



## DESIGN REVIEW BOARD DECISION

**FILE NUMBER:** DRV23-00164  
**PROJECT NAME:** MODERA BRIDLE TRAILS  
**APPLICANT:** AMANDA KEATING WITH WEBER THOMPSON  
**PROJECT PLANNER:** TONY LEAVITT

### I. SUMMARY OF DECISION

Amanda Keating with Weber Thompson for Mill Creek Residential applied for design review approval of the Modera Bridle Trails mixed use project at 13033 NE 70<sup>th</sup> Place (see Attachment 1). The applicant is proposing to construct a 5-story mixed use project with approximately 368 residential units and 7,000 square feet of retail space. Parking is proposed within a parking structure below the building (see Attachment 2). Vehicular access to the property is from 130th Avenue NE and NE 70th Place.

On July 17, 2023, the Design Review Board (DRB) approved the project as shown on the plans dated July 17, 2023 subject to the following conditions:

- A. This application is subject to the applicable requirements contained in the Kirkland Municipal Code, Zoning Code, and Building and Fire Code. It is the responsibility of the applicant to ensure compliance with the various provisions contained in these ordinances. Attachment 3, Development Standards, intended to familiarize the applicant with some of the additional development regulations. This attachment does not include all of the additional regulations.
- B. As part of the application for a building permit the applicant shall submit the following:
  1. Construction plans demonstrating compliance with the project plans approved by the DRB as shown in Attachment 2.
  2. A summary of any proposed project changes, indexed to the permit drawings, from the plans approved through Design Board Review.
- C. Prior to issuance of the building permit, the applicant shall submit signed public pedestrian access easements for the through-block pathways.
- D. Prior to final inspection of a building permit by the Planning Official, the project architect shall submit a letter stating that they have evaluated the project to ensure it is consistent with the plans approved through Design Board Review and no modifications have been made that were not previously approved by the City.

## II. DESIGN RESPONSE CONFERENCE MEETINGS

### A. Background Summary

The DRB held two Design Response Conference meetings for the project. The staff report, plans, and applicant response to the DRB's recommendations from each meeting can be found listed by meeting date at this online web address:

<https://www.kirklandwa.gov/Government/Departments/Planning-and-Building/Design-Review-Board/DRB-Meeting-Materials-Archive>

Below is a summary of the Board's discussions at the two Design Response Conferences held for the project.

#### **June 5, 2023 Conference:**

The Design Review Board reviewed the plans submitted by Weber Thompson dated June 5, 2023. Staff provided an overview of the Zoning Code and Comprehensive Plan policies for the BCX zone and the key design issues for the project. Staff's memo dated May 25, 2023 provides an analysis of project consistency with applicable zoning regulations and Design Guidelines for Pedestrian Business Districts.

After deliberating, the Board requested the applicant to return for a second meeting to respond to the following DRB comments:

- Provide design inspirations for the project and incorporate design elements that reflect the Bridle Trails Neighborhood.
- Revise the material palette to include fewer neutral colors and incorporate different brick colors.
- Revise the façade facing the current Pagliacci property to include the same modulation and materials being used on the other public facing facades.
- Address the street level blank wall near NE 70th Place and the Pagliacci site.
- Provide detailed landscape palette and plans for the project.
- Submit more details regarding site materials including paving details, furnishings, and lighting fixtures. Additionally, provide more details for the design of through block connections.
- Provide more details about the conversion of the residential amenity space to commercial space in future including space design and parking.

The meeting was continued to July 17, 2023.

#### **July 17, 2023 Conference:**

The Design Review Board reviewed the revised plans submitted by Weber Thompson dated July 17, 2023. Staff's memo dated July 6, 2023 provides an analysis of project consistency with applicable zoning regulations policies and Design Guidelines for Pedestrian Business Districts.

The applicant presented revised plans, which addressed the requested items from the DRB. The DRB discussed the changes proposed by the applicant and at the conclusion of the meeting voted to approve the project. See Section III below for further information regarding the DRB's discussions and conclusions.

#### **B. Public Comment**

One public comment letter was received during the Design Response Conference meetings that staff forwarded to the Board for consideration (see Attachment 4). The written comment is contained in the City's official file. The comment letter was focused on traffic impacts, parking requirements, and the building size. The letter was forwarded to the Public Works Department for consideration as part of their traffic review.

During the DRC Meetings, no individuals provided public testimony.

### **III. DESIGN REVIEW BOARD DISCUSSION AND CONCLUSIONS**

Below is a summary of the key issues and conclusions reached by the Design Review Board during the design review process. For more background on these issues and evaluation of how the project meets the Zoning Code, see the staff advisory reports from the Design Response Conferences contained in File DRV23-00164 and online at the previously mentioned DRB meeting page.

#### **A. BUILDING HEIGHT, ARCHITECTURAL AND HUMAN SCALE**

DRB Discussion: The DRB agreed with the applicant's preferred massing option, Massing Option 3 that was presented at the Conceptual Design Conference. Vertical modulation was achieved with building modulation, the use of upper story terraces, and placement of materials and colors. Horizontal modulation was achieved with a pedestrian friendly commercial ground floor design, required upper story setbacks, the use of residential balconies, and the use of varying cornices and architectural details. Human scale was achieved with the use of street level public courtyards, upper story balconies, and a variety of materials. Blank walls and parking garages were adequately screened with materials and landscaping.

DRB Conclusions: The DRB concluded that the proposed building massing, architectural scale and human scale are consistent with the applicable design guidelines found in the Design Guidelines for Pedestrian Business Districts.

## **B. VEHICULAR AND PEDESTRIAN ACCESS**

### DRB Discussion:

The DRB reviewed the vehicular access and pedestrian access for the site as part of their review. Vehicular access to the property is proposed from NE 70<sup>th</sup> Place and 130<sup>th</sup> Avenue NE. The site contains multiple pedestrian access points from each adjacent right-of-way (on the west and north sides of the building) and the through block pathways (on the east and south sides of the building).

The DRB reviewed the design of the public pedestrian spaces including the sidewalks and plazas. Their review looked at the location of the plazas, the design of the seating areas, the use of neighborhood character elements, lighting of these spaces, and the materials beings used. Additionally, they looked at the applicant's pedestrian weather protection plan.

DRB Conclusions: The DRB concluded that the proposed vehicular and pedestrian access plan meets the applicable design guidelines found in the Design Guidelines for Pedestrian Business Districts. They also concluded that the proposed plans for the pedestrian sidewalks and plazas meet the applicable design guidelines.

## **C. BUILDING MATERIALS, COLOR AND DETAIL**

DRB Discussion: The DRB evaluated the proposed materials, colors, and details. The DRB approved of the applicant's preferred material and color palette for the project and agreed that the colors and materials used were effective in reducing the perceived scale of the building and adding interest to the plaza and pedestrian areas. Additionally, the applicant presented Bridle Trails Neighborhood inspired design elements including art pieces, storefront entries, window muntins, canopies, lighting, door pulls and bike racks. The DRB discussed how these elements added character to the project and better connected the project to the neighborhood.

DRB Conclusions: The DRB concluded that the proposed building materials, colors and details meet the applicable design guidelines found in the Design Guidelines for Pedestrian Business Districts. The DRB approved of the Bridle Trails Neighborhood inspired design elements and required that the applicant incorporate them as part of the building permit.

## **D. LANDSCAPING**

DRB discussion: The DRB reviewed the landscape plan designed to help soften building massing, enhance the pedestrian experience, and provide seasonal visual interest. Opportunity areas discussed for landscaping included the NE 70<sup>th</sup> Place public space, through block pathways, 130<sup>th</sup> Avenue NE sidewalk and right-of-way, and treatment of blank walls. The applicant presented the DRB with a planting palette reflective of the Bridle Trails State Park with a lot of native forest plantings like ferns, bushes, and flowers.

DRB conclusions: The DRB concluded that the proposed landscaping meets the applicable design guidelines found in the Design Guidelines for Pedestrian Business Districts.

#### IV. ZONING REQUIREMENTS THAT REQUIRE DRB APPROVAL

##### A. Commercial Use

###### 1. Facts:

- a. KZC Section 35.10.040.2.k states that development shall contain commercial uses oriented to adjoining arterials and through-block pathways. The location and frontages of these commercial uses shall be reviewed through Design Review for consistency with applicable guidelines or regulations.
- b. The design guidelines state that continuous commercial building fronts should be provided along adjoining arterials and through-block pathways. Consideration should be made to maximize the usage of through-block pathways with commercial and other public activations. Any non-commercial building fronts should be located in areas where less successful commercial activity may occur. Special attention should be made in locating commercial building fronts near intersecting through-block pathways and where transit services are located.
- c. The applicant requested that commercial uses not be required along the through block pathways (on the east and south frontages of the structure) because retail in those locations would not be viable. The applicant provided an analysis (see Attachment 2, Sheets 61 through 70) to support this request.

The reasons for the request are:

- Along the eastern frontage, the applicant states that the existing conditions (including the driveway on the neighboring property, lack of access to parking on the neighboring property, and steep grade changes) will make commercial uses along this façade impractical.
  - Along the southern frontage, the applicant states that the existing frontage conditions on the neighboring property (including loading areas, large blank façade, drive lane and restricted parking) do not create the visibility, pedestrian or vehicle access, and ambiance necessary for viable commercial uses.
- d. In the southeast corner of the development, the applicant proposed residential amenity space that will help to activate the corner of the development. The space will include meeting and work areas, a fitness center, and entrances to the residential building. The space will be designed to meet commercial standards (including glazing, weather protection and floor to floor height requirements) and would allow for conversion to commercial uses if it becomes viable in the future (see Attachment 2, Sheet 69). Staff assessed the parking impact of the amenity space as part of shared parking analysis for the site. Based on the shared parking analyses, there will be a deficit of two spaces between 7 P.M. to 8 P.M. during the weekday. Up to two thousand (2,000 sf) of the amenity space can be converted to commercial use (non-medical or fitness use) without parking overflow. Any future conversion of the residential amenity space to commercial space will require SEPA (State Environmental Policy Act) and building permit reviews.

###### 2. Conclusions:

- a. The DRB concluded that the east and south building frontages along the through block pathways would not be successful commercial locations and allowed the proposed residential uses.

- b. The DRB also concluded that the design of the southeast corner residential amenity space will help to activate the area and allow for the future conversion of the space to commercial if it becomes viable.

## **B. Residential Linear Frontage Limitation**

### 1. Facts:

- a. KZC Section 35.10.040.2.m states that lobbies and amenity space for residential or assisted living uses may be allowed within the commercial frontage provided they do not exceed 20 percent of the building's linear retail frontage along the street or through-block pathway. The Design Review Board (or Planning and Building Director if not subject to Design Review) may approve a minor increase to ground floor residential lobbies and amenities if they are connected to retail use and the design of the ground floor frontage will maximize visual interest.
- b. The proposed plans show a total of 385.58 linear feet of building frontage along NE 70th Place. The proposed residential lobby and amenity space will take up 132.24 linear feet or 34.3 percent.
- c. The applicant requested a 14.3 percent increase in residential linear frontage.
- d. The applicant stated that the increase in residential linear frontage is a result of the open space carved out along NE 70TH Place at the request of the DRB. Without this carve out, the project would comply with the requirement.

2. Conclusion: The DRB reviewed the request and agreed with the applicant's analysis. The proposed open space will create an active and engaging frontage along NE 70th Place, will be connected to the retail spaces, and the design of the frontage will maximize visual interest.

## **C. Open Space**

### 1. Facts:

- a. KZC Section 35.10.040.2.e states that the development to provide publicly accessible pedestrian oriented open space(s) adjacent to the street or through-block pathway. The publicly accessible space(s) shall contain a minimum of 1,000 square feet or one square foot per 200 gross square feet of above grade building area, whichever is greater. The size calculation shall not include the required width of abutting sidewalks or pathways. Locations, dimensions, features and improvements (such as plazas, seating, public art, children's recreation space) shall be reviewed and approved through by the Design Review Board based on applicable guidelines.

- b. The proposed project will have a total of 410,588 square feet of above grade building area. The pedestrian open space requirement (based on the 1 per 200 square feet) is 2,053 square feet for the project. The open space is divided into 4 areas ranging in size from 771 square feet to 3,787 square feet for a total of 6,770 square feet (see Attachment 2, Sheet 106).
- c. KZC Section 35.10.040.2.I states that development shall provide for one north-south through-block pathway connection between NE 70th Place and NE 65th Street and two east-west through-block pathway connections between 130th Avenue NE and 132nd Avenue NE. The Design Review Board shall determine the final location and configuration of the through-block pathway connections based on convenience and utility for nonmotorized access and orientation toward commercial uses and pedestrian oriented open space.
- d. The north-south through-block pathway is located along the east facade of the building near the existing access drive for the Bridle Trails Shopping Center. The east-west through-block pathway is located along the south edge of the property and will be incorporated into the fire lane (see Attachment 2, Sheet 80).
- e. KZC Section 105.19 requires that through-block pathways be retained in a public pedestrian access easement.

2. Conclusions:

- a. The plans show compliance with the public open space size requirements. The DRB reviewed the design of these spaces to ensure compliance with the applicable design guidelines.
- b. The DRB reviewed the proposed location and configuration of the through-block pathways and approved their location and design. Prior to issuance of the building permit, the applicant should submit signed public pedestrian access easements for the through-block pathways.

**V. STATE ENVIRONMENTAL POLICY ACT (SEPA) AND CONCURRENCY**

The City issued a SEPA Determination of Nonsignificance on October 23, 2023 for the project. No appeals of the determination were filed.

**VI. DEVELOPMENT REVIEW COMMITTEE**

Comments and requirements placed on the project by City departments are found on the Development Standards, Attachment 3.

**V. SUBSEQUENT MODIFICATIONS**

Modifications to the approval may be requested and reviewed pursuant to the applicable modification procedures and criteria in effect at the time of the requested modification.

**VI. APPEALS OF DESIGN REVIEW BOARD DECISIONS AND LAPSE OF APPROVAL**

A. Appeals

Section 142.40 of the Zoning Code allows the Design Review Board's decision to be appealed to the Hearing Examiner by the applicant or any person who submitted written or oral comments to the Design Review Board. The appeal must be in the form of a letter of appeal and must be delivered, along with any fees set by ordinance, to the Planning and Building Department by 5:00 p.m., December 27, 2023, fourteen (14) calendar days following the postmarked date of distribution of the Design Review Board's decision.

Only those issues under the authority of the Design Review Board as established by Kirkland Zoning Code 142.35(2) are subject to appeal.

B. Lapse of Approval

The applicant must begin construction or submit to the City a complete building permit application for the development activity, use of land or other actions approved under this chapter within five (5) years after the final approval of the City of Kirkland on the matter, or the decision becomes void.

The applicant must substantially complete construction for the development activity, use of land or other actions approved under this chapter and complete the applicable conditions listed on the notice of decision within seven (7) years after the final approval on the matter or the decision becomes void.

**VII. ATTACHMENTS**

- 1. Vicinity Map
- 2. Plans dated July 17, 2023
- 3. Development Standards
- 4. Public Comment Letter

**VIII. APPROVAL**



\_\_\_\_\_  
Carlos Castaneda  
Chair, Design Review Board

Date: \_\_\_\_\_ 12/03/2023 \_\_\_\_\_



NE 71ST ST

NE 70TH DR

RM 3.6

NE 70TH PL

Subject Property

68TH ST

RM 3.6

130TH AVE NE

BCX

Snyder  
Corner  
Park

NE 65TH ST

PR 3.6

RM 3.6

NE 66TH ST

VE NE



# MODERA BRIDLE TRAILS – DESIGN RESPONSE CONFERENCE 2

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WEBER THOMPSON

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# OVERVIEW OF BOARD GUIDANCE FROM DRC I

**ITEM 1**      **AMPLIFY BRIDLE TRAILS CHARACTER**  
The Design Review Board liked the overall direction of the project, but felt the character of the design could be more precisely fine tuned to reflect the Bridle Trails neighborhood. They wanted to understand more about the design inspirations for the project. The Board suggested art as a way to enhance the through-block pathways and add Bridle Trails specific character elements.

**RESPONSE**  
The design uses neutral tones with richer, warm accents pulled from the picturesque neighborhood. Hints of the area’s equestrian heritage can be found in details throughout the clean design. The project seeks to engage passersby on 70th and the through-block pathways with meaningful information and still-life installations celebrating the past, present and future of Bridle Trails.

**RELEVANT DESIGN GUIDELINES**  
PI-2 Pedestrian Paths and Amenities  
PI-6 Public Art  
S-1 Fenestration Patterns  
S-2 Architectural Elements: Decks, Bay Windows, Arcades, Porches  
BM-1 Ornamental and Applied Art  
BM-2 Color

**ITEM 2**      **REVISE MATERIAL PALETTE**  
The Board liked the general direction of the materials, but felt the palette was overall too neutral. They wanted to see more depth and richness. They suggested using more than one color for brick.

**RESPONSE**  
The updated palette still reflects the sophisticated soft tones throughout the neighborhood, but makes adjustments for stronger contrast and more warmth. The addition of a second, darker brick blend and a leathery, saddle color add richness and greater variety to the palette.

**RELEVANT DESIGN GUIDELINES**  
BM-2 Color

**ITEM 3**      **REFINE FACADES FACING PAGLIACCI**  
To better respond to current conditions, the Board requested the facades facing Pagliacci reflect a similar style of modulation and materiality as the public facades for the project. They requested the at grade area closest to NE 70 but facing Pagliacci be treated to eliminate a blank wall condition since the facade is currently in public view.

**RESPONSE**  
Additional evergreen landscaping along the western facade facing Pagliacci adds texture at the lower facade within public view. Coupled with the brick pilasters coming to grade, these elements mitigate the potential blank wall condition. Beyond Pagliacci on the north facade of this project, the revised massing treats the corner the same as similar corners around the project by highlighting it with a wood look accent material.

**RELEVANT DESIGN GUIDELINES**  
BM-2 Color  
PE-10 Blank Walls  
S-3 Building Modulation Vertical  
BM-3 Street Corners

**ITEM 4**      **PLANT PALETTE DETAIL**  
The Board requested more detailed and specific explanation of intent regarding plant palette and placement.

**RESPONSE**  
Plant mixes for each of the four frontages of the project have been provided, as well as character images showing the design intent. The project pulls inspiration from the native plant palette of the Bridle Trails Park, including understory species as well as plants appropriate for an edge condition, depending on the sun exposure on each side of the site. Plants are selected for their drought tolerance, evergreen coverage to minimize maintenance and establishment of weeds, and staggered bloom schedules to support habitat for birds and insects.

**RELEVANT DESIGN GUIDELINES**  
PI-3 Street Trees  
NF-1 Visual Quality of Landscaping

**ITEM 5**      **SITE MATERIALS DETAIL**  
The Board requested more detailed and specific explanation of intent regarding site furnishings and materials. In particular the Board wanted to ensure the hardscape throughout the public realm creates an engaging and rich experience for pedestrians.

**RESPONSE**  
Site materials will support the character development of the project through relevant details and selections taken from the neighborhood. Movable and fixed site furnishings are traditional and classic, with wood, metal and colors that complement an upscale residential context. Equestrian references, such as tie posts, wrought iron detailing and natural wood tie into the area history, further reinforced by interpretive signage throughout the site. A mix of scored and colored concrete provides interest and variation to the pedestrian experience, while site walls ground the project as grade steps around the site.

**RELEVANT DESIGN GUIDELINES**  
PI-2 Pedestrian Paths and Amenities  
PE-7 Lighting From Buildings

## RESPONSE 1: AMPLIFY BRIDLE TRAILS CHARACTER

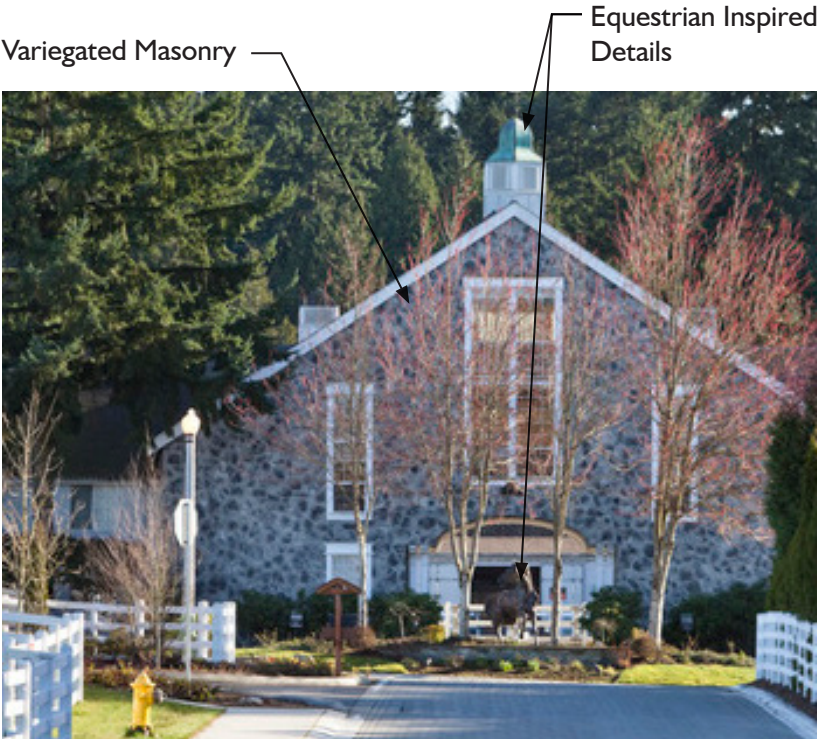
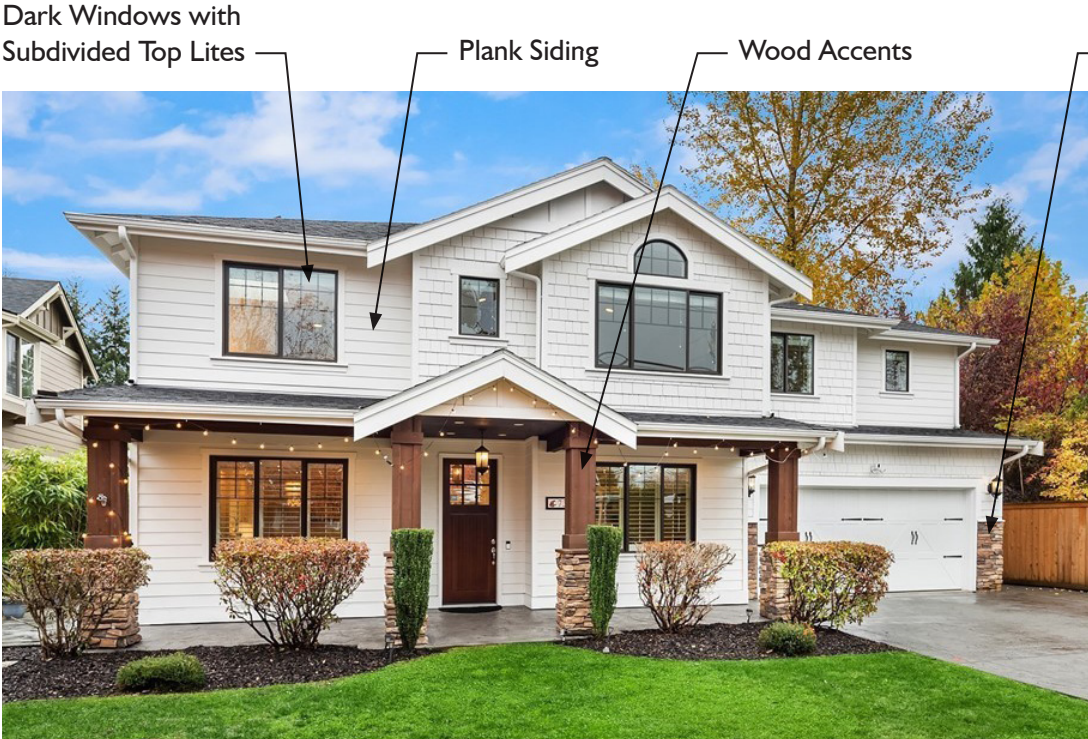
# AMPLIFY BRIDLE TRAILS CHARACTER: BOARD GUIDANCE (DRB RESPONSE 1)

The Design Review Board liked the overall direction of the project, but felt the character of the design could be more precisely fine tuned to reflect the Bridle Trails neighborhood. They wanted to understand more about the design inspirations for the project. The Board suggested art as a way to enhance the through-block pathways and add Bridle Trails specific character elements.



# AMPLIFY BRIDLE TRAILS CHARACTER: NEIGHBORHOOD CONTEXT (DRB RESPONSE 1)

The existing Bridle Trails Neighborhood primarily consists of single-family residential homes on lushly wooded lots. While each home has a distinctive character that reflects individual owners and different eras of development, there are recurring themes, materials, and colors within the architecture.



Board & Batten — Equestrian Inspired Details

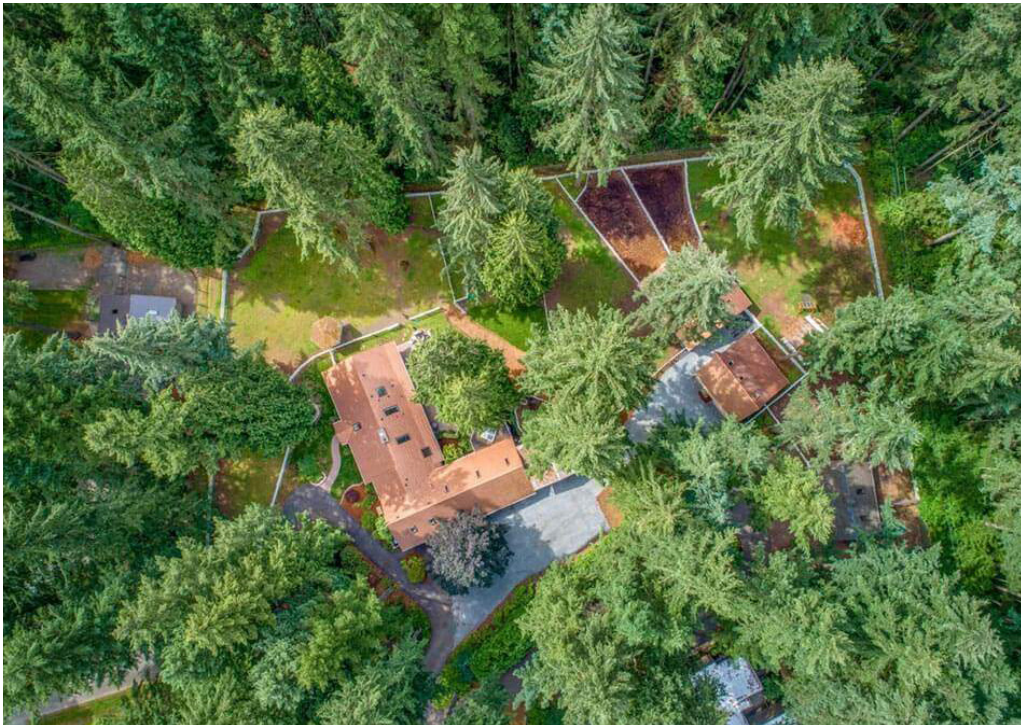
Wood Accents — Dark Windows with Subdivided Top Lites

Variegated Masonry — Board & Batten

Mottled Brick — Plank Siding

# AMPLIFY BRIDLE TRAILS CHARACTER: STATE PARK & NATURAL CONTEXT (DRB RESPONSE 1)

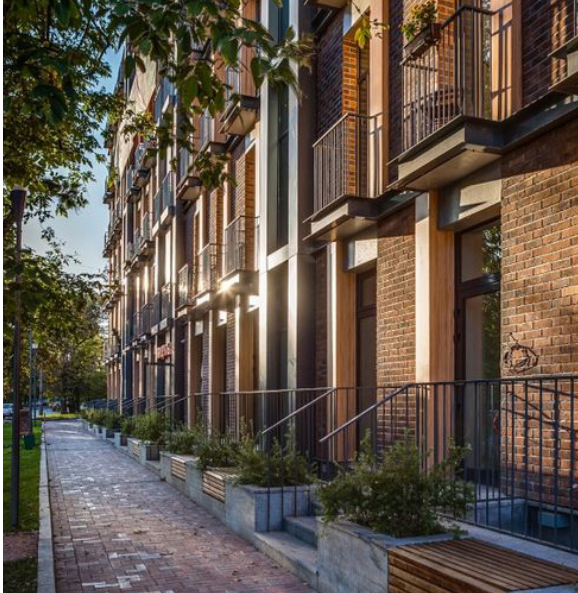
“Bridle Trails” immediately conjures images of the state park just south of Bridle Trails neighborhood. Like many natural areas in the PNW, it boasts the tall conifers of old growth forests. Unlike other parks though, it has a unique equestrian focus. The park’s character seeps into surrounding residential areas.





# AMPLIFY BRIDLE TRAILS CHARACTER: INSPIRATION & FLAVOR (DRB RESPONSE 1)

Modera Bridle Trails creates a sophisticated blend of apartment homes, engaging frontages, and lush landscaping. The design uses neutral tones with richer, warm accents pulled from the picturesque neighborhood. Hints of the area's equestrian heritage can be found in details throughout the clean design.



# AMPLIFY BRIDLE TRAILS CHARACTER: CELEBRATING BRIDLE TRAILS HISTORY (DRB RESPONSE 1)

The pedestrian-oriented space along 70th coupled with the through-block pathways provide a unique opportunity to create a progressive, engaging educational and artistic experience for pedestrians. The project seeks to engage passersby with meaningful information and still-life installations which celebrate the past, present and future of Bridle Trails.



## CONCEPTUAL EXAMPLES



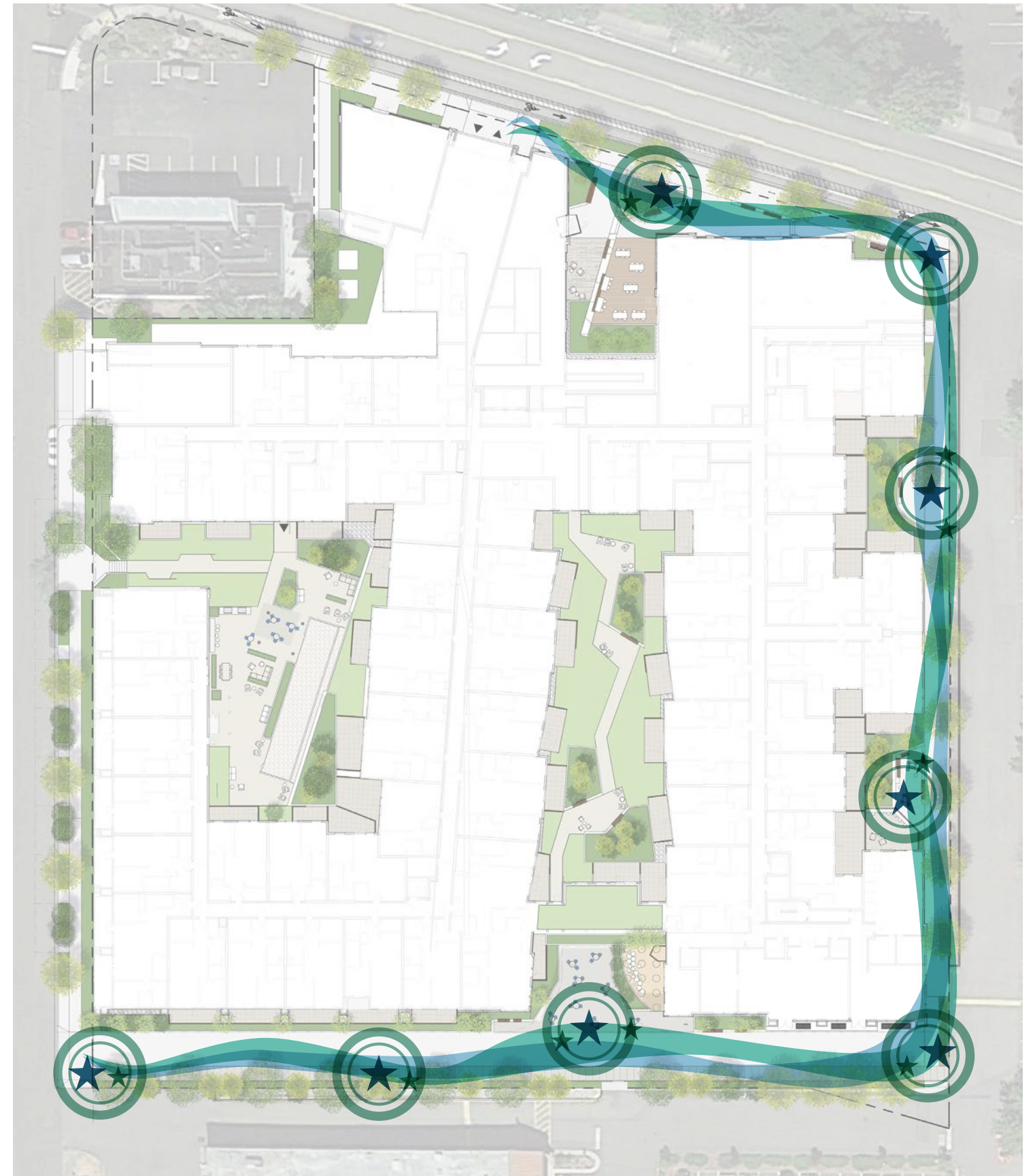
## GUIDELINES

### PI-2 PEDESTRIAN PATHS AND AMENITIES

Creating a common thread that ties together the pedestrian experience along 70th and the through-block pathways creates a continuous and enjoyable experience for pedestrians enhancing their progression in and around the project site.

### PI-6 PUBLIC ART

Each point along the sinuous timeline creates an opportunity for artistic expression through a variety of forms. The project teams continues to refine the application in each location to best engage pedestrians



**Bridle Trails Past, Present, & Future**



**PRIOR TO 1880**

Coast Salish peoples, the Duwamish, Muckleshoot, Puyallup, Skykomish, Snoqualmie, Snohomish, Suquamish and Tulalip tribes and other tribes of the Puget Sound Salish people, resided in present-day City of Kirkland which is in the traditional heartland of the Lake People and the River People.

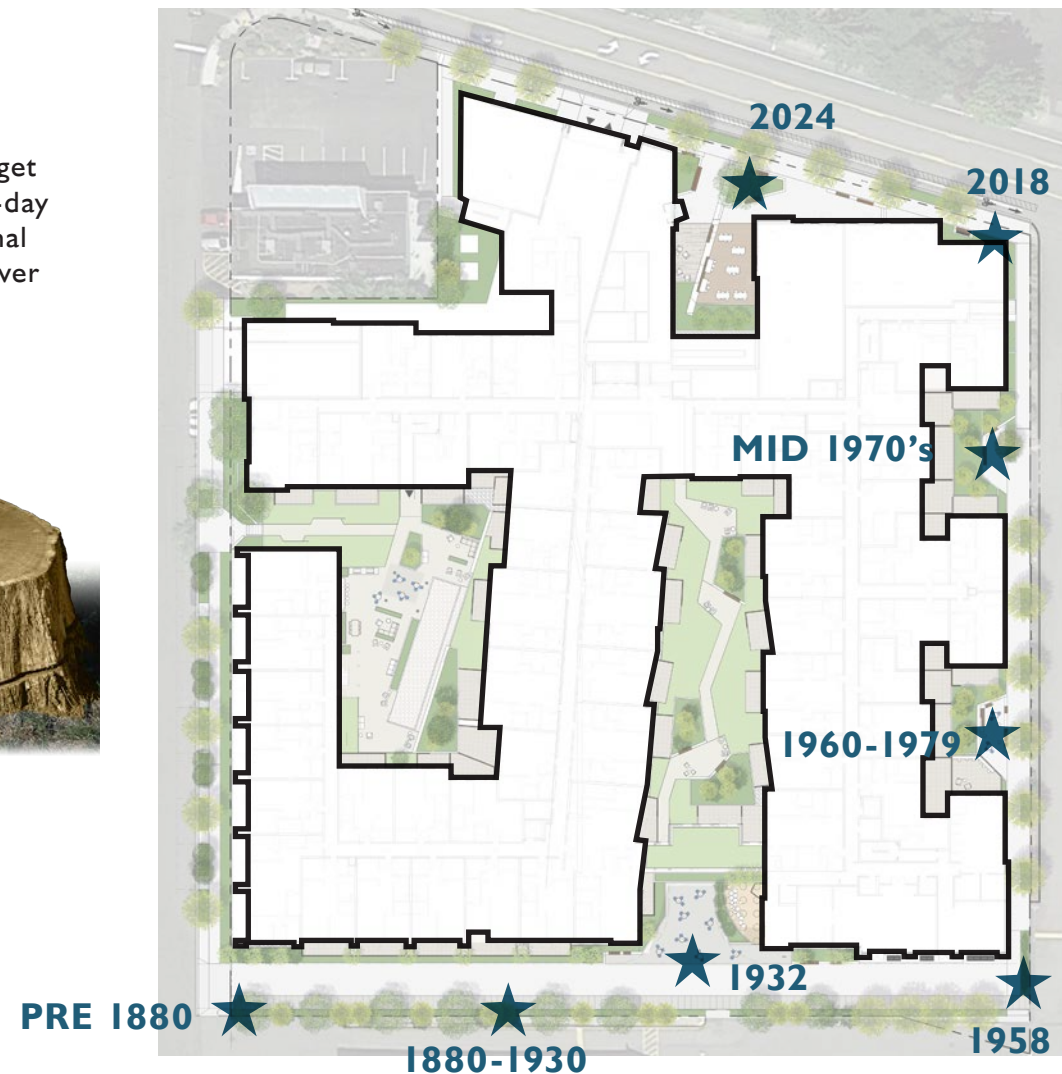
**1880 – 1930: LOGGING**

In 1889, areas in and around Bridle Trails State Park were part of a land grant to the State of Washington to be logged with the purposes of helping fund local schools. Fortunately, some of the original Douglas-firs were spared and are now as large as five feet in diameter, 190 feet tall, and more than 300 years old.



**1932: BRIDLE TRAILS STATE PARK ESTABLISHED**

The State established Bridle Trails State Park in 1932 in response to a request from local horseback riders. The Washington Commissioner of Public Lands sets aside the area of the current park for the purposes of a forested trail system shared by pedestrians and equestrians and an outdoor arena used for equestrian activities.



**2024: PROVIDING SUSTAINABLE DESIGN & FUTURE**

The first development under Kirkland's updated zoning code achieves LEED Platinum through advanced building systems, superior water management, and reduced parking infrastructure. Coupled with the introduction of increased housing the development sets the stage for a sustainable future in Kirkland.



**2018: KIRKLAND ENVISIONS FUTURE FOR BRIDLE TRAILS**

The City of Kirkland updates their comprehensive plan for Bridle Trails by allowing for mixed use development within the commercial zone. Their goal is to increase access to housing in Kirkland and require superior environmentally friendly design in new developments.

**MID 1970'S: BRIDLE TRAILS SHOPPING CENTER**

With the increased density of family homes, Jim and Freda Gaines sell the wooded portion of their property to be developed as the Bridle Trails Shopping Center and better serve the neighborhood's needs.

**1960 – 1979 RESIDENTIAL DEVELOPMENT**

The 1960s and 1970s see an increase in residential development around the bowling alley and grocery store. By the end of the 1970s the area evolves from woods and large pastures to large single family residential lots and more compact equine facilities.

**1958: GAINES'S BOWLING ALLEY**

Jim and Freda Gaines buy a large wooded property, and open "Totem Bowl" (later called Tech City Bowl), a 16 lane, family owned and operated bowling alley on NE 70th which was anticipated to be the main thoroughfare connecting Kirkland to Redmond. The area is primarily wooded with the exception of a grocery store, and a few large properties containing homes and horse pastures.



# AMPLIFY BRIDLE TRAILS CHARACTER: CHARACTER BUILDING DETAILS (DRB RESPONSE 1)

## INSET STOREFRONT ENTRIES

The commercial storefront entries on 70th angle inward in a more historic gesture. Functionally this provides slightly more maneuvering space around door swings while aesthetically it adds uniqueness to the commercial frontage.

**Design Guidelines:** PE-3 Sidewalk Width: The Storefront Activity Zone, S-1 Fenestration, S-2 Architectural Elements: Decks, Bay windows, Arcades, Porches



## WINDOW MUNTINS

The subdivision of upper window panes above storefront is a more historic detail that isn't often used in new construction. The extra detail adds character to the activated frontages and evokes Bridle Trails' quainter, more neighborly qualities.

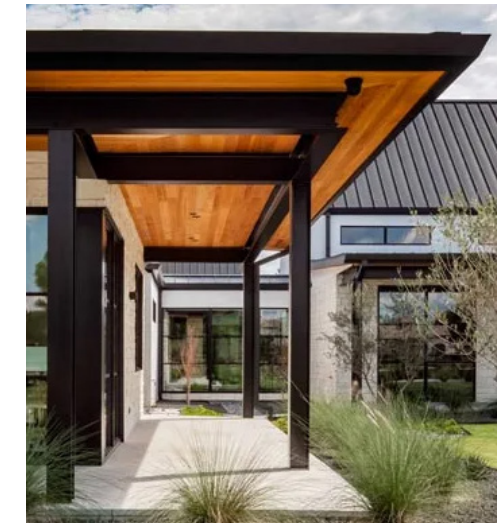
**Design Guidelines:** S-1 Fenestration, BM-1 Ornamental and Applied Art



## CEDAR AND BLACK METAL CANOPIES

The dark metal canopy frames create a crisp accent around the building and relate well to wrought iron details frequently found in equestrian design. Cedar soffits add warmth in a highly visible location for pedestrians and reflect the farm-like areas of Bridle Trails.

**Design Guidelines:** S-2 Architectural Elements: Decks, Bay windows, Arcades, Porches, PE-4 Pedestrian Coverings



## TRADITIONALLY INSPIRED CONTEMPORARY WALL SCENCE

The ornamental top of the fixture feels congruent with traditional styles while the simplicity of the fixture still feels modern.

**Design Guidelines:** PE-7 Lighting from Buildings,



## UNIQUE DOOR PULLS AT ACTIVATED FRONTAGES

Specialized door pulls that are also tackle inspired add a unique detail to the project in a prominent location for pedestrians.

**Design Guidelines:** BM-1 Ornamental and Applied Art



## HITCHING POST BIKE RACKS

In keeping with the equestrian, wrought iron, ring detail look, we are proposing hitching post styled bike racks.

**Design Guidelines:** BM-1 Ornamental and Applied Art



# AMPLIFY BRIDLE TRAILS CHARACTER: PEDESTRIAN EXPERIENCE (DRB RESPONSE 1)



## SOUTHEAST CORNER

In the foreground, pedestrians stop to review one of the many installations celebrating Bridle Trails' History. The light fixtures, storefront muntins, and specialty bike racks nod towards Bridle Trails' quaint equestrian neighborhood history. Warm cedar soffits and lush landscaping add natural touches to the new development.

**Design Guidelines:** PE-7 Lighting from Buildings, PI-2 Pedestrian Paths and Amenities, PI-6 Public Art, S-1 Fenestration Patterns, BM-1 Ornamental and Applied art, BM-2 Color



## SOUTHWEST CORNER

An installation about the history of Bridle Trails anchors the beginning of the southern through-block pathway. The wide path is modulated by distinctive bands in the paving that extend from brick pilasters on the building. Each pilaster is highlighted by a characteristic wall sconce. Native landscaping inspired by the nearby state park coupled with wood accented privacy screens buffer at-grade apartment patios from passersby.

**Design Guidelines:** PE-7 Lighting from Buildings, PI-2 Pedestrian Paths and Amenities, PI-6 Public Art, S-1 Fenestration Patterns, BM-1 Ornamental and Applied art, BM-2 Color

# AMPLIFY BRIDLE TRAILS CHARACTER: PEDESTRIAN EXPERIENCE (DRB RESPONSE 1)



## EASTERN PUBLIC OPEN SPACES

The two smaller open spaces along the north-south through-block pathway provide more intimate stopping points for pedestrians. Ample wood seating offset with black metal details echo the canopies above. Installations celebrating the history of Bridle Trails create focal points in each of the open spaces. Subdivided glazing at the active amenity spaces add character to the storefronts in keeping with more historic commercial spaces.

**Design Guidelines:** PE-7 Lighting from Buildings, PE-8 Pedestrian Oriented Plazas, PI-2 Pedestrian Paths and Amenities, PI-6 Public Art, S-1 Fenestration Patterns, BM-1 Ornamental and Applied art, BM-2 Color



## PEDESTRIAN REALM ON NE 70TH

Continuous commercial glazing on 70th is enriched by the warmth of cedar soffits, ornamental muntins in the glazing, and hanging retail blade signs. Each entry has distinction created by the angle storefront inset at each door. Wood seat topped with black metal accents echo the other design elements. Cast in place horse shoes, and another Bridle Trails historical installation engage pedestrians.

**Design Guidelines:** PE-4, Pedestrian Coverings, PE-5 Pedestrian Friendly Building Fronts, PE-7 Lighting from Buildings, PE-8 Pedestrian Oriented Plazas, PI-2 Pedestrian Paths and Amenities, PI-6 Public Art, S-1 Fenestration Patterns, BM-1 Ornamental and Applied art, BM-2 Color

## RESPONSE 2: REVISE MATERIAL PALETTE

# REVISE MATERIAL PALETTE: BOARD GUIDANCE (DRB RESPONSE 2)

The Board liked the general direction of the materials, but felt the palette was overall too neutral. They wanted to see more depth and richness. They suggested using more than one color for brick.

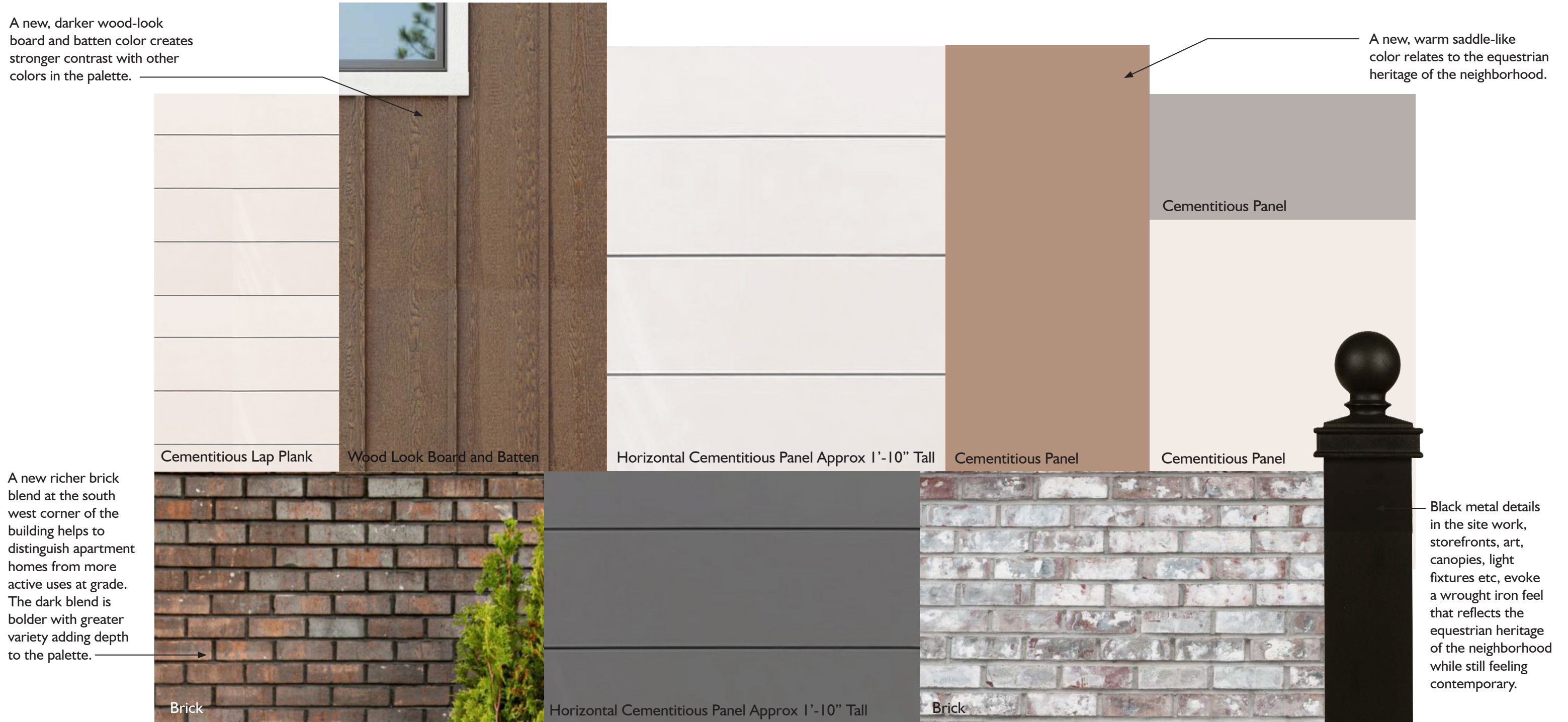
Design Guidelines: BM-2 Color



## PALETTE PRESENTED AT DRC I



# REVISE MATERIAL PALETTE: UPDATED COLOR PALETTE (DRB RESPONSE 2)



The updated palette still reflects the sophisticated neutral tones throughout the neighborhood, but makes adjustments for stronger contrast and more warmth. The addition of a second, darker brick blend and a leathery, saddle color add richness and greater variety to the palette.

# REVISE MATERIAL PALETTE: PALETTE COMPARISON (DRB RESPONSE 2)



**AT DRC I**



**PROPOSED**

Updated "saddle" color as infill in upper stories.  
Updated darker brick color surrounding residential uses.  
Darker cap flashing at brick.

# REVISE MATERIAL PALETTE: PALETTE COMPARISON (DRB RESPONSE 2)



**AT DRC I**



**PROPOSED**

Updated "saddle" color as infill in upper stories.  
Updated darker brick color surrounding residential uses.  
Darker cap flashing at brick.

# REVISE MATERIAL PALETTE: PALETTE COMPARISON (DRB RESPONSE 2)



**AT DRC I**



**PROPOSED**

Updated darker brick color surrounding residential uses.

