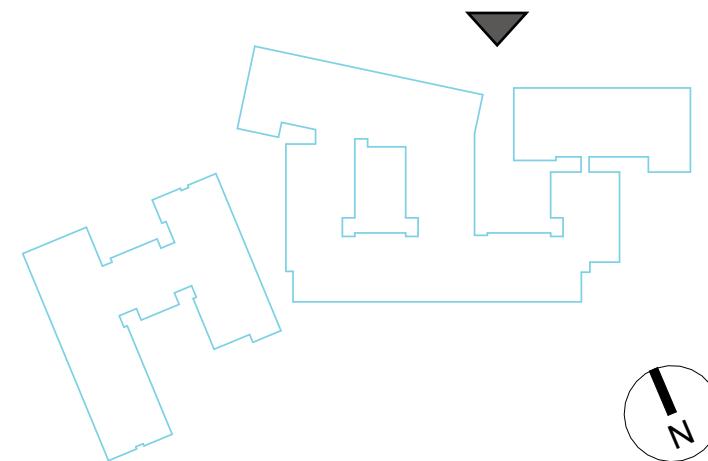
Response:

(A) We have revised the pedestrian circulation/access scheme by providing stairways at both the primary entrance into the plaza as well as the public entrance into the parking garage.

(B) We have revised the westerly "ramp" entrance down into the public plaza by increasing the overall width. By reducing the planter widths, increasing the circulation width and providing seating areas along the low planter wall this area allows gathering space to spill over from the plaza as suggested by the board.

(C) We are proposing concrete pavers or saw cut concrete joinery at these public spaces.

(D) We have enhanced the pedestrian experience along 120th Ave. NE by taking the board's advice and incorporating a "butterfly garden" through the use of specific landscaping techniques. We have also provided decorative metal panels as a backdrop to the landscaping which doubles as screening for the proposed surface parking lot in this region of the project.



PEDESTRIAN PLAZAS



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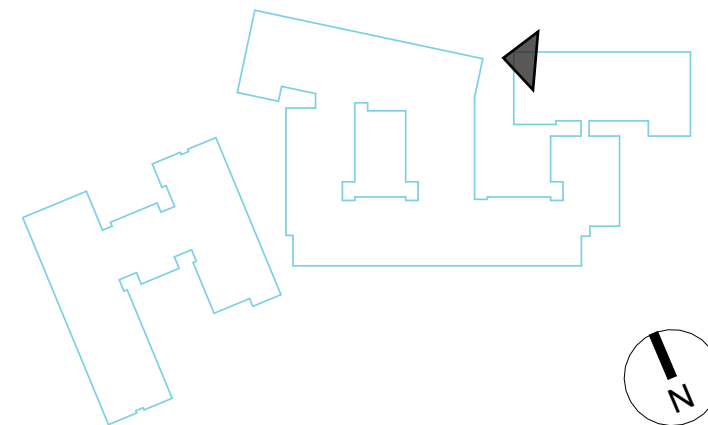
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PEDESTRIAN PLAZAS



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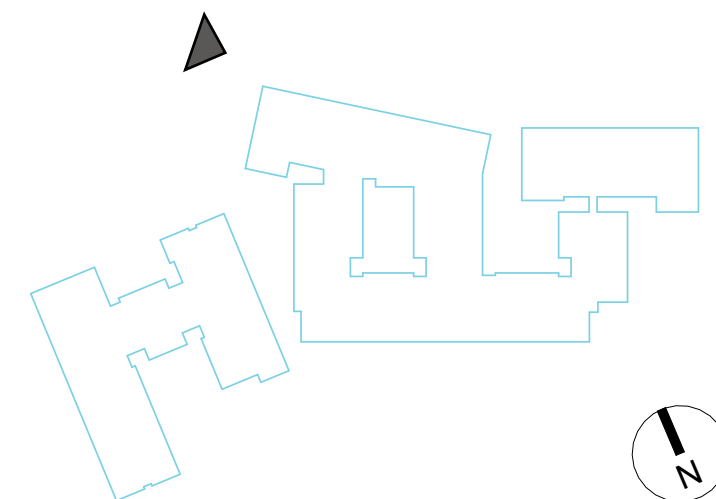
Response:

(A) We have revised the pedestrian circulation/access scheme by providing stairways at both the primary entrance into the plaza as well as the public entrance into the parking garage.

(B) We have revised the westerly "ramp" entrance down into the public plaza by increasing the overall width. By reducing the planter widths, increasing the circulation width and providing seating areas along the low planter wall this area allows gathering space to spill over from the plaza as suggested by the board.

(C) We are proposing concrete pavers or saw cut concrete joinery at these public spaces.

(D) We have enhanced the pedestrian experience along 120th Ave. NE by taking the board's advice and incorporating a "butterfly garden" through the use of specific landscaping techniques. We have also provided decorative metal panels as a backdrop to the landscaping which doubles as screening for the proposed surface parking lot in this region of the project.



EXISTING TREE PLAN

EXISTING TREES

-  OFF-SITE EVERGREEN TREE
-  OFF-SITE DECIDUOUS TREE
-  AMERICAN SYCAMORE (*PLATANUS OCCIDENTALIS*)
-  AUSTRIAN PINE (*PINUS NIGRA*)
-  BIG LEAF MAPLE (*ACER MACROPHYLLUM*)
-  BLACK COTTONWOOD (*POPULUS BALSAMIFERA TRICHOCARPA*)
-  CALLERY PEAR (*PYRUS CALLERYANA*)
-  DOUGLAS FIR (*PSEUDOTSUGA MENZIESII*)
-  EUROPEAN WHITE BIRCH (*BETULA PENDULA*)
-  KWANZAN CHERRY (*PRUNUS SERRULATA 'KWANZAN'*)
-  RED MAPLE (*ACER RUBRUM*)
-  TRIDENT MAPLE (*ACER BUERGERIANUM*)
-  WESTERN RED CEDAR (*THUJA PLICATA*)



MIXED CONIFER/DECIDUOUS FOREST
 DOMINANT CANOPY SPECIES:
Acer macrophyllum
Pseuotsuga menziesii
Thuja plicata
 UNDERSTORY SPECIES:
Corylus cornuta
Holodiscus discolor
Polystichum munitum
Mahonia nervosa