



CITY OF KIRKLAND
Planning and Building Department
123 5th Avenue, Kirkland, WA 98033
425-587-3600 ~ www.kirklandwa.gov

DESIGN REVIEW BOARD DECISION

FILE NUMBER: DRV22-00498

PROJECT NAME: ARDEA AT TOTEM LAKE

APPLICANT: JAMES BROWN WITH WATTENBARGER ARCHITECTS

PROJECT PLANNER: TONY LEAVITT

I. SUMMARY OF DECISION

James Brown with Wattenbarger Architects for Ardea TWG LLP, applied for design review approval of the Ardea at Totem Lake residential project at 12700 116th Avenue NE (see Attachment 1). The applicant is proposing to construct a 7-story residential project with approximately 165 affordable senior residential units (see Attachment 2). Parking is proposed within a small surface lot and in structured parking below the building. Vehicular access to the property is from 116th Avenue NE.

On November 21, 2022, the Design Review Board (DRB) approved the project as shown on the plans dated November 8, 2022 subject to the following conditions:

- A. This application is subject to the applicable requirements contained in the Kirkland Municipal Code, Zoning Code, and Building and Fire Code. It is the responsibility of the applicant to ensure compliance with the various provisions contained in these ordinances. Attachment 3, Development Standards, intended to familiarize the applicant with some of the additional development regulations. This attachment does not include all of the additional regulations.
- B. As part of the application for a building permit the applicant shall submit the following:
 - 1. Construction plans demonstrating compliance with the project plans approved by the DRB as shown in Attachment 2.
 - 2. A summary of any proposed project changes, indexed to the permit drawings, from the plans approved through Design Board Review.
 - 3. Plans that include additional benches (preferably with backs) along the pedestrian pathway (see Conclusion II.B)
 - 4. Plans for a decorative pedestrian gate to the north of the main entrance to the building (see Conclusion II.C).

5. If Street trees cannot be installed within the 116th Avenue right-of-way, plans showing the installation of additional trees along the sidewalk on the subject property (see Conclusion II.D).

D. Prior to final inspection of a building permit by the Planning Official, the project architect shall submit a letter stating that they have evaluated the project to ensure it is consistent with the plans approved through Design Board Review and no modifications have been made that were not previously approved by the City.

II. DESIGN RESPONSE CONFERENCE MEETINGS

A. Background Summary

The DRB held two Design Response Conference meetings for the project. The staff report, plans, and applicant response to the DRB's recommendations from each meeting can be found listed by meeting date at this online web address:

<https://www.kirklandwa.gov/Government/Departments/Planning-and-Building/Design-Review-Board/DRB-Meeting-Materials-Archive>

Below is a summary of the Board's discussions at the two Design Response Conferences held for the project.

September 12, 2022 Conference:

The Design Review Board reviewed the plans submitted by Wattenbarger Architects dated June 30, 2022. Staff provided an overview of the Zoning Code and Comprehensive Plan policies for the TL 6B and the key design issues for the project. Staff's memo dated August 31, 2022, provides an analysis of project consistency with applicable zoning regulations and Design Guidelines for the Totem Lake Neighborhood.

After deliberating, the Board requested the applicant to return for a second meeting to respond to the following DRB comments:

- Revise the west elevation to match the east façade in terms of materials and colors.
- Provide material transition details along the facades and awning details including materials.
- Submit a material board (for review by the DRB prior to the meeting)
- Provide building height sections to show compliance with height regulations.
- Revise the public plaza area to be more open and inviting to the public and ensure compliance with the zoning code size and dimension requirements.
- Provide right-of-way and site lighting details and plans.

The meeting was continued to November 7, 2022. At the November 7, 2022 meeting, the applicant requested that the design response conference be continued to the November 21, 2022 meeting.

November 21, 2022 Conference:

The Design Review Board reviewed the revised plans submitted by Wattenbarger Architects dated November 8, 2022. Staff's memo dated November 10, 2022 provides an analysis of project consistency with applicable zoning regulations and Design Guidelines for the Totem Lake Neighborhood.

The applicant presented revised plans, which addressed the requested items from the DRB. The DRB discussed the changes proposed by the applicant and at the conclusion of the meeting voted to approve the project. See Section III below for further information regarding the DRB's discussions and conclusions.

B. Public Comment

No public comment letters were received during the review of the application.

During the September 12, 2022 DRC Meeting, one individual provided public testimony. The testimony focused on traffic in the area and potential traffic impacts of the project. Staff directed the individual to submit written comments to Staff for review and response. Written comments were never submitted to Staff.

III. DESIGN REVIEW BOARD DISCUSSION AND CONCLUSIONS

Below is a summary of the key issues and conclusions reached by the Design Review Board during the design review process. For more background on these issues and evaluation of how the project meets the Zoning Code, see the staff advisory reports from the Design Response Conferences contained in File DRV22-00498 and online at the previously mentioned DRB meeting page.

A. BUILDING HEIGHT, ARCHITECTURAL AND HUMAN SCALE

DRB Discussion: The DRB agreed with the applicant's revised massing option was presented at the May 16, 2022 Conceptual Design Conference. Vertical modulation was achieved with building modulation and placement of materials and colors. Horizontal modulation was achieved with a pedestrian-friendly ground floor design, building setbacks from the right-of-way and the use of architectural elements including decorative cladding. Human scale was achieved with the use of street level public courtyards, window fenestration techniques, and a variety of materials. The basement level parking garage was adequately screened with landscaping, raised planters, berms and decorative grilles.

DRB Conclusions: The DRB concluded that the proposed building massing, architectural scale and human scale are consistent with the applicable design guidelines found in the Design Guidelines for the Totem Lake Neighborhood.

B. VEHICULAR AND PEDESTRIAN ACCESS

DRB Discussion: The DRB reviewed vehicular and pedestrian access for the site as part of their review. Vehicular access to the property is proposed from 116th Avenue NE. The main pedestrian entrance is located at the northwest corner of the site near the intersection of 116th Avenue NE and NE 128TH Street.

The DRB reviewed the design of the pedestrian spaces including the onsite walkways, adjacent sidewalks, the private residential patio and the public plazas. Their review looked at the design of the seating areas, lighting of these spaces, and the materials beings used.

DRB Conclusions: The DRB concluded that the proposed vehicular and pedestrian access plan meets the applicable design guidelines found in the Design Guidelines for the Totem Lake Neighborhood. They also concluded that the proposed plans for the pedestrian walkways, sidewalks and plazas meet the applicable design guidelines. The DRB added a condition that the proposed design include additional benches (preferably with backs) along the pathway.

C. BUILDING MATERIALS, COLOR AND DETAIL

DRB Discussion: The DRB evaluated the proposed materials, colors, and details. The DRB approved of the applicant's preferred material and color palette for the project and agreed that the colors and materials used were effective in reducing the perceived scale of the building and adding interest to the pedestrian areas. The Board also discussed the design of the pedestrian gate to the north of the main entrance to the building.

DRB Conclusions: The DRB concluded that the proposed building materials, colors and details meet the applicable design guidelines found in the Design Guidelines for the Totem Lake Neighborhood. The DRB added a condition that the pedestrian gate to the north of the main entrance to the building should be decorative.

D. LANDSCAPING

DRB discussion: The DRB reviewed the landscape plan designed to help enhance the pedestrian experience and provide seasonal visual interest along the property frontage. Opportunity areas discussed for landscaping included the 116th Avenue NE public plazas, the private residential patio, and along the onsite walkways and sidewalks.

DRB conclusions: The DRB concluded that the proposed landscaping meets the applicable design guidelines found in the Design Guidelines for the Totem Lake Neighborhood. The DRB added a condition that if street trees cannot be installed within the 116th Avenue right-of-way, the applicant should submit plans showing the installation of additional trees along the sidewalk on the subject property.

IV. ZONING REQUIREMENTS THAT REQUIRE DRB APPROVAL

A. Open Space

1. Facts:

- a. KZC Section 55.43.4 states the development is required to provide publicly accessible space(s) at the primary pedestrian frontage that extends the public realm while creating a transition between public and private spaces. These public spaces shall have no dimension less than 15 feet. The proposed development shall provide publicly accessible space(s) ranging from 1,500 to 2,000 square feet in size. Through design review, the City will review the location, size and dimensions, features and improvements (such as multi-use paths, plazas, seating, public art and water features) proposed for the publicly accessible space(s) as part of the Design Review approval. The City may also require or permit modification to the required publicly accessible space as part of the Design Review approval.
- b. The proposed project will have two publicly accessible spaces totaling 1,528 square feet. One space is located near the main tranche of the building (713 square feet) and the other space (815 square feet) is located on 116th Avenue NE near the surface parking area.

2. Conclusions: The plans show compliance with the publicly accessible space requirement. The DRB reviewed the design of these spaces to ensure compliance with the applicable design guidelines.

V. STATE ENVIRONMENTAL POLICY ACT (SEPA) AND CONCURRENCY

The City issued a SEPA Determination of Nonsignificance on October 23, 2023 for the project. No appeals of the determination were filed.

VI. DEVELOPMENT REVIEW COMMITTEE

Comments and requirements placed on the project by City departments are found on the Development Standards, Attachment 3.

V. SUBSEQUENT MODIFICATIONS

Modifications to the approval may be requested and reviewed pursuant to the applicable modification procedures and criteria in effect at the time of the requested modification.

VI. APPEALS OF DESIGN REVIEW BOARD DECISIONS AND LAPSE OF APPROVAL

A. Appeals

Section 142.40 of the Zoning Code allows the Design Review Board's decision to be appealed to the Hearing Examiner by the applicant or any person who submitted written or oral comments to the Design Review Board. The appeal must be in the form of a letter of appeal and must be delivered, along with any fees set by ordinance, to the Planning and Building Department by 5:00 p.m., January 4, 2024, fourteen (14)

calendar days following the postmarked date of distribution of the Design Review Board's decision.

Only those issues under the authority of the Design Review Board as established by Kirkland Zoning Code 142.35(2) are subject to appeal.

B. Lapse of Approval

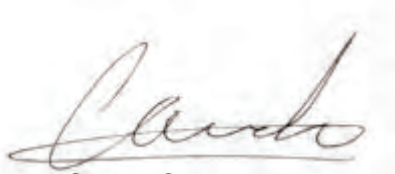
The applicant must begin construction or submit to the City a complete building permit application for the development activity, use of land or other actions approved under this chapter within five (5) years after the final approval of the City of Kirkland on the matter, or the decision becomes void.

The applicant must substantially complete construction for the development activity, use of land or other actions approved under this chapter and complete the applicable conditions listed on the notice of decision within seven (7) years after the final approval on the matter or the decision becomes void.

VII. ATTACHMENTS

1. Vicinity Map
2. Plans dated November 8, 2022
3. Development Standards

VIII. APPROVAL



Carlos Castaneda
Chair, Design Review Board

Date: 12/20/2023

**ARDEA AT TOTEM LAKE DRC
DRV22-00498**

RM 18
113TH PL NE

NE 128TH ST

DD 26
Subject Property

116TH AVE NE

TOTEM LAKE

TL 6B

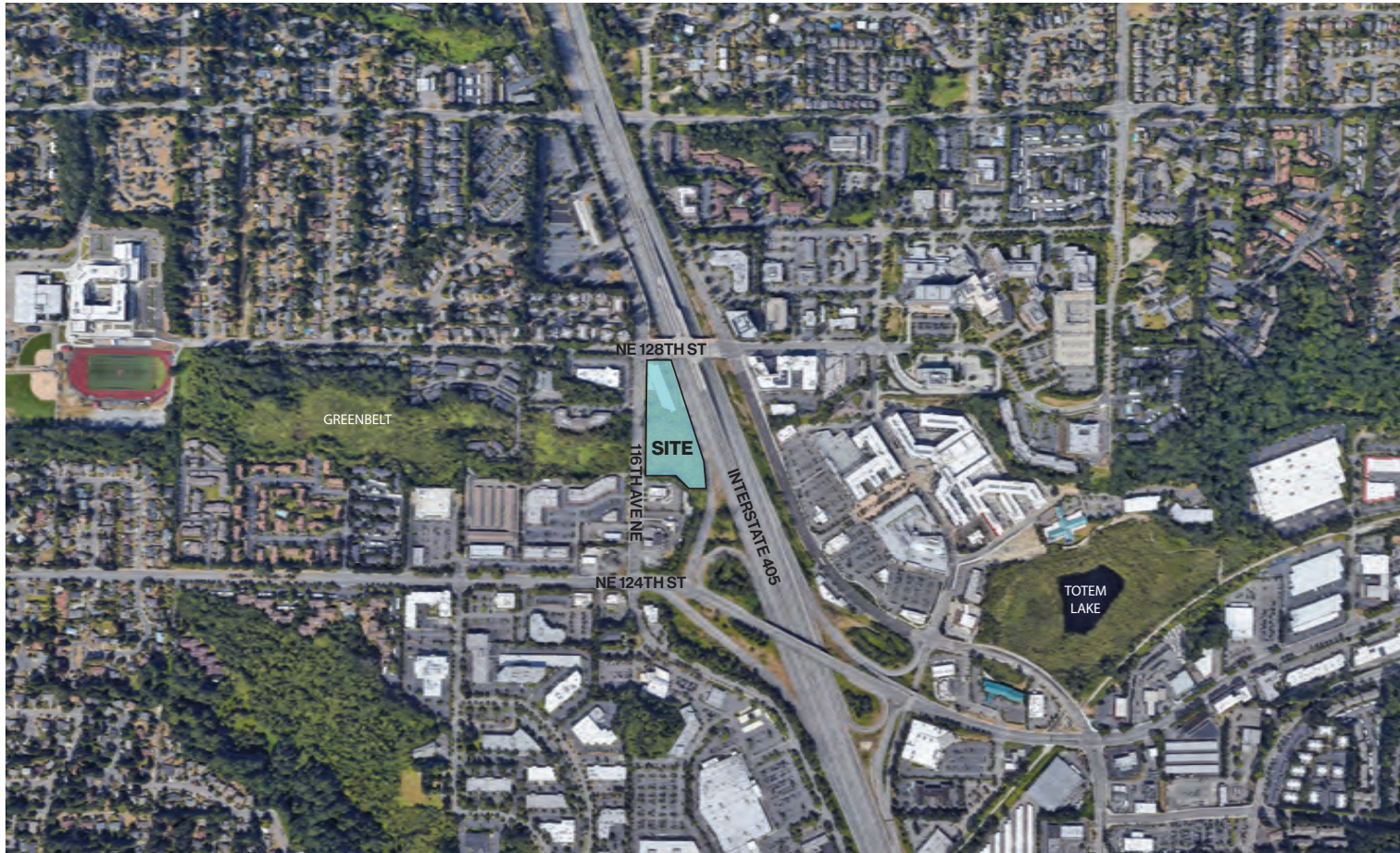


ARDEA AT TOTEM LAKE

“AN AFFORDABLE SENIOR COMMUNITY”

12700 116TH AVENUE, KIRKLAND, WA 98034





SITE LOCATION: 12700 116TH AVE NE, KIRKLAND WA, 98304
SITE CONTEXT: TOTEM LAKE | GREENBELT
MAIN STREETS + INTERSTATE: I405 | 116TH AVE NE | NE 128TH ST





MAJOR CONNECTIONS TOWARDS RESIDENTIAL NEIGHBORHOODS AND COMMERCIAL SPACES

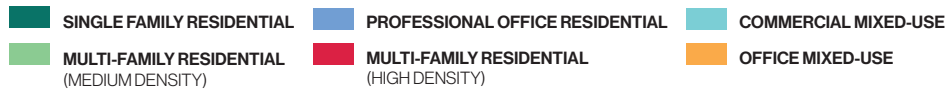
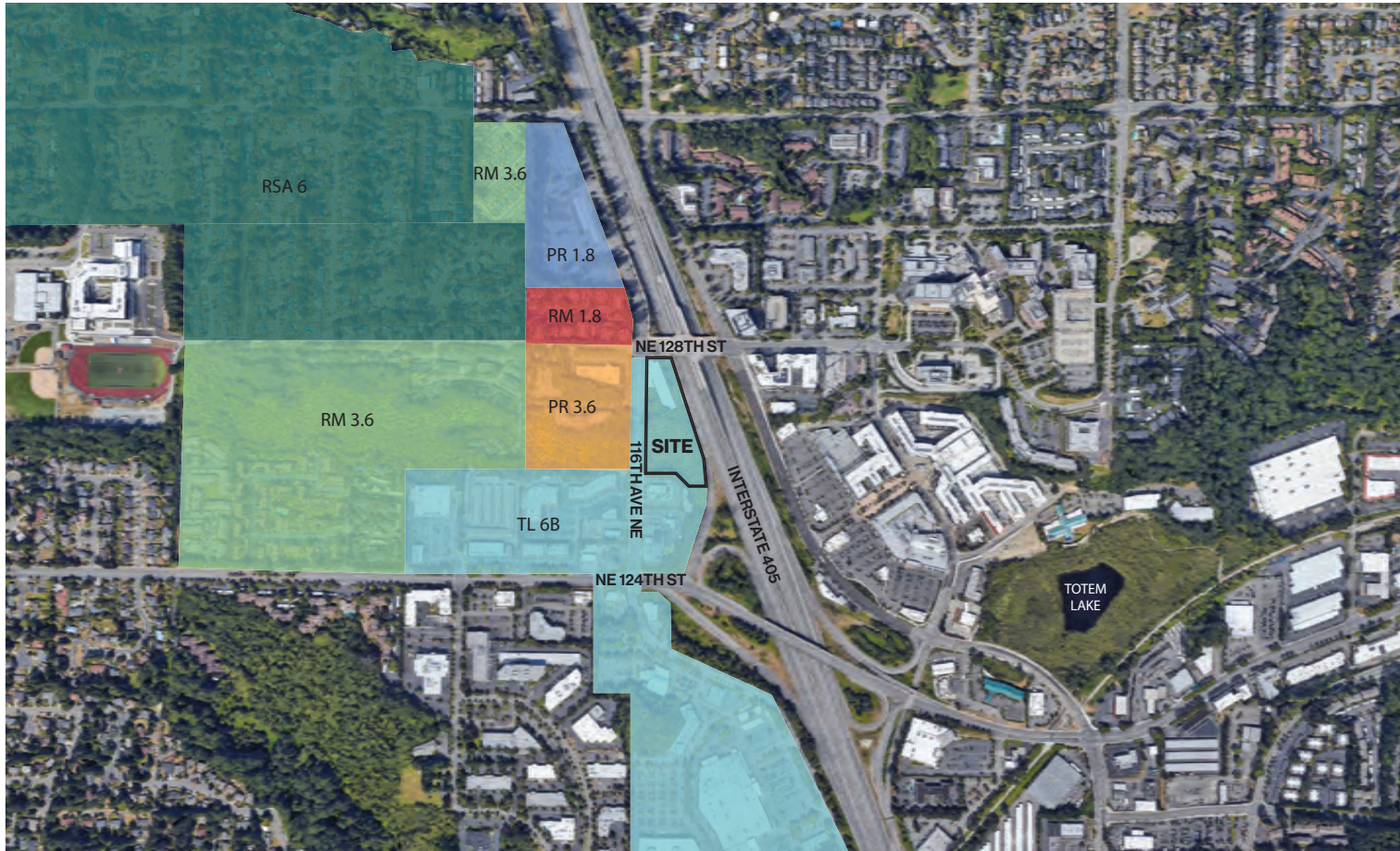
○ MAIN GATEWAY





BIKE ROUTES TRAILS PEDESTRIAN PATHS







ARDEA TOTEM LAKE IS IMMEDIATELY ADJACENT TO A GREENBELT AND FALLS IN A TRANSITIONAL AREA WITH CONNECTIONS TO THE COMMERCIAL DISTRICTS TO THE SOUTH, EAST, AND NORTHEAST; AND LOWER DENSITY RESIDENTIAL AND OFFICE TO THE WEST AND NORTH.