# REVISE MATERIAL PALETTE: PALETTE COMPARISON (DRB RESPONSE 2)

## AT DRC I



## PROPOSED

Updated “saddle” color flanking SE corner massing, and as infill on western half of facade.  
Updated darker wood tone.  
Updated darker brick color on western portion of facade (in distance).  
Darker cap flashing at light brick.



# REVISE MATERIAL PALETTE: PALETTE COMPARISON (DRB RESPONSE 2)



AT DRC I



PROPOSED

Updated "saddle" color flanking wood-look corner mass.  
Updated darker wood tone.  
Darker cap flashing at light brick

# REVISE MATERIAL PALETTE: PALETTE COMPARISON (DRB RESPONSE 2)



**AT DRC I**



**PROPOSED**

Updated "saddle" color fas back drop within public open space fronting NE 70th.  
Darker cap flashing at light brick.

RESPONSE 3: REFINE FACADES FACING PAGLIACCI



# REFINE FACADES FACING PAGLIACCI: BOARD GUIDANCE (DRB RESPONSE 3)

To better respond to current conditions, the Board requested the facades facing Pagliacci reflect a similar style of modulation and materiality as the public facades for the project. They requested the at grade area closest to NE 70 but facing Pagliacci be treated to eliminate a blank wall condition since the facades was currently in public view.

**Design Guidelines:** BM-2 Color, PE-10 Blank Walls, S-3 Building Modulation Vertical, BM-3 Street Corners

PROJECT AS PRESENTED AT DRC I



# REFINE FACADES FACING PAGLIACCI: UPDATED FACADES (DRB RESPONSE 3)

## AT DRC I



## PROPOSED

Updated massing with expressed wood-look corner that addresses the corner similarly to other corners around the project.

Updated “saddle” color in infill area beyond Pagliacci.

Added landscaping at grade to address the lower wall near 70th facing Pagliacci.



# REFINE FACADES FACING PAGLIACCI: BLANK FACADE (DRB RESPONSE 3)

## PEDESTRIAN VIEW AT DRC I



## PROPOSED PEDESTRIAN VIEW

Glazing into the at-grade commercial space wraps the corner from NE 70th to the west facade facing Pagliacci. As grade rises and glazing is no longer practical, evergreen trees and shrubs ground the building and obscure the facade behind.

**Design Guidelines:** PE-10 Blank Walls



## RESPONSE 4: PLANT PALETTE DETAIL

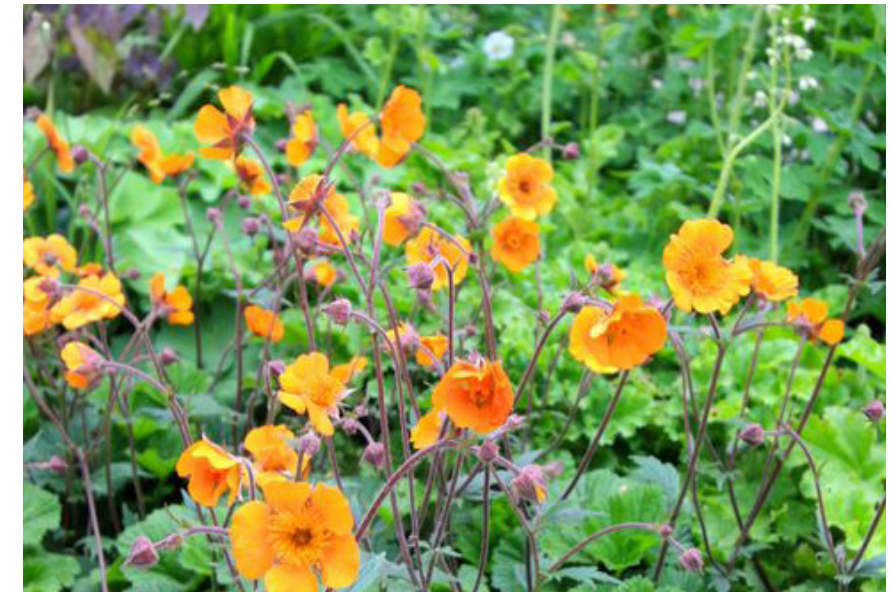
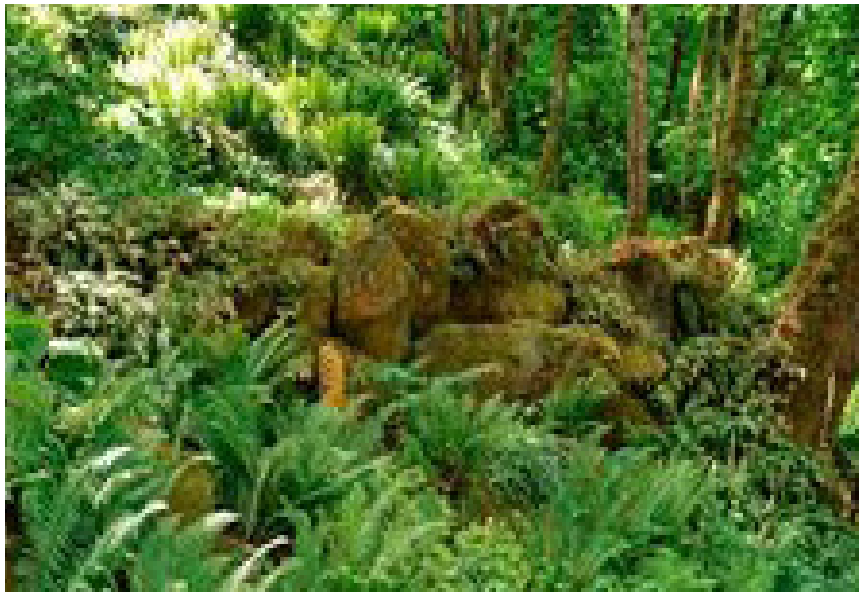
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### BOARD GUIDANCE

The Board requested more detailed and specific explanation of intent regarding plant palette and placement.

# LANDSCAPE CHARACTER – PLANTING

This planting palette is reflective of the Bridal Trails State Park with an emphasis on **native forest understory and edge plantings**. Salal, sword fern and foam flower, among other natives, create a tapestry of green with sprinklings of color from perennials and grasses. Sunnier spaces are meant to evoke meadows and forest edge habitats. Evergreen plants are emphasized for year-round seasonal interest. Ferns and grasses create a sense of movement and texture and catch the light in shady places. Plants are drought tolerant to reduce water use and maintenance.



# LANDSCAPE CHARACTER – PLANTING



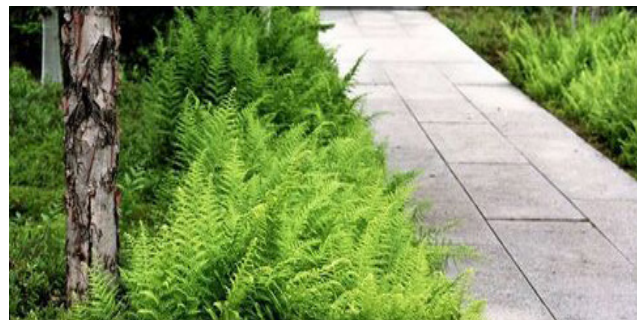
			BOTANICAL NAME	COMMON NAME	SPACING	SIZE
FOREST EVERGREEN UNDERSTORY	SHRUBS		GARRYA X ISSAQUAHENSIS	TASSEL BUSH	60" O.C.	3 GAL.
			POLYSTICHUM MUNITUM	WESTERN SWORD FERN	48" O.C.	2 GAL.
			VACCINUM OVATUM	EVERGREEN HUCKLEBERRY	60" O.C.	3 GAL.
	GROUNDCOVER/ PERENNIALS		FRAGARIA CHILOENSIS	BEACH STRAWBERRY	18" O.C.	1 GAL.
			GAULTHERIA SHALLON	SALAL	36" O.C.	2 GAL.
			MAIANTHEMUM DILATATUM	FALSE LILY	24" O.C.	1 GAL.
			TIARELLA TRIFOLIATA	FOAM FLOWER	24" O.C.	1 GAL.



EAST FOREST EDGE	SHRUBS		GAURA LINDHEIMERI	WHIRLING BUTTERFLIES	24" O.C.	1 GAL.
			PAXISTIMA MYRSINITES	OREGON BOXWOOD	24" O.C.	2 GAL.
			PENNISETUM ORIENTALE	'KARLEY ROSE' FOUNTAIN GRASS	24" O.C.	1 GAL.
			SPIRAEA BETULIFOLIA	WHITE SPIRAEA	36" O.C.	2 GAL.
	GROUNDCOVER/ PERENNIALS		ACHILLEA M. YARROW	SALMON BEAUTY	36" O.C.	2 GAL.
			ARCTOSTAPHYLOS UVA-URSI	VANCOUVER JADE BEAR BERRY	24" O.C.	1 GAL.
			ARMERIA MARITIME	SEA THRIFT	12" O.C.	1 GAL.
			CAREX PRAEGRACILIS	CLUSTERD FIELD SEDGE	18" O.C.	1 GAL.
			CAREX TUMULICOLA	WILLAMETTE GOLD SEDGE	18" O.C.	1 GAL.



SUNNY MEADOW CLEARING	SHRUBS		ARCTOSTAPHYLOS MANZANITA	SUNSET MANZANITA	48" O.C.	2 GAL.
			LAVANDULA ANGUSTIFOLIA	'PLATINUM BLONDE'	36" O.C.	1 GAL.
			SALVIA GREGGII	AMETHYST LIPS/AUTUMN SAGE	24" O.C.	1 GAL.
			SPIRAEA BETULIFOLIA	BIRCHLEAF SPIREA	36" O.C.	1 GAL.
	GROUNDCOVER/ PERENNIALS		ACHILLEA MILLEFOLIUM	SALMON BEAUTY YARROW	18" O.C.	1 GAL.
			CALLUNA VULGARIS	FLAMINGO HEATHER	18" O.C.	1 GAL.
			FESTUCA GLAUCA	ELIJAH BLUE	18" O.C.	1 GAL.
			GERANIUM OREGANUM	OREGAN GERANIUM	18" O.C.	1 GAL.
			POLEMONIUM CAERULEUM	'APRICOT DELIGHT' JACOB'S LADDER	18" O.C.	1 GAL.

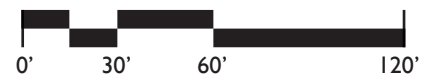
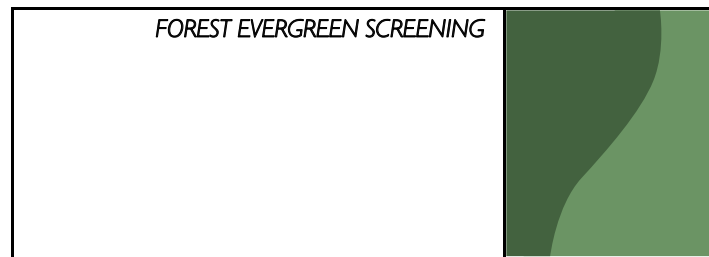
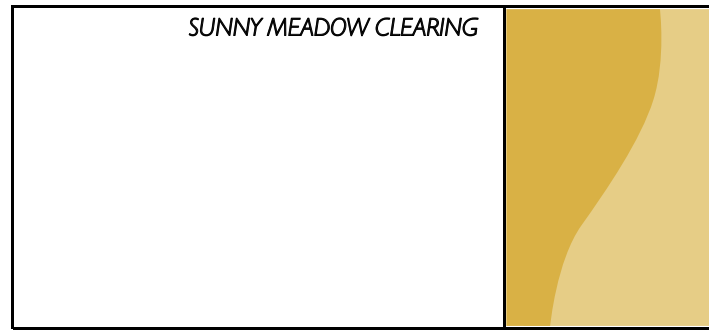
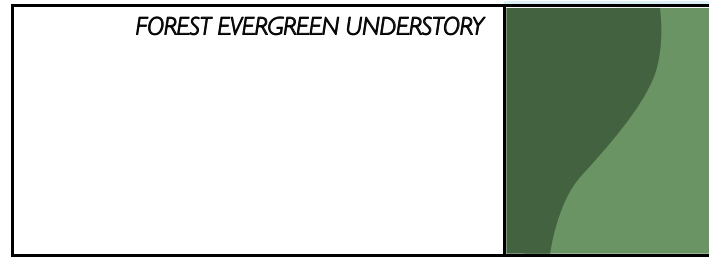


WEST FOREST EDGE	SHRUBS		ANEMONE	'HONORINE JOBERT' JAPANESE ANOMONE	24" O.C.	2 GAL.
			DAPHNE X TRANSATLANTICA	ETERNAL FRAGRANCE DAPHNE	36" O.C.	2 GAL.
			PAXISTIMA MYRSINITES	PAXISTIMA MYRSINITES	24" O.C.	1 GAL.
	GROUNDCOVER/ PERENNIALS		BLECHNUM SPICANT	DEER FERN	36" O.C.	1 GA.
			CALLUNA VULGARIS	FLAMINGO HEATHER	18" O.C.	1 GAL.
			FRAGARIA CHILOENSIS	BEACH STRAWBERRY	24" O.C.	1 GAL.
			MAIANTHEMUM DILATATUM	FALSE LILY	24" O.C.	1 GAL.
			POLYSTICHUM SETIFERUM	ALASKAN FERN	48" O.C.	2 GAL.
			VANCOUVERIA HEXANDRA	WHITE INSIDE-OUT FLOWER	18" O.C.	1 GAL.



FOREST EVERGREEN SCREENING	SHRUBS		ARCTOSTAPHYLOS 'SUNSET'	MANZANITA	60" O.C.	3 GAL.
			POLYSTICHUM MUNITUM	WESTERN SWORD FERN	48" O.C.	2 GAL.
			SPIRAEA BETULIFOLOA	BIRCHLEAF SPIREA	60" O.C.	3 GAL.
	GROUNDCOVER/ PERENNIALS		CALLUNA VULGARIS	FLAMINGO HEATHER	18" O.C.	1 GAL.
			GERANIUM OREGANUM	OREGON GERANIUM	18" O.C.	1 GAL.
			POLEMONIUM CAR.	APRICOT DELIGHT JACOBS LADDER	24" O.C.	1 GAL.
			SALVIA 'AMETHYST LIPS'	ORNAMENTAL SAGE	24" O.C.	1 GAL.

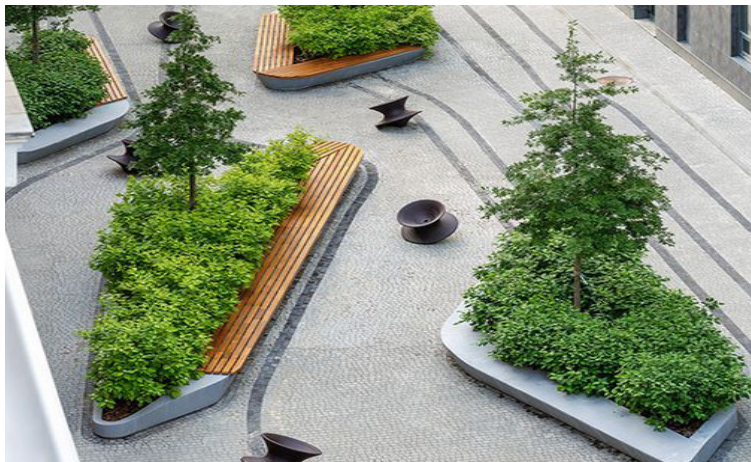
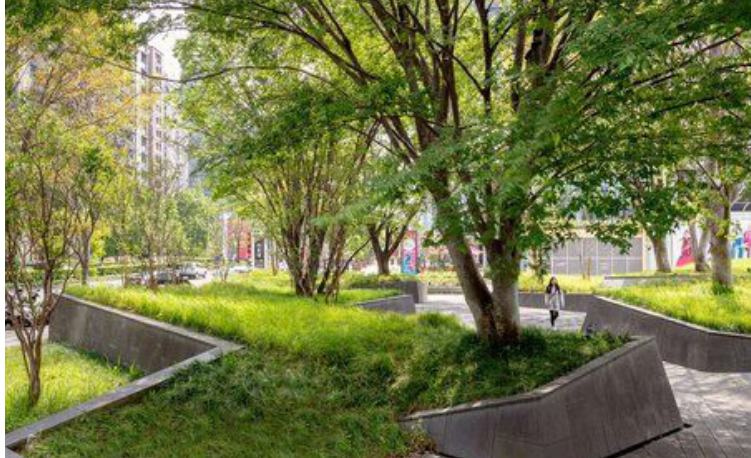
# LANDSCAPE PLAN AT GRADE (LEVELS PI & LI)



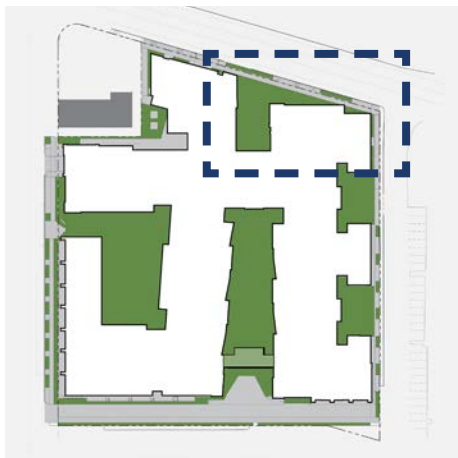
# NE 70TH PUBLIC SPACE – PLANTING

## FOREST EVERGREEN UNDERSTORY FERNY ISLANDS OF NATIVE PLANTING

This north facing courtyard and frontage offers an opportunity for shade loving northwest natives



NOT TO SCALE



### TREES



*OSTRYA VIRGINIANA*  
AMERICAN HOP HORNBEAM



*STEWARTIA PSUEDOCAMELLIA*  
STEWARTIA

### UNDERSTORY



NW NATIVE



EVERGREEN



*GARRUA X ISSAQUAHENSIS*  
TASSEL BUSH



*VACCINUM OVATUM*  
EVERGREEN HUCKLEBERRY



*GAULTEHRIA SHALLON*  
SALAL



*POLYSTICHUM MUNITUM*  
WESTERN SWORD FERN



*POLYSTICHUM SETIFERUM*  
ALASKAN FERN



*BLECHNUM SPICANT*  
DEER FERN



*MAIANthemum DILATATUM*  
FALSE LILY



*FRAGARIA CHILOENSIS*  
COASTAL STRAWBERRY



*TIARELLA TRIFOLIATA*  
FOAMFLOWER



# NE 70TH PUBLIC SPACE

## Courtyard A Detail Plan



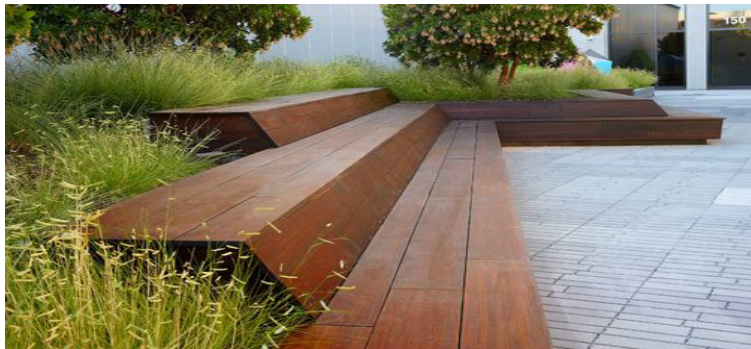
Existing Condition

# EASTERN THROUGH-BLOCK PEDESTRIAN CONNECTION

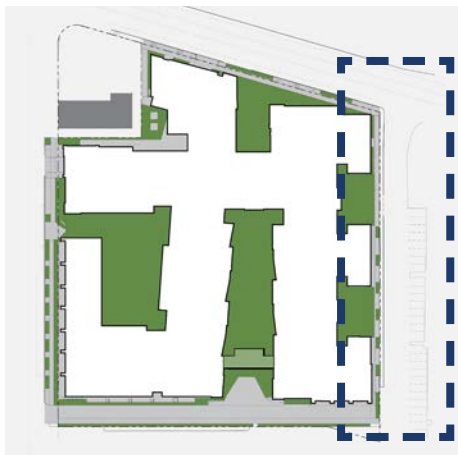
## EAST FOREST EDGE

### SOFT AND AIRY

These east facing courtyards and frontage face the morning sun with a wispy mix of native and adaptive plantings that catch the light and move with the breeze



NOT TO SCALE



## TREES



ULMUS PRPRINQUA 'JFS-BIEBERICH'  
EMERALD SUNSHINE ELM



STEWARTIA PSEUDOCAMELLIA  
STEWARTIA

## UNDERSTORY



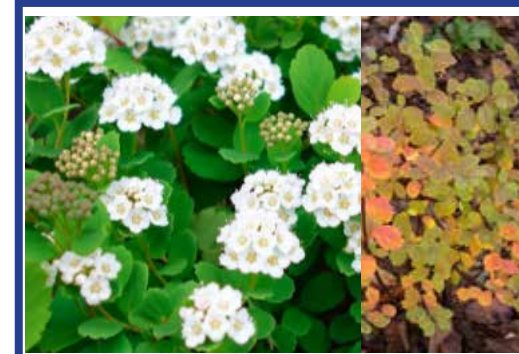
NW NATIVE



EVERGREEN



ARCTOSTAPHYLOS U.U. 'VANCOUVER JADE'  
BEARBERRY



SPIRAEA BETULIFOLIA VAR. LUCIDA  
WHITE SPIRAEA



PAXISTIMA MYRSINITES  
MOUNTAIN BOXWOOD



CAREX PRAEGRACILIS  
CLUSTERED FIELD SEDGE



PENNISETUM ORIENTALE 'KARLEY ROSE'  
FOUNTAIN GRASS



CAREX TUMULICOLA 'WILLAMETTE GOLD'  
SEDFE



ARMERIA MARITIMA  
SEA THRIFT



GAURA LINDHEIMERI  
WHIRLING BUTTERFLIES



ACHILLEA M. 'SALMON BEAUTY'  
WILD YARROW

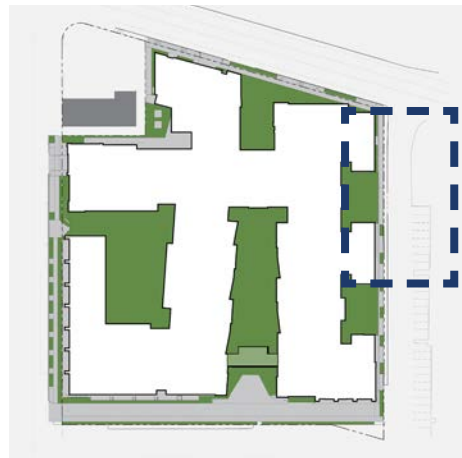
# EASTERN THROUGH-BLOCK PEDESTRIAN CONNECTION

## Courtyard B Detail Plan



Existing Condition

NOT TO SCALE



Unique accent trees in each courtyard to provide variety around the site



STEWARTIA PSEUDOCAMELLIA  
STEWARTIA



# EASTERN THROUGH-BLOCK PEDESTRIAN CONNECTION

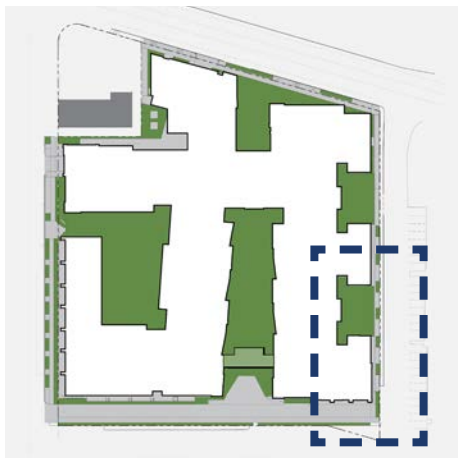
## Courtyard C Detail Plan



Existing Condition



NOT TO SCALE



Planting area with mountable curb provides visible vehicular deterrent and maintains emergency access.

Existing East Drive (Private Property)

New sidewalk with street trees and planting

# SOUTHERN THROUGH-BLOCK PEDESTRIAN CONNECTION

## SUNNY MEADOW CLEARING

### A SUNNY CLEARING

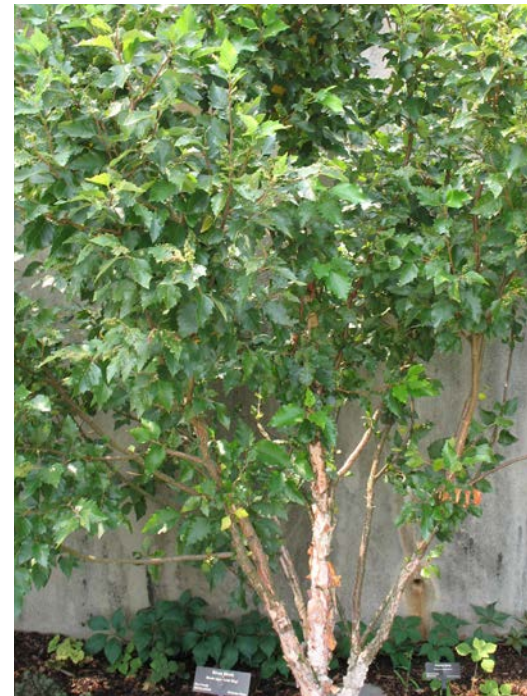
Plantings in this south facing area provide a welcoming and playful palette of light, texture and color reminiscent of a sunny meadow



## TREES



ULMUS PRPRINQUA 'JFS-BIEBERICH'  
EMERALD SUNSHINE ELM



BETULA NIGRA 'HERITAGE CULLY'  
HERITAGE RIVER BIRCH

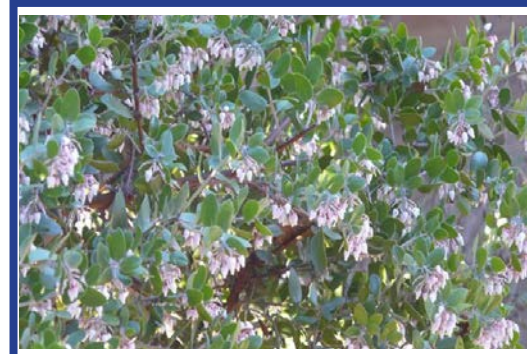
## UNDERSTORY



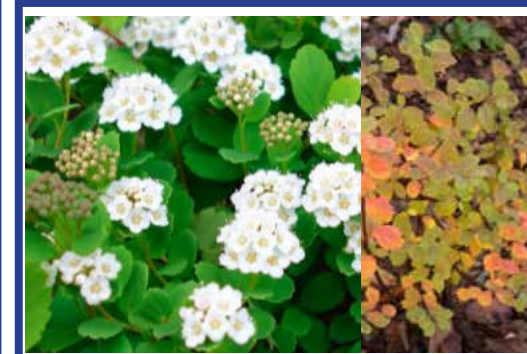
NW NATIVE



EVERGREEN



ARCTOSTAPHYLOS SUNSET  
MANZANITA



SPIRAEA BETULIFOLIA VAR. LUCIDA  
WHITE SPIRAEA



CALLUNA 'FLAMINGO'  
HEATHER



CALAMGROSTIS 'KARL FOERSTER'  
FEATHER REED GRASS



PENNISETUM ORIENTALE 'KARLEY ROSE'  
FOUNTAIN GRASS



FESTUCA 'ELIJAH BLUE'  
BLUE FESCUE



ACHILLEA M. 'SALMON BEAUTY'  
WILD YARROW

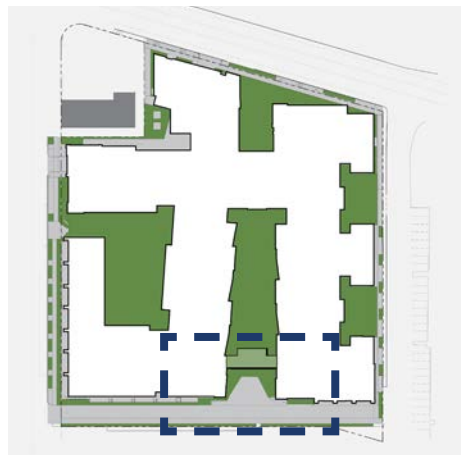


ERIGERON X GLAUCUS 'WAYNE'  
FLEABANE



IRIS TENAX  
TOUGHLEAF IRIS

NOT TO SCALE



# SOUTHERN THROUGH-BLOCK PEDESTRIAN CONNECTION

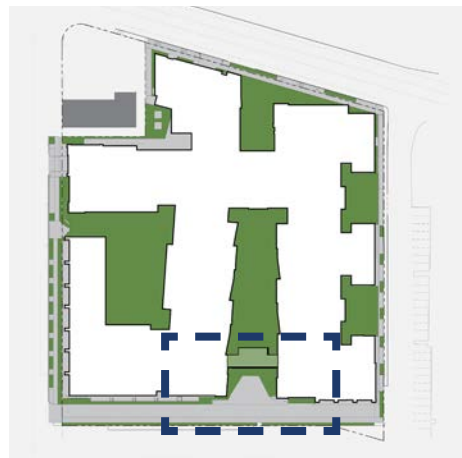
Courtyard F Detail Plan



Existing Condition



NOT TO SCALE

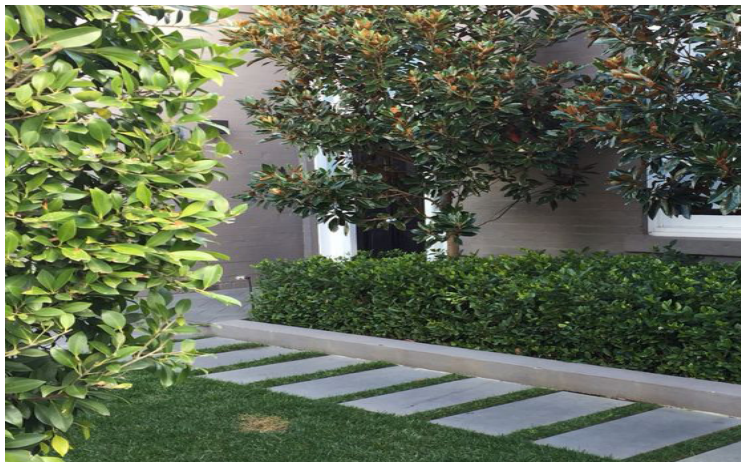


# 130TH AVE NE PEDESTRIAN GREEN STREET

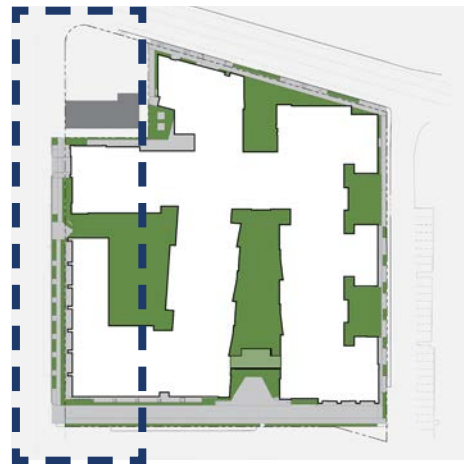
## WEST FOREST EDGE

### LUSH ISLANDS AND CLEAN CONNECTIONS

A mix of evergreen and native plantings provide an elegant entrance and a sense of comfortable lushness



NOT TO SCALE



## TREES



*ULMUS PRPRINQUA 'JFS-BIEBERICH'*  
EMERALD SUNSHINE ELM



*MAGNOLIA 'SWEETBAY'*  
SWEETBAY MAGNOLIA

## UNDERSTORY



NW NATIVE



EVERGREEN



*PAXISTIMA MYRSINITES*  
MOUNTAIN BOXWOOD



*DAPHNE 'ETERNAL FRAGRANCE'*  
DAPHNE



*CALLUNA 'FLAMINGO'*  
HEATHER



*POLYSTICHUM SETIFERUM*  
ALASKAN FERN



*ANEMONE 'HONORINE JOBERT'*  
JAPANESE ANEMONE



*BLECHNUM SPICANT*  
DEER FERN



*VANCOUVERIA HEXANDRA*  
INSIDE OUT FLOWER



*FRAGARIA CHILOENSIS*  
BEACH STRAWBERRY



*MAIANthemum DILATATUM*  
FALSE LILY

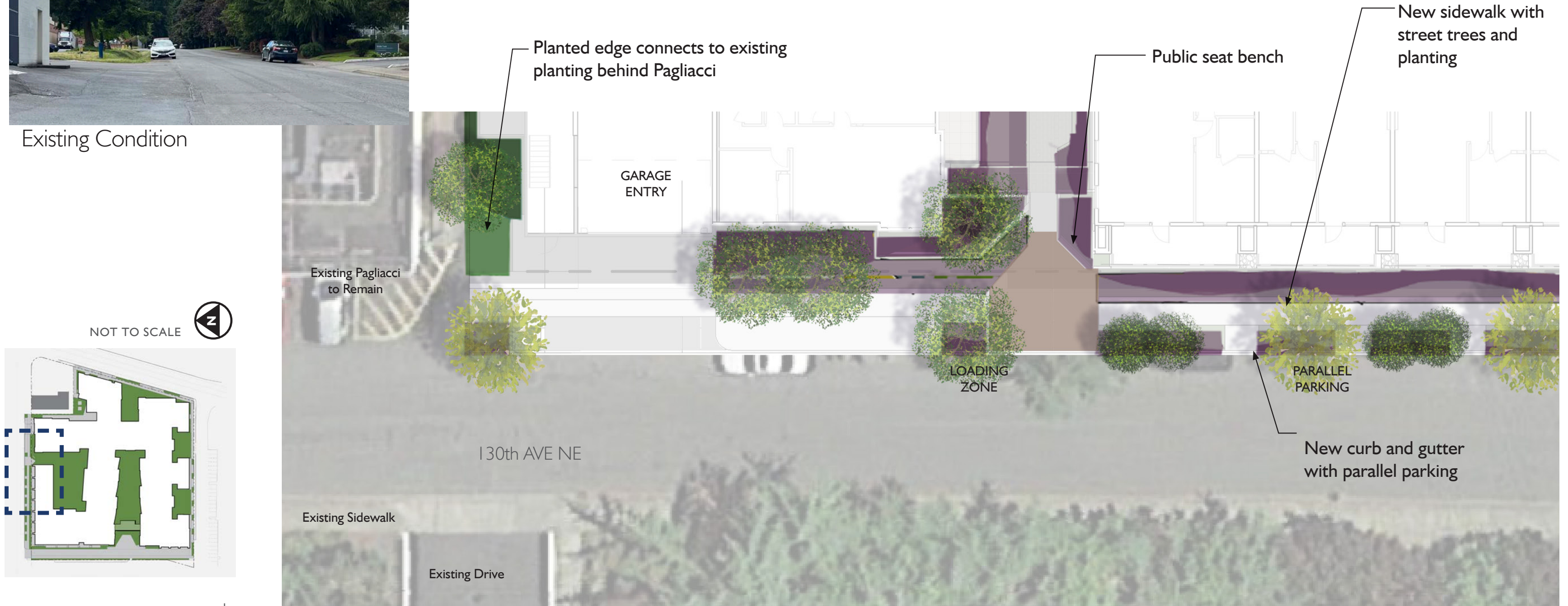


# 130TH AVE NE PEDESTRIAN GREEN STREET

## Private Courtyard Entry Detail Plan



Existing Condition





# BLANK WALL FACADE SCREENING

## FOREST EVERGREEN SCREENING

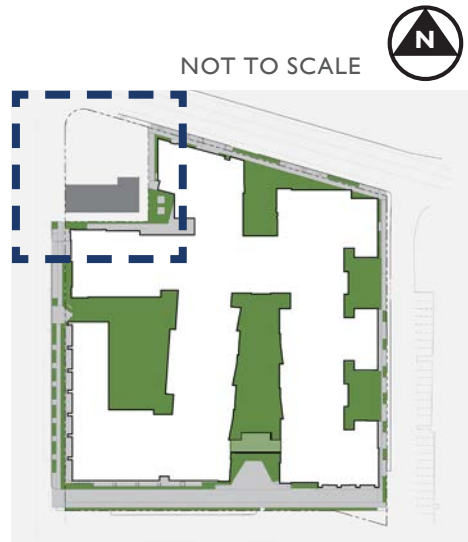
### VARIED EVERGREEN SCREEN

Tall evergreen shrubs mixed with full understory shrubs and pops of color enliven blank facades

### TREES



*CUPRESSUS SEMPERVIRENS*  
ITALIAN CYPRESS



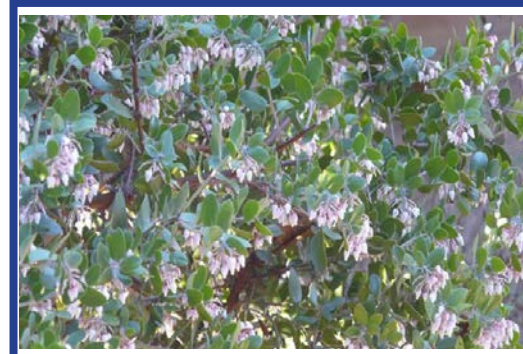
### UNDERSTORY



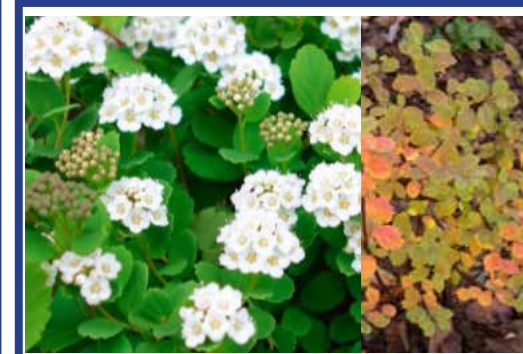
NW NATIVE



EVERGREEN



*ARCTOSTAPHYLOS SUNSET*  
MANZANITA



*SPIRAEA BETULIFOLIA VAR. LUCIDA*  
WHITE SPIRAEA



*CALLUNA 'FLAMINGO'*  
HEATHER



*GERANIUM OREGANUM*  
OREGON GERANIUM



*POLEMONIUM CAR. 'APRICOT DELIGHT'*  
JACOB'S LADDER



*FESTUCA 'ELIJAH BLUE'*  
BLUE FESCUE



*ACHILLEA M. 'SALMON BEAUTY'*  
WILD YARROW



*SALVIA 'AMETHYST LIPS'*  
ORNAMENTAL SAGE



*LAVANDULA 'PLATINUM BLONDE'*  
LAVENDER

## RESPONSE 5: SITE MATERIALS

---

# LANDSCAPE SITE MATERIALS PALETTE

The materials palette is simple, modern, warm, and bright with low maintenance and durable finishes.



*Rich wood finishes*



*Blackened metal*



*Stamped concrete brick pattern in Rustic Brown*



*Cast-in-place concrete sidewalks with select areas of special finish such as integral color or unique finish.*



*Accent areas of Rustic Brown integral color creates special moments for public engagement with seating or signage.*



*Horseshoes or horseshoe prints in pavement or walls*



# LANDSCAPE PUBLIC SITE FURNISHINGS



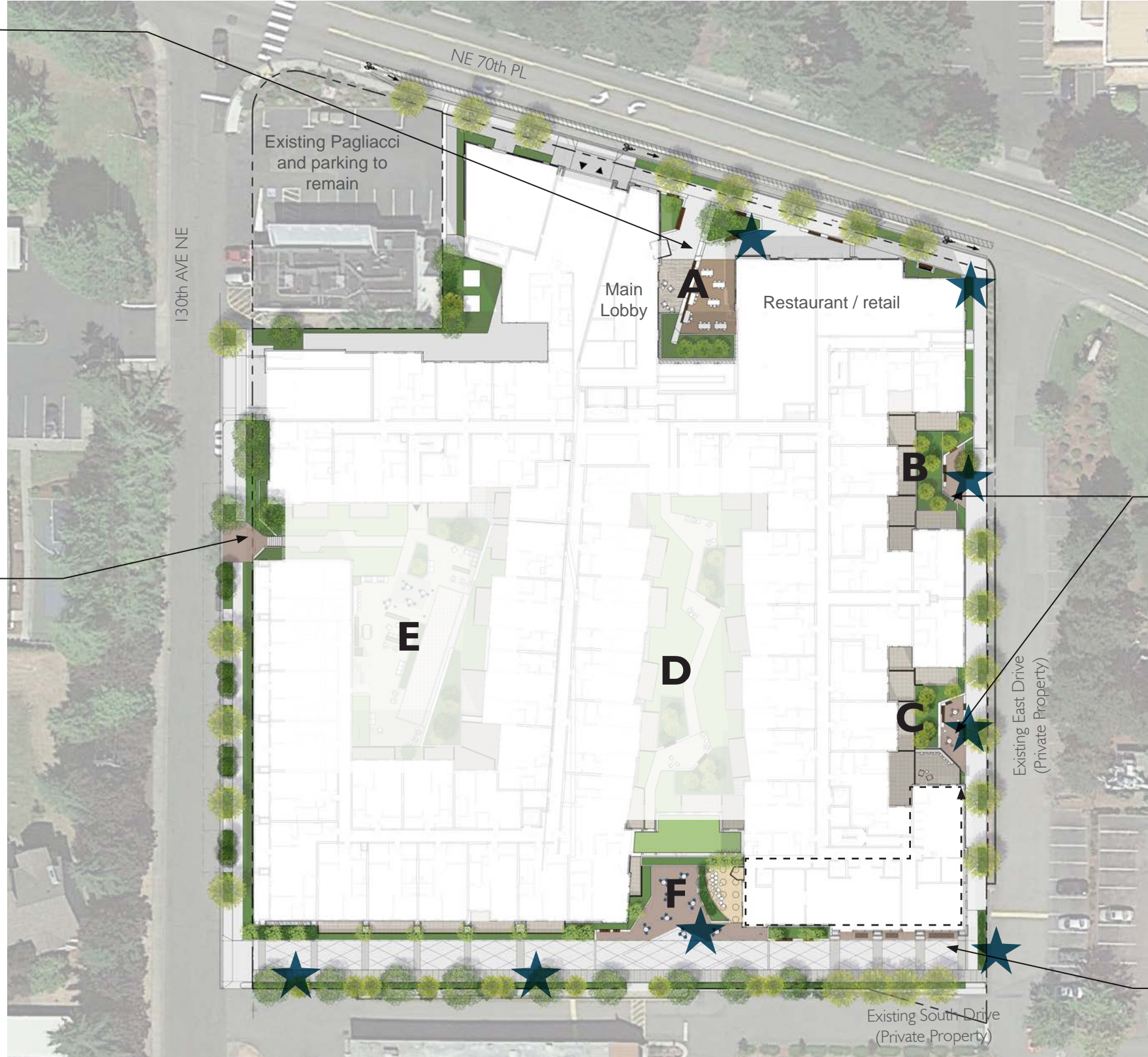
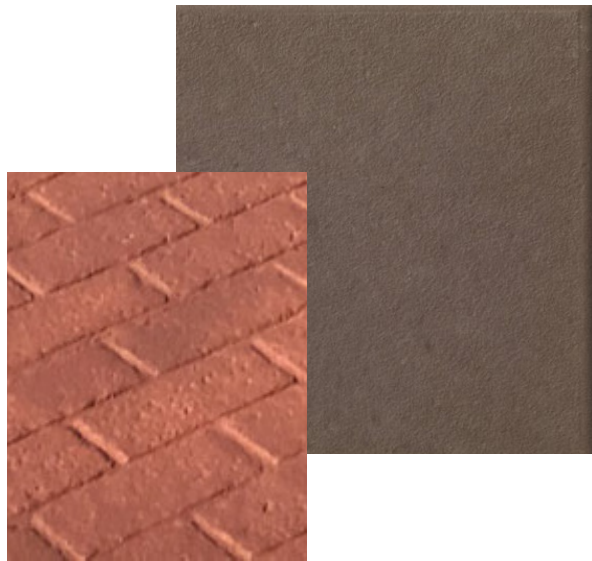
# MATERIALS PLAN AT GRADE (LEVELS PI & LI)

Main entry and retail courtyard distinguished with public wood deck to support retail, as well as accent paving at the residential entrance.

Interpretive signage will punctuate the area, along with horseshoes imbedded in paving



Colored and stamped paving at key pedestrian areas



  
Bridle Trails Past Present and Future

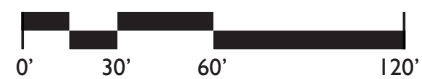


Colored and stamped paving at east courtyards

Site furnishings selected to attract multi-generations, and a variety of options.



