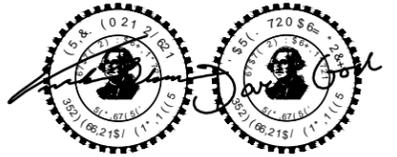


CITY OF  KIRKLAND

JUANITA DRIVE MULTIMODAL, INTERSECTION
AND SAFETY IMPROVEMENTS

JOB NO. 39-23-PW
CIP NO. STC-0089

OCTOBER 2023

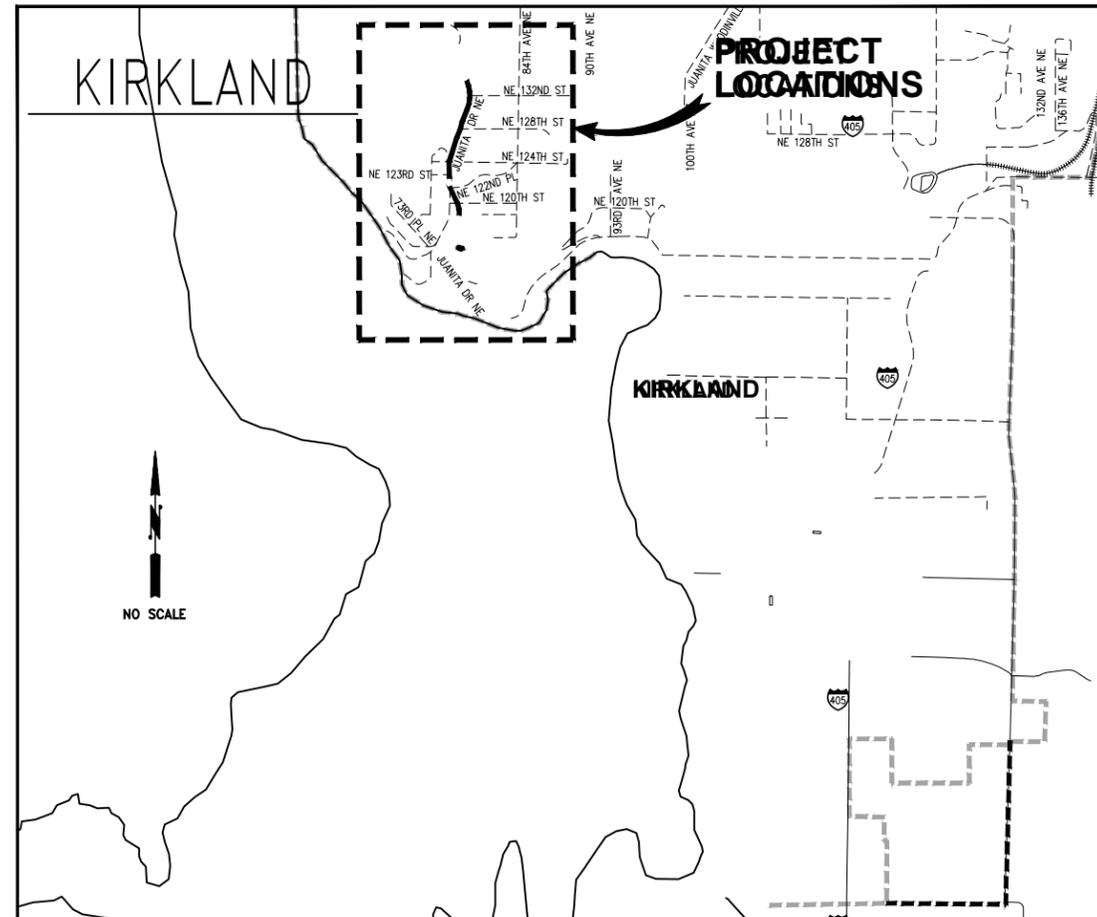


CITY OFFICIALS

PENNY SWEET	MAYOR
JAY ARNOLD	DEPUTY MAYOR
NEAL BLACK	COUNCIL MEMBER
KELLI CURTIS	COUNCIL MEMBER
AMY FALCONE	COUNCIL MEMBER
TOBY NIXON	COUNCIL MEMBER
JON PASCAL	COUNCIL MEMBER
KURT TRIPLETT	CITY MANAGER
JULIE UNDERWOOD	PUBLIC WORKS DIRECTOR
ROD STEITZER, PE	CAPITAL PROJECTS MANAGER

CONTACT PERSONNEL

NAME	AGENCY	PHONE
LAURA DRAKE, PE	COK PROJECT ENGINEER	425.587.3833
GEORGE MATOTE, PE	NUD SENIOR ENGINEER	425.521.3727
STEVE HOOPES	COK FIELD REPRESENTATIVE	425.623.5086
RIK MAYER	COK FIELD REPRESENTATIVE	206.496.4265
KIARA SKYE	PUGET SOUND ENERGY	425.213.9205
CHRIS COMBS	COMCAST	425.273.7832
CHERYL SCHNEIDER	ZIPLY	509.218.1294
EMERGENCY	NORCOM	911
POLICE MAIN LINE	COK	425.587.3400
FIRE MAIN LINE	COK	425.864.3650
SPILL RESPONSE HOTLINE	COK	425.587.3900
ONE CALL UTILITY LOCATE		800.424.5555



KPG
PSOMAS

7DFRPD
-H11HUVRQ \$YHQXH
7DFRPD :\$
6HDWWOH :HQDWFRRH

INDEX OF DRAWINGS

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EXISTING LEGEND

MIC	MONUMENT IN CASE	○	BOULDER
△	PK NAIL	⊥	SIGN
RC	REBAR & CAP	⊥	MAILBOX (# OF BOXES)
□	CATCH BASIN	○	SHRUB
⊕	STORM DRAIN MANHOLE	⊕	STUMP, DIAMETER
^	CULVERT	⊕	DECIDUOUS TREE, DIAMETER (# OF TRUNKS)
○	SEWER MANHOLE	⊕	CONIFEROUS TREE, DIAMETER (# OF TRUNKS)
◇	POWER POLE	---	CONDUIT
◇	POWER POLE W/UNDERGROUND CONNECT	---	PAINTED POWER
◇	POWER POLE WITH LUMINAIRE	---	RECORD POWER
◇	POWER POLE WITH LUMINAIRE AND UG CONNECT	---	PAINTED TELEPHONE
□	POWER VAULT LID	---	RECORD TELEPHONE
△	POWER TRANSFORMER	---	PAINTED FIBER OPTIC
◇	GUY POLE	---	PAINTED TV
→	GUY ANCHOR	---	PAINTED WATER
⊕	TRAFFIC SIGNAL	---	RECORD WATER
⊕	TRAFFIC SIGNAL WITH LUMINAIRE	---	PAINTED GAS
◇	PEDESTRIAN SIGNAL	---	RECORD GAS
⊠	JUNCTION BOX	---	OVERHEAD UTILITY LINE (GENERIC)
⊠	TRAFFIC CONTROL CABINET	---	STORM
○	TRAFFIC ATTENUATOR LOOP	---	SEWER
⊕	LOT LIGHT	---	FENCE
⊕	ROADWAY LUMINAIRE	---	HEDGE (HEIGHT NOTED)
⊕	TELEPHONE RISER	---	BUILDING
⊕	GAS VALVE	---	PAINTED STRIPE
⊕	WATER VALVE	---	PAINTED SKIP STRIPE
⊕	WATER METER	---	EDGE OF PAVEMENT
⊕	WATER MANHOLE	---	FLOWLINE
⊕	FIRE HYDRANT	---	CONTOUR
⊕	IRRIGATION CONTROL VALVE	---	AQUATIC FEATURE UNLESS OTHERWISE NOTED
⊕	UTILITY VAULT LID (GENERIC/UNKNOWN)	---	CONCRETE
⊕	UTILITY RISER (GENERIC/UNKNOWN)	---	GRAVEL
⊕	STAND-PIPE (GENERIC/UNKNOWN)	---	ROCKERY
⊕	UTILITY ACCESS (GENERIC/UNKNOWN)	---	PROPERTY LINE
⊕	BORE SITE (WITH FIELD DESCRIPTION)	---	RIGHT OF WAY
⊕	GUARD POST	---	TEMP CONSTRUCTION EASEMENT (TCE)

PROPOSED LEGEND

-----	REMOVE CURB & GUTTER OR EXTRUDED CURB
////	REMOVE/ABANDON EXISTING PIPE
-----	FULL DEPTH SAWCUT
---	CLEARING & GRUBBING LIMITS
---	HIGH VISIBILITY SILT FENCE
---	HIGH VISIBILITY FENCE
---	TREE PROTECTION FENCE (SITE PREP PLANS)
////	BUTT JOINT
---	APPROX CUT DAYLIGHT LINE
---	APPROX FILL DAYLIGHT LINE
---	FENCE (ROADWAY PLANS)
---	STORM DRAIN PIPE
---	WATER LINE
---	CONDUIT
---	CURB AND GUTTER
---	RETAINING WALL
⊕	SOLDIER PILE WALL
⊕	INLET PROTECTION
⊕	CATCH BASIN TYPE 1 OR TYPE 1L
⊕	CATCH BASIN TYPE 2
▶	DIRECTIONAL FLOW ARROW
⊕	WATER VALVE
⊕	WATER METER
⊕	LONG SLEEVE COUPLING
⊕	FL x RJ ADAPTER
⊕	FIRE HYDRANT
⊕	WATER CAP
⊕	CONC THRUST BLOCK
⊕	ROADWAY LUMINAIRE
⊕	JUNCTION BOX TYPE 1, 2, 8
⊕	ELECTRICAL SERVICE CABINET
⊕	APS PEDESTRIAN PUSH BUTTON
⊕	RECTANGULAR RAPID FLASHING BEACON (RRFB) ASSEMBLY AND POLE
⊕	SIGN
⊕	REMOVE CONIFEROUS / DECIDUOUS TREE

---	REMOVE CEMENT CONCRETE SIDEWALK
---	REMOVE ASPHALT CONC & UNDERLYING CEMENT CONC (SCHEDULE A)
---	REMOVE ASPHALT CONC & UNDERLYING CEMENT CONC (SCHEDULE B)
---	GRIND & OVERLAY (SCHEDULE A)
---	GRIND & OVERLAY (SCHEDULE B)
---	CEMENT CONC SIDEWALK
---	CEMENT CONC CURB RAMP OR DRIVEWAY ENTRANCE
---	ASPHALT ROADWAY (SCHEDULE A)
---	ASPHALT ROADWAY (SCHEDULE B)
---	ASPHALT DRIVEWAY
---	GRAVEL RESTORATION/SHOULDER/MAILBOX CLUSTER PAD
---	TRUNCATED DOME TEXTILE WARNING SURFACE

ABBREVIATIONS

AC	ASPHALT CONCRETE	L	LENGTH	SE	SOUTHEAST
ACP	ASPHALT CONCRETE PAVEMENT	LT	LEFT	SHT	SHEET
ADA	AMERICANS WITH DISABILITIES ACT	LF	LINEAR FEET	SQ	SQUARE
AP	ANGLE POINT	LAT	LATERAL	SS	SANITARY SEWER
APPROX	APPROXIMATE	LP	LOW POINT	SSMH	SANITARY SEWER MANHOLE
BLDG	BUILDING	MAX	MAXIMUM	ST	STREET
CB	CATCH BASIN	MIN	MINIMUM	STA	STATION
CCP	CEMENT CONCRETE PAVEMENT	MH	MANHOLE	STD	STANDARD
CDF	CONTROLLED DENSITY FILL	MIC	MONUMENT IN CASE	STCR	STRUCTURE
CHLK	CHAINLINK	MJ	MECHANICAL JOINT	SW	SOUTHWEST
CL	CLASS	MOD	MODULAR BLOCK	TOC	TOP OF CURB
CL	CLEAROUT	MON	MONUMENT	TYP	TYPICAL
CO	CITY OF KIRKLAND	N	NORTH OR NORTHING	UG	UNDERGROUND
COK	CITY OF KIRKLAND	NAVD	NORTH AMERICAN VERTICAL DATUM	VERT	VERTICAL
COL	COLUMN	NB	NORTHBOUND	W	WEST
CONC	CONCRETE	NO	NUMBER	YD	YARD DRAIN
CSBC	CRUSHED SURFACING BASE COURSE	NTS	NOT TO SCALE		
CSTC	CRUSHED SURFACING TOP COURSE	NUD	NORTHSHORE UTILITY DISTRICT		
DI	DUCTILE IRON	OC	ON CENTER		
DIA	DIAMETER	OD	OUTSIDE DIAMETER		
DW	DRIVEWAY	PC	POINT OF CURVE		
E	EAST OR EASTING	PCC	POINT OF COMPOUND CURVATURE		
EA	EACH	PCCP	PERVIOUS CEMENT CONCRETE PAVEMENT		
EG	EXISTING GROUND	PI	POINT OF INTERSECTION		
ELEV	ELEVATION	POB	POINT OF BEGINNING		
EOP	EDGE OF PAVEMENT	POE	POINT OF ENDING		
EW	EACH WAY	PRC	POINT OF REVERSE CURVATURE		
EXIST	EXISTING	PT	POINT OF TANGENT		
FF	FINISHED FLOOR	PVC	POLYVINYL CHLORIDE OR POINT OF VERTICAL CURVATURE		
FG	FINISHED GROUND	PVT	POINT OF VERTICAL TANGENT		
FL	FLOW LINE	PVI	POINT OF VERTICAL INTERSECTION		
FO	FIBER OPTIC	R	RADIUS		
FOC	FACE OF CURB	ROW	RIGHT OF WAY		
HMA	HOT MIX ASPHALT	RT	RIGHT		
HORIZ	HORIZONTAL	S	SLOPE OR SOUTH		
HP	HIGH POINT	S PILE	SOLDIER PILE		
ID	INSIDE DIAMETER	SB	SOUTHBOUND		
IE	INVERT ELEVATION	SD	STORM DRAIN		
IN	INCH/INCHES	SDMH	STORM DRAIN MANHOLE		
JB	JUNCTION BOX				

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KPG PSOMAS

BID DOCUMENT

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6HDWVOH :HCKPQZGHH

CITY OF KIRKLAND WASHINGTON

CITY OF KIRKLAND
DEPARTMENT OF PUBLIC WORKS
123 FIFTH AVENUE KIRKLAND, WA 98033
(425) 587-3800 www.kirklandwa.gov

JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

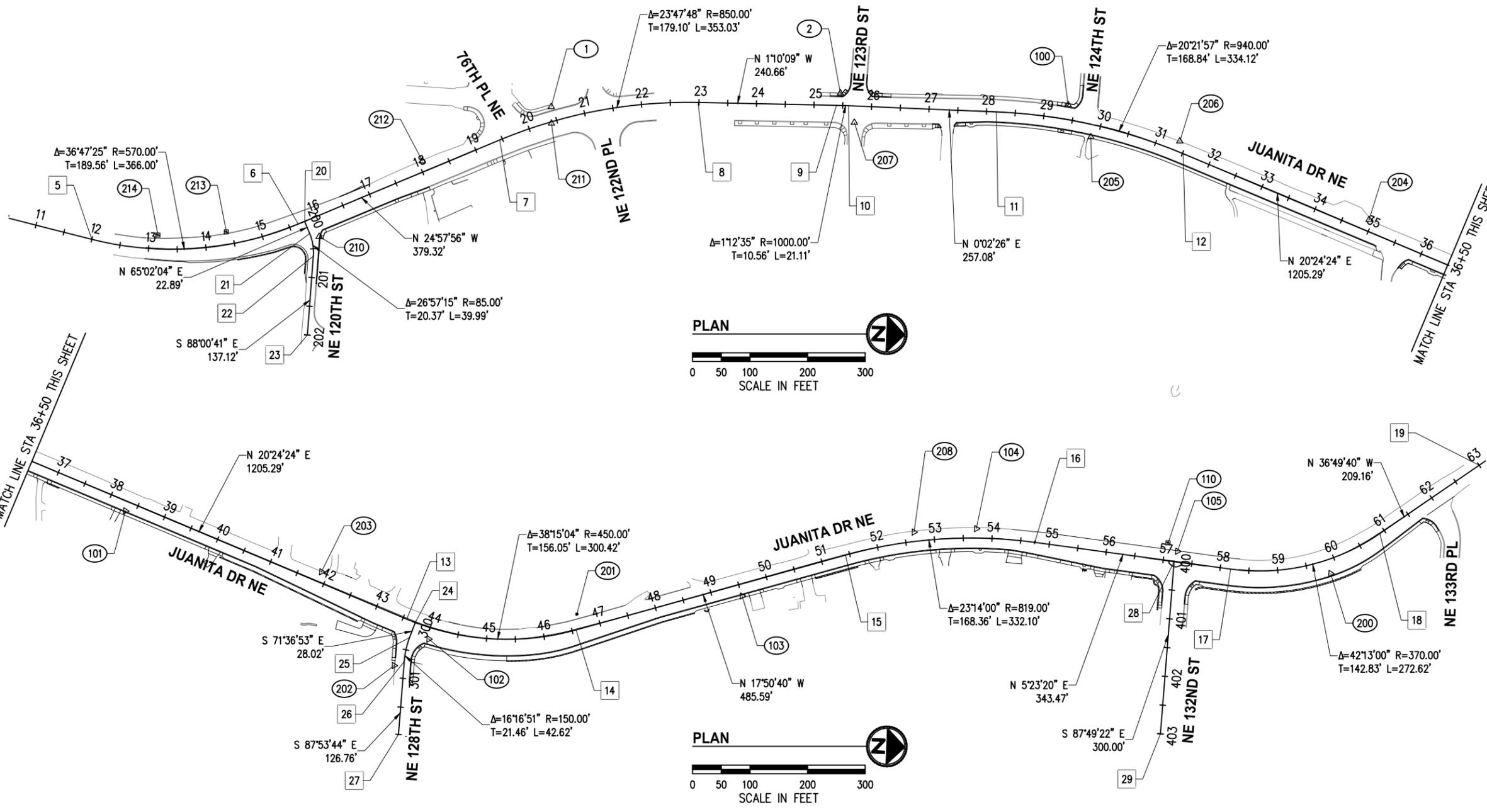
LEGEND & ABBREVIATIONS

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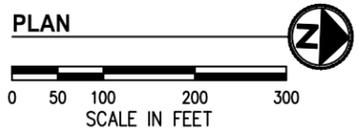
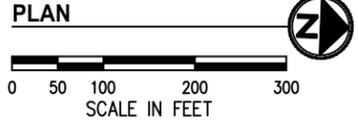
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#	CONTROL POINT LIST			
PT #	DESCRIPTION	NORTHING	EASTING	ELEVATION
1	SPIKE	261827.60	1294599.72	279.10'
2	SPIKE	262328.57	1294555.42	302.41'
100	SPIKE	262724.86	1294559.16	321.94'
101	SPIKE	263561.33	1294888.83	334.31'
102	SPIKE	264097.25	1295088.96	337.15'
103	PK	264638.09	1294991.96	334.43'
104	PK	265040.07	1294858.46	336.34'
105	SPIKE	265390.73	1294883.23	335.06'
110	HT	265373.73	1294870.06	334.80'
200	SPIKE	265658.35	1294910.94	338.31'
201	HT	264352.49	1295035.06	335.14'
202	PK	264038.94	1295137.30	341.99'
203	SPIKE	263906.12	1294980.19	335.03'
204	SPIKE	263255.45	1294736.66	332.05'
205	PK	262766.45	1294613.42	320.39'
206	PK	262921.15	1294614.30	326.93'
207	PK	262354.86	1294605.24	303.25'
208	PK	264931.97	1294868.95	336.25'
210	PK	261433.88	1294841.35	258.90'
211	PK	261829.00	1294628.39	278.04'
212	PK	261607.68	1294702.95	265.26'
213	HT	261272.35	1294839.69	252.70'
214	HT	261153.76	1294850.10	249.35'



ALIGNMENT GEOMETRY POINTS				
#	DESCRIPTION	STATION	NORTHING	EASTING
5	BEGIN JUANITA DR ALIGN, PC	12+00.0	261039.29	1294862.22
6	PT	15+66.0	261396.68	1294821.05
7	PC	19+45.3	261740.55	1294660.95
8	PT	22+98.4	262081.98	1294581.71
9	PC	25+39.0	262322.59	1294576.79
10	PT	25+60.1	262343.70	1294576.59
11	PC	28+17.2	262600.79	1294576.77
12	PT	31+51.3	262927.88	1294635.76
13	PC	43+56.6	264057.52	1295056.02

ALIGNMENT GEOMETRY POINTS				
#	DESCRIPTION	STATION	NORTHING	EASTING
14	PT	46+57.0	264352.32	1295062.61
15	PC	51+42.6	264814.55	1294913.81
16	PT	54+74.7	265142.44	1294878.03
17	PC	58+18.2	265484.39	1294910.29
18	PT	60+90.8	265740.93	1294838.09
19	END JUANITA DR ALIGN	63+00.0	265908.35	1294712.71
20	BEGIN NE 120TH ST ALIGN	200+00.0	261407.55	1294815.99
21	PC	200+22.9	261417.21	1294836.74
22	PT	200+62.9	261425.11	1294875.57

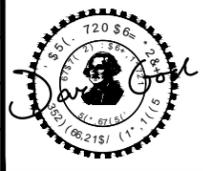
ALIGNMENT GEOMETRY POINTS				
#	DESCRIPTION	STATION	NORTHING	EASTING
23	END NE 120TH ST ALIGN	202+00.0	261420.35	1295012.61
24	BEGIN NE 128TH ST ALIGN	300+00.0	264072.49	1295061.29
25	PC	300+28.0	264063.66	1295087.88
26	PT	300+70.6	264056.10	1295129.68
27	END NE 128TH ST ALIGN	301+97.4	264051.45	1295256.36
28	BEGIN NE 132ND ST ALIGN	400+00.0	265385.62	1294900.97
29	END NE 132ND ST ALIGN	403+00.0	265374.22	1295200.75

HORIZONTAL DATUM
 WASHINGTON STATE PLANE COORDINATE SYSTEM, NORTH ZONE, NAD 83/11.

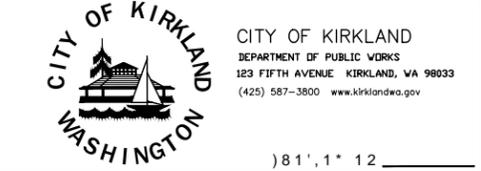
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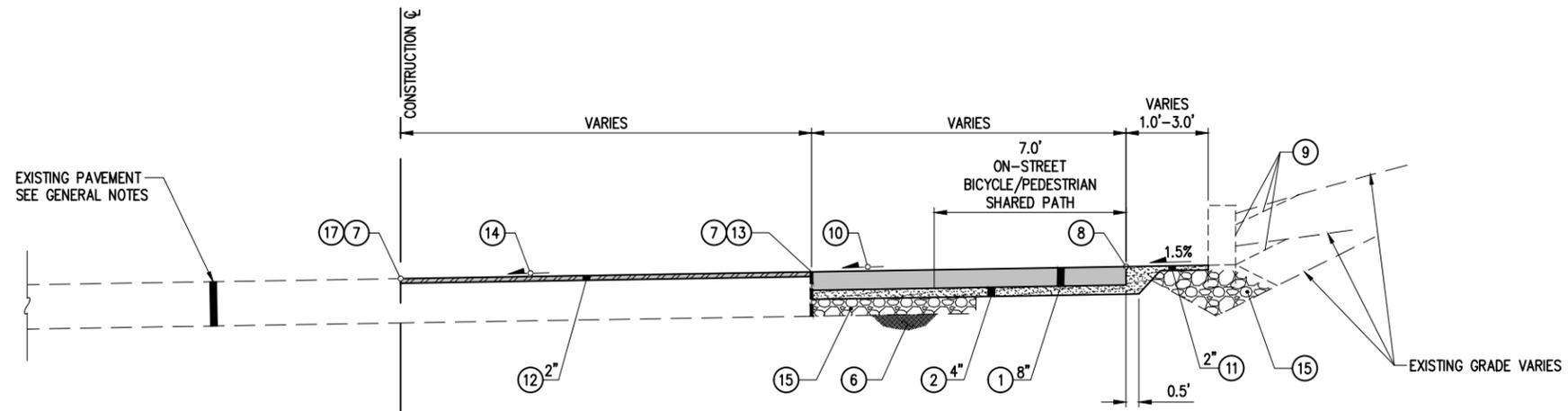
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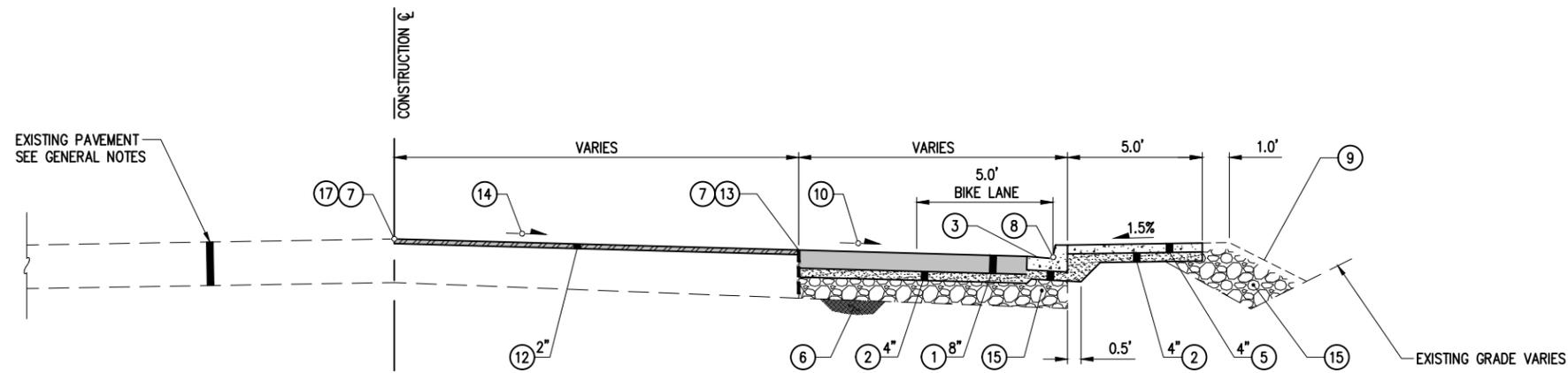
JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS
 ALIGNMENT & SURVEY CONTROL PLAN

REFERENCE SHEET NO.
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TYPICAL SECTION A
 JUANITA DR
 ~STA 12+90-15+40



TYPICAL SECTION B
 JUANITA DR
 ~STA 16+00-18+00

SECTION GENERAL NOTES

1. EXISTING PAVEMENT SECTION DEPTH AND MATERIAL VARIES, SEE SITE PREP PLAN SHEETS 9-16 AND SPECIAL PROVISIONS.
2. SIDEWALK LAYOUT AND EDGE TREATMENT VARIES, REFER TO ROADWAY PLAN SHEETS 33-46, WALL PLAN SHEETS 55-63, LANDSCAPE PLAN SHEETS 74-81, AND RESTORATION TABLE SHEET 81 FOR MORE INFORMATION.
3. ALL WORK SHALL BE COMPLETED IN ACCORDANCE WITH WSDOT/APWA STANDARD SPECIFICATIONS.
4. FOR UNSUITABLE FOUNDATION EXCAVATION AND REPAIR, IF ENCOUNTERED, SEE DETAIL 3 SHEET 8.
5. REFER TO SPECIAL PROVISIONS FOR TEMPORARY HMA ROADWAY REQUIREMENTS.
6. SIDEWALK SHALL BE POURED UP TO FACE OF WALL WHERE PRESENT. INSTALL EXPANSION JOINT BETWEEN FACE OF WALL AND BACK OF SIDEWALK.

SECTION CONSTRUCTION NOTES

- 1 HMA CL 1/2" PG 58H-22, 4" MAX LIFT
- 2 CRUSHED SURFACING TOP COURSE
- 3 CEMENT CONC TRAFFIC CURB & GUTTER PER C.O.K. STD PLAN CK-R.17. REVERSE SLOPE GUTTER PAN ALONG HIGH SIDE OF SUPERELEVATED ROADWAYS
- 4 NOT USED
- 5 CEMENT CONC SIDEWALK PER C.O.K. STD CK-R.23
- 6 COMPACTED SUBGRADE
- 7 MATCH EXISTING GRADE
- 8 HORIZONTAL/VERTICAL CONTROL POINT, SEE ROADWAY PLANS SHEETS 33-46
- 9 SEE ROADWAY PLAN SHEETS 33-46, WALL PLAN SHEETS 55-63, AND RESTORATION TABLE SHEET 81 FOR EDGE RESTORATION. FOR DAYLIGHT SLOPES PROVIDE 1.0' FLAT BENCH PRIOR TO MAX 2:1 SLOPE
- 10 CROWN AND SUPER CROSS SLOPES VARY, SEE ROADWAY PLAN SHEETS 33-46.
- 11 CRUSHED SURFACING TOP COURSE FOR SHOULDER
- 12 GRIND AND OVERLAY
- 13 FULL DEPTH SAWCUT, DEPTH AND MATERIAL VARIES, SEE SITE PREP PLAN SHEETS 9-16 AND SPECIAL PROVISIONS
- 14 MATCH EXISTING ROADWAY CROSS SLOPE
- 15 GRAVEL BORROW FILL, SEE DETAIL 3 SHEET 8 FOR UNSUITABLE FOUNDATION REPAIR IF REQUIRED
- 16 CHAIN LINK FENCE PER ROADWAY PLAN SHEETS 33-46
- 17 EXTEND GRIND & OVERLAY AT CENTERLINE AS REQUIRED TO COMPLETELY REMOVE EXISTING DOUBLE YELLOW CENTERLINE

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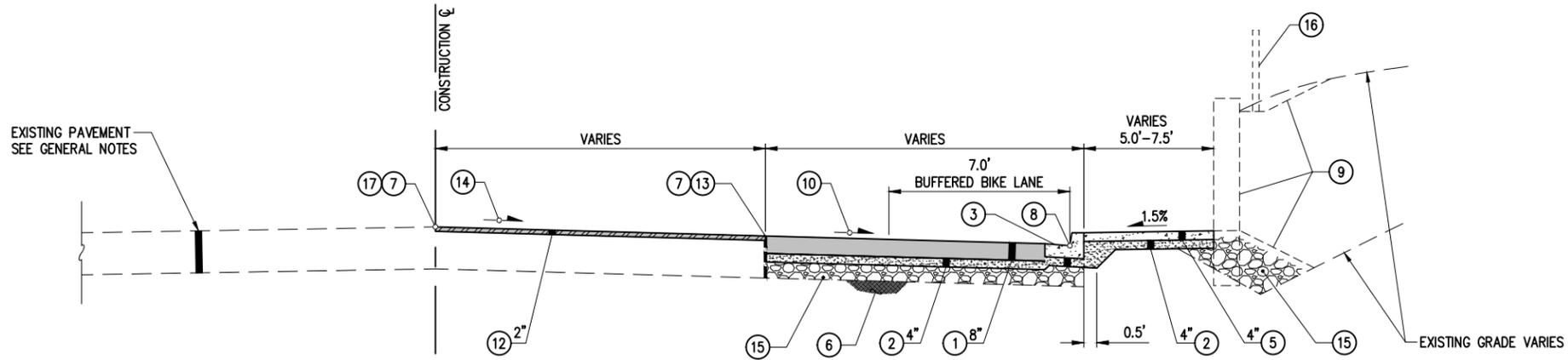


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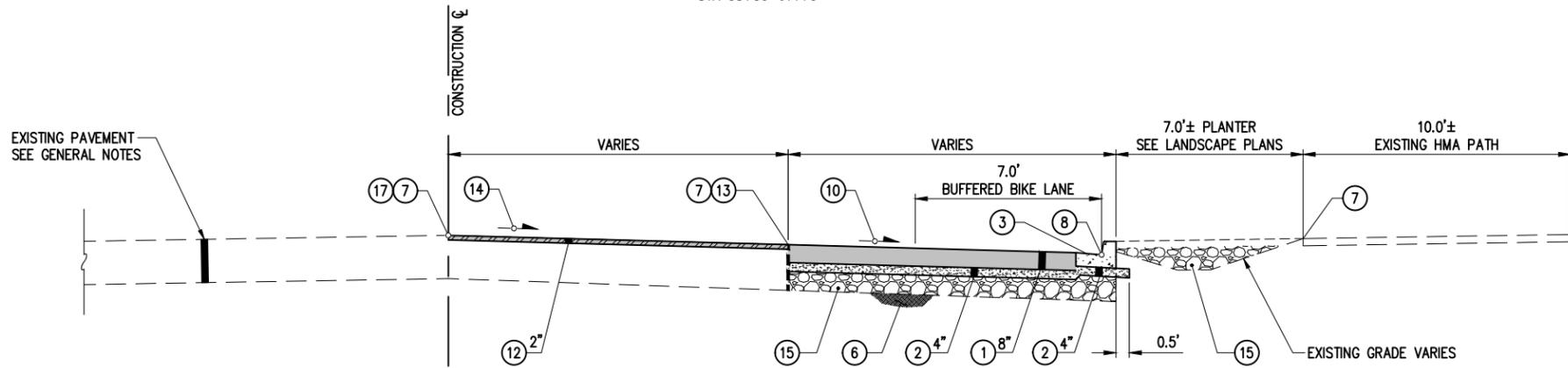


JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS
 TYPICAL SECTIONS
 REFERENCE SHEET NO.
 SHEET 2) SHEETS

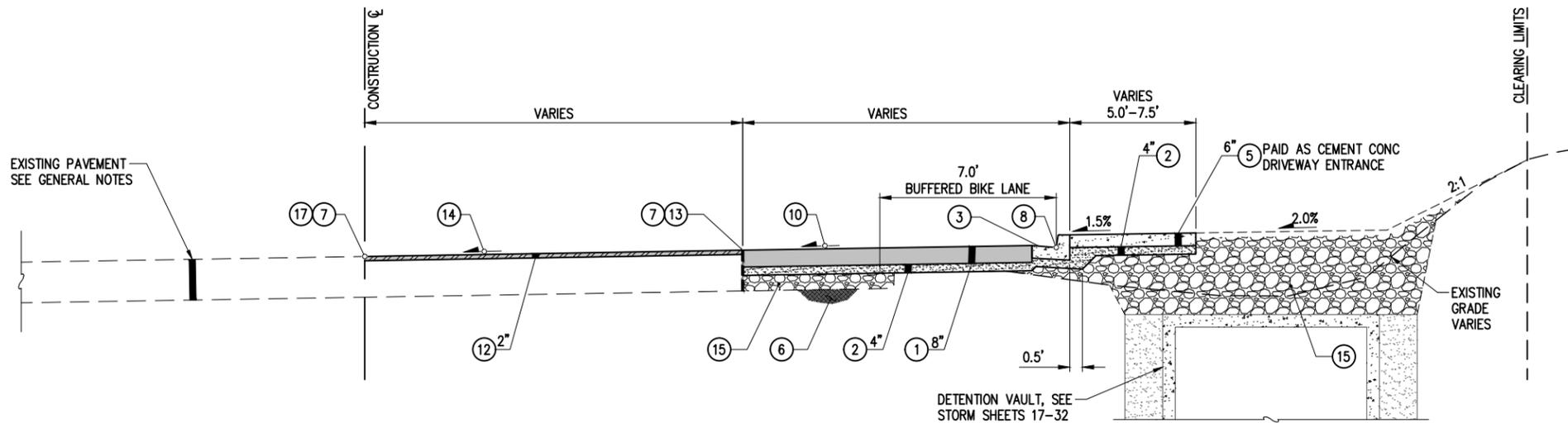
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TYPICAL SECTION C
 JUANITA DR
 ~STA 27+50-32+40
 ~STA 36+00-43+40
 ~STA 45+35-54+35
 ~STA 58+30-61+70



TYPICAL SECTION D
 JUANITA DR
 ~STA 32+40-35+20



TYPICAL SECTION E
 JUANITA DR
 ~STA 44+00-45+35

SECTION GENERAL NOTES

- EXISTING PAVEMENT SECTION DEPTH AND MATERIAL VARIES, SEE SITE PREP PLAN SHEETS 9-16 AND SPECIAL PROVISIONS.
- SIDEWALK LAYOUT AND EDGE TREATMENT VARIES, REFER TO ROADWAY PLAN SHEETS 33-46, WALL PLAN SHEETS 55-63, LANDSCAPE PLAN SHEETS 74-81, AND RESTORATION TABLE SHEET 81 FOR MORE INFORMATION.
- ALL WORK SHALL BE COMPLETED IN ACCORDANCE WITH WSDOT/APWA STANDARD SPECIFICATIONS.
- FOR UNSUITABLE FOUNDATION EXCAVATION AND REPAIR, IF ENCOUNTERED, SEE DETAIL 3 SHEET 8.
- REFER TO SPECIAL PROVISIONS FOR TEMPORARY HMA ROADWAY REQUIREMENTS.
- SIDEWALK SHALL BE POURED UP TO FACE OF WALL WHERE PRESENT. INSTALL EXPANSION JOINT BETWEEN FACE OF WALL AND BACK OF SIDEWALK.