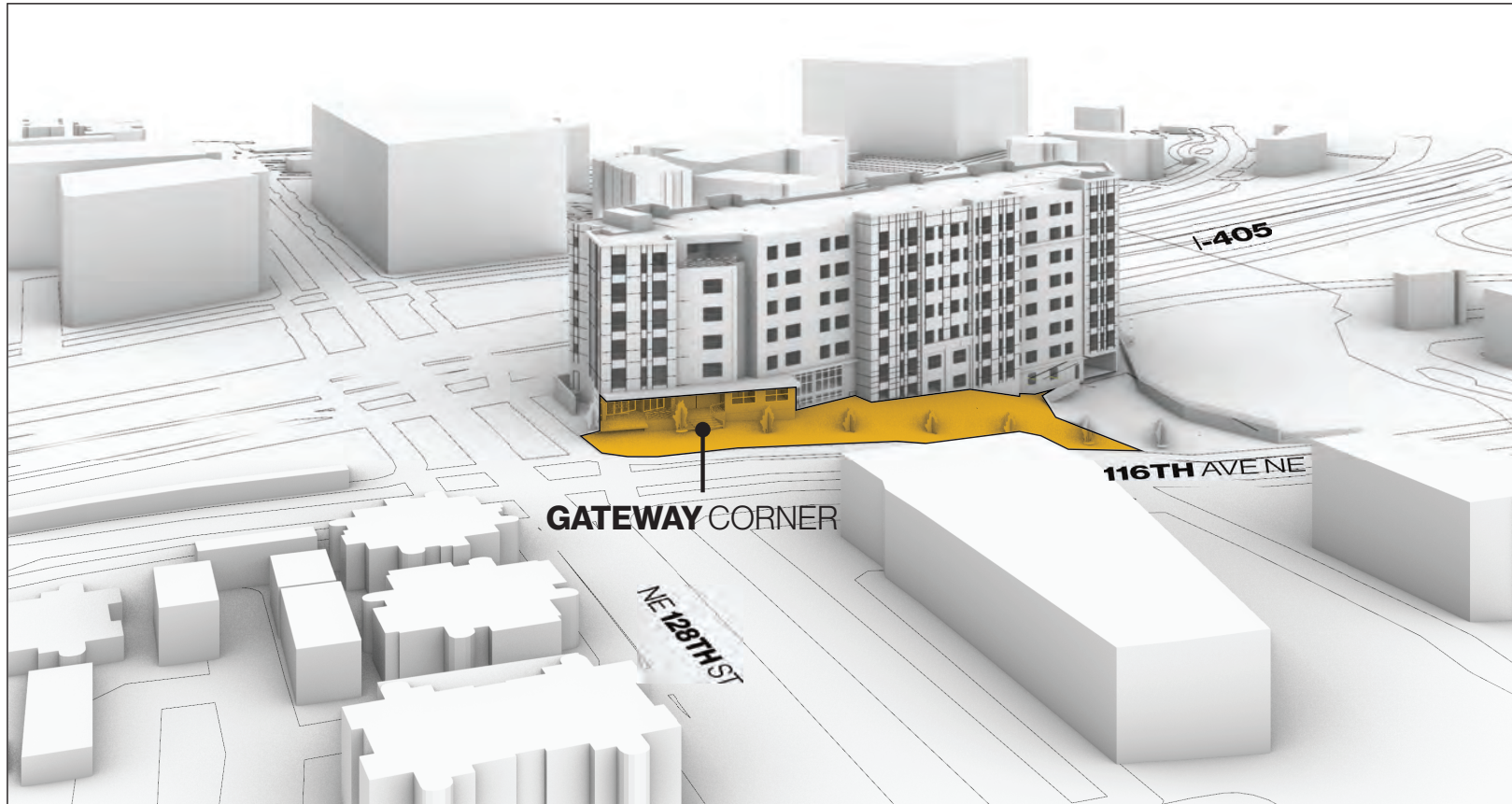
116TH AVENUE





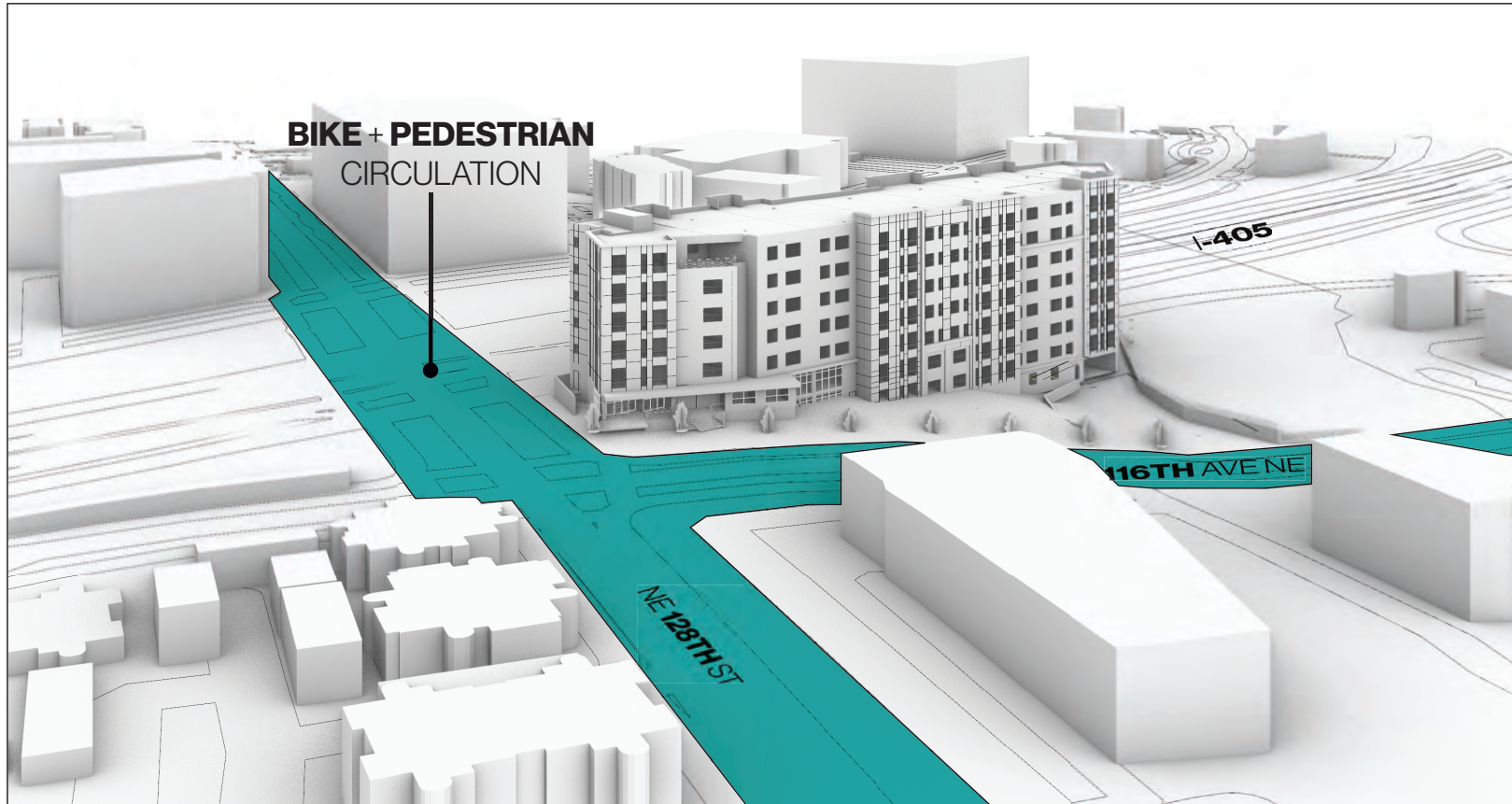
NE 128TH AVE





THE NORTHWEST CORNER IS THE MAIN GATEWAY ONTO THE SITE FROM THE BUSY INTERSECTION WHERE WE WILL INCORPORATE A SMOOTH AND SAFE TRANSITION TO THE DEVELOPMENT. THIS WILL LEAD THE RESIDENTS TO THE MAIN ENTRANCE OF THE BUILDING AND ALLOW EASY ACCESS TO TRANSPORTATION ON NE 128TH. WE PROPOSE TO ENHANCE THIS GATEWAY CORNER THROUGH A COMBINATION OF BUILDING MODULATION, ARCHITECTURAL ELEMENTS, AND LANDSCAPE FEATURES TO ENHANCE THE USER EXPERIENCE.



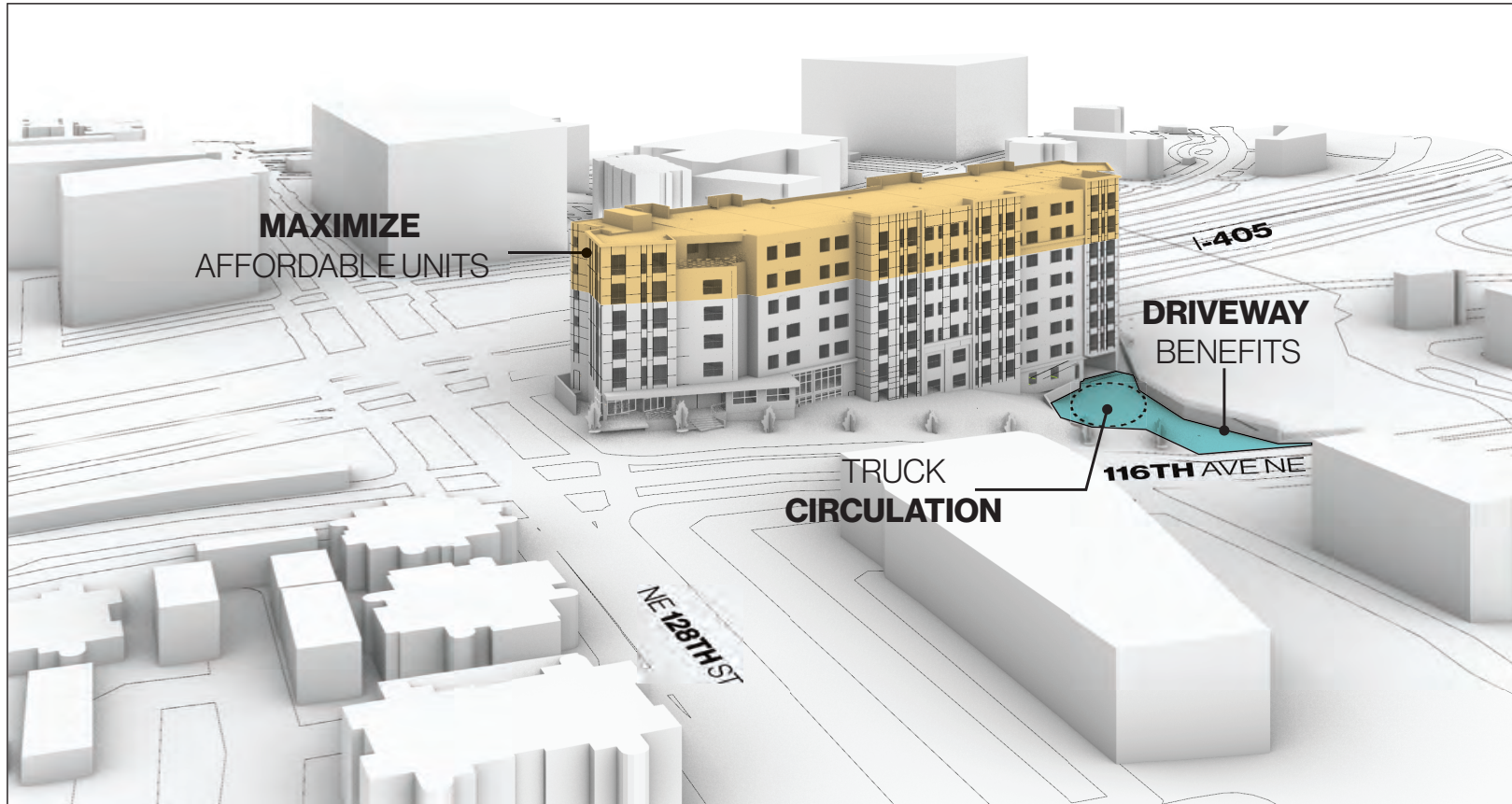


THE TWO MAIN STREETS ON OUR SITE USUALLY EXPERIENCE HEAVY TRAFFIC. MAKING THESE LANES WIDER AND INCORPORATING BIKE LANES AND SIDEWALKS ON NE 128TH HELPS INCREASE THE SAFETY. THESE LANES ALSO ARE THE MAIN ARTERIALS TO EITHER THE RESIDENTIAL OR COMMERCIAL AREAS.



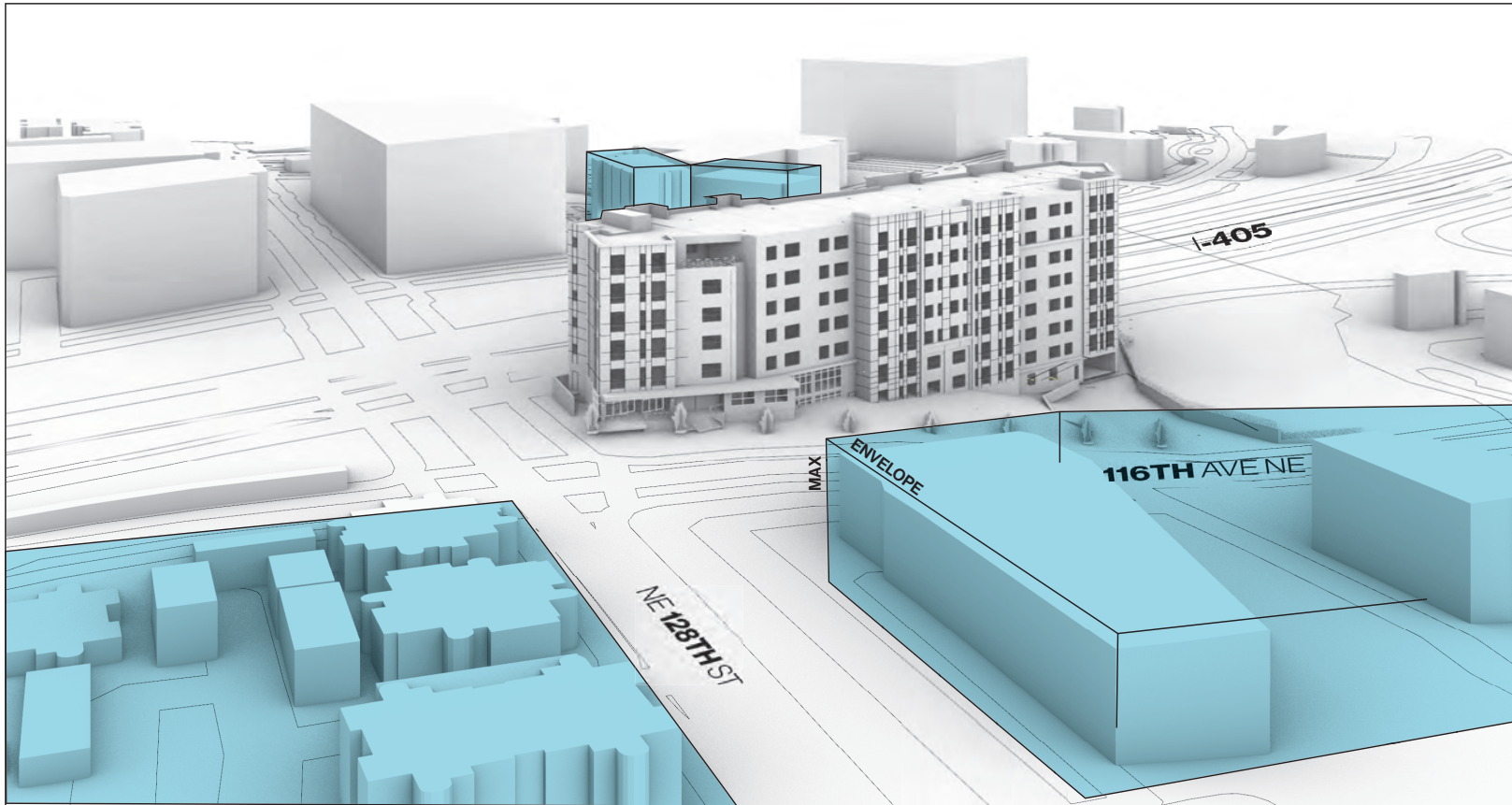


OUR SITE PROPOSES TWO MAIN ENTRANCES, A PEDESTRIAN FOCUSED ENTRANCE TOWARDS THE NORTH WEST CORNER OF THE LOT AND A VEHICULAR ENTRANCE TOWARDS THE SOUTH END. BOTH OF THESE ENTRANCES CAN BE ACCESSED FROM 116TH AVE WHICH HAS LESS TRAFFIC AND MAKES FOR A SAFER TRANSITION ONTO THE PROPERTY.



THE STRATEGIC POSITIONING OF THE DRIVEWAY ALLOWS MANEUVERING ROOM FOR GARBAGE AND FIRE TURN AROUND ACCESS ON SITE. THE LOCATION ALSO ENHANCES SAFETY BY ALLOWING US TO ALIGN WITH THE DRIVEWAY ON THE OPPOSITE SIDE OF 116TH AVE, AND STAY WELL CLEAR OF THE INTERSECTION WITH NE 128TH ST. THE DRIVEWAY HELPED INFLUENCE THE POSITION OF THE BUILDING WHICH ULTIMATELY HELPS MAXIMIZE AFFORDABLE UNITS.





THE MOST IMPACTED ZONES IMMEDIATE TO OUR ZONE ARE TO THE WEST AND NORTHWEST WHICH BOTH HAVE A MAXIMUM BUILDING HEIGHT OF 30 FEET ABOVE BUILDING ELEVATION. OUR PROPOSAL MAXIMIZES THE LOTS CODE FOR MAXIMUM BUILDING HEIGHT OF 75 FEET SO MOST VIEWS ARE NOT BEING OBSTRUCTED.







NORTHBOUND I405



NORTHBOUND I405



116TH AVE NE



NE 128TH ST

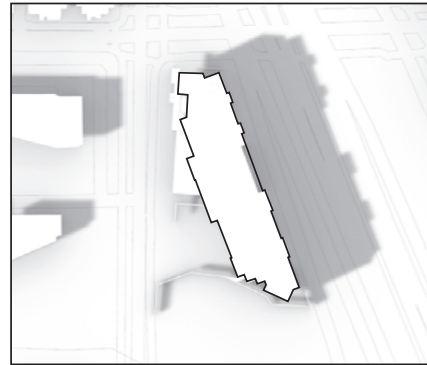
SHADOW STUDY: **MAY 8TH**



9 AM



12 PM

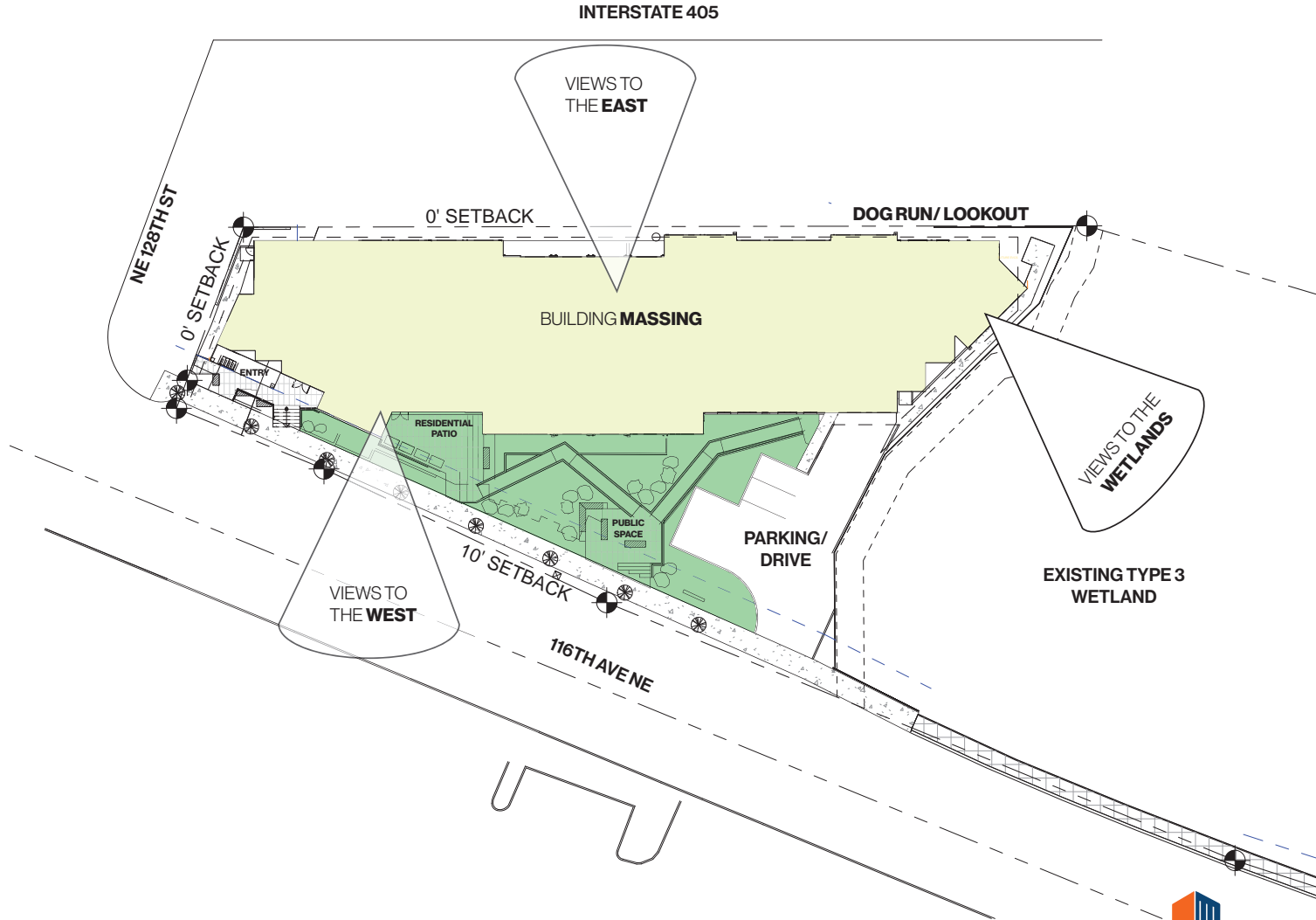


3 PM



6 PM

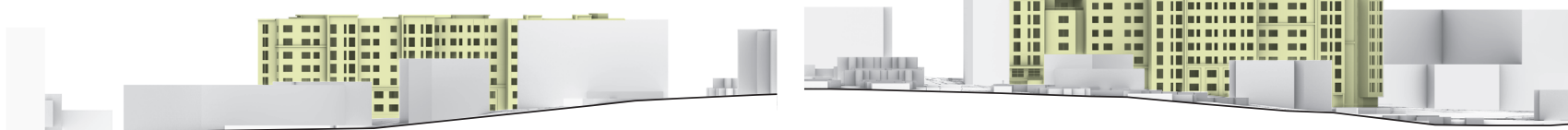
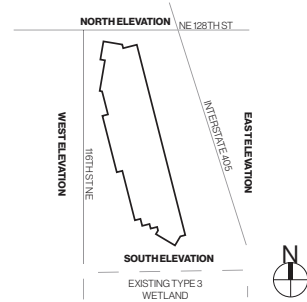
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SOUTH ELEVATION