Southern through-block area scored with diagonal accents, along with colored and stamped paving, creating the feeling of a promenade



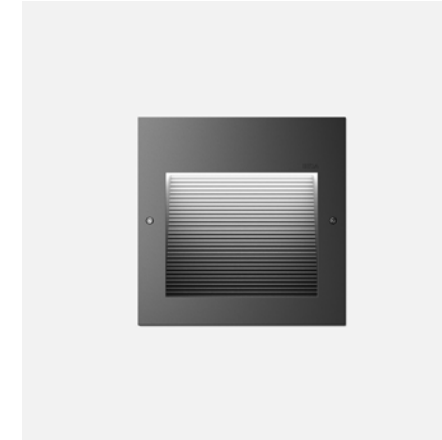
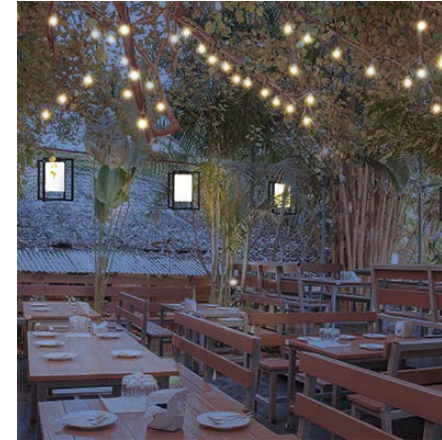
# LIGHT FIXTURES



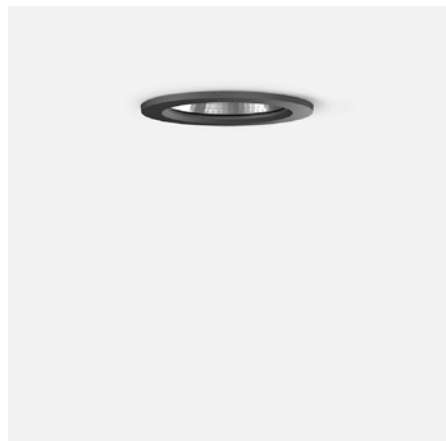
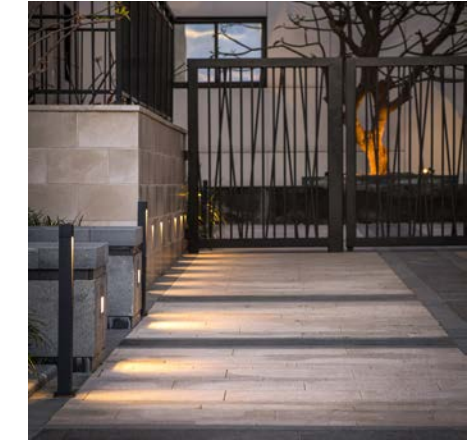
TYPE SA1 - BOLLARD



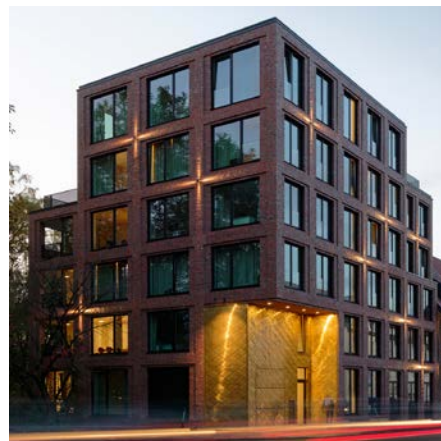
TYPE SB1 - CATENARY PENDANTS



TYPE SC1 - RECESSED STEPLIGHT



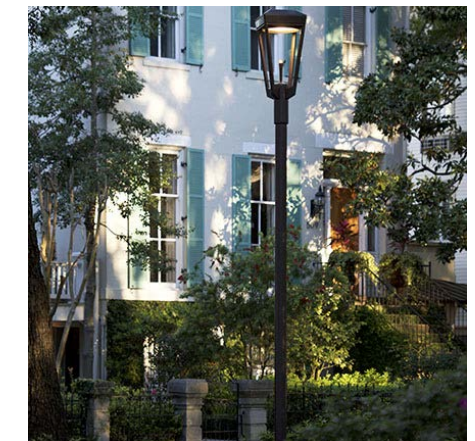
TYPE SD1 - RECESSED DOWNLIGHT



TYPE SF1 - GARDEN/STAKE MOUNTS



TYPE SH1 - PEDESTRIAN POLE



LUMINAIRE SCHEDULE													
TAG	DESCRIPTION	FINISH	LAMP			CCT	MANUFACTURER	MODEL	POWER SUPPLY		VOLTAGE	LOAD	MOUNTING
			TYPE	LUMENS	CRI				DRIVER	DIMMING			
SA1	SHIELDED BOLLARD	BLACK	LED	551	90+	2700K	BEGA	86066	INTEGRAL	0-10V	120V	7.9W	GROUND
SB1	CATENARY LUMINAIRE	BLACK	LED	1500	90+	2700K	VISA LIGHTING	LATERNA	INTEGRAL	0-10V	120V	18.0W	SUSPENDED
SC1	RECTANGULAR STEP LIGHT	BLACK	LED	488	90+	2700K	BEGA	24203	INTEGRAL	0-10V	120V	7.0W	RECESSED
SD1	ROUND RECESSED CEILING LUMINAIRE	BLACK	LED	2310	90+	2700K	BEGA	24820	INTEGRAL	0-10V	120V	20.0W	RECESSED
SF1	STAKE MOUNTED LANDSCAPE LUMINAIRE	BLACK	LED	150	90+	2700K	WAC	6011	INTEGRAL	0-10V	120V	9.8W	STAKE
SG1	LED STRIP LUMINAIRE	N/A	LED	84LM/FT	90+	2700K	QTRAN	ALTA	REMOTE	0-10V	24DC	2.0W/FT	SURFACE
SH1	PEDESTRIAN POLE TOP LUMINAIRE	BLACK	LED	4634	90+	2700K	LANDSCAPE FORMS	ASHBERY	INTEGRAL	0-10V	120V	78.0W	14-FT POLE
SJ1	UNIT PATIO LUMINAIRE	BLACK	LED	623	90+	3000K	MODERN FORMS	HAZE	INTEGRAL	ELV	120V	10.9W	WALL
SK1	EXTERIOR WALL LUMINAIRE	BLACK	LED	1200	90+	3000K	MODERN FORMS	TWO IF BY SEA	INTEGRAL	ELV	120V	15W	WALL



TYPE SJ1 - UNIT PATIO SCNCE

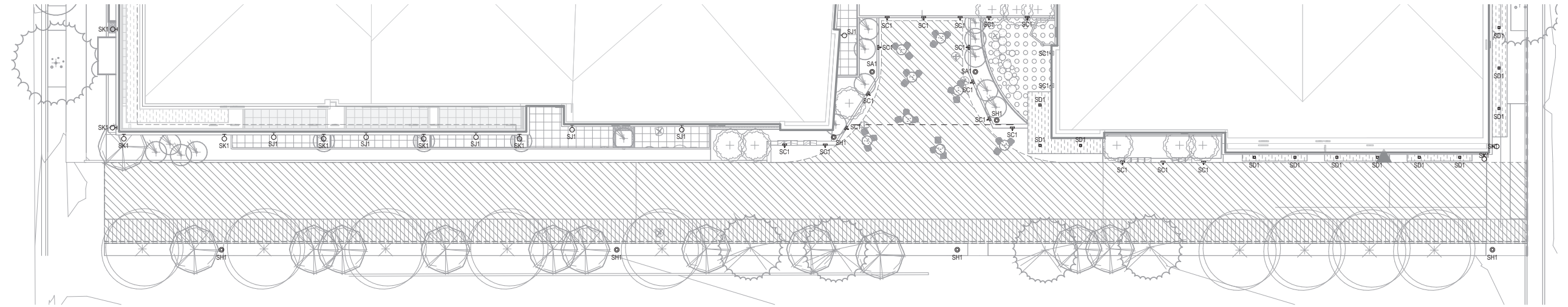
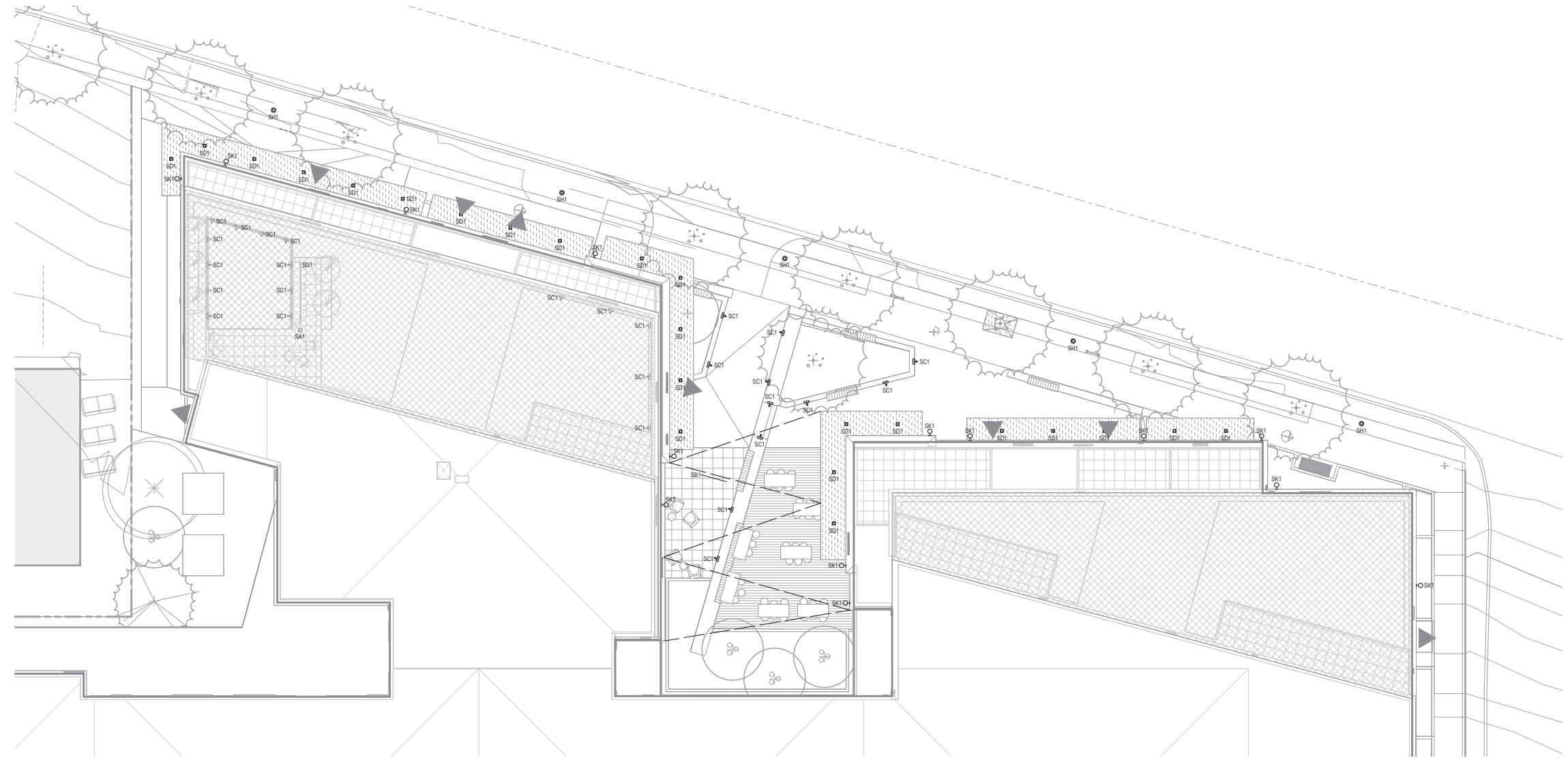


TYPE SK1 - EXTERIOR BUILDING SCNCE

## APPENDIX A: UPDATED INFORMATION

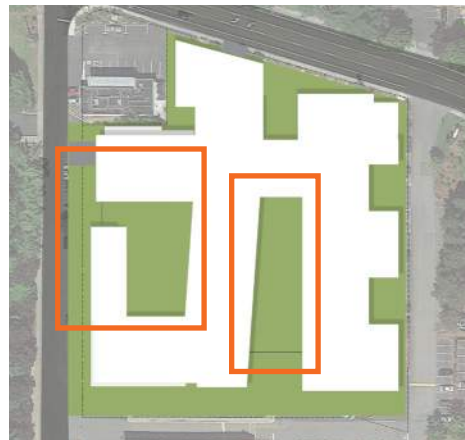
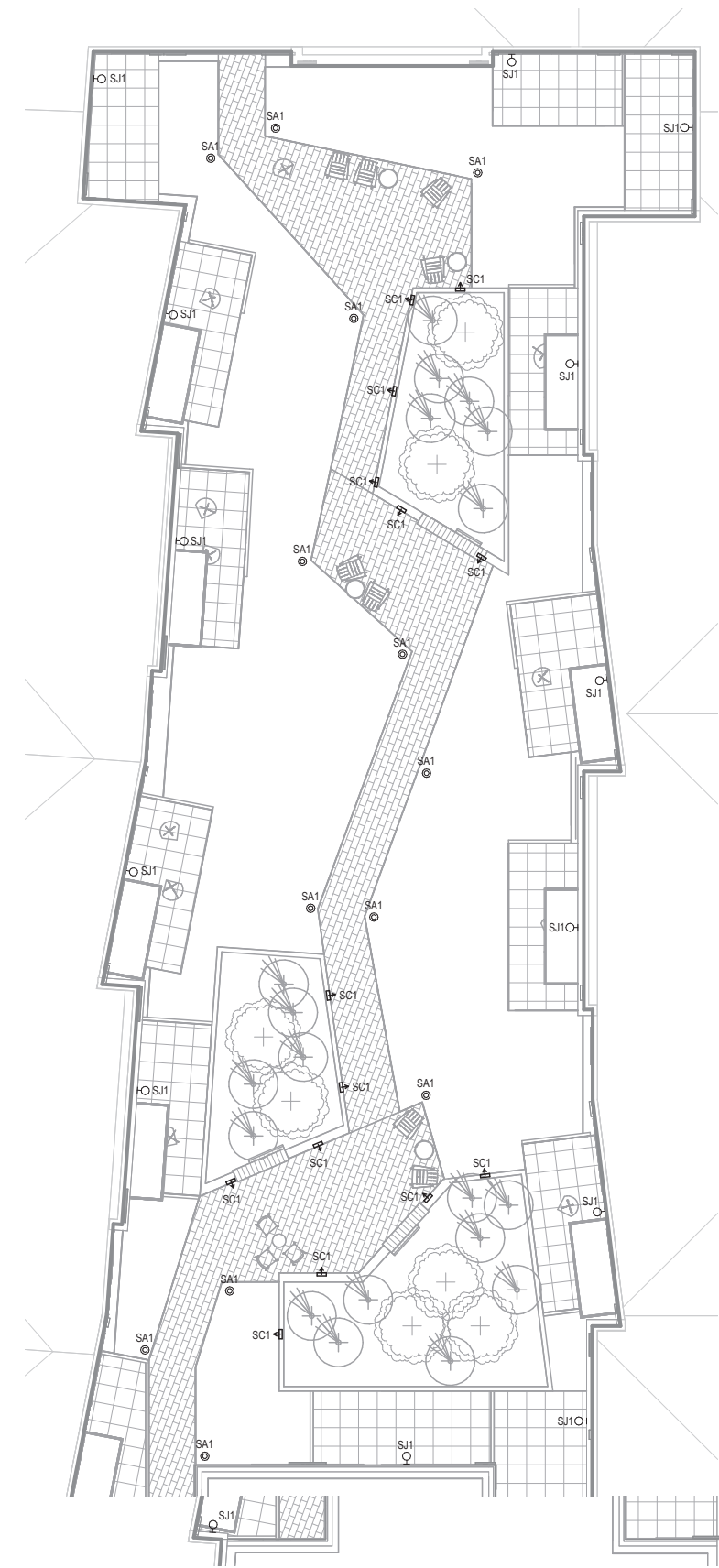
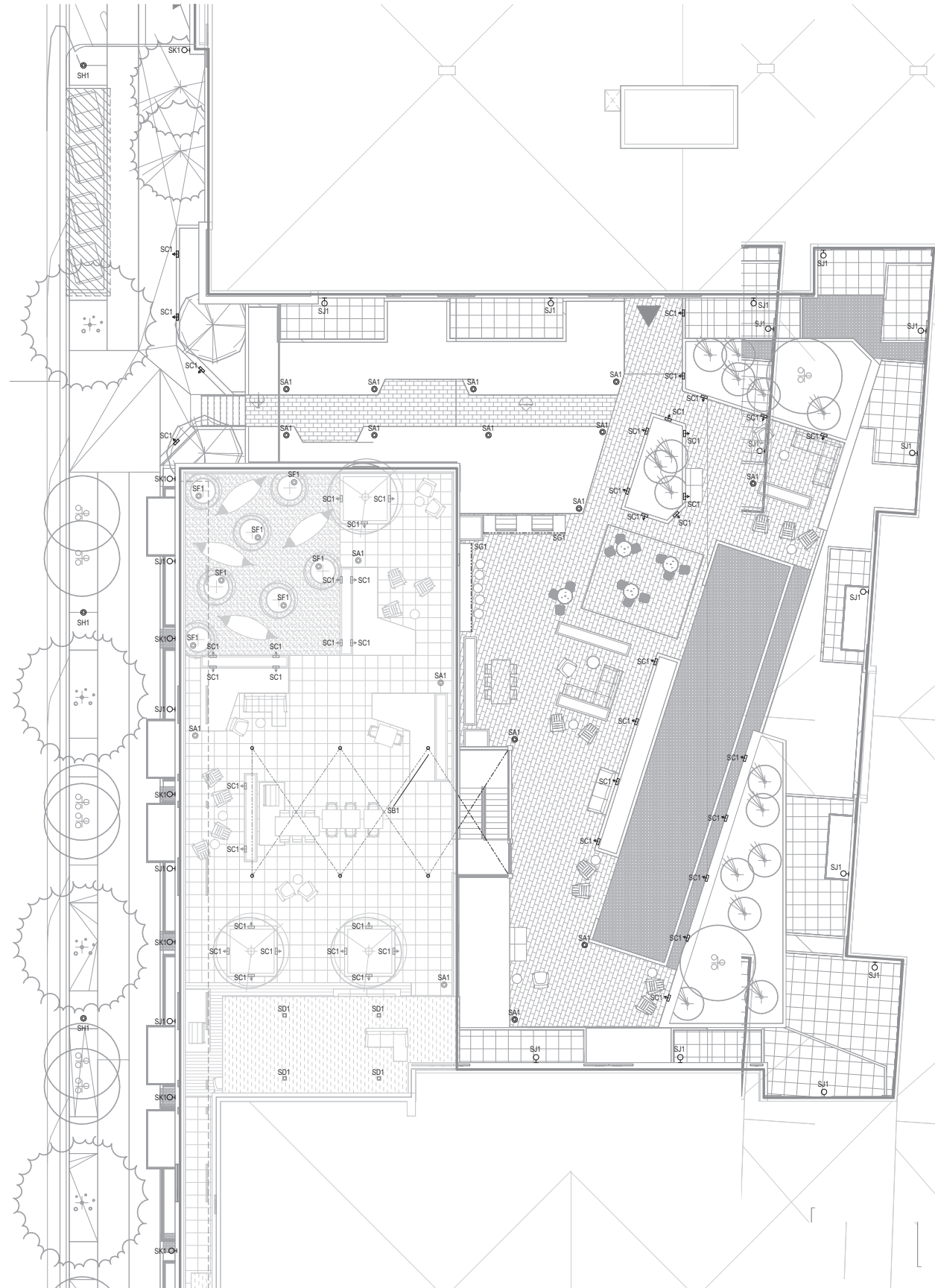
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# LIGHTING PLANS

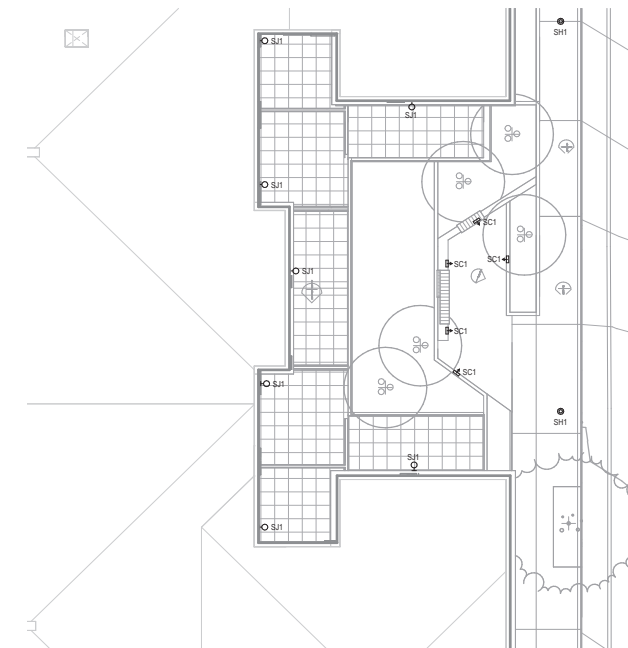
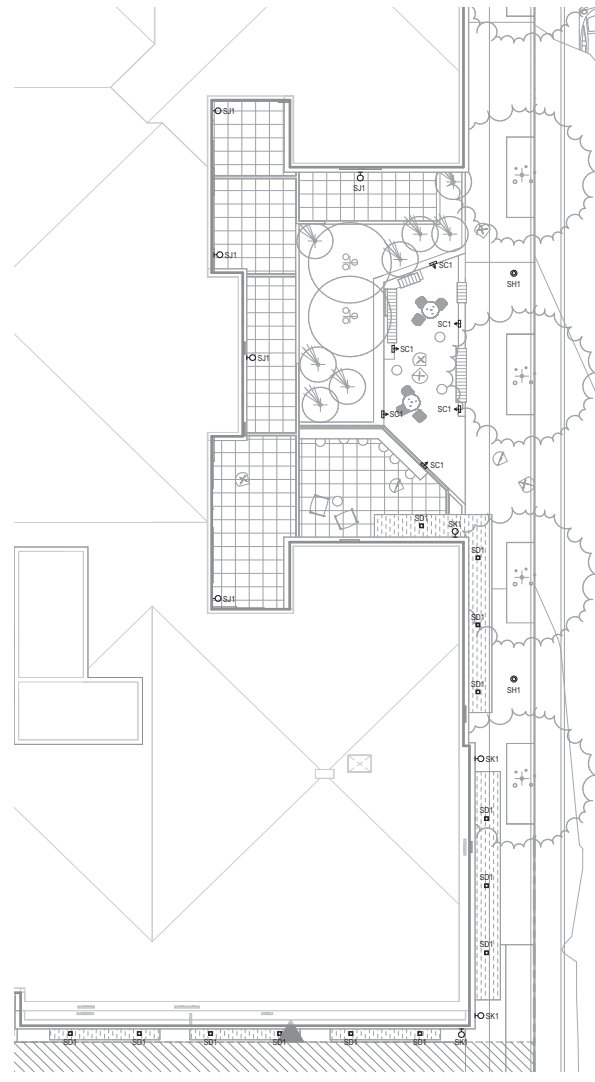
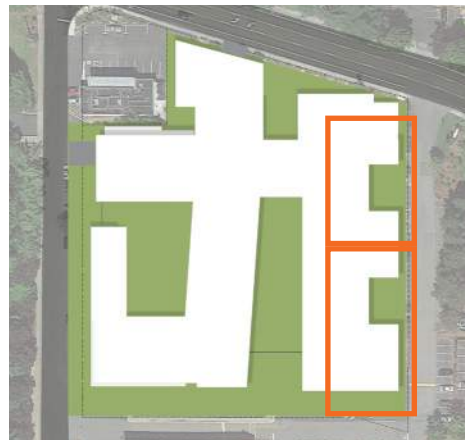




# LIGHTING PLANS



# LIGHTING PLANS



# OVERHEAD WEATHER PROTECTION COMPLIANCE (DRB RESPONSE 7)

## OVERHEAD WEATHER PROTECTION REQUIREMENTS

### KZC 92.15.2 - PEDESTRIAN-ORIENTED IMPROVEMENTS ON OR ADJACENT TO THE SUBJECT PROPERTY

2. PEDESTRIAN-ORIENTED SPACE AND PLAZAS IN BDC, CBD, BN, BNA, BCX, MSC 2, FHNC, HENC 1, HENC 3, NRHBD, RHBD AND TLBD ZONES
- A. IN THE CBD, BN, BNA, BCX, MSC 2, FHNC, HENC 1, HENC 3 OR IN BDC – IF THE SUBJECT PROPERTY ABUTS A PEDESTRIAN-ORIENTED STREET (SEE PLATE 34 IN CHAPTER 180 KZC) OR PUBLIC PARK, THE SPACE, IF ANY, BETWEEN THE SIDEWALK AND THE BUILDING MUST BE DEVELOPED CONSISTENT WITH THE FOLLOWING CRITERIA:...

### KZC PLATE 34Q - PEDESTRIAN CIRCULATION IN BCX

IDENTIFIES NE 70TH AS A PEDESTRIAN ORIENTED STREET.

### KZC 105.18.3.B - PEDESTRIAN ACCESS - OVERHEAD WEATHER PROTECTION

OVERHEAD WEATHER PROTECTION – LOCATION – THE APPLICANT SHALL PROVIDE PEDESTRIAN OVERHEAD WEATHER PROTECTION IN THE FOLLOWING LOCATIONS:

- 1) ALONG ANY PORTION OF THE BUILDING WHICH IS ADJACENT TO A PEDESTRIAN WALKWAY OR SIDEWALK;
- 2) OVER THE PRIMARY EXTERIOR ENTRANCE TO ALL BUILDINGS INCLUDING RESIDENTIAL UNITS.
- 3) EXCEPTIONS IN DESIGN DISTRICTS:  
IN RHBD, BN, BNA, BCX, MSC 2, FHNC, HENC 1, HENC 3 AND TLBD ZONES:  
ALONG AT LEAST 75 PERCENT OF A PEDESTRIAN-ORIENTED BUILDING FACADE.

### KZC 105.18.3.C - PEDESTRIAN ACCESS - OVERHEAD WEATHER PROTECTION

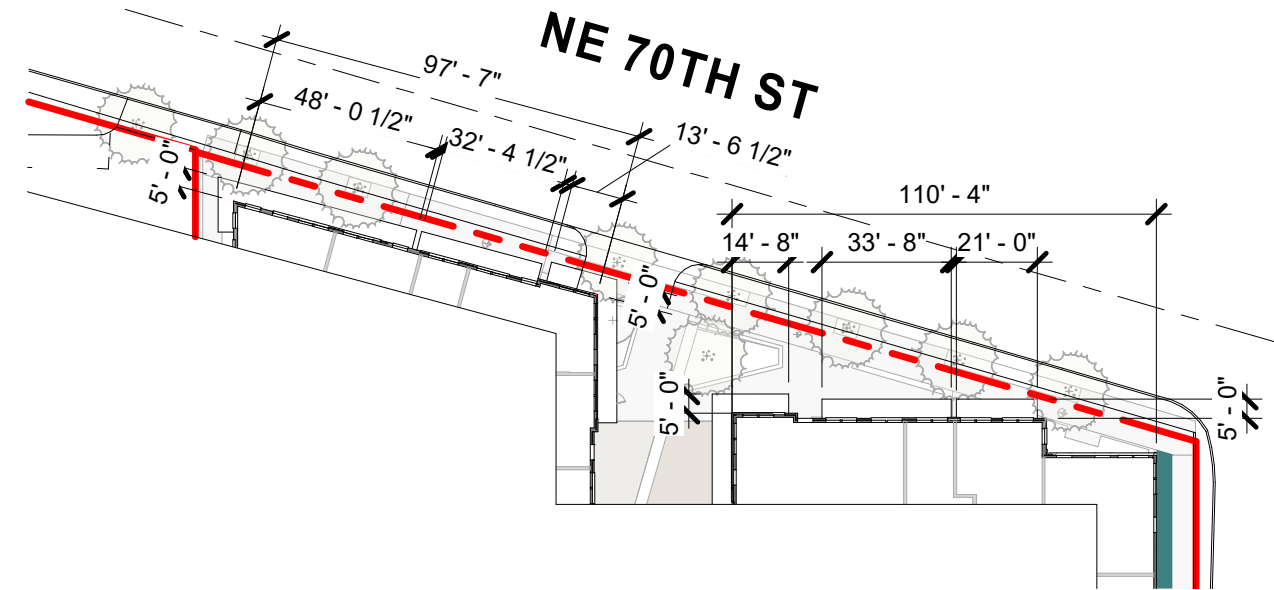
OVERHEAD WEATHER PROTECTION MUST COVER AT LEAST FIVE (5) FEET OF THE WIDTH OF THE ADJACENT WALKWAY AND MUST BE AT LEAST EIGHT (8) FEET ABOVE THE GROUND IMMEDIATELY BELOW IT.

## OVERHEAD WEATHER PROTECTION PROVIDED

### COMPLIANCE ALONG NE 70TH ST:

FRONTAGE: 97'-7" + 110'-4" = 207'-11"  
 COVERAGE: 48'-1/2" + 32'-4 1/2" + 13'-6 1/2" + 14'-8" + 33'-8" + 21'-0" = 163'-3 1/5"

$$\frac{163'-3 \frac{1}{5}"}{207'-11"} = 78.5\% > 75\% = \text{COMPLIANT}$$



KZC 92.15.2 and KZC Plate 34 Q identify NE 70th as a Pedestrian Oriented Street where overhead weather protection is required. The project complies by providing 5' deep canopies over the adjacent walking surfaces for at least 75% of the building facade.

# SETBACK AND STEPBACK COMPLIANCE: 130TH AVE NE

## STEP BACK @130TH AVE NE

### KZC 35.10.040.2.d.2 AND d.3 - UPPER STORY STEPBACK COMPLIANCE CALCULATIONS -

FOR ALL BUILDING FACADES FACING AND WITHIN 100 FEET OF THE ABUTTING RIGHT-OF-WAY, ALL PORTIONS OF A STRUCTURE GREATER THAN 3 STORIES IN HEIGHT, AS MEASURED FROM THE ABUTTING RIGHT-OF-WAY, SHALL BE STEPPED BACK FROM THE THIRD STORY BY AN AVERAGE OF 12 FEET (FROM 130TH AVE NE). THE REQUIRED UPPER STORY STEPBACKS FOR ALL FLOORS ABOVE THE THIRD STORY SHALL BE CALCULATED AS TOTAL UPPER STORY STEP BACK AREA AS FOLLOWS:

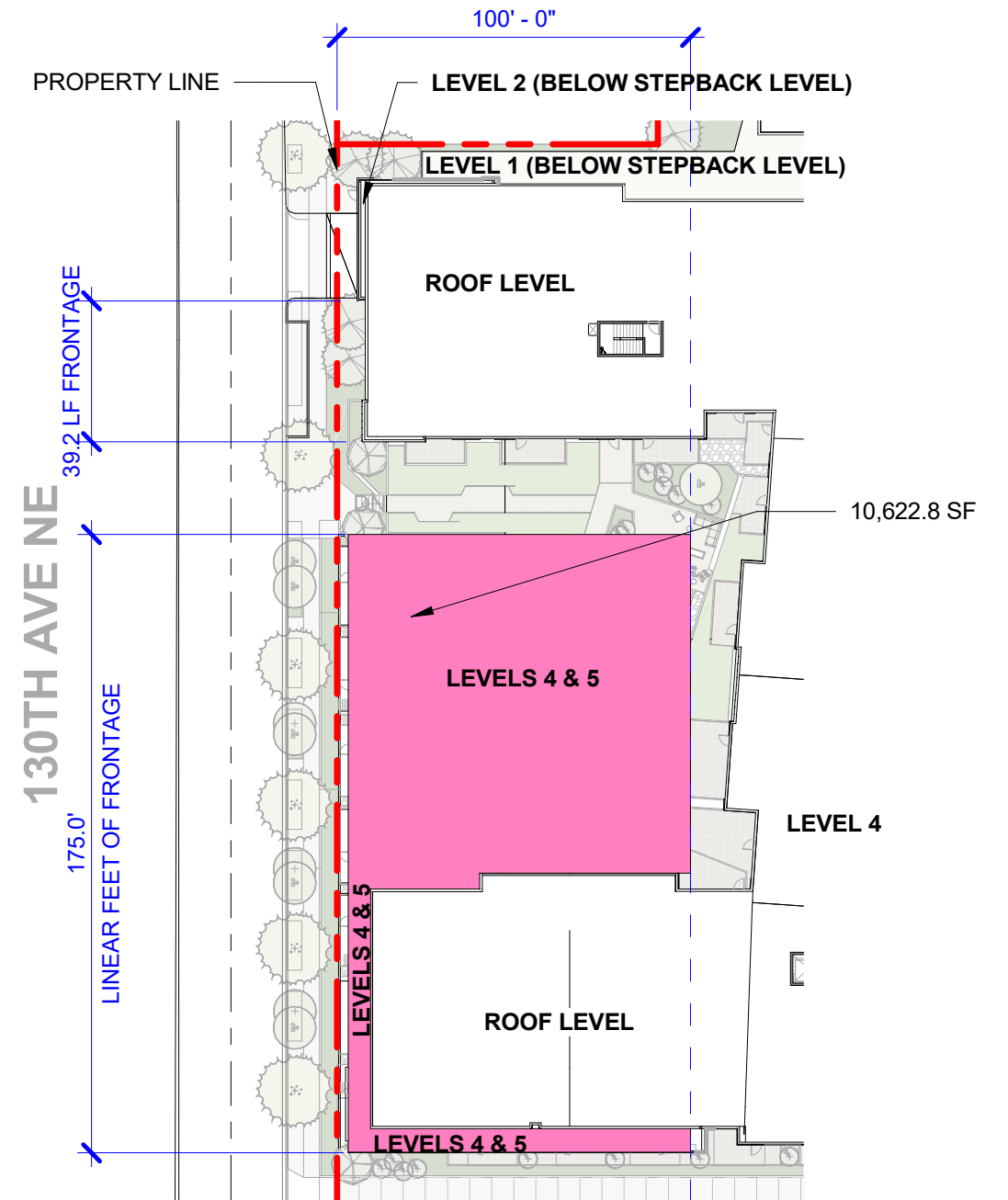
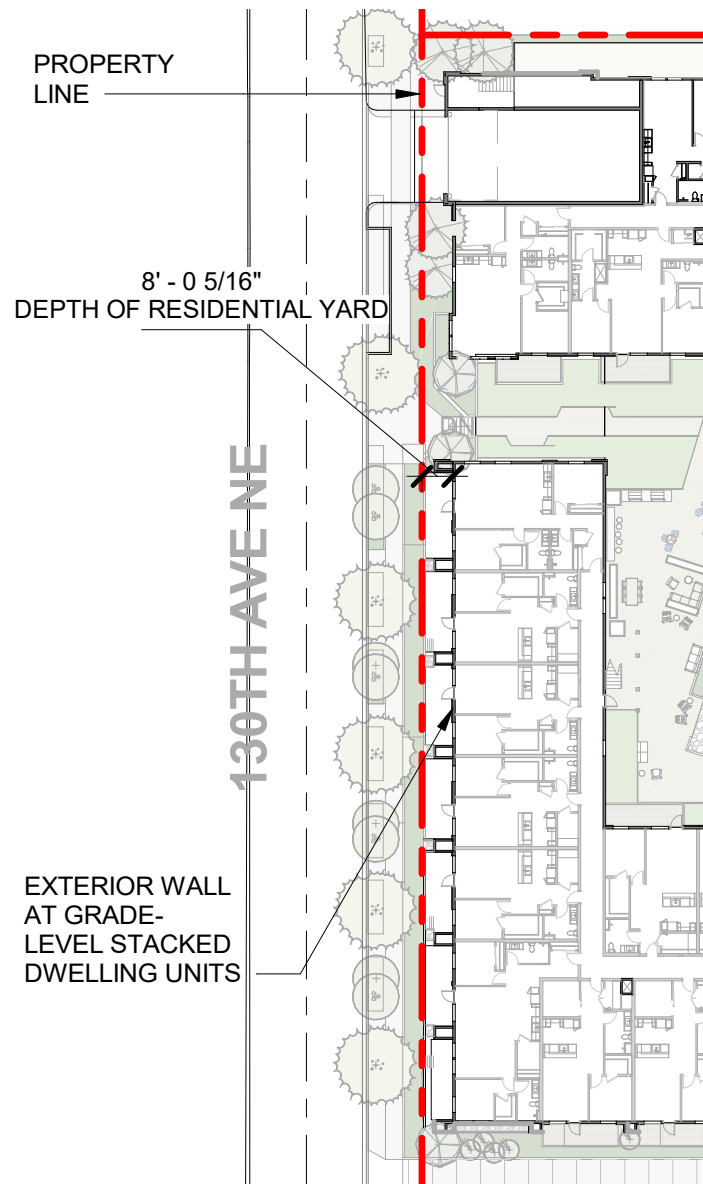
REQUIRED UPPER STORY STEP BACK AREA = LINEAR FEET OF FRONTAGE (NOT INCL PORTIONS OF THE SITE WITHOUT BUILDINGS OR FOR VEHICULAR AREAS) \* REQUIRED AVERAGE SETBACK \* NUMBER OF STORIES ABOVE THIRD STORY  
 = (174.9 FT + 39.2 FT) \* 12 FT \* 2  
 = 5,148 SF

PROVIDED STEP BACK AT LEVELS 4 & 5 = STEP BACK @ LEVEL 4 + STEP BACK @ LEVEL 5  
 = 10,622.8 SF + 10,622.8 SF  
 = **21,245.2 SF > 5,148 SF, MEETS REQUIREMENT**

### KZC 35.10.040.2.n YARD COMPLIANCE CALCULATIONS -

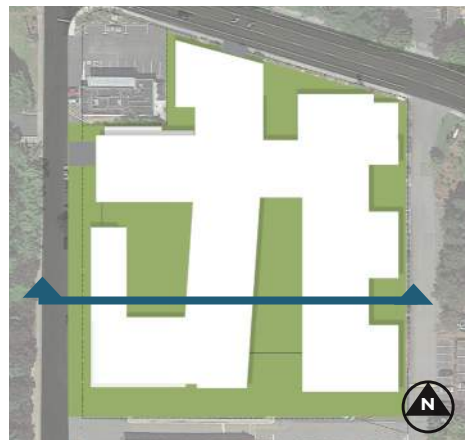
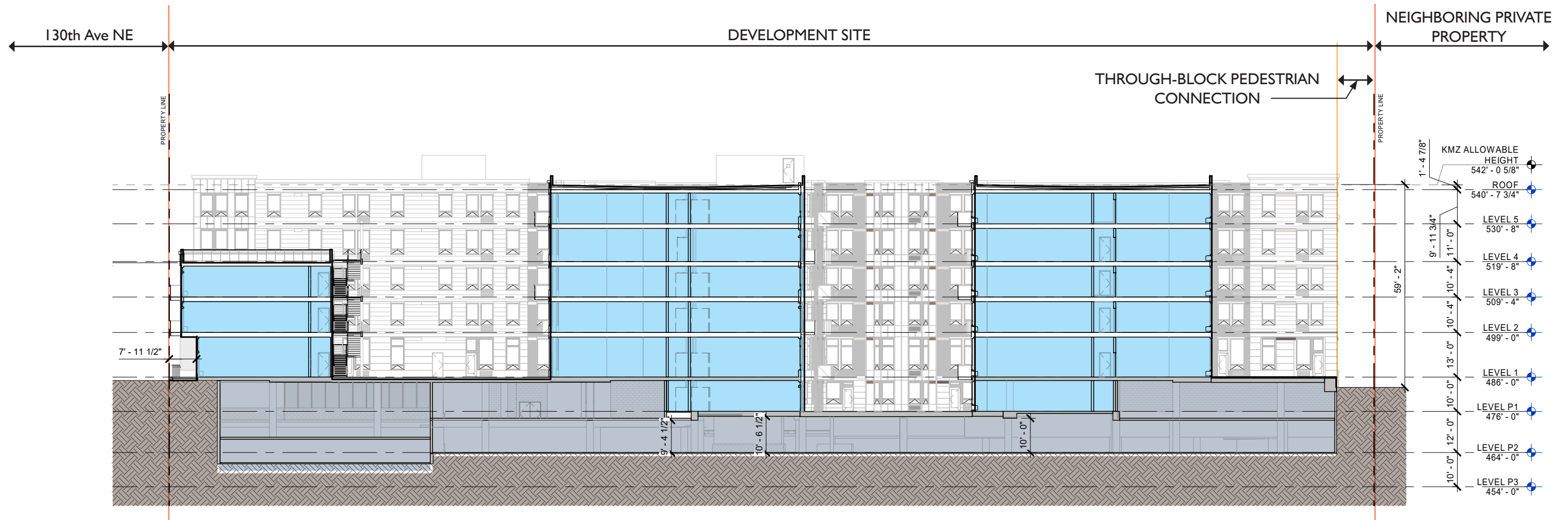
ATTACHED OR STACKED DWELLING UNITS LOCATED AT THE STREET LEVEL FLOOR ALONG 130TH AVENUE NE SHALL HAVE A MINIMUM 7 FOOT REQUIRED YARD. PER PLAN 2C/G1.02,

YARD PROVIDED = **8' - 0 5/16" > 7 FEET, MEETS REQUIREMENT**



The design team has reviewed the setback along 130th with city officials and confirmed the project is compliant as shown with pilasters, unit patios, and similar exterior detail elements within the setback at grade.

# EAST-WEST BUILDING SECTION



- COMMERCIAL
- ACTIVE USE / RESIDENTIAL AMENITY
- RESIDENTIAL LOBBY
- RESIDENTIAL APARTMENT HOMES
- PARKING, MECHANICAL, AND BACK OF HOUSE



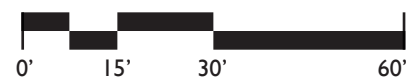
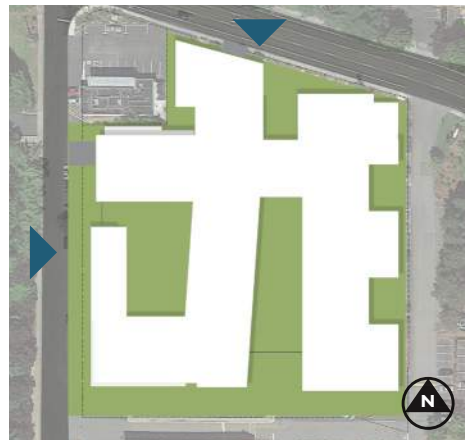
The design team reviewed the balcony and property line condition along I 30th with city officials. The design team has reduced the size of the balconies to ensure they do not cross the property line.

# BUILDING ELEVATIONS: STREETS

NORTH ELEVATION: NE 70th PLACE



WEST ELEVATION: I 30th AVE NE

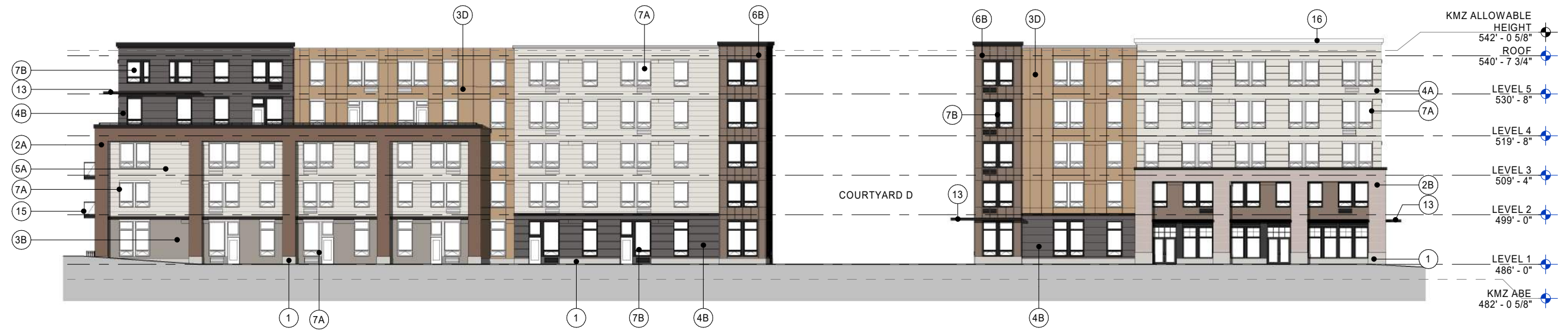


## MATERIAL LEGEND

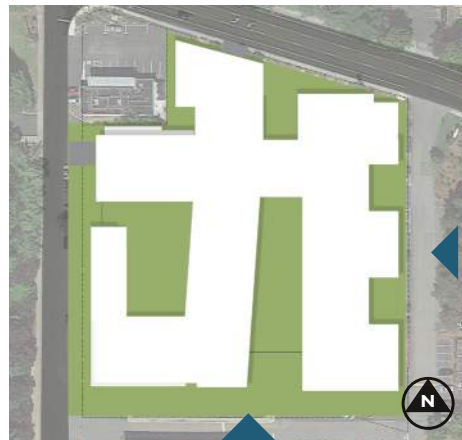
- |                                |   |                               |                                   |
|--------------------------------|---|-------------------------------|-----------------------------------|
| ① Concrete, Cast in Place      | ③D Fiber Cement Panel: Color D                        | ⑥A Wood-Look Panel            | ⑬ Metal and Glass Canopy          |
| ②A Masonry: Dark Color         | ④A Horizontally Expressed Fiber Cement Panel: Color A | ⑥B Wood-Look Board and Batten | ⑭ Metal and Glass Guardrail       |
| ②B Masonry: Light Color        | ④B Horizontally Expressed Fiber Cement Panel: Color B | ⑦A Vinyl Windows: White       | ⑮ Metal and Glass Bolt-on Balcony |
| ③A Fiber Cement Panel: Color A | ⑤ Fiber Cement Plank                                  | ⑦B Vinyl Windows: Black       | ⑯ Cornice                         |
| ③B Fiber Cement Panel: Color B |   | ⑧ French Doors                | ⑰ Roll Up Garage Door             |
| ③C Fiber Cement Panel: Color C |   | ⑨ Aluminum Storefront         |                                   |

# BUILDING ELEVATIONS: THROUGH-BLOCK CONNECTIONS

## SOUTH ELEVATION: THROUGH-BLOCK CONNECTION



## EAST ELEVATION: THROUGH-BLOCK CONNECTION



### MATERIAL LEGEND

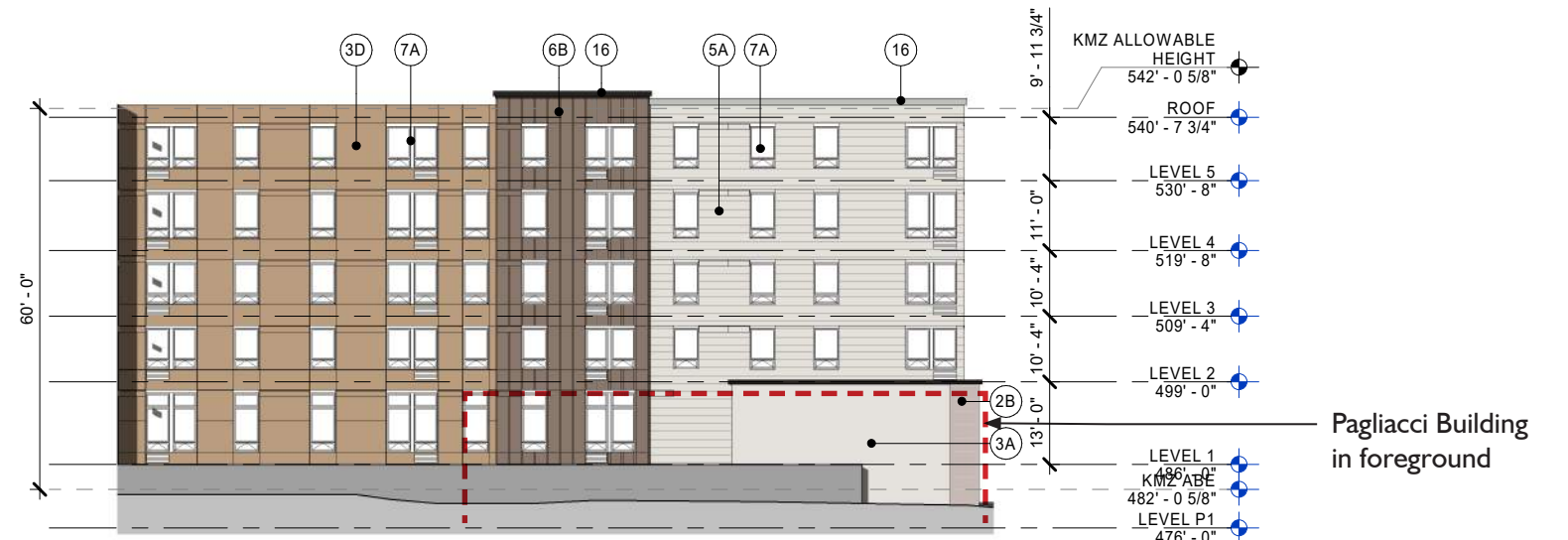
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| ① Concrete, Cast in Place      | ③D Fiber Cement Panel: Color D                      | ⑥A Wood-Look Panel            | ⑬ Metal and Glass Canopy          |
| ②A Masonry: Dark Color         | ④A Horizontally Exposed Fiber Cement Panel: Color A | ⑥B Wood-Look Board and Batten | ⑭ Metal and Glass Guardrail       |
| ②B Masonry: Light Color        | ④B Horizontally Exposed Fiber Cement Panel: Color B | ⑦A Vinyl Windows: White       | ⑮ Metal and Glass Bolt-on Balcony |
| ③A Fiber Cement Panel: Color A | ⑤ Fiber Cement Plank                                | ⑦B Vinyl Windows: Black       | ⑯ Cornice                         |
| ③B Fiber Cement Panel: Color B |   | ⑧ French Doors                | ⑰ Roll Up Garage Door             |
| ③C Fiber Cement Panel: Color C |   | ⑨ Aluminum Storefront         |                                   |

# BUILDING ELEVATIONS: ADDITIONAL NORTHERN ELEVATIONS

WEST ELEVATION: FACING PAGLIACCI SITE



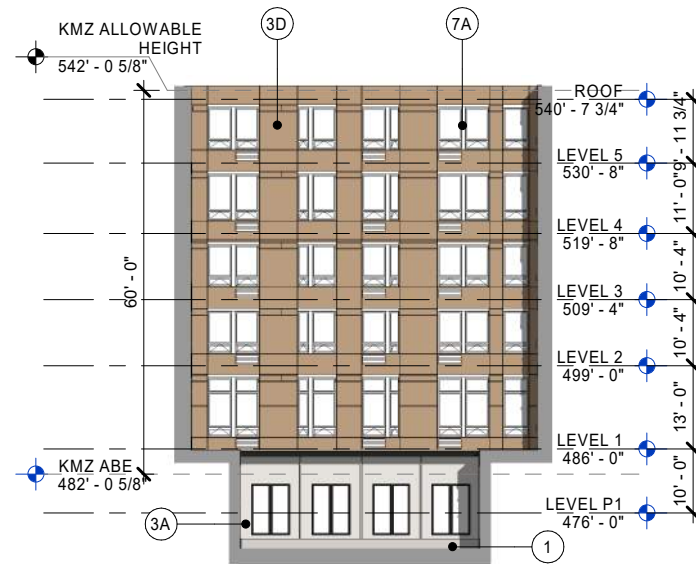
NORTH ELEVATION: FACING PAGLIACCI SITE



EAST ELEVATION: PLAZA A



SOUTH ELEVATION: PLAZA A



WEST ELEVATION: PLAZA A



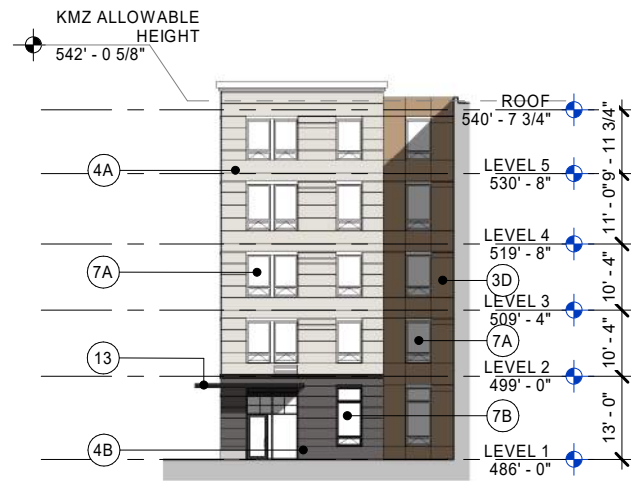
## MATERIAL LEGEND

- ① Concrete, Cast in Place
- ②A Masonry: Dark Color
- ②B Masonry: Light Color
- ③A Fiber Cement Panel: Color A
- ③B Fiber Cement Panel: Color B
- ③C Fiber Cement Panel: Color C
- ③D Fiber Cement Panel: Color D
- ④A Horizontally Expressed Fiber Cement Panel: Color A
- ④B Horizontally Expressed Fiber Cement Panel: Color B
- ⑤ Fiber Cement Plank
- ⑥A Wood-Look Panel
- ⑥B Wood-Look Board and Batten
- ⑦A Vinyl Windows: White
- ⑦B Vinyl Windows: Black
- ⑧ French Doors
- ⑨ Aluminum Storefront
- ⑬ Metal and Glass Canopy
- ⑭ Metal and Glass Guardrail
- ⑮ Metal and Glass Bolt-on Balcony
- ⑯ Cornice
- ⑰ Roll Up Garage Door



# BUILDING ELEVATIONS: EASTERN COURTYARDS C & B

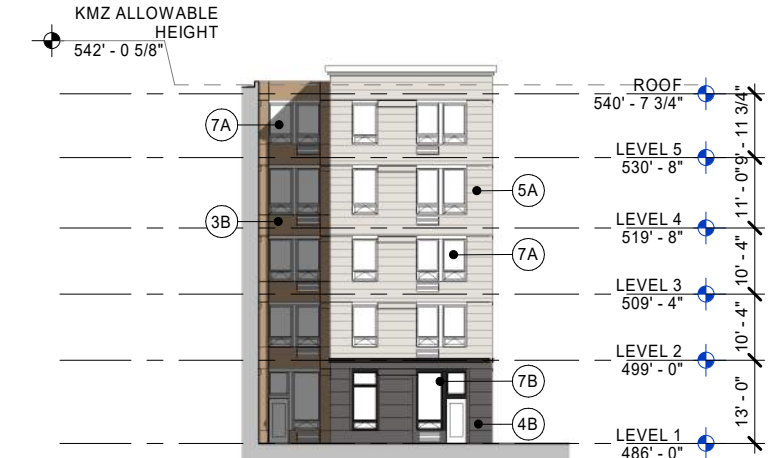
SOUTH ELEVATION: COURTYARD C



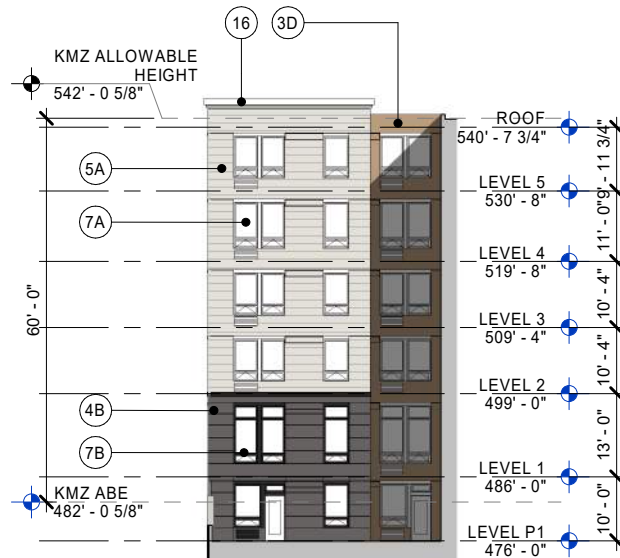
WEST ELEVATION: COURTYARD C



NORTH ELEVATION: COURTYARD C



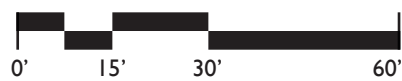
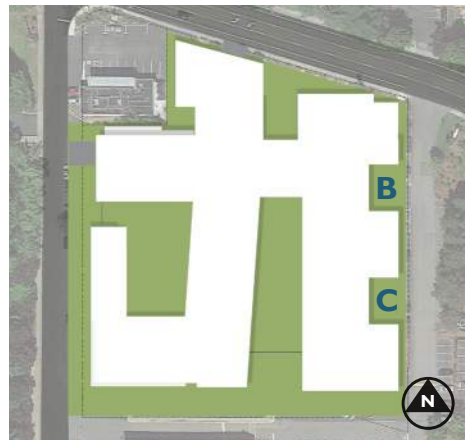
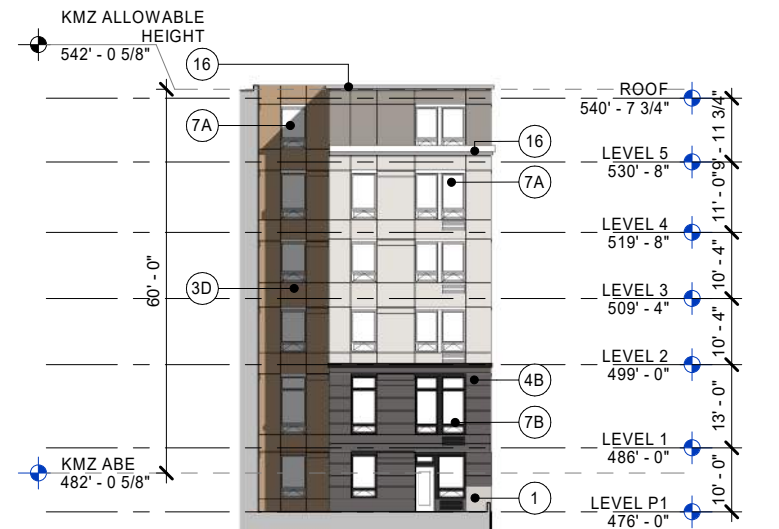
SOUTH ELEVATION: COURTYARD B