SECTION CONSTRUCTION NOTES

- HMA CL 1/2" PG 58H-22, 4" MAX LIFT
- CRUSHED SURFACING TOP COURSE
- CEMENT CONC TRAFFIC CURB & GUTTER PER C.O.K. STD PLAN CK-R.17. REVERSE SLOPE GUTTER PAN ALONG HIGH SIDE OF SUPERELEVATED ROADWAYS
- NOT USED
- CEMENT CONC SIDEWALK PER C.O.K. STD CK-R.23
- COMPACTED SUBGRADE
- MATCH EXISTING GRADE
- HORIZONTAL/VERTICAL CONTROL POINT, SEE ROADWAY PLANS SHEETS 33-46
- SEE ROADWAY PLAN SHEETS 33-46, WALL PLAN SHEETS 55-63, AND RESTORATION TABLE SHEET 81 FOR EDGE RESTORATION. FOR DAYLIGHT SLOPES PROVIDE 1.0' FLAT BENCH PRIOR TO MAX 2:1 SLOPE
- CROWN AND SUPER CROSS SLOPES VARY, SEE ROADWAY PLAN SHEETS 33-46.
- CRUSHED SURFACING TOP COURSE FOR SHOULDER
- GRIND AND OVERLAY
- FULL DEPTH SAWCUT, DEPTH AND MATERIAL VARIES, SEE SITE PREP PLAN SHEETS 9-16 AND SPECIAL PROVISIONS
- MATCH EXISTING ROADWAY CROSS SLOPE
- GRAVEL BORROW FILL, SEE DETAIL 3 SHEET 8 FOR UNSUITABLE FOUNDATION REPAIR IF REQUIRED
- CHAIN LINK FENCE PER ROADWAY PLAN SHEETS 33-46
- EXTEND GRIND & OVERLAY AT CENTERLINE AS REQUIRED TO COMPLETELY REMOVE EXISTING DOUBLE YELLOW CENTERLINE



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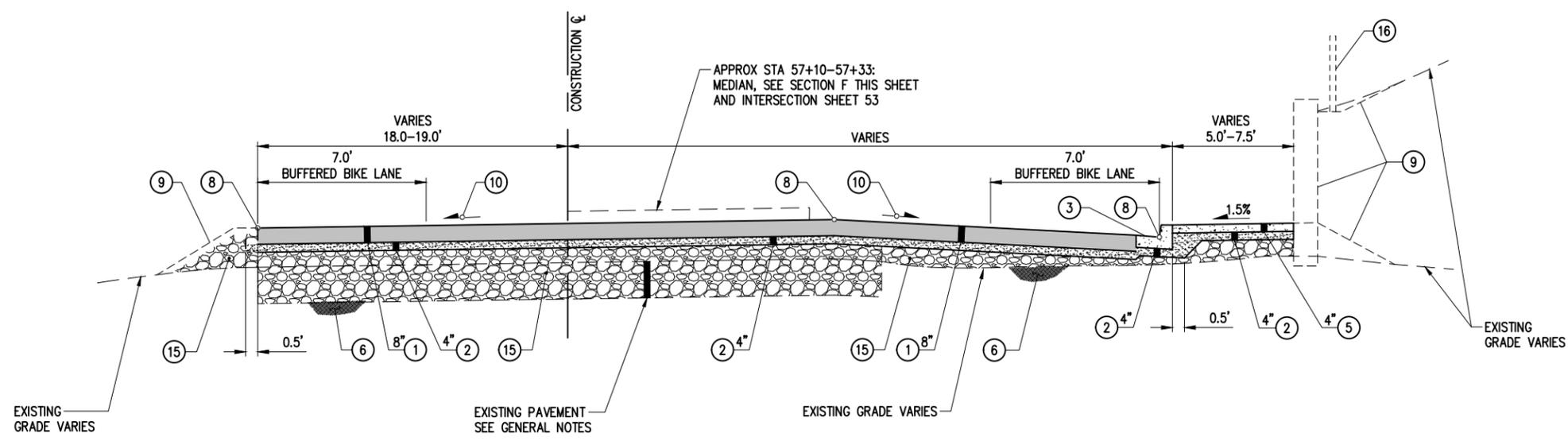
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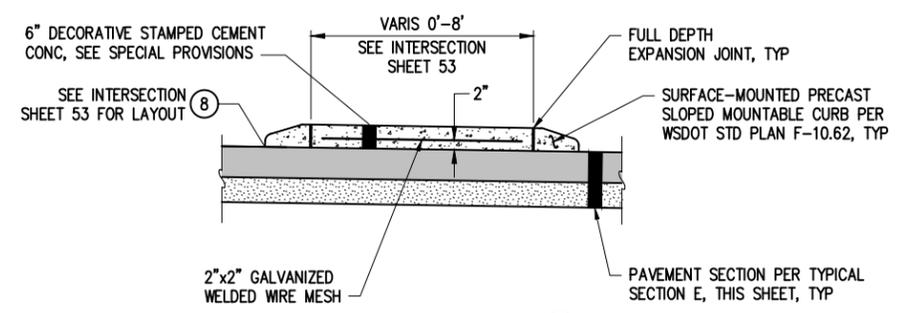
JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS
 TYPICAL SECTIONS

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TYPICAL SECTION F
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TYPICAL SECTION G
 JUANITA DR
 ~STA 57+10-57+33

SECTION GENERAL NOTES

1. EXISTING PAVEMENT SECTION DEPTH AND MATERIAL VARIES, SEE SITE PREP PLAN SHEETS 9-16 AND SPECIAL PROVISIONS.
2. SIDEWALK LAYOUT AND EDGE TREATMENT VARIES, REFER TO ROADWAY PLAN SHEETS 33-46, WALL PLAN SHEETS 55-63, LANDSCAPE PLAN SHEETS 74-81, AND RESTORATION TABLE SHEET 81 FOR MORE INFORMATION.
3. ALL WORK SHALL BE COMPLETED IN ACCORDANCE WITH WSDOT/APWA STANDARD SPECIFICATIONS.
4. FOR UNSUITABLE FOUNDATION EXCAVATION AND REPAIR, IF ENCOUNTERED, SEE DETAIL 3 SHEET 8.
5. REFER TO SPECIAL PROVISIONS FOR TEMPORARY HMA ROADWAY REQUIREMENTS.
6. SIDEWALK SHALL BE POURED UP TO FACE OF WALL WHERE PRESENT. INSTALL EXPANSION JOINT BETWEEN FACE OF WALL AND BACK OF SIDEWALK.

SECTION CONSTRUCTION NOTES

- ① HMA CL 1/2" PG 58H-22, 4" MAX LIFT
- ② CRUSHED SURFACING TOP COURSE
- ③ CEMENT CONC TRAFFIC CURB & GUTTER PER C.O.K. STD PLAN CK-R.17. REVERSE SLOPE GUTTER PAN ALONG HIGH SIDE OF SUPERELEVATED ROADWAYS
- ④ NOT USED
- ⑤ CEMENT CONC SIDEWALK PER C.O.K. STD CK-R.23
- ⑥ COMPACTED SUBGRADE
- ⑦ MATCH EXISTING GRADE
- ⑧ HORIZONTAL/VERTICAL CONTROL POINT, SEE ROADWAY PLANS SHEETS 33-46
- ⑨ SEE ROADWAY PLAN SHEETS 33-46, WALL PLAN SHEETS 55-63, AND RESTORATION TABLE SHEET 81 FOR EDGE RESTORATION. FOR DAYLIGHT SLOPES PROVIDE 1.0' FLAT BENCH PRIOR TO MAX 2:1 SLOPE
- ⑩ CROWN AND SUPER CROSS SLOPES VARY, SEE ROADWAY PLAN SHEETS 33-46.
- ⑪ CRUSHED SURFACING TOP COURSE FOR SHOULDER
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- ⑬ FULL DEPTH SAWCUT, DEPTH AND MATERIAL VARIES, SEE SITE PREP PLAN SHEETS 9-16 AND SPECIAL PROVISIONS
- ⑭ MATCH EXISTING ROADWAY CROSS SLOPE
- ⑮ GRAVEL BORROW FILL, SEE DETAIL 3 SHEET 8 FOR UNSUITABLE FOUNDATION REPAIR IF REQUIRED
- ⑯ CHAIN LINK FENCE PER ROADWAY PLAN SHEETS 33-46
- ⑰ EXTEND GRIND & OVERLAY AT CENTERLINE AS REQUIRED TO COMPLETELY REMOVE EXISTING DOUBLE YELLOW CENTERLINE



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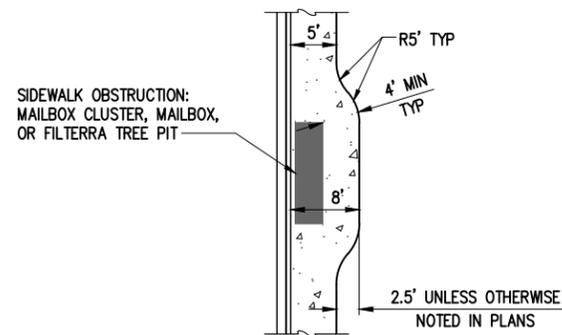


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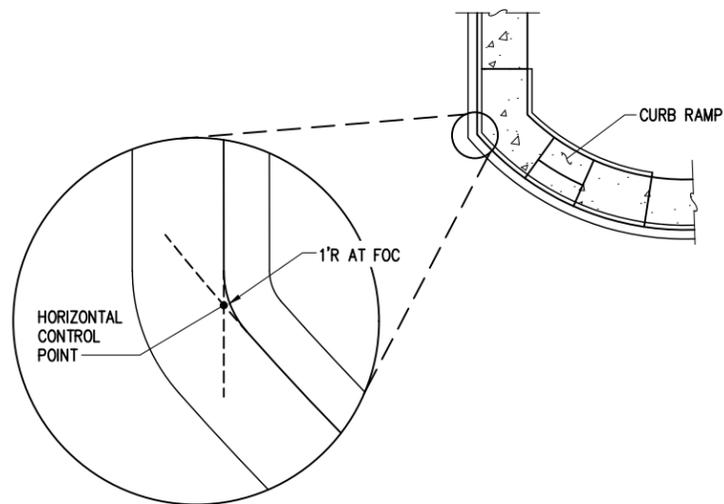
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JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS
 TYPICAL SECTIONS

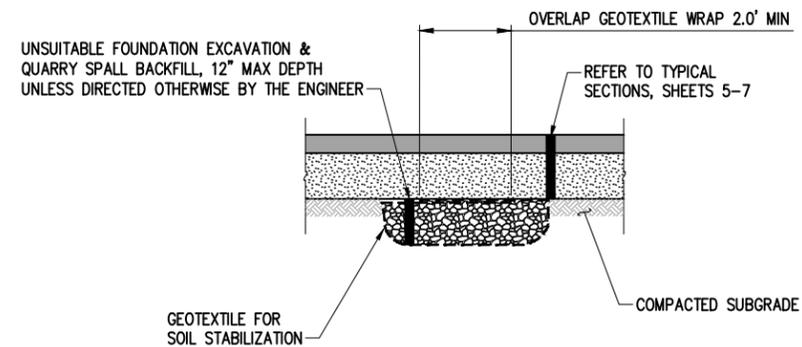
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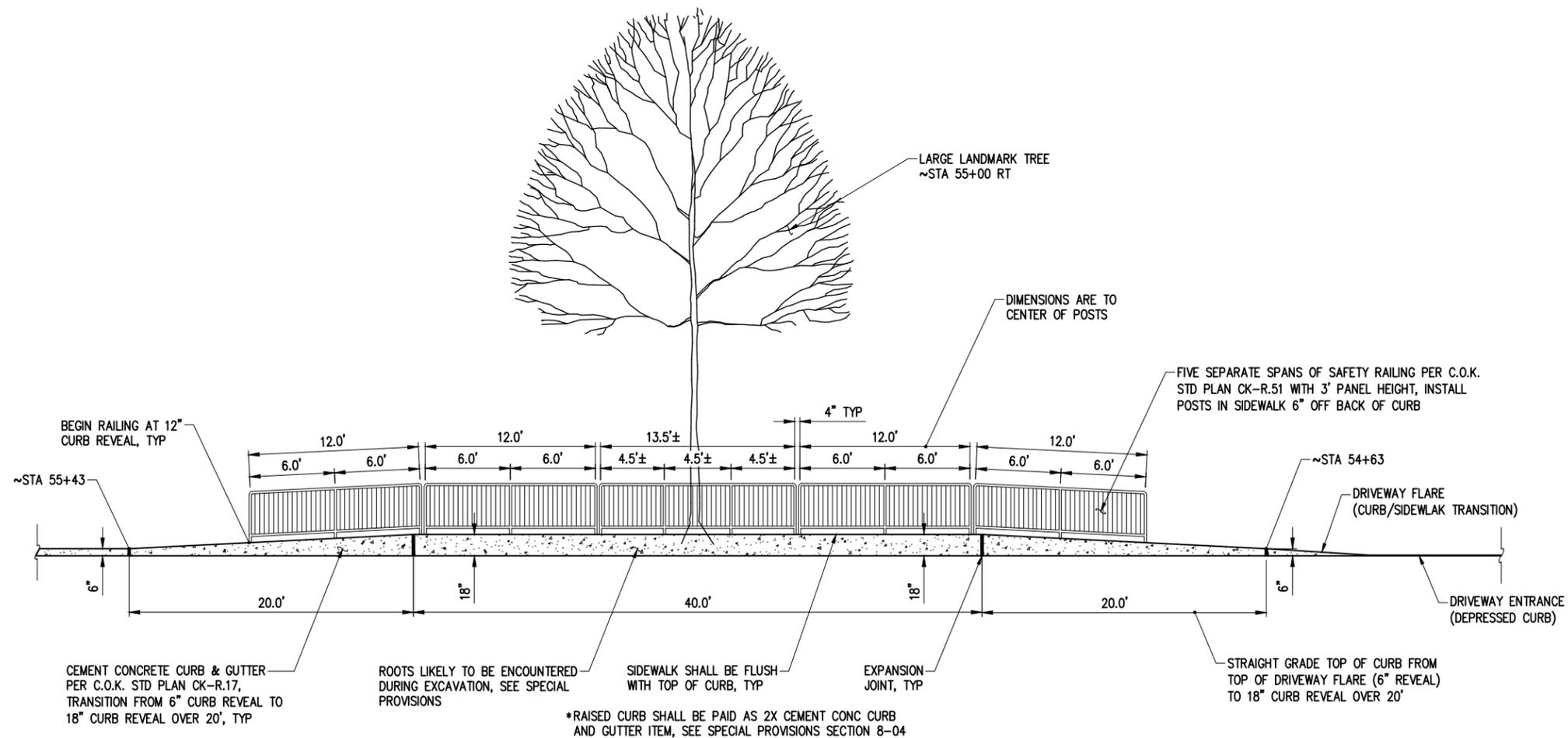
PEDESTRIAN PATH WIDENING DETAIL **1**
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CURB RETURN RADIUS DETAIL **2**
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UNSUITABLE FOUNDATION REPAIR DETAIL **3**
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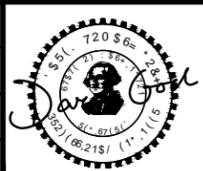


RAISED CURB & SIDEWALK DETAIL **4**
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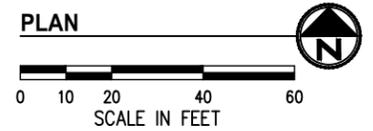
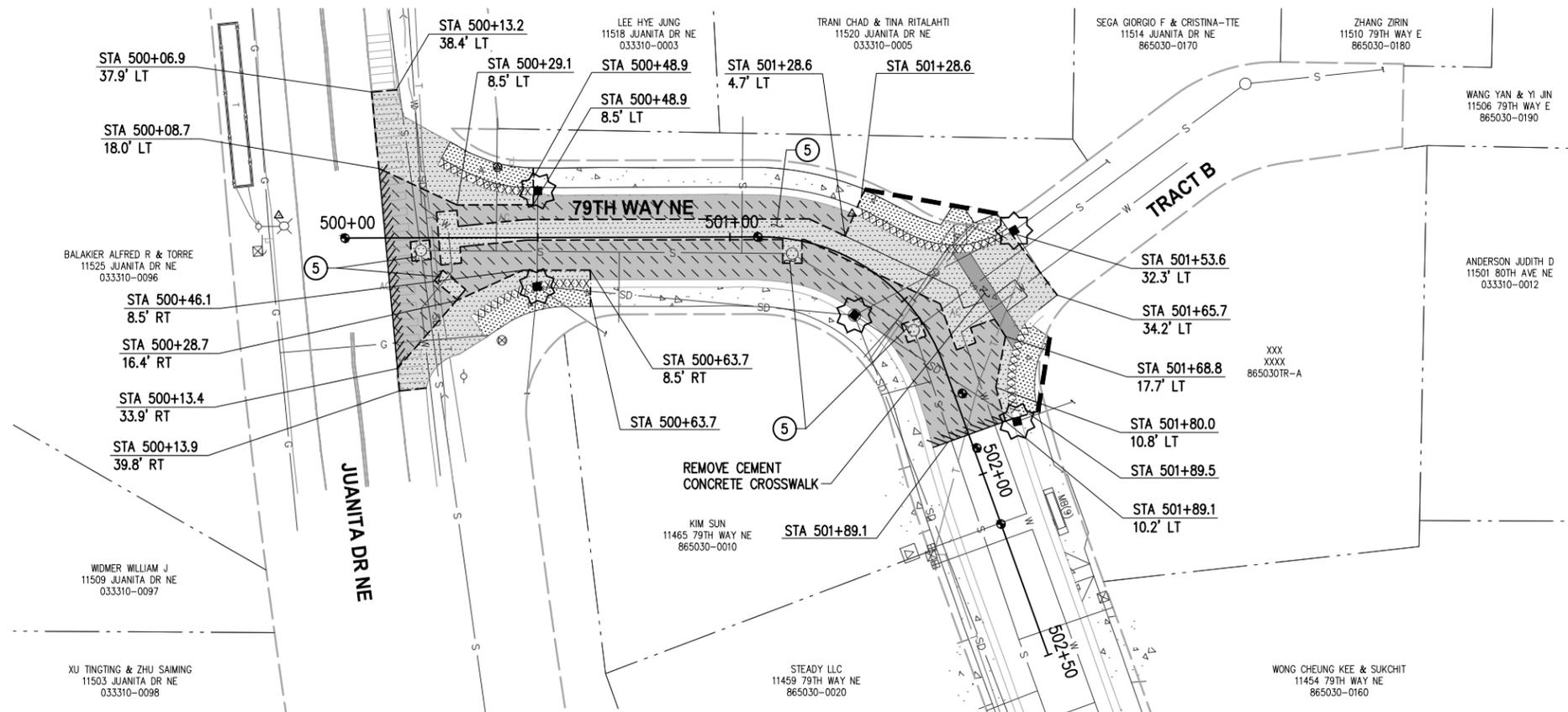
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**JUANITA DRIVE MULTIMODAL,
INTERSECTION & SAFETY IMPROVEMENTS**

TYPICAL DETAILS

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ALL COSTS ASSOCIATED WITH SAWCUTTING & REMOVALS ON THIS SHEET ARE CONSIDERED INCIDENTAL TO THE VARIOUS BID ITEMS IN SCHEDULE B AND WILL NOT BE MEASURED FOR PAYMENT

GENERAL NOTES

1. PRESERVE AND PROTECT ALL UTILITIES, STRUCTURES, AND VEGETATION NOT CALLED OUT FOR REMOVAL.
2. FOR TREATMENT OF EXISTING ITEMS NOT NOTED IN THESE SITE PREP PLANS, REFER TO THE OTHER PLAN SUBSETS PROVIDED HEREIN.
3. ITEMS NOTED FOR REMOVAL SHALL BE DEMOLISHED, HAULED, AND DISPOSED OF AT CONTRACTOR'S EXPENSE, UNLESS NOTED OTHERWISE.
4. MAINTAIN DRIVEWAY AND BUSINESS ACCESS AT ALL TIMES, SEE SPECIAL PROVISIONS.
5. ASPHALT & CONCRETE REMOVAL LIMITS AND CLEARING & GRUBBING LIMITS SHOWN ARE APPROXIMATE. LIMITS SHALL BE MARKED IN THE FIELD BY THE CONTRACTOR AND APPROVED BY THE ENGINEER PRIOR TO ANY DEMOLITION/CLEARING ACTIVITIES.
6. ADJUST SAWCUT LINES AS NECESSARY TO AVOID UTILITIES.
7. SIDEWALK SHALL BE REMOVED TO NEAREST JOINT BEYOND LIMITS SHOWN UNLESS DIRECTED OTHERWISE.
8. EXISTING UTILITY LOCATIONS AND DEPTHS ARE BASED ON AS-BUILT RECORDS AND BEST AVAILABLE INFORMATION. CONTRACTOR SHALL FIELD VERIFY UTILITY LOCATIONS AND DEPTHS PRIOR TO CONSTRUCTION ACTIVITIES.

CONSTRUCTION NOTES

- 5 SAWCUT AND REMOVE PAVEMENT AS REQUIRED TO CONSTRUCT STORM, SEWER, & WATER IMPROVEMENTS; SEE STORM SHEETS 17-29 AND WATER & SEWER SHEETS 131-159

LEGEND

- RIGHT OF WAY
- - - TEMPORARY CONSTRUCTION EASEMENT
- [Pattern] REMOVE CEMENT CONC SIDEWALK
- [Pattern] REMOVE ASPHALT CONC AND UNDERLYING CEMENT CONC PAVEMENT WHERE PRESENT - PAID UNDER SCHED A
- [Pattern] REMOVE ASPHALT CONC AND UNDERLYING CEMENT CONC PAVEMENT WHERE PRESENT - PAID UNDER SCHED B
- [Pattern] 2" DEPTH PAVEMENT PLANING - PAID UNDER SCHED A
- [Pattern] 2" DEPTH PAVEMENT PLANING - PAID UNDER SCHED B
- XXXXXXXXXXXX REMOVE CURB & GUTTER OR EXTRUDED CURB
- - - - - FULL DEPTH SAWCUT, LOCATIONS NOTED
- [Pattern] BUTT JOINT
- [Pattern] CLEARING & GRUBBING LIMITS, LOCATIONS NOTED
- HIGH VISIBILITY SILT FENCE PER C.O.K. STD PLAN CK-E.03
- HIGH VISIBILITY FENCE
- x TREE PROTECTION FENCE PER C.O.K. STD PLAN CK-R.49
- [Symbol] INLET PROTECTION PER C.O.K. STD PLAN CK-E.11
- [Symbol] REMOVE CONIFEROUS / DECIDUOUS TREE
- [Symbol] TREE ID, SEE ARBORIST REPORT IN APPENDIX OF SPECS

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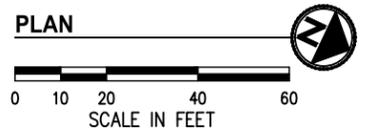
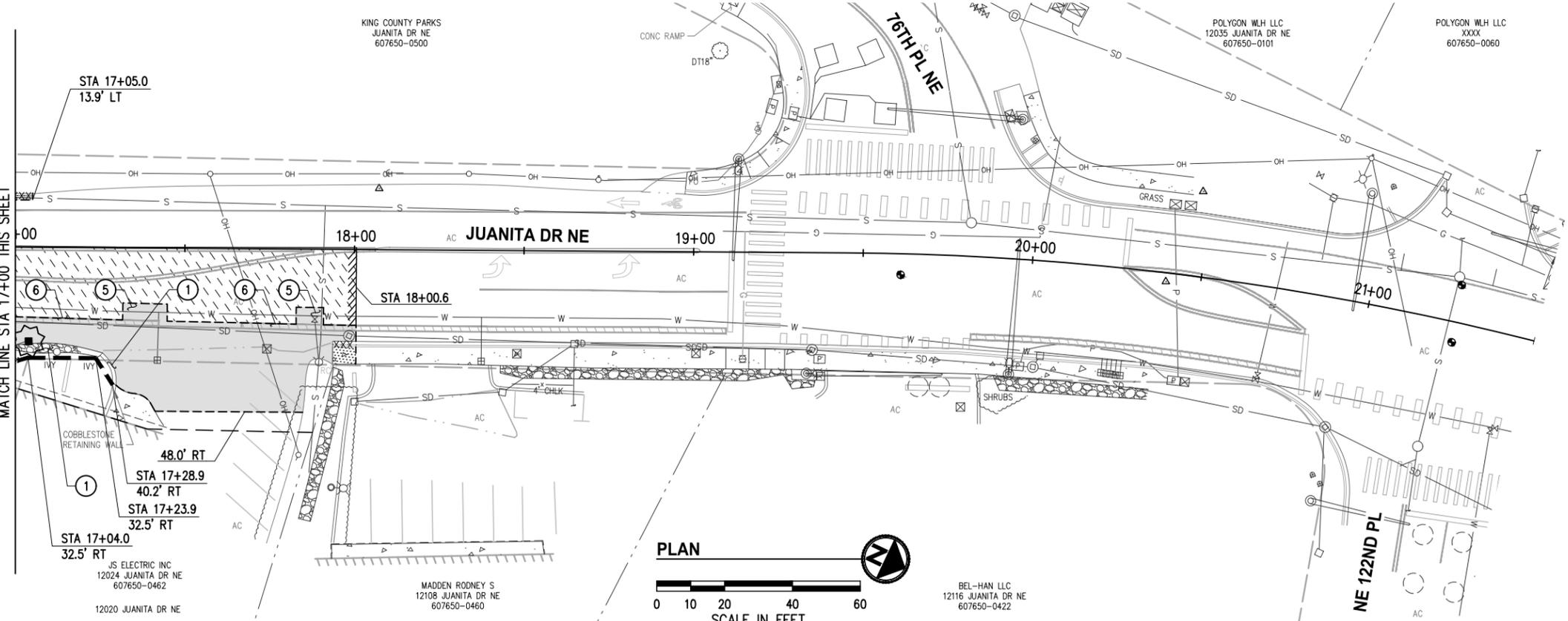
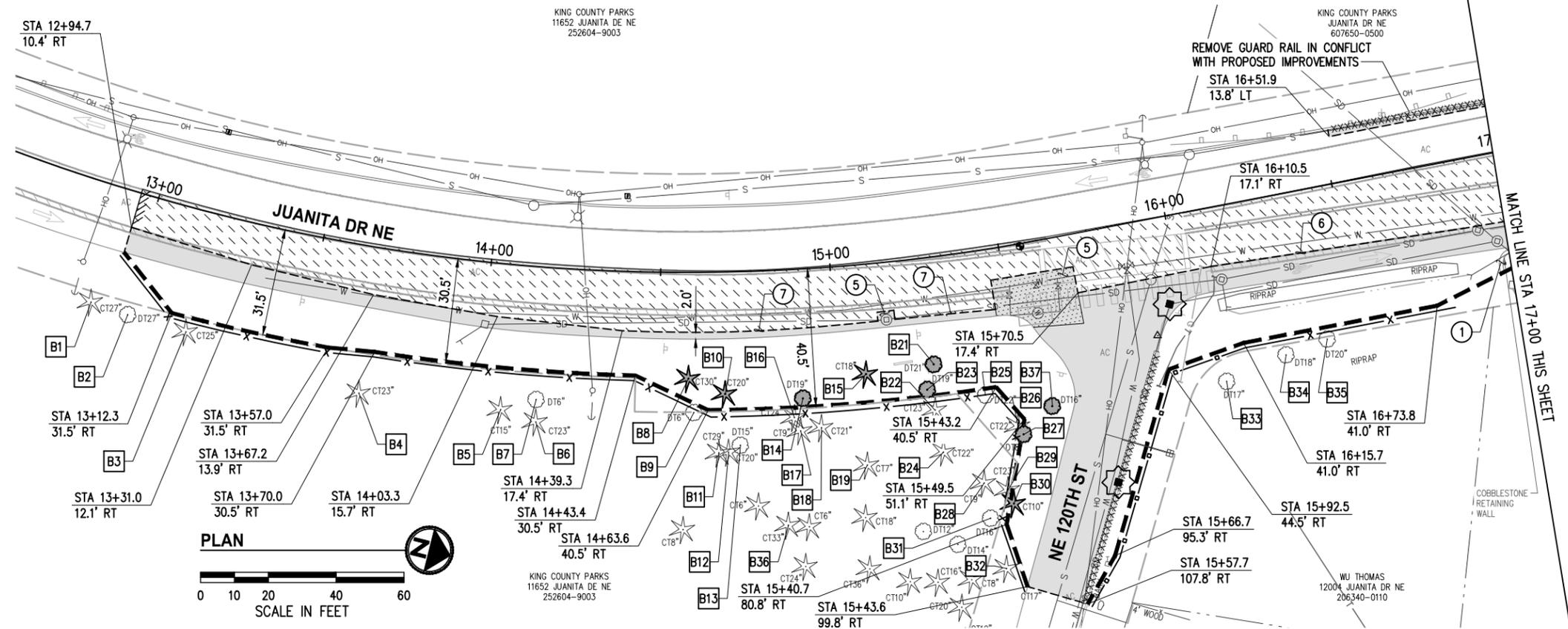


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JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS
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SITE PREPARATION & TESC PLAN
 REFERENCE SHEET NO.
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GENERAL NOTES

- PRESERVE AND PROTECT ALL UTILITIES, STRUCTURES, AND VEGETATION NOT CALLED OUT FOR REMOVAL.
- FOR TREATMENT OF EXISTING ITEMS NOT NOTED IN THESE SITE PREP PLANS, REFER TO THE OTHER PLAN SUBSETS PROVIDED HEREIN.
- ITEMS NOTED FOR REMOVAL SHALL BE DEMOLISHED, HAULED, AND DISPOSED OF AT CONTRACTOR'S EXPENSE, UNLESS NOTED OTHERWISE.
- MAINTAIN DRIVEWAY AND BUSINESS ACCESS AT ALL TIMES, SEE SPECIAL PROVISIONS.
- ASPHALT & CONCRETE REMOVAL LIMITS AND CLEARING & GRUBBING LIMITS SHOWN ARE APPROXIMATE. LIMITS SHALL BE MARKED IN THE FIELD BY THE CONTRACTOR AND APPROVED BY THE ENGINEER PRIOR TO ANY DEMOLITION/CLEARING ACTIVITIES.
- ADJUST SAWCUT LINES AS NECESSARY TO AVOID UTILITIES.
- SIDEWALK SHALL BE REMOVED TO NEAREST JOINT BEYOND LIMITS SHOWN UNLESS DIRECTED OTHERWISE.
- EXISTING UTILITY LOCATIONS AND DEPTHS ARE BASED ON AS-BUILT RECORDS AND BEST AVAILABLE INFORMATION. CONTRACTOR SHALL FIELD VERIFY UTILITY LOCATIONS AND DEPTHS PRIOR TO CONSTRUCTION ACTIVITIES.