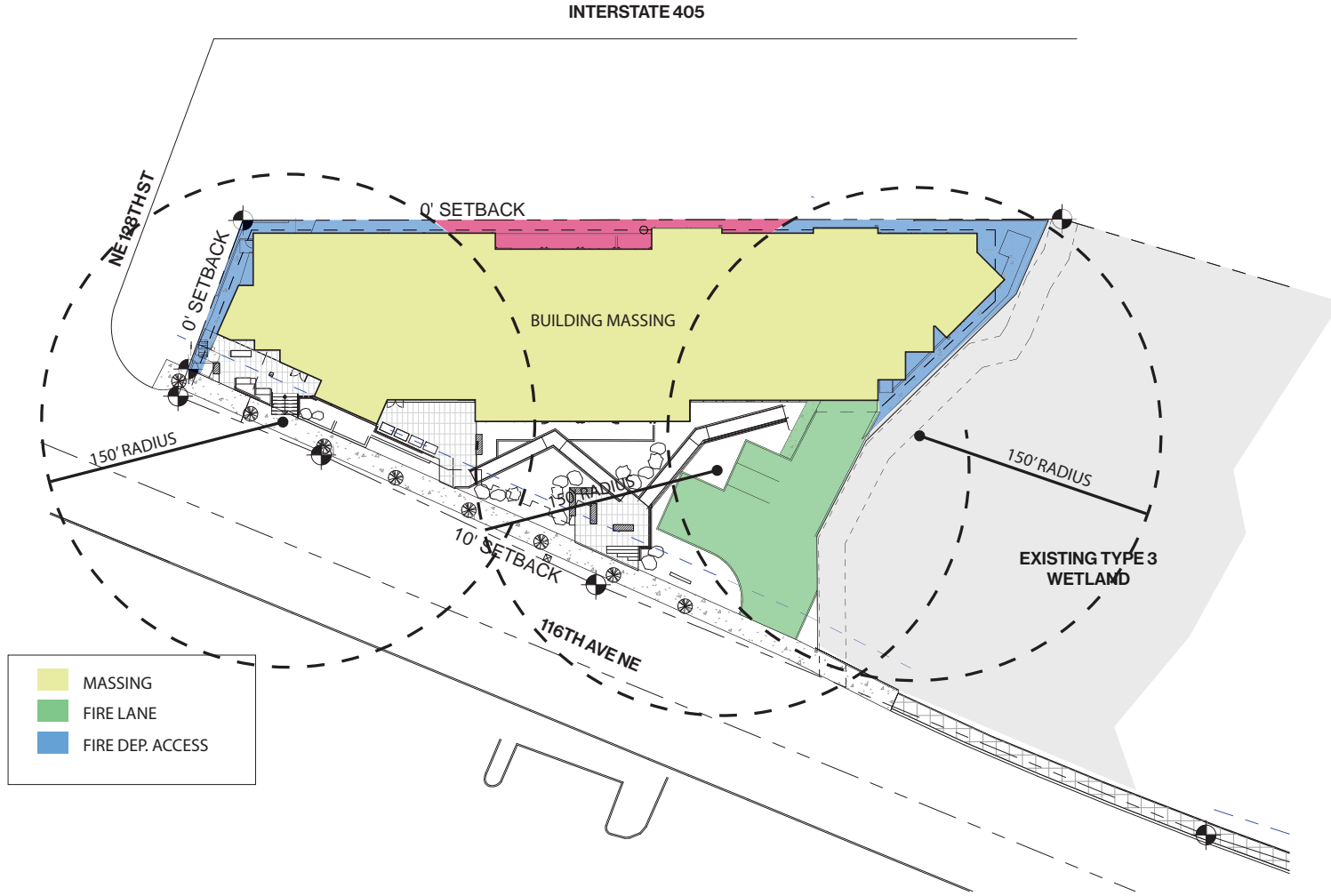
NORTH ELEVATION

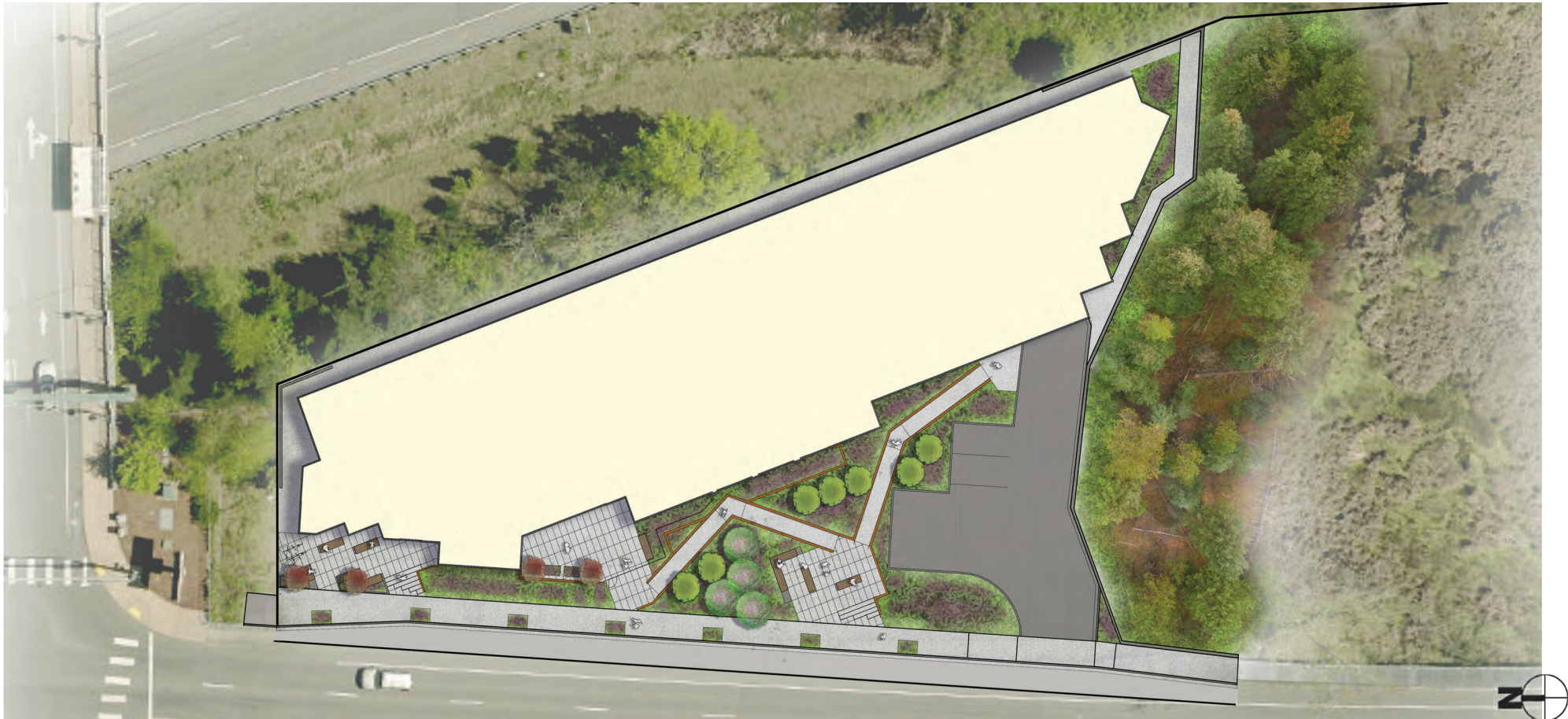
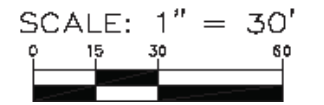


EAST ELEVATION

WEST ELEVATION

SCALE:
1" = 40'-0"







DOUBLEFILE VIBURNUM



KARL FOERSTER REED GRASS



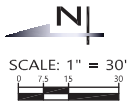
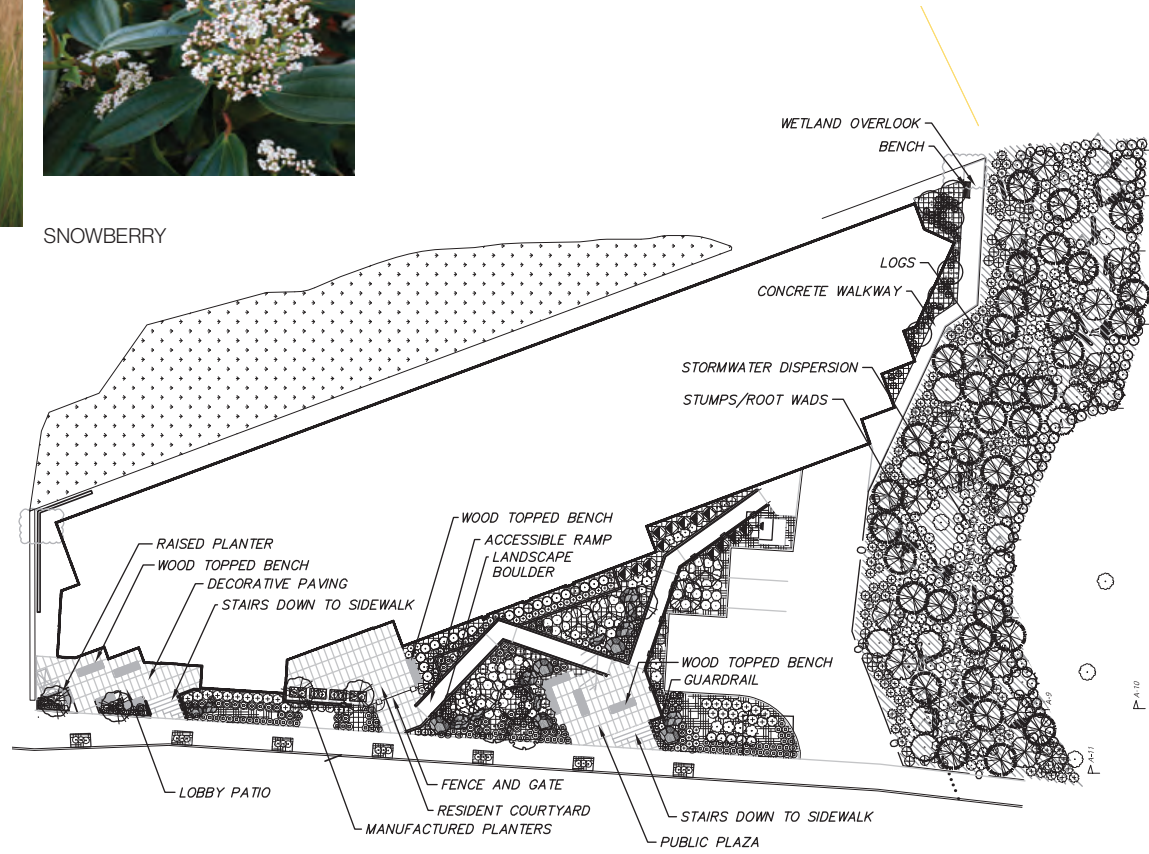
SNOWBERRY



DAYLILY



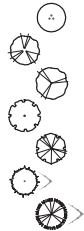
BLACK EYE SUSAN



SCHEMATIC PRICING SET

PLANT SCHEDULE SITE

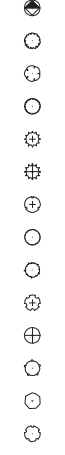
TREES



BOTANICAL / COMMON NAME

- Acer circinatum* / Vine Maple
- Acer palmatum* / Japanese Maple
- Amelanchier alnifolia* / Serviceberry
- Cornus kousa* / Kousa Dogwood
- Fraxinus latifolia* / Oregon Ash
- Picea sitchensis* / Sitka Spruce
- Thuja plicata* / Western Red Cedar

SHRUBS



BOTANICAL / COMMON NAME

- Choisya ternata* 'Sundance' / Sundance Mexican Mock Orange
- Cornus sericea* / Red Twig Dogwood
- Lonicera involucrata* / Twinberry
- Lonicera nitida* / Box Leaf Honeysuckle
- Mahonia aquifolium* / Oregon Grape
- Mahonia repens* / Creeping Mahonia
- Pinus strobus* 'Nana' / Dwarf White Pine
- Rosa pisocarpa* / Clustered Wild Rose
- Rubus spectabilis* / Salmonberry
- Salix scouleriana* / Scouler's Willow
- Sambucus racemosa* / Red Elderberry
- Symphoricarpos albus* / Common White Snowberry
- Viburnum davidii* / David Viburnum
- Viburnum plicatum tomentosum* 'Summer Snowflake' / Summer Snowflake Doublefile Viburnum

FERNS



BOTANICAL / COMMON NAME

- Polystichum munitum* / Western Sword Fern

GRASSES



BOTANICAL / COMMON NAME

- Calamagrostis x acutiflora* 'Karl Foerster' / Karl Foerster Feather Reed Grass
- Schizachyrium scoparium* 'Twilight Zone' / Twilight Zone Little Bluestem

PERENNIALS



BOTANICAL / COMMON NAME

- Artemisia x 'Powis Castle'* / Powis Castle Artemisia
- Hemerocallis x 'Red'* / Red Daylily
- Rudbeckia hirta* / Black-eyed Susan

GROUND COVERS



BOTANICAL / COMMON NAME

- Erosion Control Hydroseed Mix*
- Fragaria chiloensis* / Beach Strawberry
- Polystichum munitum* / Western Sword Fern
- Prunus laurocerasus* 'Mount Vernon' / Mount Vernon English Laurel
- Rosmarinus officinalis* 'Prostratus' / Dwarf Rosemary



COMMON WHITE SNOWBERRY



WESTERN SWORD FERN



BEACH STRAWBERRY



VINE MAPLE TREE



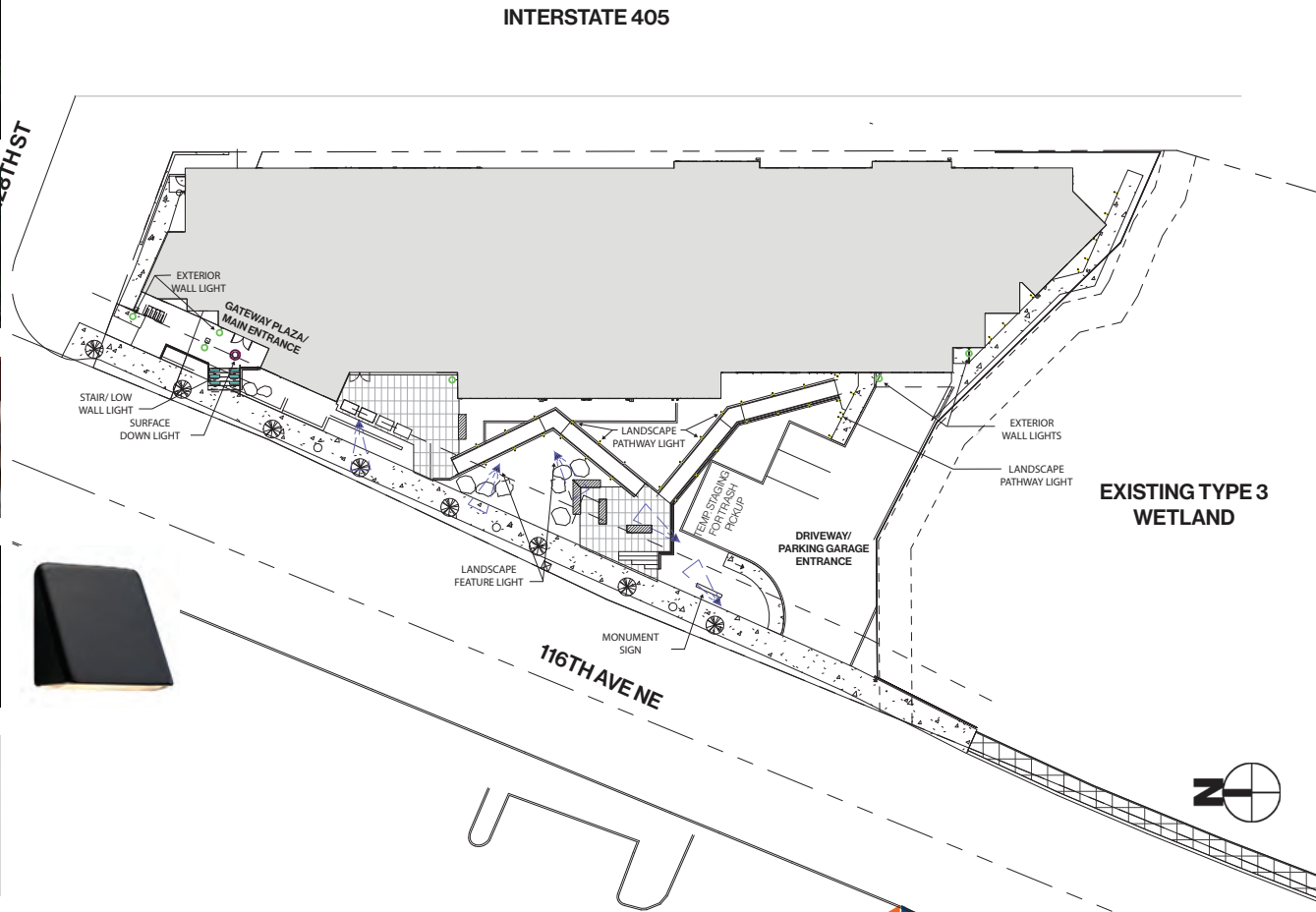
SERVICEBERRY TREE



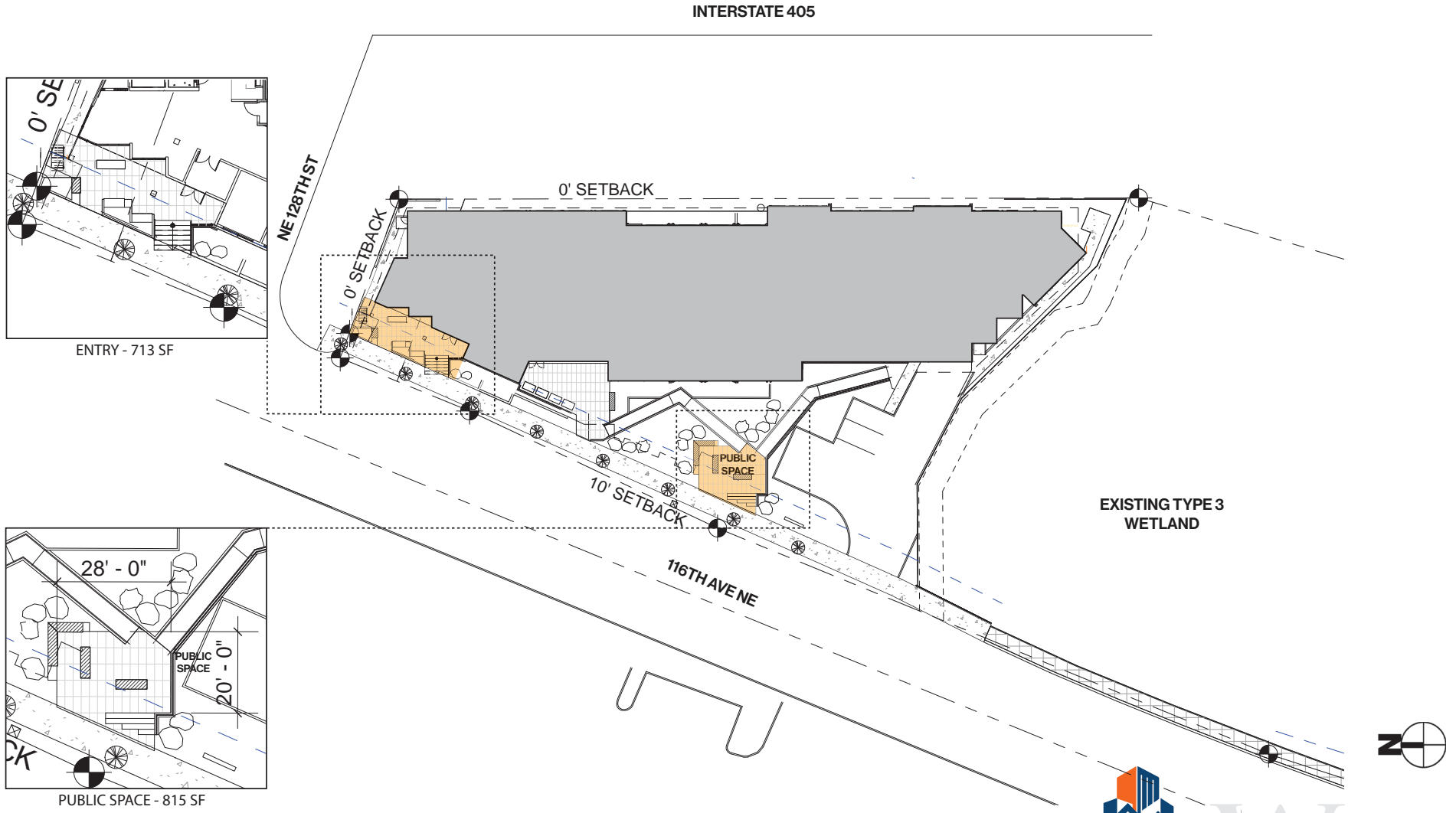
KOREAN DOGWOOD

SYMBOL
MANUFACTURER/MODEL/DESCRIPTION

	<p>FX Luminaire CC (FEATURE) Small scale up light used for small scale elements. 5.9" L x 1.6" D straight shroud. Order code: CC, Brass, (AB) Antique Bronze, 3-Prong Spike Lamp: CC-1LED, 2W 2.4VA, 2700K, Beamspread: Spot</p>	
	<p>FX Luminaire M-PL (PATHWAY) Die-cast aluminum path light with powder-coated finish. 2.2" W x 7.4" W x 21.3" H. Order code: M-PL, Aluminum, (FB) Flat Black, Ground Mount Lamp: M-PL-1LED, 2W 2.4VA, 2700K, Beamspread: Flood</p>	
	<p>FX Luminaire ZC 9 (SURFACE DOWNLIGHT) Die-cast aluminum or copper downlight with powder-coated finish options. 2.1" D x 5.5" H. Order code: ZC, Aluminum, (BZ) Bronze Metallic, Direct Mount Lamp: ZC-1LED, 2W 2.4VA, 2700K, Beamspread: Flood</p>	
	<p>FX Luminaire MO-SQ (STAIR/LOW WALL) Hardscape light ideal for in grade and in wall applications. 3.25" W x 3.25" H x 1.4" D. Order code: MO-SQ, Brass, (FB) Flat Black, MO Mounting Bracket Lamp: MO-SQ-1LED, 2W 2.4VA, 2700K, Beamspread: Wide Flood</p>	
	<p>Kichler (EXT. WALL LIGHT) 9246BK Outdoor Cylinder Wall Mount Sconce UpLight Downlight, Black 2-Light (6" W x 15" H)</p>	