WEST ELEVATION: COURTYARD B



NORTH ELEVATION: COURTYARD B

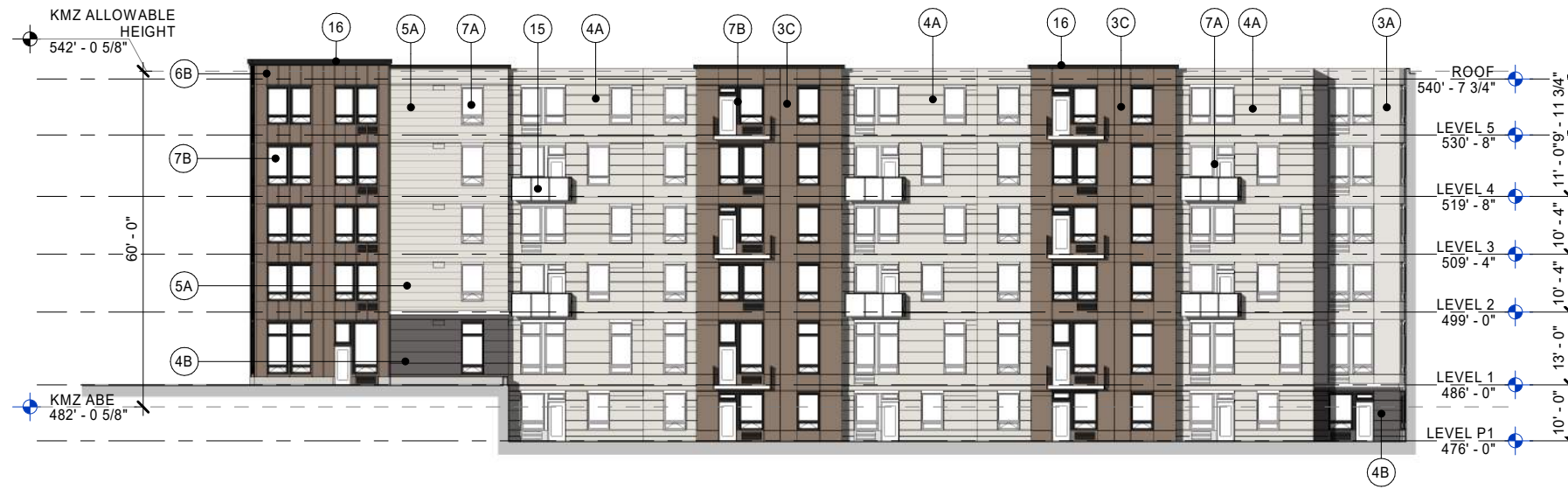


## MATERIAL LEGEND

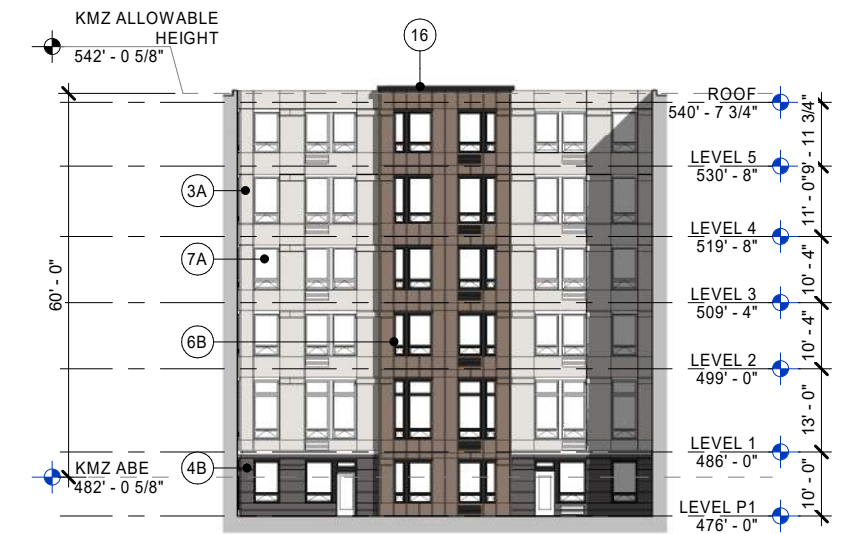
- |                                |   |                               |                                   |
|--------------------------------|---|-------------------------------|-----------------------------------|
| ① Concrete, Cast in Place      | ③D Fiber Cement Panel: Color D                        | ⑥A Wood-Look Panel            | ⑬ Metal and Glass Canopy          |
| ②A Masonry: Dark Color         | ④A Horizontally Expressed Fiber Cement Panel: Color A | ⑥B Wood-Look Board and Batten | ⑭ Metal and Glass Guardrail       |
| ②B Masonry: Light Color        | ④B Horizontally Expressed Fiber Cement Panel: Color B | ⑦A Vinyl Windows: White       | ⑮ Metal and Glass Bolt-on Balcony |
| ③A Fiber Cement Panel: Color A | ⑤ Fiber Cement Plank                                  | ⑦B Vinyl Windows: Black       | ⑯ Cornice                         |
| ③B Fiber Cement Panel: Color B |   | ⑧ French Doors                | ⑰ Roll Up Garage Door             |
| ③C Fiber Cement Panel: Color C |   | ⑨ Aluminum Storefront         |                                   |

# BUILDING ELEVATIONS: SOUTHERN COURTYARD D

COURTYARD D WEST ELEVATION



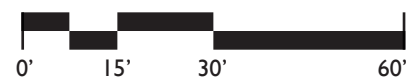
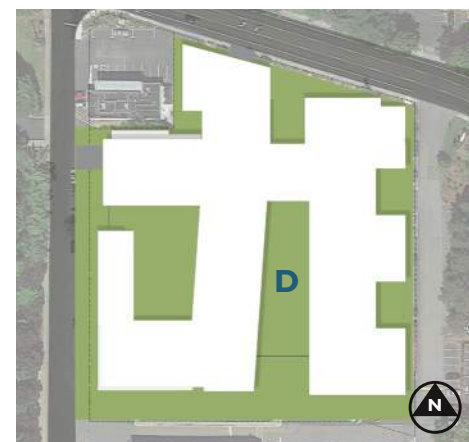
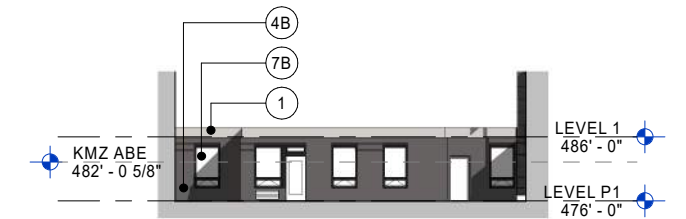
COURTYARD D NORTH ELEVATION



COURTYARD D EAST ELEVATION



COURTYARD D SOUTH ELEVATION



**MATERIAL LEGEND**

- ① Concrete, Cast in Place
- ②A Masonry: Dark Color
- ②B Masonry: Light Color
- ③A Fiber Cement Panel: Color A
- ③B Fiber Cement Panel: Color B
- ③C Fiber Cement Panel: Color C
- ③D Fiber Cement Panel: Color D
- ④A Horizontally Expressed Fiber Cement Panel: Color A
- ④B Horizontally Expressed Fiber Cement Panel: Color B
- ⑤ Fiber Cement Plank
- ⑥A Wood-Look Panel
- ⑥B Wood-Look Board and Batten
- ⑦A Vinyl Windows: White
- ⑦B Vinyl Windows: Black
- ⑧ French Doors
- ⑨ Aluminum Storefront
- ⑬ Metal and Glass Canopy
- ⑭ Metal and Glass Guardrail
- ⑮ Metal and Glass Bolt-on Balcony
- ⑯ Cornice
- ⑰ Roll Up Garage Door

# BUILDING ELEVATIONS: WESTERN COURTYARD E

COURTYARD E: NORTH ELEVATION



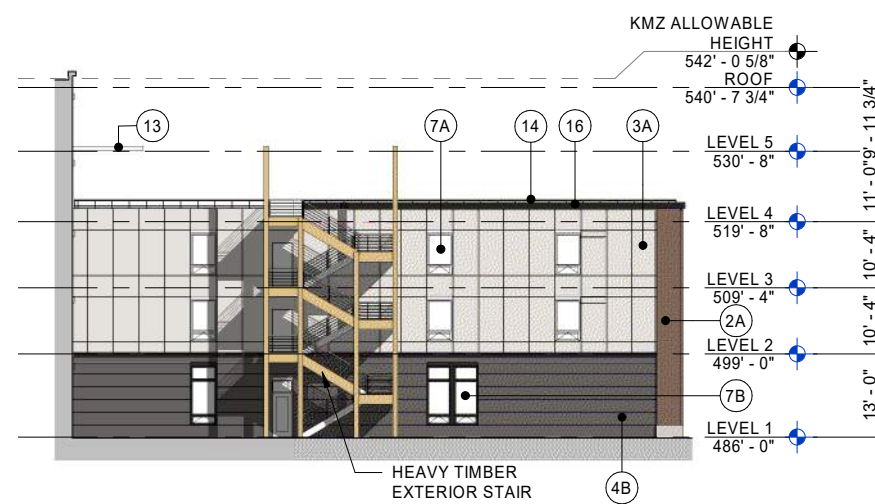
COURTYARD E: EAST ELEVATION



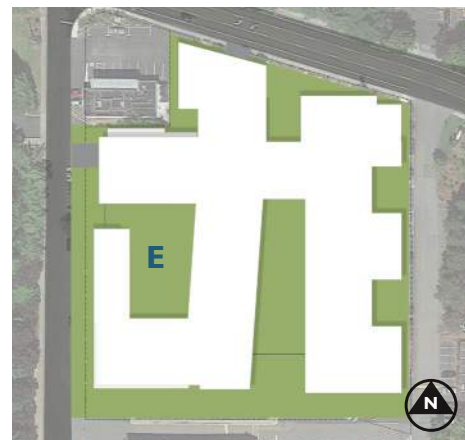
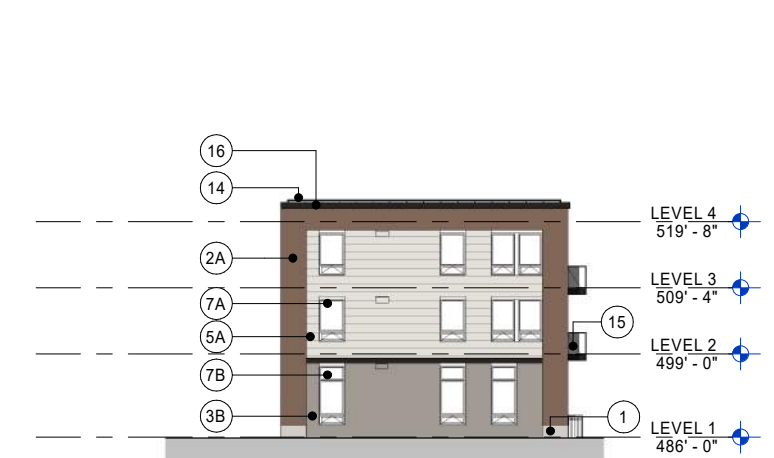
COURTYARD E: SOUTH ELEVATION



COURTYARD E: WEST ELEVATION



COURTYARD E: ELEVATION AT END OF 3 STORY BAR

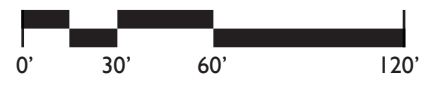
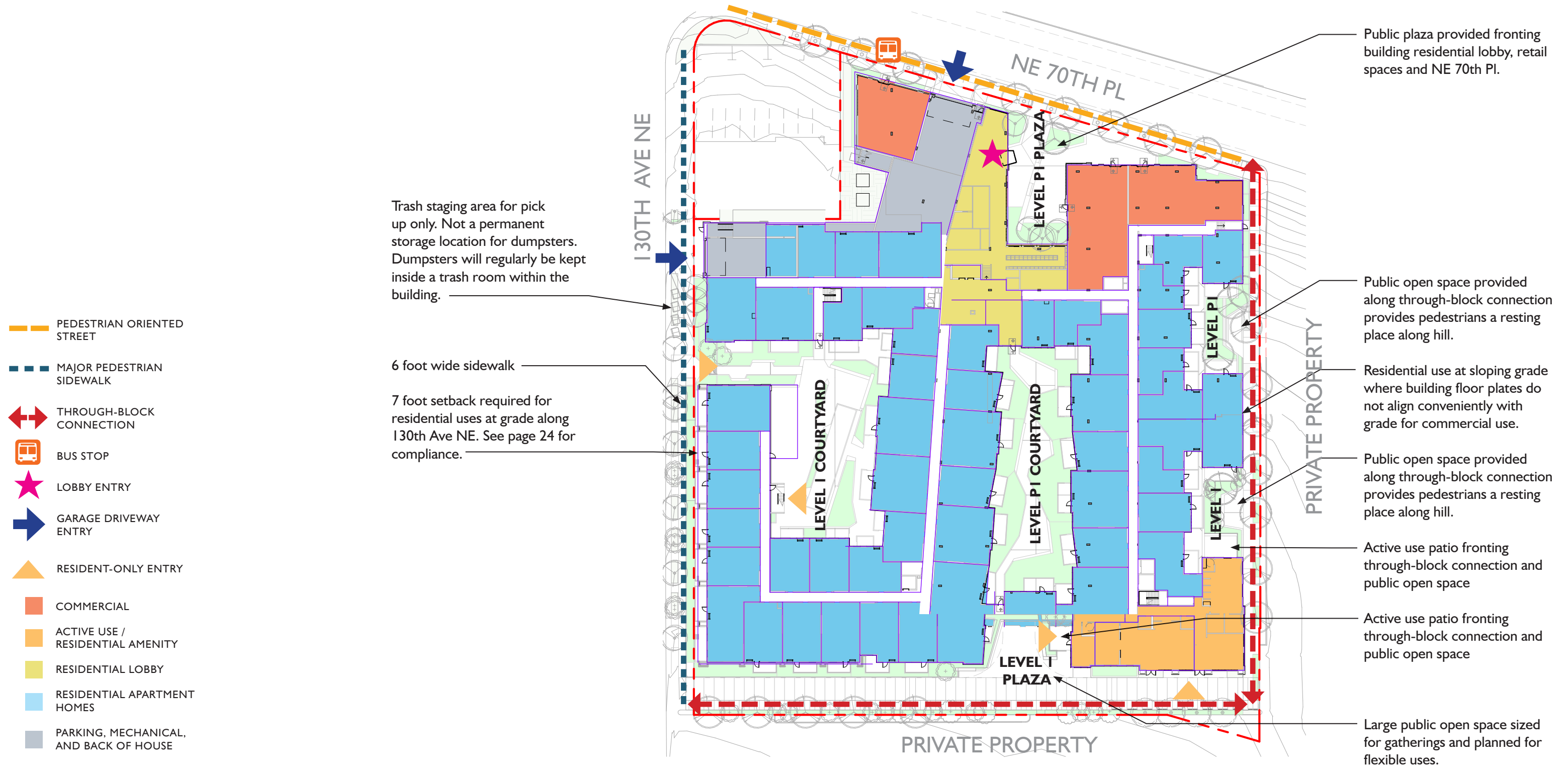


## MATERIAL LEGEND

- |                                |   |                               |                                   |
|--------------------------------|---|-------------------------------|-----------------------------------|
| ① Concrete, Cast in Place      | ③D Fiber Cement Panel: Color D                        | ⑥A Wood-Look Panel            | ⑬ Metal and Glass Canopy          |
| ②A Masonry: Dark Color         | ④A Horizontally Expressed Fiber Cement Panel: Color A | ⑥B Wood-Look Board and Batten | ⑭ Metal and Glass Guardrail       |
| ②B Masonry: Light Color        | ④B Horizontally Expressed Fiber Cement Panel: Color B | ⑦A Vinyl Windows: White       | ⑮ Metal and Glass Bolt-on Balcony |
| ③A Fiber Cement Panel: Color A | ⑤ Fiber Cement Plank                                  | ⑦B Vinyl Windows: Black       | ⑯ Cornice                         |
| ③B Fiber Cement Panel: Color B |   | ⑧ French Doors                | ⑰ Roll Up Garage Door             |
| ③C Fiber Cement Panel: Color C |   | ⑨ Aluminum Storefront         |                                   |

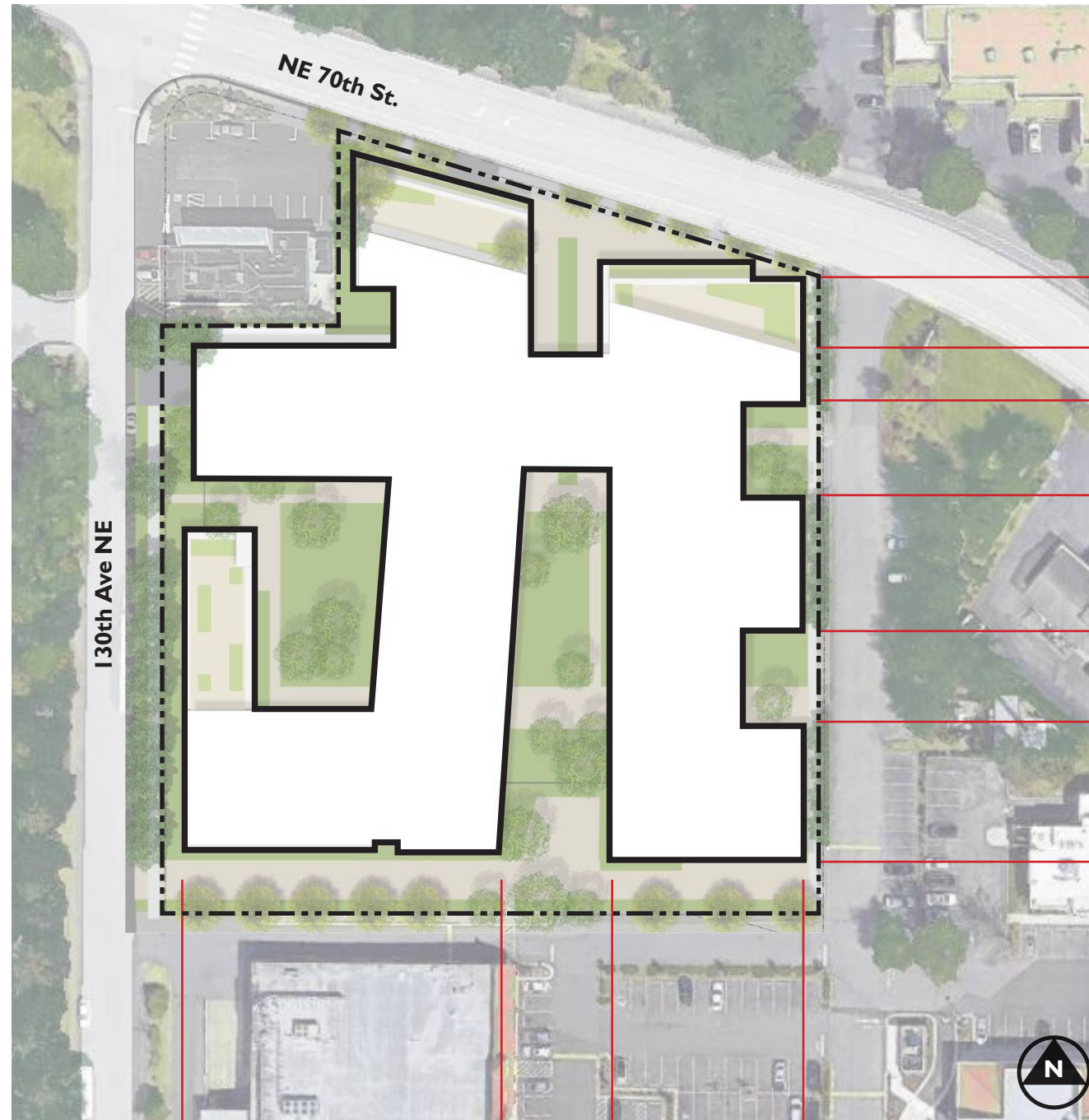
APPENDIX B: COMMERCIAL USE DEPARTURE REQUEST FROM DRC I

# GRADE RELATED USES



# FRONTAGE CONDITIONS ON THROUGH-BLOCK CONNECTIONS

The project seeks a departure regarding non-commercial frontage along the through-block pathways due to the lack of visibility and access for commercial spaces. See subsequent pages for analysis.



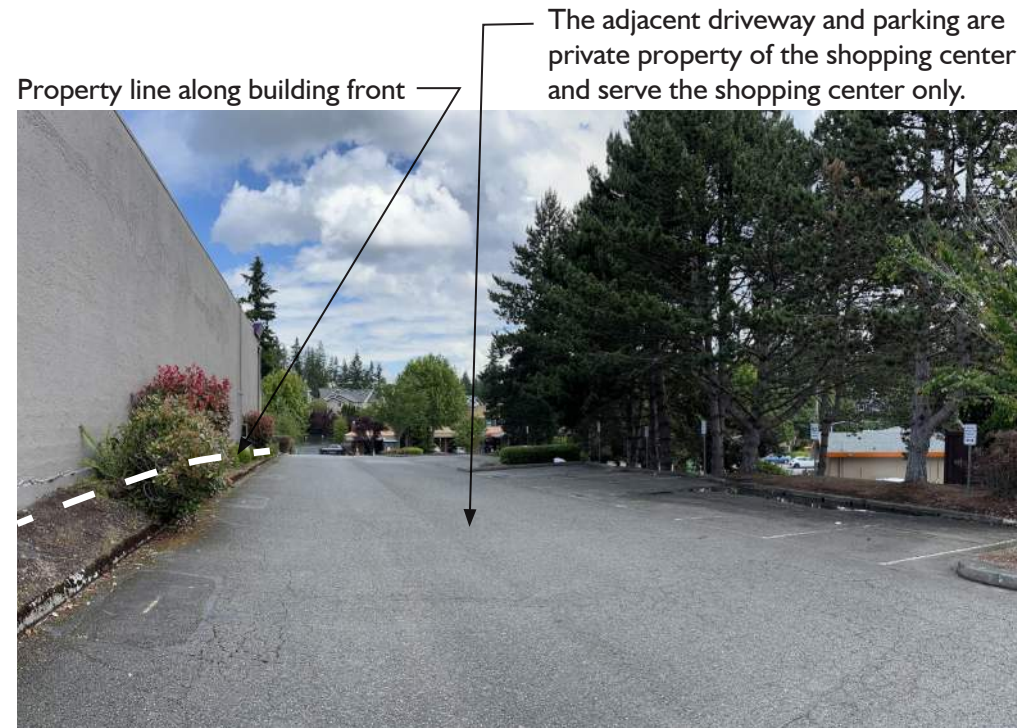
- Retail already included.
- Floor Levels do not reach grade.
- Open space requested by DRB. Helps modulate building.
- Floor Levels do not reach grade.
- Open space requested by DRB. Helps modulate building.
- Active residential uses provided create link between building residents and surrounding properties. Facing inaccessible parking not conducive for commercial uses.

New development faces adjacent retail service, trash, loading, and blank wall. Existing condition not feasible for successful commercial frontage.

Open space requested by DRB. Helps modulate building.

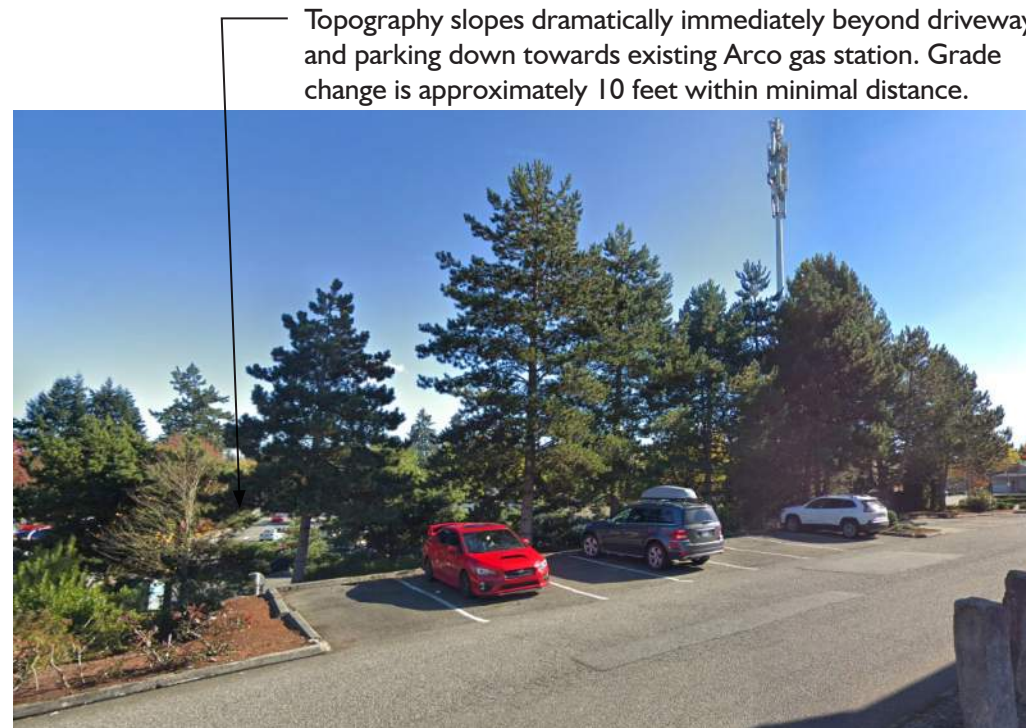
Active residential uses provided create link between building residents and surrounding properties. Facing inaccessible parking not conducive for commercial uses.

# EASTERN FRONTAGE CONDITIONS



Property line along building front

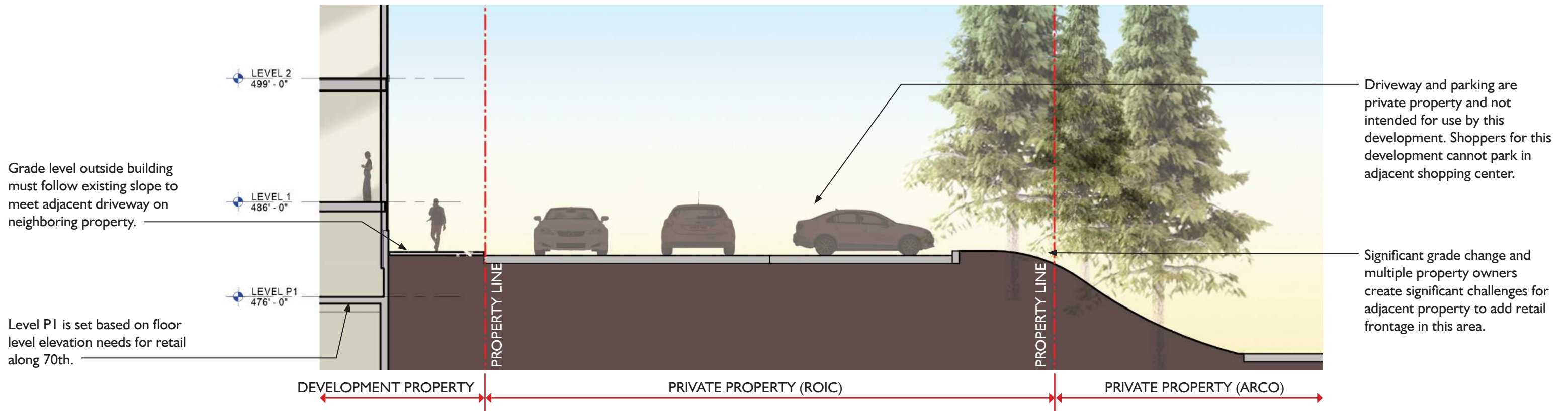
The adjacent driveway and parking are private property of the shopping center and serve the shopping center only.



Topography slopes dramatically immediately beyond driveway and parking down towards existing Arco gas station. Grade change is approximately 10 feet within minimal distance.

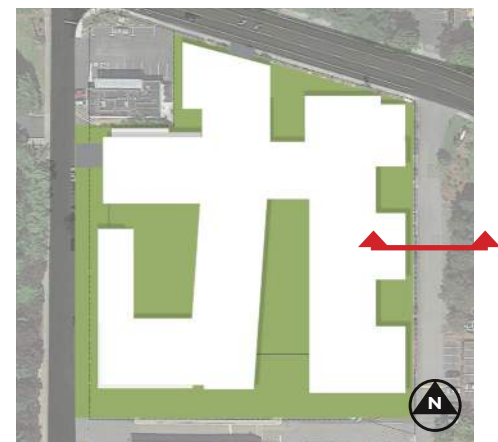
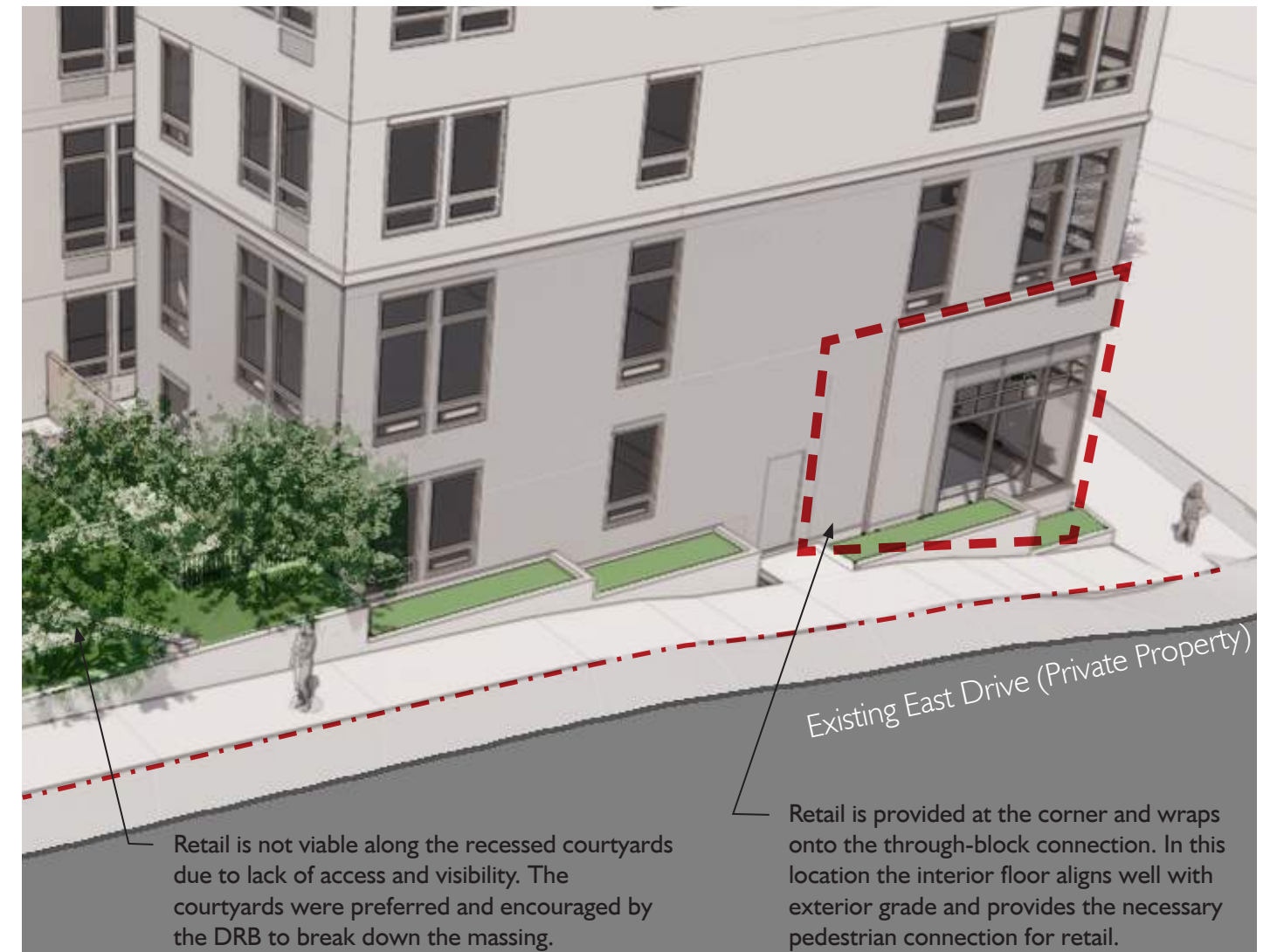
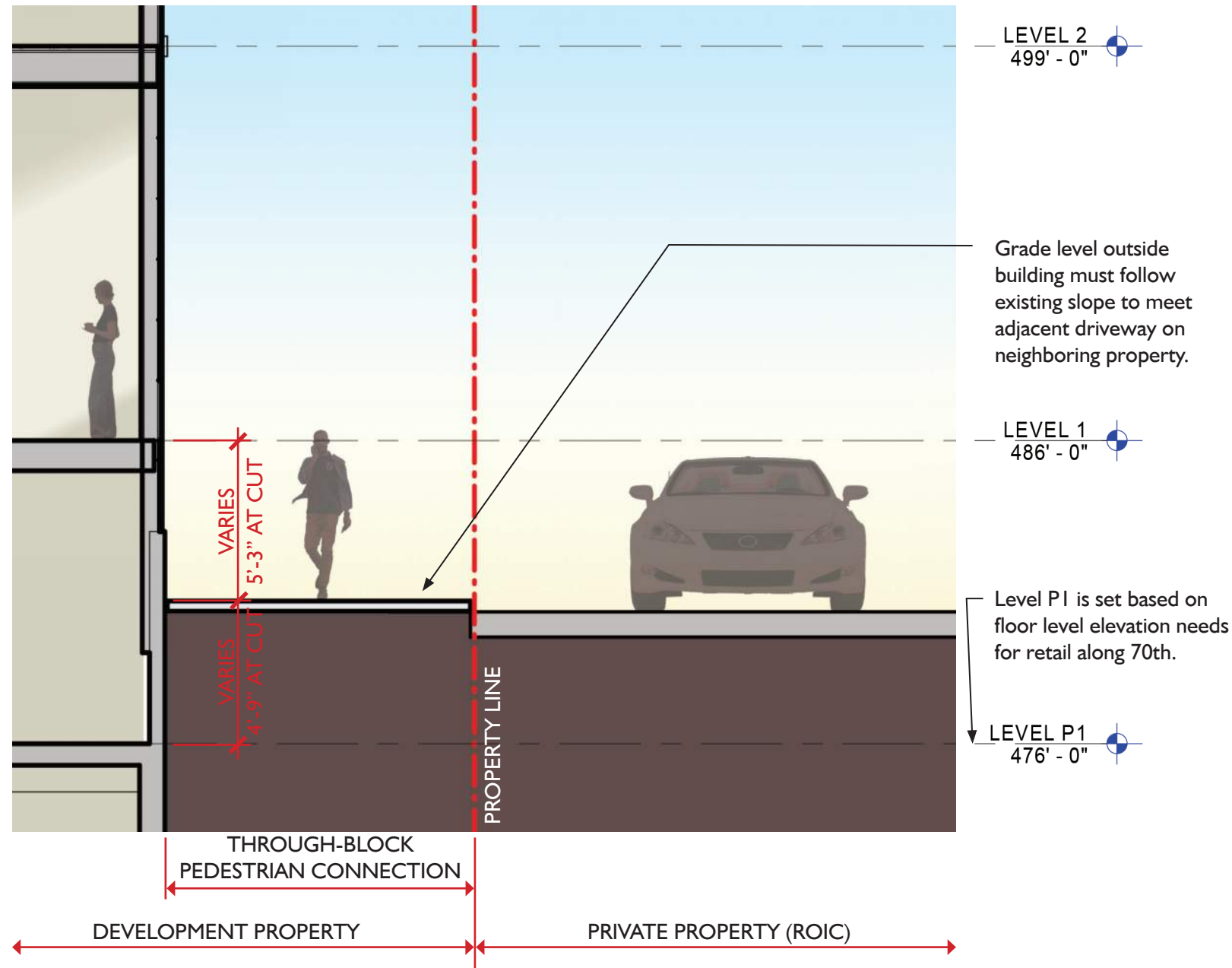
It is likely the new development will face a driveway and parking that are not intended for use by this development in perpetuity due to the narrow adjacent properties, access needs of neighboring property, multiple property owners, and steep grade changes.

Existing Adjacent Frontage



Section Through Eastern Edge Condition

# EASTERN FRONTAGE ENLARGED SECTION



Because the surrounding topography slopes significantly along the eastern edge the building, exterior grade and interior floor levels only become close enough for access at far north and far south of the frontage.



# SOUTHERN FRONTAGE CONDITIONS

Neighboring property's parking is inaccessible and not allowed to be used by the new development. Patrons of commercial uses for the new development cannot park in this lot even though it is highly visible from the frontage.

Neighboring property drive lane aligns with new development's open space. This was a preferred condition at the CDC meeting.

The neighboring existing blank facade extends across approximately half of the new development frontage. This frontage is not conducive to successful commercial uses.

Neighboring Property's loading, and trash pick up faces new development.



The frontage conditions do not create the visibility, pedestrian or vehicle access, or ambiance necessary for viable commercial uses on the new development.

# COMMERCIAL USE PRACTICAL NEEDS

Curb cuts for trash pick up, garage access, or vehicular circulation are not permitted in this area because of proximity to existing curb-cuts.

**Public Parking Access:**

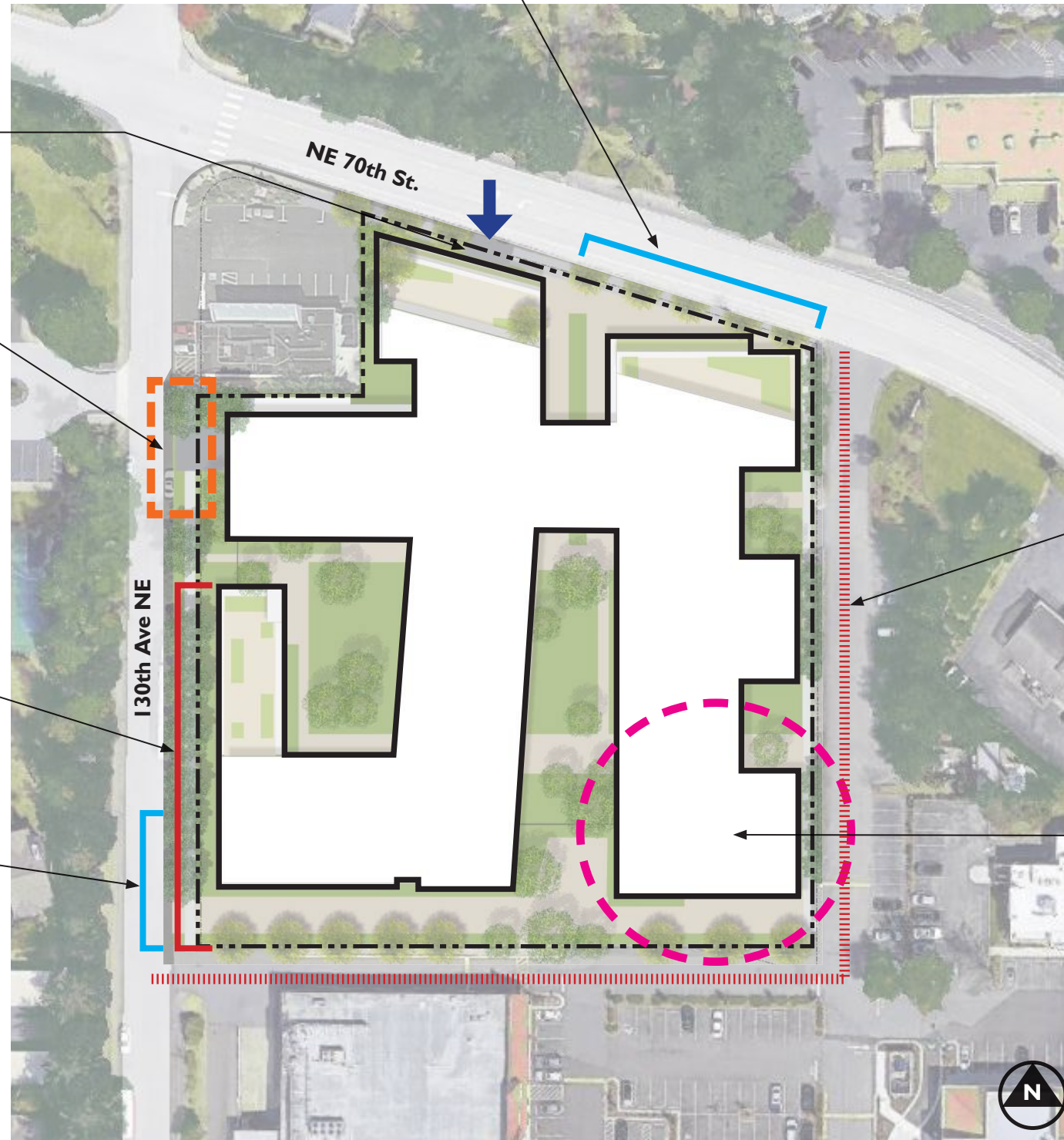
Located on NE 70th, parking access here provides the best visibility and best connection to parking below grade without impacting building frontage and pedestrian experience.

**Waste Staging for Pick Up:**

Located adjacent to existing Pagliacci waste pickup, this location was best relative to allowable curb-cut locations and viable truck access.

Waste staging was not preferred in this area at the first CDC meeting because it was too close to residential apartment homes at grade as well as too close to the through-block connection.

Curb cuts for trash pick up, garage access, or vehicular circulation are not permitted in this area because of proximity to existing curb-cuts.



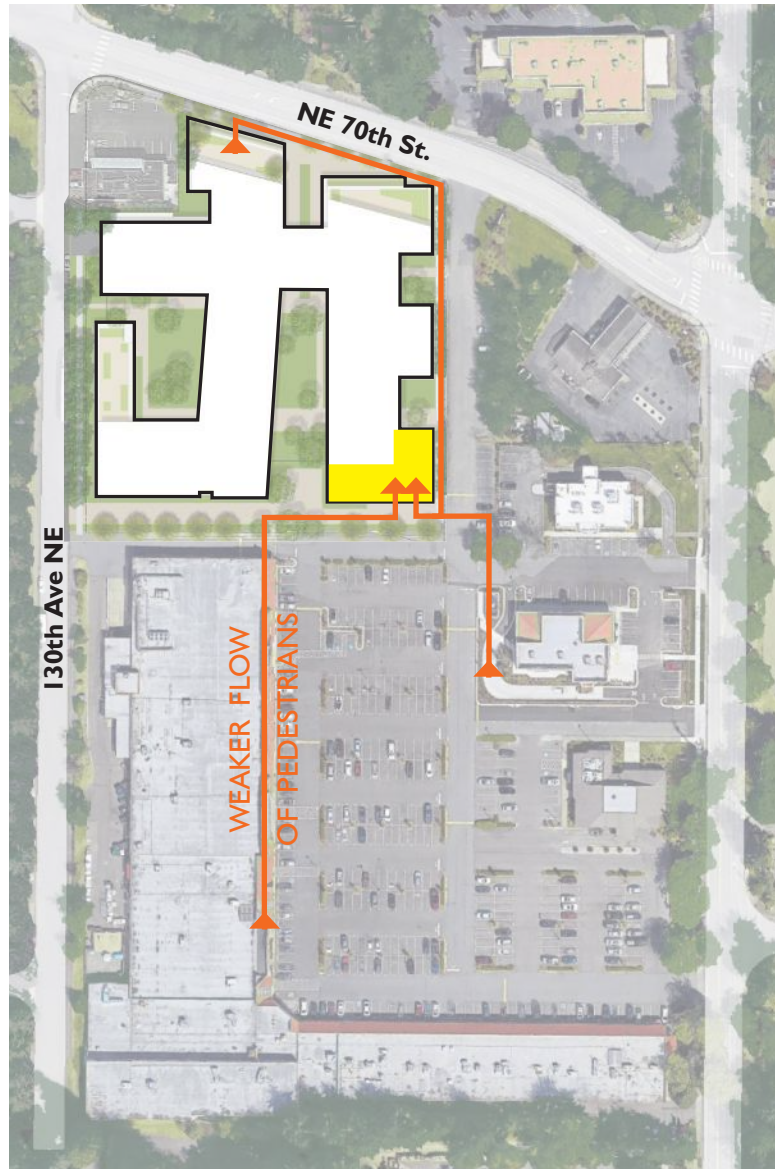
Vehicular access to the southeast corner is not permitted from adjacent, private property. People attempting to reach the southeast corner must park via the garage entrance of 70th and either traverse through the interior of a private residential building, or walk the perimeter of the building with sloping grade.

Commercial uses located at the southeast corner of the new development would be as far as possible from parking and waste infrastructure, creating serious wayfinding and operational issues.

# CREATING THE BEST ACTIVATION & CONNECTION WITHIN THE BCX ZONE

## Commercial Uses at the Southeast Corner

Competes with Existing Shopping Center Uses  
Relies on Future Development Changes for Success

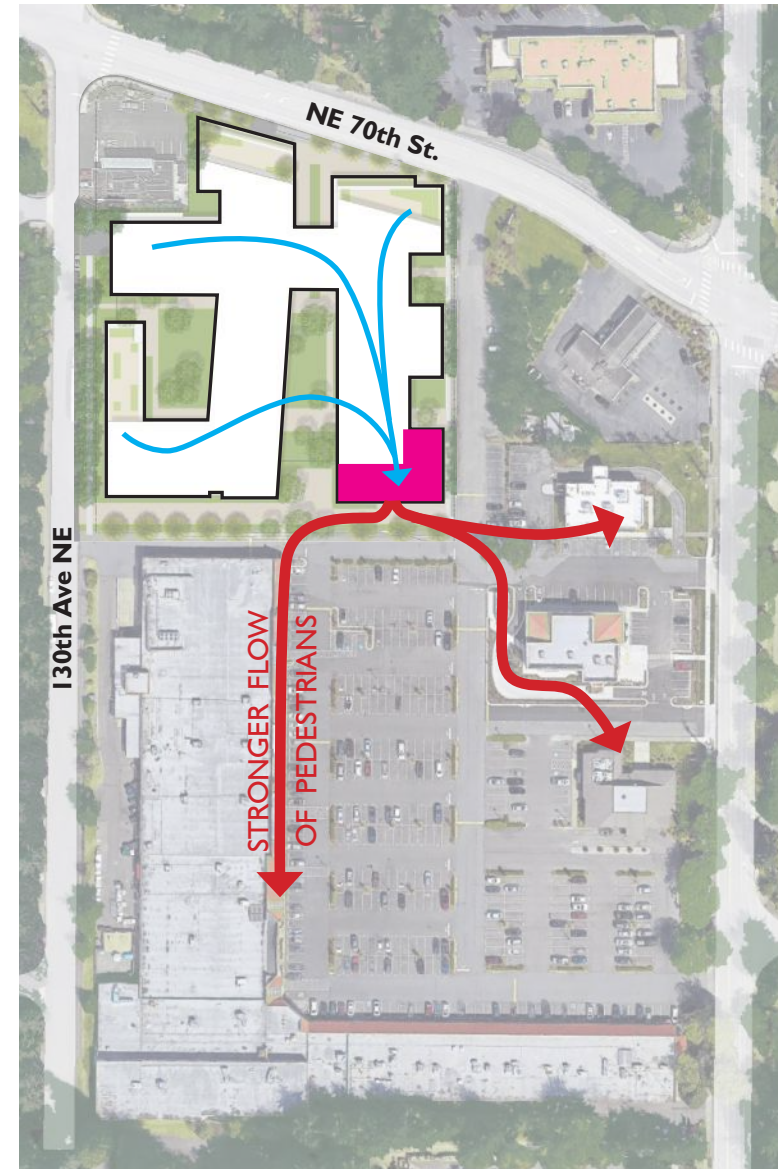


- Commercial use viability relies on visibility from adjacent shopping center parking creating a competitive draw on shopping center patrons. There is no visibility from surrounding streets.
- Access for patrons of commercial uses at southeast corner must know to park via entry on NE 70th, and then walk around perimeter of building. (Travel through building would create a security risk for residents.) The adjacent driveways and surface parking are not available to the new development's residents, retail tenants or customers.
- Worst case, redevelopment of adjacent property could reduce visibility or connection to southeast corner commercial uses.