CONSTRUCTION NOTES

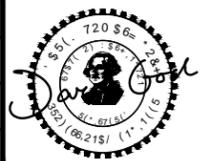
- REMOVAL RETAINING/LANDSCAPE/ROCKERY WALL IN CONFLICT WITH PROPOSED IMPROVEMENTS
- SAWCUT AND REMOVE PAVEMENT AS REQUIRED TO CONSTRUCT STORM, SEWER, & WATER IMPROVEMENTS; SEE STORM SHEETS 17-29 AND WATER & SEWER SHEETS 131-159
- SAWCUT ALONG EXISTING FOG LINE
- SAWCUT 2' FROM EXISTING EDGE OF PAVEMENT

LEGEND

- RIGHT OF WAY
- TEMPORARY CONSTRUCTION EASEMENT
- REMOVE CEMENT CONC SIDEWALK
- REMOVE ASPHALT CONC AND UNDERLYING CEMENT CONC PAVEMENT WHERE PRESENT - PAID UNDER SCHED A
- REMOVE ASPHALT CONC AND UNDERLYING CEMENT CONC PAVEMENT WHERE PRESENT - PAID UNDER SCHED B
- 2" DEPTH PAVEMENT PLANING - PAID UNDER SCHED A
- 2" DEPTH PAVEMENT PLANING - PAID UNDER SCHED B
- XXXXXXXXXXXXXXXXX REMOVE CURB & GUTTER OR EXTRUDED CURB
- FULL DEPTH SAWCUT, LOCATIONS NOTED
- BUTT JOINT
- CLEARING & GRUBBING LIMITS, LOCATIONS NOTED
- O-O- HIGH VISIBILITY SILT FENCE PER C.O.K. STD PLAN CK-E.03
- O-O- HIGH VISIBILITY FENCE
- X- TREE PROTECTION FENCE PER C.O.K. STD PLAN CK-R.49
- INLET PROTECTION PER C.O.K. STD PLAN CK-E.11
- REMOVE CONIFEROUS / DECIDUOUS TREE
- # TREE ID, SEE ARBORIST REPORT IN APPENDIX OF SPECS



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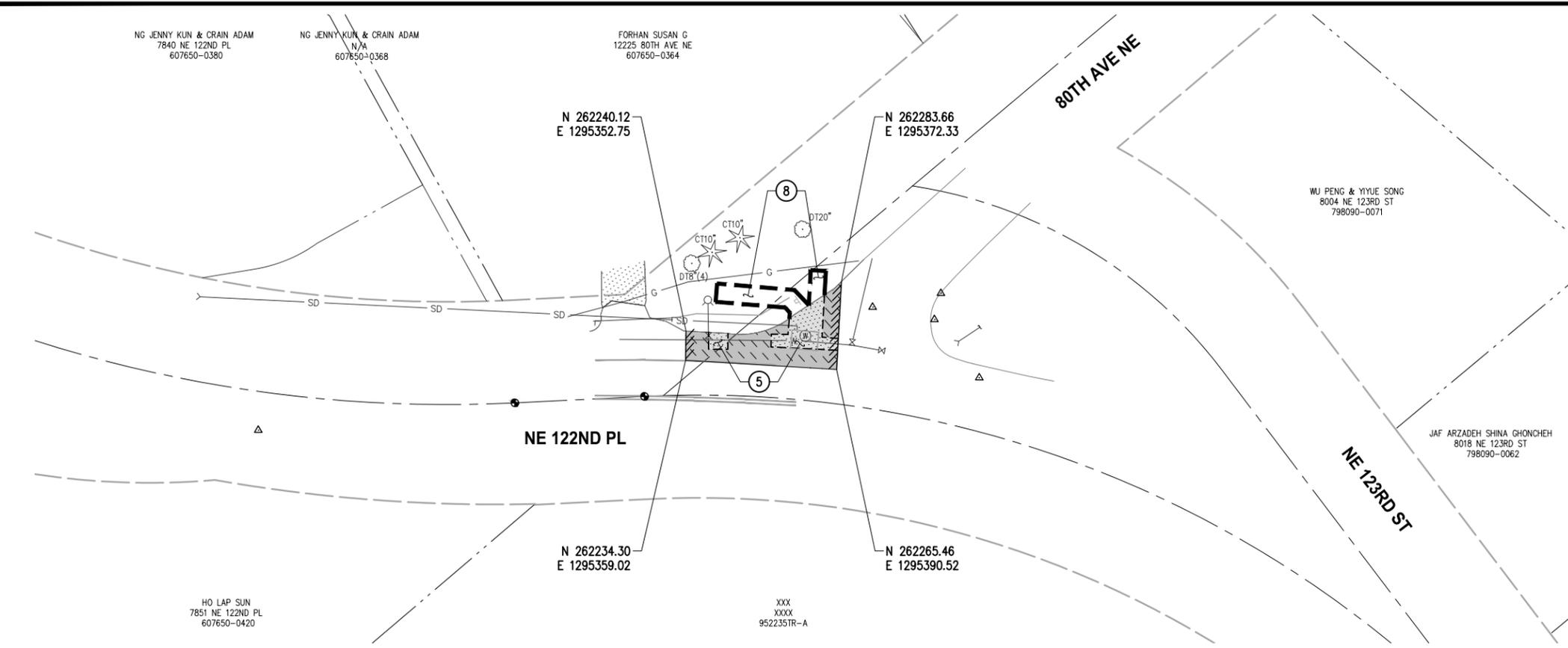


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JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS		REFERENCE SHEET NO.
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ALL COSTS ASSOCIATED WITH SAWCUTTING AND REMOVALS ON THIS SHEET ARE CONSIDERED INCIDENTAL TO THE VARIOUS BID ITEMS IN SCHEDULE B AND WILL NOT BE MEASURED FOR PAYMENT

GENERAL NOTES

1. PRESERVE AND PROTECT ALL UTILITIES, STRUCTURES, AND VEGETATION NOT CALLED OUT FOR REMOVAL.
2. FOR TREATMENT OF EXISTING ITEMS NOT NOTED IN THESE SITE PREP PLANS, REFER TO THE OTHER PLAN SUBSETS PROVIDED HEREIN.
3. ITEMS NOTED FOR REMOVAL SHALL BE DEMOLISHED, HAULED, AND DISPOSED OF AT CONTRACTOR'S EXPENSE, UNLESS NOTED OTHERWISE.
4. MAINTAIN DRIVEWAY AND BUSINESS ACCESS AT ALL TIMES, SEE SPECIAL PROVISIONS.
5. ASPHALT & CONCRETE REMOVAL LIMITS AND CLEARING & GRUBBING LIMITS SHOWN ARE APPROXIMATE. LIMITS SHALL BE MARKED IN THE FIELD BY THE CONTRACTOR AND APPROVED BY THE ENGINEER PRIOR TO ANY DEMOLITION/CLEARING ACTIVITIES.
6. ADJUST SAWCUT LINES AS NECESSARY TO AVOID UTILITIES.
7. SIDEWALK SHALL BE REMOVED TO NEAREST JOINT BEYOND LIMITS SHOWN UNLESS DIRECTED OTHERWISE.
8. EXISTING UTILITY LOCATIONS AND DEPTHS ARE BASED ON AS-BUILT RECORDS AND BEST AVAILABLE INFORMATION. CONTRACTOR SHALL FIELD VERIFY UTILITY LOCATIONS AND DEPTHS PRIOR TO CONSTRUCTION ACTIVITIES.

CONSTRUCTION NOTES

- ⑤ SAWCUT AND REMOVE PAVEMENT AS REQUIRED TO CONSTRUCT STORM, SEWER, & WATER IMPROVEMENTS; SEE STORM SHEETS 17-29 AND WATER & SEWER SHEETS 131-159
- ⑧ CLEAR & GRUB AS REQUIRED TO INSTALL WATER IMPROVEMENTS, SEE WATER SHEETS 131-159

LEGEND

- RIGHT OF WAY
- - - - - TEMPORARY CONSTRUCTION EASEMENT
- [Pattern] REMOVE CEMENT CONC SIDEWALK
- [Pattern] REMOVE ASPHALT CONC AND UNDERLYING CEMENT CONC PAVEMENT WHERE PRESENT - PAID UNDER SCHED A
- [Pattern] REMOVE ASPHALT CONC AND UNDERLYING CEMENT CONC PAVEMENT WHERE PRESENT - PAID UNDER SCHED B
- [Pattern] 2" DEPTH PAVEMENT PLANING - PAID UNDER SCHED A
- [Pattern] 2" DEPTH PAVEMENT PLANING - PAID UNDER SCHED B
- XXXXXXXXXXXX REMOVE CURB & GUTTER OR EXTRUDED CURB
- FULL DEPTH SAWCUT, LOCATIONS NOTED
- ////// BUTT JOINT
- - - - - CLEARING & GRUBBING LIMITS, LOCATIONS NOTED
- ○ ○ ○ ○ HIGH VISIBILITY SILT FENCE PER C.O.K. STD PLAN CK-E.03
- □ □ □ □ HIGH VISIBILITY FENCE
- x TREE PROTECTION FENCE PER C.O.K. STD PLAN CK-R.49
- [Symbol] INLET PROTECTION PER C.O.K. STD PLAN CK-E.11
- [Symbol] REMOVE CONIFEROUS / DECIDUOUS TREE
- [Symbol] TREE ID, SEE ARBORIST REPORT IN APPENDIX OF SPECS



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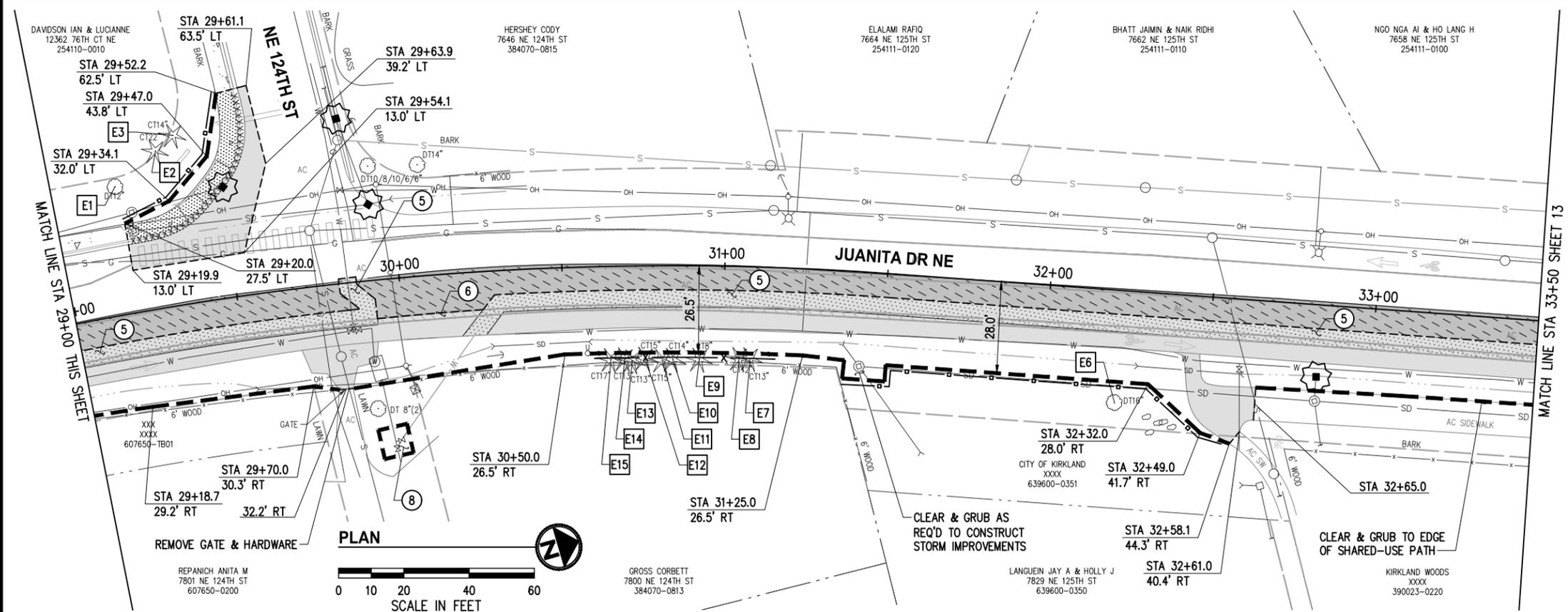
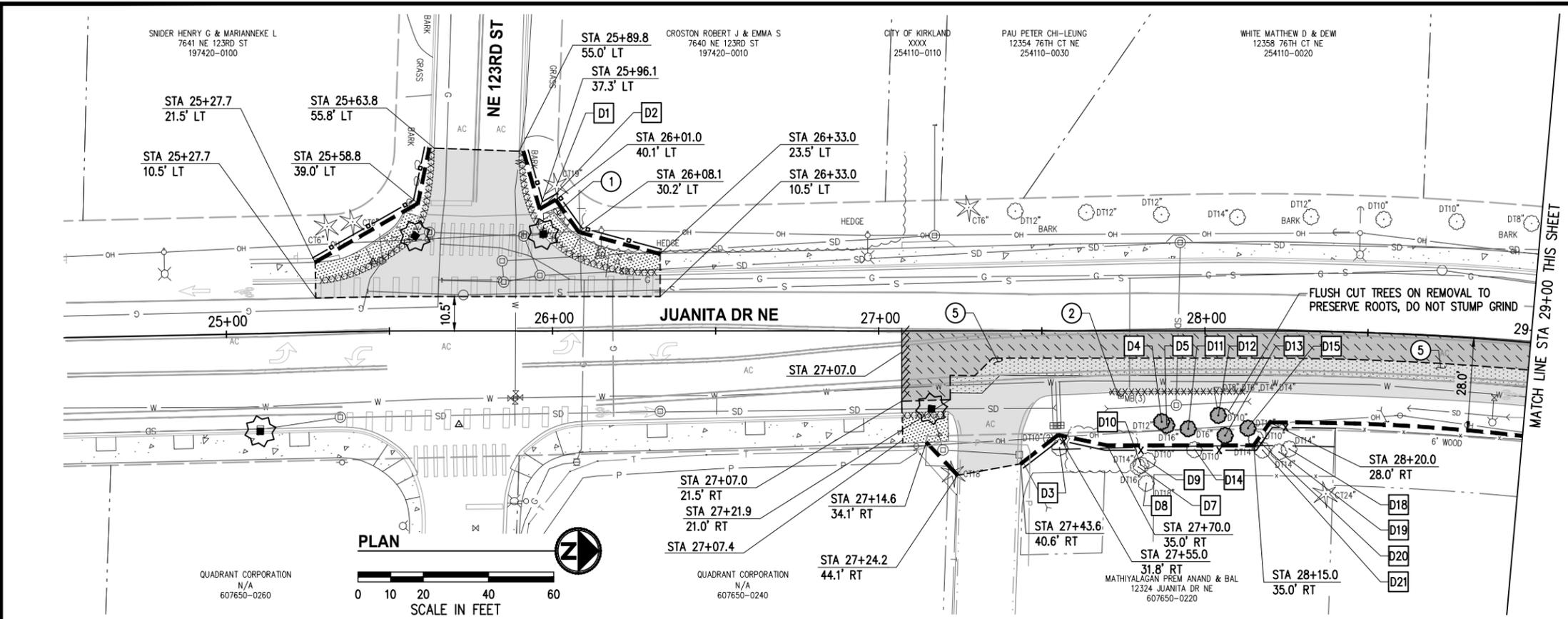
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JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS
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- ### GENERAL NOTES
1. PRESERVE AND PROTECT ALL UTILITIES, STRUCTURES, AND VEGETATION NOT CALLED OUT FOR REMOVAL.
 2. FOR TREATMENT OF EXISTING ITEMS NOT NOTED IN THESE SITE PREP PLANS, REFER TO THE OTHER PLAN SUBSETS PROVIDED HEREIN.
 3. ITEMS NOTED FOR REMOVAL SHALL BE DEMOLISHED, HAULED, AND DISPOSED OF AT CONTRACTOR'S EXPENSE, UNLESS NOTED OTHERWISE.
 4. MAINTAIN DRIVEWAY AND BUSINESS ACCESS AT ALL TIMES, SEE SPECIAL PROVISIONS.
 5. ASPHALT & CONCRETE REMOVAL LIMITS AND CLEARING & GRUBBING LIMITS SHOWN ARE APPROXIMATE. LIMITS SHALL BE MARKED IN THE FIELD BY THE CONTRACTOR AND APPROVED BY THE ENGINEER PRIOR TO ANY DEMOLITION/CLEARING ACTIVITIES.
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 7. SIDEWALK SHALL BE REMOVED TO NEAREST JOINT BEYOND LIMITS SHOWN UNLESS DIRECTED OTHERWISE.
 8. EXISTING UTILITY LOCATIONS AND DEPTHS ARE BASED ON AS-BUILT RECORDS AND BEST AVAILABLE INFORMATION. CONTRACTOR SHALL FIELD VERIFY UTILITY LOCATIONS AND DEPTHS PRIOR TO CONSTRUCTION ACTIVITIES.

- ### CONSTRUCTION NOTES
- ① REMOVE RETAINING/LANDSCAPE/ROCKERY WALL IN CONFLICT WITH PROPOSED IMPROVEMENTS
 - ② REMOVE MAILBOX ASSEMBLY AND/OR CLUSTER, SALVAGE MAILBOX(ES) FOR REINSTALLATION
 - ⑤ SAWCUT AND REMOVE PAVEMENT AS REQUIRED TO CONSTRUCT STORM, SEWER, & WATER IMPROVEMENTS; SEE STORM SHEETS 17-29 AND WATER & SEWER SHEETS 131-159
 - ⑥ SAWCUT ALONG EXISTING FOG LINE
 - ⑧ CLEAR & GRUB AS REQUIRED TO INSTALL WATER IMPROVEMENTS, SEE WATER SHEETS 131-159

- ### LEGEND
- RIGHT OF WAY
 - TEMPORARY CONSTRUCTION EASEMENT
 - [Pattern] REMOVE CEMENT CONC SIDEWALK
 - [Pattern] REMOVE ASPHALT CONC AND UNDERLYING CEMENT CONC PAVEMENT WHERE PRESENT - PAID UNDER SCHED A
 - [Pattern] REMOVE ASPHALT CONC AND UNDERLYING CEMENT CONC PAVEMENT WHERE PRESENT - PAID UNDER SCHED B
 - [Pattern] 2" DEPTH PAVEMENT PLANING - PAID UNDER SCHED A
 - [Pattern] 2" DEPTH PAVEMENT PLANING - PAID UNDER SCHED B
 - XXXXXXXXXXXX REMOVE CURB & GUTTER OR EXTRUDED CURB
 - - - FULL DEPTH SAWCUT, LOCATIONS NOTED
 - [Pattern] BUTT JOINT
 - [Pattern] CLEARING & GRUBBING LIMITS, LOCATIONS NOTED
 - - - HIGH VISIBILITY SILT FENCE PER C.O.K. STD PLAN CK-E.03
 - - - HIGH VISIBILITY FENCE
 - - - TREE PROTECTION FENCE PER C.O.K. STD PLAN CK-R.49
 - [Symbol] INLET PROTECTION PER C.O.K. STD PLAN CK-E.11
 - [Symbol] REMOVE CONIFEROUS / DECIDUOUS TREE
 - [Symbol] TREE ID, SEE ARBORIST REPORT IN APPENDIX OF SPECS

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Know what's below. Call 811 before you dig.



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BID DOCUMENT

CITY OF KIRKLAND WASHINGTON

CITY OF KIRKLAND
DEPARTMENT OF PUBLIC WORKS
123 FIFTH AVENUE KIRKLAND, WA 98003
(425) 587-3800 www.kirklandwa.gov

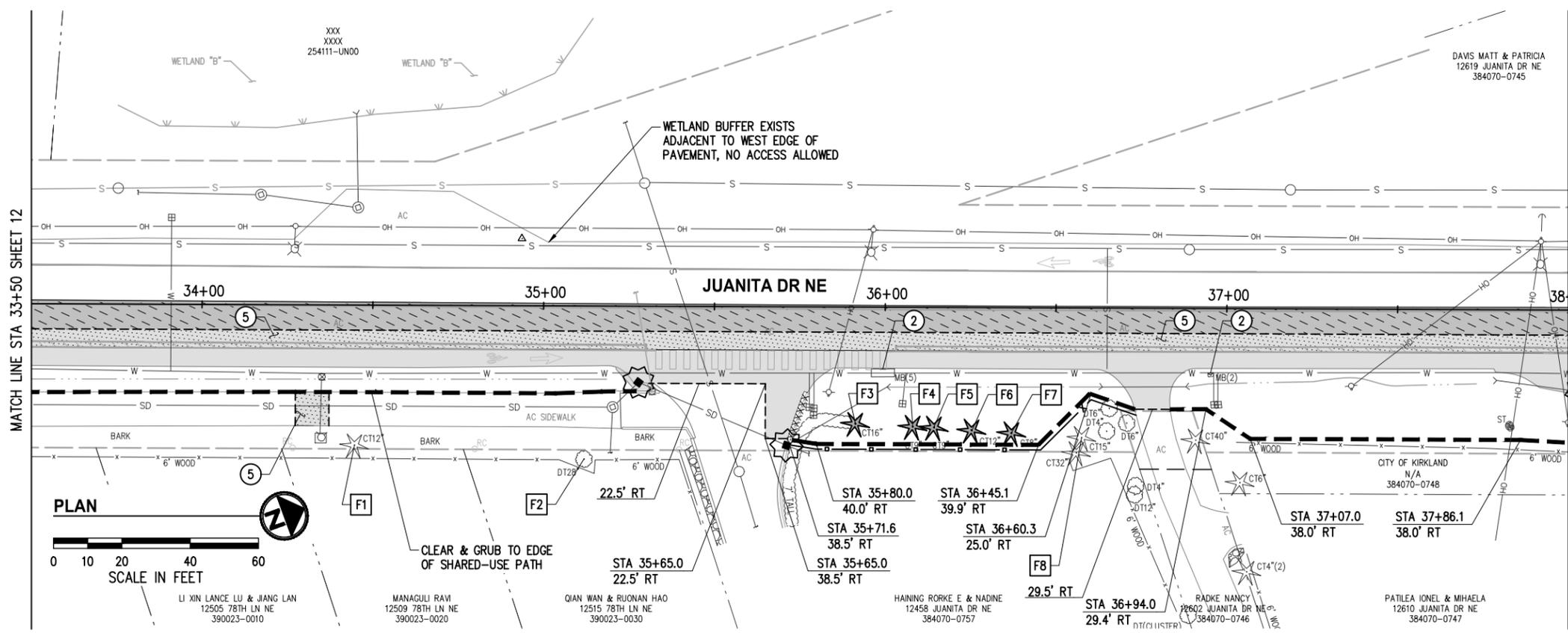
**JUANITA DRIVE MULTIMODAL,
INTERSECTION & SAFETY IMPROVEMENTS**

67\$ 72 67\$

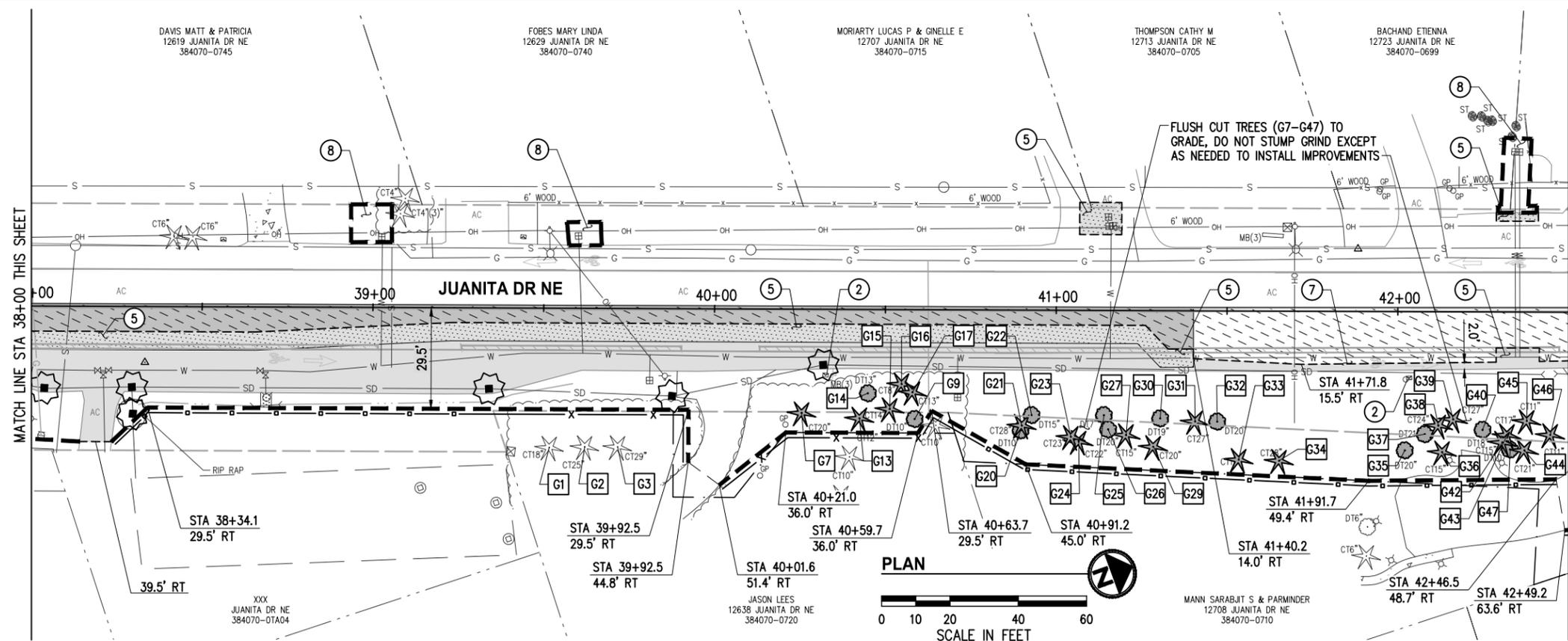
SITE PREPARATION & TESC PLAN

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- ### GENERAL NOTES
1. PRESERVE AND PROTECT ALL UTILITIES, STRUCTURES, AND VEGETATION NOT CALLED OUT FOR REMOVAL.
 2. FOR TREATMENT OF EXISTING ITEMS NOT NOTED IN THESE SITE PREP PLANS, REFER TO THE OTHER PLAN SUBSETS PROVIDED HEREIN.
 3. ITEMS NOTED FOR REMOVAL SHALL BE DEMOLISHED, HAULED, AND DISPOSED OF AT CONTRACTOR'S EXPENSE, UNLESS NOTED OTHERWISE.
 4. MAINTAIN DRIVEWAY AND BUSINESS ACCESS AT ALL TIMES, SEE SPECIAL PROVISIONS.
 5. ASPHALT & CONCRETE REMOVAL LIMITS AND CLEARING & GRUBBING LIMITS SHOWN ARE APPROXIMATE. LIMITS SHALL BE MARKED IN THE FIELD BY THE CONTRACTOR AND APPROVED BY THE ENGINEER PRIOR TO ANY DEMOLITION/CLEARING ACTIVITIES.
 6. ADJUST SAWCUT LINES AS NECESSARY TO AVOID UTILITIES.
 7. SIDEWALK SHALL BE REMOVED TO NEAREST JOINT BEYOND LIMITS SHOWN UNLESS DIRECTED OTHERWISE.
 8. EXISTING UTILITY LOCATIONS AND DEPTHS ARE BASED ON AS-BUILT RECORDS AND BEST AVAILABLE INFORMATION. CONTRACTOR SHALL FIELD VERIFY UTILITY LOCATIONS AND DEPTHS PRIOR TO CONSTRUCTION ACTIVITIES.
- ### CONSTRUCTION NOTES
- ② REMOVE MAILBOX ASSEMBLY AND/OR CLUSTER, SALVAGE MAILBOX(ES) FOR REINSTALLATION
 - ⑤ SAWCUT AND REMOVE PAVEMENT AS REQUIRED TO CONSTRUCT STORM, SEWER, & WATER IMPROVEMENTS; SEE STORM SHEETS 17-29 AND WATER & SEWER SHEETS 131-159
 - ⑦ SAWCUT 2' FROM EXISTING EDGE OF PAVEMENT
 - ⑧ CLEAR & GRUB AS REQUIRED TO INSTALL WATER IMPROVEMENTS, SEE WATER SHEETS 131-159



- ### LEGEND
- - - - - RIGHT OF WAY
 - - - - - TEMPORARY CONSTRUCTION EASEMENT
 - REMOVE CEMENT CONC SIDEWALK
 - REMOVE ASPHALT CONC AND UNDERLYING CEMENT CONC PAVEMENT WHERE PRESENT - PAID UNDER SCHED A
 - REMOVE ASPHALT CONC AND UNDERLYING CEMENT CONC PAVEMENT WHERE PRESENT - PAID UNDER SCHED B
 - 2" DEPTH PAVEMENT PLANING - PAID UNDER SCHED A
 - 2" DEPTH PAVEMENT PLANING - PAID UNDER SCHED B
 - XXXXXXXXXXXX REMOVE CURB & GUTTER OR EXTRUDED CURB
 - - - - - FULL DEPTH SAWCUT, LOCATIONS NOTED
 - ////// BUTT JOINT
 - - - - - CLEARING & GRUBBING LIMITS, LOCATIONS NOTED
 - HIGH VISIBILITY SILT FENCE PER C.O.K. STD PLAN CK-E.03
 - HIGH VISIBILITY FENCE
 - TREE PROTECTION FENCE PER C.O.K. STD PLAN CK-R.49
 - INLET PROTECTION PER C.O.K. STD PLAN CK-E.11
 - ★ / ○ REMOVE CONIFEROUS / DECIDUOUS TREE
 - # TREE ID, SEE ARBORIST REPORT IN APPENDIX OF SPECS

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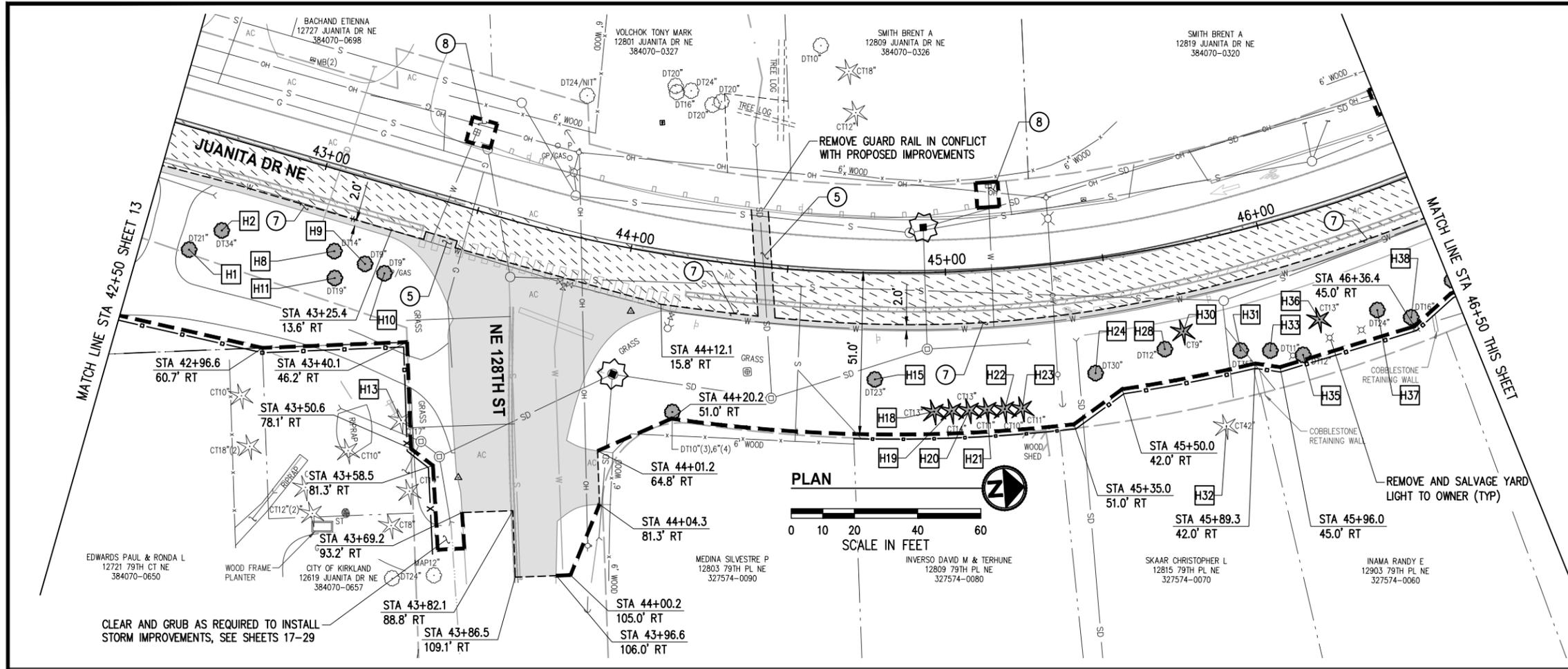
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(425) 587-3800 www.kirklandwa.gov

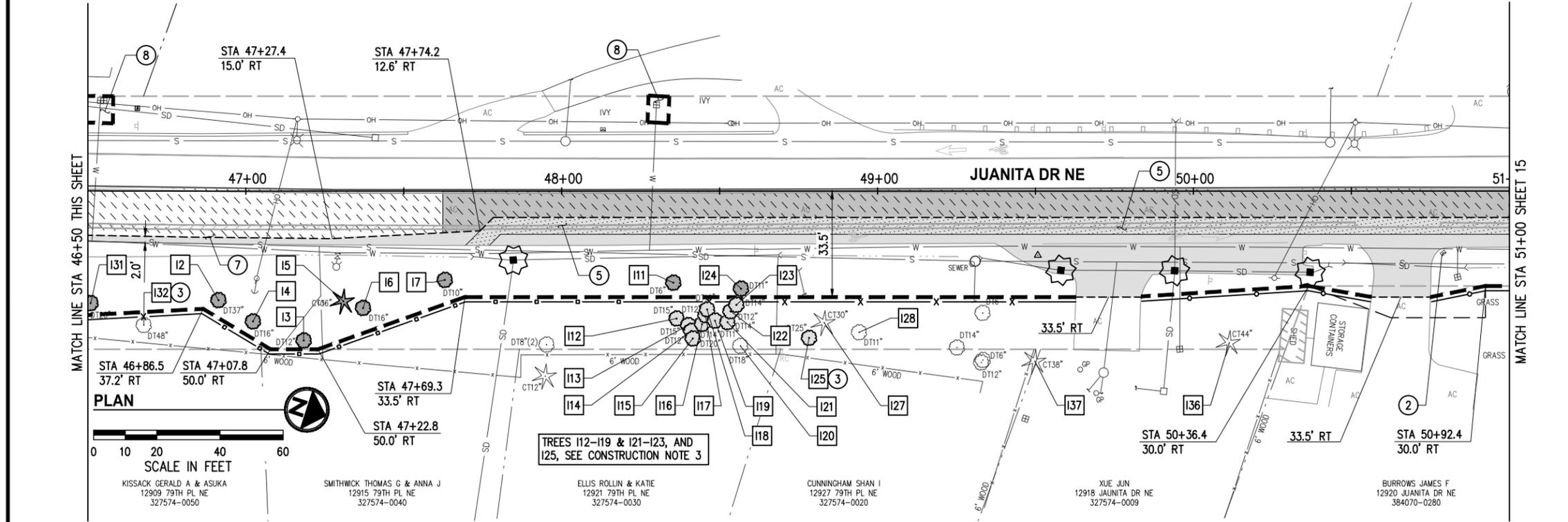
JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS		REFERENCE SHEET NO.
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- ### CONSTRUCTION NOTES
- REMOVE MAILBOX ASSEMBLY AND/OR CLUSTER, SALVAGE MAILBOX(ES) FOR REINSTALLATION
 - CONSTRUCT WILDLIFE SNAG; SEE SPECIAL PROVISIONS
 - SAWCUT AND REMOVE PAVEMENT AS REQUIRED TO CONSTRUCT STORM, SEWER, & WATER IMPROVEMENTS; SEE STORM SHEETS 17-29 AND WATER & SEWER SHEETS 131-159
 - SAWCUT 2' FROM EXISTING EDGE OF PAVEMENT
 - CLEAR & GRUB AS REQUIRED TO INSTALL WATER IMPROVEMENTS, SEE WATER SHEETS 131-159