SCALE:
1" = 40'-0"



SCALE:
3/32" = 1'-0"

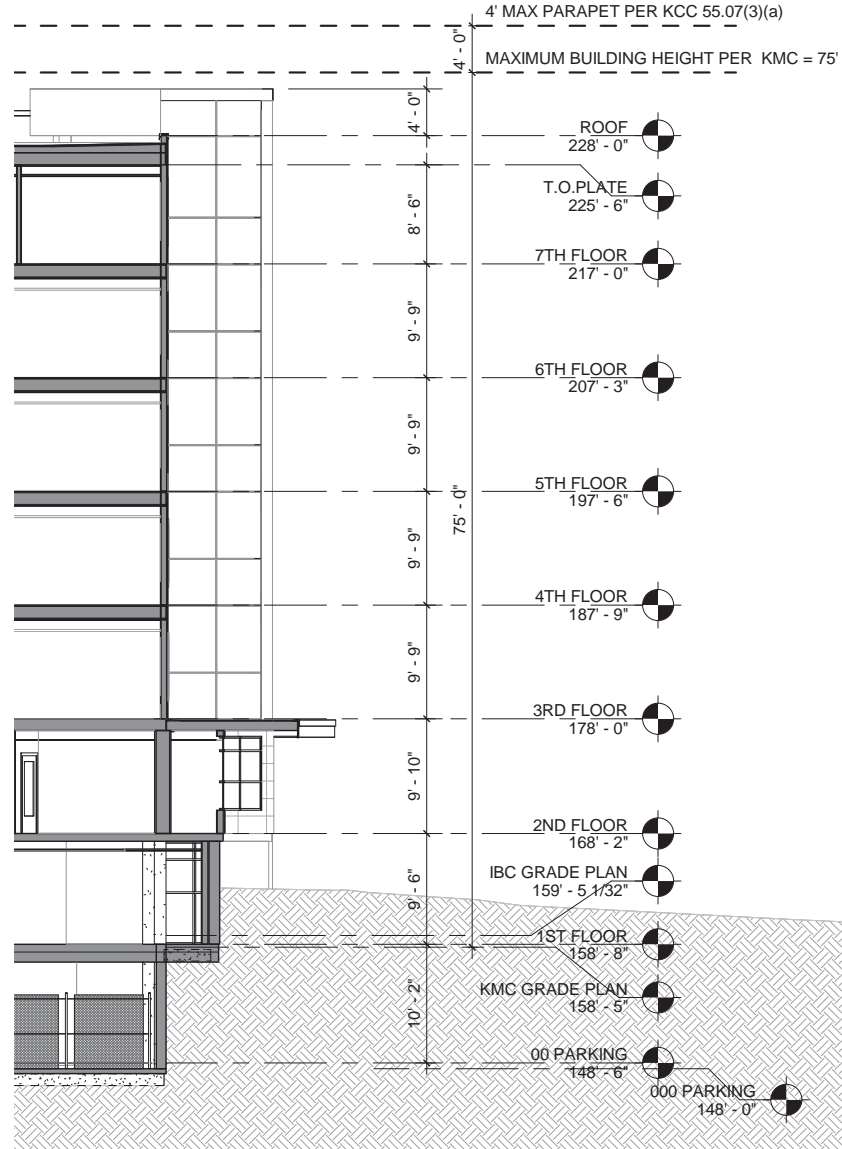
Average Building Grid Plane

150	160	169	153.5
150	160	169	149.5
150	160	169	149.5
150	160	169	149.5
150	163	162	149
150	166	159	148.5
152	169	159	148
155	170	159	148
158	173	159	148
160	174	159	148
160	174	159	148
160	169	159	148
160	169	159	148
160	171	159	149
160	171	158	150
160	171	156	150
160	170.5	154	148
160	169	152	146
160	169	153.5	
160	169	153.5	

Average: **12357**

Sum: **158.4**

Total Numbers: **78**

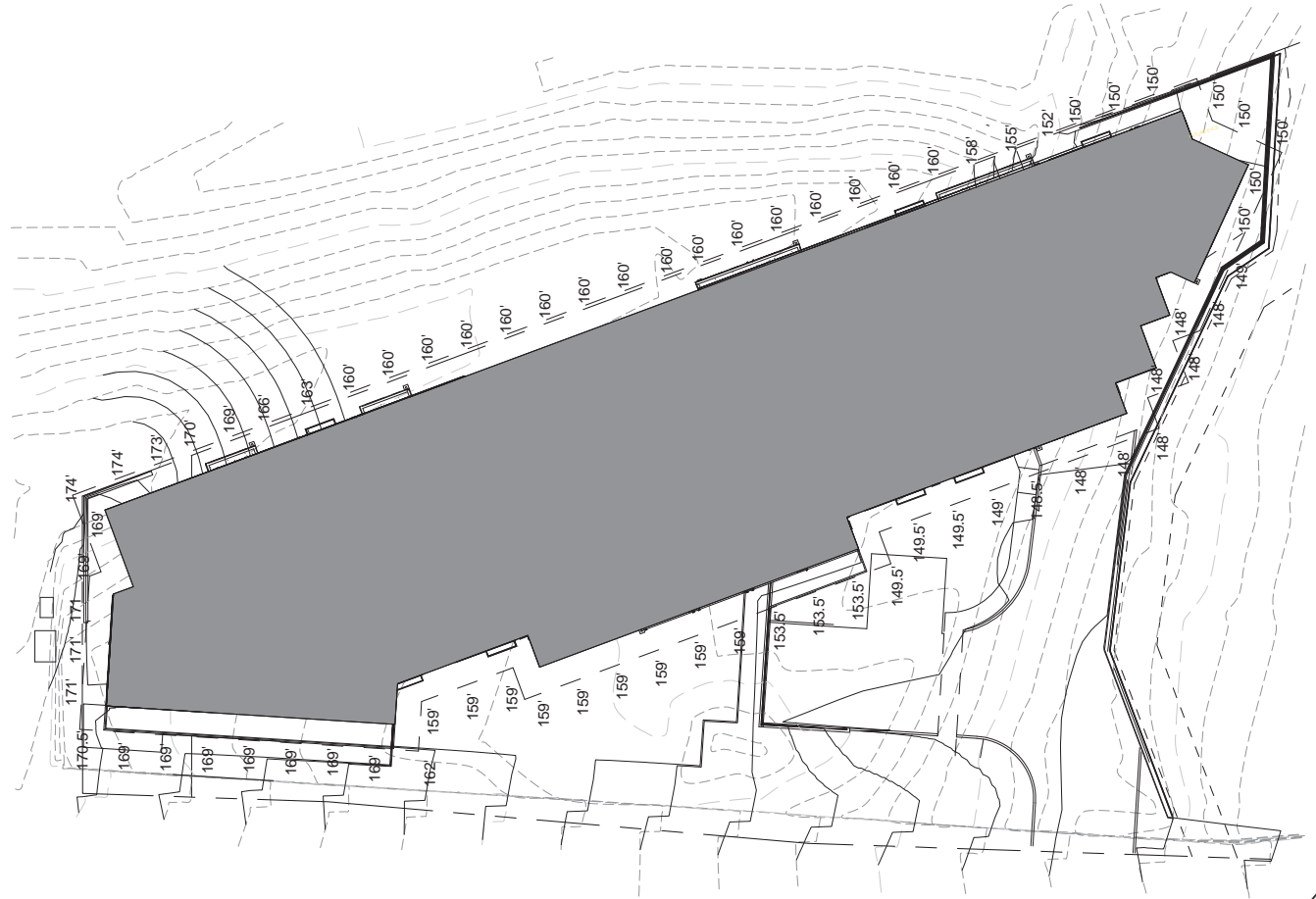


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NONE

Average Building Grid Plane

150	160	169	153.5
150	160	169	149.5
150	160	169	149.5
150	160	169	149.5
150	163	162	149
150	166	159	148.5
152	169	159	148
155	170	159	148
158	173	159	148
160	174	159	148
160	174	159	148
160	169	159	148
160	169	159	148
160	171	159	149
160	171	158	150
160	171	156	150
160	170.5	154	148
160	169	152	146
160	169	153.5	
160	169	153.5	

Average: **12357**
Sum: **158.4**
Total Numbers: **78**

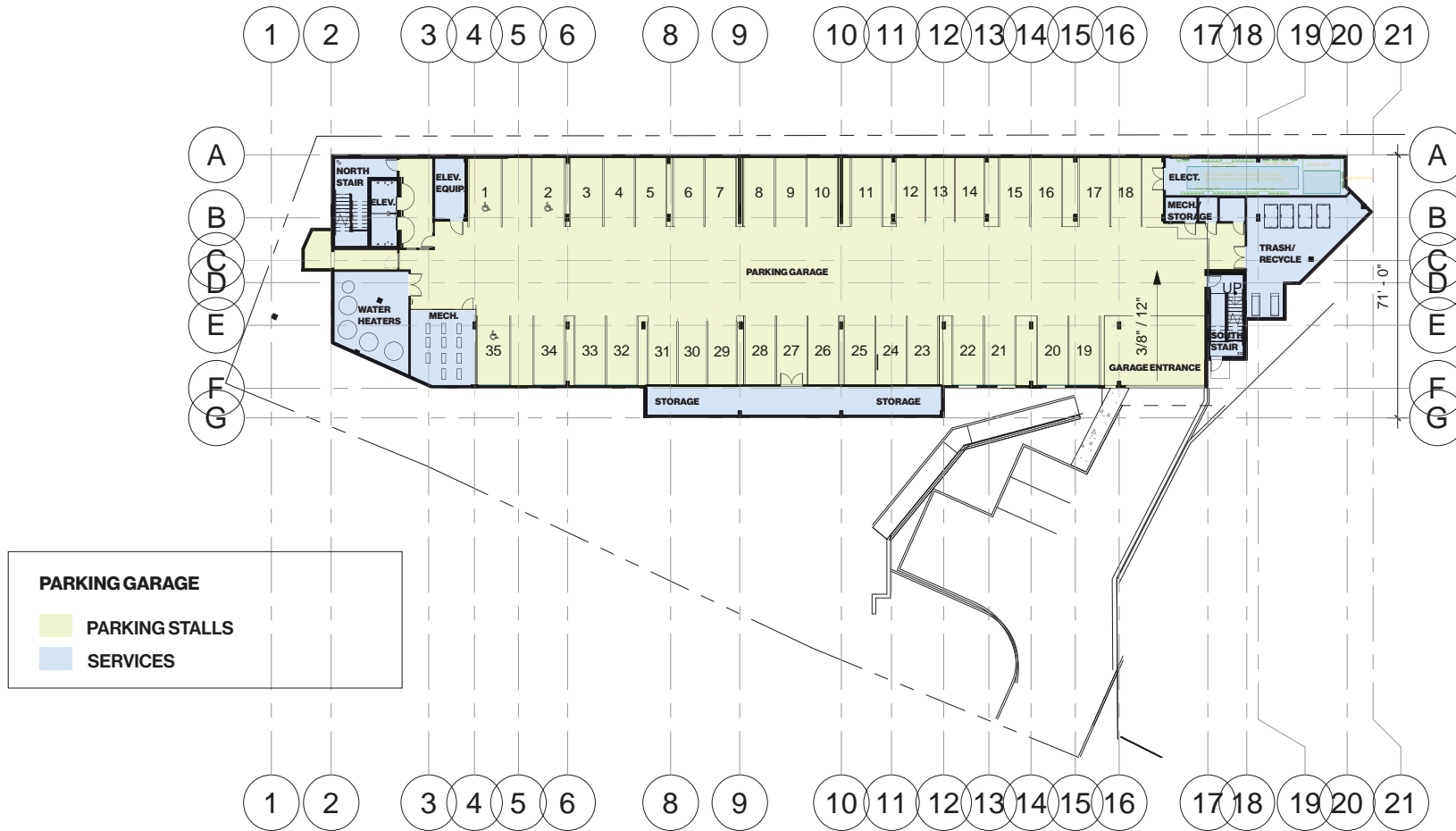


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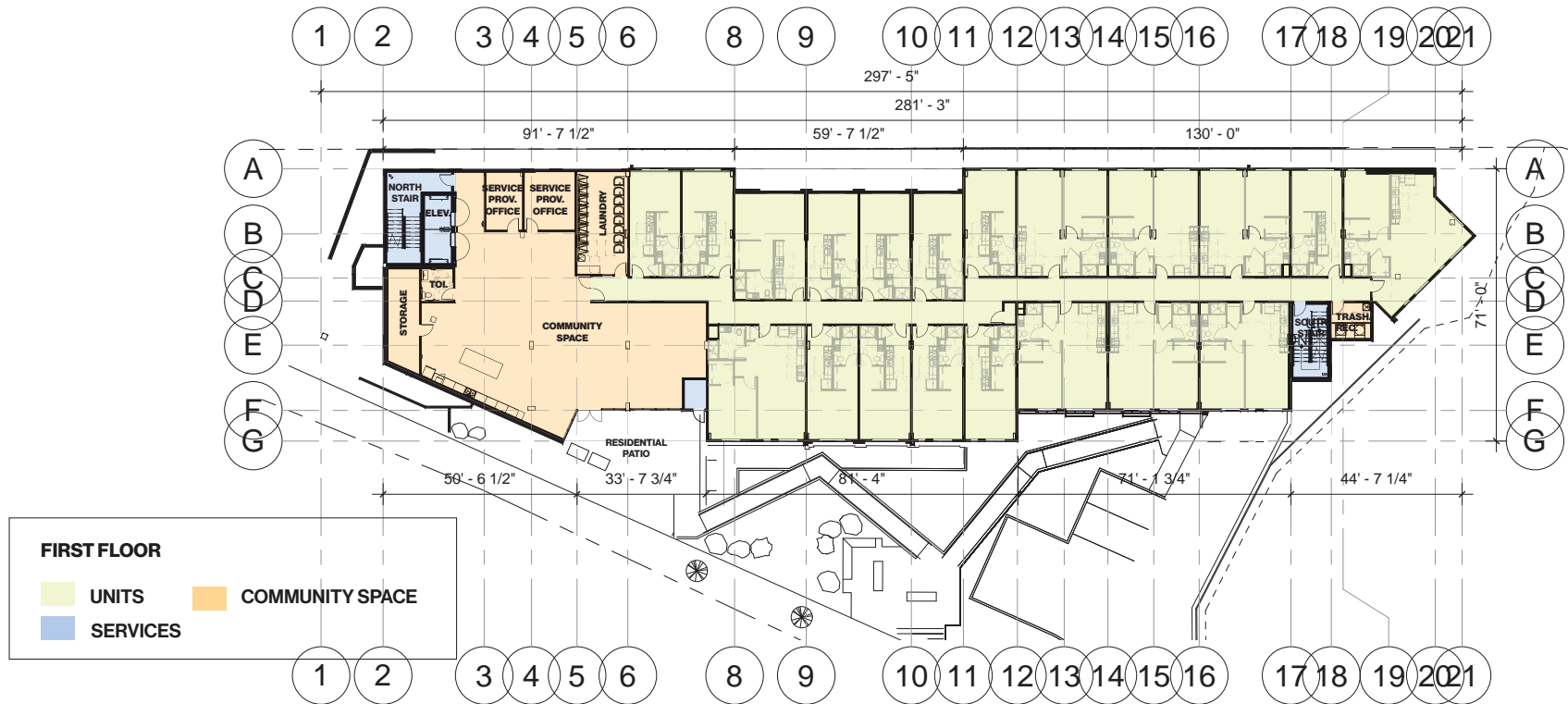


- BUILDING MASS
- RESIDENTIAL PATIO
- GATEWAY PLAZA/ MAIN ENTRANCE
- PARKING / GARAGE ENTRANCE
- PUBLIC SPACE/COURTYARD

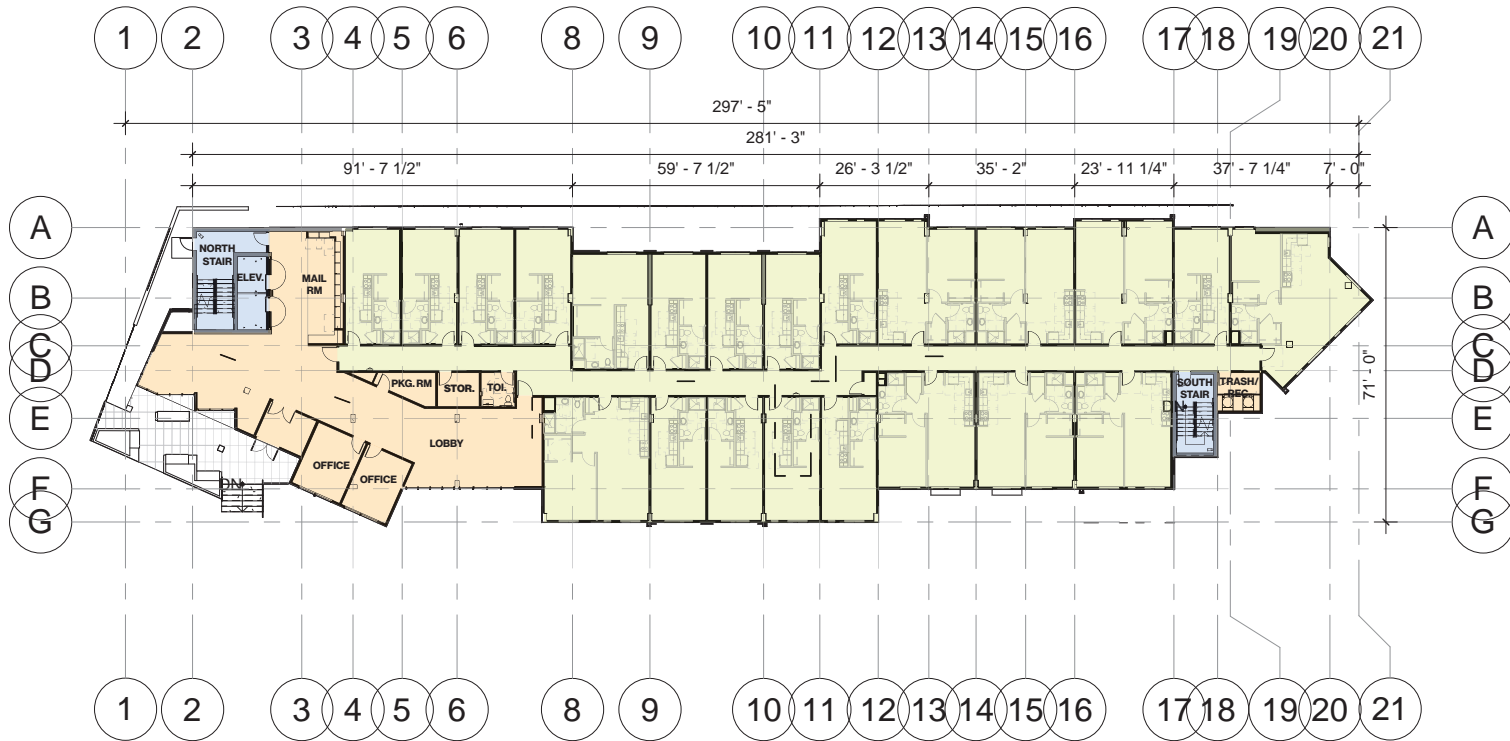
SCALE:
1" = 30'-0"