Vacant storefronts of unsuccessful commercial spaces do nothing to activate throughblock connections or create connectivity within the BCX zone.

## Active Amenity Uses at the Southeast Corner

Imbibes Existing Shopping Center with Patrons  
Creates Lively Frontage Immediately as well as in the Future



- Active amenity spaces are well used by residents of the building.
- Grouping active amenity spaces at the southeast corner creates a destination for residents that pulls them to the center of the BCX zone.
- Easy pedestrian access to the shopping center from the southeast corner turns residents into patrons for the shopping center – boosting success and viability of adjacent commercial uses.
- Popular residential amenities provide eyes on the through-block connections for safety.
- The existing southeast corner frontages are already designed to appear commercial in nature with commercial height ceilings, large glazing, overhead weather protection, and doors.
- The versatile design means if commercial uses become viable after redevelopment of adjacent properties, the owner has the ability to convert to commercial uses.
- Active residential amenities do not rely on exterior access or visibility for success, meaning they are immediately successful at activating the through-block connections.

Given the current conditions, active residential amenity space will be most successful at generating connection and strengthening commercial viability throughout the BCX zone.

# DETAILED LOOK AT SE CORNER RESIDENTIAL AMENITY



Exterior, enclosed play space amenity for residents

Interior children's play space residential amenity

Public open space connected to through-block connection.

Ingress and egress for residents to enclosed play space. Playspace has gate access to public open space and through-block connection

Large commercial style windows and overhead weather protection create visible connection between interior and exterior activity.

Residential amenity patio connected to co-working space front through-block connection and public open space.

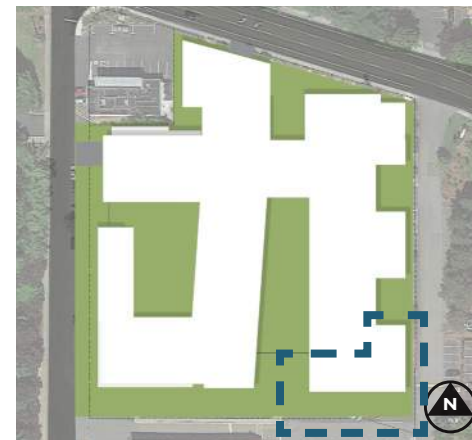
Large commercial style windows and overhead weather protection face through-block connection. Windows create visible connection between interior and exterior activity.

Ingress and egress doors for residents, linking residents to through-block connections and existing shopping center.

Worktop bar height counter faces exterior windows.

Large commercial style windows and overhead weather protection face through-block connection.

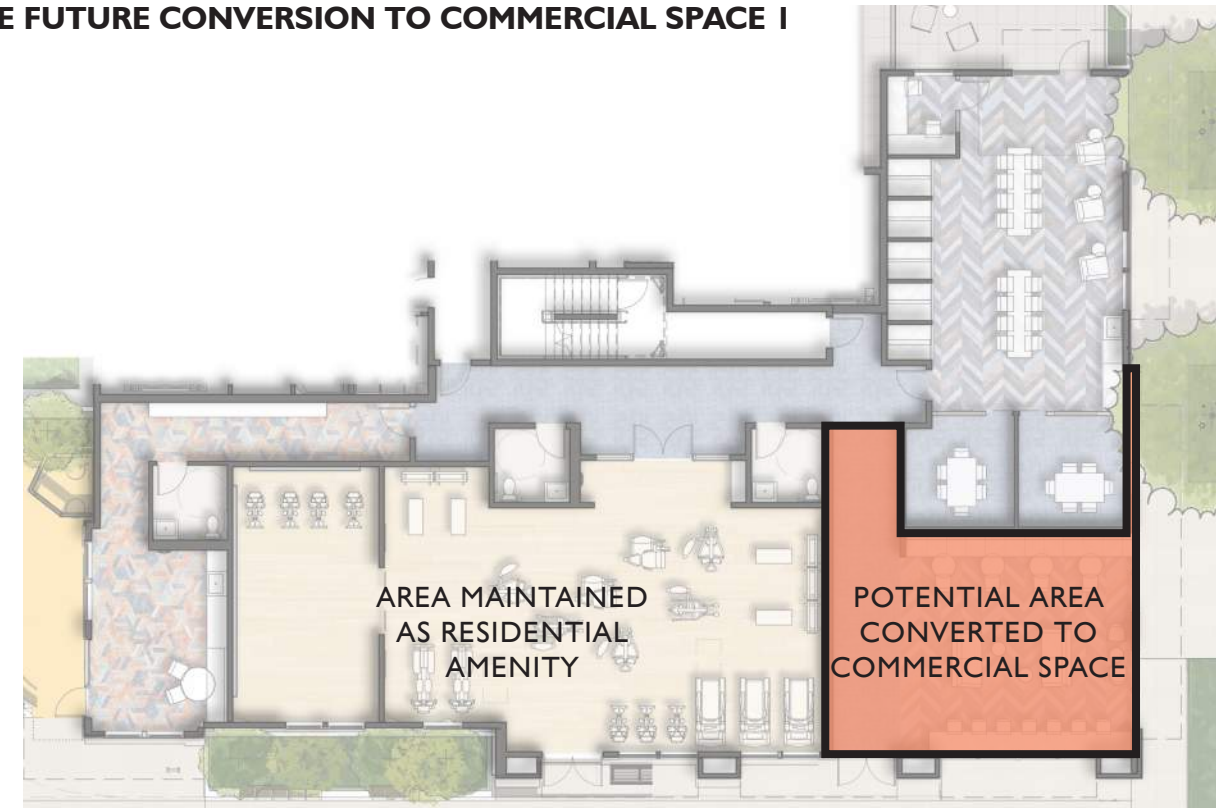
Ingress and egress doors from fitness center amenity allow residents easy access to through-block connection exterior for fitness activities like jogging or outdoor yoga.



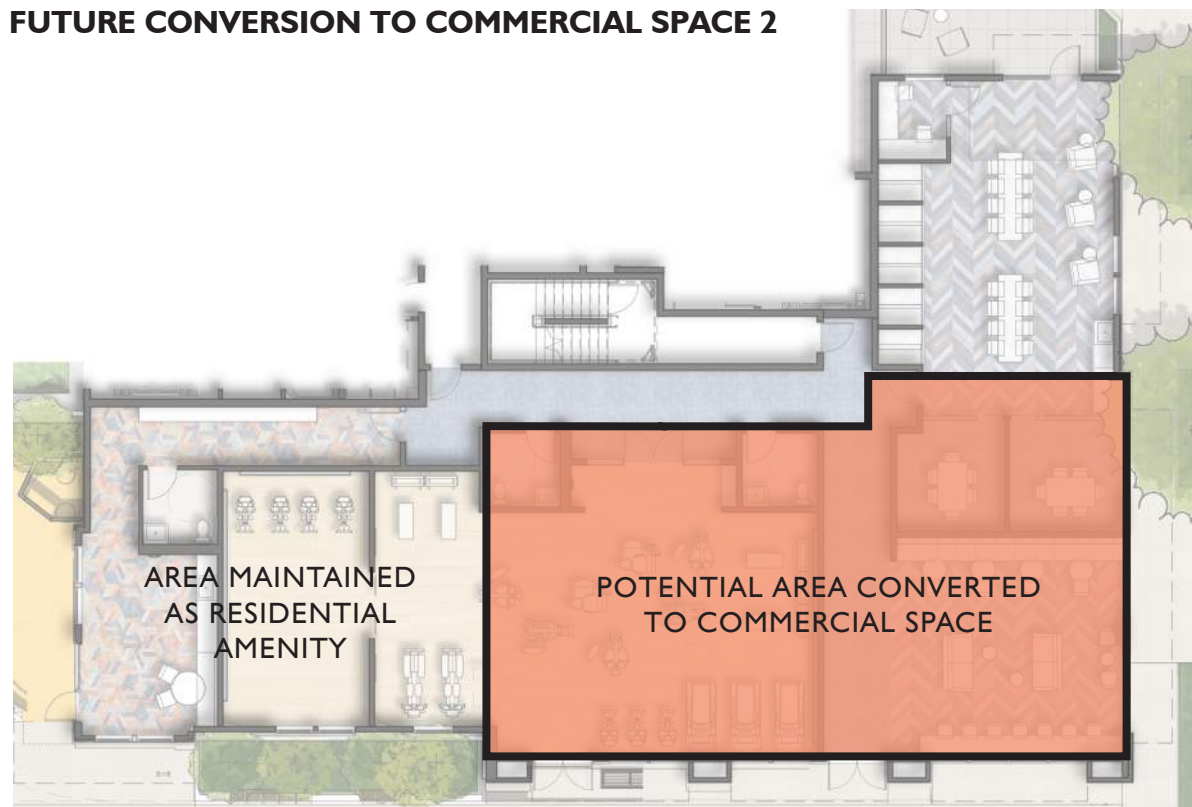
# PLANNED FOR FUTURE FLEXIBILITY

The existing SE corner is designed to commercial standards with taller floor to floor heights, overhead weather protection and large glazing matching that on NE 70th Street. Furthermore, none of the building's apartment homes have doors within the SE amenity spaces. If conditions within the BCX zone change in a way that makes commercial space viable, the building is set to be able to convert to different uses in the future.

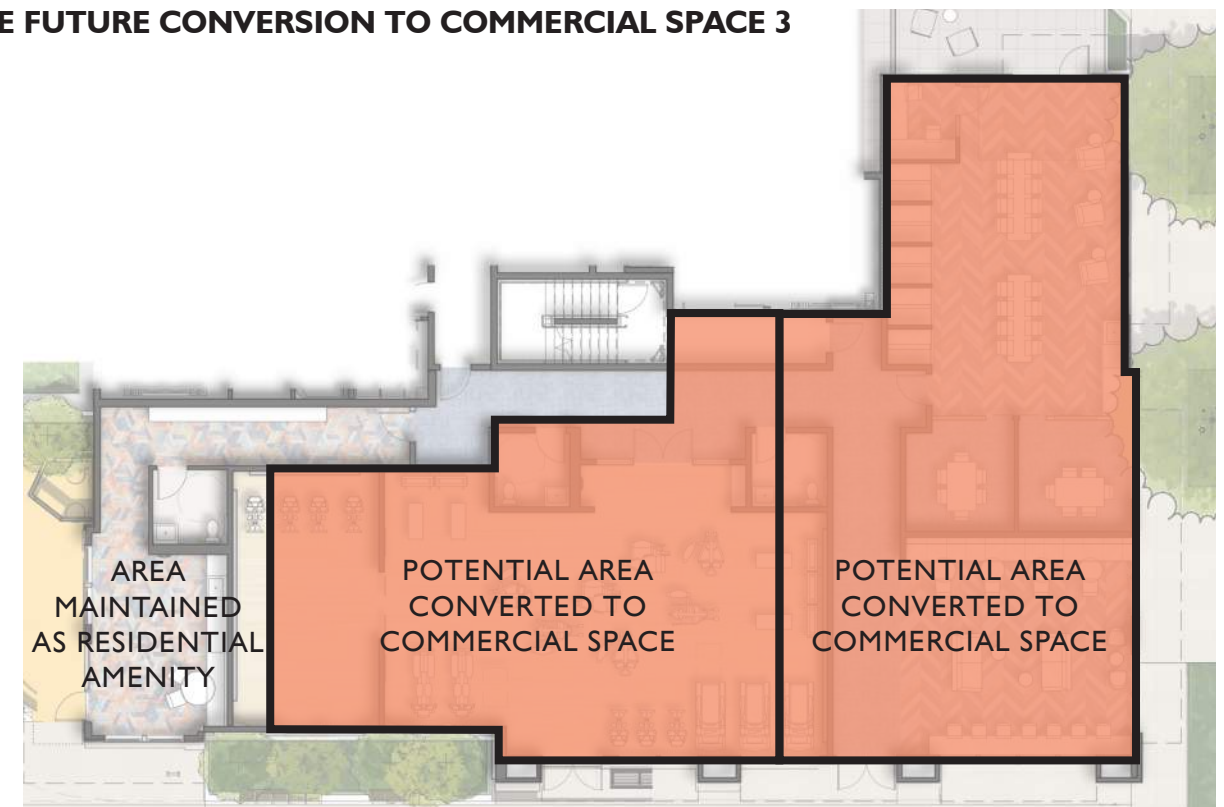
### POSSIBLE FUTURE CONVERSION TO COMMERCIAL SPACE I



### POSSIBLE FUTURE CONVERSION TO COMMERCIAL SPACE 2

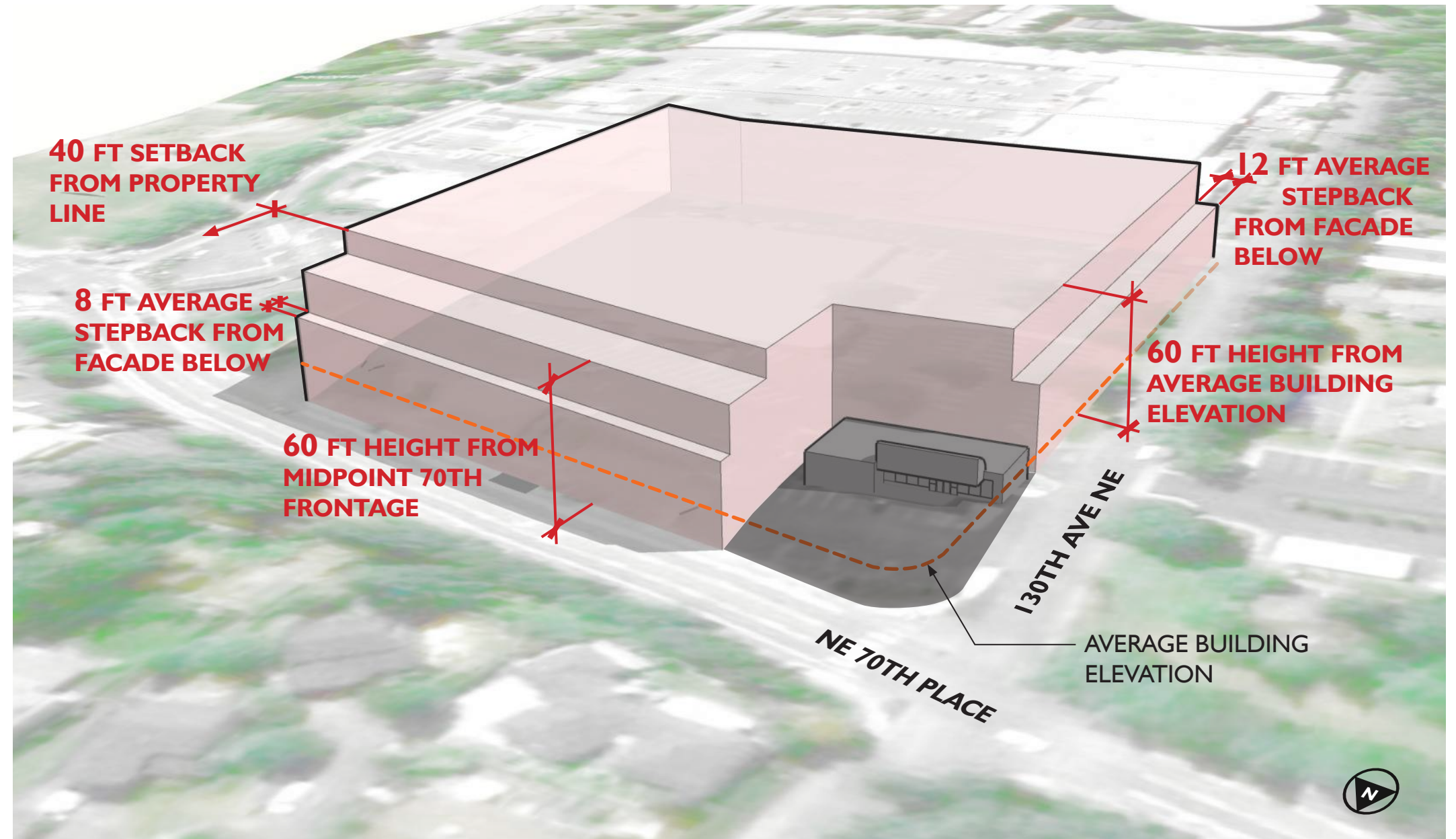


### POSSIBLE FUTURE CONVERSION TO COMMERCIAL SPACE 3



## APPENDIX C: INFORMATION PRESENTED AT DRC I

# REVIEW OF ZONING ENVELOPE



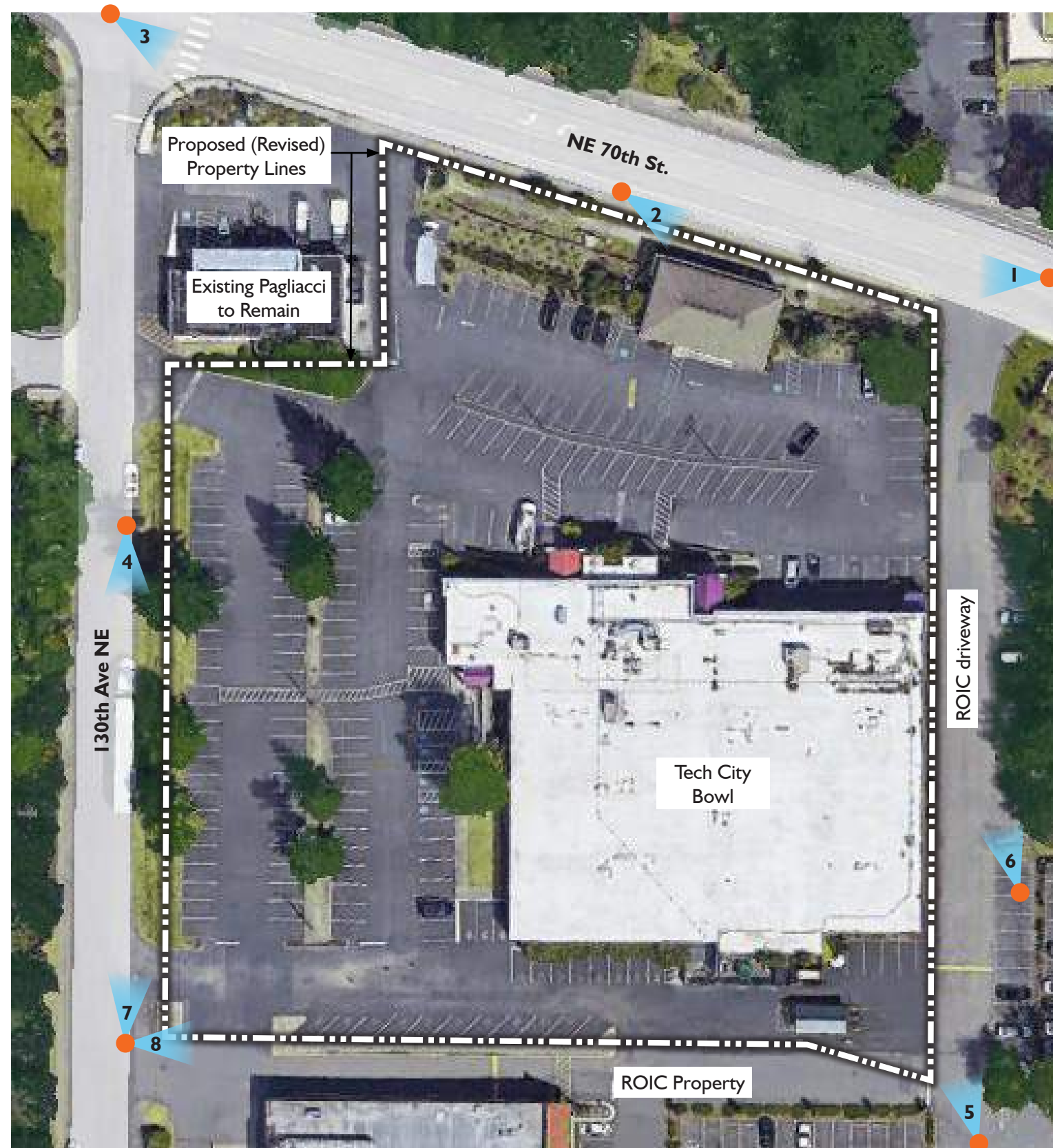
# ZONING SUMMARY

BCX Zone		
Building Height	35.10.040.2	Maximum height of structure is Sixty feet above average building elevation if:
Use limitation	35.10.040.2.a	Uses above the 3rd story are limited to Assisted Living Facility and Attached or Stacked Dwelling Units.
Setback along NE 70th	35.10.040.2.b	No portion of a building within 40 feet of NE 70th Place exceeds a height of 60 feet above NE 70th Place measured at the midpoint of the frontage of the subject property on the NE 70th Place right-of-way. The Design Review Board is authorized to allow rooftop amenities where this height limit is imposed.
Stepbacks along ROW	35.10.040.2.d	For all building facades facing and within 100 feet of the abutting right-of-way, all portions of a structure greater than three stories in height, as measured from the abutting right-of-way, shall be stepped back from the third story façade as follows: 1. From NE 70th Place by an average of 8 feet. 2. From 130th Avenue NE by an average of 12 feet. 3. The required upper story step backs for all floors above the third story shall be calculated as Total Upper Story Step Back Area as follows: Total Upper Story Step Back Area = (Linear feet of front property line(s), not including portions of the site without buildings that are set aside for vehicular areas) x (Required average step back) x (Number of stories proposed above the third story). The Design Review Board is authorized to allow rooftop amenities within the step back area.
Required Open space	35.10.040.2.e	The development shall provide publicly accessible pedestrian oriented open space(s) adjacent to the street or through-block pathway. The publicly accessible space(s) shall contain a minimum of 1,000 square feet or one square foot per 200 gross square feet of above grade building area, whichever is greater. The size calculation shall not include the required width of abutting sidewalks or pathways. Locations, dimensions, features and improvements (such as plazas, seating, public art, children's recreation space) shall be reviewed and approved through by the Design Review Board based on applicable guidelines.
Affordable housing	35.10.040.2.g	Developments creating four or more new dwelling units shall provide at least 10 percent of the units as affordable housing units as defined in Chapter 5 KZC. See Chapter 112 KZC for additional affordable housing incentives and requirements.
Green Building Standards	35.10.040.2.h	Development shall be designed, built and certified to achieve or exceed the high performance building standards described in KZC 115.62
Commercial Floor Height	35.10.40.2.i	The commercial floor shall be a minimum of 13 feet in height
Commercial Use Placement	35.10.040.2.j	Except along NE 65th Street and 130th Avenue NE, residential uses, assisted living uses, and parking for those uses shall not be located on the street level floor unless an intervening commercial frontage is provided between the street and those other uses or parking subject to the standards above. The intervening commercial frontage shall have a minimum depth of 20 feet and an average depth of at least 30 feet (as measured from the face of the building). The Design Review Board (or Planning and Building Director if not subject to Design Review) may approve a minor reduction in the depth requirements if the applicant demonstrates that the requirement is not feasible given the configuration of existing or proposed improvements and that the design of the commercial frontage will maximize visual interest.
Commercial Frontage	35.10.040.2.k	Development shall contain commercial uses oriented to adjoining arterials and through-block pathways. The location and frontages of these commercial uses shall be reviewed through Design Review for consistency with applicable guidelines or regulations.
Through-Block Pathways	35.10.040.2.l	Development shall provide for one north-south through-block pathway connection between NE 70th Place and NE 65th Street and two east-west through-block pathway connections between 130th Avenue NE and 132nd Avenue NE (see Plate 34Q). The Design Review Board shall determine the final location and configuration of the through-block pathway connections based on convenience and utility for nonmotorized access and orientation toward commercial uses and pedestrian-oriented open space.
Residential Lobbies and Amenities	35.10.040.2.m	Lobbies and amenity space for residential or assisted living uses may be allowed within the commercial frontage provided they do not exceed 20 percent of the building's linear retail frontage along the street or through-block pathway. The Design Review Board (or Planning and Building Director if not subject to Design Review) may approve a minor increase to ground floor residential lobbies and amenities if they are connected to retail use and the design of the ground floor frontage will maximize visual interest.
Residential Yards	35.10.040.1.n	Attached or Stacked Dwelling Units and Assisted Living Facilities located at the street level floor along NE 65th Street and 130th Avenue NE shall have a minimum seven-foot required yard.
Height Exceptions	35.10.040.1.o	In addition to the height exceptions established by KZC 115.60, the following exceptions to height regulations zone are established: 1) An additional 5 feet is allowed for buildings providing a grocery store. 2) Decorative parapets may exceed the height limit by a maximum of four feet; provided, that the average height of the parapet around the perimeter of the structure shall not exceed two feet. 3) For structures with a peaked roof, the peak may extend eight feet above the height limit if the slope of the roof is equal to or greater than four feet vertical to 12 feet horizontal.

Street Designation	35.10.040.2.l Plate 34Q	NE 70th = Pedestrian Oriented Street / Minor Arterial 130th Ave NE = Major Ped Sidewalk / ((R-28 Neighborhood Access Street (KZC 110.22)?))
Lot Coverage	35.30.020 table	Stacked Dwelling Units allowed at 100% lot coverage.
Required Parking	35.40 table	
Attached or Stacked Dwelling Units	35.40.020	Attached Dwelling Units: 1.2 per studio unit. 1.3 per 1 bedroom unit. 1.6 per 2 bedroom unit. 1.8 per 3 or more bedroom unit.
Restaurant	35.40 table note DS-15	Within the BCX zone the required parking for Restaurants and Taverns uses is 1 per each 300 sq. ft. of gross floor area.
Retail	35.40.170-190,220-240	1 per each 300 sq. ft. of gross floor area.
Exceptions	35.40 table note DS-2	Within the BCX zone a parking modification to decrease in the required number of spaces for attached or stacked dwelling units is not required to increase the total parking demand rate by fifteen (15) percent per KZC 105.103(3).
Landscape Category	35.40 table Note DS-1	Retail, Restaurant or Tavern = category E Residential Use at grade = category E
Pedestrian Oriented Improvements	92.15.1  92.15.2	All Zones – Pedestrian-Oriented Space and Plazas in Parking Areas – The applicant must provide at least 175 square feet of pedestrian-oriented space at the main building entrance in a central location, or adjacent to a parking area. This area must be raised at least six (6) inches above the parking lot surface and must be paved with concrete or unit pavers.  Pedestrian-Oriented Space and Plazas in BCX Zone a. In BCX – If the subject property abuts a pedestrian-oriented street (see Plate 34 in Chapter 180 KZC) or public park, the space, if any, between the sidewalk and the building must be developed consistent with the following criteria: 1) Enhance visual and pedestrian access, including handicapped access, onto the subject property from the sidewalk. 2) Contain paved walking surface of either concrete or approved unit pavers. 3) Contain on-site or building-mounted lighting which provides adequate illumination. 4) Contain two (2) linear feet of seating area or one (1) individual seat per 65 square feet of area between the sidewalk and the building. 5) Contain landscaping such as trees, shrubs, trellises, or potted plants. 6) It may not include asphalt or gravel pavement or be adjacent to an unscreened parking area, a chain link fence or a blank wall which does not comply with the requirements of subsection (3) of this section, Blank Wall Treatment. 7) An alternative solution for the pedestrian-oriented space may be established through a Conceptual Master Plan in TL 2.
Landscape Buffer Requirements	95.42 Footnote	If the adjoining property is zoned Bridle Trails Neighborhood Center (BCX Zone) this section KZC 95.42 does not apply.
Overhead Weather Protection	105.18.3.b	The applicant shall provide pedestrian overhead weather protection along at least 75 percent of a pedestrian-oriented building facade.
Through Block Pathways	105.19.2.b	The through-block pathway shall be installed pursuant to the following standard 1) A minimum unobstructed pavement width of eight (8) feet, paved with decorative concrete. A minimum of five (5) feet may be approved for residential uses. 2) Trees placed at an average of 30 feet on-center between the pathway and any parking or vehicular access area (see Figure 105.19.A). Exceptions: a) To increase business visibility and accessibility, the City may allow modifications in the required tree coverage adjacent to primary building entries; however, no less than one (1) tree per 60 lineal feet of the required pathway 5) The through-block pathway may be retained within dedicated rights-of-way, tracts, or easements at the City's option. The width of the pathway right-of-way, tract, or easement will be determined by the Planning Official. 6) If subject to Design Review the City will specifically review and approve the material and configuration of all through-block pathways as part of the Design Review decision.
Pedestrian Oriented Streets	110.52.2	The applicant shall install a 10-foot-wide sidewalk along the entire frontage of the subject property abutting each pedestrian-oriented street.
Major Pedestrian Sidewalk	110.52.3	The applicant shall install that sidewalk on and/or adjacent to the subject property consistent with the following standards: a. Install in the approximate location and make the connections shown in Plate 34; b. A sidewalk width of at least eight (8) feet, unless otherwise noted in Plate 34; c. Have adequate lighting with increased illumination around building entrances and transit stops; and d. If parcels are developed in aggregate, then alternative solutions may be proposed.
Landscape and Street Trees	110.60.4  110.60.4.e	Landscape strips are typically found between the curb and the sidewalk and are planted with grass and street trees spaced 30 feet on-center.  If a landscape strip or street trees in tree grates is not required, street trees planted 30 feet on-center 2.5 feet behind the sidewalk will be required, where feasible.



# EXISTING SITE



Additional views of the existing site can be found in the appendix.



# PROJECT OVERVIEW

## Project Description:

The proposed project is a five story mixed-use (multi-family residential and retail) project. The project will include amenities (fitness, lounges, roof deck, etc), two levels of below grade parking for commercial and residential use and approximately 7,000 sf of retail, 368 units and 444 parking stalls, with an additional 14 stalls of on-street parking along 130th Ave NE. Construction type is assumed as Type IIIA over Type IA. The project includes approximately 400,000 SF of development.



### BUILDING USE BREAKDOWN:

LEVEL	AMENITY	COMMERCIAL	RESIDENTIAL	PARKING	TOTAL GSF
LEVEL P3	0	0	0	3,426	3,426 SF
LEVEL P2	0	0	0	95,637	110,045 SF
LEVEL P1	1,740	6,978	20,014	53,545	98,293 SF
LEVEL 1	4,821	0	62,043	1,736	77,373 SF
LEVEL 2	0	0	69,227	0	78,062 SF
LEVEL 3	0	0	67,427	0	76,266 SF
LEVEL 4	1,601	0	60,764	0	71,062 SF
LEVEL 5	0	0	57,021	0	65,419 SF
ROOF	0	0	0	0	951 SF
Grand total	8,162	6,978	336,496	154,344	580,897 SF



**SITE ADDRESS:** 13033 NE 70<sup>TH</sup> PLACE KIRKLAND, WA 98033

**TAX ASSESSOR'S PARCEL NO:** 124150-0285, 124150-0276

**PROPOSED USE:** RESIDENTIAL APARTMENTS AND ASSOCIATED AMENITY SPACES, SMALL RETAIL ON LEVEL P1, WITH 1.5 LEVELS OF BELOW-GRADE PARKING

**LOT COVERAGE (TABLE 35.30):** ENTERTAINMENT, CULTURAL AND/OR RECREATIONAL FACILITY, RESTAURANT OR TAVERN, OR RETAIL ESTABLISHMENT ALLOWED AT 100% LOT COVERAGE.

**MAXIMUM HEIGHT (KZC 35.10.040):** MAXIMUM HEIGHT OF STRUCTURE IS SIXTY FEET ABOVE AVERAGE BUILDING ELEVATION (SEE DRC PAGE ).

**SET BACKS (35.10.040.02.b):** SEE DRC PAGES

**STEP BACKS (35.10.040.02.d):** SEE DRC PAGES

### VEHICLE PARKINGSUMMARY:

#### RETAIL PARKING:

PER KZC 35.40.160, 1 STALL IS REQUIRED FOR EVERY 300 SQFT OF GROSS FLOOR AREA.  
 $\frac{7014 \text{ SQFT OF RETAIL SPACE}}{300 \text{ SF}} = 24 \text{ RETAIL STALLS REQUIRED}$

TOTAL RETAIL STALLS REQUIRED: 24 RETAIL STALLS, 12 OF THESE MAY BE COMPACT  
**TOTAL RETAIL STALLS PROVIDED: 24 RETAIL STALLS, MEETS REQUIREMENT**

#### RESIDENTIAL PARKING:

SECURE RESIDENT STALLS PROVIDED = 407 RESIDENT STALLS  
 GUEST STALLS PROVIDED = 23 GUEST STALLS  
**TOTAL RESIDENTIAL STALLS PROVIDED = 430 STALLS\*\***

\*\*FOR ANALYSIS OF RESIDENTIAL PARKING DEMAND, REFERENCE THE 8/16/22 TRANSPORTATION IMPACT ANALYSIS PREPARED BY HEFFRON AND ASSOCIATES, WHICH IS CURRENTLY UNDER CITY REVIEW.

### BICYCLE PARKING REQUIREMENTS:

PER KZC 105.32: BICYCLE PARKING SPACES SHALL BE PROVIDED AT A RATIO OF ONE (1) BICYCLE SPACE FOR EACH 12 REQUIRED MOTOR VEHICLE PARKING SPACES.

PER KMZ 105.34: IF COVERED AND SECURED BICYCLE STORAGE IS PROVIDED ON SITE, A CREDIT TOWARDS PARKING REQUIREMENTS AT A RATIO OF ONE (1) LESS PARKING STALL PER SIX (6) BICYCLE SPACES WILL BE GRANTED.

RETAIL BICYCLE STALLS REQUIRED: 24 REQUIRED VEHICLE STALLS\*\* / 12 = 2 BICYCLE STALLS REQUIRED  
 RETAIL BICYCLE STALLS PROVIDED: **2 STALLS, MEETS REQUIREMENT**

RESIDENTIAL BICYCLE STALLS REQUIRED: 436 REQUIRED VEHICLE STALLS\*\* / 12 = 37 BICYCLE STALLS REQUIRED  
 MINUS SECURED STORAGE REDUCTION 37/6  
 = 31 BICYCLE STALLS REQUIRED

RESIDENTIAL BICYCLE STALLS PROVIDED: **31 STALLS, MEETS REQUIREMENT**

**TOTAL BICYCLE PARKING REQUIRED: 33 STALLS**  
**TOTAL BICYCLE PARKING PROVIDED: 33 STALLS, MEETS REQUIREMENT**

\*\*SEE "VEHICLE PARKING SUMMARY" SECTION FOR REQUIRED PARKING CALCULATIONS.

**GARBAGE AND RECYCLING REQUIREMENT:** SEE DRC PAGE

# RESPONSES TO DESIGN GUIDELINES

## GUIDELINE

### PE-1 SIDEWALK WIDTH: MOVEMENT ZONE

A sidewalk should support a variety and concentration of activity yet avoid overcrowding and congestion. The average sidewalk width should be between 10' and 18'. New buildings on pedestrian-oriented streets should be set back a sufficient distance to provide at least 10' of sidewalk. If outdoor dining, seating, vending, or displays are desired, an additional setback is necessary.

## RESPONSE

A direct 10' wide sidewalk is provided along all NE 70th frontage in keeping with design guidelines as well as code requirements for pedestrian oriented streets.

### PE-2 SIDEWALK WIDTH: CURB ZONE

Street elements trees, parking meters, signs should be organized in the curb zone to reduce congestion. During busy periods, pedestrians may use the curb zone for walking. Where pedestrian traffic is the heaviest, sidewalk bulbs can be constructed to accommodate bike racks, waste receptacles, and newspaper racks. Corner bulbs also increase pedestrian visibility.

Street trees and other pedestrian amenities are provided within the curb zone of the sidewalk.

### PE-3 SIDEWALK WIDTH: THE STOREFRONT ACTIVITY ZONE

New buildings should be set back a sufficient distance from the front property line a minimum of 10' to allow enough room for pedestrian movement. Wider setbacks should be considered to accommodate other sidewalk uses that would benefit their businesses and the pedestrian environment. Lighting and special paving of the storefront activity zone are also beneficial.

Retail storefronts angle away from the primary sidewalk, graciously opening towards the central plaza. This angle creates wider sidewalk areas along the retail frontage and opportunities for retail to spill out, engaging pedestrians without impeding their movement.

### PE-4 PEDESTRIAN COVERINGS

Awnings or canopies should be required on facades facing pedestrian-oriented sidewalks. A variety of styles and colors should be encouraged on pedestrian-oriented streets, and a more continuous, uniform style encouraged for large developments on entry arterial streets.

Canopies are provided along all of 70th. In keeping with larger development and arterial frontage guidance, the canopies are similar in design and style. However the canopy above the primary residential entry is located higher on the facade highlighting this entry and providing variety along the street frontage.

Although overhead weather protection is not required along the through-block connections, the development includes canopies in areas where there is active use frontage as a way to enhance both pedestrian comfort and engagement between interior and exterior activities.

## GUIDELINE

### PE-5 "PEDESTRIAN-FRIENDLY" BUILDING FRONTS

Building fronts should have pedestrian-friendly features transparent or decorative windows, public entrances, murals, bulletin boards, display windows, seating, or street vendors that cover at least 75 percent of the ground-level storefront surface between 2' and 6' above the sidewalk.

#### Special Considerations for Bridle Trails Neighborhood Center (BCX Zone)

Continuous commercial building fronts should be provided along adjoining arterials and through-block pathways. Consideration should be made to maximize the usage of through-block pathways with commercial and other public activations. Any non-commercial building fronts should be located in areas where less successful commercial activity may occur. Special attention should be made in locating commercial building fronts near intersecting through-block pathways and where transit services are located.

## RESPONSE

Commercial frontage is prioritized along 70th where commercial uses are likely to be most successful. NE 70th provides the best storefront visibility as well as the easiest access to on site parking both of which are critical for commercial success. The project strategically places other active uses, like residential amenities, along the through-block connections to activate the pedestrian experience and better accommodate grade related challenges on this sloping site. The ground-level facade between 2' and 6' above the sidewalk is primarily transparent at commercial and active uses to provide views into active spaces. Grade-related amenity spaces throughout the building maintain commercial floor heights so they may be converted at a later date if adjacent properties redevelop and the frontage becomes more conducive for true commercial use. This design response works best with the existing context, but allows for flexibility as redevelopment occurs in the future. The active use space adjacent to the southern public open space includes a patio adding an additional layer of activation and engagement to the open space. Similarly, the southeast corner amenity space includes a patio fronting the small public open space just to the north for additional activation.

### PE-6 UPPER-STORY ACTIVITIES OVERLOOKING THE STREET

All buildings on pedestrian-oriented streets should be encouraged to have upper-story activities overlooking the street, as well as balconies and roof decks with direct access from living spaces. Planting trellises and architectural elements are encouraged in conjunction with decks and bay windows. Upper-story commercial activities are also encouraged.

Resident outdoor amenity space and private residential decks are provided at upper story setbacks adding an additional level of activation along the pedestrian-oriented street frontages and through-block connections.

### PE-7 LIGHTING FROM BUILDINGS

All building entries should be well lit. Building facades in pedestrian areas should provide lighting to walkways and sidewalks through building-mounted lights, canopy or awning-mounted lights, and display window lights. Encourage variety in the use of light fixtures to give visual variety from one building facade to the next. Back-lit or internally-lit translucent awnings should be prohibited.

Masonry pilasters throughout the development are highlighted by accent lights that add detail to the facade and illumination to the street frontage. Additional down lights on canopies illuminate retail entries and sidewalks for pedestrian. Bollard lights line the southern through-block pathway since this wider walkway is further from the building frontage. Where soffits occur above apartment patios, soffits include lighting that will be controlled by residents. These features add to the safety of both residents and pedestrians.

# RESPONSES TO DESIGN GUIDELINES

GUIDELINE	RESPONSE	GUIDELINE	RESPONSE
<p><b>PE-8 PEDESTRIAN-ORIENTED PLAZAS</b></p> <p>Successful pedestrian-oriented plazas are generally located in sunny areas along a well-traveled pedestrian route. Plazas must provide plenty of sitting areas and amenities and give people a sense of enclosure and safety.</p> <p>Design all major pedestrian pathways to be at least 8' wide. Other pathways with less activity can be 6' wide.</p> <p><b>Special Considerations for Bridle Trails Neighborhood Center (BCX Zone)</b></p> <p>Plazas should be located facing pedestrian-friendly building fronts, near intersecting through-block pathways and where transit services are located. Plaza should be a focal point of public gather and seek opportunities to provide space for performances, passive and children's recreation.</p>	<p><b>PEDESTRIAN-ORIENTED PLAZAS</b></p> <p>The large public open space at the south end of the site has excellent solar access and responds well to the existing shopping center organization. Ringed by seat walls and lush planting, the center of the open space includes non-permanent seating to maintain flexibility. Located adjacent to active uses and active use patios, the open space is ideal for larger gatherings.</p> <p>On the north side of the site, retail or restaurants and the primary residential lobby surround and activate a large public plaza on 70th. Located near the block's bus stop, the plaza provides opportunities for gathering and passive recreation.</p>	<p><b>PI-2 PEDESTRIAN PATHS AND AMENITIES</b></p> <p><b>Special Considerations for Bridle Trails Neighborhood Center (BCX Zone)</b></p> <p>Streets and pathways should enhance the pedestrian experience and find opportunities to provide passive seating areas, cafe seating, green space, small scale performance area, public art, and children's recreation and small public gathering space. Sidewalks along 130th Avenue NE should enhance the City's Greenways connection.</p>	<p><b>PEDESTRIAN PATH AND AMENITIES</b></p> <p>The landscape design throughout includes plenty of seat walls, places for tables, and larger open areas appropriate for gathering or active recreation. On 70th and in the 70th plaza, seat walls and tables create pleasant places for shoppers to linger while in the smaller opens spaces along the north-south through-block connection, public amenities focus on resting places for pedestrians climbing the hill and places for smaller groups and conversations. A comfortable sidewalk and ample planting along 130th enhance the pedestrian experience in keeping with the city's Greenways connection.</p>
<p><b>PE-9 PEDESTRIAN CONNECTIONS</b></p> <p>Commercial developments should have well defined, safe pedestrian walkways that minimize distances from the public sidewalk, the Cross Kirkland Corridor and Eastside Rail Corridor, and transit facilities to the internal pedestrian system and building entrances.</p>	<p><b>PEDESTRIAN CONNECTIONS</b></p> <p>The primary residential entry as well as commercial entries are located adjacent to NE 70th where the block's bus stop is located. Entries are easily identifiable and have clear, convenient access from the public sidewalk.</p>	<p><b>PI-3 STREET TREES</b></p> <p>The City should prepare a comprehensive street tree planting plan recommending species and generalized locations.</p>	<p><b>STREET TREES</b></p> <p>Street trees are provided along 70th and 130th per Kirkland code and the design guidelines. Although not required, the design treats through-block connections similarly and provides trees in a street tree like cadence.</p>
<p><b>PE-10 BLANK WALLS</b></p> <p>Blank walls should be avoided near sidewalks, parks, the Cross Kirkland Corridor and Eastside Rail Corridor, and pedestrian areas. Where unavoidable, blank walls should be treated with landscaping, art, or other architectural treatments.</p>	<p><b>BLANK WALLS</b></p> <p>The project does not have blank walls near sidewalks or other pedestrian areas.</p>	<p><b>PI-4 PUBLIC IMPROVEMENTS AND SITE FEATURES</b></p> <p>The Planning and Building Department, along with other City departments, should develop a set of public improvement and site feature standards for use in pedestrian-oriented business districts. The standards can be the same or unique for each district. A master plan for public spaces within a district should be adopted to coordinate placement of the features and otherwise carry out the Comprehensive Plan.</p>	<p><b>PUBLIC IMPROVEMENTS AND SITE FEATURES</b></p> <p>The design team has reviewed and is actively following applicable codes, guidelines and standards for this site. We will continue to work with city staff to ensure the project design meets the expectations for public improvements and site features.</p>
<p><b>PI-1 PATHWAY WIDTH</b></p> <p>Design all major pedestrian pathways to be at least 8' wide. Other pathways with less activity can be 6' wide.</p>	<p><b>PATHWAY WIDTH</b></p> <p>The east-west through-block connection varies in width but is over eight feet wide in all locations, providing plenty of space for residential amenities to spill out, as well as space for joggers, kids bike riding, strollers and other pedestrians. The wide through-block connection is safely separated from the adjacent shopping center parking lot by continuous planting and street trees.</p>	<p><b>PI-5 ENTRY GATEWAY FEATURES</b></p> <p>Construct entry gateway features at locations noted in the Comprehensive Plan. Gateways may be constructed in conjunction with commercial development. Emphasis should be placed on framing the view into the district.</p>	<p><b>ENTRY GATEWAY FEATURES</b></p> <p>This development site does not contain an entry gateway feature as noted in the Comprehensive Plan. The project design does appropriately celebrate important nodes for the area through unique materials, strong massing, and opening spaces.</p>
<p><b>Special Considerations for Bridle Trails Neighborhood Center (BCX Zone)</b></p> <p>The through-block pathway connecting NE 70th Place to shops and services within the neighborhood center should be designed with amenities and help transition pedestrian traffic from lower grade at the street to the more level grade within the neighborhood center. Design of this pathway should provide places for people to pause and gather and provide a planted buffer from vehicular traffic.</p>	<p>The project provides two smaller public open spaces along the north-south through-block connection in keeping with the special considerations for BCX. These open spaces provide stopping points along the through-block connection for pedestrians climbing grade between 70th and the center of the BCX zone and are enhanced with seating, and planting.</p>	<p><b>PI-6 PUBLIC ART</b></p> <p>Kirkland should continue its tradition of encouraging public art pieces.</p>	<p><b>PUBLIC ART</b></p> <p>The project team is researching and contacting local artists to find a good fit for this project and the Bridle Trails neighborhood.</p>
		<p><b>PL-1 PARKING LOCATIONS &amp; ENTRANCES</b></p> <p>Minimize the number of driveways by restricting curb cuts and by encouraging property and business owners to combine parking lot entrances and coordinate parking areas. Encourage side and rear yard parking areas by restricting parking in front yards. Require extensive screening where there is front yard parking.</p>	<p><b>PARKING LOCATIONS &amp; ENTRANCES</b></p> <p>Combined commercial and residential parking entries meet the city requirements for separation from adjacent driveways and are considerate of neighborhood traffic patterns. This project does not provide on grade, visible parking in any yards.</p>

**GUIDELINE**

**S-1 FENESTRATION PATTERNS**

Varied Window treatments should be encouraged. Ground floor uses should have large windows that showcase storefront displays to increase pedestrian interest. Architectural detailing at all window jambs, sills, and heads should be emphasized.

**S-2 ARCHITECTURAL ELEMENTS: DECKS, BAY WINDOWS, ARCADES, PORCHES**

Architectural building elements such as arcades, balconies, bay windows, roof decks, trellises, landscaping, awnings, cornices, friezes, art concepts, and courtyards should be encouraged.

**S-3 BUILDING MODULATION VERTICAL**

Vertical building modulation should be used to add variety and to make large buildings appear to be an aggregation of smaller buildings.

**Special Considerations for Bridle Trails Neighborhood Center (BCX Zone)**

Façades over 120 feet in length should incorporate vertical definition including substantial modulation of the exterior wall carried through all floors above the ground floor combined with changes in color and material.

**S-4 BUILDING MODULATION HORIZONTAL**

Horizontal building modulation may be used to reduce the perceived mass of a building and to provide continuity at the ground level of large building complexes. Building design should incorporate strong pedestrian-oriented elements at the ground level and distinctive roof treatments.

**RESPONSE**

**FENESTRATION PATTERNS**

Paired and singular punched window openings provide pattern to the facades — a language typical in residential — while finer grain muntins add character to the storefront windows at active uses.

**ARCHITECTURAL ELEMENTS: DECKS, BAY WINDOWS, ARCADES, PORCHES**

Balconies fronting the residential street emphasize the residential nature of the building and enhance the human scale of the facade. Roof decks are provided in places where the building steps back and patios are provided for apartment homes at grade. The building design also utilizes a variety of cornice shapes and profiles which add detail to the building modulation.

**BUILDING MODULATION VERTICAL**

All building facades are broken down with vertical modulation into segments shorter than 120 feet in length. Material changes add distinction between masses reinforcing how the massing feels more like a collection of smaller buildings.

**BUILDING MODULATION HORIZONTAL**

Street-facing portions of the building are required to have large setbacks which provide horizontal modulation and help reduce the apparent height of the building as perceived by pedestrians. While building setbacks are not required along the through-block connections, the building design does step back the massing along much of the southern frontage. Furthermore, the building design adds horizontal modulation and distinguishes the pedestrian realm through material changes. The lowest levels of the facade along the north-south and east-west through-block connection are clad in differentiated materials and capped with trim to add texture to the pedestrian experience and scale to the building. The roof line around the building is distinguished by differentiated cornices that add architectural detail to the building.

**GUIDELINE**

**S-5 UPPER STORY STEP BACKS**

**Special Considerations for Bridle Trails Neighborhood Center (BCX Zone)**

Buildings above the second story (or third story where applicable in the Downtown Plan) should utilize upper story step backs to create receding building forms as building height increases, allow for additional solar access, and maintain human scale at the street level.

The final arrangement of building mass should be placed in context with existing and/or planned improvements, solar access, important street corners, and orientation with the public realm. A rigid stair step or “wedding cake” approach to upper story step backs is not appropriate.

Decks and/or balconies should be designed so that they do not significantly increase the apparent mass of the building within the required upper story setback area.

In addition to applying setbacks to upper stories, building facades should be well modulated to avoid blank walls and provide architectural interest.

Along pedestrian oriented streets, upper story building facades should be stepped back to provide enough space for decks, balconies and other activities overlooking the street.

Landscaping on upper story terraces should be included where appropriate to soften building forms and provide visual interest. Continuous two or three story street walls should be avoided by incorporating vertical and horizontal modulations into the building form.

Limited areas of vertical three, four, or five story walls can be used to create vertical punctuation at key facades. Special attention to maintain an activated streetscape is important in these areas.

For properties on Park Lane which front multiple streets and upper story setbacks are proposed to be averaged, concentration of upper story building mass along Park Lane should be avoided.