- ### LEGEND
- RIGHT OF WAY
 - - - TEMPORARY CONSTRUCTION EASEMENT
 - [Hatched Box] REMOVE CEMENT CONC SIDEWALK
 - [Dotted Box] REMOVE ASPHALT CONC AND UNDERLYING CEMENT CONC PAVEMENT WHERE PRESENT - PAID UNDER SCHED A
 - [Cross-hatched Box] REMOVE ASPHALT CONC AND UNDERLYING CEMENT CONC PAVEMENT WHERE PRESENT - PAID UNDER SCHED B
 - [Diagonal Lines Box] 2" DEPTH PAVEMENT PLANING - PAID UNDER SCHED A
 - [Diagonal Lines Box] 2" DEPTH PAVEMENT PLANING - PAID UNDER SCHED B
 - - - - - REMOVE CURB & GUTTER OR EXTRUDED CURB
 - - - - - FULL DEPTH SAWCUT, LOCATIONS NOTED
 - ||| BUTT JOINT
 - - - - - CLEARING & GRUBBING LIMITS, LOCATIONS NOTED
 - - - - - HIGH VISIBILITY SILT FENCE PER C.O.K. STD PLAN CK-E.03
 - - - - - HIGH VISIBILITY FENCE
 - - - - - TREE PROTECTION FENCE PER C.O.K. STD PLAN CK-R.49
 - [Star Symbol] INLET PROTECTION PER C.O.K. STD PLAN CK-E.11
 - [Star Symbol] REMOVE CONIFEROUS / DECIDUOUS TREE
 - [# Symbol] TREE ID, SEE ARBORIST REPORT IN APPENDIX OF SPECS

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Tacoma

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JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

67\$ 72 67\$

SITE PREPARATION & TESC PLAN

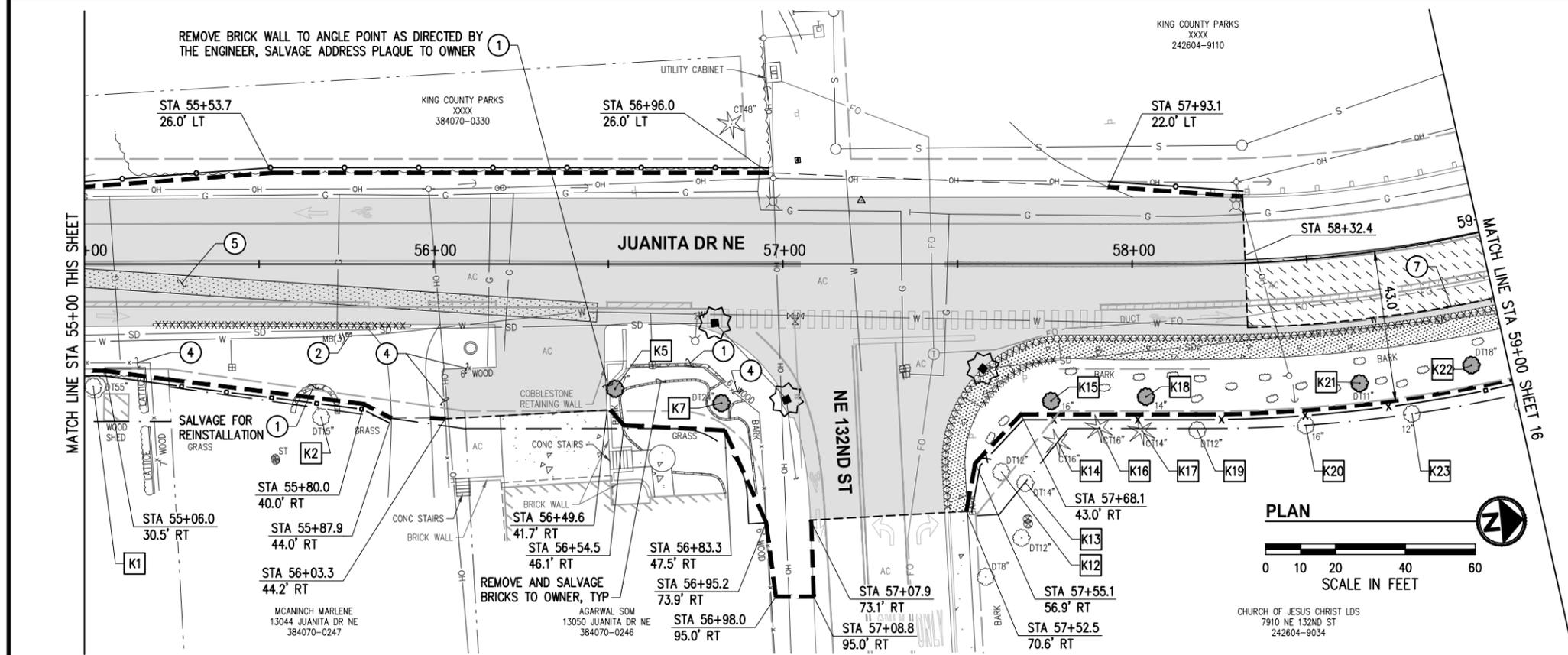
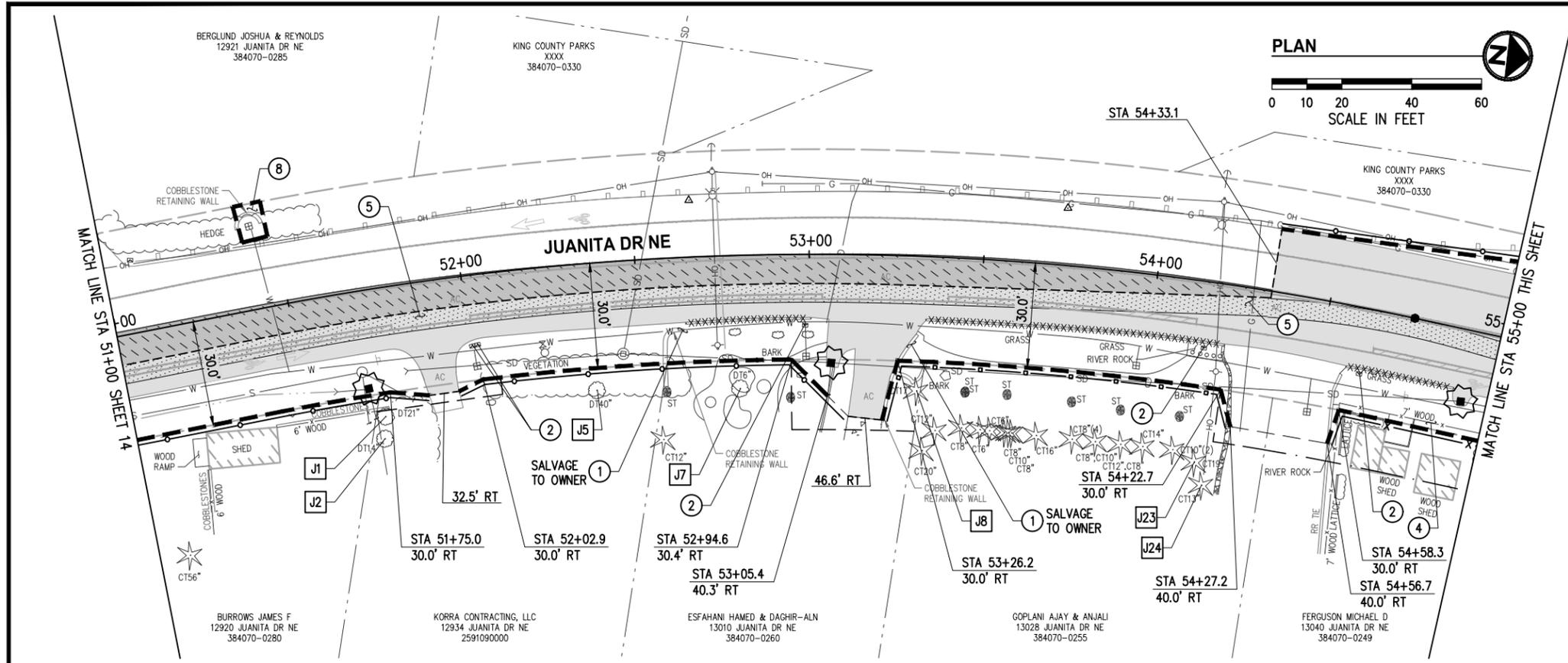
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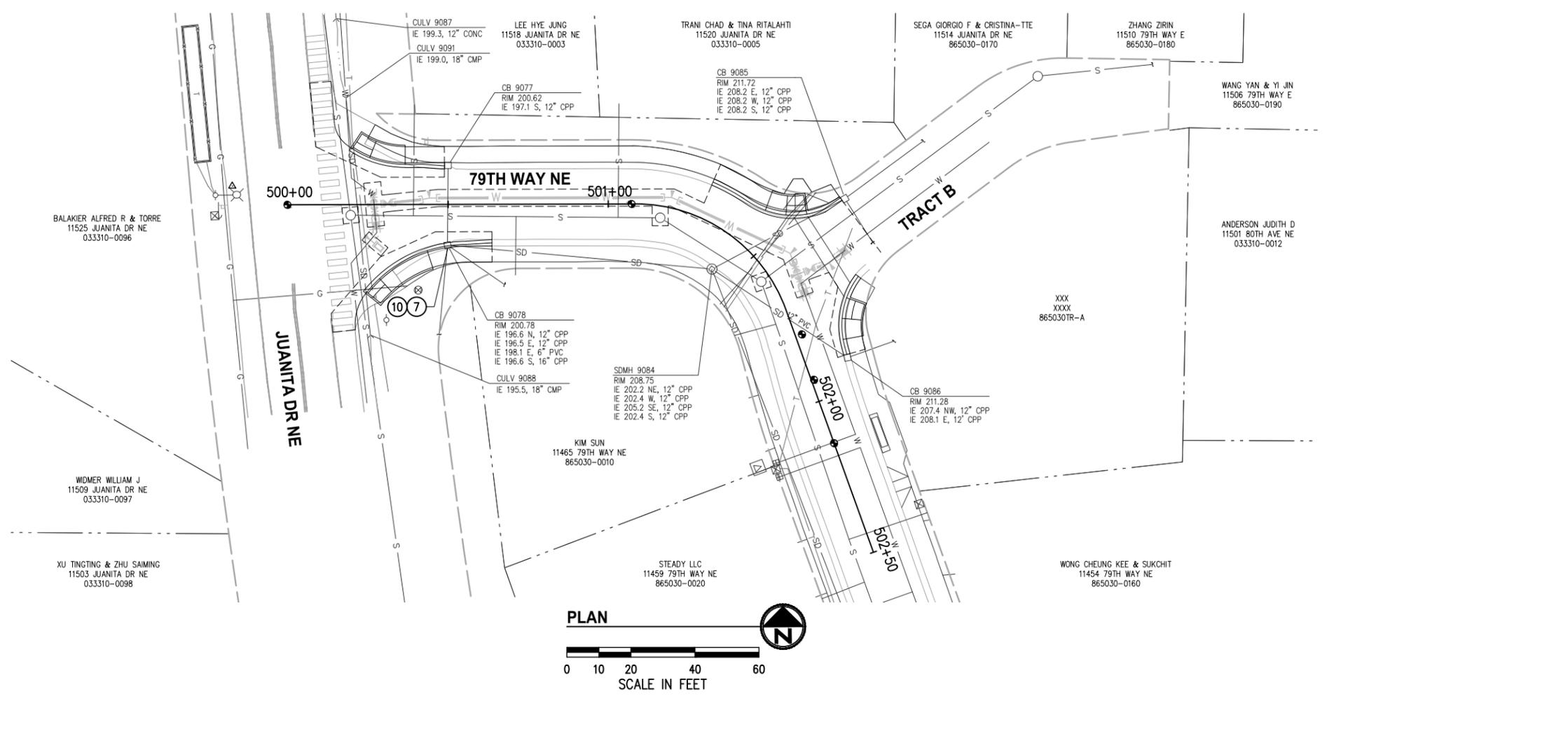
JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

67\$ 72 67\$

SITE PREPARATION & TESC PLAN

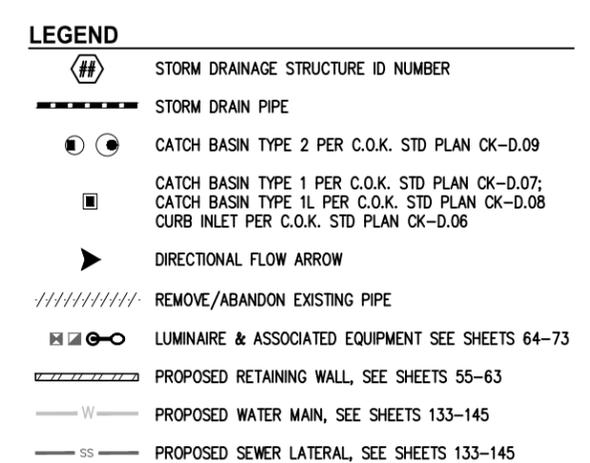
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- ### GENERAL NOTES
1. STORM TRENCH SHALL BE PER C.O.K. STD PLAN CK-D.02, EXCEPT ALL BACKFILL MATERIAL SHALL BE CSTC.
 2. CATCH BASIN LOCATIONS AND RIM ELEVATIONS ARE APPROXIMATED FINISHED GRADE AT CENTER OF STRUCTURE. MATCH FINISHED GRADES AND FLOWLINE ELEVATIONS UNLESS OTHERWISE NOTED.
 3. STORM PIPES SHALL BE PVC SDR 35, UNLESS OTHERWISE NOTED.
 4. ALL PENETRATIONS SHALL BE ACCOMPLISHED BY CORING UNLESS THE STRUCTURE HAS KNOCKOUTS OR AN EXISTING CORE DRILLED PENETRATION AT THE CORRECT INVERT ELEVATION.
 5. EXISTING UTILITY LOCATIONS AND DEPTHS ARE BASED ON AS-BUILT RECORDS AND BEST AVAILABLE INFORMATION. CONTRACTOR SHALL FIELD VERIFY UTILITY LOCATIONS AND DEPTHS PRIOR TO CONSTRUCTION ACTIVITIES.

- ### CONSTRUCTION NOTES
- 7 ADJUST EXISTING STORM DRAIN STRUCTURE TO GRADE PER C.O.K. STD PLAN CK-D.11
 - 10 INSTALL OPEN CURB FACE FRAME AND GRATE PER C.O.K. STD PLANS CK-D.15 AND CK-D.16



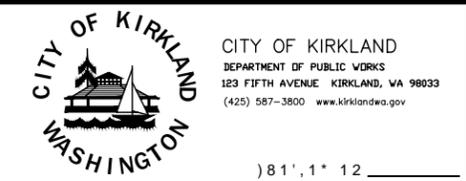
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JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

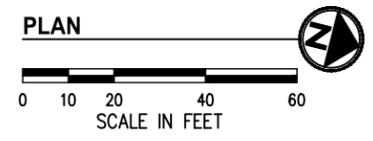
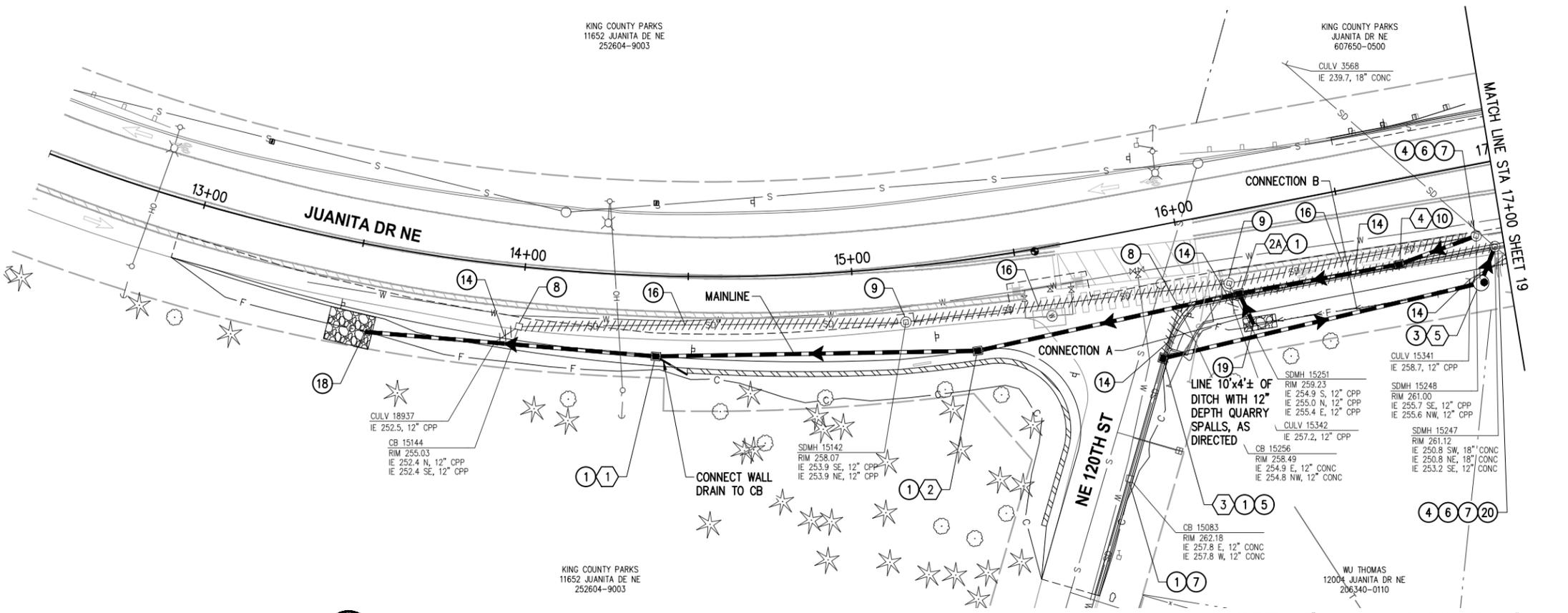
67\$ 72 67\$

STORMWATER PLAN & PROFILE

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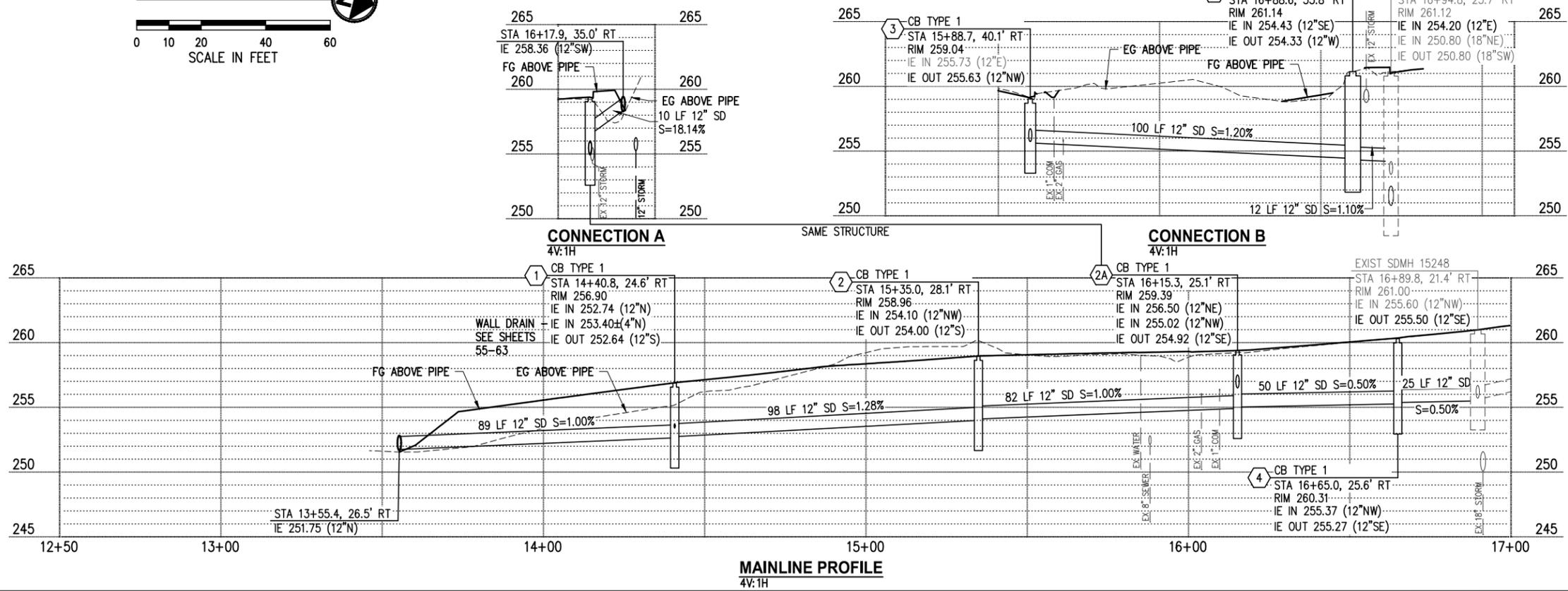
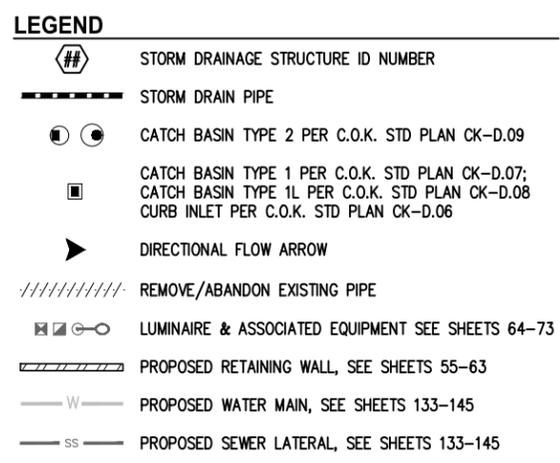
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- GENERAL NOTES**
- STORM TRENCH SHALL BE PER C.O.K. STD PLAN CK-D.02, EXCEPT ALL BACKFILL MATERIAL SHALL BE CSTC.
 - CATCH BASIN LOCATIONS AND RIM ELEVATIONS ARE APPROXIMATED FINISHED GRADE AT CENTER OF STRUCTURE. MATCH FINISHED GRADES AND FLOWLINE ELEVATIONS UNLESS OTHERWISE NOTED.
 - STORM PIPES SHALL BE PVC SDR 35, UNLESS OTHERWISE NOTED.
 - ALL PENETRATIONS SHALL BE ACCOMPLISHED BY CORING UNLESS THE STRUCTURE HAS KNOCKOUTS OR AN EXISTING CORE DRILLED PENETRATION AT THE CORRECT INVERT ELEVATION.
 - EXISTING UTILITY LOCATIONS AND DEPTHS ARE BASED ON AS-BUILT RECORDS AND BEST AVAILABLE INFORMATION. CONTRACTOR SHALL FIELD VERIFY UTILITY LOCATIONS AND DEPTHS PRIOR TO CONSTRUCTION ACTIVITIES.

- CONSTRUCTION NOTES**
- INSTALL RECTANGULAR FRAME WITH VANED GRATE PER C.O.K. STD PLAN CK-D.14. WHERE IN CURB & GUTTER FLOWLINE, INSTALL FRAME AND GRATE PER C.O.K. STD PLAN CK-D.16A
 - INSTALL ROUND SOLID LOCKING COVER PER C.O.K. STD PLAN CK-D.18
 - INSTALL ROUND SOLID LOCKING COVER PER C.O.K. STD PLAN CK-D.18A
 - CONNECT EXISTING STORM DRAIN PIPE TO NEW STRUCTURE
 - CONNECT NEW STORM DRAIN PIPE TO EXISTING STRUCTURE
 - ADJUST EXISTING STORM DRAIN STRUCTURE TO GRADE PER C.O.K. STD PLAN CK-D.11
 - REMOVE EXISTING DRAINAGE STRUCTURE
 - ABANDON EXISTING DRAINAGE STRUCTURE; REMOVE TOP 3' OF STRUCTURE, BREAK BOTTOM OF STRUCTURE, AND FILL WITH CSTC
 - INSTALL OPEN CURB FACE FRAME AND GRATE PER C.O.K. STD PLANS CK-D.15 AND CK-D.16
 - REMOVE PIPE
 - FILL PIPE WITH CDF
 - BEVEL PIPE END PER C.O.K. STD PLAN CK-D.30 & INSTALL OUTFALL PROTECTION PER C.O.K. STD PLAN CK-D.43
 - INSTALL DEBRIS BARRIER PER C.O.K. STD PLAN CK-D.27
 - EXPOSE CONE SECTION AND ROTATE EXISTING ECCENTRIC CONE SO THAT THE OPENING IS WITHIN THE PROPOSED SIDEWALK, REGROUT STRUCTURE SO IT IS WATER TIGHT, AND INSTALL NEW STEPS OR LADDER PER C.O.K. STD PLAN CK-D.09.



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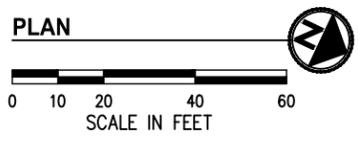
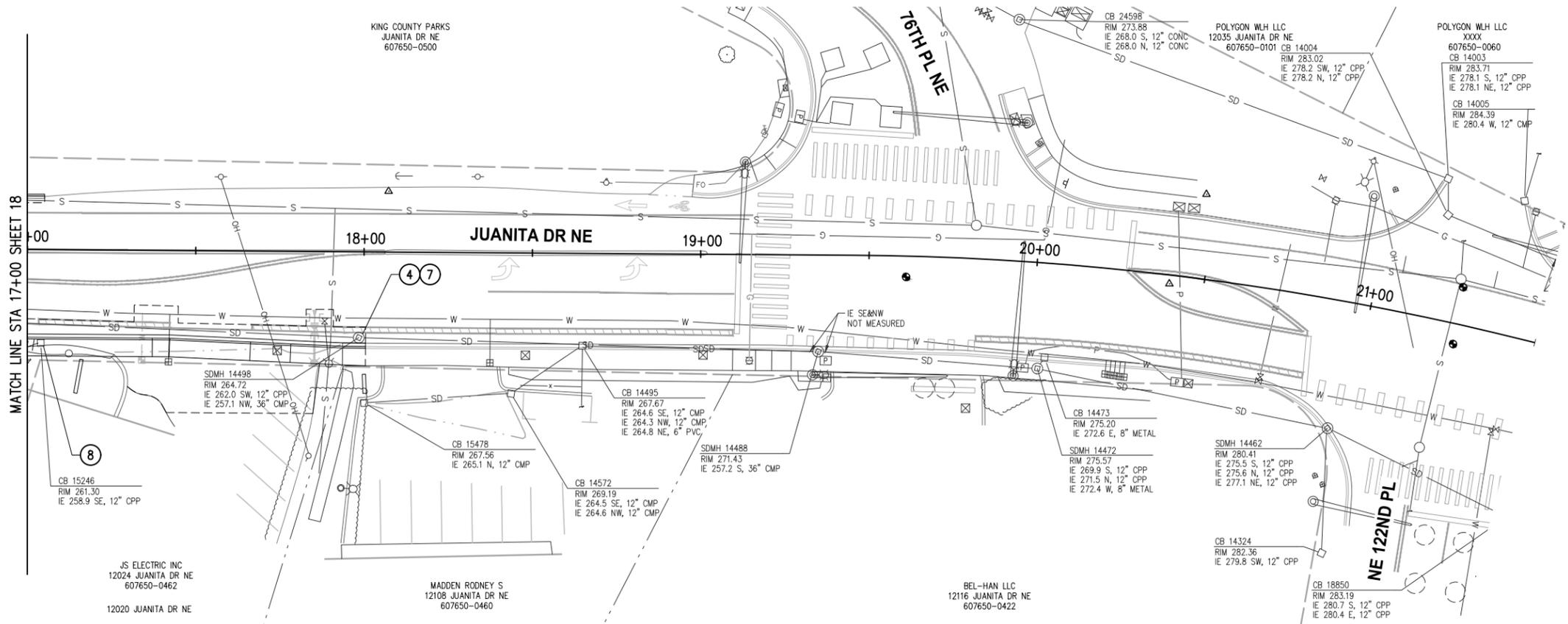
JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

67\$ 72 67\$

STORMWATER PLAN & PROFILE

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- ### CONSTRUCTION NOTES
- INSTALL ROUND SOLID LOCKING COVER PER C.O.K. STD PLAN CK-D.18A
 - ADJUST EXISTING STORM DRAIN STRUCTURE TO GRADE PER C.O.K. STD PLAN CK-D.11
 - REMOVE EXISTING DRAINAGE STRUCTURE

- ### LEGEND
- ## STORM DRAINAGE STRUCTURE ID NUMBER
 - STORM DRAIN PIPE
 - CATCH BASIN TYPE 2 PER C.O.K. STD PLAN CK-D.09
 - CATCH BASIN TYPE 1 PER C.O.K. STD PLAN CK-D.07; CATCH BASIN TYPE 1L PER C.O.K. STD PLAN CK-D.08; CURB INLET PER C.O.K. STD PLAN CK-D.06
 - DIRECTIONAL FLOW ARROW
 - REMOVE/ABANDON EXISTING PIPE
 - LUMINAIRE & ASSOCIATED EQUIPMENT SEE SHEETS 64-73
 - PROPOSED RETAINING WALL, SEE SHEETS 55-63
 - W PROPOSED WATER MAIN, SEE SHEETS 133-145
 - SS PROPOSED SEWER LATERAL, SEE SHEETS 133-145

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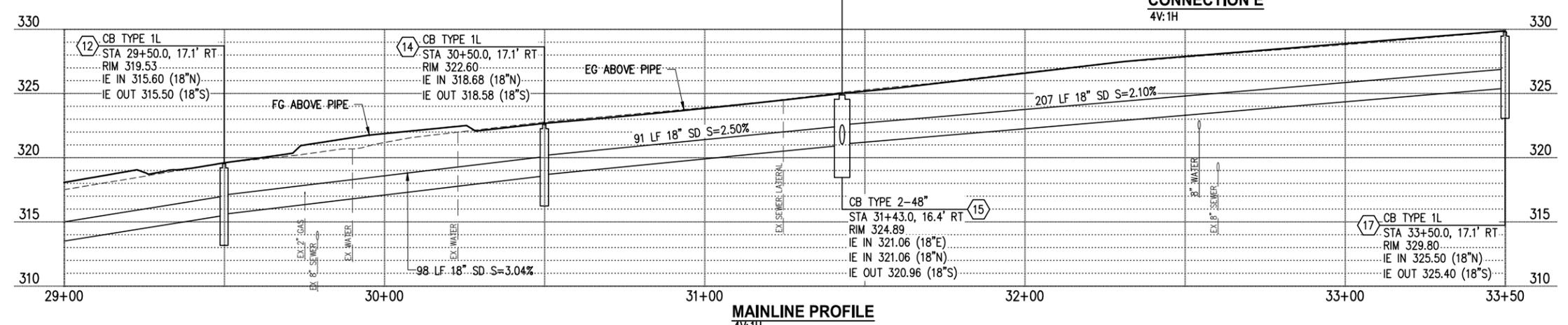
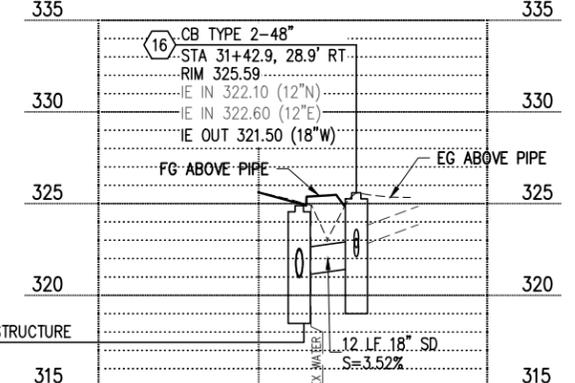
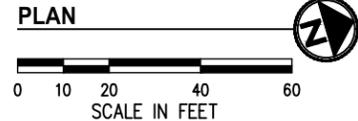
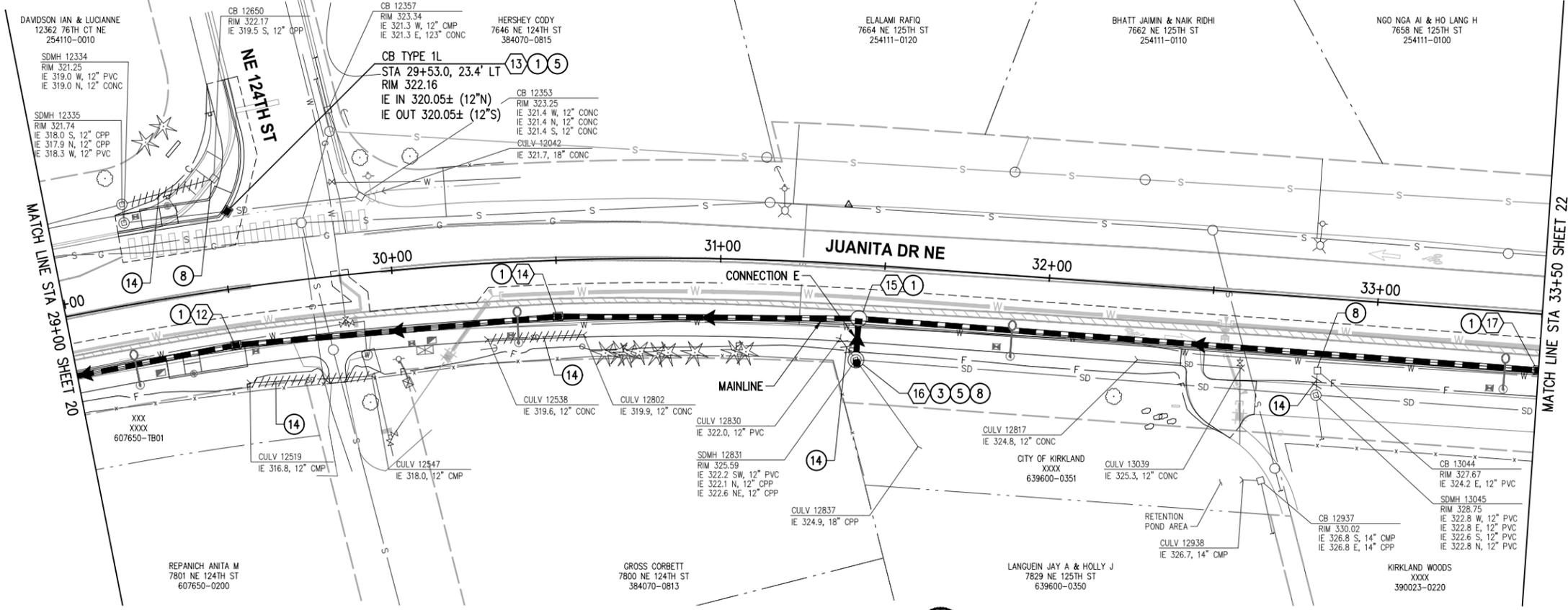
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67\$ 72 67\$