SCALE:
1" = 30'-0"



SCALE:
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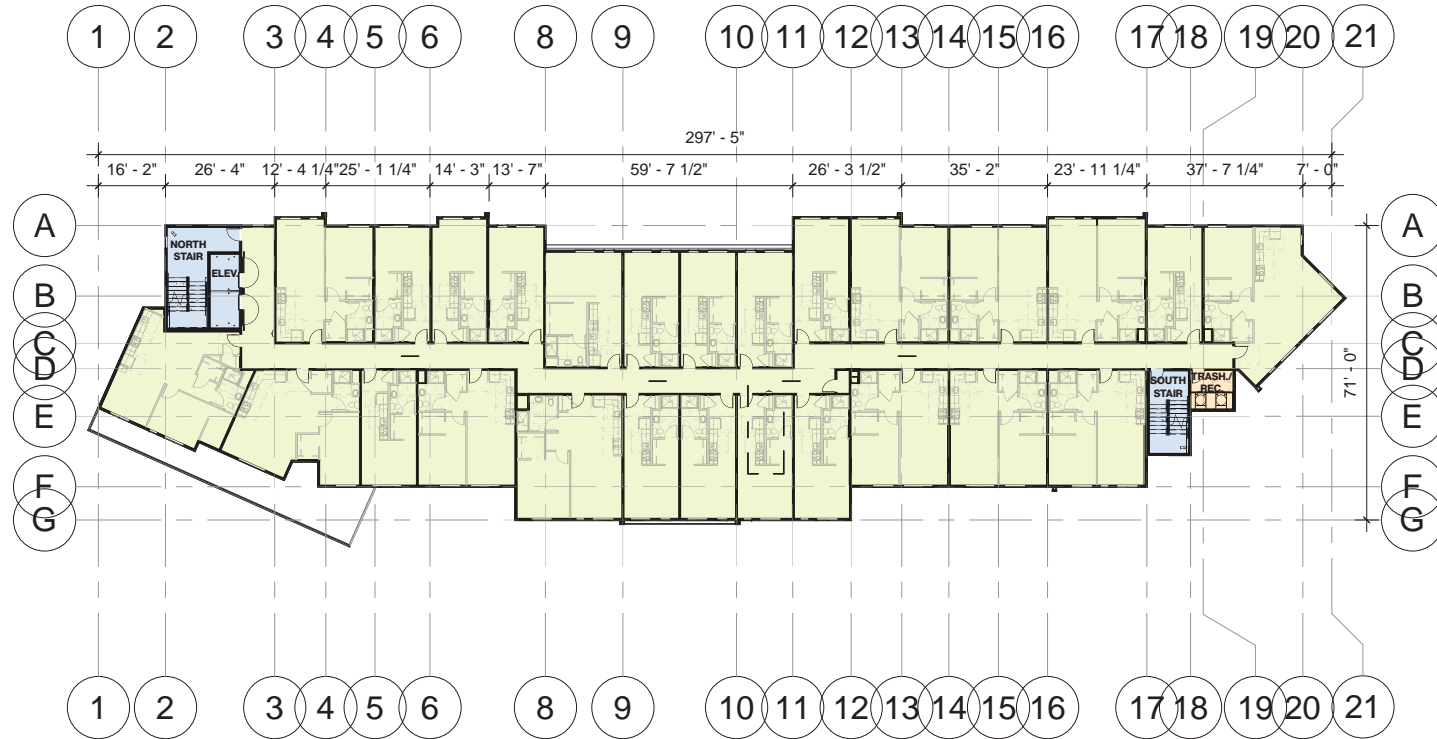


SECOND FLOOR

- UNITS
- COMMUNITY SPACE
- SERVICES



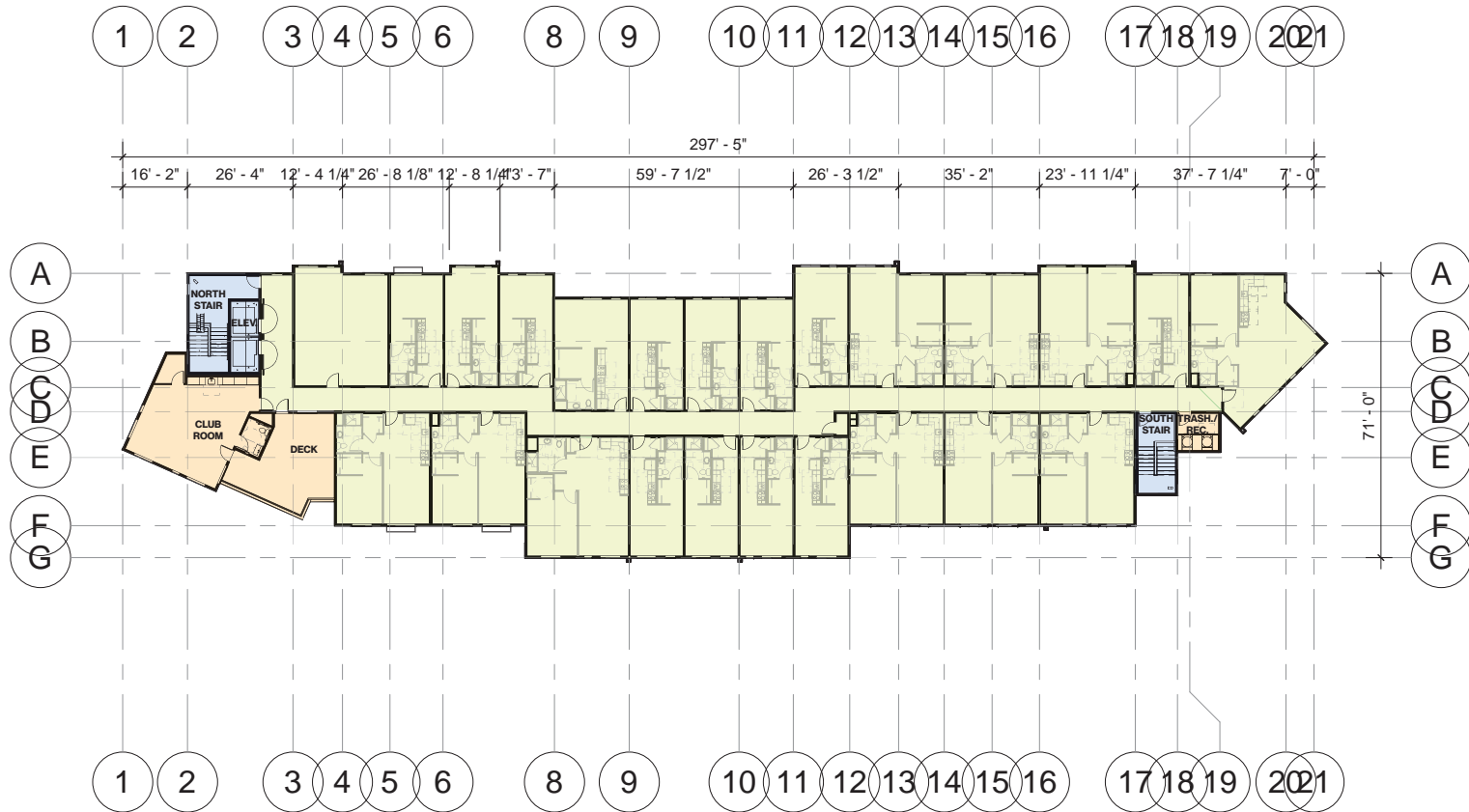
SCALE:
1" = 30'-0"



FLOORS 3-6	
 UNITS	 COMMUNITY SPACE
 SERVICES	



SCALE:
1" = 30'-0"



SEVENTH FLOOR

- UNITS
- COMMUNITY SPACE
- SERVICES



SCALE:
1" = 20'-0"

MATERIALS:

- A** - "NEWBURY PORT BLUE" LAP SIDING
- B** - NICHIIHA CORBOSA "MOONDUST"
- C** - "GRAY CLOUD" HARDIE PANEL
- D** - "STARDUST" HARDIE PANEL
- E** - "SILVER FOX" HARDIE PANEL
- F** - "SUMMER WHEAT" LAP SIDING
- G** - "CHAR BROWN" HARDIE PANEL
- H** - "GULL WING GRAY" HARDIE PANEL
- I** - "GREENFIELD PUMPKIN" HARDIE PANEL

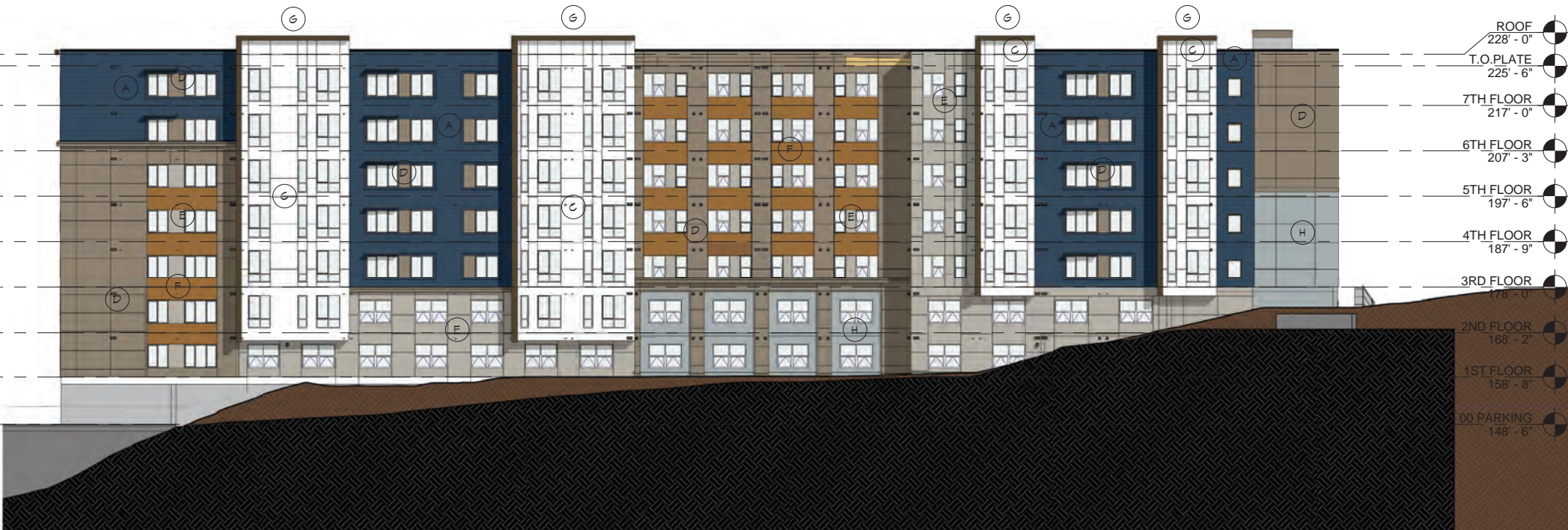


SITE SECTION - WEST

SCALE:
1" = 20'-0"

MATERIALS:

- A** - "NEWBURY PORT BLUE" LAP SIDING
- B** - NICHIIHA CORBOSA "MOONDUST"
- C** - "GRAY CLOUD" HARDIE PANEL
- D** - "STARDUST" HARDIE PANEL
- E** - "SILVER FOX" HARDIE PANEL
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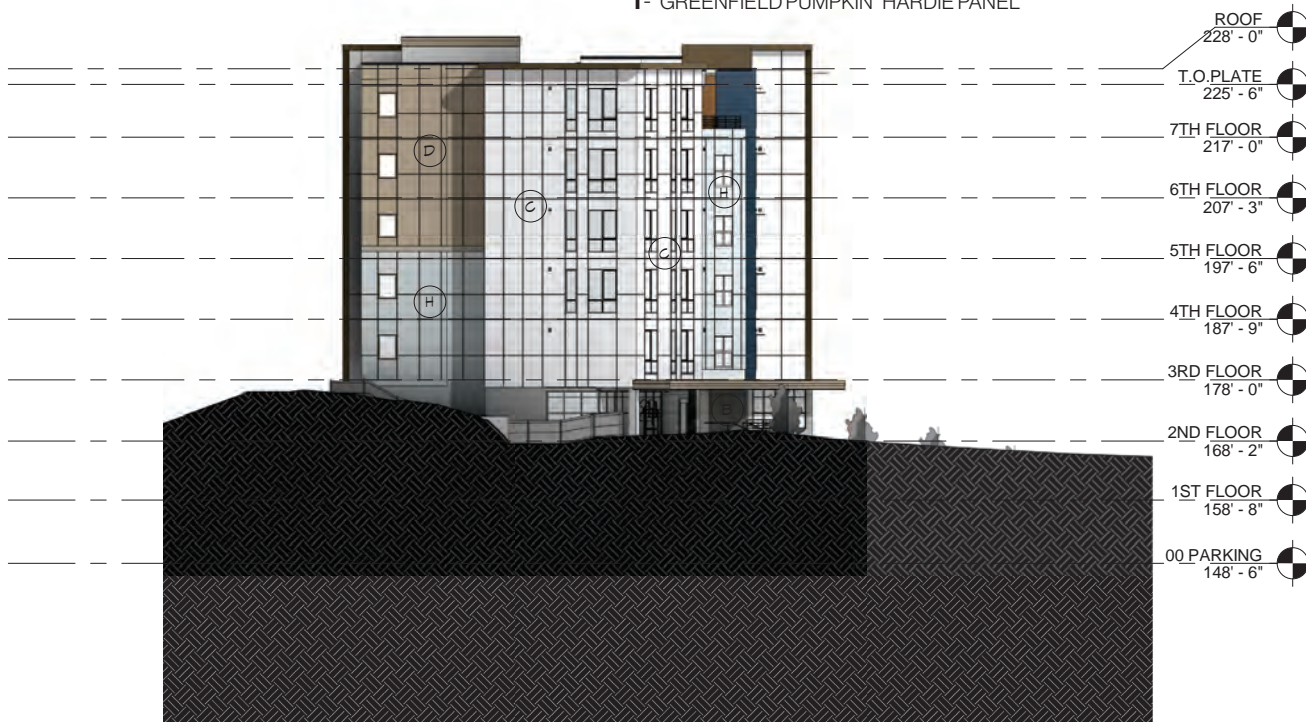


SITE SECTION - EAST

SCALE:
1" = 20'-0"

MATERIALS:

- A** - "NEWBURY PORT BLUE" LAP SIDING
- B** - NICHIIHA CORBOSA "MOONDUST"
- C** - "GRAY CLOUD" HARDIE PANEL
- D** - "STARDUST" HARDIE PANEL
- E** - "SILVER FOX" HARDIE PANEL
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- G** - "CHAR BROWN" HARDIE PANEL
- H** - "GULL WING GRAY" HARDIE PANEL
- I** - "GREENFIELD PUMPKIN" HARDIE PANEL



SITE SECTION - NORTH

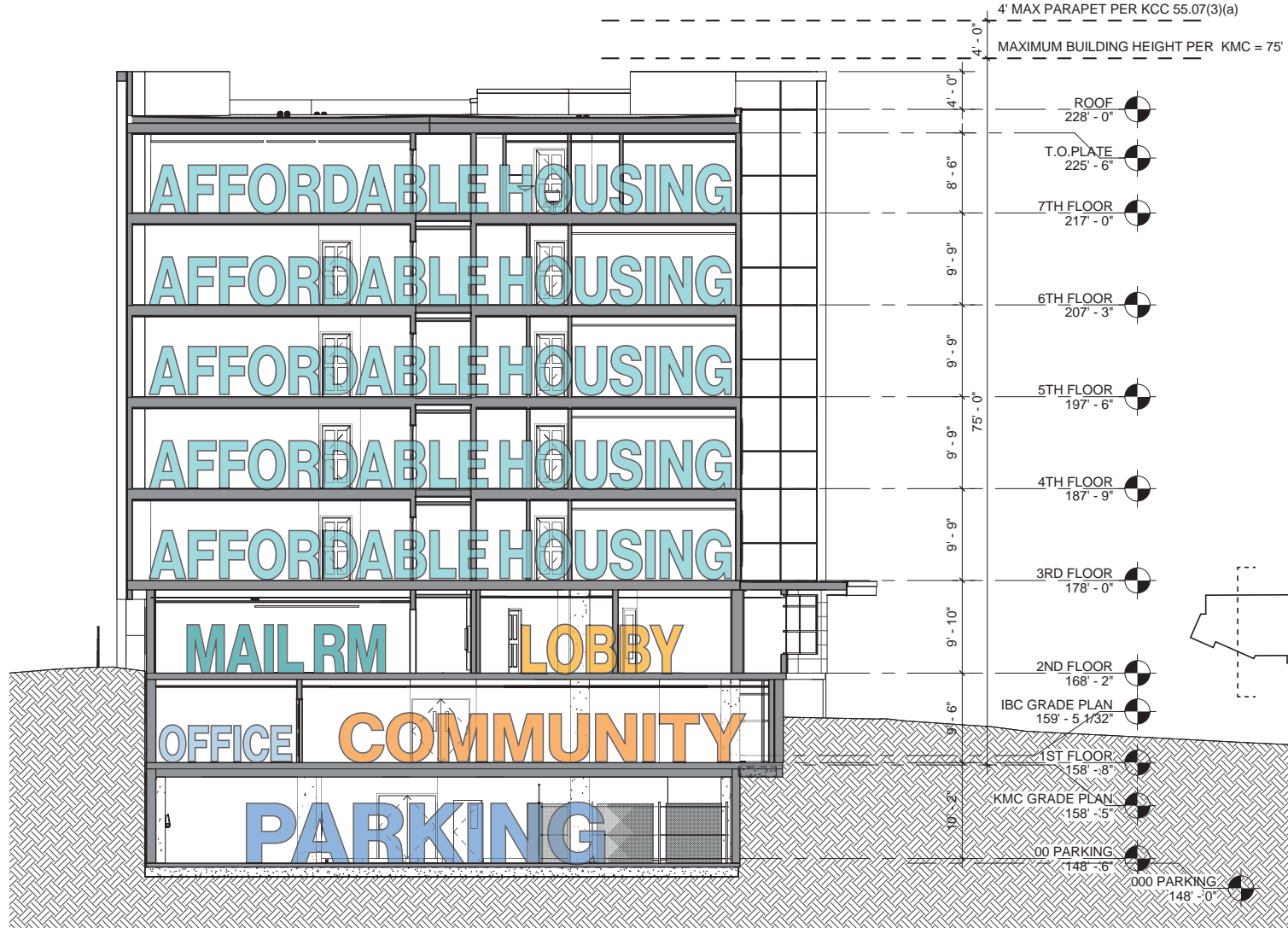
SCALE:
1" = 20'-0"

MATERIALS:

- A** - "NEWBURYPORT BLUE" LAP SIDING
- B** - NICHIIHA CORBOSA "MOONDUST"
- C** - "GRAY CLOUD" HARDIE PANEL
- D** - "STARDUST" HARDIE PANEL
- E** - "SILVER FOX" HARDIE PANEL
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- H** - "GULL WING GRAY" HARDIE PANEL
- I** - "GREENFIELD PUMPKIN" HARDIE PANEL