**Open Space at Street Level**

- Reductions to required upper story setbacks may be appropriate where an equal amount of beneficial public open space is created at the street level consistent with the following principles:

**UPPER STORY STEP BACKS CONTINUED...**

- Public open space should be open to the sky except where overhead weather protection is provided (e.g. canopies and awnings).
- The space should appear and function as public space rather than private space. A combination of lighting, paving, landscaping and seating should be utilized to enhance the pedestrian experience within the public open space.
- Public open space should be activated with adjacent shops, outdoor dining, art, water features, and/or landscaping while still allowing enough room for pedestrian flow.
- Where substantial open space “trade-offs” are proposed, site context should be the primary factor in the placement of the public open space (e.g. important corners, solar access).

**RESPONSE**

**UPPER STORY STEP BACKS**

Prescriptive building setbacks are required along 70th and 130th, but the code does allow for averaging which provides opportunities for some areas of coplanar facade. These limited areas where the building continues from grade to roof line reduce the appearance of “wedding cake” massing as desired by the design guidelines. The areas with large setbacks provide horizontal modulation to help reduce the apparent height of the building as perceived by pedestrians.

Occupied roof decks are encouraged and provided in places where the building steps back. To minimize the height of the massing, parapets are kept low at roof decks and a secondary, glass rail is provided behind the parapet as a guardrail for safety.

**GUIDELINE**

**BM-1 ORNAMENTAL AND APPLIED ART**

Ornament and applied art should be integrated with the structures and the site environment and not haphazardly applied. Significant architectural features should not be hidden, nor should the urban context be overshadowed. Emphasis should be placed on highlighting building features such as doors, windows, eaves, and on materials such as wood siding and ornamental masonry. Ornament may take the form of traditional or contemporary elements. Original artwork or hand-crafted details should be considered in special areas.

**BM-2 COLOR**

Color schemes should adhere to the guidelines enumerated above. The use of a range of colors compatible within a coordinated color scheme should be encouraged.  
**Special Considerations for Bridle Trails Neighborhood Center (BCX Zone)**  
Special attention to the use of colors and materials should be used on a building's upper stories to reduce the appearance of taller buildings.

**BM-3 STREET CORNERS**

Buildings should be designed to architecturally enhance building corners.

**BM-4 SIGNS**

- All signs should be building-mounted or below 12' in height if ground mounted. Maximum height is measured from the top of the sign to the ground plane.
- No off-premises commercial signs, except public directional signs, should be permitted. No billboards should be permitted.
- Signs for individual parking stalls should be discouraged. If necessary, they should not be higher than necessary to be seen above bumpers. Parking lot signs should be limited to one sign per entrance and should not extend more than 12' above the ground.
- Neon signs, sculptural signs, and signs incorporating artwork are encouraged.
- Signs that are integrated with a building's architecture are encouraged.

**RESPONSE**

**ORNAMENTAL AND APPLIED ART**

The project team is researching and contacting local artists to find a good fit for this project and the Bridle Trails neighborhood. This artwork could take a variety of forms and the project team will look for ways to best integrate it into the building design. The building design does include ornamental trim above each window that echoes more traditional styles typically found in the surrounding residential neighborhood. Additionally, windows at active uses include ornamental muntins that add character and detail to these special uses.

**COLOR**

The building's design focuses on a natural and fresh color palette. A mottled brick has been chosen for the brick frame elements to add variety and depth to the facade and warm, earthy accent colors enhance the color palette. The upper most level fronting 70th is a neutral gray tone that helps the top floor disappear from view while the base of the building is grounded with darker colors. Wood-look board and batten material wraps massing corners adding texture, detail and richness in these areas.

**STREET CORNERS**

Many of building corners flanking residential courtyards, are highlighted by massing with contrasting cladding, and raised parapets. At the southwest corner a canopy wraps the frontage where there is amenity space highlighting the building corner as well as the unique use.

**SIGNS CONTINUED...**

- Shingle signs and blade signs hung from canopies or from building facades are encouraged.
- Traditional signs such as barber poles are encouraged.

**RESPONSE**

**SIGNS**

Retail signs are planned to hang below the storefront canopies adjacent to retail entries. This orientation works well for pedestrians. Additional signage is planned for atop the canopy which is easier for passing vehicles to identify.

**GUIDELINE**

**NF-1 VISUAL QUALITY OF LANDSCAPES**

The placement and amount of landscaping for new and existing development should be mandated through design standards. Special consideration should be given to the purpose and context of the proposed landscaping. The pedestrian/auto landscape requires strong plantings of a structural nature to act as buffers or screens. The pedestrian landscape should emphasize the subtle characteristics of the plant materials. The building landscape should use landscaping that complements the building's favorable qualities and screens its faults. Along the Cross Kirkland Corridor and Eastside Rail Corridor, landscape design should screen where necessary, but generally soften the edge between the public and private space to integrate and complement corridor functions.

**Special Considerations for Bridle Trails Neighborhood Center (BCX Zone)**

A combination of both street and private trees with associated landscaping should be used to help mitigate the urban edges of the neighborhood center adjacent to residential neighborhoods.

**NF-3 HEIGHT MEASUREMENT ON HILLSIDES**

The top of the building should roughly follow the slope of the existing terrain.










**RESPONSE**

**VISUAL QUALITY OF LANDSCAPES**

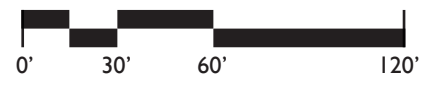
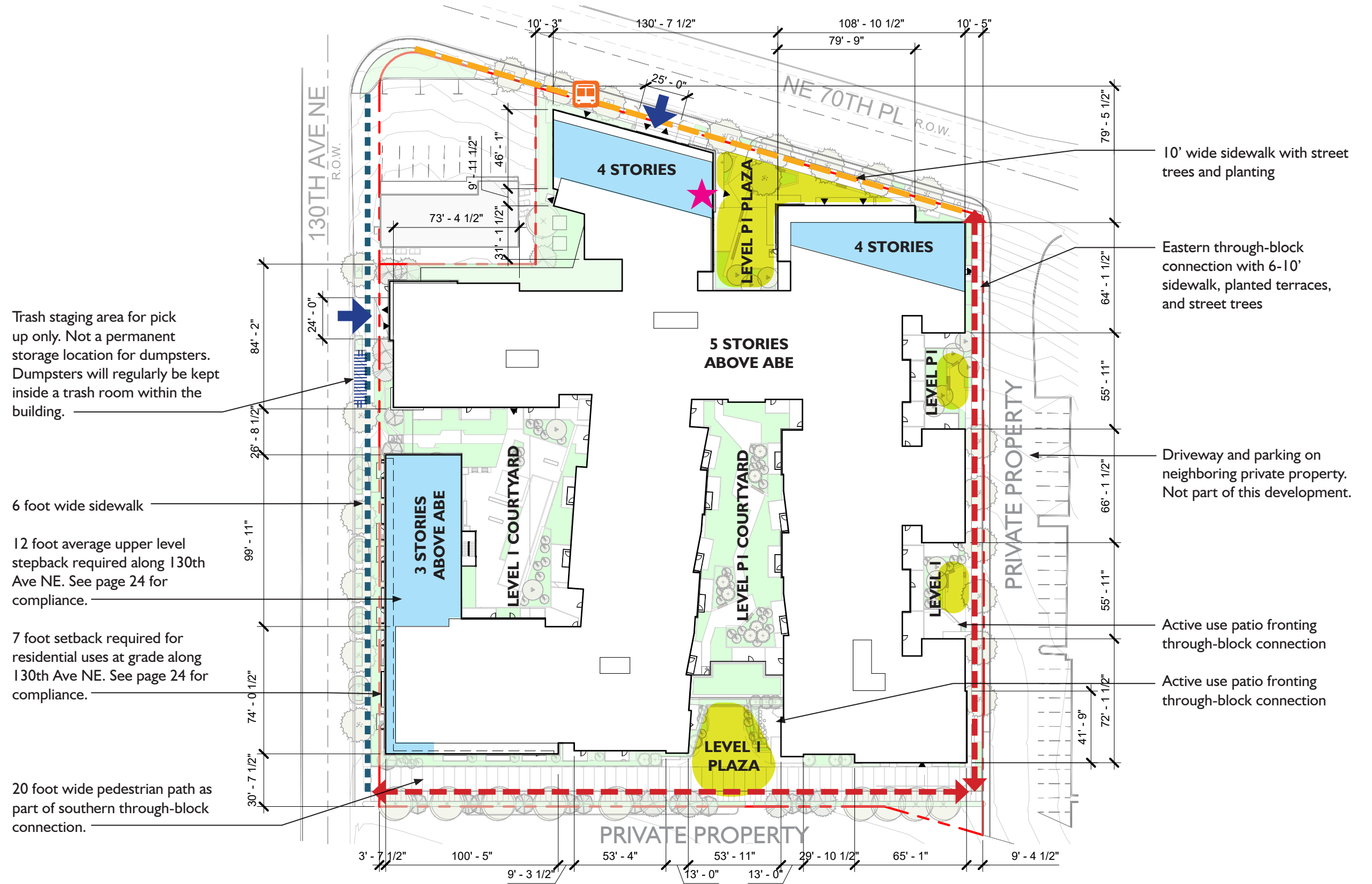
Street trees coupled with landscaping and additional on-site trees mimic the lush landscaping of the adjacent neighborhood and soften the development's frontage to feel coherent with the Bridle Trails neighborhood.

# SITE PLAN

**LOT COVERAGE:** Lot coverage is unlimited. Project is compliant.

-  PEDESTRIAN ORIENTED STREET
-  MAJOR PEDESTRIAN SIDEWALK
-  THROUGH-BLOCK CONNECTION
-  BUS STOP
-  PUBLICLY ACCESSIBLE PEDESTRIAN-ORIENTED OPEN SPACE\*
-  LOBBY ENTRY
-  GARAGE DRIVEWAY ENTRY
-  RESIDENTIAL AMENITY ROOF DECK
-  TRASH STAGING

\*Elsewhere in the packet, this term may be shorthanded for brevity.



# BUILDING MODULATION DIAGRAMS: RIGHT OF WAYS

- Major Vertical Modulation (Includes significant plane change)
- Secondary Vertical Modulation (Includes lesser plane change and/or material change)
- Grade to Roof line Vertically Distinct Area
- Major Horizontal Modulation (Includes significant plane change)
- Secondary Horizontal Modulation (Includes lesser plane change and/or material change)
- Major Cornice (Distinguishes roof line and emphasizes individual masses)
- Secondary Cornice (smaller roof line element that adds detail but also contrast from major cornices)
- Expressed Base Material
- Expressed Pedestrian Scaled Massing
- Canopy Plane



Northern Facade Modulation (NE 70th Place)

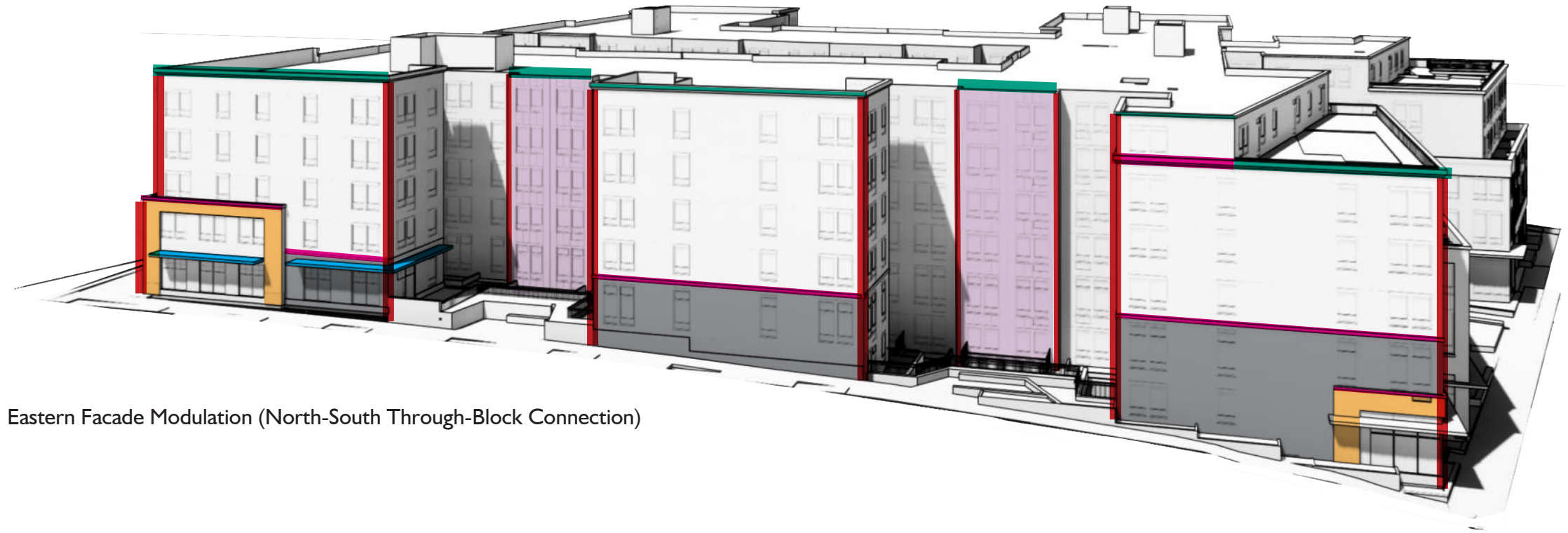


Western Facade Modulation (130th Ave NE)

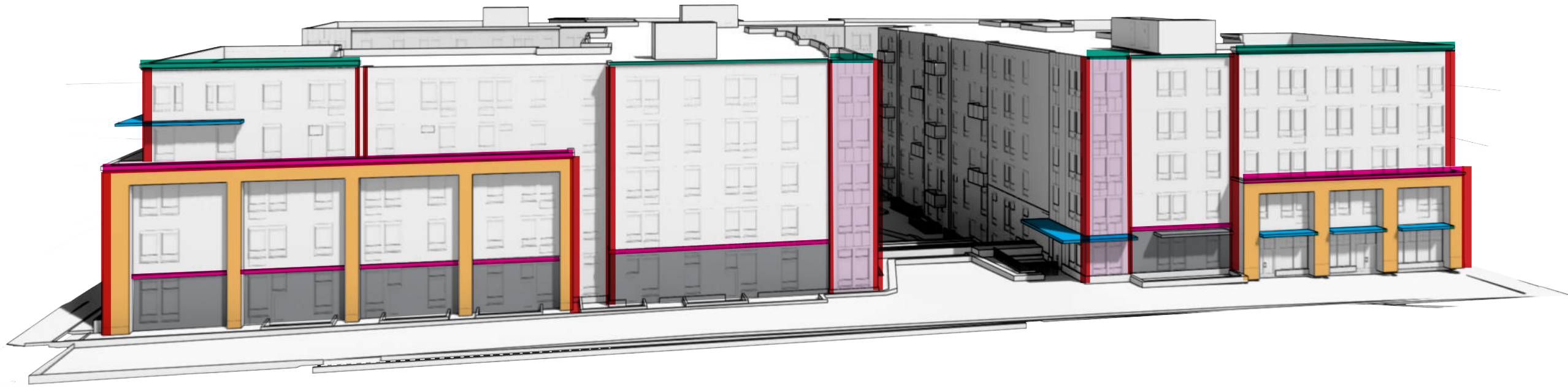


# BUILDING MODULATION DIAGRAMS: THROUGH-BLOCK CONNECTIONS

- Major Vertical Modulation (Includes significant plane change)
- Secondary Vertical Modulation (Includes lesser plane change and/or material change)
- Grade to Roof line Vertically Distinct Area
- Major Horizontal Modulation (Includes significant plane change)
- Secondary Horizontal Modulation (Includes lesser plane change and/or material change)
- Major Cornice (Distinguishes roof line and emphasizes individual masses)
- Secondary Cornice (smaller roof line element that adds detail but also contrast from major cornices)
- Expressed Base Material
- Expressed Pedestrian Scaled Massing
- Canopy Plane



Eastern Facade Modulation (North-South Through-Block Connection)



Southern Facade Modulation (East-West Through-Block Connection)

# NORTHERN FRONTAGE ARTICULATION



## PE-4 PEDESTRIAN COVERINGS

Canopies are provided along all of 70th. In keeping with larger development and arterial frontage guidance, the canopies are similar in design and style. However the canopy above the primary residential entry is located higher on the facade highlighting this entry and providing variety along the street frontage.

## PE-5 “PEDESTRIAN-FRIENDLY” BUILDING FRONTS

The ground-level facade between 2’ and 6’ above the sidewalk is primarily transparent providing views into active uses and retail. Commercial frontage is prioritized along 70th where commercial activity is likely to be most successful.

## PE-6 UPPER-STORY ACTIVITIES OVERLOOKING THE STREET

Resident outdoor amenity space and private residential decks are provided at upper story setbacks adding an additional level of activation along the pedestrian-oriented street frontage.

## PE-8 PEDESTRIAN-ORIENTED PLAZAS

Retail / restaurants and the primary residential lobby surround and activate a large public plaza on 70th. Located near the block’s bus stop, the plaza provides opportunities for gathering and passive recreation.

## S-1 FENESTRATION PATTERNS

Paired and singular punched window openings provide pattern to the facades — a language typical in residential — while finer grain muntins add character to the storefront windows at active uses.

## S-2 ARCHITECTURAL ELEMENTS: DECKS, BAY WINDOWS, ARCADES, PORCHES

The building design utilizes a variety of cornice shapes and profiles which add detail to the building modulation.

## S-4 & BUILDING MODULATION HORIZONTAL, S-5 & UPPER STORY STEP BACKS

While prescriptive building setbacks are required along 70th and 130th, the code does allow for averaging which provides opportunities for some areas of coplanar facade. These limited areas where the building continues from grade to roof line coupled with variations in the setback parapets reduce the appearance of “wedding cake” massing. The areas with large setbacks provide horizontal modulation to help reduce the apparent height of the building as perceived by pedestrians.

## BM-2 COLOR

The building’s design focuses on a natural and fresh color palette. A mottled brick has been chosen for the brick frame elements to add variety and depth to the facade and warm, earthy accent colors enhance the richness of the color palette. The upper most level fronting 70th is a neutral gray tone that helps the top floor disappear from view.

## BM-3 STREET CORNERS

Raised parapets, wrapping storefront windows, and material changes are architectural gestures that help highlight building corners.

# EASTERN FRONTAGE ARTICULATION



## PE-4 PEDESTRIAN COVERINGS

Although overhead weather protection is not required along the through-block connections, the development includes canopies in areas where there is active use frontage as a way to enhance pedestrian comfort and also enhance engagement between interior and exterior activities.

## PE-5 “PEDESTRIAN-FRIENDLY” BUILDING FRONTS

The ground-level facade between 2’ and 6’ above the sidewalk is primarily transparent at active uses to provide views into active spaces. Commercial frontage is prioritized along 70th where commercial activity is likely to be most successful. However, the project strategically places other active uses, like residential amenities along the through-block connections, to activate pedestrian experience.

## PI-1 PATHWAY WIDTH

The project provides two smaller public open spaces along the north-south through-block connection in keeping with the special considerations for BCX in design guidelines PI-1. These open spaces provide stopping points along the through-block connection for pedestrians climbing grade between 70th and the center of the BCX zone.

## S-1 FENESTRATION PATTERNS

Paired and singular punched window openings provide pattern to the facades — a language typical in residential — while finer grain muntins add character to the windows at active uses.

## S-2 ARCHITECTURAL ELEMENTS: DECKS, BAY WINDOWS, ARCADES, PORCHES

The building design utilizes a variety of cornice shapes and profiles which add detail to the building modulation.

## S-3 BUILDING MODULATION VERTICAL

Two, courtyard recesses in the eastern frontage break the frontage into segments shorter than 120 feet. This building modulation makes the massing feel more like a collection of smaller buildings along the eastern frontage. Above the pedestrian-scale base, the upper exterior material varies between massing segments, reinforcing their distinction from one another. The southern massing is clad in larger horizontal banding, while the center mass’s upper stories are clad in a similar, but smaller scale plank. The eastern mass utilizes the same panel as the upper facade on 70th.

## S-4 BUILDING MODULATION HORIZONTAL,

While building setbacks are not required along the through-block connections, the building design does add horizontal modulation and distinguishes the pedestrian realm through material changes. The lowest levels of the facade along the north-south through-block connection are clad in large horizontal bands reminiscent of stone modules and capped with trim adding texture to the pedestrian experience and scale to the building.

## BM-2 COLOR

The building’s design focuses on a natural and fresh color palette. A mottled brick has been chosen for the brick frame elements to add variety and depth to the facade and warm, earthy accent colors enhance the richness of the color palette. The upper most level fronting 70th is a neutral gray tone that helps the top floor disappear from view.

# SOUTHERN FRONTAGE ARTICULATION



## PE-4 PEDESTRIAN COVERINGS

Although overhead weather protection is not required along the through-block connections, the development includes canopies in areas where there is active use frontage as a way to enhance pedestrian comfort and also enhance engagement between interior and exterior activities.

## PE-5 “PEDESTRIAN-FRIENDLY” BUILDING FRONTS

The ground-level facade between 2’ and 6’ above the sidewalk is primarily transparent at active uses to provide views into active spaces. Commercial uses are prioritized along 70th where they are likely to be most successful. The project strategically places other active uses, like residential amenities along the through-block connections activating the pedestrian experience. The ground floor is kept to commercial height for future flexibility.

## PI-1 PATHWAY WIDTH

The east-west through-block connection varies in width but is over eight feet wide in all locations, providing plenty of space for residential amenities to spill out, as well as space for joggers, kids bike riding, strollers and other pedestrians. The wide through-block connection is safely separated from the adjacent shopping center parking lot by continuous planting and street trees.

## S-2 ARCHITECTURAL ELEMENTS: DECKS, BAY WINDOWS, ARCADES, PORCHES

The building design utilizes a variety of cornice shapes and profiles which add detail to the building modulation.

## S-3 BUILDING MODULATION VERTICAL

A large break in the frontage separates the massing into shorter building segments. The resulting two masses are further broken down by vertical gestures running from grade to roof line. These gestures are highlighted by plane changes in the massing as well as material changes on the facades and changes in the roof line. No segment on the southern frontage is over 120 feet long.

## S-4 & S-5 BUILDING MODULATION HORIZONTAL & UPPER STORY STEPBACKS

While building stepbacks are not required along the through-block connections, the building design does step back the massing along much of the southern frontage. Additionally the lowest levels of the facade are clad in large horizontal bands reminiscent of stone modules and capped with trim, further horizontally modulating the massing distinguishing the pedestrian realm.

## BM-2 COLOR

The building’s design focuses on a natural and fresh color palette. A mottled brick has been chosen for the brick frame elements to add variety and depth to the facade and warm, earthy accent colors enhance the richness of the color palette.

## BM-3 STREET CORNERS

At the facade opening at the southern facing residential courtyard, the building corners are highlighted by their massing, contrasting cladding, and raised parapets.

# WESTERN FRONTAGE ARTICULATION AT SOUTHWEST CORNER



## PE-6 UPPER-STORY ACTIVITIES OVERLOOKING THE STREET

Resident outdoor amenity space and private residential decks are provided at upper story setbacks adding an additional level of activation along the through-block connection.

## PI-I PATHWAY WIDTH

The east-west through-block connection varies in width but is over eight feet wide in all locations, providing plenty of space for residential amenities to spill out, as well as space for joggers, kids bike riding, strollers and other pedestrians. The wide through-block connection is safely separated from the adjacent shopping center parking lot by continuous planting and street trees.

## S-3 BUILDING MODULATION VERTICAL

A large break in the frontage separates the massing into shorter building segments. The resulting two masses are further broken down by vertical gestures running from grade to roof line. These gestures are highlighted by plane changes in the massing as well as material changes on the facades and changes in the roof line. No segment on the southern frontage is over 120 feet long.

## S-4 & S-5 BUILDING MODULATION HORIZONTAL & UPPER STORY STEP BACKS

While building setbacks are not required along the through-block connections, the building design does step back the massing along much of the southern frontage. Additionally the lowest levels of the facade are clad in a different material and capped with trim. This expression further horizontally modulates the massing and distinguishes the pedestrian realm, adding scale to the building.

## BM-2 COLOR

The building's design focuses on a natural and fresh color palette. A mottled brick has been chosen for the brick frame elements to add variety and depth to the facade and warm, earthy accent colors enhance the richness of the color palette.

## BM-3 STREET CORNERS

At the facade opening at the southern facing residential courtyard, the building corners are highlighted by their massing, contrasting cladding, and raised parapets. The upper level interior amenity space at the south west corner is similarly distinguished.

# WESTERN FRONTAGE ARTICULATION



**PE-6 UPPER-STORY ACTIVITIES OVERLOOKING THE STREET**

Resident outdoor amenity space and private residential decks are provided at upper story setbacks adding an additional level of activation along 130th street frontage.

**PL-1 PARKING LOCATIONS & ENTRANCES**

A parking entry is provided at the north end of 130th in a location where it is best separated from residential patios at grade and the intersection with the east-west through-block connection.

**S-1 FENESTRATION PATTERNS**

Paired and singular punched window openings provide pattern to the facades — a language typical in residential typology.

**S-2 ARCHITECTURAL ELEMENTS: DECKS, BAY WINDOWS, ARCADES, PORCHES**

Balconies fronting the residential street emphasize the residential nature of the building and enhance the human scale of the facade. The building design also utilizes a variety of cornice shapes and profiles which add detail to the building modulation.

**S-3 BUILDING MODULATION VERTICAL**

A large opening in the massing at the western facing courtyard breaks the building into two smaller massing segments. To the north, vertical gestures running from grade to roof line further break down the frontage. To the south, the frontage is broken down by the rhythm of brick pilasters that run grade to parapet and the groupings of paired balconies modulating the frontage.

**S-4 & S-5 BUILDING MODULATION HORIZONTAL & UPPER STORY STEP BACKS**

While prescriptive building setbacks are required along 70th and 130th, the code allows for averaging which provides opportunities for areas of coplanar facade. These limited areas coupled with the large portion of the frontage with no upper stories reduce the appearance of “wedding cake” massing. Additionally the lowest levels of the facade are clad differently and capped with trim further horizontally modulating the massing.

**BM-2 COLOR**

The building’s design focuses on a natural and fresh color palette. A mottled brick has been chosen for the brick frame elements to add variety and depth to the facade and warm, earthy accent colors enhance the richness of the color palette.

**BM-3 STREET CORNERS**

At the facade opening to the western facing residential courtyard, the building corners are highlighted by their massing, contrasting cladding, and raised parapets. At the southwest upper massing, a canopy wraps the frontage where there is amenity space highlighting the building corner as well as the unique use.

**NF-1 VISUAL QUALITY OF LANDSCAPES**

Street trees coupled with landscaping and additional trees mimic the lush landscaping of the adjacent neighborhood and soften the developments frontage to feel coherent with the Bridle Trails neighborhood.

# WESTERN FRONTAGE ARTICULATION AT THE NORTHWEST CORNER

## PE-4 PEDESTRIAN COVERINGS

Canopies are provided along all of 70th. In keeping with larger development and arterial frontage guidance, the canopies are similar in design and style. However the canopy above the primary residential entry is located higher on the facade highlighting this entry and providing variety along the street frontage.

## PE-5 “PEDESTRIAN-FRIENDLY” BUILDING FRONTS

The ground-level facade between 2’ and 6’ above the sidewalk is primarily transparent providing views into active uses and retail. Commercial frontage is prioritized along 70th where commercial activity is likely to be most successful.

## PE-6 UPPER-STORY ACTIVITIES OVERLOOKING THE STREET

Resident outdoor amenity space and private residential decks are provided at upper story setbacks adding an additional level of activation along the pedestrian-oriented street frontage.

## S-1 FENESTRATION PATTERNS

Paired and singular punched window openings provide pattern to the facades — a language typical in residential — while finer grain muntins add character to the storefront windows at active uses.

## S-4 & BUILDING MODULATION HORIZONTAL, S-5 & UPPER STORY STEPBACKS

While prescriptive building setbacks are required along 70th and 130th, the code allows for averaging which provides opportunities for areas of coplanar facade. These limited areas where the building continues from grade to roof line coupled with variations in the setback parapets reduce the appearance of “wedding cake” massing. The areas with large stepbacks provide horizontal modulation to help reduce the apparent height of the building as perceived by pedestrians.

## BM-2 COLOR

The building’s design focuses on a natural and fresh color palette. A mottled brick has been chosen for the brick frame elements to add variety and depth to the facade and warm, earthy accent colors enhance the richness of the color palette. The upper most level fronting 70th is a neutral gray tone that helps the top floor disappear from view.



# PEDESTRIAN VIEW ON NE 70TH



EXISTING PEDESTRIAN CONDITION



## PE-1 SIDEWALK WIDTH: MOVEMENT ZONE

A direct 10' wide sidewalk is provided along all NE 70th frontage in keeping with design guidelines as well as code requirements for pedestrian oriented streets.

## PE-2 SIDEWALK WIDTH: CURB ZONE

Street trees and other pedestrian amenities are provided within the curb zone of the sidewalk.

## PE-3 SIDEWALK WIDTH: THE STOREFRONT ACTIVITY ZONE

Retail storefronts angle away from the primary sidewalk, graciously opening towards the central plaza. This angle creates wider sidewalk areas along the retail frontage and opportunities for retail to spill out, engaging pedestrians without impeding their movement.

## PE-4 PEDESTRIAN COVERINGS

Canopies are provided along all of 70th. In keeping with larger development and arterial frontage guidance, the canopies are similar in design and style. However the canopy above the primary residential entry is located higher on the facade highlighting this entry and providing variety along the street frontage.

## PE-5 "PEDESTRIAN-FRIENDLY" BUILDING FRONTS

The ground-level facade between 2' and 6' above the sidewalk is primarily transparent providing views into active uses and retail. Commercial frontage is prioritized along 70th where commercial activity is likely to be most successful.

## PE-7 LIGHTING FROM BUILDINGS

Masonry pilasters along 70th are highlighted by accent lights that add detail to the facade and illumination to the street frontage. Additional down lights on the canopies illuminate retail entries and sidewalks for pedestrian ease and safety.

## PI-2 PEDESTRIAN PATH AND AMENITIES

The landscape design includes plenty of seat walls and places for tables, as well as more open areas appropriate for gathering or active recreation. On 70th and in the 70th plaza, built-in seat walls create pleasant places for shoppers to rest, set down belongings, or wait for a friend.

## PI-3 STREET TREES

Street trees are provided along 70th in keeping with Kirkland codes and design guidelines.

## BM-2 COLOR

The building's design focuses on a natural and fresh color palette. A mottled brick has been chosen for the brick frame elements to add variety and depth to the facade and warm, earthy accent colors enhance the richness of the color palette.

## BM-4 SIGNS

Retail signs are planned to hang below the storefront canopies adjacent to retail entries. This orientation works well for pedestrians. Additional signage is planned for atop the canopy which is easier for passing vehicles to identify.

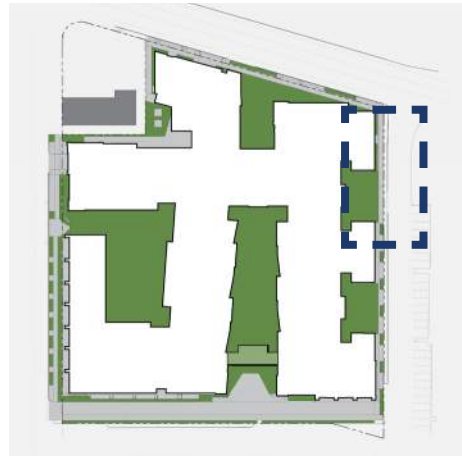


# EASTERN THROUGH-BLOCK PEDESTRIAN CONNECTION

## Courtyard B Birdseye Diagram



NOT TO SCALE



10' sidewalk: 6' clear, with 4' planting areas with street trees

Public resting area with seating and planting

Terraced planting beds

New 6' clear sidewalk with terraced street planting (7-18% slope to match adjacent roadway)

Existing East Drive (Private Property)

# PEDESTRIAN VIEWS ON EASTERN THROUGH-BLOCK CONNECTION

Looking North at Northern Public Open Space closer to 70th



Looking South at Southern Public Open Space Adjacent to Active Use Residential Amenity Spaces



## PE-5 “PEDESTRIAN-FRIENDLY” BUILDING FRONTS

Commercial frontage is prioritized along 70th where commercial activity is likely to be most successful. The project strategically places other active uses, like residential amenities along the through-block connections, to activate pedestrian experience. The ground-level facade between 2’ and 6’ above the sidewalk is primarily transparent at active uses to provide views into active spaces. Grade-related amenity spaces throughout the building maintain commercial floor heights so they may be converted at a later date if adjacent properties redevelop and the frontage becomes more conducive for true commercial use. The southeast corner amenity space includes a patio fronting the mini public open space. This provides an added layer of activation in what would otherwise be a residential courtyard.

## PE-4 PEDESTRIAN COVERINGS

Although overhead weather protection is not required along the through-block connections, the development includes canopies in areas where there is active use frontage as a way to enhance pedestrian comfort and engagement between interior and exterior activities.

## PI-1 PATHWAY WIDTH

The project provides two smaller public open spaces along the north-south through-block connection in keeping with the special considerations for BCX in design guidelines PI-1. These open spaces provide stopping points along the through-block connection for pedestrians climbing grade between 70th and the center of the BCX zone and are enhanced with seating, and planting.

## PI-2 PEDESTRIAN PATH AND AMENITIES

The landscape design includes plenty of seat walls and places for tables, as well as more open areas appropriate for gathering or active recreation. In the smaller open spaces along the north-south through-block connection, public amenities focus on resting places for pedestrians climbing the hill and places for smaller groups and conversations in lieu of large gatherings.

## PI-3 STREET TREES

Street trees are not required on through block connections. However, because this through-block is located adjacent to the shopping center’s primary vehicle entry, the design treats this front similarly to a street and includes street trees separating pedestrians from vehicles. Along this edge the design calls for conifers in keeping with the evergreens on the other side of the driveway.

## BM-2 COLOR

The building’s design focuses on a natural and fresh color palette. A mottled brick has been chosen for the brick frame elements to add variety and depth to the facade and warm, earthy accent colors enhance the richness of the color palette. The upper most level fronting 70th is a neutral gray tone that helps the top floor disappear from view.

# PEDESTRIAN VIEWS ON SOUTHERN THROUGH-BLOCK CONNECTION



EXISTING PEDESTRIAN CONDITION



## PE-5 “PEDESTRIAN-FRIENDLY” BUILDING FRONTS

Commercial frontage is prioritized along 70th where commercial activity is likely to be most successful. The project strategically places other active uses, like residential amenities along the through-block connections, to activate pedestrian experience. The ground-level facade between 2’ and 6’ above the sidewalk is primarily transparent at active uses to provide views into active spaces. Grade-related amenity spaces throughout the building maintain commercial floor heights so they may be converted at a later date if adjacent properties redevelop and the frontage becomes more conducive for true commercial use. The active use space adjacent to the large public open space includes a patio adding an additional layer of activation and engagement between the two spaces.

## PE-4 PEDESTRIAN COVERINGS

Although overhead weather protection is not required along the through-block connections, the development includes canopies in areas where there is active use frontage as a way to enhance pedestrian comfort and engagement between interior and exterior activities.

## PE-7 LIGHTING FROM BUILDINGS

Masonry pilasters are highlighted by accent lights that add detail to the facade and illumination to the street frontage. Additional down lights on the canopies illuminate entries, while bollard lights line the through-block pathway adding to pedestrian safety.

## PE-8 PEDESTRIAN-ORIENTED PLAZAS

The large public open space at the south end of the site has excellent solar access and responds well to the existing shopping center organization. Ringed by seat walls and lush planting, the center of the open space includes non-permanent seating to maintain flexibility. Located adjacent to active uses and active use patios, the open space is ideal for larger gatherings.

## PI-1 PATHWAY WIDTH

In keeping with the larger public open space centered on the southern frontage, and the wide open spaces of the adjacent shopping center, the east-west through-block connection is wider than required. The additional width allows for a variety of activities and pedestrian needs.

## PI-2 PEDESTRIAN PATH AND AMENITIES

The landscape design includes plenty of seat walls and places for tables, as well as more open areas appropriate for gathering or active recreation.

## BM-2 COLOR

The building’s design focuses on a natural and fresh color palette. A mottled brick has been chosen for the brick frame elements to add variety and depth to the facade and warm, earthy accent colors enhance the richness of the color palette.

# PEDESTRIAN VIEW ON I30TH AVE NE



EXISTING PEDESTRIAN CONDITION

## PE-6 UPPER-STORY ACTIVITIES OVERLOOKING THE STREET

Resident outdoor amenity space and private residential decks are provided at upper story setbacks adding an additional level of activation along I30th street frontage.

## PE-7 LIGHTING FROM BUILDINGS

Masonry pilasters along I30th are highlighted by accent lights that add detail to the facade and illumination to the street frontage. Soffits above apartment patios have lighting as well that will be controlled by residents. These features add to the safety of both residents and pedestrians.

## PI-2 PEDESTRIAN PATHS AND AMENITIES

A comfortable sidewalk and ample planting along I30th enhance pedestrian experience in keeping with the city's Greenways connection.

## S-2 ARCHITECTURAL ELEMENTS: DECKS, BAY WINDOWS, ARCADES, PORCHES

Balconies fronting the residential street emphasize the residential nature of the building and enhance the human scale of the facade. The building design also utilizes a variety of cornice shapes and profiles which add detail to the building modulation.

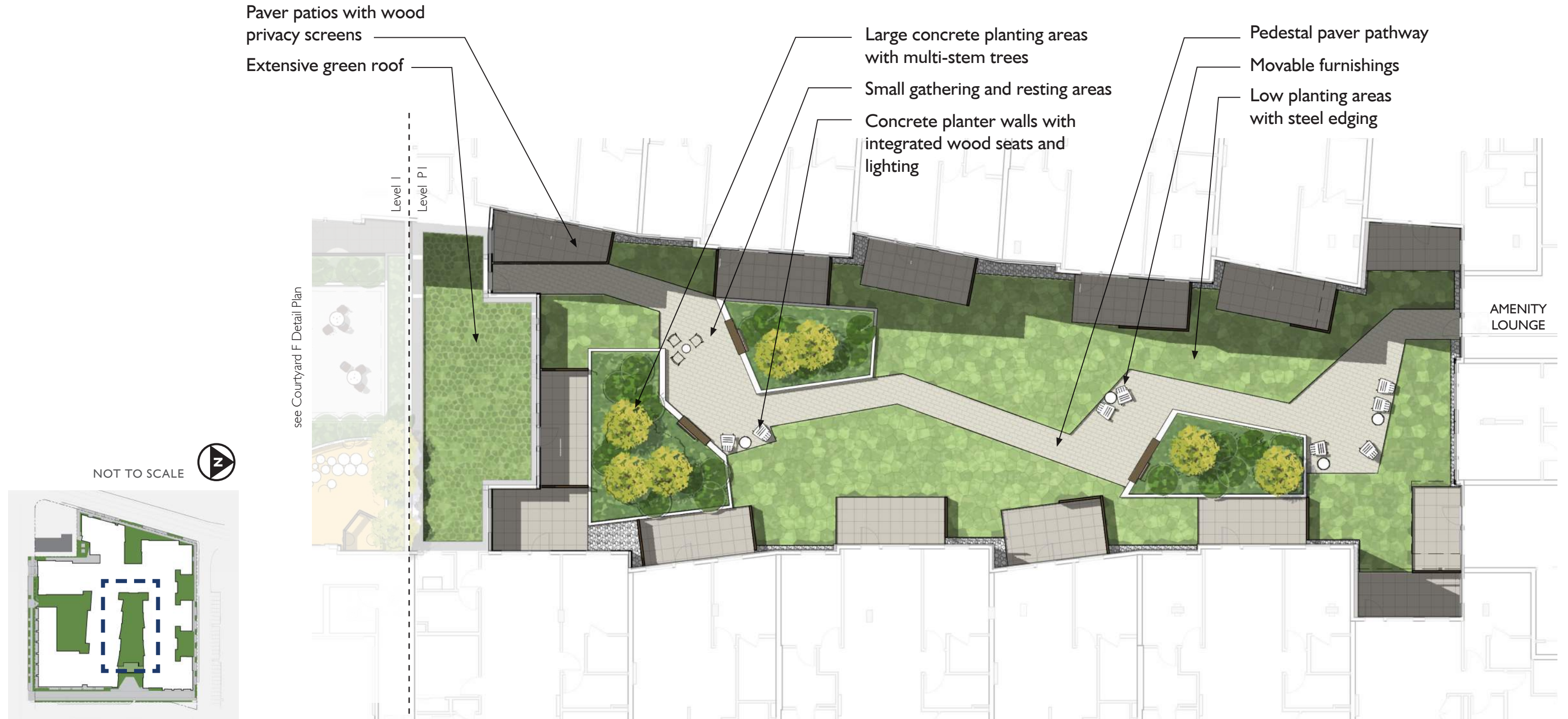
## NF-1 VISUAL QUALITY OF LANDSCAPES

Street trees coupled with landscaping and additional trees mimic the lush landscaping of the adjacent neighborhood and soften the developments frontage to feel coherent with the Bridle Trails neighborhood.



# PRIVATE RESIDENTIAL COURTYARDS

## Courtyard D, Level P1 Detail Plan



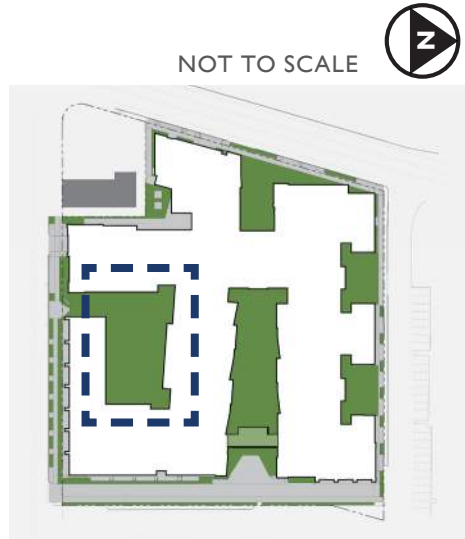
# PRIVATE RESIDENTIAL COURTYARDS

Courtyard D Inspirations and View



# PRIVATE RESIDENTIAL COURTYARDS

## Courtyard E, Level I Detail Plan



# PRIVATE RESIDENTIAL COURTYARDS

Courtyard E Inspirations and View





# PRIVATE RESIDENTIAL COURTYARDS

## Roofdeck Amenity Level 4 Detail Plan



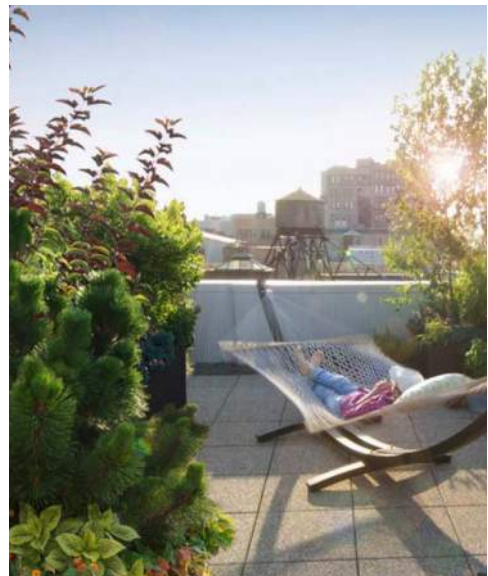
see Courtyard F Detail Plan

NOT TO SCALE



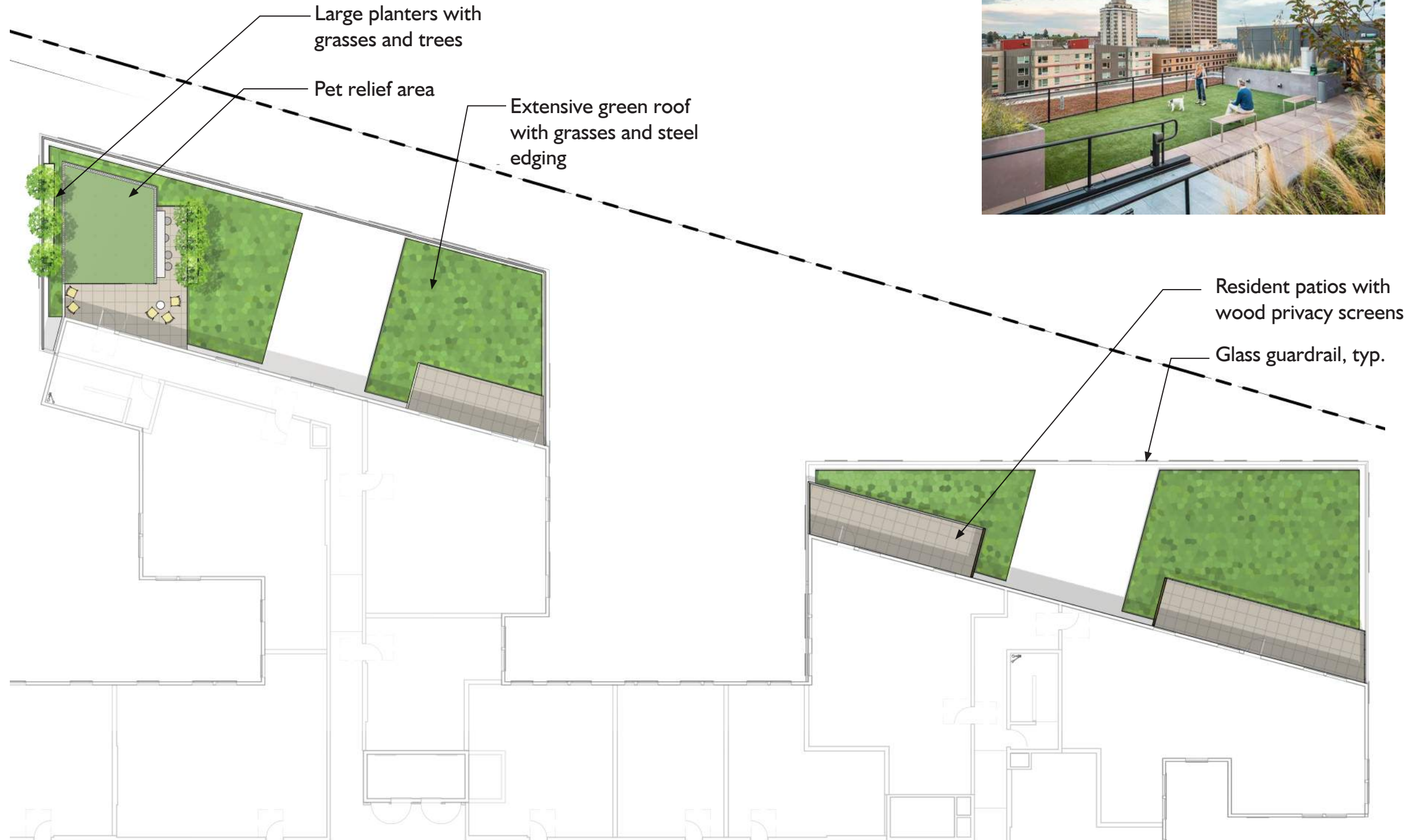
# PRIVATE RESIDENTIAL COURTYARDS

Roofdeck Amenity Level 4 Inspirations and Rendering

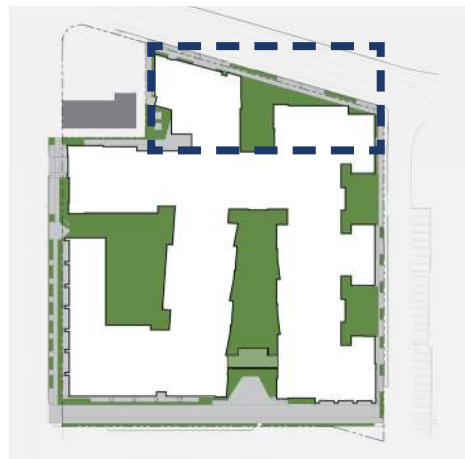


# PRIVATE RESIDENTIAL COURTYARDS

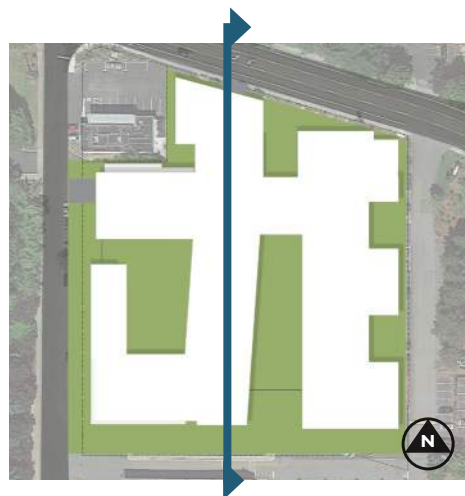
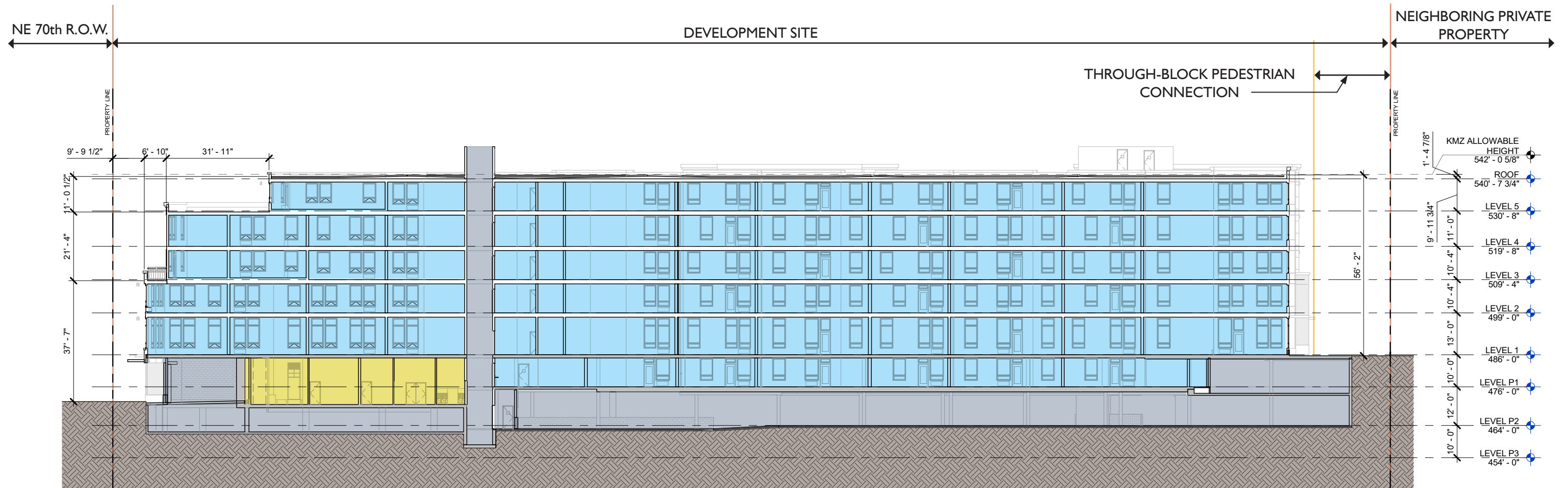
## Roofdeck Amenity Level 5 Detail Plan



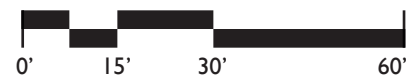
NOT TO SCALE



# NORTH-SOUTH BUILDING SECTION

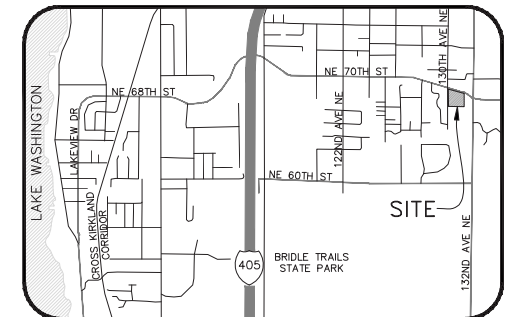


- COMMERCIAL
- ACTIVE USE / RESIDENTIAL AMENITY
- RESIDENTIAL LOBBY
- RESIDENTIAL APARTMENT HOMES
- PARKING, MECHANICAL, AND BACK OF HOUSE



NE 1/4 OF SE 1/4, SECTION 09, TOWNSHIP 25 NORTH, RANGE 05 EAST, W.M.