STORMWATER PLAN & PROFILE

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CONSTRUCTION NOTES

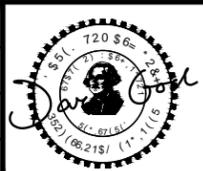
- INSTALL RECTANGULAR FRAME WITH VANED GRATE PER C.O.K. STD PLAN CK-D.14. WHERE IN CURB & GUTTER FLOWLINE, INSTALL FRAME AND GRATE PER C.O.K. STD PLAN CK-D.16A
- INSTALL ROUND SOLID LOCKING COVER PER C.O.K. STD PLAN CK-D.18
- CONNECT EXISTING STORM DRAIN PIPE TO NEW STRUCTURE
- REMOVE EXISTING DRAINAGE STRUCTURE
- REMOVE PIPE

LEGEND

- STORM DRAINAGE STRUCTURE ID NUMBER
- STORM DRAIN PIPE
- CATCH BASIN TYPE 2 PER C.O.K. STD PLAN CK-D.09
- CATCH BASIN TYPE 1 PER C.O.K. STD PLAN CK-D.07; CATCH BASIN TYPE 1L PER C.O.K. STD PLAN CK-D.08 CURB INLET PER C.O.K. STD PLAN CK-D.06
- DIRECTIONAL FLOW ARROW
- REMOVE/ABANDON EXISTING PIPE
- LUMINAIRE & ASSOCIATED EQUIPMENT SEE SHEETS 64-73
- PROPOSED RETAINING WALL, SEE SHEETS 55-63
- PROPOSED WATER MAIN, SEE SHEETS 133-145
- PROPOSED SEWER LATERAL, SEE SHEETS 133-145



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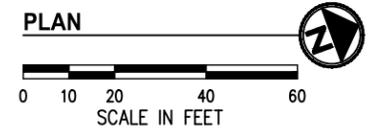
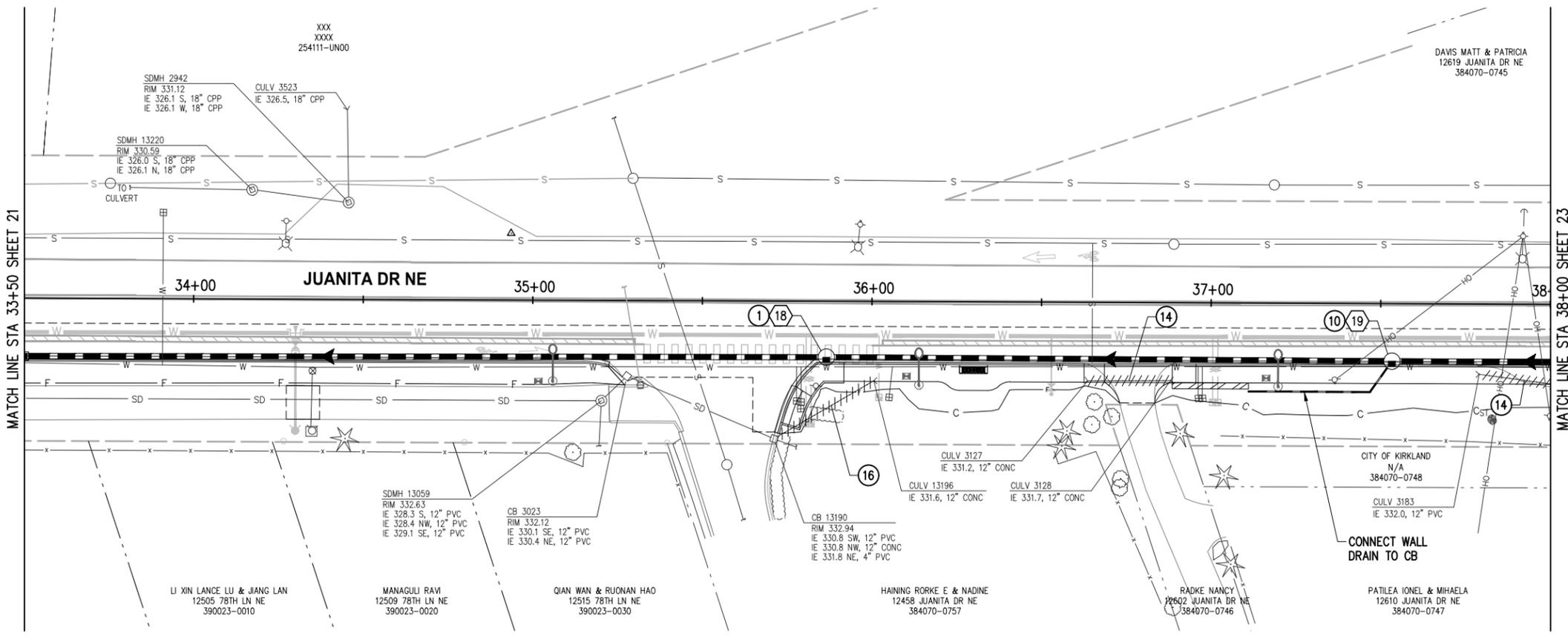
JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

67\$ 72 67\$

STORMWATER PLAN & PROFILE

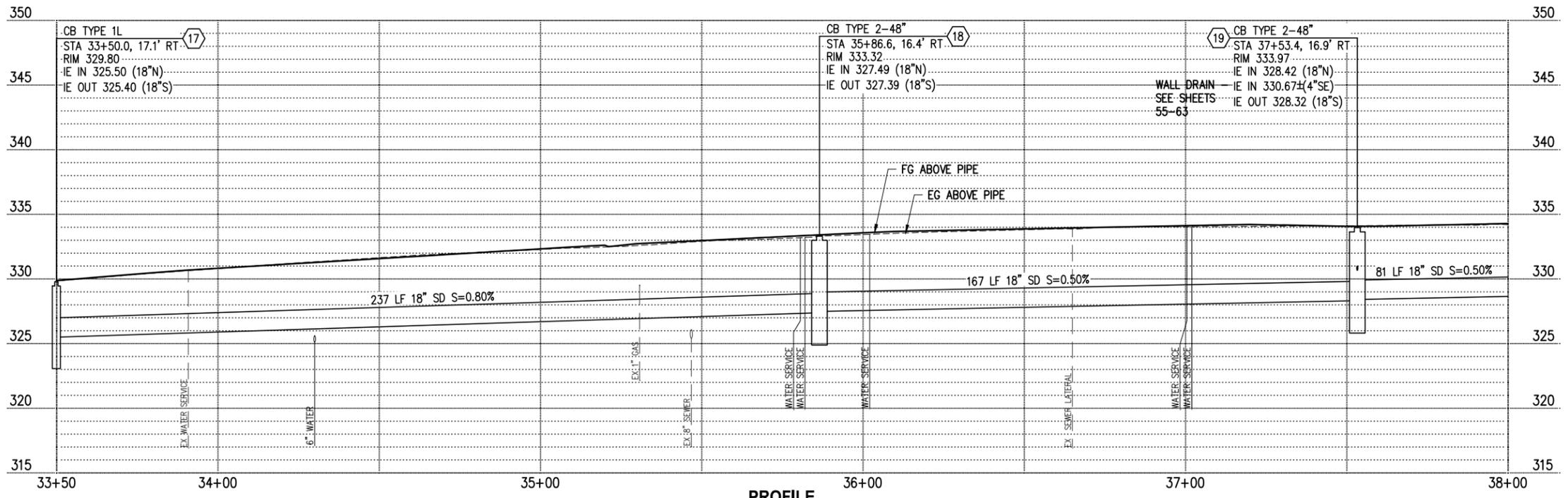
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- ### GENERAL NOTES
- STORM TRENCH SHALL BE PER C.O.K. STD PLAN CK-D.02, EXCEPT ALL BACKFILL MATERIAL SHALL BE CSTC.
 - CATCH BASIN LOCATIONS AND RIM ELEVATIONS ARE APPROXIMATED FINISHED GRADE AT CENTER OF STRUCTURE. MATCH FINISHED GRADES AND FLOWLINE ELEVATIONS UNLESS OTHERWISE NOTED.
 - STORM PIPES SHALL BE PVC SDR 35, UNLESS OTHERWISE NOTED.
 - ALL PENETRATIONS SHALL BE ACCOMPLISHED BY CORING UNLESS THE STRUCTURE HAS KNOCKOUTS OR AN EXISTING CORE DRILLED PENETRATION AT THE CORRECT INVERT ELEVATION.
 - EXISTING UTILITY LOCATIONS AND DEPTHS ARE BASED ON AS-BUILT RECORDS AND BEST AVAILABLE INFORMATION. CONTRACTOR SHALL FIELD VERIFY UTILITY LOCATIONS AND DEPTHS PRIOR TO CONSTRUCTION ACTIVITIES.

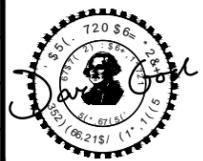
- ### CONSTRUCTION NOTES
- INSTALL RECTANGULAR FRAME WITH VANED GRATE PER C.O.K. STD PLAN CK-D.14. WHERE IN CURB & GUTTER FLOWLINE, INSTALL FRAME AND GRATE PER C.O.K. STD PLAN CK-D.16A
 - INSTALL OPEN CURB FACE FRAME AND GRATE PER C.O.K. STD PLANS CK-D.15 AND CK-D.16
 - REMOVE PIPE
 - FILL PIPE WITH CDF



- ### LEGEND
- STORM DRAINAGE STRUCTURE ID NUMBER
 - STORM DRAIN PIPE
 - CATCH BASIN TYPE 2 PER C.O.K. STD PLAN CK-D.09
 - CATCH BASIN TYPE 1 PER C.O.K. STD PLAN CK-D.07; CATCH BASIN TYPE 1L PER C.O.K. STD PLAN CK-D.08 CURB INLET PER C.O.K. STD PLAN CK-D.06
 - DIRECTIONAL FLOW ARROW
 - REMOVE/ABANDON EXISTING PIPE
 - LUMINAIRE & ASSOCIATED EQUIPMENT SEE SHEETS 64-73
 - PROPOSED RETAINING WALL, SEE SHEETS 55-63
 - PROPOSED WATER MAIN, SEE SHEETS 133-145
 - PROPOSED SEWER LATERAL, SEE SHEETS 133-145



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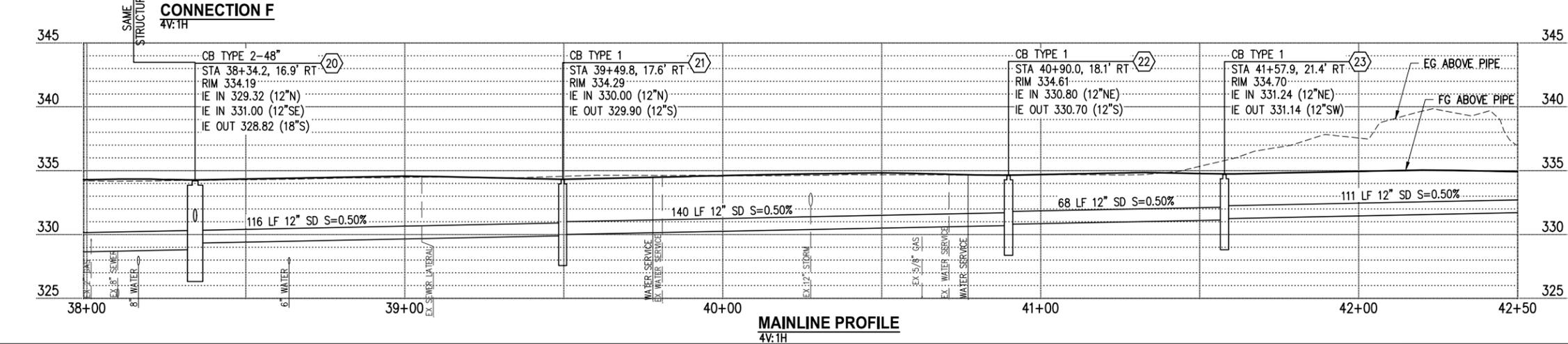
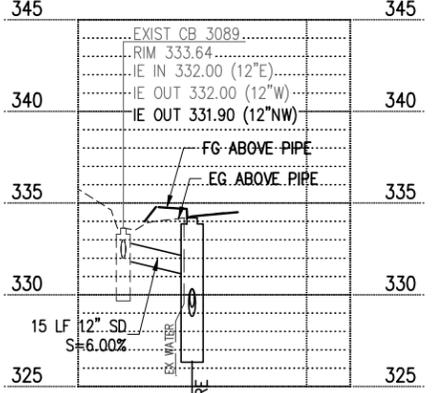
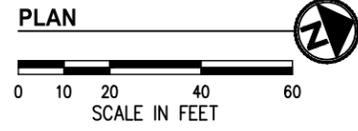
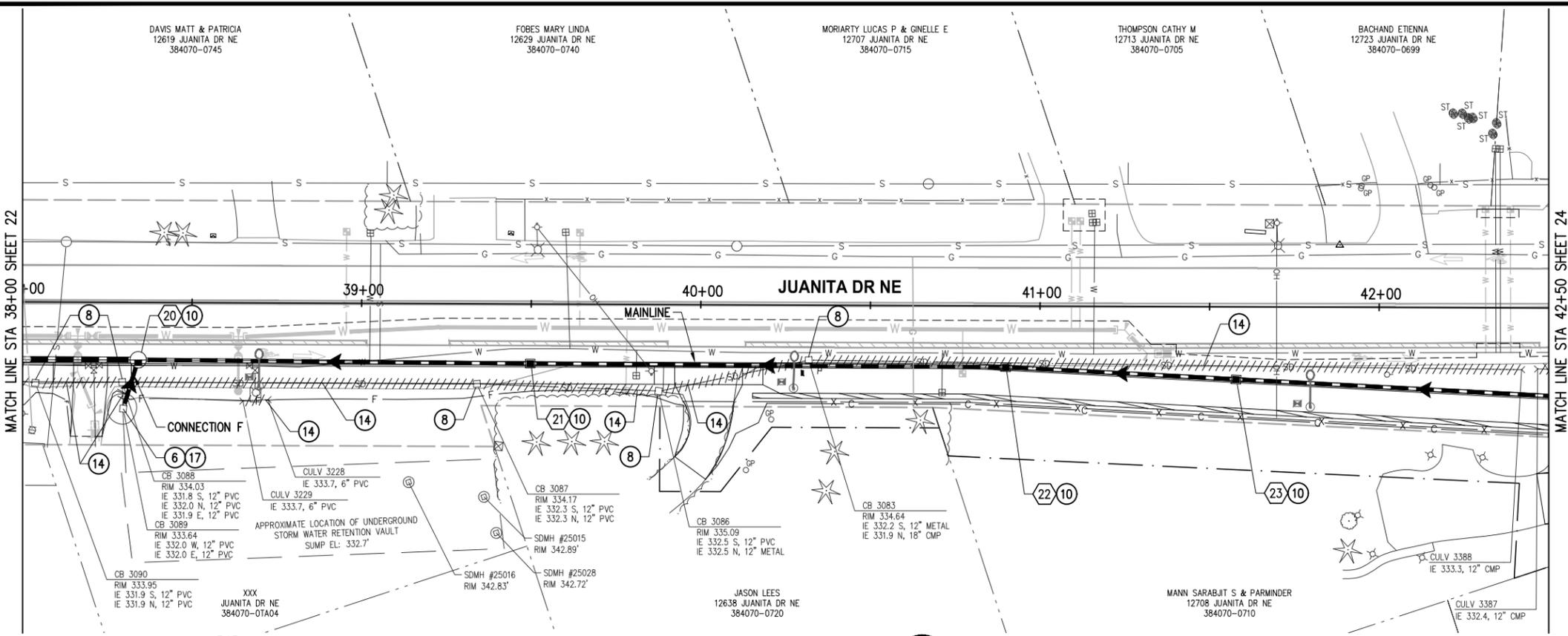
**JUANITA DRIVE MULTIMODAL,
INTERSECTION & SAFETY IMPROVEMENTS**

67\$ 72 67\$

STORMWATER PLAN & PROFILE

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 - EXISTING UTILITY LOCATIONS AND DEPTHS ARE BASED ON AS-BUILT RECORDS AND BEST AVAILABLE INFORMATION. CONTRACTOR SHALL FIELD VERIFY UTILITY LOCATIONS AND DEPTHS PRIOR TO CONSTRUCTION ACTIVITIES.

- ### CONSTRUCTION NOTES
- CONNECT NEW STORM DRAIN PIPE TO EXISTING STRUCTURE
 - REMOVE EXISTING DRAINAGE STRUCTURE
 - INSTALL OPEN CURB FACE FRAME AND GRATE PER C.O.K. STD PLANS CK-D.15 AND CK-D.16
 - REMOVE PIPE
 - INSTALL DEBRIS CAGE ON EX STRUCTURE PER C.O.K. STD PLAN CK-D.44

- ### LEGEND
- STORM DRAINAGE STRUCTURE ID NUMBER
 - STORM DRAIN PIPE
 - CATCH BASIN TYPE 2 PER C.O.K. STD PLAN CK-D.09
 - CATCH BASIN TYPE 1 PER C.O.K. STD PLAN CK-D.07; CATCH BASIN TYPE 1L PER C.O.K. STD PLAN CK-D.08 CURB INLET PER C.O.K. STD PLAN CK-D.06
 - DIRECTIONAL FLOW ARROW
 - REMOVE/ABANDON EXISTING PIPE
 - LUMINAIRE & ASSOCIATED EQUIPMENT SEE SHEETS 64-73
 - PROPOSED RETAINING WALL, SEE SHEETS 55-63
 - PROPOSED WATER MAIN, SEE SHEETS 133-145
 - PROPOSED SEWER LATERAL, SEE SHEETS 133-145

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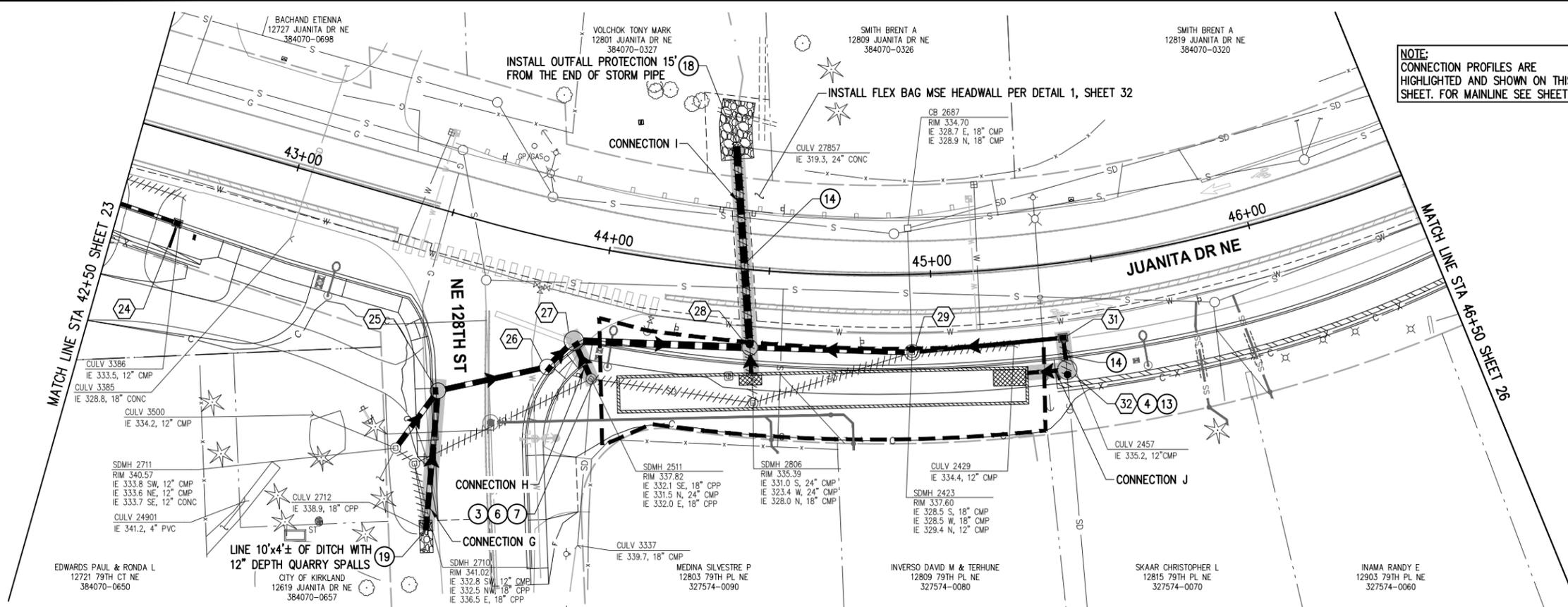
**JUANITA DRIVE MULTIMODAL,
INTERSECTION & SAFETY IMPROVEMENTS**

67\$ 72 67\$

STORMWATER PLAN & PROFILE

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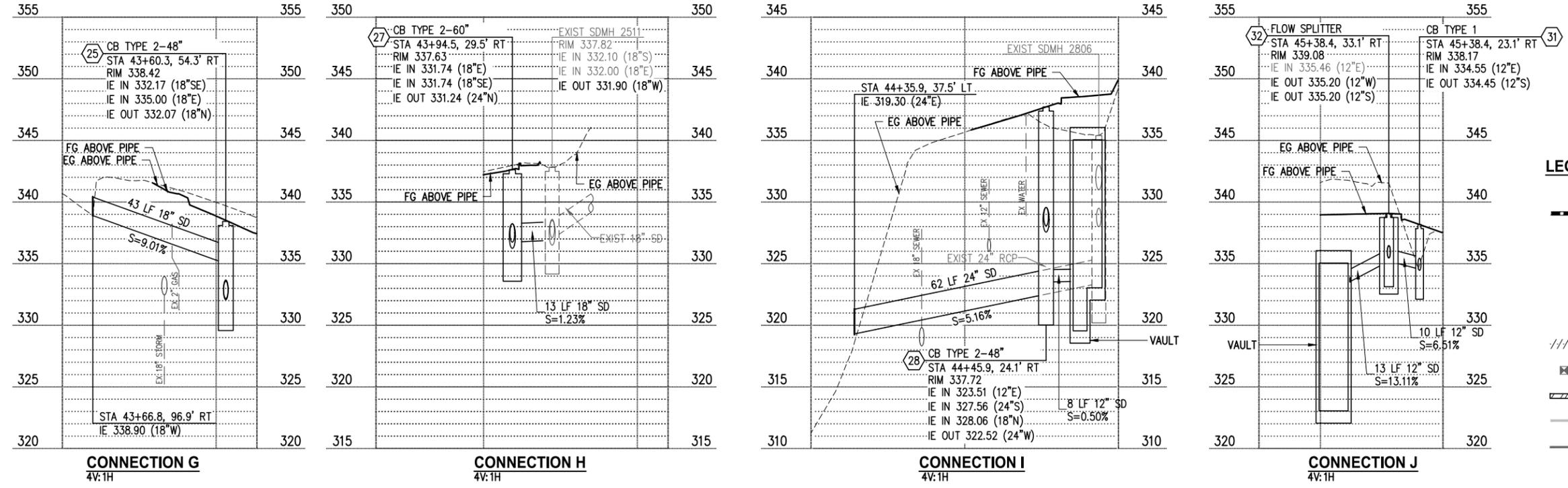
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NOTE:
CONNECTION PROFILES ARE HIGHLIGHTED AND SHOWN ON THIS SHEET. FOR MAINLINE SEE SHEET 24.

- GENERAL NOTES**
- STORM TRENCH SHALL BE PER C.O.K. STD PLAN CK-D.02, EXCEPT ALL BACKFILL MATERIAL SHALL BE CSTC.
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 - ALL PENETRATIONS SHALL BE ACCOMPLISHED BY CORING UNLESS THE STRUCTURE HAS KNOCKOUTS OR AN EXISTING CORE DRILLED PENETRATION AT THE CORRECT INVERT ELEVATION.
 - EXISTING UTILITY LOCATIONS AND DEPTHS ARE BASED ON AS-BUILT RECORDS AND BEST AVAILABLE INFORMATION. CONTRACTOR SHALL FIELD VERIFY UTILITY LOCATIONS AND DEPTHS PRIOR TO CONSTRUCTION ACTIVITIES.

- CONSTRUCTION NOTES**
- INSTALL ROUND SOLID LOCKING COVER PER C.O.K. STD PLAN CK-D.18
 - INSTALL ROUND SOLID LOCKING COVER PER C.O.K. STD PLAN CK-D.18A
 - CONNECT NEW STORM DRAIN PIPE TO EXISTING STRUCTURE
 - ADJUST EXISTING STORM DRAIN STRUCTURE TO GRADE PER C.O.K. STD PLAN CK-D.11
 - INSTALL FLOW SPLITTER PER DETAIL, SHEET 31
 - REMOVE PIPE
 - BEVEL PIPE END PER C.O.K. STD PLAN CK-D.30 & INSTALL OUTFALL PROTECTION PER C.O.K. STD PLAN CK-D.43
 - INSTALL DEBRIS BARRIER PER C.O.K. STD PLAN CK-D.27



- LEGEND**
- ## STORM DRAINAGE STRUCTURE ID NUMBER
 - STORM DRAIN PIPE
 - CATCH BASIN TYPE 2 PER C.O.K. STD PLAN CK-D.09
 - CATCH BASIN TYPE 1 PER C.O.K. STD PLAN CK-D.07;
CATCH BASIN TYPE 1L PER C.O.K. STD PLAN CK-D.08
CURB INLET PER C.O.K. STD PLAN CK-D.06
 - DIRECTIONAL FLOW ARROW
 - //// REMOVE/ABANDON EXISTING PIPE
 - LUMINAIRE & ASSOCIATED EQUIPMENT SEE SHEETS 64-73
 - ▨ PROPOSED RETAINING WALL, SEE SHEETS 55-63
 - W— PROPOSED WATER MAIN, SEE SHEETS 133-145
 - SS— PROPOSED SEWER LATERAL, SEE SHEETS 133-145

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WASHINGTON

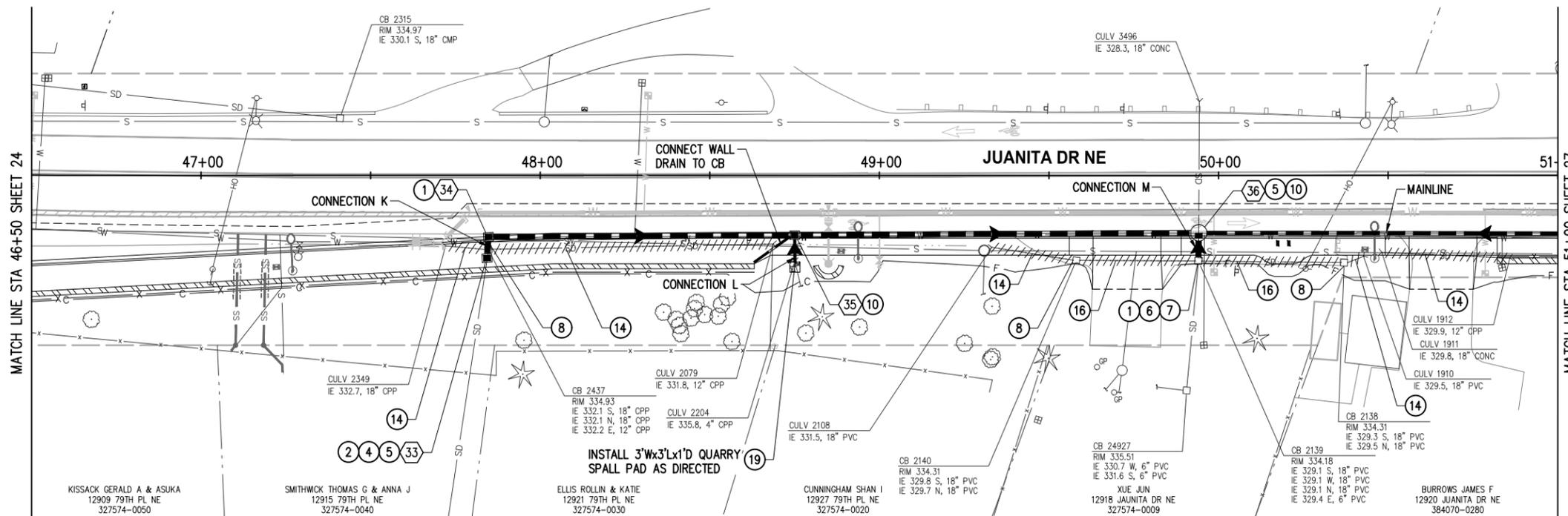
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**JUANITA DRIVE MULTIMODAL,
INTERSECTION & SAFETY IMPROVEMENTS**

STORMWATER PLAN & PROFILE

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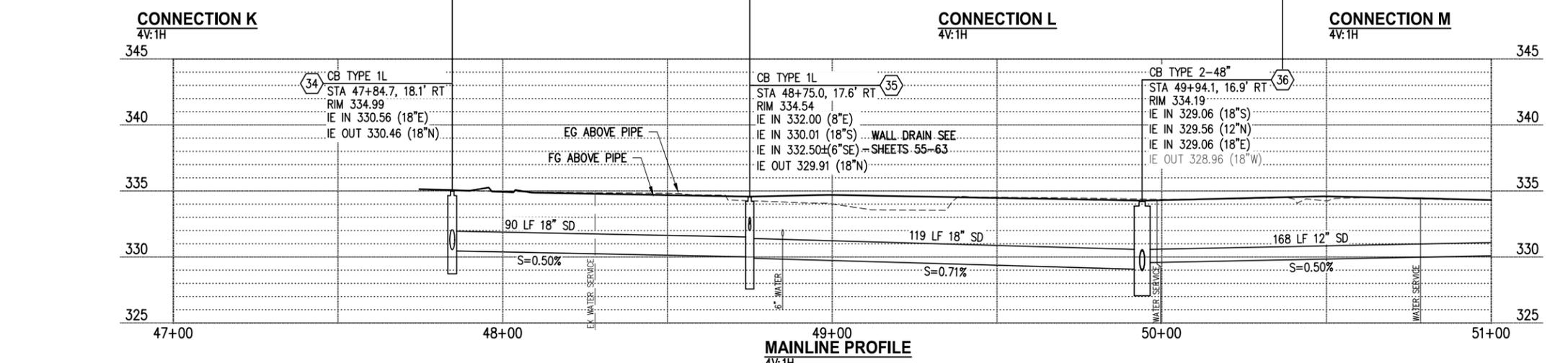
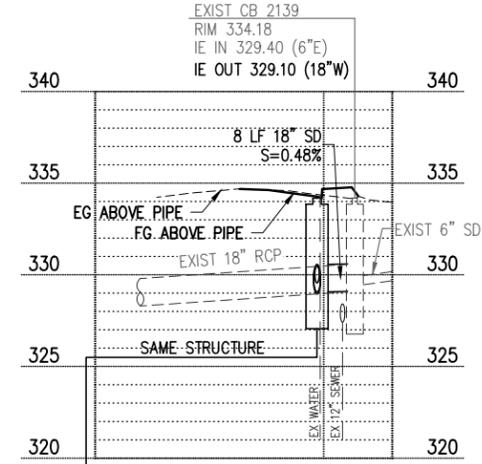
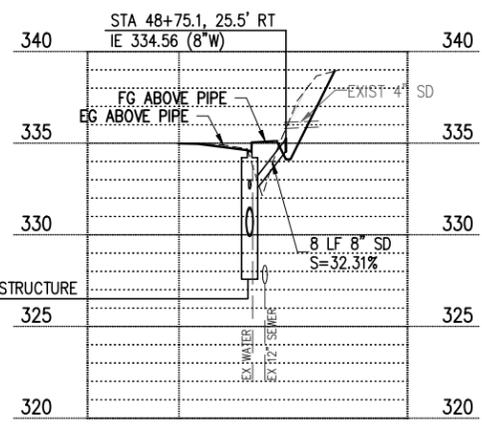
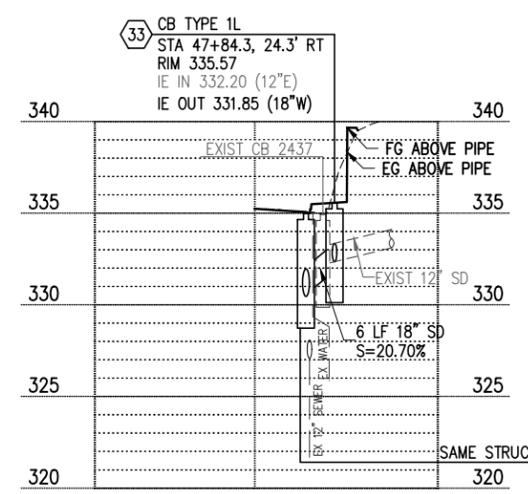
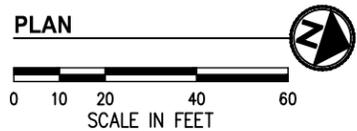


GENERAL NOTES

- STORM TRENCH SHALL BE PER C.O.K. STD PLAN CK-D.02, EXCEPT ALL BACKFILL MATERIAL SHALL BE C5TC.
- CATCH BASIN LOCATIONS AND RIM ELEVATIONS ARE APPROXIMATED FINISHED GRADE AT CENTER OF STRUCTURE. MATCH FINISHED GRADES AND FLOWLINE ELEVATIONS UNLESS OTHERWISE NOTED.
- STORM PIPES SHALL BE PVC SDR 35, UNLESS OTHERWISE NOTED.
- ALL PENETRATIONS SHALL BE ACCOMPLISHED BY CORING UNLESS THE STRUCTURE HAS KNOCKOUTS OR AN EXISTING CORE DRILLED PENETRATION AT THE CORRECT INVERT ELEVATION.
- EXISTING UTILITY LOCATIONS AND DEPTHS ARE BASED ON AS-BUILT RECORDS AND BEST AVAILABLE INFORMATION. CONTRACTOR SHALL FIELD VERIFY UTILITY LOCATIONS AND DEPTHS PRIOR TO CONSTRUCTION ACTIVITIES.