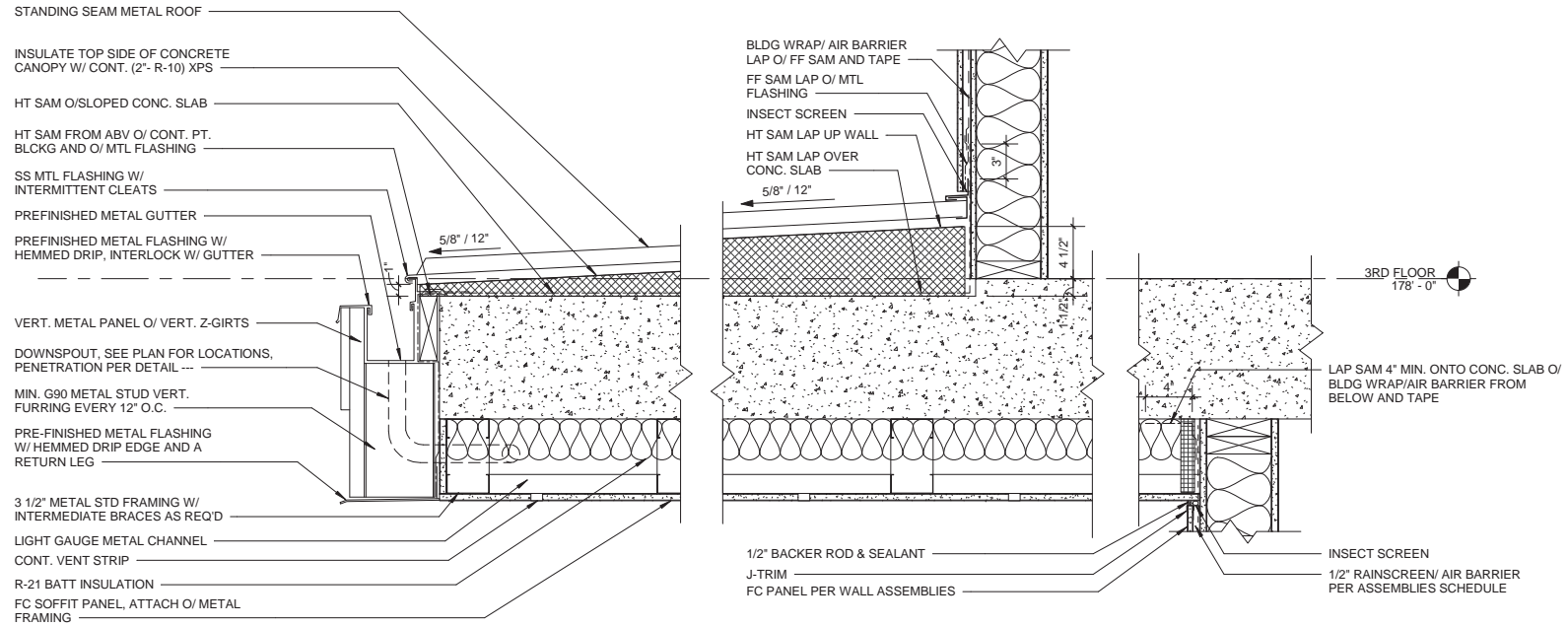


SITE SECTION - SOUTH



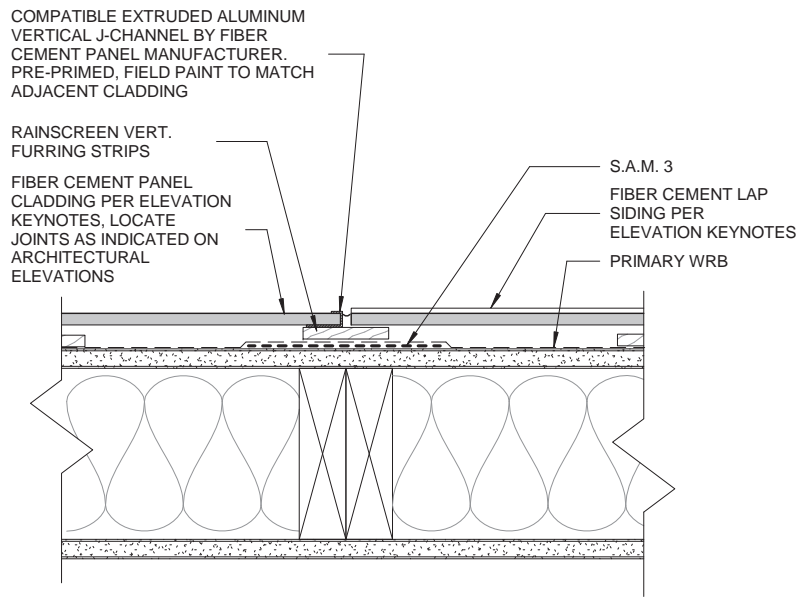
*NOT TO SCALE

***NOT TO SCALE**

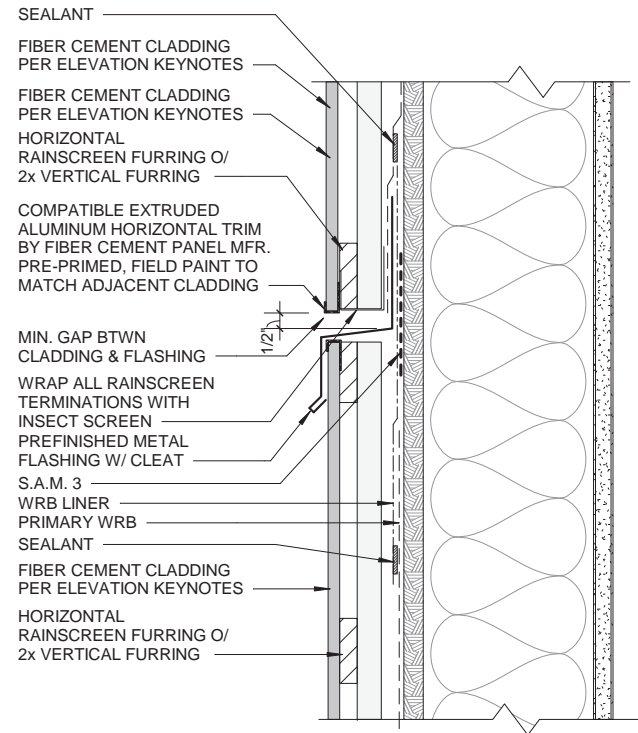


ENTRY ROOF @ THIRD FLOOR

***NOT TO SCALE**



VERTICAL JOINT, LAP TO PANEL



HORZ. THROUGH WALL FLASHING

1. ENTRY GATEWAY FEATURES:



MUNICIPAL GUIDELINES:

- Distinctive landscaping to soften the street level approach.
- Use of high quality materials to identify the public entrance of the building.
- Distinct architectural element at the corner to express an active and lively space.
- Building identification / way-finding visible from the corner intersection.

OUR APPROACH:

The design locates the main entrance at the corner, but focuses towards 116th due to the inaccessibility of 128th.

The building identification has been included at the face of the canopy above the main entrance and is readily visible from the intersection to assist with way finding.

The stairs and elevator tower have been pushed to the back of the building to allow for a more impactful and inviting architectural language at the corner.

High quality materials including storefront window system and the use of nichiha 'corbosa' siding help distinguish the public entrance from the residential portions of the building.

We've incorporated raised planters with a colorful plant palate to soften the entrance at the street level and help mask some of the retaining walls which result from the steep slope along 116th.

2. STREET TREES:

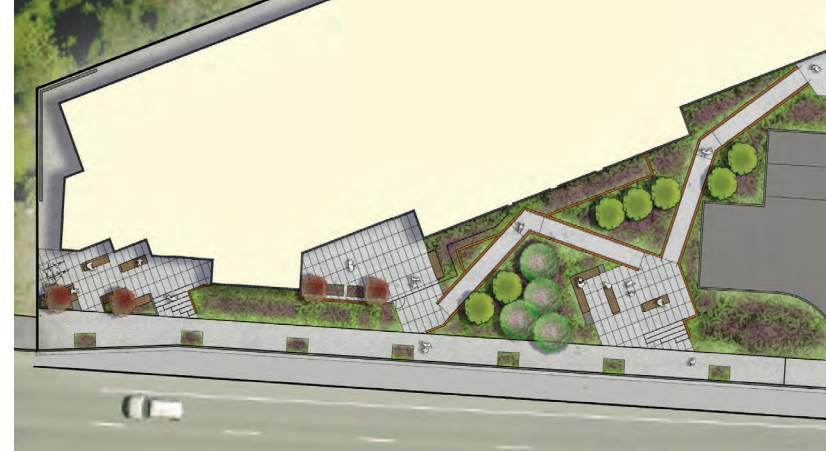


MUNICIPAL OBJECTIVES:

- To upgrade the character and identity of the Totem Lake Business District.
- To enhance the pedestrian environment in the totem lake Business District.
- To use trees that provide seasonal interest.
- To use trees appropriate to the urban environment of the Totem Lake Business District.

MUNICIPAL GUIDELINES:

- a. Incorporate street trees along the street, internal access roads and pathways.
- b. Encourage developments to use street trees as a unifying feature of the development.
- c. Select and maintain tree species that will accommodate pedestrian and vehicular traffic, and maintain visibility into and through sites for safety purposes.



OUR APPROACH:

The project is proposing 4'x6' planting wells at the back of curb. Due to a utility conflict the project is unable to install street trees. The project will provide the planting wells to give interest and some separation for the pedestrians. To provide scale and interest the project will have numerous garden scale trees on the building site between the proposed building and the sidewalk to provide visual relief and interest for the pedestrians and the users of the proposed spaces.

3. STREET CORNERS:



MUNICIPAL GUIDELINES:

- Encourage design treatments that emphasize street corners through the use of building location and design, plaza spaces, landscaping, distinctive architectural features, and/or signage.
- Encourage all buildings located at or near street corner to incorporate special architectural elements that add visual interest and provide a sense of human proportion and scale.

OUR APPROACH:

We've incorporated significant modulation in both directions as well as a shift in the geometry of the building to make this element distinct.

A large canopy provides coverage for the entrance stairs and ramp, as well as an generous gathering area just outside the entrance.

High quality materials including storefront window system and the use of Nichiha 'Corbosa' siding help distinguish the public entrance from the residential portions of the building.

A secondary level of architectural detail includes window sun shades, belly bands, and carefully incorporated landscaping at street level to give the corner additional impact at the human scale in addition to the significant modulation provided in the larger massing.

4. PEDESTRIAN-FRIENDLY BUILDING FRONTS:



MUNICIPAL GUIDELINES:

- Incorporate transparent windows, doors, and weather protection features along all non-residential facades adjacent to a sidewalk or internal pathway.
- Alternative treatments may be considered if they meet the objectives.

OUR APPROACH:

Storefront windows are included at both floor 1 and 2 to provide transparency and imply an active and public street presence.

The landscaping and resident courtyard build upon the building design to extend it to a human scale along the sidewalk and street frontage.

Street trees provide shade along the sidewalk and help to serve as a buffer between vehicular traffic and pedestrian traffic along 116th street.

5. BUILDING LOCATION AND ORIENTATION:



OUR APPROACH:

The irregular shape of the site required a unique solution. The primary pedestrian entrance has been located immediately off the street and sidewalk at the intersection.

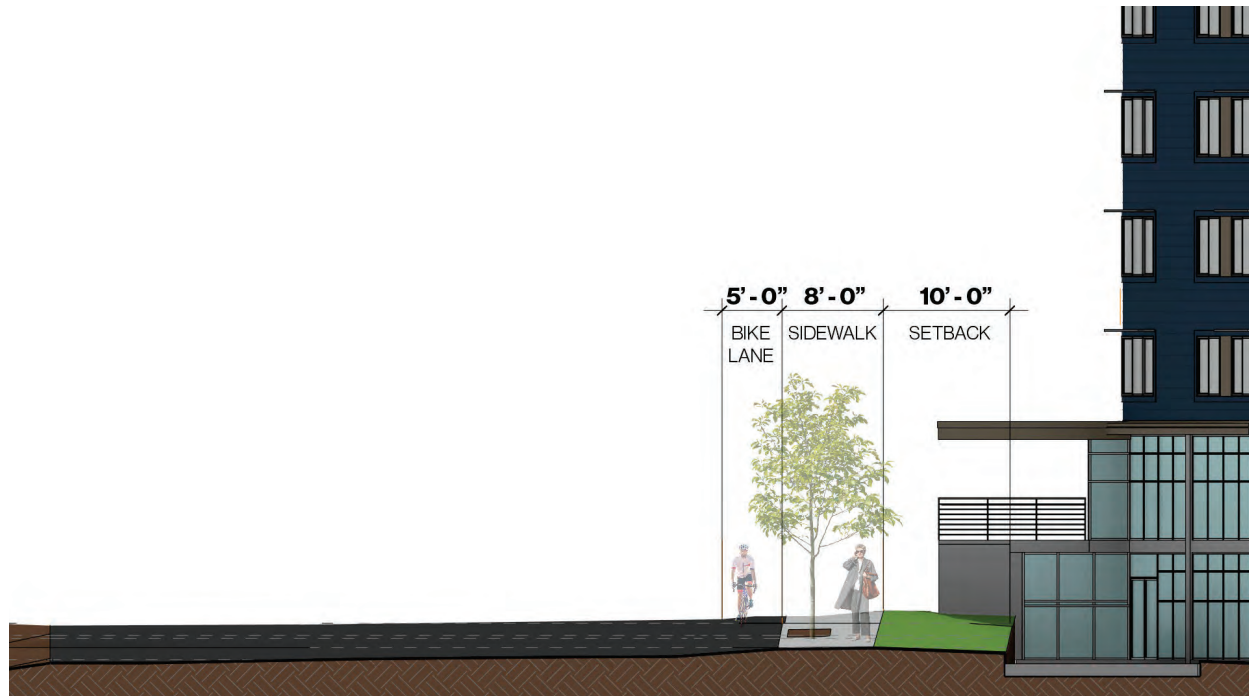
As the structure continues south, the building angles away from the street. This gives additional privacy to the residential portion of the building and allows for a courtyard off of the public sidewalk, encouraging activity and interaction along the public row.

Vehicular access is at the far south of the building which allows us sufficient room for maneuverability to provide a pickup and turnaround for loading and garbage, and access to the parking garage. This location was also preferred to hold the site access as far as possible from the intersection for safety reasons.

MUNICIPAL GUIDELINES:

- Locate and orient buildings toward streets, plazas or common open spaces, and major internal pathways with parking to the side and/or rear.
- Configure buildings to create focal points of pedestrian activity. This is particularly important on large sites.
- Configure development to provide opportunities for coordinated pedestrian and vehicular access.
- Site and orient multi-story buildings to minimize impacts to adjacent single family residents.
- Encourage buildings located adjacent to any street to orient to the street.

6. SIDEWALK AND PATHWAY WIDTHS:



OUR APPROACH:

Despite the narrow site, we have dedicated additional row to accommodate the required 5' bike lane, 8' sidewalk.

Street tree wells have been incorporated into the sidewalk to provide shade and a visual buffer from the street.

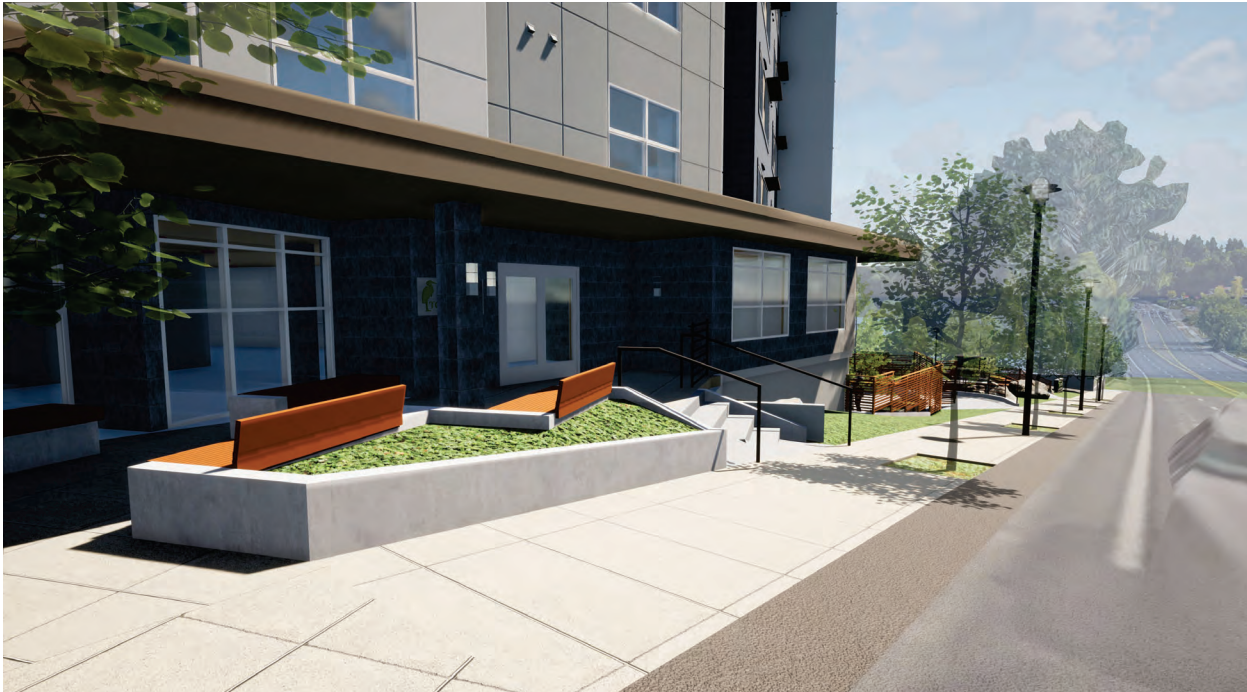
We are maintaining a 10' setback to create additional buffer between the building and the row. This begins at 10' along the more public spaces at the intersection, and then increases to the south at the more residential portions of the building.

Note that we are requesting relief to allow for our entrance canopy to encroach approximately 6'-6" into the setback to allow for coverage of the stairs and ramp at the main entrance to shelter the ramp and stairs.

MUNICIPAL GUIDELINES:

- Integrate a "curb zone" into sidewalk (street trees, street signs, garbage cans, etc.)
- Sidewalks or pathways need buffers for safety and to be inviting.
- Sidewalks or pathways should support various activities.

7. PEDESTRIAN COVERINGS:



OUR APPROACH:

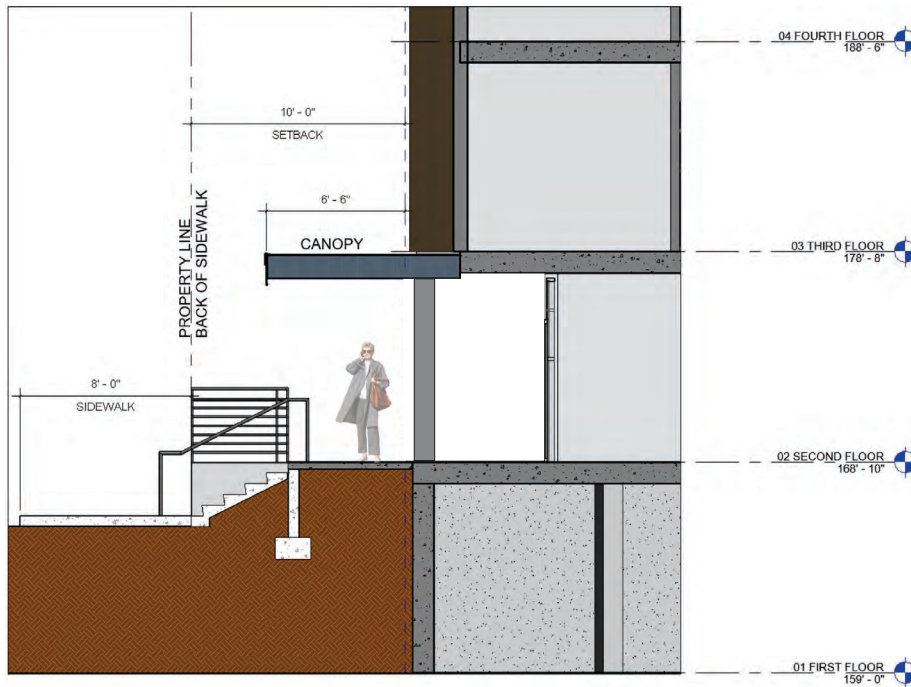
The building is pushed back slightly from the street given the building setback requirements and its residential nature.

The main and lower entrances are provided with generous canopies for gathering, waiting, and transition. The building at street level is further recessed to provide additional usable space at the building frontage.

MUNICIPAL GUIDELINES:

- Provide weather protection along the primary exterior entrance of all businesses, residential units, and other buildings.
- Design weather protection features to provide adequate width and depth at building entries and along building Facades that are oriented toward sidewalks and pathways

DEVIATION OF SETBACK:



OUR APPROACH:

The building is pushed back slightly from the street given the building setback requirements and its residential nature.

Note that we are requesting a deviation to extend the canopy 6'-6" feet into the 10' setback to provide better coverage for the ramp and stairs from the sidewalk to the primary entrance.

Staff Note:

As noted in the Staff Memo and at the September 12th Meeting, a deviation or modification is not needed for the proposed canopy is allowed to extend into the required setback yard per code.

MUNICIPAL GUIDELINES:

- Provide weather protection along the primary exterior entrance of all businesses, residential units, and other buildings.
- Design weather protection features to provide adequate width and depth at building entries and along building Facades that are oriented toward sidewalks and pathways

8. BLANK WALLS:



MUNICIPAL GUIDELINES:

- Minimize visible blank walls exposed to public view
- Provide planting to screen most of blank walls



OUR APPROACH:

The site does contain a number of retaining walls due to the slope along 116th. At the immediate street frontage, careful grading and the use of tiered planter boxes helps avoid any resulting blank wall.

Along the basement level some portions of the garage wall are exposed. This is broken up with decorative grilles which provide fresh air to the garage, and a combination of berms, raised planters, and landscaping.

The large retaining wall adjacent to the wetland buffer will be heavily planted as part of the buffer mitigation strategy, and is approximately 12' below the level of sidewalk. This area is not readily visible and will be almost completely screened as the vegetation matures.