<ul style="list-style-type: none"> <li>ASPHALT (ASPH)</li> <li>BUILDING ENTRANCE</li> <li>BOLLARD (BLRD)</li> <li>BUILDING LINE</li> <li>BUILDING CORNER</li> <li>BIKE RACK</li> <li>CANOPY</li> <li>CATCH BASIN (CB)</li> <li>CONCRETE SURFACE</li> <li>ASPHALT/CONCRETE/EXTRUDED CURB &amp; GUTTER</li> <li>CONCRETE/BRICK/ASPHALT WALK</li> <li>CONCRETE/BLOCK/WOOD RETAINING WALL</li> <li>CHAIN LINK FENCE (CLF)</li> </ul>	<ul style="list-style-type: none"> <li>COLUMN CENTERLINE/MONUMENT LINE</li> <li>CONCRETE/METAL/WOOD STAIRS</li> <li>H/C PARKING SPACE</li> <li>CONIFEROUS TREE</li> <li>DECIDUOUS TREE</li> <li>DRIVEWAY</li> <li>ELECTRICAL CONDUIT (BURIED)</li> <li>ELECTRICAL METER</li> <li>ELECTRICAL VAULT/TRANSFORMER</li> <li>FOUND SURVEY MONUMENT (AS NOTED)</li> <li>FIRE HYDRANT</li> <li>FIRE DEPT. CONNECTION (FDC)</li> <li>FIBER OPTIC CONDUIT</li> <li>FIBER OPTIC VAULT</li> </ul>	<b>LEGEND</b> <ul style="list-style-type: none"> <li>G GAS MAIN</li> <li>GM GAS METER</li> <li>ID GAS VALVE</li> <li>H HANDRAIL</li> <li>INLET (TYPE 2525)</li> <li>L LANDSCAPE/PLANTER</li> <li>M MANHOLE</li> <li>MAILBOX (FEDERAL/PRIVATE)</li> <li>OVERHEAD POWER/FIBER</li> <li>P.P.S. PARKING SPACE(S)</li> <li>PROPERTY LINE (PL)</li> <li>CONCRETE/IRON/METAL/CLAY (PIPE TYPES)</li> <li>POST INDICATOR VALVE</li> <li>PEDESTRIAN PUSH BUTTON (PPB)</li> </ul>	<ul style="list-style-type: none"> <li>RE/PPPT ROOF/PARAPET ELEVATION</li> <li>ROCKERY</li> <li>RE ROOF ELEVATION</li> <li>SDCO/SSCO STORM DRAIN/SANITARY SEWER CLEANOUT</li> <li>SS SANITARY SEWER</li> <li>SS SIGN/STREET NAME SIGN</li> <li>TS TELEPHONE SENTRY</li> <li>TRAFFIC FLOW DIRECTION</li> <li>ULBL UPPER LEVEL BUILDING LINE</li> <li>W WATER MAIN</li> <li>WM WATER METER</li> <li>WV WATER VALVE</li> <li>VO/CO VACATION/CONDEMNATION ORDINANCE</li> <li>WOOD FENCE (WF)</li> </ul>
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VICINITY MAP  
NO SCALE

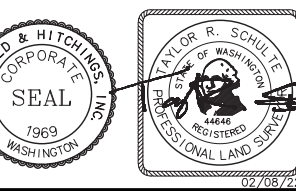
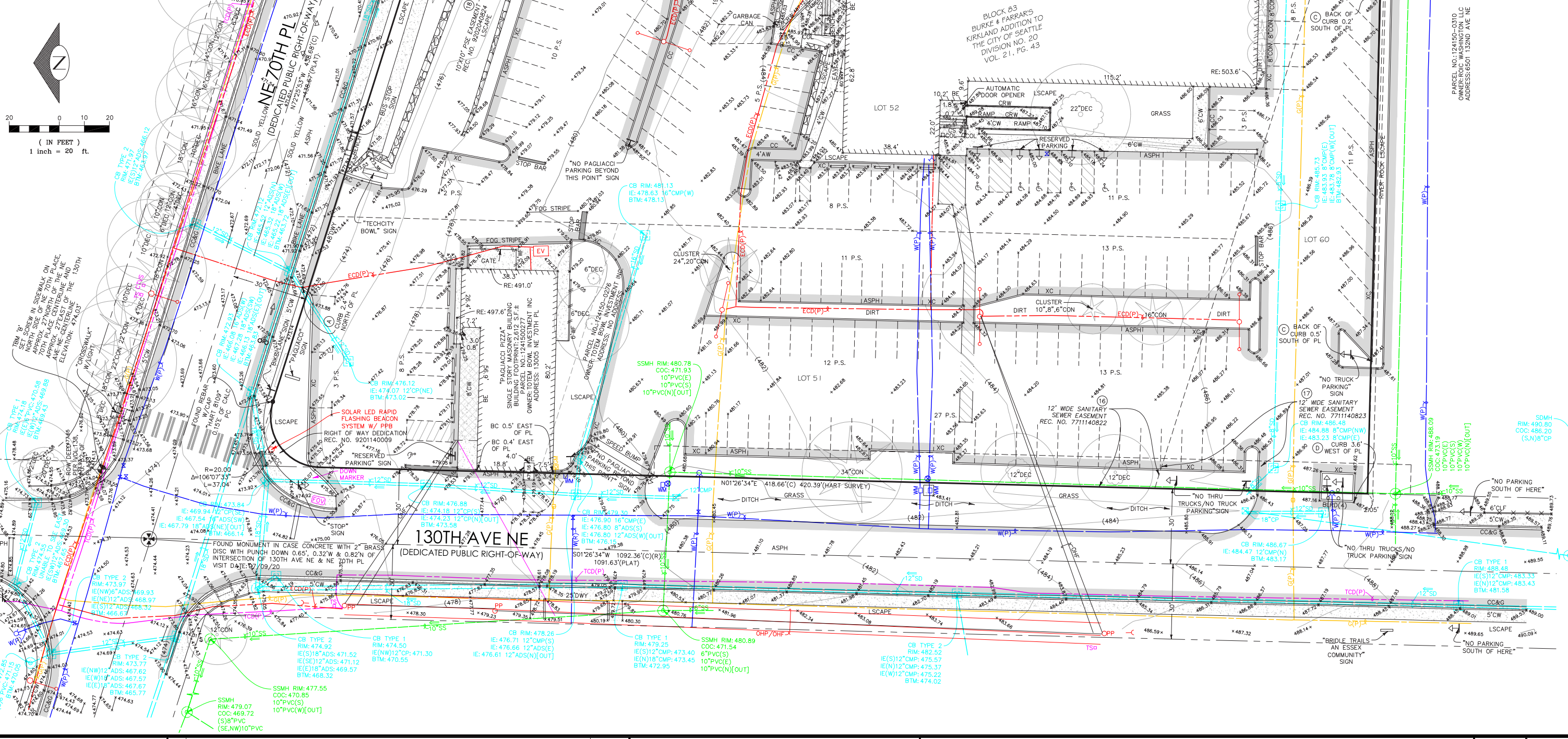
S01°29'42"W 529.84'(C)  
529.51'(DEED)

FOUND MONUMENT IN CASE  
CONCRETE WITH 2" BRASS DISC  
WITH PUNCH  
DOWN 1.4'  
VISIT DATE: 07/09/20

132ND AVE NE  
(DEDICATED PUBLIC RIGHT-OF-WAY)

FOUND PC MONUMENT IN CASE  
CONCRETE WITH 2" BRASS DISC  
WITH PUNCH, DOWN 0.65', 0.2'S  
OF CENTERLINE OF NE 70TH PL  
VISIT DATE: 07/09/20

SEE SHEET 2



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**TOPOGRAPHIC & BOUNDARY SURVEY**  
**MILL CREEK RESIDENTIAL TRUST**  
**130TH AVE NE AND NE 70TH PL**

drawn by	checked by
HAK	TRS
scale	date
1"=20'	02/08/22
job no.	
2020093.03	
sheet	of
1	2

KIRKLAND KING COUNTY WASHINGTON

22"x34" 2/8/2022 \\BRH02\CAD\C3D\2020093\SURVEYING\DWG\XS-SUR-03.DWG

STATEMENT OF TOPOGRAPHIC MAP ELEMENTS (WAC 332-130-145)

2(b) PURPOSE: FEASIBILITY AND DESIGN

2(c) CONTOUR SOURCE: CONTOURS DERIVED FROM DIRECT FIELD OBSERVATIONS

2(f) CONTOUR ACCURACY: COMPLIES WITH UNITED STATES NATIONAL MAP ACCURACY STANDARDS (90% OR GREATER OF ALL SURVEY POINTS CHECKED ARE CORRECT WITHIN HALF OF ONE CONTOUR INTERVAL).

2(g) LIMITATIONS: THE PURPOSE OF THIS LIMITED TOPOGRAPHIC SURVEY IS TO SUPPORT SPECIFIC EFFORTS WITHIN THE AREA OF MAPPING SHOWN, AND IS NOT INTENDED TO ILLUSTRATE ANY TITLE INVESTIGATIONS.

2(h) BOUNDARY SOURCE: FIELD SURVEY OF CONTROLLING MONUMENTS, AND CONSIDERATION OF EXISTING RECORDS OF SURVEYS FOR DETERMINING ON THE GROUND POSITIONS OF DEEDED PROPERTY AND EASEMENT LINES.

3(a) & 3(b) UTILITIES: UNDERGROUND UTILITIES ARE SHOWN BY ONE OR MORE OF THE FOLLOWING METHODS:

1. SURVEY FIELD OBSERVATION OF MARKINGS PRODUCED BY DIRECT UTILITY DETECTION WORK.
2. DIRECT OBSERVATIONS OF UNDERGROUND, GRAVITY FLOW PIPES PERFORMED AT VISIBLE CONTROLLING STRUCTURES.
3. SCALING OF AS-BUILTS, DESIGN DRAWINGS OR OTHER RECORDS.

3(c) SCOPE STATEMENT: UTILITY INVESTIGATIONS ARE SUBJECT TO THE LIMITATIONS OF ACCURACY OF CONVENTIONAL UNDERGROUND UTILITY DETECTION EQUIPMENT, THE EXISTENCE / ACCURACY OF RECORD UTILITY MAPS PRODUCED BY OTHERS, OR THE AWARENESS OR LOCAL KNOWLEDGE OF ANYTHING CONCEALED UNDERGROUND. THE COMPREHENSIVENESS OF SAID INVESTIGATIONS ARE THEREFORE LIMITED TO THE CAPACITY OF SAID TECHNOLOGIES AND /OR THE AVAILABILITY OF SUCH RECORDS OR KNOWLEDGE.

HORIZONTAL DATUM: NAD 83/2011 (EPOCH 2010)

**BENCHMARKS:**

**OWNER:** CITY OF KIRKLAND  
**ID#:** NONE  
**DESCRIPTION:** FOUND 2" BRASS DISC WITH PUNCH, IN CASE  
**LOCATION:** ±413'NW FROM 130TH AVE NE, ALONG CENTERLINE OF NE 70TH PL  
**ELEVATION:** 246.165.43

**NORTHING:** 246165.43  
**EASTING:** 1312297.08

**OWNER:** CITY OF KIRKLAND  
**ID#:** NONE  
**DESCRIPTION:** FOUND 2" BRASS DISC WITH PUNCH, IN CASE  
**LOCATION:** 0.32'W & 0.82'N OF INTERSECTION OF 130TH AVE NE & NE 70TH PL  
**NORTHING:** 246291.08  
**EASTING:** 1311903.04

VERTICAL DATUM: NAVD 88

**BENCHMARKS:**

**OWNER:** CITY OF KIRKLAND  
**ID#:** 16/REDMOND GLO-2FW  
**DESCRIPTION:** 2" BRASS DISC WITH PUNCH  
**LOCATION:** CENTERLINE OF INTERSECTION AT 132ND AVE NE & NE 70TH ST  
**ELEVATION:** 466.28'

**OWNER:** BRH  
**ID#:** 'A'  
**DESCRIPTION:** SET SCREW  
**LOCATION:** SET IN SIDEWALK ON NORTH SIDE OF NE 70TH PLACE, APPROX. 56' NORTH OF THE NORTHEAST PROPERTY CORNER  
**ELEVATION:** 466.38'

**SUBSTRUCTURES:** BURIED UTILITIES ARE SHOWN AS INDICATED ON RECORDS MAPS FURNISHED BY OTHERS AND VERIFIED WHERE POSSIBLE BY FEATURES LOCATED IN THE FIELD. WE ASSUME NO LIABILITY FOR THE ACCURACY OF THOSE RECORDS. FOR THE FINAL LOCATION OF EXISTING UTILITIES IN AREAS CRITICAL TO DESIGN CONTACT THE UTILITY OWNER/AGENCY.

**TELECOMMUNICATIONS/FIBER OPTIC DISCLAIMER:** RECORDS OF UNDERGROUND TELECOMMUNICATIONS AND/OR FIBER OPTIC LINES ARE NOT ALWAYS AVAILABLE TO THE PUBLIC. BRH HAS NOT CONTACTED EACH OF THE MANY COMPANIES IN THE COURSE OF THIS SURVEY, WHICH COULD HAVE UNDERGROUND LINES WITHIN ADJACENT RIGHTS-OF-WAY. THEREFORE, BRH DOES NOT ACCEPT RESPONSIBILITY FOR THE EXISTENCE OF UNDERGROUND TELECOMMUNICATIONS/FIBER OPTIC LINES WHICH ARE NOT MADE PUBLIC RECORD WITH THE LOCAL JURISDICTION. AS ALWAYS, CALL 1-800-424-5555 BEFORE CONSTRUCTION.

NE 1/4 OF SE 1/4, SECTION 09, TOWNSHIP 25 NORTH, RANGE 05 EAST, W.M.

**SITE NOTES**

**SITE ADDRESS:** 13005 & 13033 N.E. 70TH PLACE, KIRKLAND, WA 98033

**TAX ACCOUNT NOS.:** 124150-0276-03 (PORTION OF LOT 51), 124150-0277-02 (PORTION OF LOT 51), 124150-0285-02 (PORTION OF LOT 51 AND LOTS 52, 53, 54 & 60)

**ZONING:** BCX, COMMERCIAL

**ZONING AGENCY:** CITY OF KIRKLAND, DEPARTMENT OF PLANNING AND COMMUNITY DEVELOPMENT, 123 5TH AVENUE, KIRKLAND, WA 98033-6189, (425) 587-3600, FAX: (425) 587-3232

**SETBACKS:** CURRENT SETBACK REQUIREMENTS SUBJECT TO SITE PLAN REVIEW. CURRENT SETBACKS MAY DIFFER FROM THOSE IN EFFECT DURING DESIGN/CONSTRUCTION OF EXISTING IMPROVEMENTS.

**FLOOD ZONE:** THIS SITE APPEARS ON NATIONAL FLOOD INSURANCE RATE MAP, DATED AUGUST 19, 2020, COMMUNITY PANEL NO. 530330366H, AND IS SITUATED IN ZONE "X". AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN.

**HORIZONTAL DATUM:** NAD 83/2011 (EPOCH 2010.00)

**AREA:** SITE AS SHOWN CONTAINS 138,851 SQUARE FEET OR 3.1876 ACRES, MORE OR LESS.

**PARKING SPACE COUNT:** PARKING SPACES TOTAL 233 INCLUDING 8 DISABLED PARKING SPACES.

**DESCRIPTION:**

LOT 51, EXCEPT THAT PORTION CONVEYED TO THE CITY OF KIRKLAND BY DEED RECORDED JANUARY 14, 1992 UNDER RECORDING NUMBER 9201140009, AND LOTS 52, 53 AND 54 AND THE NORTH 85 FEET OF THE LOT 60 (MEASURED AT RIGHT ANGLES TO 130TH AVENUE NORTHEAST), BLOCK 83, BURKE & FARRAR'S KIRKLAND ADDITION TO THE CITY OF SEATTLE DIVISION NUMBER 20, ACCORDING TO THE PLAT THEREOF RECORDED IN VOLUME 21 OF PLATS, PAGE 43, IN KING COUNTY, WASHINGTON;

AND EXCEPT THAT PORTION OF SAID LOT 54 LYING EASTERLY OF A LINE DESCRIBED AS FOLLOWS:

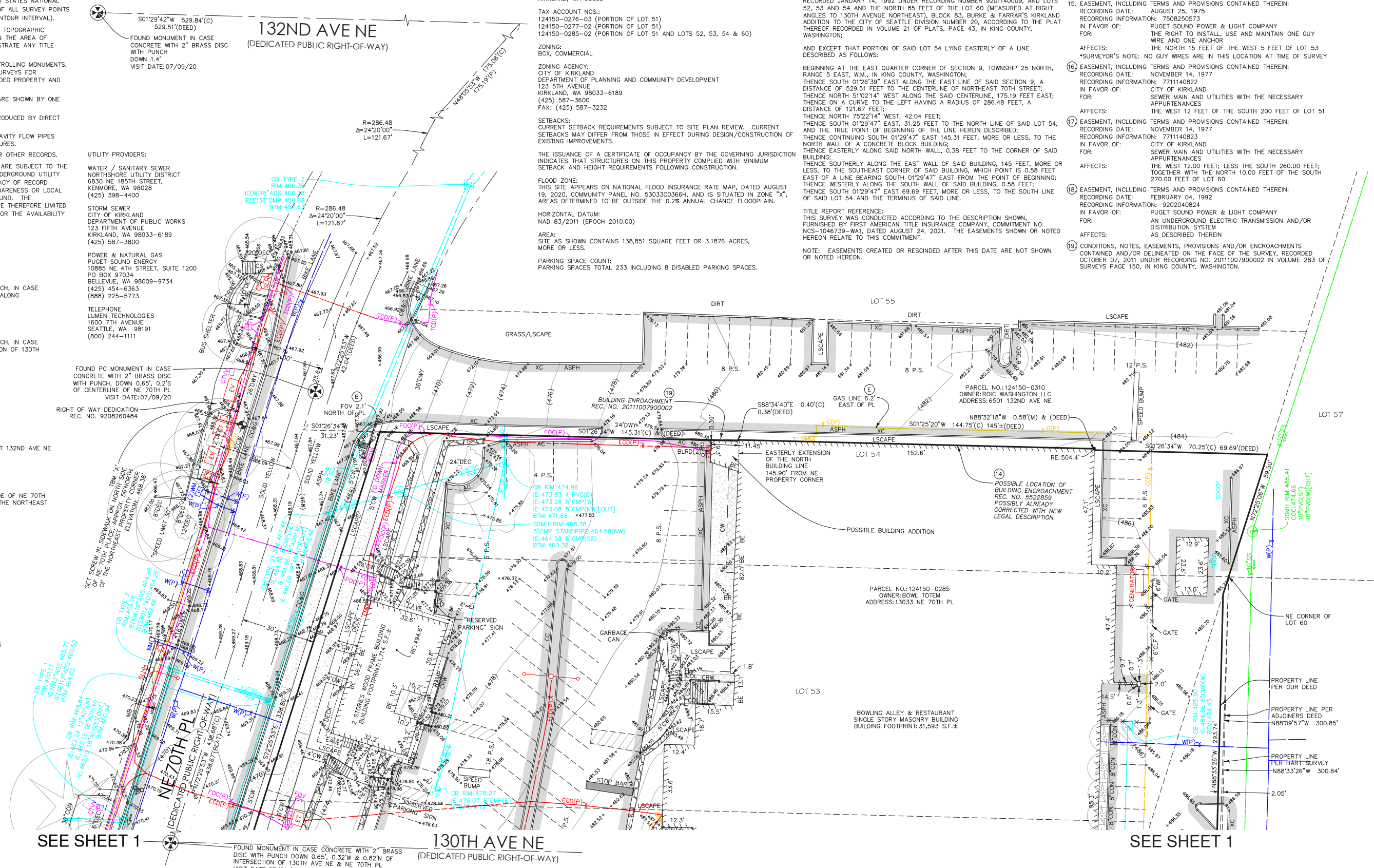
BEGINNING AT THE EAST QUARTER CORNER OF SECTION 9, TOWNSHIP 25 NORTH, RANGE 5 EAST, W.M., IN KING COUNTY, WASHINGTON;  
 THENCE SOUTH 01°29'47" EAST ALONG THE EAST LINE OF SAID SECTION 9, A DISTANCE OF 529.51 FEET TO THE CENTERLINE OF NORTHEAST 70TH STREET;  
 THENCE NORTH 51°02'14" WEST ALONG THE SAID CENTERLINE, 175.19 FEET EAST;  
 THENCE ON A CURVE TO THE LEFT HAVING A RADIUS OF 286.48 FEET, A DISTANCE OF 121.67 FEET;  
 THENCE NORTH 75°22'14" WEST, 42.04 FEET;  
 THENCE SOUTH 01°29'47" EAST, 31.25 FEET TO THE NORTH LINE OF SAID LOT 54, AND THE TRUE POINT OF BEGINNING OF THE LINE HEREIN DESCRIBED;  
 THENCE CONTINUING SOUTH 01°29'47" EAST 145.31 FEET, MORE OR LESS, TO THE NORTH WALL OF A CONCRETE BLOCK BUILDING;  
 THENCE EASTERLY ALONG SAID NORTH WALL, 0.38 FEET TO THE CORNER OF SAID BUILDING;  
 THENCE SOUTHERLY ALONG THE EAST WALL OF SAID BUILDING, 145 FEET, MORE OR LESS, TO THE SOUTHEAST CORNER OF SAID BUILDING, WHICH POINT IS 0.58 FEET EAST OF A LINE BEARING SOUTH 01°29'47" EAST FROM THE POINT OF BEGINNING;  
 THENCE WESTERLY ALONG THE SOUTH WALL OF SAID BUILDING, 0.58 FEET;  
 THENCE SOUTH 01°29'47" EAST 69.69 FEET, MORE OR LESS, TO THE SOUTH LINE OF SAID LOT 54 AND THE TERMINUS OF SAID LINE.

**TITLE REPORT REFERENCE:** THIS SURVEY WAS CONDUCTED ACCORDING TO THE DESCRIPTION SHOWN, FURNISHED BY FIRST AMERICAN TITLE INSURANCE COMPANY, COMMITMENT NO. NCS-1046739-WAL, DATED AUGUST 24, 2021. THE EASEMENTS SHOWN OR NOTED HEREON RELATE TO THIS COMMITMENT.

**NOTE:** EASEMENTS CREATED OR RESCINDED AFTER THIS DATE ARE NOT SHOWN OR NOTED HEREON.

**TITLE REPORT SCHEDULE B EXCEPTIONS:** ITEMS CIRCLED ARE SHOWN ON MAP.

14. THE TERMS AND PROVISIONS CONTAINED IN THE DOCUMENT ENTITLED "AGREEMENT" RECORDED DECEMBER 21, 1962 AS RECORDING NO. 5522859 OF OFFICIAL RECORDS.
15. EASEMENT, INCLUDING TERMS AND PROVISIONS CONTAINED THEREIN: RECORDING DATE: AUGUST 25, 1975; RECORDING INFORMATION: 7508250573; IN FAVOR OF: PUGET SOUND POWER & LIGHT COMPANY; FOR: THE RIGHT TO INSTALL, USE AND MAINTAIN ONE GUY WIRE AND ONE ANCHOR; AFFECTS: THE NORTH 15 FEET OF THE WEST 5 FEET OF LOT 53; \*SURVEYOR'S NOTE: NO GUY WIRES ARE IN THIS LOCATION AT TIME OF SURVEY
16. EASEMENT, INCLUDING TERMS AND PROVISIONS CONTAINED THEREIN: RECORDING DATE: NOVEMBER 14, 1977; RECORDING INFORMATION: 7711140822; IN FAVOR OF: CITY OF KIRKLAND; FOR: SEWER MAIN AND UTILITIES WITH THE NECESSARY APPURTENANCES; AFFECTS: THE WEST 12 FEET OF THE SOUTH 200 FEET OF LOT 51
17. EASEMENT, INCLUDING TERMS AND PROVISIONS CONTAINED THEREIN: RECORDING DATE: NOVEMBER 14, 1977; RECORDING INFORMATION: 7711140823; IN FAVOR OF: CITY OF KIRKLAND; FOR: SEWER MAIN AND UTILITIES WITH THE NECESSARY APPURTENANCES; AFFECTS: THE WEST 12.00 FEET; LESS THE SOUTH 260.00 FEET; TOGETHER WITH THE NORTH 10.00 FEET OF THE SOUTH 270.00 FEET OF LOT 60
18. EASEMENT, INCLUDING TERMS AND PROVISIONS CONTAINED THEREIN: RECORDING DATE: FEBRUARY 04, 1992; RECORDING INFORMATION: 9202040824; IN FAVOR OF: PUGET SOUND POWER & LIGHT COMPANY; FOR: AN UNDERGROUND ELECTRIC TRANSMISSION AND/OR DISTRIBUTION SYSTEM AS DESCRIBED THEREIN
19. CONDITIONS, NOTES, EASEMENTS, PROVISIONS AND/OR ENCROACHMENTS CONTAINED AND/OR DELINEATED ON THE FACE OF THE SURVEY, RECORDED OCTOBER 07, 2011 UNDER RECORDING NO. 2011007900002 IN VOLUME 283 OF SURVEYS PAGE 150, IN KING COUNTY, WASHINGTON.

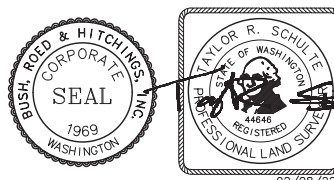


SEE SHEET 1

130TH AVE NE (DEDICATED PUBLIC RIGHT-OF-WAY)

SEE SHEET 1

FOUND MONUMENT IN CASE CONCRETE WITH 2" BRASS DISC WITH PUNCH DOWN 0.65', 0.32'W & 0.82'N OF INTERSECTION OF 130TH AVE NE & NE 70TH PL  
 VISIT DATE: 07/09/20



**BUSH, ROED & HITCHINGS, INC.**  
 LAND SURVEYORS & CIVIL ENGINEERS

2009 MINOR AVE. EAST SEATTLE, WASHINGTON 98102-3513 (206) 323-4144 1-800-935-0508 WWW.BRHINC.COM

TOPOGRAPHIC & BOUNDARY SURVEY  
 MILL CREEK RESIDENTIAL TRUST  
 130TH AVE NE AND NE 70TH PL

drawn by	checked by
HAK	TRS
scale	date
1"=20'	02/08/22
job no.	
2020093.03	
sheet	of
2	2

NO	REVISION	DATE

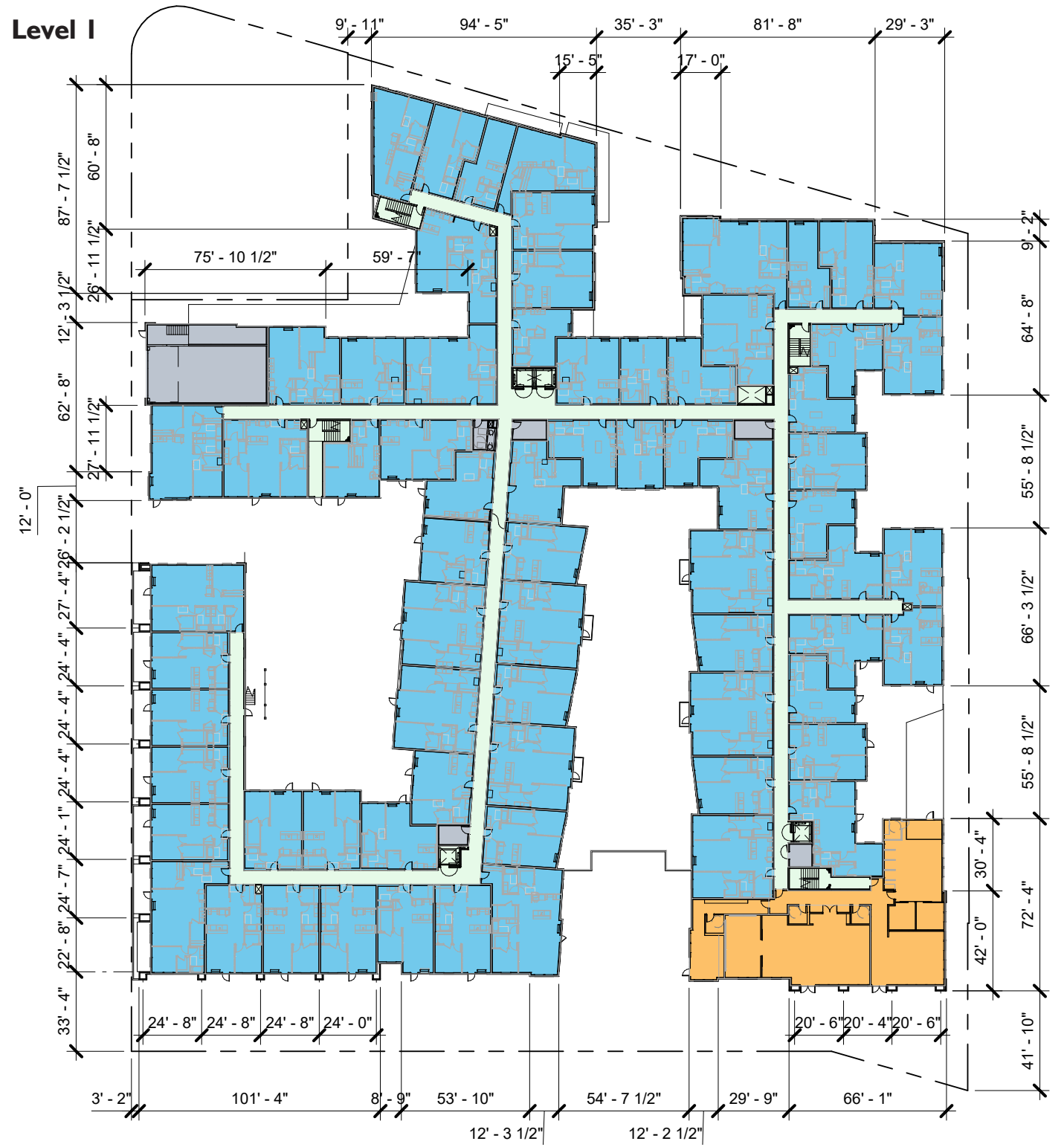
KIRKLAND KING COUNTY WASHINGTON

# FLOOR PLANS

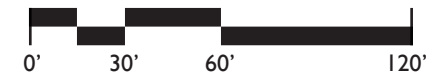
**Level PI**



**Level I**

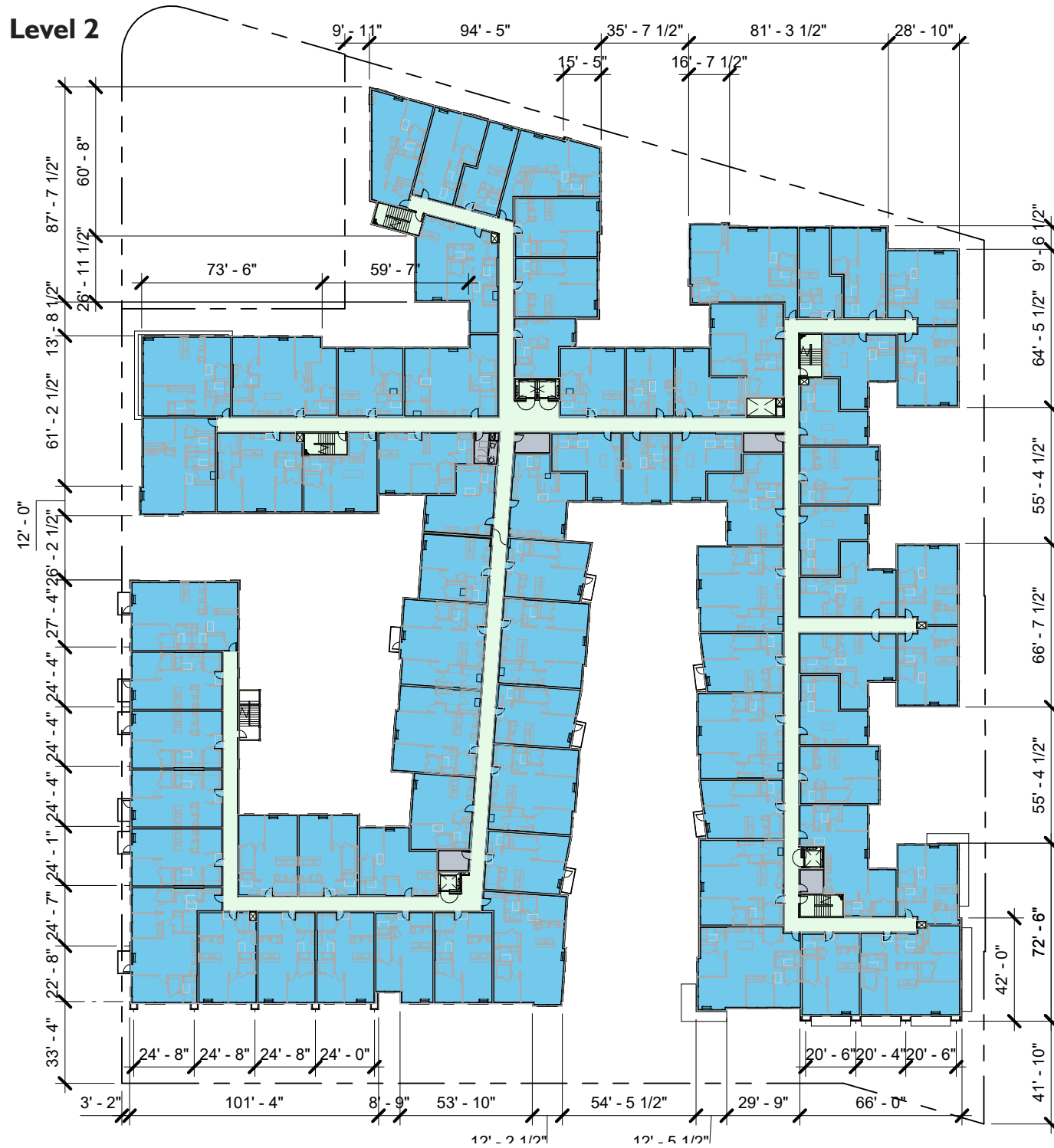


- COMMERCIAL
- ACTIVE USE / RESIDENTIAL AMENITY
- RESIDENTIAL LOBBY
- RESIDENTIAL APARTMENT HOMES
- PARKING, MECHANICAL, AND BACK OF HOUSE

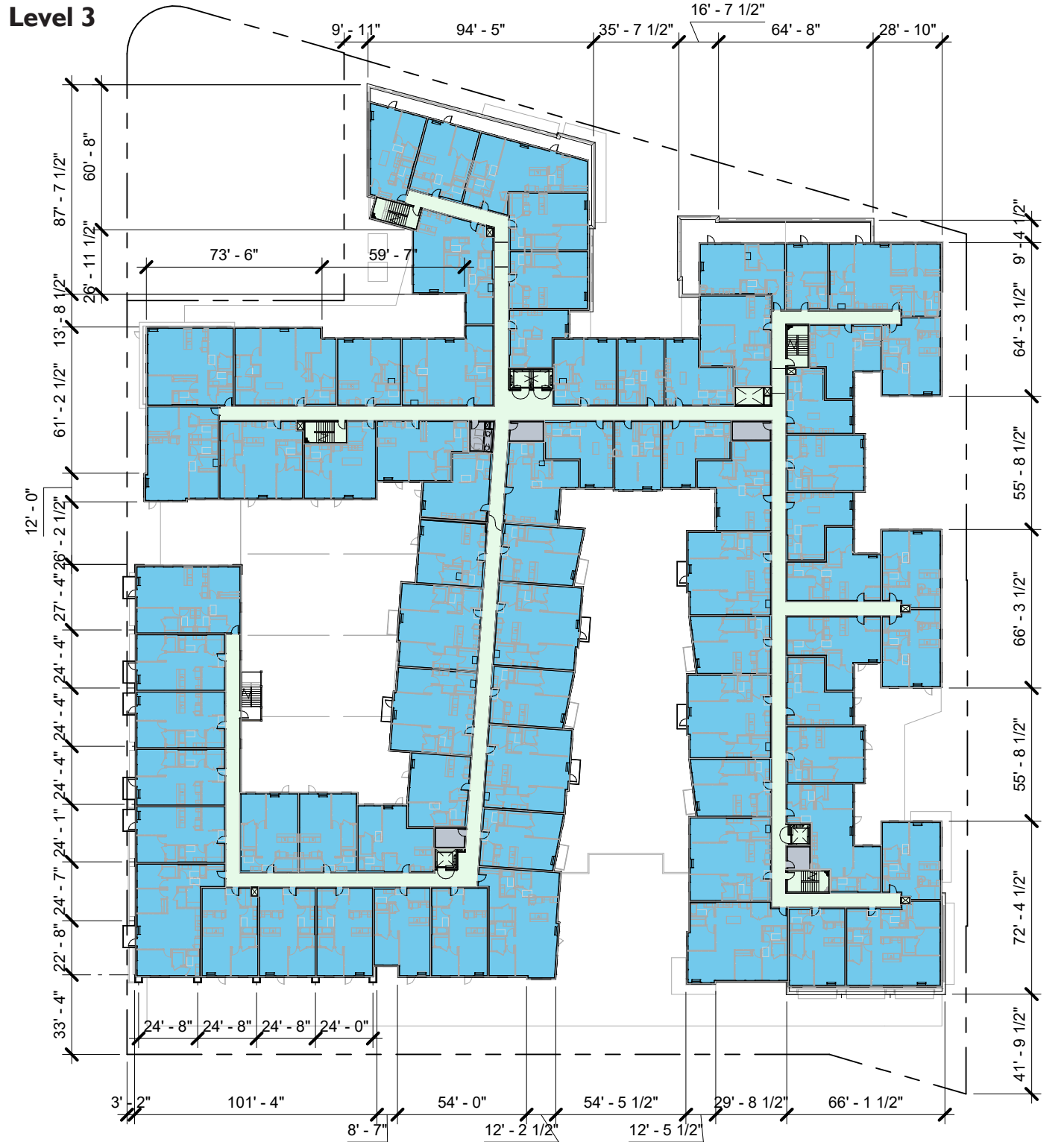


# FLOOR PLANS

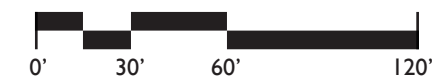
## Level 2



## Level 3



- COMMERCIAL
- ACTIVE USE / RESIDENTIAL AMENITY
- RESIDENTIAL LOBBY
- RESIDENTIAL APARTMENT HOMES
- PARKING, MECHANICAL, AND BACK OF HOUSE





# PUBLIC OPEN SPACE COMPLIANCE

## KZC 35.10.040.2.e - PEDESTRIAN-ORIENTED OPEN SPACES

THE DEVELOPMENT SHALL PROVIDE PUBLICLY ACCESSIBLE PEDESTRIAN ORIENTED OPEN SPACE(S) ADJACENT TO THE STREET OR THROUGH-BLOCK PATHWAY. THE PUBLICLY ACCESSIBLE SPACE(S) SHALL CONTAIN A MINIMUM OF 1,000 SQUARE FEET OR ONE SQUARE FOOT PER 200 GROSS SQUARE FEET OF ABOVE GRADE BUILDING AREA, WHICHEVER IS GREATER. THE SIZE CALCULATION SHALL NOT INCLUDE THE REQUIRED WIDTH OF ABUTTING SIDEWALKS OR PATHWAYS. LOCATIONS, DIMENSIONS, FEATURES AND IMPROVEMENTS (SUCH AS PLAZAS, SEATING, PUBLIC ART, CHILDREN'S RECREATION SPACES SHALL BE REVIEWED AND APPROVED THROUGH BY THE DESIGN REVIEW BOARD BASED ON APPLICABLE GUIDELINES.

REQUIRED =  $\frac{410,588 \text{ GROSS SQUARE FEET OF ABOVE GRADE BUILDING AREA}}{200 \text{ GSF}}$

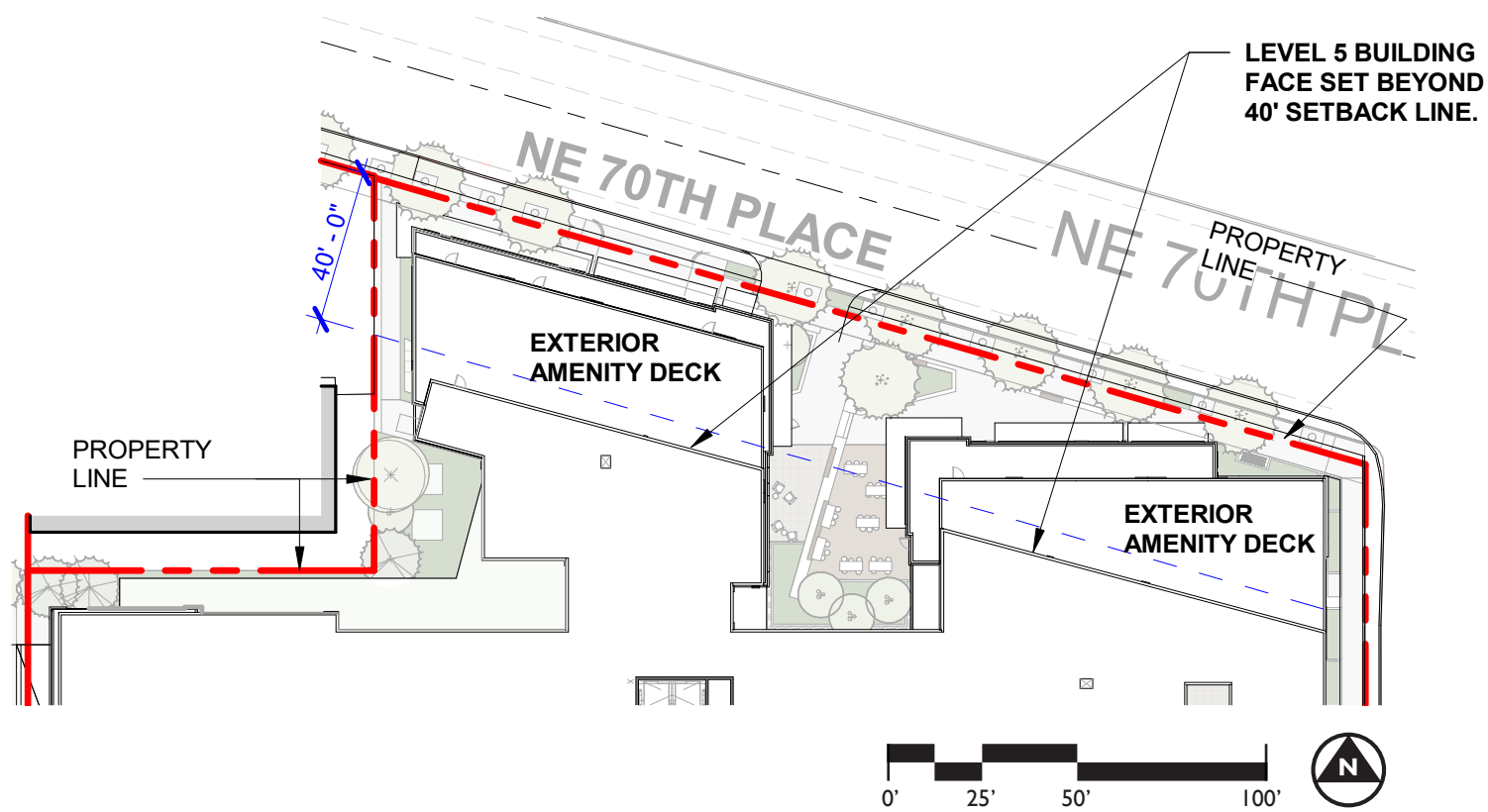
= 2,053 SF

PROVIDED = 3787 SF + 909 SF + 771 SF + 1303 SF

= **6,770 SF > 2,053 SF, MEETS REQUIREMENT**



# SETBACK AND STEPBACK COMPLIANCE: NE 70TH PL

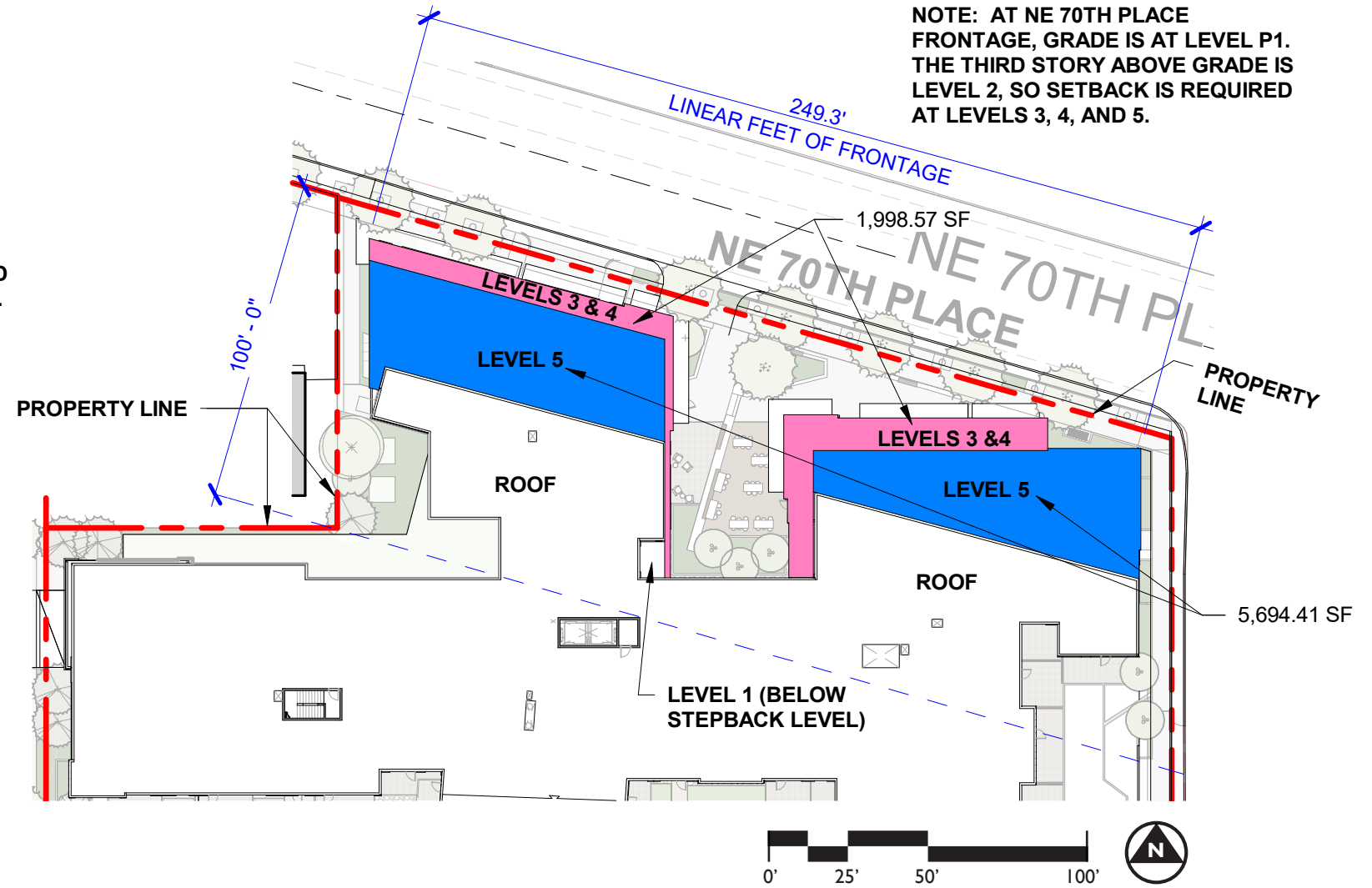


## SET BACK @ NE 70TH PLACE

### KZC 35.10.040.2.b COMPLIANCE CALCULATIONS-

NO PORTION OF A BUILDING WITHIN 40' OF NE 70TH PLACE SHALL EXCEED A HEIGHT OF 60' ABOVE NE 70TH PLACE MEASURED AT THE MIDPOINT OF THE FRONTAGE OF THE SUBJECT PROPERTY ON THE NE 70TH PLACE RIGHT-OF-WAY.