CONSTRUCTION NOTES

- INSTALL RECTANGULAR FRAME WITH VANED GRATE PER C.O.K. STD PLAN CK-D.14. WHERE IN CURB & GUTTER FLOWLINE, INSTALL FRAME AND GRATE PER C.O.K. STD PLAN CK-D.16A
- INSTALL CIRCULAR RISER AND TRANSITION FOR TYPE 1 CATCH BASIN PER C.O.K. STD PLAN CK-D.07A
- INSTALL ROUND SOLID LOCKING COVER PER C.O.K. STD PLAN CK-D.18A
- CONNECT EXISTING STORM DRAIN PIPE TO NEW STRUCTURE
- CONNECT NEW STORM DRAIN PIPE TO EXISTING STRUCTURE
- ADJUST EXISTING STORM DRAIN STRUCTURE TO GRADE PER C.O.K. STD PLAN CK-D.11
- REMOVE EXISTING DRAINAGE STRUCTURE
- INSTALL OPEN CURB FACE FRAME AND GRATE PER C.O.K. STD PLANS CK-D.15 AND CK-D.16
- REMOVE PIPE
- FILL PIPE WITH CDF
- INSTALL DEBRIS BARRIER PER C.O.K. STD PLAN CK-D.27

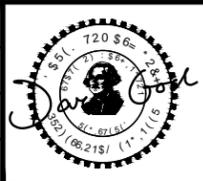


LEGEND

- #-# STORM DRAINAGE STRUCTURE ID NUMBER
- STORM DRAIN PIPE
- CATCH BASIN TYPE 2 PER C.O.K. STD PLAN CK-D.09
- CATCH BASIN TYPE 1 PER C.O.K. STD PLAN CK-D.07; CATCH BASIN TYPE 1L PER C.O.K. STD PLAN CK-D.08 CURB INLET PER C.O.K. STD PLAN CK-D.06
- DIRECTIONAL FLOW ARROW
- //// REMOVE/ABANDON EXISTING PIPE
- ⊠ LUMINAIRE & ASSOCIATED EQUIPMENT SEE SHEETS 64-73
- PROPOSED RETAINING WALL, SEE SHEETS 55-63
- W- PROPOSED WATER MAIN, SEE SHEETS 133-145
- SS- PROPOSED SEWER LATERAL, SEE SHEETS 133-145



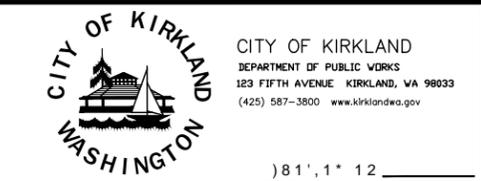
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JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

67\$ 72 67\$

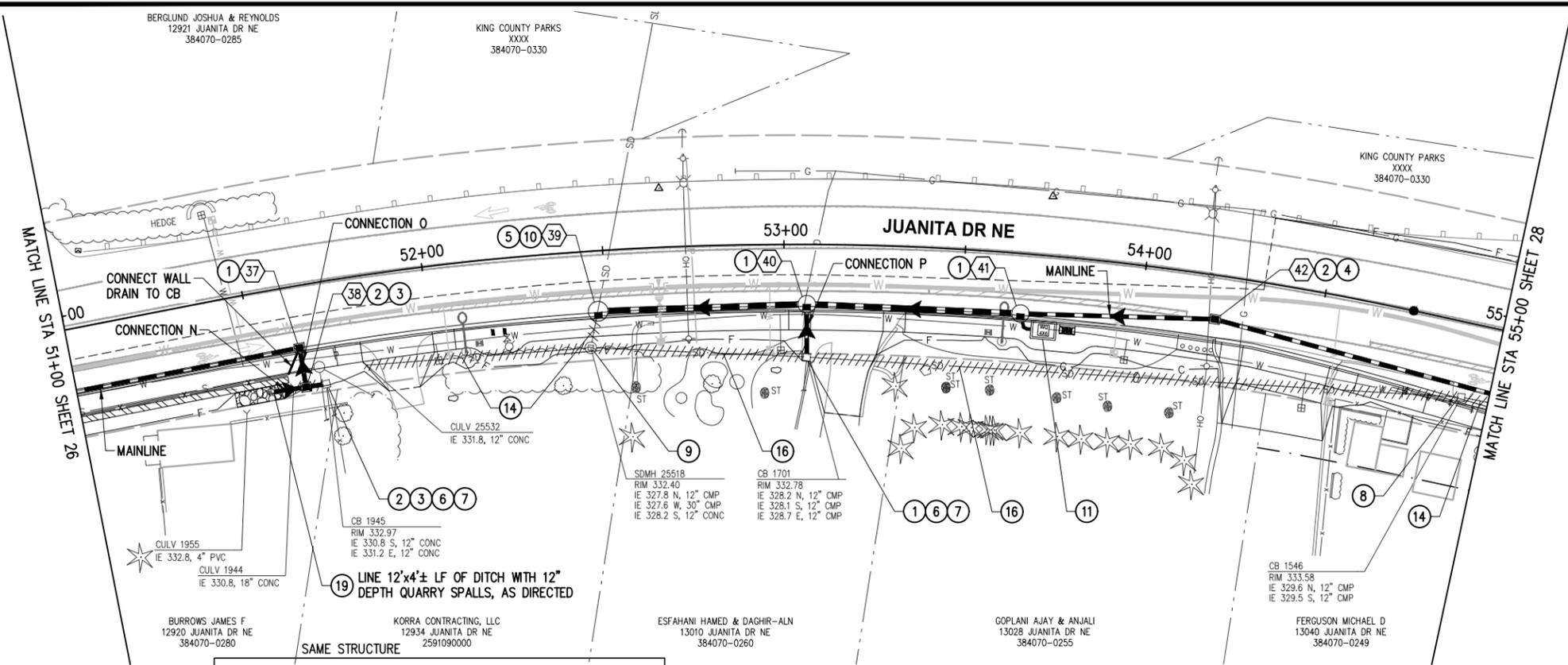
STORMWATER PLAN & PROFILE

REFERENCE SHEET NO.

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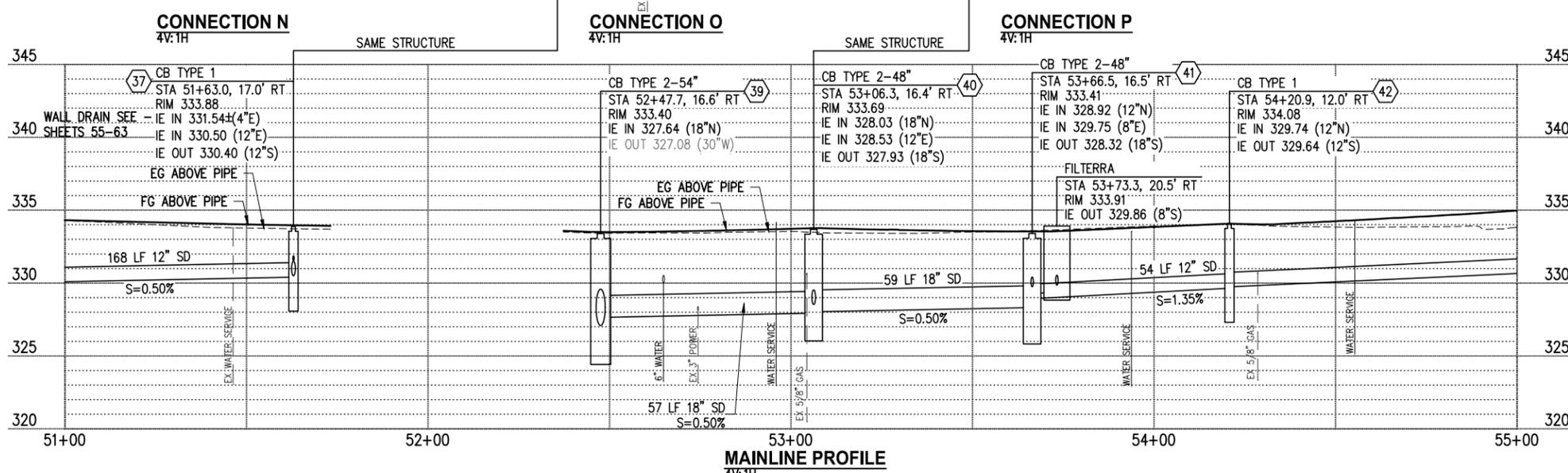
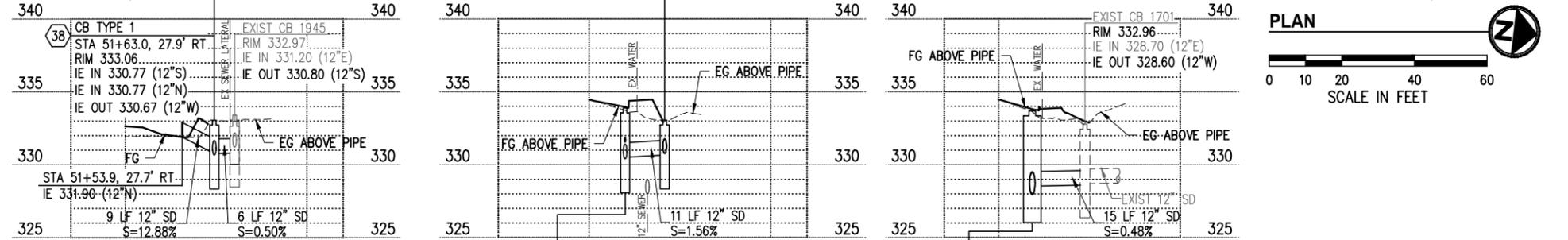
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- ### GENERAL NOTES
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 - INSTALL CIRCULAR RISER AND TRANSITION FOR TYPE 1 CATCH BASIN PER C.O.K. STD PLAN CK-D.07A
 - INSTALL ROUND SOLID LOCKING COVER PER C.O.K. STD PLAN CK-D.18
 - INSTALL ROUND SOLID LOCKING COVER PER C.O.K. STD PLAN CK-D.18A
 - CONNECT EXISTING STORM DRAIN PIPE TO NEW STRUCTURE
 - CONNECT NEW STORM DRAIN PIPE TO EXISTING STRUCTURE
 - ADJUST EXISTING STORM DRAIN STRUCTURE TO GRADE PER C.O.K. STD PLAN CK-D.11
 - REMOVE EXISTING DRAINAGE STRUCTURE
 - ABANDON EXISTING DRAINAGE STRUCTURE; REMOVE TOP 3' OF STRUCTURE, BREAK BOTTOM OF STRUCTURE, AND FILL WITH CSTC
 - INSTALL OPEN CURB FACE FRAME AND GRATE PER C.O.K. STD PLANS CK-D.15 AND CK-D.16
 - INSTALL 4'x6' FILTERRA UNIT PER DETAIL 2, SHEET 31
 - REMOVE PIPE
 - FILL PIPE WITH CDF
 - INSTALL DEBRIS BARRIER PER C.O.K. STD PLAN CK-D.27



- ### LEGEND
- ## STORM DRAINAGE STRUCTURE ID NUMBER
 - STORM DRAIN PIPE
 - CATCH BASIN TYPE 2 PER C.O.K. STD PLAN CK-D.09
 - CATCH BASIN TYPE 1 PER C.O.K. STD PLAN CK-D.07; CATCH BASIN TYPE 1L PER C.O.K. STD PLAN CK-D.08 CURB INLET PER C.O.K. STD PLAN CK-D.06
 - ▶ DIRECTIONAL FLOW ARROW
 - //// REMOVE/ABANDON EXISTING PIPE
 - ⊠ LUMINAIRE & ASSOCIATED EQUIPMENT SEE SHEETS 64-73
 - ▨ PROPOSED RETAINING WALL, SEE SHEETS 55-63
 - W PROPOSED WATER MAIN, SEE SHEETS 133-145
 - SS PROPOSED SEWER LATERAL, SEE SHEETS 133-145

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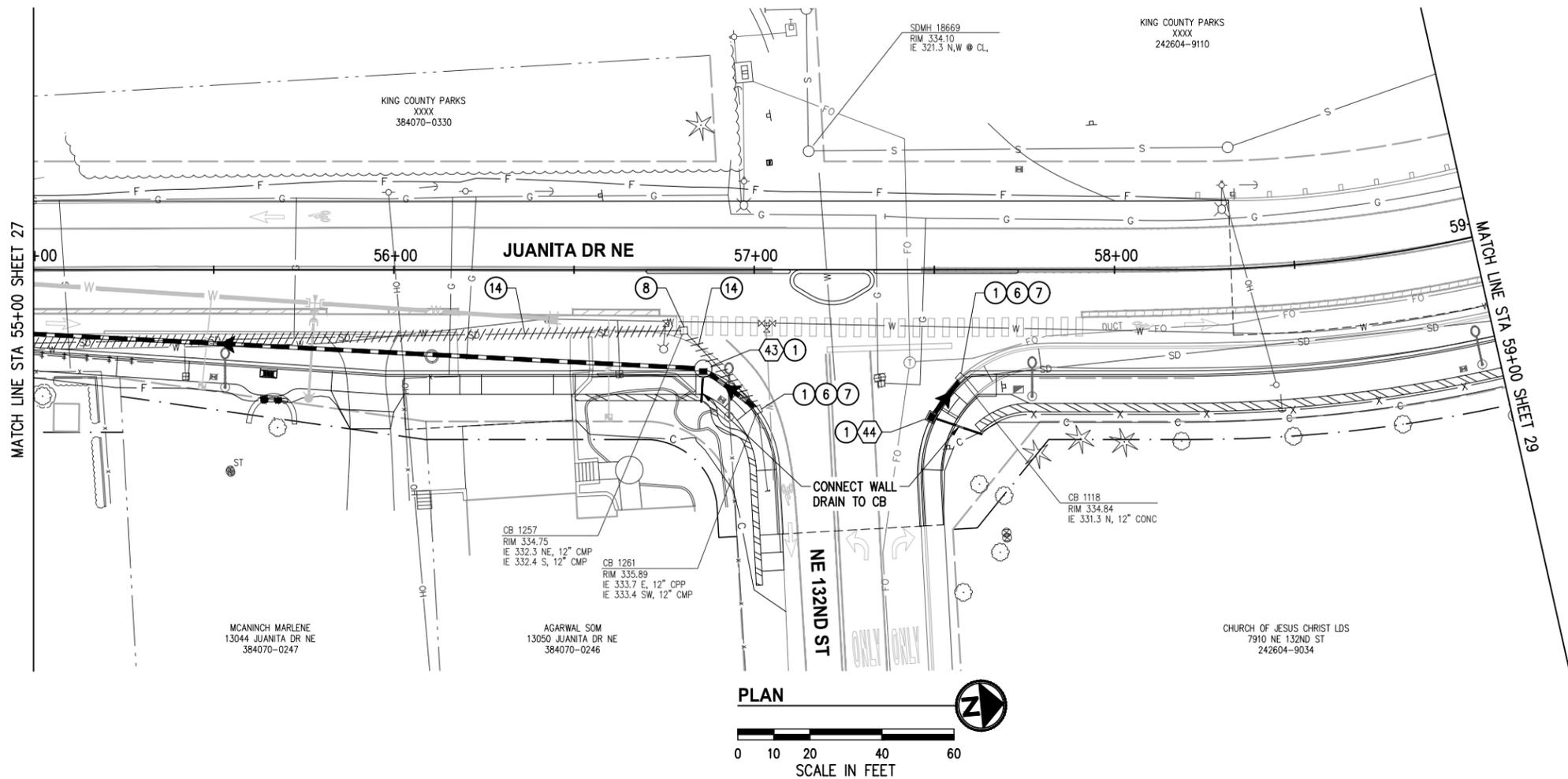
JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

67\$ 72 67\$

STORMWATER PLAN & PROFILE

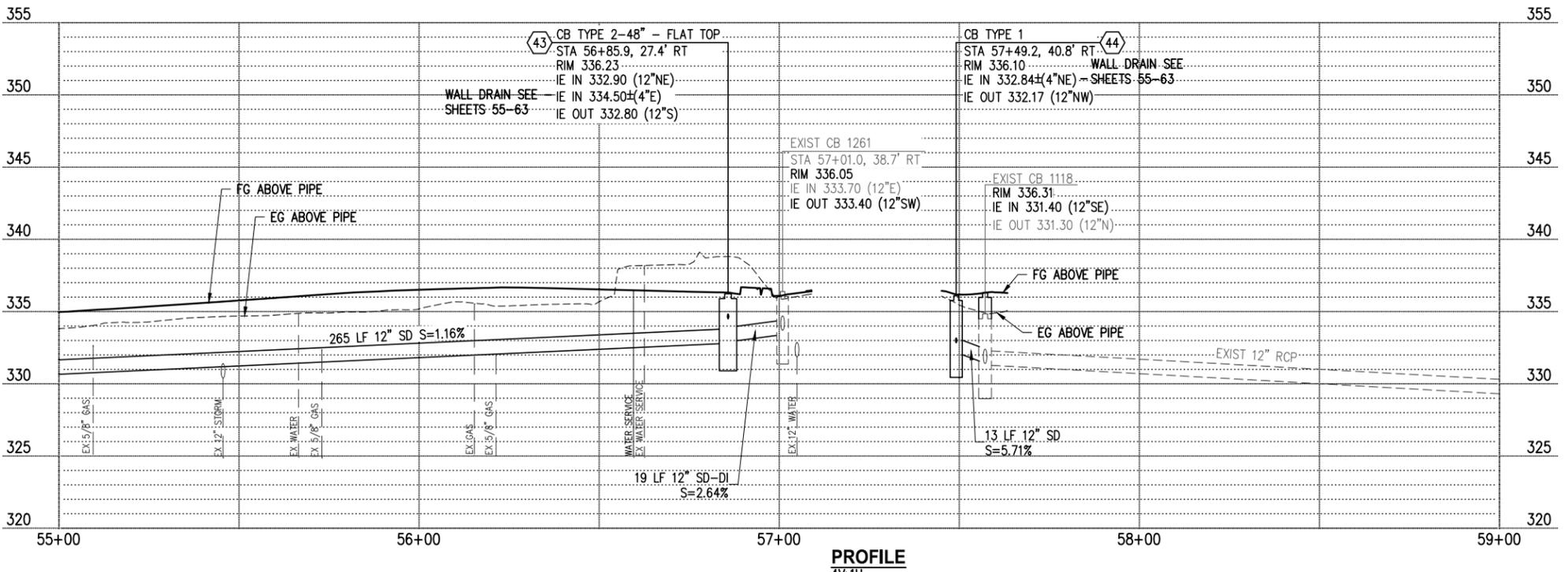
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 - EXISTING UTILITY LOCATIONS AND DEPTHS ARE BASED ON AS-BUILT RECORDS AND BEST AVAILABLE INFORMATION. CONTRACTOR SHALL FIELD VERIFY UTILITY LOCATIONS AND DEPTHS PRIOR TO CONSTRUCTION ACTIVITIES.

- ### CONSTRUCTION NOTES
- INSTALL RECTANGULAR FRAME WITH VANED GRATE PER C.O.K. STD PLAN CK-D.14. WHERE IN CURB & GUTTER FLOWLINE, INSTALL FRAME AND GRATE PER C.O.K. STD PLAN CK-D.16A
 - CONNECT NEW STORM DRAIN PIPE TO EXISTING STRUCTURE
 - ADJUST EXISTING STORM DRAIN STRUCTURE TO GRADE PER C.O.K. STD PLAN CK-D.11
 - REMOVE EXISTING DRAINAGE STRUCTURE
 - REMOVE PIPE



- ### LEGEND
- #** STORM DRAINAGE STRUCTURE ID NUMBER
 - - -** STORM DRAIN PIPE
 - ●** CATCH BASIN TYPE 2 PER C.O.K. STD PLAN CK-D.09
 - CATCH BASIN TYPE 1 PER C.O.K. STD PLAN CK-D.07; CATCH BASIN TYPE 1L PER C.O.K. STD PLAN CK-D.08 CURB INLET PER C.O.K. STD PLAN CK-D.06
 - ▶** DIRECTIONAL FLOW ARROW
 - |||||** REMOVE/ABANDON EXISTING PIPE
 - ⊠** LUMINAIRE & ASSOCIATED EQUIPMENT SEE SHEETS 64-73
 - ▨** PROPOSED RETAINING WALL, SEE SHEETS 55-63
 - W —** PROPOSED WATER MAIN, SEE SHEETS 133-145
 - SS —** PROPOSED SEWER LATERAL, SEE SHEETS 133-145

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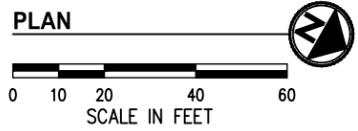
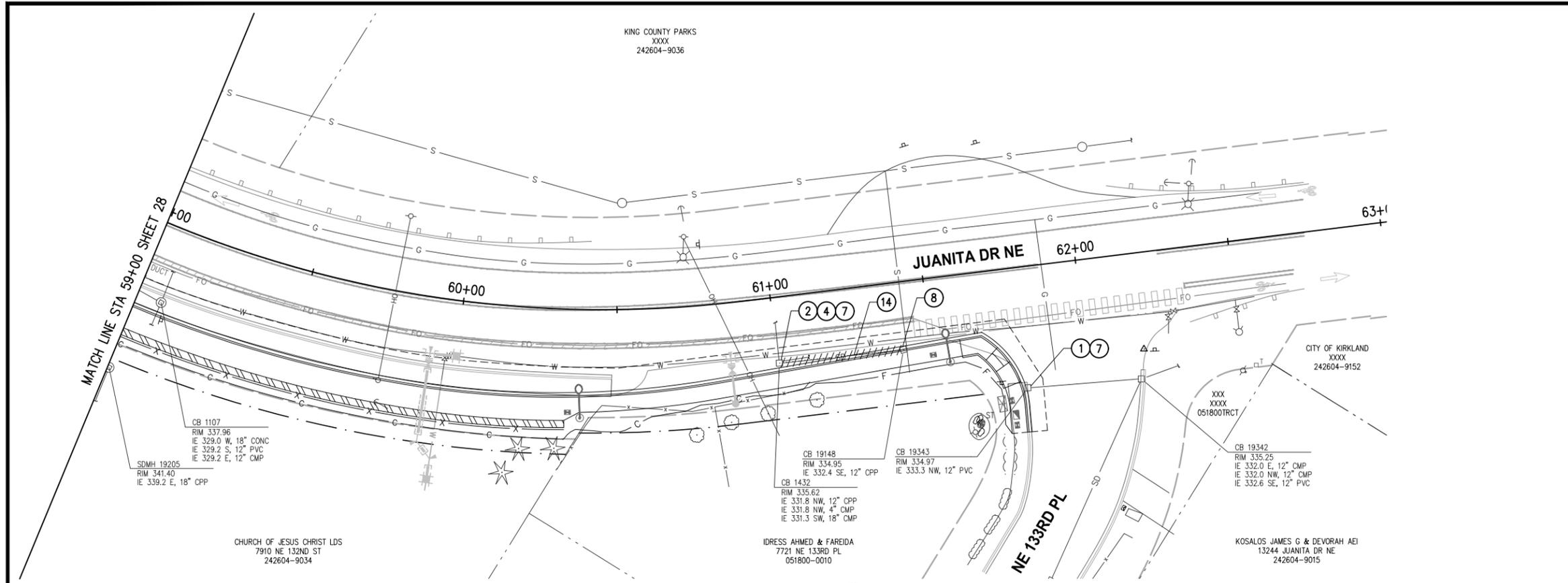
JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

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STORMWATER PLAN & PROFILE

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- GENERAL NOTES**
1. STORM TRENCH SHALL BE PER C.O.K. STD PLAN CK-D.02, EXCEPT ALL BACKFILL MATERIAL SHALL BE CSTC.
 2. CATCH BASIN LOCATIONS AND RIM ELEVATIONS ARE APPROXIMATED FINISHED GRADE AT CENTER OF STRUCTURE. MATCH FINISHED GRADES AND FLOWLINE ELEVATIONS UNLESS OTHERWISE NOTED.
 3. STORM PIPES SHALL BE PVC SDR 35, UNLESS OTHERWISE NOTED.
 4. ALL PENETRATIONS SHALL BE ACCOMPLISHED BY CORING UNLESS THE STRUCTURE HAS KNOCKOUTS OR AN EXISTING CORE DRILLED PENETRATION AT THE CORRECT INVERT ELEVATION.
 5. EXISTING UTILITY LOCATIONS AND DEPTHS ARE BASED ON AS-BUILT RECORDS AND BEST AVAILABLE INFORMATION. CONTRACTOR SHALL FIELD VERIFY UTILITY LOCATIONS AND DEPTHS PRIOR TO CONSTRUCTION ACTIVITIES.

- CONSTRUCTION NOTES**
- ① INSTALL RECTANGULAR FRAME WITH VANED GRATE PER C.O.K. STD PLAN CK-D.14. WHERE IN CURB & GUTTER FLOWLINE, INSTALL FRAME AND GRATE PER C.O.K. STD PLAN CK-D.16A
 - ② INSTALL CIRCULAR RISER AND TRANSITION FOR TYPE 1 CATCH BASIN PER C.O.K. STD PLAN CK-D.07A
 - ④ INSTALL ROUND SOLID LOCKING COVER PER C.O.K. STD PLAN CK-D.18A
 - ⑦ ADJUST EXISTING STORM DRAIN STRUCTURE TO GRADE PER C.O.K. STD PLAN CK-D.11
 - ⑧ REMOVE EXISTING DRAINAGE STRUCTURE
 - ⑭ REMOVE PIPE

- LEGEND**
- ## STORM DRAINAGE STRUCTURE ID NUMBER
 - STORM DRAIN PIPE
 - CATCH BASIN TYPE 2 PER C.O.K. STD PLAN CK-D.09
 - CATCH BASIN TYPE 1 PER C.O.K. STD PLAN CK-D.07; CATCH BASIN TYPE 1L PER C.O.K. STD PLAN CK-D.08 CURB INLET PER C.O.K. STD PLAN CK-D.06
 - DIRECTIONAL FLOW ARROW
 - //// REMOVE/ABANDON EXISTING PIPE
 - ☒ LUMINAIRE & ASSOCIATED EQUIPMENT SEE SHEETS 64-73
 - ▨ PROPOSED RETAINING WALL, SEE SHEETS 55-63
 - W — PROPOSED WATER MAIN, SEE SHEETS 133-145
 - SS — PROPOSED SEWER LATERAL, SEE SHEETS 133-145

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BID DOCUMENT

Tacoma
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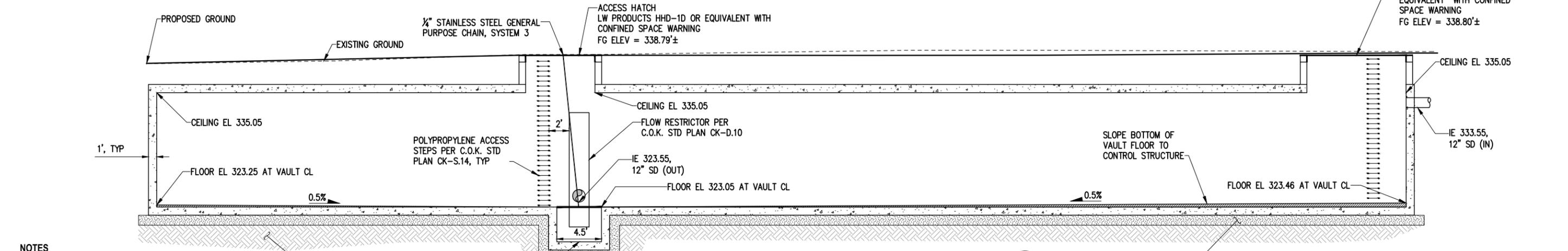
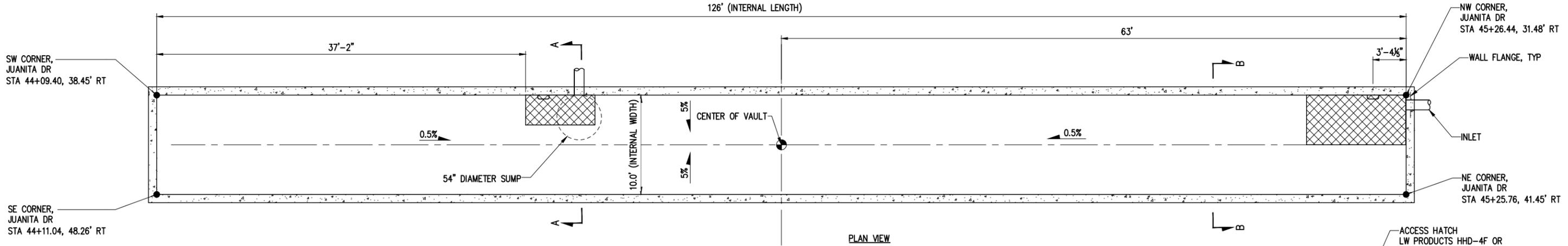
JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

67\$ 72 67\$

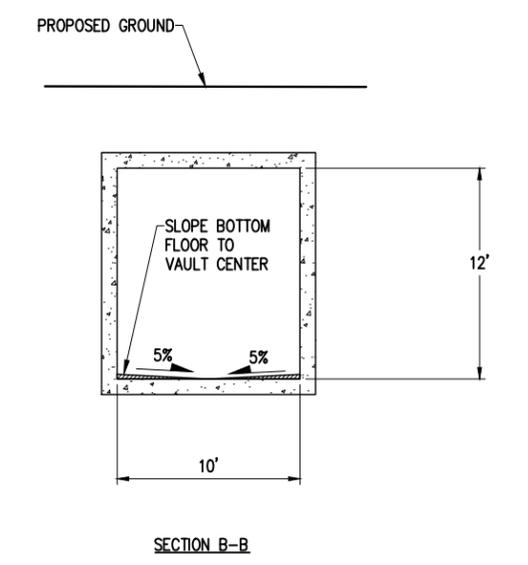
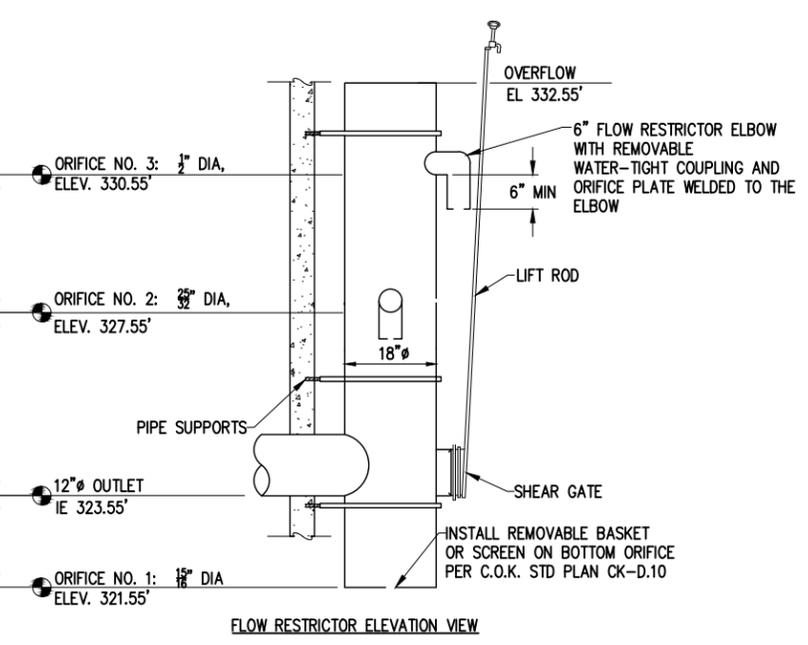
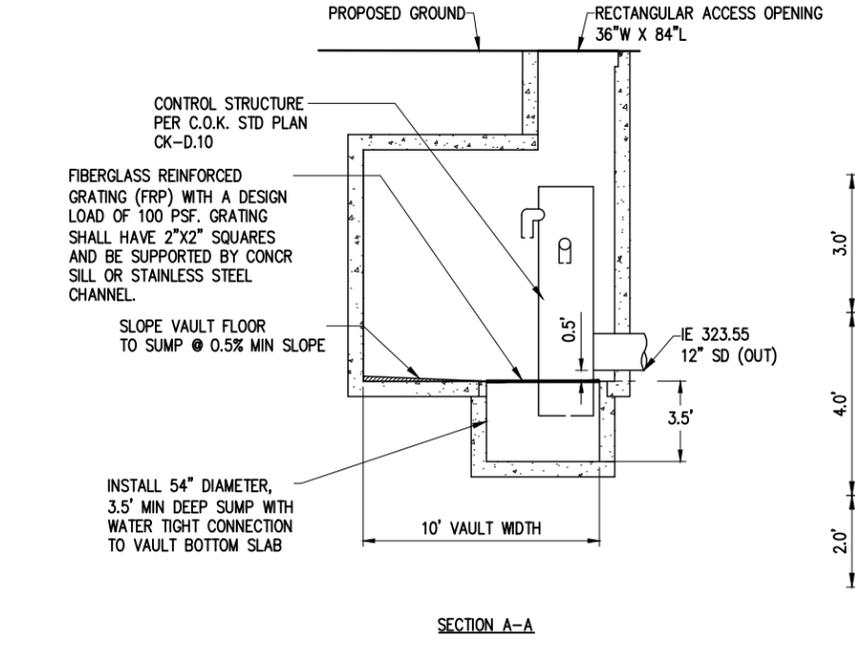
STORMWATER PLAN & PROFILE

REFERENCE SHEET NO.
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- NOTES**
1. DETENTION VAULT PER C.O.K. STD PLAN CK-D.35 UNLESS OTHERWISE NOTED ON THIS SHEET.
 2. FLOW RESTRICTOR PER C.O.K. STD PLAN CK-D.10 TO BE LOCATED INSIDE OF DETENTION VAULT. SEE ELEVATION VIEW FOR ORIFICE SIZE AND LOCATIONS.
 3. CONTROL STRUCTURE TO BE LOCATED AT ACCESS OPENING. POSITION ORIFICES, AND SHEAR GATE SO THAT EACH IS VISIBLE FROM TOP OPENING.
 4. PROVIDE WATER STOP AT ALL CAST-IN-PLACE CONSTRUCTION JOINTS. PRE-CAST VAULTS SHALL HAVE APPROVED RUBBER GASKET SYSTEM WITH JOINTS GROUTED AFTER INSTALL.
 5. ALL METAL PARTS SHALL BE CORROSION RESISTANT, EITHER ALUMINUM OR STAINLESS STEEL. FASTENERS MAY BE STAINLESS STEEL.
 6. DESIGN LIVE LOAD (OVER AND AROUND VAULTS) FOR FIRE DEPARTMENT APPARATUS LOADING:
 - a. HS 20 TRUCK LOADING, AND;
 - b. STABILIZER OUTRIGGER MAXIMUM REACTION = 45,000-LBS CONCENTRATED LOAD; THIS LOAD MUST BE APPLIED ON AN 18-INCH BY 18-INCH AREA AND ALSO APPLIED AS AN UNFACTORED LOAD ON A 10-INCH BY 14-INCH AREA
 7. ACCESS HATCHES FROM LW PRODUCTS COMPANY, OR APPROVED EQUIVALENT. ACCESS HATCH TO HAVE STAINLESS STEEL SPRING LIFTS, LOCKING SUPPORTS, STAINLESS STEEL HARDWARE, REMOVABLE ACCESS HATCH HANDLES, SELF-LATCHING STAINLESS STEEL SLAM LOCK, RECESSED PADLOCK HASP, AND NON-SKID ADA COMPLIANT SURFACE
 8. SAND COLLARS SHALL BE USED ON PIPE TO STRUCTURE CONNECTIONS FOR ALL SMOOTH WALL PIPES WITH 12 IN DIAMETER OR SMALLER, EXCEPT DUCTILE IRON AND CONCRETE. SAND COLLARS ARE NOT REQUIRED FOR PIPES LARGER THAN 12 IN DIAMETER.