9. LIGHTING:



MUNICIPAL GUIDELINES:

- Provide adequate lighting levels in all areas used by pedestrians and automobiles, including building entries, Walkways, parking areas, circulation areas, and open spaces.
- Lighting should be provided at consistent levels, with gradual transitions.
- Building facades in pedestrian areas should provide lighting to walkways and sidewalks through building mounted lights, canopy- or awning-mounted lights, and display window lights.
- Minimizing impacts of lighting on adjoining activities and uses should be considered in the design of lighting.

OUR APPROACH:

Periodic wall sconces on the building exterior provide indirect ambient lighting to help orientation and way finding at night.

Indirect down lighting is provided at the covered canopies to create a well lit and safe environment at public entrances.

Downward directional landscape path lighting is provided in the resident courtyard to allow safe maneuvering at night.

Limited landscape spot lighting will also assist in way finding and enhancing the building aesthetics at night.

10. PEDESTRIAN AMENITIES:



MUNICIPAL OBJECTIVES:

- Provide pedestrian amenities along all sidewalks, interior pathways and with plazas and other open spaces. Desired amenities include:
 - Pedestrian scaled lighting (12-15' height).
 - Seating space - benches, steps, railings and planting ledges (12-20" height acceptable, 16-18" height preferred, 6-24" width).
 - Pedestrian furniture such as trash receptacles, consolidated newspaper racks, bicycle racks and drinking fountains.
 - Planting beds and/or potted plants.
 - Unit paving such as stones, bricks, or tiles.
 - Decorative pavement patterns and tree grates.
 - Water features.
 - Informational kiosks.
 - Transit shelters.
 - Decorative clocks.
 - Artwork.



OUR APPROACH:

Lobby Patio

The project is proposing a large covered patio space which will have manufactured planters, and benches for residents and visitors. The patio sits above the public sidewalk and is separated by some raised landscape planting beds. The raised planters adjacent to the sidewalk can be utilized for informal pedestrian seating. The raised planters and retaining walls are proposed to be a 'board-form' finish to provide texture and interest.

The plantings will provide interesting forms, textures and colors.

The canopy will provide shelter and relief for residents and visitors, and create a space that can be utilized in all-seasons.

The lobby patio will be lighted from above with down lights within the canopy.

Resident Courtyard

The project is proposing a large resident courtyard which will have several areas for different types of seating. The courtyard area immediately outside the resident amenity will include decorative paving, ornamental plantings, and table and bench seat. The south portion of the resident courtyard area will include a shelter for shading, decorative paving, ornamental plantings and bench seating for group gathering. The two areas are connected by an informal gathering space with a flagstone style paving material landscape boulders and individual bench seating.

The resident courtyard shall be secured from the public sidewalk by horizontal board fencing which will provide security, while allowing transparency and interest.

The resident courtyard will be lighted for safety and aesthetics. The pedestrian surface shall be lighted from above in the gathering spaces and smaller scale pedestrian pathway lights will define edges. Trees within the courtyard will be uplighted for dramatic effect.

Wetland Overlook

The project is proposing an small gathering area at the southeast corner of the building which can be utilized to observe the improved wetland buffer and should be an excellent location for observing local songbirds.

The overlook space will have a single bench for individual seating.

The wetland overlook area will be lighted from above for safety and security.

11. INTERIOR PEDESTRIAN CONNECTIONS:



MUNICIPAL GUIDELINES:

- Provide convenient pedestrian access between the street, bus stops, buildings, parking areas, and open spaces.
- Design all buildings abutting a public sidewalk or major internal pathways to provide direct pedestrian access to the sidewalk or pathway.
- Provide interior pedestrian connections to adjacent properties containing similar uses or complementary uses.
- Provide paved walkways through large parking lots.

OUR APPROACH:

At all grade levels of the building have direct access to the sidewalk and public right of way.

Public areas on level 1 and 2 have clear and direct access to the sidewalk.

The site is bounded by public row on 3 sides and a wetland to the south, and has no opportunities to provide connectivity to adjacent properties.

We have included internal pathways to promote use of the courtyard and a viewpoint overlooking the wetlands to the south of the building.

The vast majority of the parking is internal. A single sidewalk abuts the exterior stalls leading to the parking garage and public sidewalk

12. PEDESTRIAN PLAZAS:

MUNICIPAL OBJECTIVES:

- To provide a variety of pedestrian-oriented areas to attract shoppers to commercial areas and enrich the pedestrian environment.
- To create gathering spaces for the community.
- To configure buildings and uses to encourage pedestrian activity and pedestrian focal points.
- To use trees appropriate to the urban environment of the Totem Lake Business District.

MUNICIPAL GUIDELINES:

- Provide pedestrian plaza in conjunction with mixed-use development and non-residential uses.
- Publicly accessible space at the primary frontage and between buildings will extend the public realm while creating a transition between public and private spaces, and attract public use by being well-designed, interesting spaces that are integrated with the street environment. The space should be of sufficient size to allow for a variety of features, including pedestrian/multi-use paths, plaza, seating, public art and water features.
- Position plazas in visible locations on major streets, major internal circulation routes, close to bus stops, or where there are strong pedestrian flows on neighboring sidewalks. For large sites development should be configured to create a focal plaza or plazas. Plazas should be no more than 3-feet above or below the adjacent sidewalk or internal pathway to enhance visibility and accessibility.
- Incorporate plenty of benches, steps, and ledges for seating. A combination of permanent and movable seating is encouraged. Seating areas should be provided with views of amenities, landscaping elements, or people watching.
- Provide storefronts, street vendors, or other pedestrian-oriented uses, to the extent possible, around the perimeter of the plaza.
- Provide landscaping elements that add color and seasonal interest. This can include trees, planting beds, potted plants, trellises, and hanging plants.
- Incorporate pedestrian amenities, as described in Section 10.
- Consider the solar orientation and the wind patterns in the design of the open space and choice of landscaping.
- Provide transitional zones along the building edges to allow for outdoor eating areas and a planted buffer.



OUR APPROACH:

The project is proposing a west facing patio that is associated with the building lobby. The lobby patio will be warmed by the afternoon sun, creating a space for the residents in the afternoon. The lobby patio is located at the upper end of the site overlooking the public sidewalk. The space includes: fixed wood topped benches, raised planters, and bicycle racks. The raised planters will have interesting colorful plantings. The lobby patio will be separated from the public sidewalk by cast-in-place concrete planters. The raised planters are designed to encourage informal seating along the sidewalk. The building overhangs the lobby patio will provide shade and relief for people who linger on the benches in the space.

13. RESIDENTIAL OPEN SPACE:



MUNICIPAL OBJECTIVES:

- To create usable space that is suitable for leisure or recreational activities for residents.
- To create open space that contributes to the residential setting.

MUNICIPAL GUIDELINES:

- Incorporate common open space into multi-family residential uses. In the Totem Lake Business District, where:
 - Very high density residential uses are allowed, the quality of the space in providing respite from the buildings on the site is more critical than the amount of space provided.
 - In some developments, multiple smaller spaces may be more useful than one, larger space.
- Special recommendations for common open space:
 - Consider open space as a focal point of the residential development.
 - Where possible, open space should be large enough to provide functional leisure or recreational activity. For example, long narrow spaces rarely, if ever, can function as usable common space.
 - Open space should provide for a range of activities and age groups. Children's play areas in particular should be visible from dwelling units and positioned near pedestrian activity.
- Residential units adjacent to the open space should have individual entrances to the space. Preferably, these units should include a small area of semi-private open space enclosed by low level landscaping or hedges (no taller than 42").
- Open space should feature paths, seating, lighting, and other pedestrian amenities to make the area more functional and enjoyable. It should be oriented to receive sunlight, (preferably south).
- Separate common space from ground floor windows, streets, service areas, and parking lots with landscaping and/or low-level fencing. However, care should be used to maintain visibility from dwelling units towards open space for safety.



OUR RESPONSE:

The project is proposing several spaces which the resident can utilize.

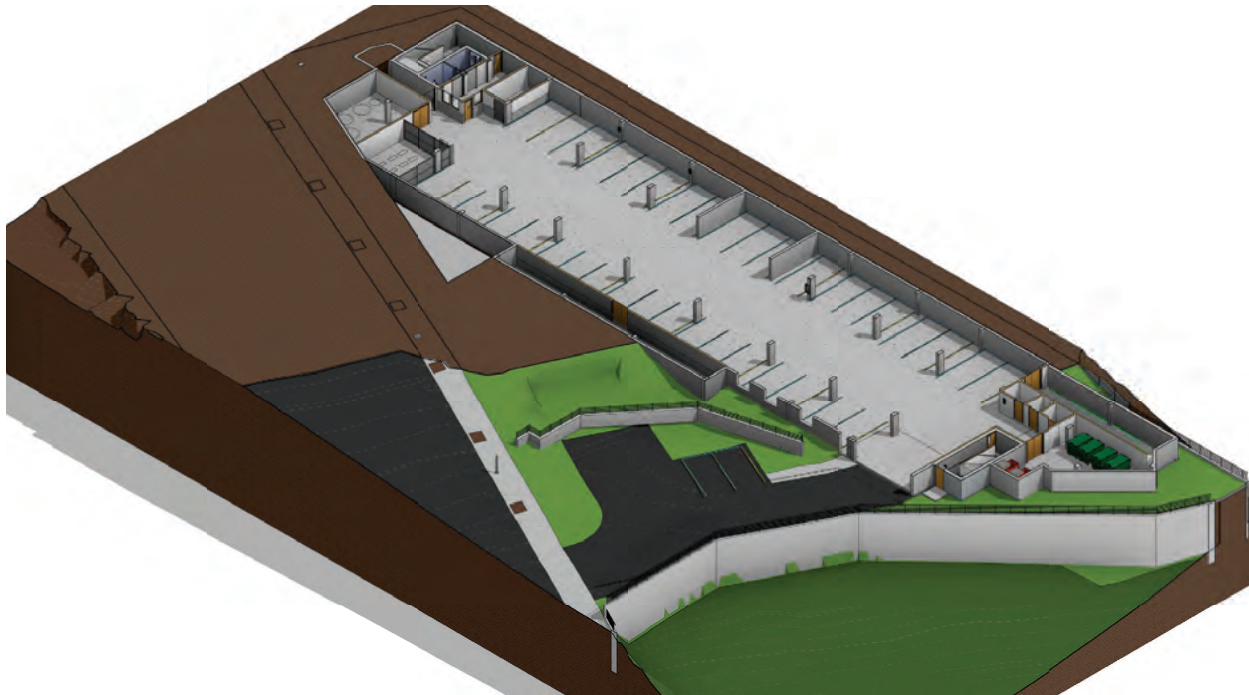
The project is proposing a west facing resident courtyard that is associated with the interior community space. The resident courtyard is separated from the public sidewalk by a horizontal board fence and gate and planting areas. The courtyard shall include: fixed wood topped benches, tables and chairs, manufactured planters, decorative paving materials, and interesting plantings with many textures and colors. The plantings are designed to provide interesting textures, colors and seasonal interest. The plantings are also intended to provide privacy for the adjacent residential units. Trees are located to provide shade and be visible from interior spaces. Tree selection was considered to provide spring flowers, interesting complimentary forms, textures and fall color. The space shall have the trees uplighted for interest in the evenings. The space will be lighted for safety, while being considerate of light spillage into adjacent residential units.

The lobby patio will allow residents to step out the main entrance into a partially covered space that will have fixed wood topped benches, decorative paving, raised planters and interesting plantings.

The public plaza will be easily accessible for the residents by the accessible ramps and the sidewalk. The public plaza will include fixed wood topped benches, decorative paving and interesting plantings.

Residents will also be able to enjoy the wetland overlook. The wetland overlook space can be accessed by the residents either through the building garage or by the accessible ramps which connect the resident courtyard and public plaza.

14. PARKING LOTS AND VEHICULAR CIRCULATION:



MUNICIPAL GUIDELINES:

- Minimize curb cuts into the development
- Break up large parking lots into smaller one
- Take advantage of topography to hide parking under buildings
- Incorporate landscaping into parking lots
- Develop vehicular access system that minimizes conflict with pedestrians and vehicle
- Avoid parking lots that dominate a development and locate parking on side or rear

OUR APPROACH:

We have only one curb cut on our site. It has been located away from the intersection to enhance safety.

The majority of our parking is in a garage, which takes advantage of the topography to minimize visual impact and bring most parking below grade.

Two convenience stalls are located towards the front of the site. These are screened by the landscaping and topography.

The basement level parking location and use of a garage help separate the vehicular access from the pedestrian access and amenity areas at levels 1 and 2.

15. PARKING GARAGES:



OUR APPROACH:

The parking garage is partially below grade, and only exposed at the south end of the building.

The visible portion of the garage is set back from the street and well screened with landscaping and avoids detracting from the more public entrance and areas of the building.

We have carried the same materials and architectural language to the parking level to work with the whole building form and design.

MUNICIPAL GUIDELINES:

- Mitigate the intrusive qualities of parking garages. Along streets, pedestrian pathways and in pedestrian areas
- Design and site parking garage entries to complement, not subordinate the pedestrian entry. If possible, locate the parking entry away from the primary street, to either the side or rear of the building.
- Use similar architectural forms, materials, and/or details to integrate the garage with the development.

16. ARCHITECTURAL STYLE:



OUR APPROACH:

Given the lack of a predominant historical style of architecture in the totem lake area, we've used recent adjacent development as a starting point.

Similar to other development in the area, we've used color and material transitions to enhance the modulation and help break up the long facades.

We've adopted cornice, parapet, and projection elements similar to adjacent developments to provide a secondary level of detail and visual relief.

Material selections include a mix of hardie panel and plank siding, with the use of decorative niches panels to emphasize more public areas and key modulation in the building.

MUNICIPAL GUIDELINES:

- Provide an architectural scale of development that is compatible with totem lake district
- Add visual interests to the building
- Incorporate fenestration techniques that indicate the scale of the building (size, location, number of windows, proportions, patterns, etc.)
- Encourage vertical modulation on multi-story building, horizontal modulation, roof line modulation
- Improve the architectural design of commercial buildings in the business district
- Provide architecture that fits into the context of the adjacent uses surrounding the business district

17. ARCHITECTURAL SCALE:



MUNICIPAL GUIDELINES:

- Incorporate fenestration techniques that indicate the scale of the building
- Encourage vertical modulation on multi-story buildings
- Encourage variety of horizontal building modulation
- Encourage a variety of roofline modulation techniques

OUR APPROACH:

The design addresses both vertical and horizontal modulation most directly through a series of pop outs and recesses to physically break the building up.

At a secondary level, modulation is emphasized with a variety of material and color transitions, and secondary architectural elements including belly bands, as well physical changes to the building facade.

Several different approaches at the roof line work with the materials and physical modulation to support both as well as create variety at the roof line.

18. HUMAN SCALE:



MUNICIPAL GUIDELINES:

- Add visual interests to buildings
- Encourage a combination of architectural building elements that lend the building a human scale

OUR APPROACH:

Significant modulation has been created to break the building into smaller components visually and bring it to a more individual scale.

A secondary level of detail including the use of window shade canopies, belly bands, material and color transitions help to further bring the building to a more human scale.

Finally, the entrances and pedestrian areas use a higher level of finish materials and rich landscaping to create a more immediate welcoming and comfortable environment.

19. BUILDING MATERIALS:



MUNICIPAL GUIDELINES:

- Encourage the integration of ornament and applied art with the structures and the site environment.
- Use a variety of quality building materials such as brick, stone, timber, and metal, to add visual interest to the Buildings and reduce their perceived scale.

OUR APPROACH:

The design relies upon material selection, color, and transitions to create visual interest.

Higher quality materials such as nichiha siding and store front windows are used at more public areas.

Subtle projections and recesses along with the use of belly bands, siding transitions and color changes and other architectural elements avoid blank walls, and bulky masses and support visual interest.

MATERIALS:

- A - "NEWBURYPORT BLUE" LAP SIDING
- B - NICHIIHA CORBOSA "MOONDUST"
- C - "GRAY CLOUD" HARDIE PANEL
- D - "STARDUST" HARDIE PANEL
- E - "SILVER FOX" HARDIE PANEL
- F - "SUMMER WHEAT" LAP SIDING
- G - "CHAR BROWN" HARDIE PANEL

20. SIGNS:



MUNICIPAL GUIDELINES:

- Provide pedestrian oriented signs on all commercial facades where adjacent to a sidewalk or walkway.
- Includes signs located within 15' of the ground plane, such as "blade" signs which hang below canopies
- Small signs located on canopies or awnings are also effective along building facades at the street.
- External lighting is preferred.
- Ground-mounted signs should feature a substantial base and be integrated with the landscaping and other site features.
- Mounting supports should reflect the materials and design character of the building or site elements or both.

OUR APPROACH:

Given the residential nature of the building, signage is limited to way finding and building identification.

Wall mounted signage is located on the retaining wall adjacent to the stairs to the main pedestrian image.

A second monument sign identifying the building is located at the vehicular entrance.

21. SERVICE AREAS:



MUNICIPAL GUIDELINES:

- Provide service areas without impacting the development
- Design site services and storage areas to promote ease of use, safety, and visual cohesion

OUR APPROACH:

Garbage is collected and stored at the garage level and is only brought out for pickup.

All mechanical equipment and storage are within the building footprint to avoid visual impacts.

Garbage pickup and stalls for short term drop off and loading are set back from the street and recessed into the slope to reduce visual impact.

Additional landscaping helps screen the views of the garbage pickup location.

22. VISUAL QUALITY OF LANDSCAPING:



MUNICIPAL OBJECTIVES:

The Building Landscape
Landscaping around urban buildings particularly buildings with blank walls, can reduce scale and add diversity through pattern, color, and form. Examples of how landscaping is used to soften and enhance the visual quality of the urban environments include:

- Dense screening of parking lots.
- Tall cylindrical trees to mark an entry.
- Continuous street tree plantings to protect pedestrians.
- Several clusters of dense trees along long building facades.
- Cluster plantings at focal points.
- Parking with trees and shrubs planted internally as well as on the perimeter.

MUNICIPAL GUIDELINES:

- a. Consider the purpose and context of the proposed landscaping.
- b. The pedestrian/auto oriented landscape requires strong plantings of a structural nature to act as buffers or screens.
- c. The pedestrian landscape should emphasize the subtle characteristics of the plant materials.
- d. The building landscape should use landscaping that compliments the building's favorable qualities and screens it faults while not blocking views of the business or signage.

Other Municipal Considerations:

- e. Encourage a colorful mix of drought tolerant and low maintenance trees, shrubs, and perennials. Except in special circumstances, ivy and grass should be avoided.
- f. Take advantage of on-site topography to hide and enhance views.
- g. Use wood slopes as a natural site amenity and to screen unwanted views, where applicable.

OUR RESPONSE:

The project is proposing landscaping that compliments the architecture and existing site. In the enhanced wetland buffer a mixture of native trees and shrubs will supplement existing retained native plants. Douglas-fir, Western Red Cedar and Oregon Ash will provide scale and screening to the south building elevation.

Mass plantings groups are proposed for southern portion of the improved site. Groupings of hardy Dwarf White Pine and Viburnum Davidii will help screen the parking area from the public sidewalk. Serviceberry Trees will provide shade and relief and scale to the building. Between the building and the parking area Mexican Orange will provide relief for the retaining and blank walls.

Within the public plaza and the resident courtyard plant materials were selected to provide greater texture and color. Korean Dogwood and Serviceberry will provide garden scaled canopies with four season interest. Red Daylily, Black-eye Susan and Western Sword Ferns will provide complimentary colors and textures.

Adjacent to lobby patio plant materials were again selected to provide greater texture and color. Karl Foerster Grass, and Artemisia were selected to provide textural and color interest.

Along the public sidewalk plantings were proposed which would be easily readable from a vehicle. Plantings are strongly ordered, colored and distinctly textured. The Karl Foerster Grass has a distinctive upright form, the Dwarf White Pine and Artemisia have dramatic colors, texture, and forms.

23. TERRITORIAL VIEWS:



MUNICIPAL GUIDELINES:

- Encourage roof lines to slightly follow the slope of the terrain
- Parking garages should be terraced into slope

OUR APPROACH:

The building does not disrupt any territorial views. And is consistent with the forms and architectural along the 405 corridor.

The parking garage sinks into the slope of the site masking the majority of it from view.



CITY OF KIRKLAND
Planning and Building Department
123 5th Avenue, Kirkland, WA 98033
425.587.3600 ~ www.kirklandwa.gov

DEVELOPMENT STANDARDS LIST

ARDEA AT TOTEM LAKE, DRV22-00498

ZONING CODE STANDARDS

95.51.2.a Required Landscaping. All required landscaping shall be maintained throughout the life of the development. The applicant shall submit an agreement to the city to be recorded with King County which will perpetually maintain required landscaping. Prior to issuance of a certificate of occupancy, the proponent shall provide a final as-built landscape plan and an agreement to maintain and replace all landscaping that is required by the City.