**AS SHOWN IN THE DIAGRAM ABOVE, THE BUILDING IS SETBACK AT LEVEL 5 SUCH THAT NO PART ENCLOSES INTO THE 40' SETBACK LINE.**



**NOTE: AT NE 70TH PLACE FRONTAGE, GRADE IS AT LEVEL P1. THE THIRD STORY ABOVE GRADE IS LEVEL 2, SO SETBACK IS REQUIRED AT LEVELS 3, 4, AND 5.**

## STEP BACK @ NE 70TH PLACE

### KZC 35.10.040.2.d.1 AND d.3 - UPPER STORY STEPBACK COMPLIANCE CALCULATIONS -

FOR ALL BUILDING FACADES FACING AND WITHIN 100 FEET OF THE ABUTTING RIGHT-OF-WAY, ALL PORTIONS OF A STRUCTURE GREATER THAN 3 STORIES IN HEIGHT, AS MEASURED FROM THE ABUTTING RIGHT-OF-WAY, SHALL BE STEPPED BACK FROM THE THIRD STORY BY AN AVERAGE OF 8 FEET (FROM NE 70TH PLACE). THE REQUIRED UPPER STORY STEP BACKS FOR ALL FLOORS ABOVE THE THIRD STORY SHALL BE CALCULATED AS TOTAL UPPER STORY STEP BACK AREA AS FOLLOWS:

$$\begin{aligned} \text{REQUIRED UPPER STORY STEP BACK AREA} &= \text{LINEAR FEET OF FRONTAGE (NOT INCL PORTIONS OF THE SITE WITHOUT BUILDINGS OR FOR VEHICULAR AREAS)* REQUIRED AVERAGE SETBACK * NUMBER OF STORIES ABOVE THIRD STORY} \\ &= 249.3 \text{ FT} * 8 \text{ FT} * 3 \text{ STORYS} = 5,983.2 \text{ SF} \end{aligned}$$

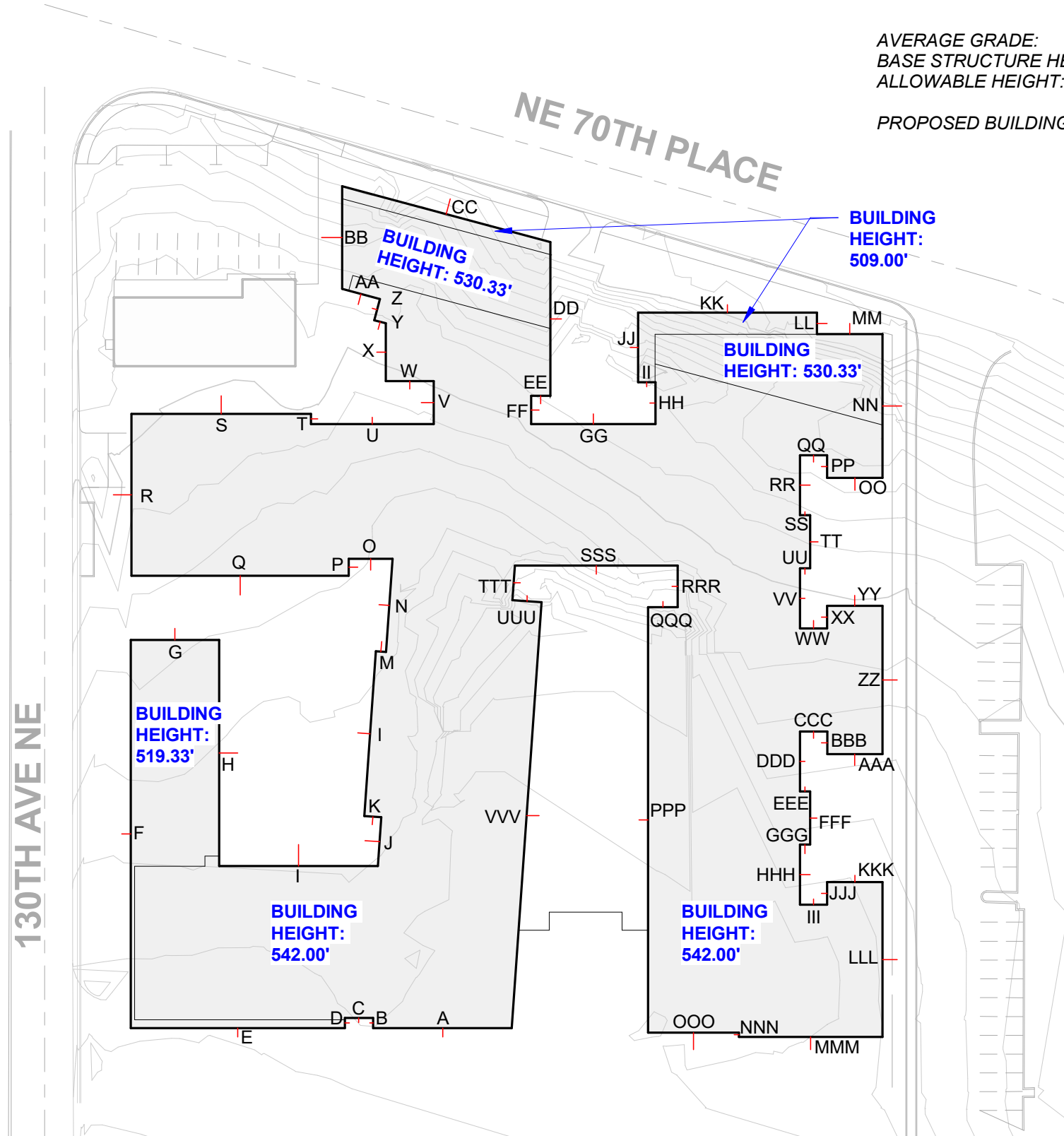
$$\begin{aligned} \text{PER DIAGRAM ABOVE} \\ \text{PROVIDED UPPER STORY STEP BACK AREA} &= \text{STEP BACK @ LEVEL 3 + STEP BACK @ LEVEL 4 + STEP BACK @ LEVEL 5} \\ &= 1,998.57 \text{ SF} + 1,998.57 \text{ SF} + (1,998.57 \text{ SF} + 5,694.41 \text{ SF}) \\ &= \mathbf{11,690.12 \text{ SF} > 5,983.2 \text{ SF, MEETS REQUIREMENT} \end{aligned}$$

# BUILDING HEIGHT CALCULATION

## AVERAGE BUILDING ELEVATION:

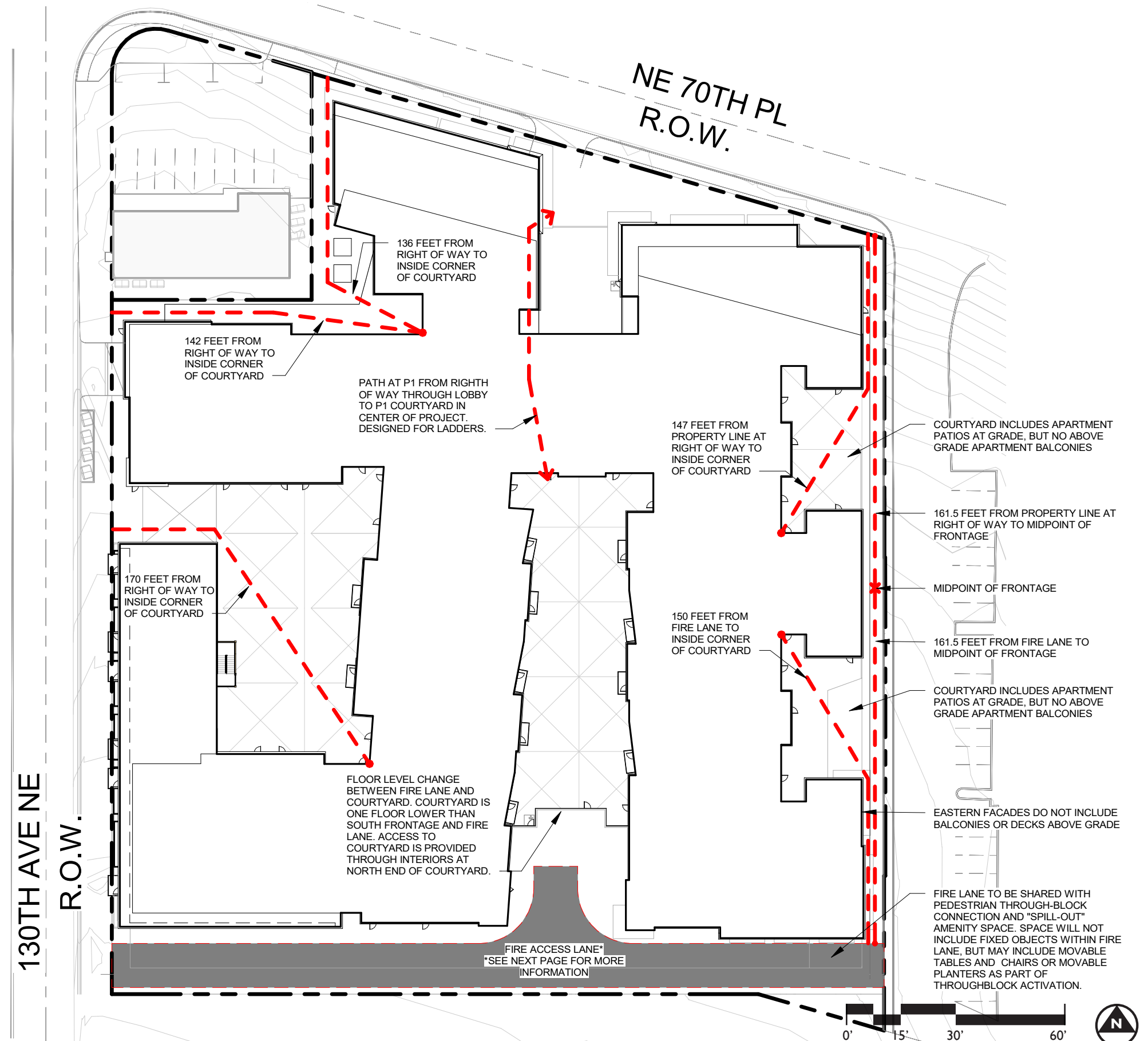
$$1,331,856.96 / 2,762.88 = +482.05' \text{ AVERAGE GRADE}$$

AVERAGE GRADE: +482.05'  
 BASE STRUCTURE HEIGHT: +60.00'  
 ALLOWABLE HEIGHT: **+542.05'**  
 PROPOSED BUILDING HEIGHT: +542.00'

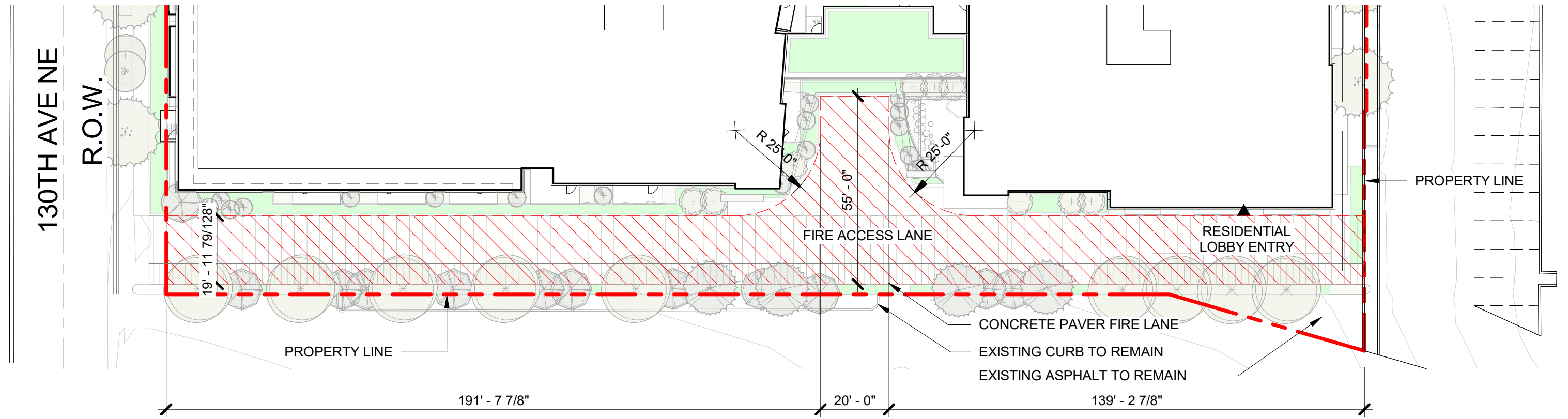


AVERAGE BUILDING ELEVATION DATA			
SEGMENT	MIDPOINT ELEVATION (EL)	SEGMENT LENGTH (L)	(EL) X (L) IN SQFT
A	486.44	61.25	29794.14
B	485.90	4.50	2186.56
C	485.92	12.50	6074.00
D	486.18	4.50	2187.81
E	486.87	94.50	46009.50
F	484.65	171.50	83117.30
G	482.80	39.00	18829.12
H	484.09	99.92	48368.26
I	484.93	70.17	34025.64
J	484.92	21.58	10466.25
K	484.73	7.50	3635.46
L	484.20	73.00	35346.89
M	483.85	4.50	2177.33
N	483.47	41.25	19943.30
O	483.03	19.33	9338.66
P	482.91	7.50	3621.85
Q	482.65	96.00	46334.78
R	480.76	71.50	34373.98
S	480.36	79.25	38068.61
T	480.29	4.50	2161.31
U	481.46	54.25	26119.10
V	480.51	19.00	9129.73
W	480.18	21.17	10163.79
X	479.64	25.50	12230.85
Y	479.11	5.33	2555.25
Z	478.87	9.67	4629.06
AA	478.57	17.25	8255.33
BB	476.59	45.33	21605.59
CC	473.83	95.25	45131.93
DD	475.47	67.92	32292.61
EE	478.92	8.50	4070.78
FF	479.41	12.50	5992.68
GG	478.79	55.00	26333.34
HH	478.33	18.50	8849.11
II	479.08	7.71	3692.92
JJ	477.25	30.75	14675.31
KK	469.67	79.00	37104.25
LL	468.70	9.50	4452.62
MM	468.31	29.00	13581.05
NN	474.36	63.50	30121.92
OO	476.45	24.50	11672.90
PP	476.55	10.00	4765.50
QQ	476.24	12.00	5714.84
RR	477.52	26.50	12654.33
SS	478.20	4.50	2151.90
TT	478.92	23.50	11254.55
UU	479.71	4.50	2158.68
VV	480.15	26.50	12723.84
WW	480.52	12.00	5766.28
XX	480.32	10.00	4803.21
YY	480.11	24.50	11762.60
ZZ	481.30	65.50	31525.35
aaa	482.79	24.50	11828.33
bbb	482.52	10.00	4825.16
ccc	482.28	12.00	5787.34
ddd	482.77	26.50	12793.48
eee	483.33	4.50	2174.99
fff	483.82	23.50	11369.70
ggg	484.28	4.50	2179.26
hhh	484.80	26.50	12847.31
iii	485.37	12.00	5824.48
jjj	485.21	10.00	4852.06
kkk	484.94	24.50	11881.01
lll	484.88	68.50	33214.21
mmm	486.00	63.50	30860.87
nnn	486.34	2.00	972.68
ooo	486.15	40.00	19446.08
ppp	487.30	187.83	91531.18
qqq	484.70	13.00	6301.15
rrr	483.65	18.42	8907.18
sss	482.51	72.00	34740.72
ttt	485.85	15.25	7409.18
uuu	488.15	13.00	6345.92
vvv	486.81	188.50	91762.74
TOTALS		2,762.88	1,331,856.96
Average Building Elevation (Option 2 Method) =	$\Sigma \text{ of (EL) X (L) =}$	2,762.88	482.05
	$\Sigma \text{ of (L) =}$	1,331,856.96	

# FIRE ACCESS



# FIRE ACCESS ENLARGEMENT



# TRASH STAGING





**CITY OF KIRKLAND**  
**Planning and Building Department**  
123 5th Avenue, Kirkland, WA 98033  
425.587.3600 ~ [www.kirklandwa.gov](http://www.kirklandwa.gov)

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## **DEVELOPMENT STANDARDS LIST**

### **MODERA BRIDLE TRAILS, DRV23-00164**

#### **ZONING CODE STANDARDS**

**95.51.2.a Required Landscaping.** All required landscaping shall be maintained throughout the life of the development. The applicant shall submit an agreement to the city to be recorded with King County which will perpetually maintain required landscaping. Prior to issuance of a certificate of occupancy, the proponent shall provide a final as-built landscape plan and an agreement to maintain and replace all landscaping that is required by the City.

**95.52 Prohibited Vegetation.** Plants listed as prohibited in the Kirkland Plant List shall not be planted in the City.

**100.25 Sign Permits.** Separate sign permit(s) are required. In JBD and CBD cabinet signs are prohibited.

**105.32 Bicycle Parking.** All uses, except single family dwelling units and duplex structures with 6 or more vehicle parking spaces must provide covered bicycle parking within 50 feet of an entrance to the building at a ratio of one bicycle space for each twelve motor vehicle parking spaces. Check with Planner to determine the number of bike racks required and location.

**105.18 Entrance Walkways.** All uses, except single family dwellings and duplex structures, must provide pedestrian walkways between the principal entrances to all businesses, uses, and/or buildings on the subject property.

**105.18 Overhead Weather Protection.** All uses, except single family dwellings, multifamily, and industrial uses, must provide overhead weather protection along any portion of the building, which is adjacent to a pedestrian walkway.

**105.18.2 Walkway Standards.** Pedestrian walkways must be at least 5' wide; must be distinguishable from traffic lanes by pavement texture or elevation; must have adequate lighting for security and safety. Lights must be non-glare and mounted no more than 20' above the ground.

**105.18.2 Overhead Weather Protection Standards.** Overhead weather protection must be provided along any portion of the building adjacent to a pedestrian walkway or sidewalk; over the primary exterior entrance to all buildings. May be composed of awnings, marquees, canopies or building overhangs; must cover at least 5' of the width of the adjacent walkway; and must be at least 8 feet above the ground immediately below it. In design districts, translucent awnings may not be backlit; see section for the percent of property frontage or building facade.

**105.19 Public Pedestrian Walkways.** The height of solid (blocking visibility) fences along pedestrian pathways that are not directly adjacent a public or private street right-of-way shall be limited to 42 inches unless otherwise approved by the Planning or Public Works Directors. All new building structures shall be setback a minimum of five feet from any pedestrian access right-of-way, tract, or easement that is not directly adjacent a public or private street right-of-way. If in a design district, see section and Plate 34 for through block pathways standards.

**105.58 Parking Lot Locations in Design Districts.** See section for standards unique to each district.

**105.65 Compact Parking Stalls.** Up to 50% of the number of parking spaces may be

designated for compact cars.

**105.60.2 Parking Area Driveways.** Driveways which are not driving aisles within a parking area shall be a minimum width of 20 feet.

**105.60.3 Wheelstops.** Parking areas must be constructed so that car wheels are kept at least 2' from pedestrian and landscape areas.

**105.60.4 Parking Lot Walkways.** All parking lots which contain more than 25 stalls must include pedestrian walkways through the parking lot to the main building entrance or a central location. Lots with more than 25,000 sq. ft. of paved area must provide pedestrian routes for every 3 aisles to the main entrance.

**105.77 Parking Area Curbing.** All parking areas and driveways, for uses other than detached dwelling units must be surrounded by a 6" high vertical concrete curb.

**105.96 Drive Through Facilities.** See section for design criteria for approving drive through facilities.

**110.52 Sidewalks and Public Improvements in Design Districts.** See section, Plate 34 and public works approved plans manual for sidewalk standards and decorative lighting design applicable to design districts.

**110.60.5 Street Trees.** All trees planted in the right-of-way must be approved as to species by the City. All trees must be two inches in diameter at the time of planting as measured using the standards of the American Association of Nurserymen with a canopy that starts at least six feet above finished grade and does not obstruct any adjoining sidewalks or driving lanes.

**115.25 Work Hours.** It is a violation of this Code to engage in any development activity or to operate any heavy equipment before 7:00 am. or after 8:00 pm Monday through Friday, or before 9:00 am or after 6:00 pm Saturday. No development activity or use of heavy equipment may occur on Sundays or on the following holidays: New Year's Day, Memorial Day, Independence Day, Labor Day, Thanksgiving, and Christmas Day. The applicant will be required to comply with these regulations and any violation of this section will result in enforcement action, unless written permission is obtained from the Planning official.

**115.45 Garbage and Recycling Placement and Screening.** For uses other than detached dwelling units, duplexes, moorage facilities, parks, and construction sites, all garbage receptacles and dumpsters must be setback from property lines, located outside landscape buffers, and screened from view from the street, adjacent properties and pedestrian walkways or parks by a solid sight-obscuring enclosure.

**115.47 Service Bay Locations.** All uses, except single family dwellings and multifamily structures, must locate service bays away from pedestrian areas. If not feasible must screen from view.

**115.75.2 Fill Material.** All materials used as fill must be non-dissolving and non-decomposing. Fill material must not contain organic or inorganic material that would be detrimental to the water quality, or existing habitat, or create any other significant adverse impacts to the environment.

**115.95 Noise Standards.** The City of Kirkland adopts by reference the Maximum Environmental Noise Levels established pursuant to the Noise Control Act of 1974, RCW 70.107. See Chapter 173-60 WAC. Any noise, which injures, endangers the comfort, repose, health or safety of persons, or in any way renders persons insecure in life, or in the use of property is a violation of this Code.

**115.115 Required Setback Yards.** This section establishes what structures, improvements and activities may be within required setback yards as established for each use in each zone.

**115.115.3.g Rockerries and Retaining Walls.** Rockerries and retaining walls are limited to a maximum height of four feet in a required yard unless certain modification criteria in this section are met. The combined height of fences and retaining walls within five feet of each other in a required yard is limited to a maximum height of 6 feet, unless certain modification criteria in this section are met.

**115.120 Rooftop Appurtenance Screening.** New or replacement appurtenances on existing



buildings shall be surrounded by a solid screening enclosure equal in height to the appurtenance. New construction shall screen rooftop appurtenances by incorporating them in to the roof form.

***Prior to issuance of a grading or building permit:***

**27.06.030 Park Impact Fees.** New residential units are required to pay park impact fees prior to issuance of a building permit. Please see KMC 27.06 for the current rate. Exemptions and/or credits may apply pursuant to KMC 27.06.050 and KMC 27.06.060. If a property contains an existing unit to be removed, a "credit" for that unit shall apply to the first building permit of the subdivision.

***Prior to occupancy:***

**95.51.2.a Required Landscaping.** All required landscaping shall be maintained throughout the life of the development. The applicant shall submit an agreement to the city to be recorded with King County which will perpetually maintain required landscaping. Prior to issuance of a certificate of occupancy, the proponent shall provide a final as-built landscape plan and an agreement to maintain and replace all landscaping that is required by the City

**110.60.5 Landscape Maintenance Agreement.** The owner of the subject property shall sign a landscape maintenance agreement, in a form acceptable to the City Attorney, to run with the subject property to maintain landscaping within the landscape strip and landscape island portions of the right-of-way. It is a violation to pave or cover the landscape strip with impervious material or to park motor vehicles on this strip.

# DEVELOPMENT STANDARDS

## DRV23-00164



### BUILDING DEPARTMENT

#### Building Department Conditions

Please call 425-587-3600 for Building Department questions related to these conditions.

#### General Conditions

##### Permits & Codes:

1. Permit applications applied for before July 1, 2023 shall demonstrate compliance with the 2018 editions of the International Residential, Mechanical Codes and the Uniform Plumbing Code as adopted and amended by the State of Washington and the City of Kirkland. (IRC, IMC, UPC). Permit applications applied for on July 1, 2023 or later shall comply with the 2021 editions of the previously mentioned codes.
2. Permit applications applied for before July 1, 2023 shall demonstrate compliance with the 2018 edition of the International Energy Conservation Code as adopted and amended by the State of Washington (WSEC). Permit applications applied for on July 1, 2023 or later shall comply with the 2021 edition of the International Energy Conservation Code as adopted and amended by the State of Washington (WSEC).
3. An Electrical Permit is required to be obtained separately. Kirkland reviews, issues and inspects all electrical permits in the city. Electrical permit applications shall demonstrate compliance with the 2020 Washington Cities Electrical Code Chapters 1 and 3 as published by WABO. (WCEC)
4. A separate Demolition Permit is required for removal of existing structure(s) prior to recording.
5. Structural:
6. Structural components must be designed for seismic design category D, wind speed of 110 miles per hour and Exposure B.

##### Other:

1. Prior to issuance of Building, Demolition or Land Surface Modification permit applicant must submit a proposed rat baiting program for review and approval. Kirkland Municipal Ordinance 9.04.040
2. Construction type is measured from the 'Grade Plane' as defined by the International Building Code. The allowable height for the proposed construction type of IIIA, is 60 feet. The provided plans do not have enough information to determine what the current actual height of the building is. This will be reviewed with the building permit application.

### FIRE DEPARTMENT

#### FIRE DEPARTMENT COMMENTS

Contact: Captain Chappell at 425-587-3655; or [jchappell@kirklandwa.gov](mailto:jchappell@kirklandwa.gov)

#### ACCESS

The building fronts on one right of way. The distance around the building is approximately 380 feet. The fire department access is NOT met.

#### FIRE FLOW

Fire flow requirement is based on total square footage of the building and type of construction. With allowed IFC reduction, required fire flow for this building appears to be 2000 gpm. The existing fire flow appears to be approximately 400gpm. Duration of 4 hours needs to be confirmed with Water Department.

#### HYDRANTS

Fire hydrants will need to be placed so that there is a hydrant within 150 feet of every part of the building accessible by fire department vehicles. The east side of the structure appears to be lacking in hydrant coverage.

#### FIRE SPRINKLERS

A sprinkler system is required to be installed throughout the building. Submit plans, specifications and calculations

electronically for approval at [www.MyBuildingPermit.com](http://www.MyBuildingPermit.com). All plans shall be designed and stamped by a person holding a State of Washington Certificate of Competency Level III certification. The system shall be installed by a state licensed sprinkler contractor. REF RCW 18.60 State of Washington.

A dedicated sprinkler riser room is required, and it shall be placed on an exterior wall. The underground line shall run from the outside directly up into the riser room (meaning, it shall not run under the slab for any distance). If the riser room has direct access from the outside, a PIV is not required. The sprinkler riser room may be used for other mechanical equipment, but not for the main electrical room nor shall it be used for storage; it may be used to house the fire alarm panel.

NOTE: TWO PERMITS are required from the Fire Department for installation of the fire sprinkler system, one for the underground and one for the sprinkler system itself. No work shall be performed on the sprinkler system without a Fire Department permit.

The civil drawings may be used as reference but do not constitute permission to install the fire sprinkler underground. The underground permit is NOT over-the-counter, so should be applied for well in advance of the anticipated date of start of construction.

\*\*\*City of Kirkland has zoning requirements for sprinkler and fire alarm systems. Further information found in KMC at [www.kirklandwa.gov](http://www.kirklandwa.gov) and Operating Policies.

#### STANDPIPES

Standpipe systems shall be installed throughout buildings where the floor level of the highest story is located more than 30 feet above the lowest level of the fire department vehicle access, or where the floor level of the lowest story is located more than 30 feet below the highest level of fire department vehicle access.

A standpipe is required. Submit plans, specifications and calculations electronically for approval at [www.MyBuildingPermit.com](http://www.MyBuildingPermit.com). The plans shall include isometric elevation drawing of the entire standpipe system including location of any isolation valves. It may be incorporated into the fire sprinkler system.

Note: Per the IFC 3313, standpipes shall be operational when the progress of construction is not more than 40 feet in height above the lowest level of fire department access. The standpipe shall be extended as construction progresses to within one floor of the highest point of construction having secured decking or flooring.

#### FIRE ALARM

A fire alarm system is required to be installed throughout the building. A separate permit is required from the Fire Department prior to installation. Submit plans and specifications for approval electronically at [MyBuildingPermit.com](http://MyBuildingPermit.com). The system shall comply with Washington State Barrier Free requirements regarding installation of visual devices and pull stations. The low-frequency requirement is also required for this project. The specific requirements for the system can be found in Kirkland Operating Policy 10.

#### FIRE EXTINGUISHERS

Portable fire extinguishers are required per Section 906 of the IFC. Minimum rating is 2A10BC. Extinguishers shall be mounted or in cabinets so that the top of the extinguisher is no more than 5 feet above the finished floor. Travel distance to a fire extinguisher shall not exceed 75 feet as measured along the route of travel.

#### COMMERCIAL COOKING

If any of the tenants are restaurants, a commercial cooking hood and duct extinguishing system is required to be installed. The permit may be applied for electronically at [MyBuildingPermit.com](http://MyBuildingPermit.com). The system shall be listed for application or specifically designed for such application. In addition, a K-class (Kitchen) fire extinguisher with a UL rating of 1-A:K is required to be installed within 30 feet of cooking equipment. The hood and duct suppression system is required to be tied into the building fire alarm system.

#### KEY BOX

A Key box is required (Knox Box 4400, recessed, and tampered). It shall be installed in an approved accessible location no

higher than six feet above grade. In most cases it will be located at the front entrance to the building. The box may be purchased on-line at [www.knoxbox.com](http://www.knoxbox.com); or by filling out an order form which is available from the Fire Department office. Contact the Fire Prevention Bureau at 425-587-3661 for more information.

#### EMERGENCY RADIO COVERAGE (Effective 7-1-16)

This is a required fire protection system for this project. The permit may be applied for electronically at [MyBuildingPermit.com](http://MyBuildingPermit.com).

#### SMOKE CONTROL

Depending upon the type of construction, a smoke control system may be required. Elevation of building is not confirmed at this time. This needs to be dialed in before permitting.

#### FIRE SAFETY DURING CONSTRUCTION

The fire prevention program superintendent shall develop and maintain an approved prefire plan in cooperation with the fire chief. Prefire plans for buildings exceeding 50,000 s.f. shall be approved prior to the issuance of the building permit.

3308.8.1 Job Site Security. After above grade combustible construction has begun, the job site shall be secured with controlled access. In addition, off hours guard service and/or motion-controlled surveillance may be required at the discretion of the fire code official.

3308.8.2 Job shacks and other temporary structures. Job shacks and other temporary structures located within or less than 20' from the permanent building shall be:

- Constructed of non-combustible materials or 1 hour fire-resistive construction.
- Shall not be equipped with fuel fired heaters
- Shall be equipped with monitored fire alarm system when located below grade
- Shall not function as offices unless protected with automatic sprinkler systems

#### COURTYARD

Courtyards provide unique Fire Department response challenges. Access to courtyard is required via straight/direct access corridor and/or stairway from exterior to courtyard at a location acceptable to the Fire Code official. If a stairway is used it shall comply with International Fire Code Section 1011 and a corridor shall comply with International Fire Code Section 1020.

The access shall have a minimum width of 4 feet (or as directed by the fire code official) and be large enough to carry a 35-foot-long sectional ladder (minimum folded length 20 feet) directly from the exterior to the courtyard without obstructions. The access door shall be marked at the street as "Direct access to courtyard."

## PUBLIC WORKS DEPARTMENT

#### PUBLIC WORKS CONDITIONS

Permit #:

Project Name:

Project Address:

Date:

#### Public Works Staff Contacts

Ryan Schauble, Senior Development Engineer

Phone: 425-587-3842 / E-mail: [rschauble@kirklandwa.gov](mailto:rschauble@kirklandwa.gov)

#### General Conditions:

1. All public improvements associated with this project including street and utility improvements, must meet the City of Kirkland Public Works Pre-Approved Plans and Policies Manual. A Public Works Pre-Approved Plans and Policies manual can be purchased from the Public Works Department, or it may be retrieved from the Public Works Department's page at the City of Kirkland's web site.

2. Permit Fees, Connection Charges and Impact Fees. Click on the hyperlinks to view the latest fee schedules. Contact Public Works staff if there are general questions about fees. Fees for a specific permit will not be determined until plan review is complete. The applicant is responsible for completing and submitting the Public Works Improvement Evaluation Packet (available in either Excel or PDF) before fees and securities are determined. If the project site is located in a water/sewer service area other than City of Kirkland (i.e., Northshore Utility District or Woodinville Water District), then those utility connection charges will be collected by the respective utility district.

3. All street and utility improvements for Short Plats and Subdivisions shall be permitted through a Land Surface Modification (LSM) Permit. Click on the hyperlink to view the LSM Checklist. Street and utility improvements covered under a building permit will not require a separate LSM permit, unless specified otherwise by staff.

4. Right-of-Way (ROW) Restoration Performance and Maintenance Securities: The Developer must post a performance security to cover all ROW restoration requirements. The security amount will be based on the scope of work in the right-of-way and other risk factors to public infrastructure. The performance security will be released once the project is complete and the permit(s) passed Final Inspections. Prior to Final Inspections, the Developer must also post a Maintenance Security covering all public improvements installed by the project for a period of two (2) years from the permit Final date. The security amount will be determined by the Public Works Department.

5. Traffic Concurrency Review:

Prior to submittal of a Building or Zoning Permit, the applicant must apply for a Concurrency Test Notice. Contact Thang Nguyen, Transportation Engineer, at 425-587-3869 for more information. A separate Concurrency Permit will be created. After concurrency has passed, the project will receive a concurrency test notice that allows the applicant to proceed with all development permits. A "Certificate of Concurrency" is established with a development or building permit. It will read as follows: CERTIFICATE OF CONCURRENCY: This project has been reviewed and approved for water, sewer, and traffic concurrency. Any water and sewer mitigating conditions are listed within the conditions below. Any traffic mitigating conditions will be found in an attached memorandum from the Public Works Traffic Engineering Analyst to the Planning Department Project Planner. Upon issuance of this permit, this project shall have a valid Certificate of Concurrency and concurrency vesting until the permit expires. This condition shall constitute issuance of a Certificate of Concurrency pursuant to chapter 25.12 of the Kirkland Municipal Code.

6. All civil engineering plans which are submitted in conjunction with a building, grading, or right-of-way permit must conform to the Public Works Policy G-7, Engineering Plan Requirements. This policy is contained in the Public Works Pre-Approved Plans and Policies Manual. All street improvements and underground utility improvements (storm, sewer, and water) must be designed by a Washington State Licensed Engineer. All drawings shall bear the licensed engineer's stamp. All plans submitted in conjunction with a building, grading or right-of-way permit must have elevations which are based on the King County datum only (NAVD 88).

7. Prior to issuance of any commercial or multifamily Building Permit, the applicant shall provide an analysis and plan for garbage, recycling and composting storage and pickup. Refer to Policy G-9 in the Public Works Pre-Approved Plans as a guide. The plan must be approved by Waste Management and by Public Works. The applicant shall submit a letter report to summarize the analysis and the plan, supported by such details as container sizing calculations, storage area sizing calculations, and truck access and turnaround details.

8. The required tree plan shall include any significant tree in the public right-of-way along the property frontage.

Sanitary Sewer Conditions:

1. The existing sanitary sewer main in the right-of-way is adequate to serve the project. However, the sewer along the west and south property lines appears to be on or near the parcel; need verification and an public sewer easement extending 10 feet from the center of the pipes, and an understanding of the potential impacts of construction (where is the building foundation in relation to the sewer lines).

2. Provide a plan and profile design for the sewer line extension.
3. A 20 foot wide public sanitary sewer easement must be recorded with the property.
4. Provide a new sides sewer for the property sized per the Uniform Plumbing Code. Lines 8 inches or greater in diameter shall enter the public system at manhole.
5. Access for maintenance of the sewer manholes is required. Provide a 15' wide access easement from the right-of-way to each sanitary sewer manhole.
6. Any businesses serving food or drink are required to have grease interceptor on the waste line prior to discharge to the City sewer system. The interceptor shall be sized per the Uniform Plumbing Code (minimum).

Water System Conditions:

1. The existing water main in the right-of-way is adequate to serve the domestic needs of the project.
2. The existing water service shall be abandoned at the main, unless expressly approved otherwise by Public Works Department.
3. See Fire Department conditions for fire flow requirements. If water system improvements are required to meet fire flow the City's consultant will model the system and provide the necessary system upgrades; applicant will need to inform the City of the needed fire flow based upon they project details (type of construction, height, etc.).
4. In mixed-use projects each use shall have a separate water meter (i.e., the retail use shall have a separate water meter from residential use).

Surface Water Conditions:

1. Provide temporary and permanent storm water control in accordance with the 2021 King County Surface Water Design Manual (KCSWDM) and the City of Kirkland Addendum (Policy D-10). Refer to Policies D-2 and D-3 in the Public Works Pre-Approved Plans and Policies Manual for design guidance, or contact Kirkland Surface Water staff at (425) 587-3800 for assistance. Based on the pre-submittal information provided by the applicant, this project should expect a Full Drainage Review. The drainage review level and requirements may change based on the actual development proposal at the time of permit application.
2. Vesting of Surface Water Regulations and Design Manual:
  - Only a "Complete Building Permit Application" or "Complete Short Plat / Subdivision Application" will vest a development project to the current surface water design requirements (i.e., current Design Manual). Other Land Use Permits, Zoning Permits, or Design Review Process does not vest a project with regard to surface water requirements. For example: Master Plan, Variance, Use Permits, Design Review Board - does not vest. Complete application means, at a minimum, a complete project description, site plan, and if applicable, SEPA checklist; and shall satisfy Kirkland Municipal Code 20.12.210.
  - If a new Design Manual is adopted by the City of Kirkland after a project is vested to the former Design Manual, then the vested project must start construction within 5 years from the date of new Design Manual adoption to remain vested to the former requirements. Start construction means the site work associated with, and directly related to the approved project has begun. For example: grading the project site to final grade or utility installation. Simply clearing the project site does not constitute the start of construction. A performance bond is not equivalent to construction start.
3. Calculating impervious surface area based on lot coverage: This is important for flow control analysis (hydrologic modeling purposes). In accordance with KZC Chapter 115.90 – Calculating Lot Coverage. The regulation update allowed conventional (sand set) pavers to be counted as a "Partially Exempt Material", allowed to received 50 percent exemption for zoning lot coverage for the area they cover, and up to 10 percent of the total lot size. Conventional pavers do not have to meet surface water mitigation specifications (e.g. not designed as LID BMP pervious pavers per Public Works Pre-Approved Plan CK-L-09). As a result, lots are allowed 10 percent more runoff generating surface area, and thus have to provide flow control accordingly.

Furthermore, impervious surface areas shall also include frontage and street improvements – streets, sidewalks, trails, etcetera and shall be taken from the layouts of the proposed plans. Building footprint and driveways or building coverage shall be as follows:

- For commercial or multi-family development, the impervious coverage shall either:
  - o Assume the maximum impervious coverage permitted by the KZC plus an additional 10% OR
  - o Estimate impervious coverage from layouts of the proposal. If estimated from the layouts of the proposal, the impervious coverage shall include calculations of all impervious surfaces, including eaves. This option may require a Reduced Impervious Surface Limit to be recorded on the property.
  
- 4. A drainage report (Technical Information Report or TIR) must be submitted with the Land Use application or permit application. A downstream analysis is required for all projects (except for Basic Drainage Review). For Simplified Drainage Review, use the Simplified TIR Submittal Template available on the City of Kirkland website.
  
- 5. This project is in a Level 2 Flow Control Area and is required to comply with core drainage requirements in the KCSWDM. Historic (forested) conditions shall be used as the pre-developed modeling condition for design of the stormwater detention system.
  
- 6. The 15-minute time step must be used to perform the flow control analysis. Do not use the 1-hour time step. Approved hydrologic modeling programs are MGS Flood and WWHM (latest version of the software).
  
- 7. Evaluate the feasibility and applicability of dispersion, infiltration, and other stormwater Low Impact Development (LID) Best Management Practices (BMPs) per the KCSWDM. If feasible, stormwater LID BMPs are required to the maximum extent feasible. If LID BMPs are infeasible, pervious pavement cannot be used to reduce overall impervious lot coverage. The Private Maintenance Agreement will be recorded on all projects that construct a stormwater LID BMP or facility, per Policy D-7.
  
- 8. Soil information may be necessary for designing LID BMPs per the KCSWDM, and there are other reasons a soil report is necessary for a project (e.g., steep slopes, sensitive areas, etc.). Refer to Policy D-8 for details.
  
- 9. Special inspections may be required for LID BMPs on this project. Provide documentation of inspections by a licensed geotechnical professional that the BMP will function as designed.
  
- 10. If the project will create or replace more than 5,000 square feet of pollution generating impervious surface (PGIS), provide water quality treatment in accordance with the KCSWDM. The enhanced treatment level is required for multi-family residential, commercial, industrial projects, and single family residential projects with eight or more dwelling units per acre density.
  
- 11. Soil Amendment per Pre-Approved Plan E.12 is required for all landscaped areas.
  
- 12. All roof and driveway drainage must be tight-lined to the storm drain system or utilize low impact development techniques on-site.
  
- 13. Provide collection and conveyance of right-of-way storm drainage. Provide a plan and profile design for the storm sewer system. Size and material of construction shall be in accordance with the City Kirkland Pre-Approved Plans and Notes. Refer to Policy D-5 for details.
  
- 14. Construction Stormwater Pollution Prevention Plan (CSWPPP):
  - All proposed projects that will conduct construction activities onsite, or offsite must provide stormwater pollution prevention and spill controls to prevent, reduce, or eliminate the discharge of pollutants (including sediment) to onsite or adjacent stormwater systems or watercourses.
  - Refer to Core Requirement No. 5 in the KCSWDM and Policy D-12.
  - Provide an erosion control report and plan with the Building or Land Surface Modification Permit application. The plan shall be in accordance with the KCSWDM.

- Construction drainage control shall be maintained by the developer and will be subject to periodic inspections. During the period from May 1 and September 30, all denuded soils must be covered within 7 days; between October 1 and April 30, all denuded soils must be covered within 12 hours. Additional erosion control measures may be required based on site and weather conditions. Exposed soils shall be stabilized at the end of the workday prior to a weekend, holiday, or predicted rain event.

15. If the total disturbed land area is one acre or greater, the following conditions apply:

- The applicant is responsible to apply for a Construction Stormwater General Permit from Washington State Department of Ecology. Provide the City with a copy of the Notice of Intent for the permit. Permit Information can be found at the following website: <http://www.ecy.wa.gov/programs/wq/stormwater/construction/>
  - o Among other requirements, this permit requires the applicant to prepare a Storm Water Pollution Prevention Plan (SWPPP) and identify a Certified Erosion and Sediment Control Lead (CESCL) prior to the start of construction. The CESCL shall attend the City of Kirkland PW Dept. pre-construction meeting with a completed SWPPP.
- Turbidity monitoring by the developer/contractor is required for any surface water leaving the site.
- A Stormwater Pollution Prevention and Spill (SWPPS) Plan must be kept on site during all phases of construction and shall address construction-related pollution generating activities. Follow the guidelines in the Ecology Pollution Prevention Manual for plan preparation.

Street and Pedestrian Improvement Conditions:

1. The subject property abuts 130th Ave NE and NE 70th Place rights-of-way. Code sections 110.10 and 110.25 require the applicant to make half-street improvements in rights-of-way abutting the subject property. Section 110.30-110.50 establishes that this street must be improved with the following:

A. 130th Ave NE

- Dedicate sufficient right-of-way (ROW) abutting the property to install half-street improvements. Appears no dedication is required; will need survey to verify.
- Install new Type-A concrete curb and gutter along the frontage. The face of curb shall be 30 feet from the face of curb across the street to allow for 2-11 ft travel lanes and 8 ft parking lane along the property frontage. Widen the street pavement to meet the curb and gutter. The curb transitions shall happen outside of the property limits.
- Install a minimum 8-ft wide concrete sidewalk behind the curb with street trees 30 ft on-center in 4x6 tree wells.
- Provide pedestrian lighting 60 ft on-center between the street trees; city to provide lighting specification (has not been determined based on the new zoning).
- Provide no parking anytime signs along the west side of the street opposite the frontage improvements.
- Provide a striping plan for the new buffered bike lane; show existing channelization for the street.
- Install drainage to pick the new curb line with through-curb inlet structures. Structures in the travel lane shall have round hinged solid locking lids.
- Contact the Planning Department regarding other requirements that are not right-of-way related; increased building lighting, etc.
- Identify and protect trees with retention value in the right-of-way.
- Coordinate improvements with planned Kirkland street projects, if any.

B. NE 70th Place

- Dedicate sufficient right-of-way (ROW) abutting the property to install half-street improvements. Appears a 6 ft dedication is required (see 110.52 for allowed easement for some of the dedication); will need survey to verify.
- Install new Type-A concrete curb and gutter along the frontage. The face of curb shall be 3 feet from the existing face to allow for the existing travel lane widths and an 8 ft buffered bike lane. Widen the street pavement to meet the curb and gutter. The curb transitions shall happen outside of the property limits.
- Install a minimum 10-ft wide concrete sidewalk behind the curb with street trees 30 ft on-center in 4x6 tree wells.
- Provide pedestrian lighting 60 ft on-center between the street trees; city to provide lighting specification (has not been determined based on the new zoning).
- Install drainage to pick the new curb line with through-curb inlet structures. Structures in the travel lane shall have round hinged solid locking lids.
- A striping plan for channelization of the street may be required, contact Thang Nguyen for guidance.



- Coordinate with King County Metro for the relocation of the 245 bus stop; process and requirements.
- Contact the Planning Department regarding other requirements that are not right-of-way related; increased building lighting, etc.
- Identify and protect trees with retention value in the right-of-way.
- Coordinate improvements with planned Kirkland street projects, if any.

2. Access Requirements (KZC Chapter 105.10 and 105.18):

- A. An east west pedestrian connection is required along the south property line; either 8 ft paved path or a 5 ft concrete sidewalk. The alignment along the north side of the fire lane is fine, and should be raised or separated from the fire lane. See 105.18 for additional requirements; the access should connect at grade with a walkable service at both ends and provide lighting (consult planning for requirements).
- B. Access from NE 70th Place is allowed, but shall only be from the existing shared access drive along the east property line. The new access as shown/proposed does not meet the City's driveway separation requirements, and the City is limiting new access to Arterial/Collector streets when alternative accesses exist; existing shared driveway from NE 70th Place and access from 130th Ave NE. Access from the shared driveway is recommended at 150 ft from the intersection with 70th, but may be 100 ft (minimum required distance)
- C. Access from 130th Ave NE is allowed. The access as proposed does not meet driveway separation requirements; minimum 50ft. However, the City would consider allowing the access if an access easement was granted to the adjacent parcels along the entire south property line to allow for a future joint access driveway.
- D. The unobstructed paved access shall be 24 ft wide. Internal parking garage drive isles shall meet the requirements of the parking plates.
- E. Contact Thang Nguyen, 425.587.3869, regarding access, parking, concurrency or Traffic Impact Analysis (TIA) questions.

3. Meet the requirements of the Kirkland Driveway Policy R-4.

4. Meet the requirements of the Kirkland Intersection Sight Distance Policy R.13. All street and driveway intersections shall not have any visual obstructions within the sight distance triangle.

5. When three or more utility trench crossings occur within 150 lineal ft. of street length or where utility trenches parallel the street centerline, the street shall be overlaid with new asphalt or the existing asphalt shall be removed and replaced per the City of Kirkland Street Asphalt Overlay Policy R-7.

- Existing streets with 4-inches or more of existing asphalt shall receive a 2-inch (minimum thickness) asphalt overlay. Grinding of the existing asphalt to blend in the overlay will be required along all match lines.
- Existing streets with 3-inches or less of existing asphalt shall have the existing asphalt removed and replaced with an asphalt thickness equal or greater than the existing asphalt provided however that no asphalt shall be less than 2-inches thick and the subgrade shall be compacted to 95% density.

6. Prior to the final of the building or grading permit, pay for the installation of stop and street signs at the new intersections. Public Works will fabricate the signs and provide the developer with the poles and bases for the developer to install.

7. It shall be the responsibility of the applicant to relocate any above-ground or below-ground utilities which conflict with the project, associated street, or utility improvements.

8. Underground all new and existing on-site utility lines and overhead transmission lines. Underground any new off-site transmission lines.

9. Zoning Code Section 110.60.7.b establishes the requirement that existing utility and transmission (power, telephone, etc.) lines on-site and in rights-of-way adjacent to the site must be underground; underground the existing overhead utility in the 130th Ave NE right-of-way the length of the property (~140 ft).

10. New LED street lights may be required per Puget Sound Energy (PSE) design and Public Works approval. Contact PSE or a

third-party consultant may perform lighting analysis. If new lighting or upgrades are necessary, design plans must be submitted for review prior to issuance of an LSM or building permit. Contact PSE Street Lighting Account Manager: Lyndsey Goldsmith at Lyndsey.Goldsmith@pse.com, 425-396-3838 or 425-395-5225.

11. A striping plan for the street must be submitted with the building or grading permit.

Comments on Permit DRV23-00164 Modera at Bridle Trails, 13033 NE 70<sup>th</sup> Place

Three concerns:

- 1- Traffic – I question whether it is even possible to sufficiently mitigate the impact of hundreds of cars entering and leaving the building each day. The intersection of 70<sup>th</sup> Place and 130<sup>th</sup> Ave NE will require a traffic light without a doubt, yet will that be enough to prevent significant backups on each street, especially on 130<sup>th</sup> trying to get into or out of the Bridle Trails Apartments?
- 2- Parking – It is reasonable to assume that of 368 residential units 90% or more will have at least one car per unit and perhaps half will have two cars per unit. Will the parking on site accommodate more than 500 cars? If not, where will the others park? In the shopping center, occupying spots the shoppers currently use? Up along 130<sup>th</sup> NE in either direction?
- 3- Building itself - The size will be out of proportion to the character of the neighborhood. A 5-story building with retail on the ground floor means 4 floors for 368 units, 92 units per floor. The footprint of that building has to be immense to accommodate that! At 5 stories tall the building will loom over the surrounding buildings and road itself, essentially occupying and dominating the skyline of the area. The esthetics of the exterior of the building cannot possibly compensate for the size of it and its intrusion on the immediately surrounding area.

Respectfully submitted,

David P. Pomeroy  
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Pom2hnl@yahoo.com