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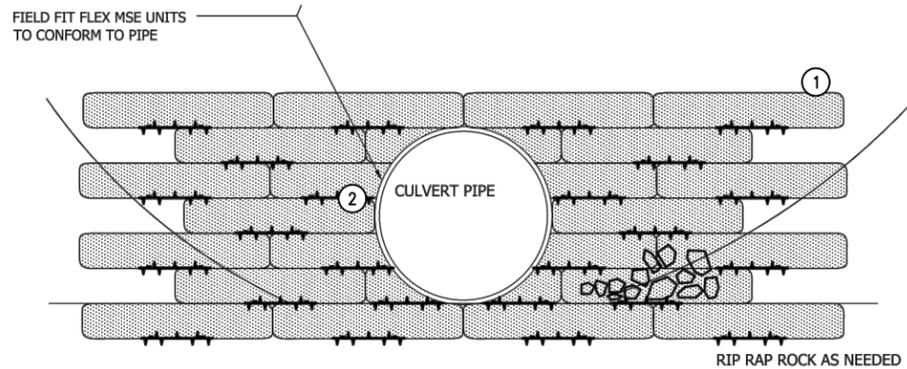
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JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

DETENTION VAULT DETAILS

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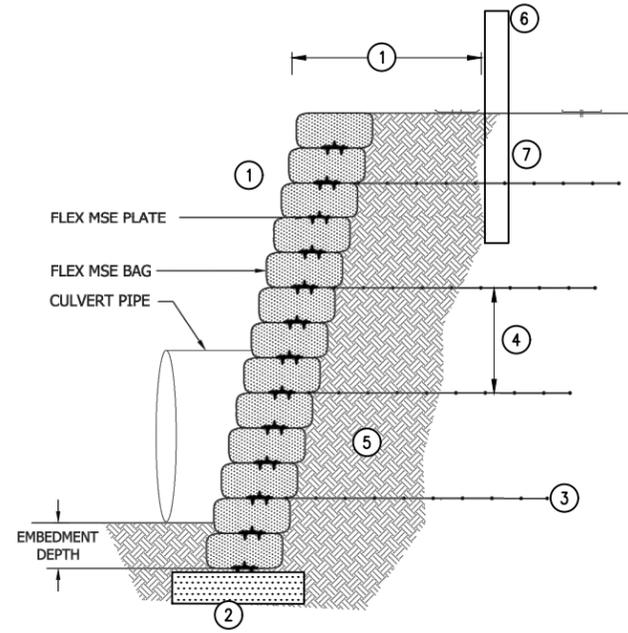
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ROUND CULVERT ELEVATION

NOTES

1. TURN END BAGS OF EACH ROW INTO EXISTING SLOPE AND EMBED A MINIMUM OF 1 FOOT BELOW SLOPE SURFACE.
2. FIELD FIT FLEX MSE UNITS TO CONFORM TO CULVERT PIPE.



SECTION

NOTES

1. FACE BATTER: INCLINE FACE TO PROVIDE 3-FOOT HORIZONTAL OFFSET FROM GUARDRAIL POST AT TOP OF SLOPE. MAXIMUM SLOPE 70-DEGREES.
2. FOUNDATION: EMBED 12-INCHES AND PLACE 6-INCH THICK CRUSHED GRAVEL PAD WSS-9-03.9(3) BASE COURSE.
3. GEOGRID REINFORCEMENT: TENSAR UX1500
LENGTH = 0.6H
WHERE H=TOTAL HEIGHT OF WALL
4. GEOGRID SPACING VERTICAL SPACING 18" MAXIMUM (APPROXIMATELY EVERY THIRD ROW OF FLEXMSE BAGS)
5. GRAVEL BORROW FOR STRUCTURAL EARTH WALL WSS 9-03.14(4) IN REINFORCED ZONE.
6. GUARDRAIL POST EMBEDMENT DEPTH PER WSS SPECIFICATIONS.
7. GUARDRAIL POST INSTALLATION: PLACE TUBE, PVC PIPE OR CANTILEVER FORM DURING CONSTRUCTION, CUT GEOGRID TRANSVERSE TO RIBS TO FIT AROUND FORM OR MARK LOCATIONS WITH EMBEDDED REBAR, CUT GRID TO POST DIMENSION AND HAND EXCAVATE AFTER CONSTRUCTION.

FLEX BAG MSE CULVERT HEADWALL NEAR STATION 44+50 DETAIL 1

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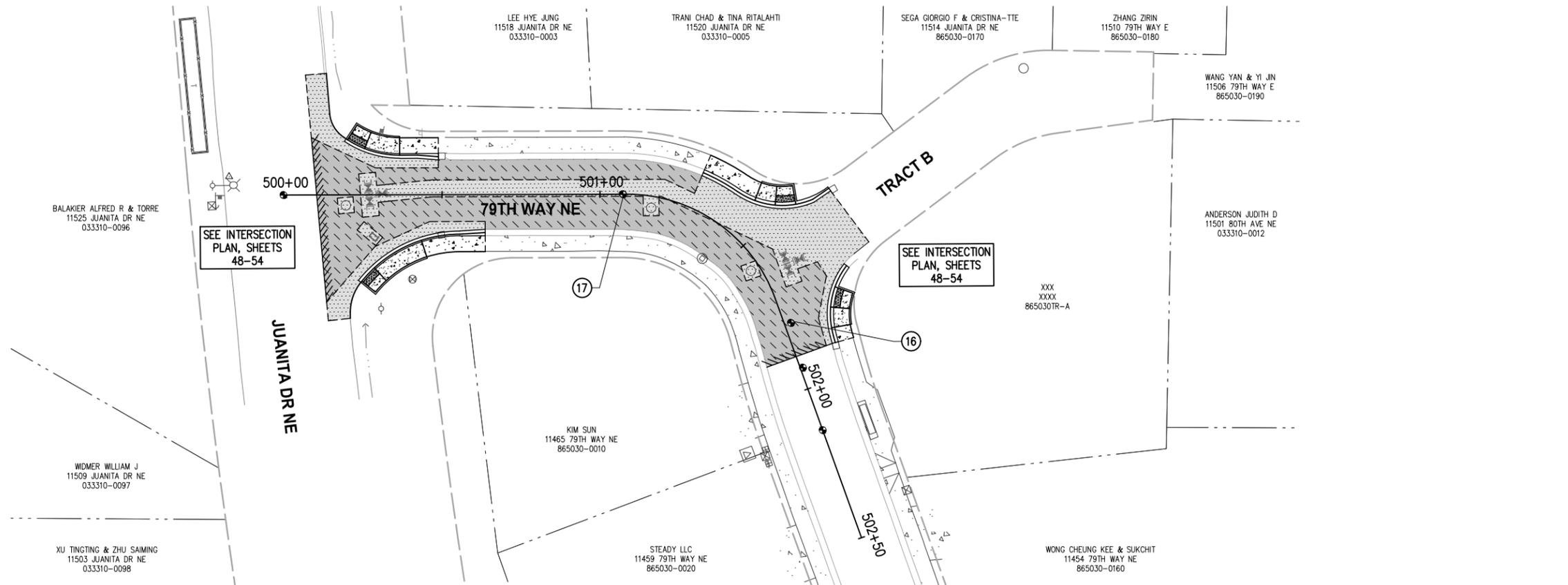
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JUANITA DRIVE MULTIMODAL,
INTERSECTION & SAFETY IMPROVEMENTS

STORMWATER DETAILS

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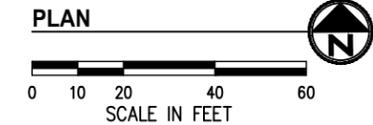


GENERAL NOTES

- FOR ALIGNMENT & SURVEY CONTROL, SEE SHEETS 3-4.
- FOR TYPICAL SECTIONS & PAVEMENT DEPTHS, SEE SHEETS 5-7.
- FOR TREATMENT OF EXIST STORM STRUCTURES, SEE SHEETS 17-29.
- FOR TREATMENT OF EXIST WATER APPURTENANCES AND SEWER STRUCTURES SEE SHEETS 131-159.
- FOR LAYOUT & GRADING OF CURB RETURNS, RAMPS, AND DRIVEWAYS, SEE DRIVEWAY SHEET 47 AND INTERSECTION SHEETS 48-54.
- FOR LANDSCAPE RESTORATION SEE SHEETS 74-81.
- CONTRACTOR SHALL PROTECT ALL EXISTING FEATURES AND UTILITIES NOT SPECIFIED FOR REMOVAL.
- AT PROPOSED LUMINAIRE LOCATIONS GRADE A FLAT AREA AROUND POLE (18" MIN FROM EDGE OF POLE).

CONSTRUCTION NOTES

- ADJUST MONUMENT CASE AND COVER
- PERPETUATE MONUMENT, INSTALL MONUMENT CASE AND COVER PER C.O.K. STD PLAN CK-R.03



LEGEND

- RIGHT OF WAY
- TEMPORARY CONSTRUCTION EASEMENT
- o- APPROX. CUT DAYLIGHT LINE
- f- APPROX. FILL DAYLIGHT LINE
- SAWCUT PER SITE PREP PLAN SHEETS 9-16
- //// BUTT JOINT PER SITE PREP PLAN SHEETS 9-16
- x- FENCE, SEE WALL DETAILS FOR LOCATION
- //// WALL PER WALL PLAN SHEETS 55-63
- CEMENT CONC SIDEWALK PER C.O.K. STD PLAN CK-R.23, MATCH STD WIDTH UNLESS OTHERWISE NOTED
- CEMENT CONC CURB RAMP OR DRIVEWAY ENTRANCE, AS NOTED
- ASPHALT DRIVEWAY, 3" COMMERCIAL HMA ON 2" CSTC
- ASPHALT ROADWAY, 8" HMA ON 4" CSTC - SCHEDULE A
- ASPHALT ROADWAY, 8" HMA ON 4" CSTC - SCHEDULE B
- 2" GRIND & OVERLAY - SCHEDULE A
- 2" GRIND & OVERLAY - SCHEDULE B
- 2" CSTC DRIVEWAY/SHOULDER/MAILBOX CLUSTER PAD
- STORM STRUCTURES, SEE STORM SHEETS 17-29
- SIGN, SEE CHANNELIZATION & SIGNING SHEETS 82-89
- WATER APPURTENANCES, SEE NUD WATER SHEETS 131-159
- ILLUMINATION EQUIPMENT, SEE ILLUMINATION SHEETS 64-73

ALL COSTS ASSOCIATED WITH IMPROVEMENTS ON THIS SHEET WILL BE MEASURED AND PAID UNDER BID ITEMS IN SCHEDULE B

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JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

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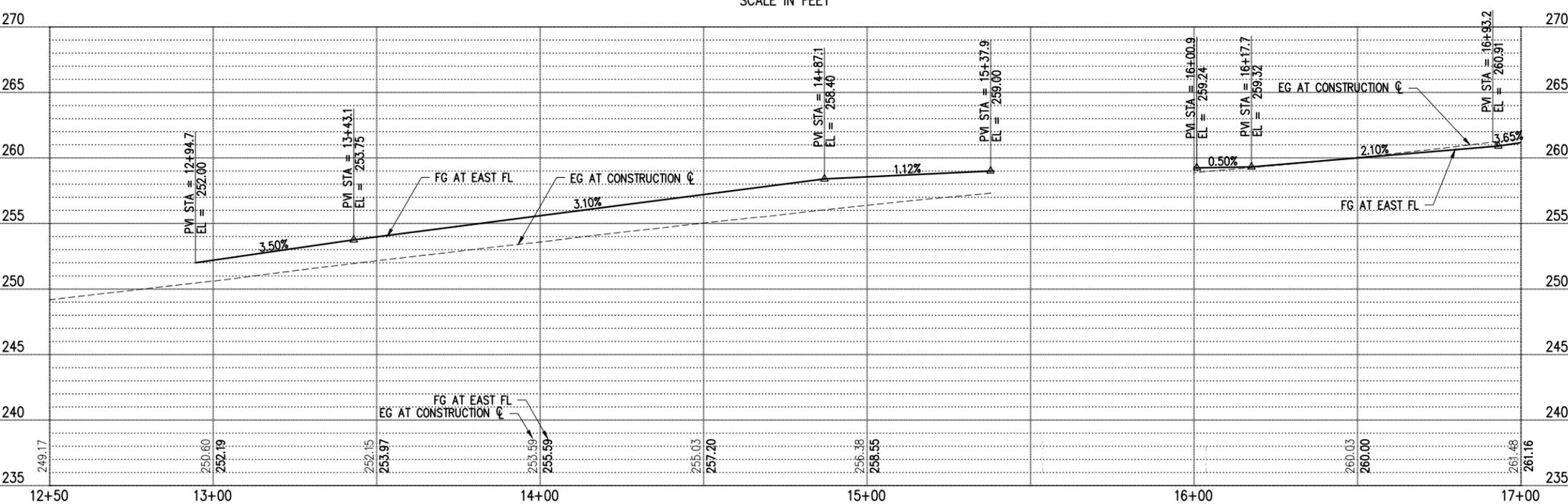
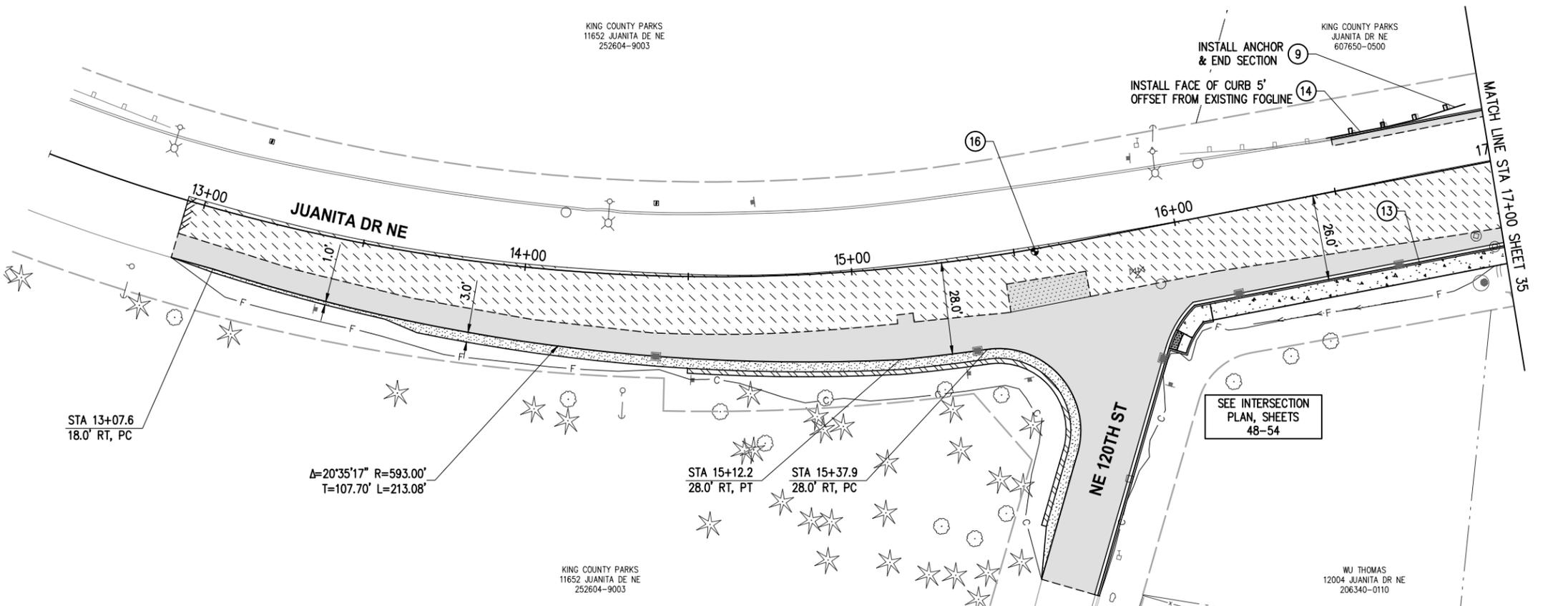
ROADWAY PLAN & PROFILE

REFERENCE SHEET NO. _____

SHEET 2)

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PROFILE

GENERAL NOTES

- 1. FOR ALIGNMENT & SURVEY CONTROL, SEE SHEETS 3-4.
- 2. FOR TYPICAL SECTIONS & PAVEMENT DEPTHS, SEE SHEETS 5-7.
- 3. FOR TREATMENT OF EXIST STORM STRUCTURES, SEE SHEETS 17-29.
- 4. FOR TREATMENT OF EXIST WATER APPURTENANCES AND SEWER STRUCTURES SEE SHEETS 131-159.
- 5. FOR LAYOUT & GRADING OF CURB RETURNS, RAMP, AND DRIVEWAYS, SEE DRIVEWAY SHEET 47 AND INTERSECTION SHEETS 48-54.
- 6. FOR LANDSCAPE RESTORATION SEE SHEETS 74-81.
- 7. CONTRACTOR SHALL PROTECT ALL EXISTING FEATURES AND UTILITIES NOT SPECIFIED FOR REMOVAL.
- 8. AT PROPOSED LUMINAIRE LOCATIONS GRADE A FLAT AREA AROUND POLE (18" MIN FROM EDGE OF POLE).

CONSTRUCTION NOTES

- 9. INSTALL GUARDRAIL PER WSDOT STD PLAN C-20.10. ALIGN FACE OF GUARDRAIL WITH BACK OF CURB OR EDGE OF PAVEMENT. CONNECT TO EXISTING GUARDRAIL; INSTALL GUARDRAIL ANCHOR AND END SECTION PER WSDOT STD PLAN C-23.70 WHERE NOTED.
- 13. CEMENT CONC CURB AND GUTTER PER C.O.K. STD PLAN CK-R.17
- 14. EXTRUDED CEMENT CONC CURB PER C.O.K. STD PLAN CK-R.19
- 16. ADJUST MONUMENT CASE AND COVER

LEGEND

- - - - RIGHT OF WAY
- . - . - . TEMPORARY CONSTRUCTION EASEMENT
- - - - - c - - - - APPROX. CUT DAYLIGHT LINE
- - - - - F - - - - APPROX. FILL DAYLIGHT LINE
- - - - - SAWCUT PER SITE PREP PLAN SHEETS 9-16
- ////// BUTT JOINT PER SITE PREP PLAN SHEETS 9-16
- x - - - - FENCE, SEE WALL DETAILS FOR LOCATION
- ||||| WALL PER WALL PLAN SHEETS 55-63
- [Pattern] CEMENT CONC SIDEWALK PER C.O.K. STD PLAN CK-R.23, MATCH STD WIDTH UNLESS OTHERWISE NOTED
- [Pattern] CEMENT CONC CURB RAMP OR DRIVEWAY ENTRANCE, AS NOTED
- [Pattern] ASPHALT DRIVEWAY, 3" COMMERCIAL HMA ON 2" CSTC
- [Pattern] ASPHALT ROADWAY, 8" HMA ON 4" CSTC - SCHEDULE A
- [Pattern] ASPHALT ROADWAY, 8" HMA ON 4" CSTC - SCHEDULE B
- [Pattern] 2" GRIND & OVERLAY - SCHEDULE A
- [Pattern] 2" GRIND & OVERLAY - SCHEDULE B
- [Pattern] 2" CSTC DRIVEWAY/SHOULDER/MAILBOX CLUSTER PAD
- [Symbol] STORM STRUCTURES, SEE STORM SHEETS 17-29
- [Symbol] SIGN, SEE CHANNELIZATION & SIGNING SHEETS 82-89
- [Symbol] WATER APPURTENANCES, SEE NUD WATER SHEETS 131-159
- [Symbol] ILLUMINATION EQUIPMENT, SEE ILLUMINATION SHEETS 64-73

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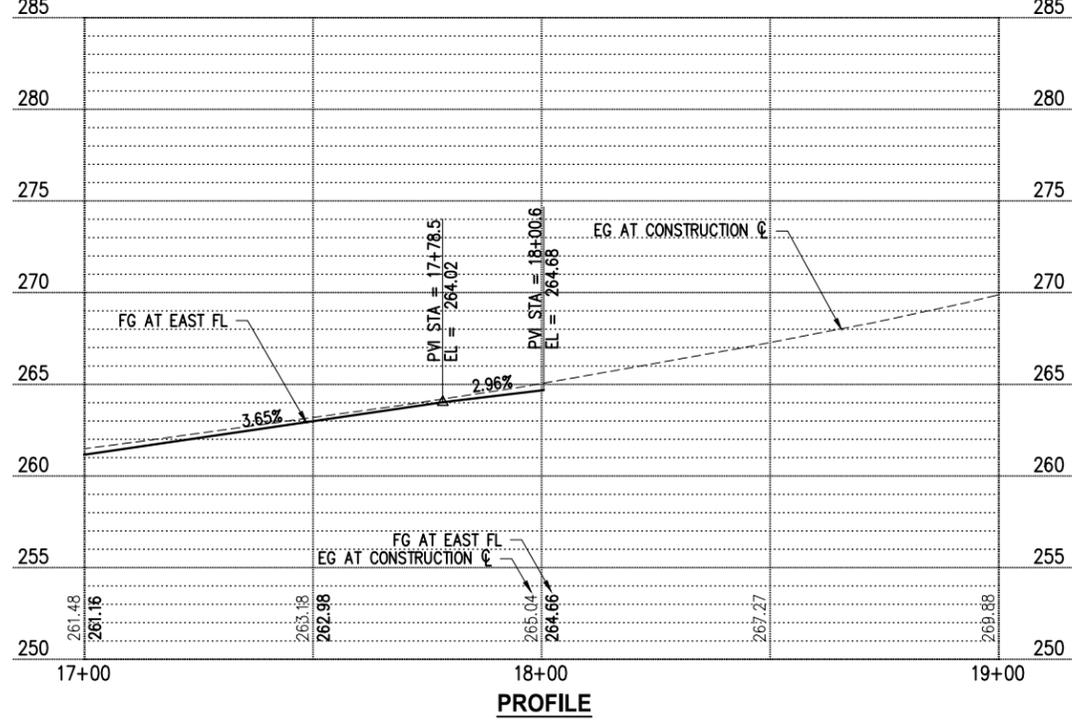
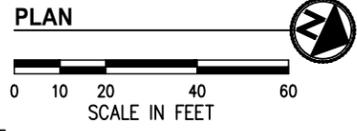
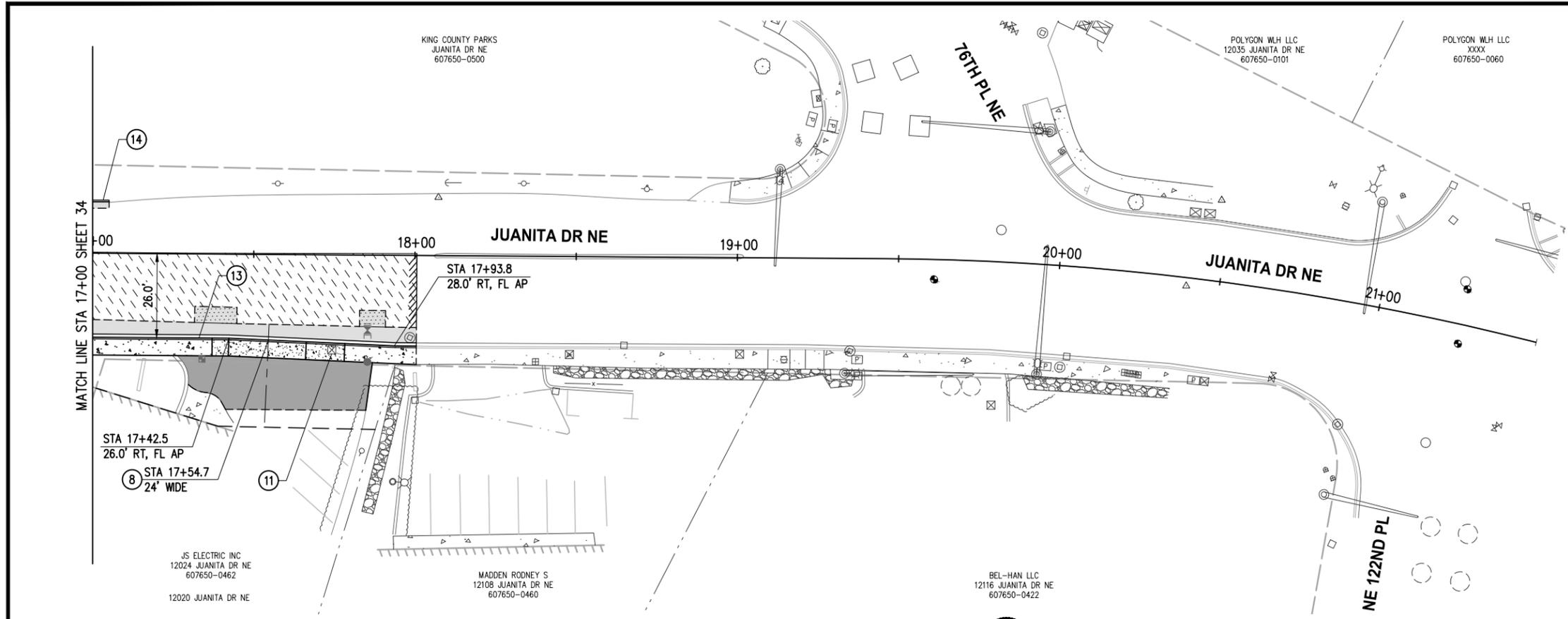
JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

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ROADWAY PLAN & PROFILE

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GENERAL NOTES

- 1. FOR ALIGNMENT & SURVEY CONTROL, SEE SHEETS 3-4.
- 2. FOR TYPICAL SECTIONS & PAVEMENT DEPTHS, SEE SHEETS 5-7.
- 3. FOR TREATMENT OF EXIST STORM STRUCTURES, SEE SHEETS 17-29.
- 4. FOR TREATMENT OF EXIST WATER APPURTENANCES AND SEWER STRUCTURES SEE SHEETS 131-159.
- 5. FOR LAYOUT & GRADING OF CURB RETURNS, RAMP, AND DRIVEWAYS, SEE DRIVEWAY SHEET 47 AND INTERSECTION SHEETS 48-54.
- 6. FOR LANDSCAPE RESTORATION SEE SHEETS 74-81.
- 7. CONTRACTOR SHALL PROTECT ALL EXISTING FEATURES AND UTILITIES NOT SPECIFIED FOR REMOVAL.
- 8. AT PROPOSED LUMINAIRE LOCATIONS GRADE A FLAT AREA AROUND POLE (18" MIN FROM EDGE OF POLE).

CONSTRUCTION NOTES

- ⑧ CEMENT CONC DRIVEWAY ENTRANCE TYPE 1 PER WSDOT STD PLAN F-80.10
- ⑪ RELOCATE/ADJUST/REMOVE UTILITY BY OTHERS, COORDINATE WITH UTILITY PURVEYOR
- ⑬ CEMENT CONC CURB AND GUTTER PER C.O.K. STD PLAN CK-R.17
- ⑭ EXTRUDED CEMENT CONC CURB PER C.O.K. STD PLAN CK-R.19

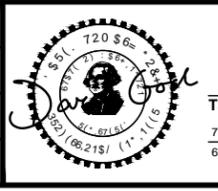
LEGEND

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- - - - TEMPORARY CONSTRUCTION EASEMENT
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- [Pattern] ASPHALT ROADWAY, 8" HMA ON 4" CSTC - SCHEDULE A
- [Pattern] ASPHALT ROADWAY, 8" HMA ON 4" CSTC - SCHEDULE B
- [Pattern] 2" GRIND & OVERLAY - SCHEDULE A
- [Pattern] 2" GRIND & OVERLAY - SCHEDULE B
- [Pattern] 2" CSTC DRIVEWAY/SHOULDER/MAILBOX CLUSTER PAD
- [Symbol] STORM STRUCTURES, SEE STORM SHEETS 17-29
- [Symbol] SIGN, SEE CHANNELIZATION & SIGNING SHEETS 82-89
- [Symbol] WATER APPURTENANCES, SEE NUD WATER SHEETS 131-159
- [Symbol] ILLUMINATION EQUIPMENT, SEE ILLUMINATION SHEETS 64-73

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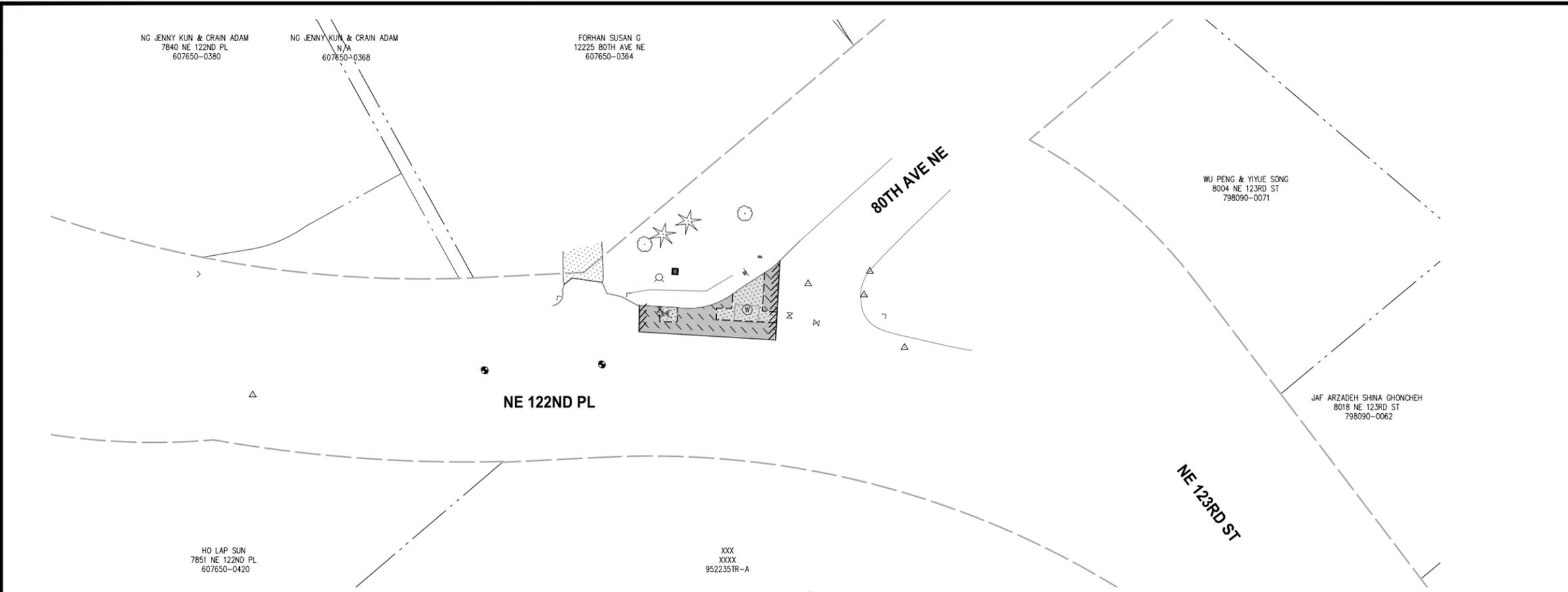
**JUANITA DRIVE MULTIMODAL,
INTERSECTION & SAFETY IMPROVEMENTS**

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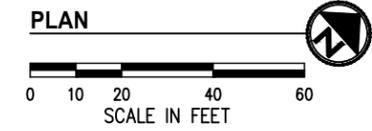
ROADWAY PLAN & PROFILE

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- ### GENERAL NOTES
- FOR ALIGNMENT & SURVEY CONTROL, SEE SHEETS 3-4.
 - FOR TYPICAL SECTIONS & PAVEMENT DEPTHS, SEE SHEETS 5-7.
 - FOR TREATMENT OF EXIST STORM STRUCTURES, SEE SHEETS 17-29.
 - FOR TREATMENT OF EXIST WATER APPURTENANCES AND SEWER STRUCTURES SEE SHEETS 131-159.
 - FOR LAYOUT & GRADING OF CURB RETURNS, RAMP, AND DRIVEWAYS, SEE DRIVEWAY SHEET 47 AND INTERSECTION SHEETS 48-54.
 - FOR LANDSCAPE RESTORATION SEE SHEETS 74-81.
 - CONTRACTOR SHALL PROTECT ALL EXISTING FEATURES AND UTILITIES NOT SPECIFIED FOR REMOVAL.
 - AT PROPOSED LUMINAIRE LOCATIONS GRADE A FLAT AREA AROUND POLE (18" MIN FROM EDGE OF POLE).