95.52 Prohibited Vegetation. Plants listed as prohibited in the Kirkland Plant List shall not be planted in the City.

100.25 Sign Permits. Separate sign permit(s) are required. In JBD and CBD cabinet signs are prohibited.

105.32 Bicycle Parking. All uses, except single family dwelling units and duplex structures with 6 or more vehicle parking spaces must provide covered bicycle parking within 50 feet of an entrance to the building at a ratio of one bicycle space for each twelve motor vehicle parking spaces. Check with Planner to determine the number of bike racks required and location.

105.18 Entrance Walkways. All uses, except single family dwellings and duplex structures, must provide pedestrian walkways between the principal entrances to all businesses, uses, and/or buildings on the subject property.

105.18 Overhead Weather Protection. All uses, except single family dwellings, multifamily, and industrial uses, must provide overhead weather protection along any portion of the building, which is adjacent to a pedestrian walkway.

105.18.2 Walkway Standards. Pedestrian walkways must be at least 5' wide; must be distinguishable from traffic lanes by pavement texture or elevation; must have adequate lighting for security and safety. Lights must be non-glare and mounted no more than 20' above the ground.

105.18.2 Overhead Weather Protection Standards. Overhead weather protection must be provided along any portion of the building adjacent to a pedestrian walkway or sidewalk; over the primary exterior entrance to all buildings. May be composed of awnings, marquees, canopies or building overhangs; must cover at least 5' of the width of the adjacent walkway; and must be at least 8 feet above the ground immediately below it. In design districts, translucent awnings may not be backlit; see section for the percent of property frontage or building facade.

105.19 Public Pedestrian Walkways. The height of solid (blocking visibility) fences along pedestrian pathways that are not directly adjacent a public or private street right-of-way shall be limited to 42 inches unless otherwise approved by the Planning or Public Works Directors. All new building structures shall be setback a minimum of five feet from any pedestrian access right-of-way, tract, or easement that is not directly adjacent a public or private street right-of-way. If in a design district, see section and Plate 34 for through block pathways standards.

105.58 Parking Lot Locations in Design Districts. See section for standards unique to each district.

105.65 Compact Parking Stalls. Up to 50% of the number of parking spaces may be

designated for compact cars.

105.60.2 Parking Area Driveways. Driveways which are not driving aisles within a parking area shall be a minimum width of 20 feet.

105.60.3 Wheelstops. Parking areas must be constructed so that car wheels are kept at least 2' from pedestrian and landscape areas.

105.60.4 Parking Lot Walkways. All parking lots which contain more than 25 stalls must include pedestrian walkways through the parking lot to the main building entrance or a central location. Lots with more than 25,000 sq. ft. of paved area must provide pedestrian routes for every 3 aisles to the main entrance.

105.77 Parking Area Curbing. All parking areas and driveways, for uses other than detached dwelling units must be surrounded by a 6" high vertical concrete curb.

105.96 Drive Through Facilities. See section for design criteria for approving drive through facilities.

110.52 Sidewalks and Public Improvements in Design Districts. See section, Plate 34 and public works approved plans manual for sidewalk standards and decorative lighting design applicable to design districts.

110.60.5 Street Trees. All trees planted in the right-of-way must be approved as to species by the City. All trees must be two inches in diameter at the time of planting as measured using the standards of the American Association of Nurserymen with a canopy that starts at least six feet above finished grade and does not obstruct any adjoining sidewalks or driving lanes.

115.25 Work Hours. It is a violation of this Code to engage in any development activity or to operate any heavy equipment before 7:00 am. or after 8:00 pm Monday through Friday, or before 9:00 am or after 6:00 pm Saturday. No development activity or use of heavy equipment may occur on Sundays or on the following holidays: New Year's Day, Memorial Day, Independence Day, Labor Day, Thanksgiving, and Christmas Day. The applicant will be required to comply with these regulations and any violation of this section will result in enforcement action, unless written permission is obtained from the Planning official.

115.45 Garbage and Recycling Placement and Screening. For uses other than detached dwelling units, duplexes, moorage facilities, parks, and construction sites, all garbage receptacles and dumpsters must be setback from property lines, located outside landscape buffers, and screened from view from the street, adjacent properties and pedestrian walkways or parks by a solid sight-obscuring enclosure.

115.47 Service Bay Locations. All uses, except single family dwellings and multifamily structures, must locate service bays away from pedestrian areas. If not feasible must screen from view.

115.75.2 Fill Material. All materials used as fill must be non-dissolving and non-decomposing. Fill material must not contain organic or inorganic material that would be detrimental to the water quality, or existing habitat, or create any other significant adverse impacts to the environment.

115.95 Noise Standards. The City of Kirkland adopts by reference the Maximum Environmental Noise Levels established pursuant to the Noise Control Act of 1974, RCW 70.107. See Chapter 173-60 WAC. Any noise, which injures, endangers the comfort, repose, health or safety of persons, or in any way renders persons insecure in life, or in the use of property is a violation of this Code.

115.115 Required Setback Yards. This section establishes what structures, improvements and activities may be within required setback yards as established for each use in each zone.

115.115.3.g Rockeries and Retaining Walls. Rockeries and retaining walls are limited to a maximum height of four feet in a required yard unless certain modification criteria in this section are met. The combined height of fences and retaining walls within five feet of each other in a required yard is limited to a maximum height of 6 feet, unless certain modification criteria in this section are met.

115.120 Rooftop Appurtenance Screening. New or replacement appurtenances on existing

buildings shall be surrounded by a solid screening enclosure equal in height to the appurtenance. New construction shall screen rooftop appurtenances by incorporating them in to the roof form.

Prior to issuance of a grading or building permit:

27.06.030 Park Impact Fees. New residential units are required to pay park impact fees prior to issuance of a building permit. Please see KMC 27.06 for the current rate. Exemptions and/or credits may apply pursuant to KMC 27.06.050 and KMC 27.06.060. If a property contains an existing unit to be removed, a "credit" for that unit shall apply to the first building permit of the subdivision.

Prior to occupancy:

95.51.2.a Required Landscaping. All required landscaping shall be maintained throughout the life of the development. The applicant shall submit an agreement to the city to be recorded with King County which will perpetually maintain required landscaping. Prior to issuance of a certificate of occupancy, the proponent shall provide a final as-built landscape plan and an agreement to maintain and replace all landscaping that is required by the City

110.60.5 Landscape Maintenance Agreement. The owner of the subject property shall sign a landscape maintenance agreement, in a form acceptable to the City Attorney, to run with the subject property to maintain landscaping within the landscape strip and landscape island portions of the right-of-way. It is a violation to pave or cover the landscape strip with impervious material or to park motor vehicles on this strip.



DEVELOPMENT STANDARDS

DRV22-00498

PUBLIC WORKS DEPARTMENT

PUBLIC WORKS CONDITIONS

Permit #: DRV22-00498

Project Name: Ardea at Totem Lake

Project Address: 12700 116th Avenue NE

Date: August 3, 2022

Public Works Staff Contacts

Ryan Schauble, Senior Development Engineer

Phone: 425-587-3842 / E-mail: rschauble@kirklandwa.gov

General Conditions:

1. All public improvements associated with this project including street and utility improvements, must meet the City of Kirkland Public Works Pre-Approved Plans and Policies Manual. A Public Works Pre-Approved Plans and Policies manual can be purchased from the Public Works Department, or it may be retrieved from the Public Works Department's page at the City of Kirkland's web site.
2. Permit Fees, Connection Charges and Impact Fees. Click on the hyperlinks to view the latest fee schedules. Contact Public Works staff if there are general questions about fees. Fees for a specific permit will not be determined until plan review is complete. The applicant is responsible for completing and submitting the Public Works Improvement Evaluation Packet (available in either Excel or PDF) before fees and securities are determined. If the project site is located in a water/sewer service area other than City of Kirkland (i.e., Northshore Utility District or Woodinville Water District), then those utility connection charges will be collected by the respective utility district.
3. All street and utility improvements for Short Plats and Subdivisions shall be permitted through a Land Surface Modification (LSM) Permit. Click on the hyperlink to view the LSM Checklist. Street and utility improvements covered under a building permit will not require a separate LSM permit, unless specified otherwise by staff.
4. Performance and Maintenance Securities:
 - If a recording Performance Security has not yet been posted, then prior to issuance of the LSM Permit a standard right of way restoration security ranging from \$20,000.00 to 50,000.00 (value determined based on amount of ROW disruption) shall be posted with Public Works Department. This security will be held until the project has been completed.
 - Prior to Final Inspection of the Land Surface Modification improvements, there will be a condition of the permit to establish a two year Maintenance security.
5. Subdivision and Short Plat recording notes and provisions. Refer to the attachment for a list of notes and provisions required by Public Works to be recorded with each subdivision or short plat recording. Contact the Development Engineer for more details.
6. Traffic Concurrency Review:

Prior to submittal of a Building or Zoning Permit, the applicant must apply for a Concurrency Test Notice. Contact Thang Nguyen, Transportation Engineer, at 425-587-3869 for more information. A separate Concurrency Permit will be created. After concurrency has passed, the project will receive a concurrency test notice that allows the applicant to proceed with all development permits. A "Certificate of Concurrency" is established with a development or building permit. It will read as follows: CERTIFICATE OF CONCURRENCY: This project has been reviewed and approved for water, sewer, and traffic concurrency. Any water and sewer mitigating conditions are listed within the conditions below. Any traffic mitigating conditions will be found in an attached memorandum from the Public Works Traffic Engineering Analyst to the Planning

Department Project Planner. Upon issuance of this permit, this project shall have a valid Certificate of Concurrency and concurrency vesting until the permit expires. This condition shall constitute issuance of a Certificate of Concurrency pursuant to chapter 25.12 of the Kirkland Municipal Code.

7. All civil engineering plans which are submitted in conjunction with a building, grading, or right-of-way permit must conform to the Public Works Policy G-7, Engineering Plan Requirements. This policy is contained in the Public Works Pre-Approved Plans and Policies Manual. All street improvements and underground utility improvements (storm, sewer, and water) must be designed by a Washington State Licensed Engineer. All drawings shall bear the licensed engineer's stamp. All plans submitted in conjunction with a building, grading or right-of-way permit must have elevations which are based on the King County datum only (NAVD 88).

8. Prior to issuance of any commercial or multifamily Building Permit, the applicant shall provide an analysis and plan for garbage, recycling and composting storage and pickup. Refer to Policy G-9 in the Public Works Pre-Approved Plans as a guide. The plan must be approved by Waste Management and by Public Works. The applicant shall submit a letter report to summarize the analysis and the plan, supported by such details as container sizing calculations, storage area sizing calculations, and truck access and turnaround details.

9. The required tree plan shall include any significant tree in the public right-of-way along the property frontage.

Water and Sanitary Sewer Conditions:

1. Northshore Utility District (NUD) approval required for sanitary sewer and water service. A letter of sewer/water availability is required. Contact NUD at 425-398-4400.
2. See Fire Department conditions for fire flow requirements.

Surface Water Conditions:

1. Provide temporary and permanent storm water control in accordance with the 2021 King County Surface Water Design Manual (KCSWDM) and the City of Kirkland Addendum (Policy D-10). Refer to Policies D-2 and D-3 in the Public Works Pre-Approved Plans and Policies Manual for design guidance, or contact Kirkland Surface Water staff at (425) 587-3800 for assistance. Based on the pre-submittal information provided by the applicant, this project should expect a Full Drainage Review. The drainage review level and requirements may change based on the actual development proposal at the time of permit application.
2. Calculating impervious surface area based on lot coverage: This is important for flow control analysis (hydrologic modeling purposes). In accordance with KZC Chapter 115.90 – Calculating Lot Coverage. The regulation update allowed conventional (sand set) pavers to be counted as a "Partially Exempt Material", allowed to received 50 percent exemption for zoning lot coverage for the area they cover, and up to 10 percent of the total lot size. Conventional pavers do not have to meet surface water mitigation specifications (e.g. not designed as LID BMP pervious pavers per Public Works Pre-Approved Plan CK-L-09). As a result, lots are allowed 10 percent more runoff generating surface area, and thus have to provide flow control accordingly. Furthermore, impervious surface areas shall also include frontage and street improvements – streets, sidewalks, trails, etcetera and shall be taken from the layouts of the proposed plans. Building footprint and driveways or building coverage shall be as follows:
 - For commercial or multi-family development, the impervious coverage shall either:
 - a Assume the maximum impervious coverage permitted by the KZC plus an additional 10% OR
 - b Estimate impervious coverage from layouts of the proposal. If estimated from the layouts of the proposal, the impervious coverage shall include calculations of all impervious surfaces, including eaves. This option may require a Reduced Impervious Surface Limit to be recorded on the property.
3. A drainage report (Technical Information Report or TIR) must be submitted with the Land Use application or permit application. A downstream analysis is required for all projects (except for Basic Drainage Review).

4. This project is in a Level 2 Flow Control Area and is required to comply with core drainage requirements in the KCSWDM. Historic (forested) conditions shall be used as the pre-developed modeling condition for design of the stormwater detention system.
 5. The project may qualify for an exception to detention if the target surfaces will generate no more than a 0.15 cfs increase in the historic (forested) conditions 100-year peak flow. The 15-minute time step must be used to perform the flow control analysis. Do not use the 1-hour time step. Approved hydrologic modeling programs are MGS Flood and WWHM (latest version of the software).
 6. Evaluate the feasibility and applicability of dispersion, infiltration, and other stormwater Low Impact Development (LID) Best Management Practices (BMPs) per the KCSWDM. If feasible, stormwater LID BMPs are required to the maximum extent feasible. If LID BMPs are infeasible, pervious pavement cannot be used to reduce overall impervious lot coverage. The Private Maintenance Agreement will be recorded on all projects that construct a stormwater LID BMP or facility, per Policy D-7.
 7. Soil information may be necessary for designing LID BMPs per the KCSWDM, and there are other reasons a soil report is necessary for a project (e.g., steep slopes, sensitive areas, etc.). Refer to Policy D-8 for details.
 8. Special inspections may be required for LID BMPs on this project. Provide documentation of inspections by a licensed geotechnical professional that the BMP will function as designed.
 9. If the project will create or replace more than 5,000 square feet of pollution generating impervious surface (PGIS), provide water quality treatment in accordance with the KCSWDM. The enhanced treatment level is required for multi-family residential, commercial, industrial projects, and single family residential projects with eight or more dwelling units per acre density.
 10. Soil Amendment per Pre-Approved Plan E.12 is required for all landscaped areas.
 11. Provide a separate storm drain connection to each lot for conveyance. All roof and driveway drainage must be tight-lined to the storm drain system or utilize low impact development techniques on-site.
 12. Provide collection and conveyance of right-of-way storm drainage to accommodate the new frontage improvements as needed. Provide a plan and profile design for the storm sewer system. Size and material of construction shall be in accordance with the City Kirkland Pre-Approved Plans and Notes. Refer to Policy D-5 for details.
 13. Maintenance access road(s) per Pre-Approved Plan CK-D.37 shall be provided to the control structure and other drainage structures associated with the detention facility (e.g., inlet, access openings, emergency overflow or bypass structures). Vehicular access to the drainage structures associated with the detention facility shall not be obstructed on a temporary or permanent basis without prior approval from the City. Location of parking stalls shall not obstruct maintenance access. Where maintenance access is required on a driveway that shares access to residential units, a min. 16' wide drive line is required to accommodate two-way traffic.
 14. If working within an existing ditch, the applicant is hereby given notice that the Army Corps of Engineers (COE) has asserted jurisdiction over upland ditches draining to streams. Either an existing Nationwide COE permit or an Individual COE permit may be necessary for work within ditches, depending on the project activities. Applicants should obtain the applicable COE permit; information about COE permits can be found at: U.S. Army Corps of Engineers, Seattle District Regulatory Branch
<http://www.usace.army.mil/Missions/CivilWorks/RegulatoryProgramandPermits.aspx>
- Specific questions can be directed to: Seattle District, Corps of Engineers, Regulatory Branch, CENWS-OD-RG, Post Office Box 3755, Seattle, WA 98124-3755, Phone: (206) 764-3495
15. A Hydraulic Project Approval (HPA) from WA State Department of Fish and Wildlife (WDFW) may be required for this project. Contact Stewart Reinbold at WDFW at 425-313-5660 or stewart.reinbold@dfw.wa.gov for determination, obtain an HPA if required, and submit a copy to COK. If an HPA is not required, the applicant will be required to provide written documentation from WDFW as verification. More information on HPAs can be found at the following website:
<http://wdfw.wa.gov/licensing/hpa/>
 16. Construction Stormwater Pollution Prevention Plan (CSWPPP):

- All proposed projects that will conduct construction activities onsite, or offsite must provide stormwater pollution prevention and spill controls to prevent, reduce, or eliminate the discharge of pollutants (including sediment) to onsite or adjacent stormwater systems or watercourses.
- Refer to Core Requirement No. 5 in the KCSWDM and Policy D-12.
- Provide an erosion control report and plan with the Building or Land Surface Modification Permit application. The plan shall be in accordance with the KCSWDM.
- Construction drainage control shall be maintained by the developer and will be subject to periodic inspections. During the period from May 1 and September 30, all denuded soils must be covered within 7 days; between October 1 and April 30, all denuded soils must be covered within 12 hours. Additional erosion control measures may be required based on site and weather conditions. Exposed soils shall be stabilized at the end of the workday prior to a weekend, holiday, or predicted rain event.

17. If the total disturbed land area is one acre or greater, the following conditions apply:

- The applicant is responsible to apply for a Construction Stormwater General Permit from Washington State Department of Ecology. Provide the City with a copy of the Notice of Intent for the permit. Permit Information can be found at the following website: <http://www.ecy.wa.gov/programs/wq/stormwater/construction/>
 - o Among other requirements, this permit requires the applicant to prepare a Storm Water Pollution Prevention Plan (SWPPP) and identify a Certified Erosion and Sediment Control Lead (CESCL) prior to the start of construction. The CESCL shall attend the City of Kirkland PW Dept. pre-construction meeting with a completed SWPPP.
- Turbidity monitoring by the developer/contractor is required for any surface water leaving the site.
- A Stormwater Pollution Prevention and Spill (SWPPS) Plan must be kept on site during all phases of construction and shall address construction-related pollution generating activities. Follow the guidelines in the Ecology Pollution Prevention Manual for plan preparation.

Street and Pedestrian Improvement Conditions:

1. The subject property abuts 116th Avenue NE, a minor arterial street. Zoning Code sections 110.10 and 110.25 require the applicant to make half-street improvements in rights-of-way abutting the subject property. Section 110.30-110.50 establishes that this street must be improved with the following:
 - A. KZC 110.52 specifies this section of 116th Avenue NE to be a major pedestrian sidewalk.
 - B. Dedicate sufficient right-of-way (ROW) abutting the property to install half-street improvements.
 - C. Remove existing curb and replace with new Type A curb set 8 ft from the existing bike lane stripe; allows for two travel lanes and a 8 ft buffered bike lane. The curb can transition at the culvert crossing to maintain the existing 5 ft bike lane and road width. The bike lane at the intersection with 128th may be reduced to 5 ft and also have a transition; please have your Civil work with Staff prior to design – need survey to understand impacts.
 - D. Install an 8 ft sidewalk with street trees in 4ft x 6 ft tree wells 30 ft on center and pedestrian lighting 60 ft on-center (11 total).
2. Access Requirements (KZC Chapter 105.10):
 - A. The unobstructed paved access shall be 24 feet wide.
 - B. The proposed driveway shall line up with the driveway opposite the street in order to avoid conflicting left hand turn lanes.
 - C. Provide vehicle turnarounds where needed to allow vehicles to drive forward onto 116th Avenue NE.
 - D. The driveway for each lot shall be long enough so that parked cars do not extend into any easement, tract, or right-of-way (20' minimum).
3. Meet the requirements of the Kirkland Driveway Policy R-4.
4. Meet the requirements of the Kirkland Intersection Sight Distance Policy R.13. All street and driveway intersections shall not have any visual obstructions within the sight distance triangle.
5. When three or more utility trench crossings occur within 150 lineal ft. of street length or where utility trenches parallel the street centerline, the street shall be overlaid with new asphalt or the existing asphalt shall be removed and replaced per the City of Kirkland Street Asphalt Overlay Policy R-7.
 - Existing streets with 4-inches or more of existing asphalt shall receive a 2-inch (minimum thickness) asphalt overlay. Grinding of the existing asphalt to blend in the overlay will be required along all match lines.
 - Existing streets with 3-inches or less of existing asphalt shall have the existing asphalt removed and replaced with an

asphalt thickness equal or greater than the existing asphalt provided however that no asphalt shall be less than 2-inches thick and the subgrade shall be compacted to 95% density.

6. It shall be the responsibility of the applicant to relocate any above-ground or below-ground utilities which conflict with the project, associated street, or utility improvements.

7. Underground all new and existing on-site utility lines and overhead transmission lines. Underground any new off-site transmission lines.

8. New LED street lights may be required per Puget Sound Energy (PSE) design and Public Works approval. Contact PSE or a third-party consultant may perform lighting analysis. If new lighting or upgrades are necessary, design plans must be submitted for review prior to issuance of an LSM or building permit. Contact PSE Street Lighting Account Manager: Lyndsey Goldsmith at Lyndsey.Goldsmith@pse.com, 425-396-3838 or 425-395-5225.

9. A striping plan for the street must be submitted with the building or grading permit.