ALL COSTS ASSOCIATED WITH IMPROVEMENTS ON THIS SHEET WILL BE MEASURED AND PAID UNDER BID ITEMS IN SCHEDULE B

- ### LEGEND
- RIGHT OF WAY
 - TEMPORARY CONSTRUCTION EASEMENT
 - APPROX. CUT DAYLIGHT LINE
 - APPROX. FILL DAYLIGHT LINE
 - SAWCUT PER SITE PREP PLAN SHEETS 9-16
 - BUTT JOINT PER SITE PREP PLAN SHEETS 9-16
 - FENCE, SEE WALL DETAILS FOR LOCATION
 - WALL PER WALL PLAN SHEETS 55-63
 - CEMENT CONC SIDEWALK PER C.O.K. STD PLAN CK-R.23, MATCH STD WIDTH UNLESS OTHERWISE NOTED
 - CEMENT CONC CURB RAMP OR DRIVEWAY ENTRANCE, AS NOTED
 - ASPHALT DRIVEWAY, 3" COMMERCIAL HMA ON 2" CSTC
 - ASPHALT ROADWAY, 8" HMA ON 4" CSTC - SCHEDULE A
 - ASPHALT ROADWAY, 8" HMA ON 4" CSTC - SCHEDULE B
 - 2" GRIND & OVERLAY - SCHEDULE A
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 - 2" CSTC DRIVEWAY/SHOULDER/MAILBOX CLUSTER PAD
 - STORM STRUCTURES, SEE STORM SHEETS 17-29
 - SIGN, SEE CHANNELIZATION & SIGNING SHEETS 82-89
 - WATER APPURTENANCES, SEE NUD WATER SHEETS 131-159
 - ILLUMINATION EQUIPMENT, SEE ILLUMINATION SHEETS 64-73

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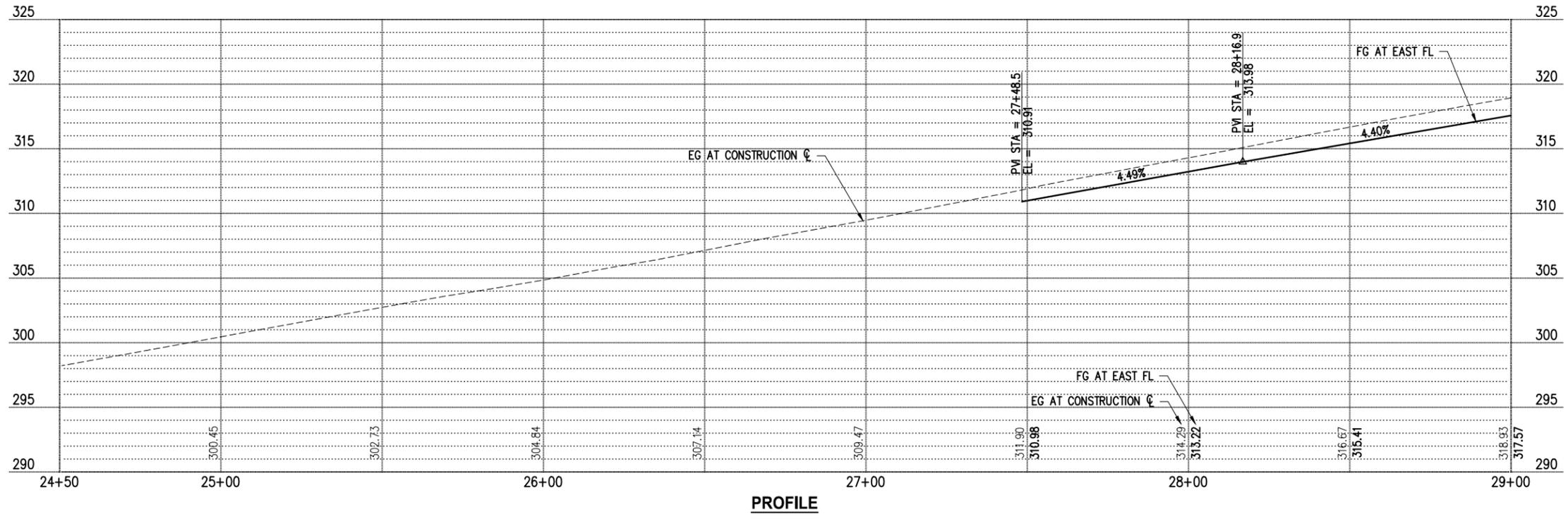
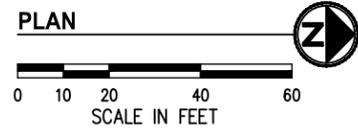
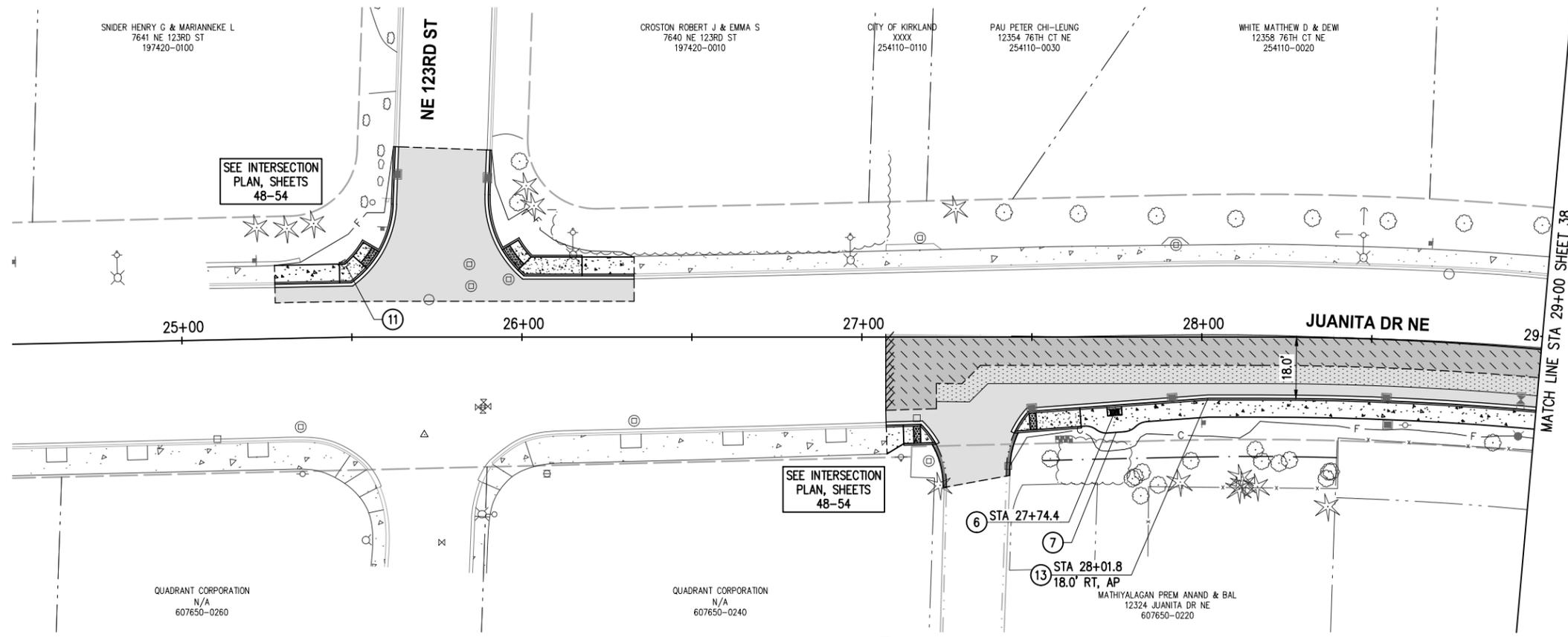
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JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS
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SHEETS

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 - FOR TREATMENT OF EXIST WATER APPURTENANCES AND SEWER STRUCTURES SEE SHEETS 131-159.
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 - AT PROPOSED LUMINAIRE LOCATIONS GRADE A FLAT AREA AROUND POLE (18" MIN FROM EDGE OF POLE).

- CONSTRUCTION NOTES**
- MAILBOX CLUSTER PER C.O.K. STD PLAN CK-R.45A. INSTALL 4" CSTC PAD BELOW MAILBOX CLUSTER. CSTC PAD DIMENSIONS SHALL MATCH MAILBOX CLUSTER ROOF PERIMETER AND EXTEND TO BACK OF CURB
 - PEDESTRIAN PATH WIDENING PER DETAIL 1, SHEET 8
 - RELOCATE/ADJUST/REMOVE UTILITY BY OTHERS, COORDINATE WITH UTILITY PURVEYOR
 - CEMENT CONC CURB AND GUTTER PER C.O.K. STD PLAN CK-R.17

- LEGEND**
- RIGHT OF WAY
 - TEMPORARY CONSTRUCTION EASEMENT
 - APPROX. CUT DAYLIGHT LINE
 - APPROX. FILL DAYLIGHT LINE
 - SAWCUT PER SITE PREP PLAN SHEETS 9-16
 - BUTT JOINT PER SITE PREP PLAN SHEETS 9-16
 - FENCE, SEE WALL DETAILS FOR LOCATION
 - WALL PER WALL PLAN SHEETS 55-63
 - CEMENT CONC SIDEWALK PER C.O.K. STD PLAN CK-R.23, MATCH STD WIDTH UNLESS OTHERWISE NOTED
 - CEMENT CONC CURB RAMP OR DRIVEWAY ENTRANCE, AS NOTED
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 - ASPHALT ROADWAY, 8" HMA ON 4" CSTC - SCHEDULE A
 - ASPHALT ROADWAY, 8" HMA ON 4" CSTC - SCHEDULE B
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 - 2" CSTC DRIVEWAY/SHOULDER/MAILBOX CLUSTER PAD
 - STORM STRUCTURES, SEE STORM SHEETS 17-29
 - SIGN, SEE CHANNELIZATION & SIGNING SHEETS 82-89
 - WATER APPURTENANCES, SEE NUD WATER SHEETS 131-159
 - ILLUMINATION EQUIPMENT, SEE ILLUMINATION SHEETS 64-73

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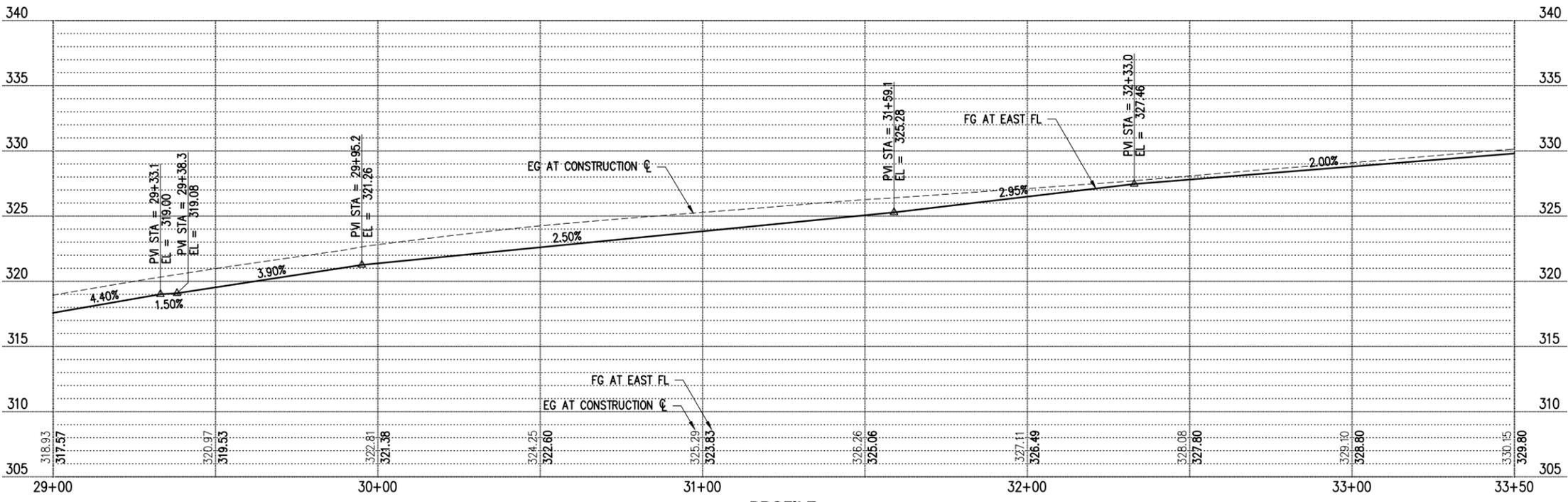
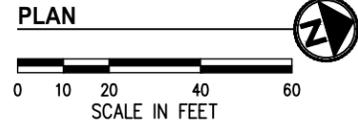
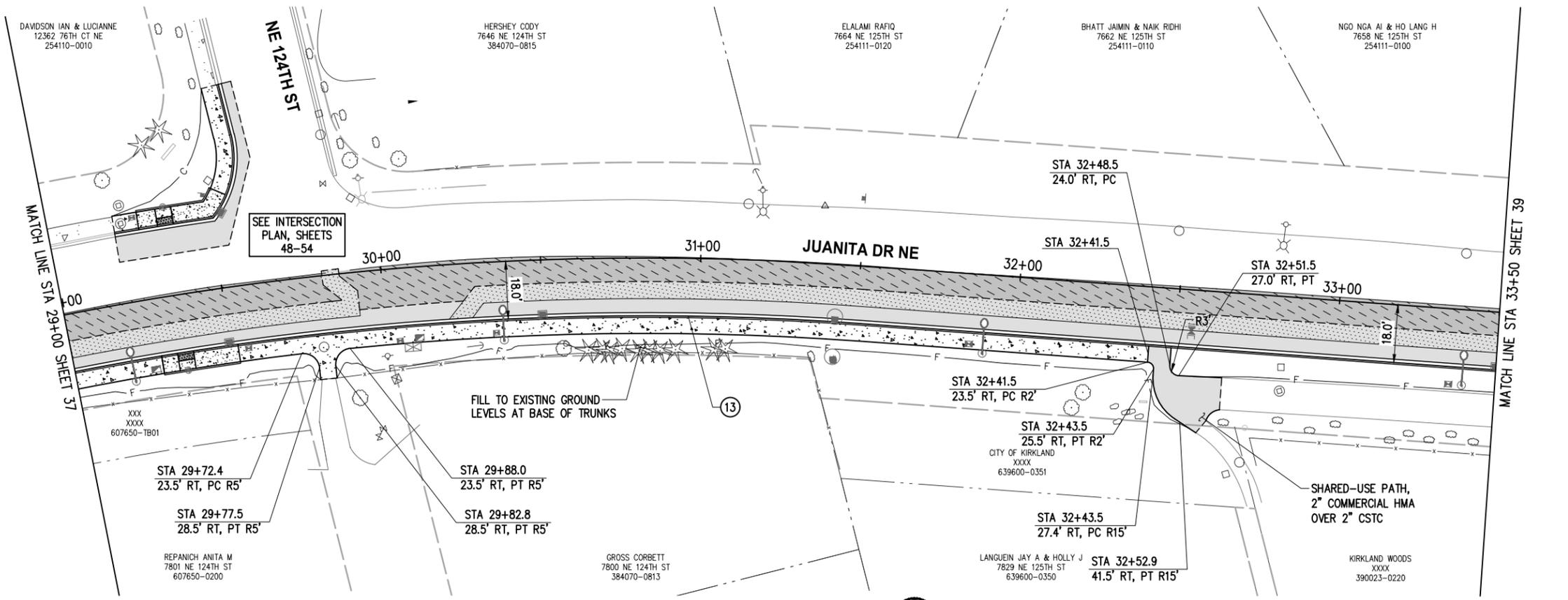
JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

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ROADWAY PLAN & PROFILE

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PROFILE

GENERAL NOTES

- FOR ALIGNMENT & SURVEY CONTROL, SEE SHEETS 3-4.
- FOR TYPICAL SECTIONS & PAVEMENT DEPTHS, SEE SHEETS 5-7.
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- AT PROPOSED LUMINAIRE LOCATIONS GRADE A FLAT AREA AROUND POLE (18" MIN FROM EDGE OF POLE).

CONSTRUCTION NOTES

- CEMENT CONC CURB AND GUTTER PER C.O.K. STD PLAN CK-R.17

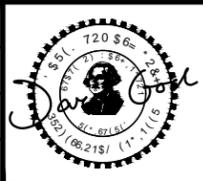
LEGEND

- RIGHT OF WAY
- TEMPORARY CONSTRUCTION EASEMENT
- APPROX. CUT DAYLIGHT LINE
- APPROX. FILL DAYLIGHT LINE
- SAWCUT PER SITE PREP PLAN SHEETS 9-16
- BUTT JOINT PER SITE PREP PLAN SHEETS 9-16
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- ILLUMINATION EQUIPMENT, SEE ILLUMINATION SHEETS 64-73

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JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

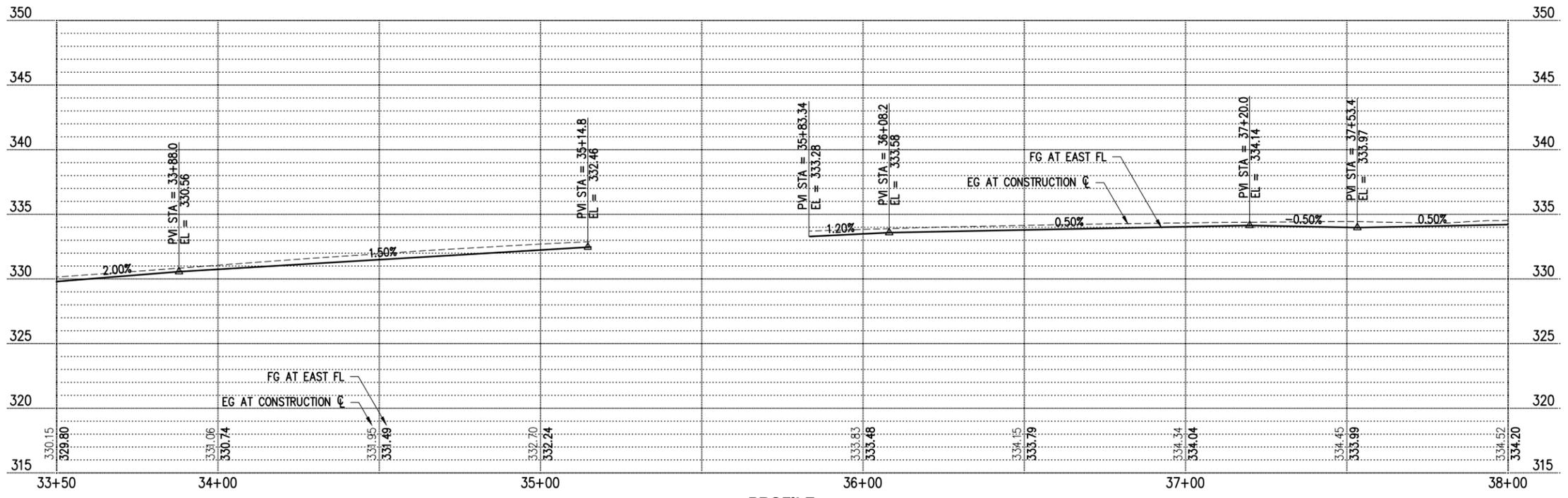
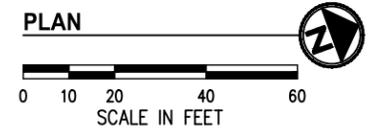
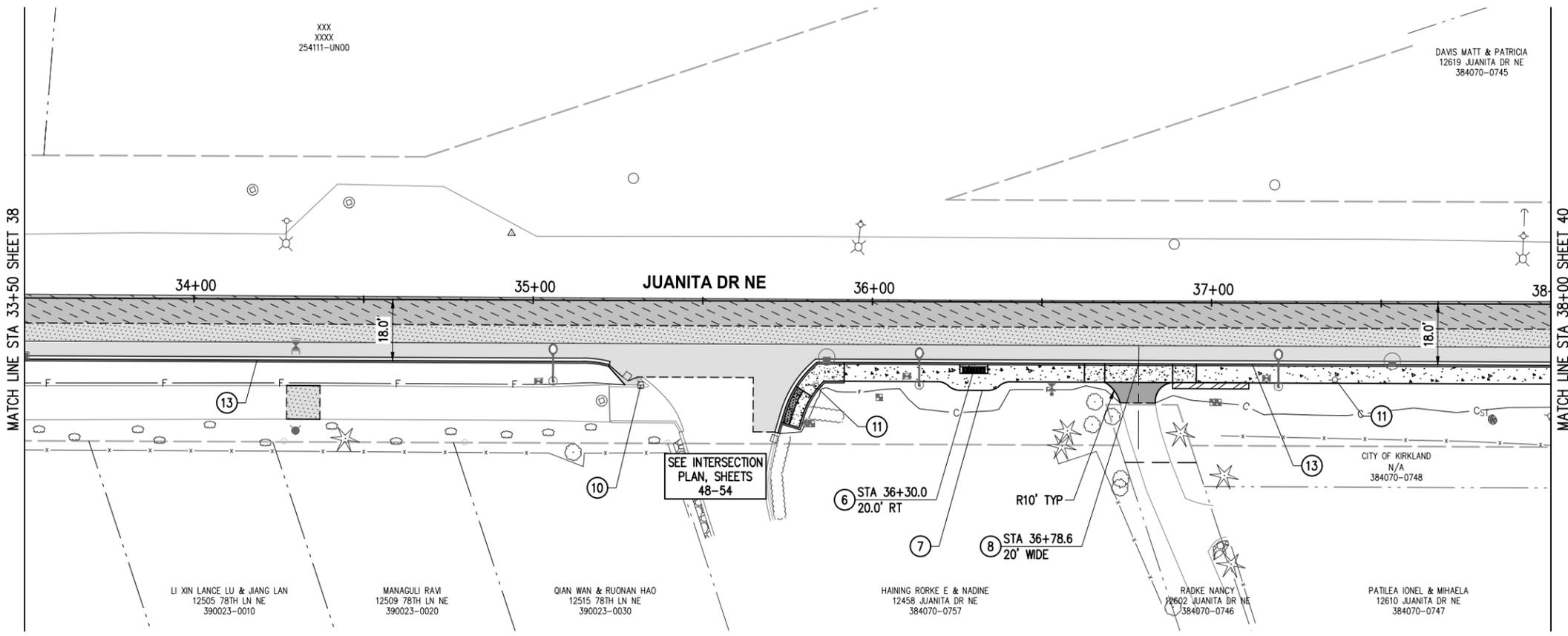
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ROADWAY PLAN & PROFILE

REFERENCE SHEET NO.

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PROFILE

GENERAL NOTES

1. FOR ALIGNMENT & SURVEY CONTROL, SEE SHEETS 3-4.
2. FOR TYPICAL SECTIONS & PAVEMENT DEPTHS, SEE SHEETS 5-7.
3. FOR TREATMENT OF EXIST STORM STRUCTURES, SEE SHEETS 17-29.
4. FOR TREATMENT OF EXIST WATER APPURTENANCES AND SEWER STRUCTURES SEE SHEETS 131-159.
5. FOR LAYOUT & GRADING OF CURB RETURNS, RAMPS, AND DRIVEWAYS, SEE DRIVEWAY SHEET 47 AND INTERSECTION SHEETS 48-54.
6. FOR LANDSCAPE RESTORATION SEE SHEETS 74-81.
7. CONTRACTOR SHALL PROTECT ALL EXISTING FEATURES AND UTILITIES NOT SPECIFIED FOR REMOVAL.
8. AT PROPOSED LUMINAIRE LOCATIONS GRADE A FLAT AREA AROUND POLE (18" MIN FROM EDGE OF POLE).

CONSTRUCTION NOTES

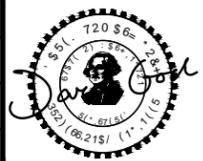
6. MAILBOX CLUSTER PER C.O.K. STD PLAN CK-R.45A. INSTALL 4" CTSC PAD BELOW MAILBOX CLUSTER. CTSC PAD DIMENSIONS SHALL MATCH MAILBOX CLUSTER ROOF PERIMETER AND EXTEND TO BACK OF CURB
7. PEDESTRIAN PATH WIDENING PER DETAIL 1, SHEET 8
8. CEMENT CONC DRIVEWAY ENTRANCE TYPE 1 PER WSDOT STD PLAN F-80.10
10. ADJUST GAS VALVE TO FINISHED GRADE
11. RELOCATE/ADJUST/REMOVE UTILITY BY OTHERS, COORDINATE WITH UTILITY PURVEYOR
13. CEMENT CONC CURB AND GUTTER PER C.O.K. STD PLAN CK-R.17

LEGEND

- RIGHT OF WAY
- TEMPORARY CONSTRUCTION EASEMENT
- c- APPROX. CUT DAYLIGHT LINE
- F- APPROX. FILL DAYLIGHT LINE
- SAWCUT PER SITE PREP PLAN SHEETS 9-16
- /// BUTT JOINT PER SITE PREP PLAN SHEETS 9-16
- x- FENCE, SEE WALL DETAILS FOR LOCATION
- /// WALL PER WALL PLAN SHEETS 55-63
- [Pattern] CEMENT CONC SIDEWALK PER C.O.K. STD PLAN CK-R.23, MATCH STD WIDTH UNLESS OTHERWISE NOTED
- [Pattern] CEMENT CONC CURB RAMP OR DRIVEWAY ENTRANCE, AS NOTED
- [Pattern] ASPHALT DRIVEWAY, 3" COMMERCIAL HMA ON 2" CTSC
- [Pattern] ASPHALT ROADWAY, 8" HMA ON 4" CTSC - SCHEDULE A
- [Pattern] ASPHALT ROADWAY, 8" HMA ON 4" CTSC - SCHEDULE B
- [Pattern] 2" GRIND & OVERLAY - SCHEDULE A
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- [Pattern] 2" CTSC DRIVEWAY/SHOULDER/MAILBOX CLUSTER PAD
- [Symbol] STORM STRUCTURES, SEE STORM SHEETS 17-29
- [Symbol] SIGN, SEE CHANNELIZATION & SIGNING SHEETS 82-89
- [Symbol] WATER APPURTENANCES, SEE NUD WATER SHEETS 131-159
- [Symbol] ILLUMINATION EQUIPMENT, SEE ILLUMINATION SHEETS 64-73



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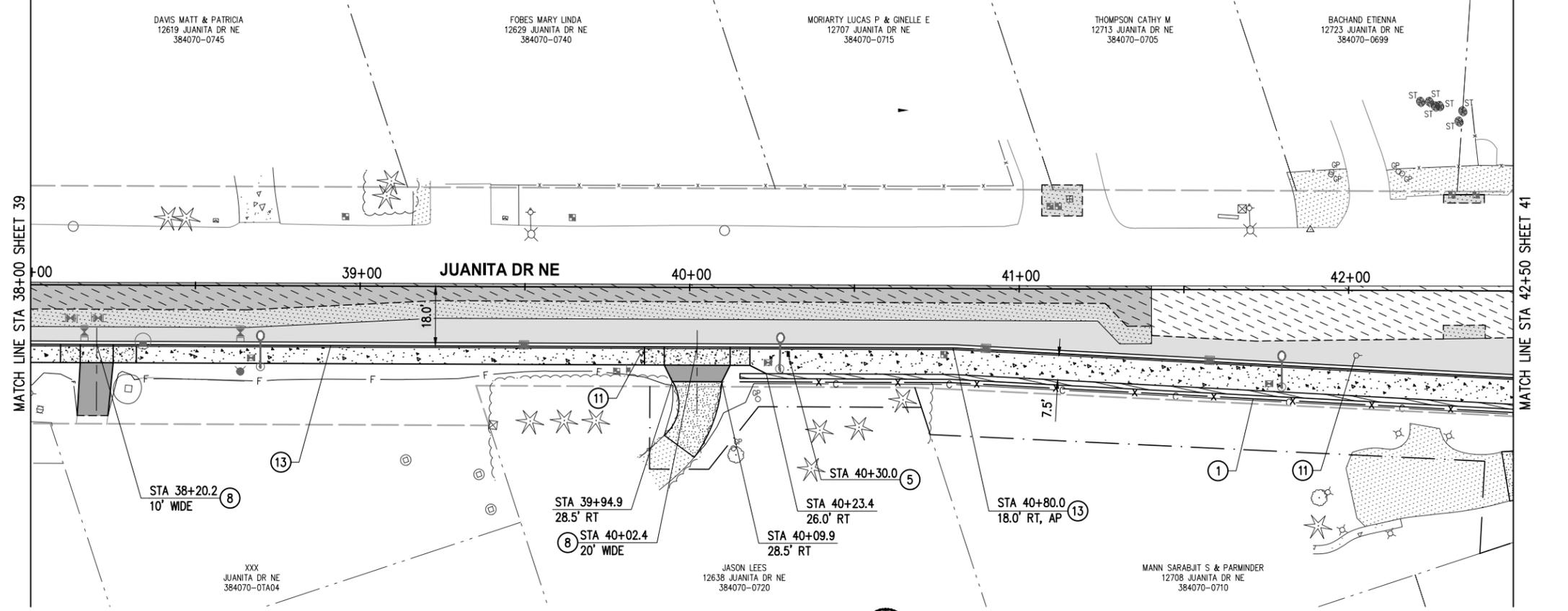
JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

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ROADWAY PLAN & PROFILE

REFERENCE SHEET NO.
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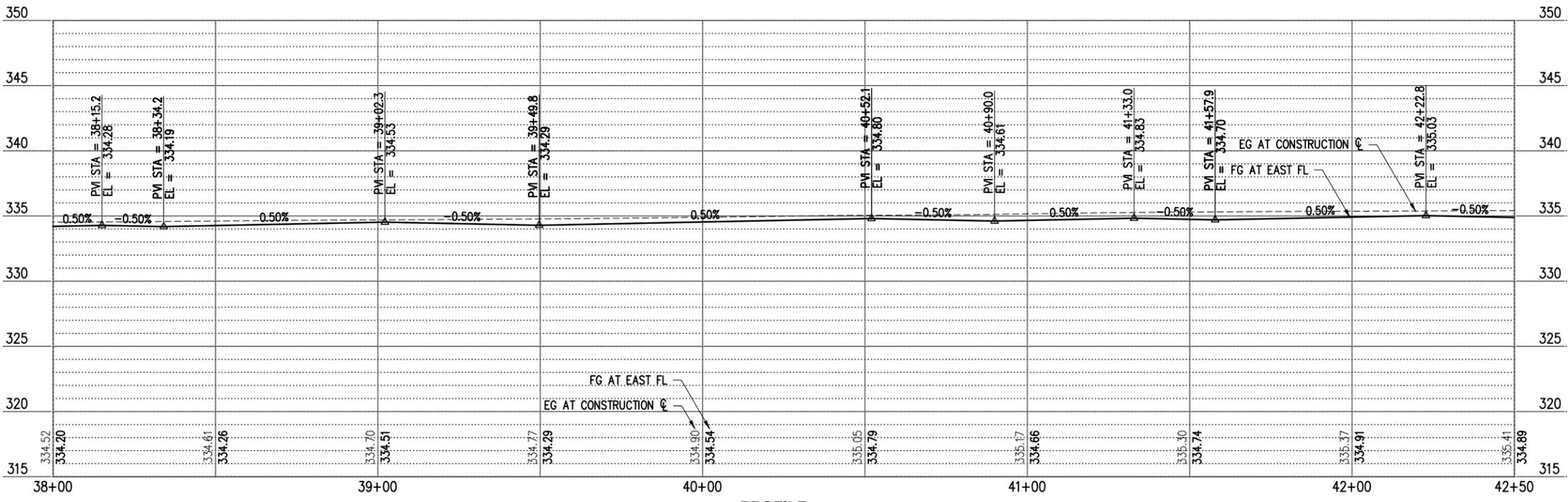
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- ### GENERAL NOTES
- FOR ALIGNMENT & SURVEY CONTROL, SEE SHEETS 3-4.
 - FOR TYPICAL SECTIONS & PAVEMENT DEPTHS, SEE SHEETS 5-7.
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- ### CONSTRUCTION NOTES
- 3' CHAIN LINK SAFETY RAIL PER C.O.K. STD PLAN CK-R.51A
 - REINSTALL MAILBOX WITH NEW WOOD POST & HARDWARE PER WSDOT STD PLAN H-70.10
 - CEMENT CONC DRIVEWAY ENTRANCE TYPE 1 PER WSDOT STD PLAN F-80.10
 - RELOCATE/ADJUST/REMOVE UTILITY BY OTHERS, COORDINATE WITH UTILITY PURVEYOR
 - CEMENT CONC CURB AND GUTTER PER C.O.K. STD PLAN CK-R.17

- ### LEGEND
- RIGHT OF WAY
 - TEMPORARY CONSTRUCTION EASEMENT
 - APPROX. CUT DAYLIGHT LINE
 - APPROX. FILL DAYLIGHT LINE
 - SAWCUT PER SITE PREP PLAN SHEETS 9-16
 - BUTT JOINT PER SITE PREP PLAN SHEETS 9-16
 - FENCE, SEE WALL DETAILS FOR LOCATION
 - WALL PER WALL PLAN SHEETS 55-63
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 - ASPHALT ROADWAY, 8" HMA ON 4" CSTC - SCHEDULE B
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 - WATER APPURTENANCES, SEE NUD WATER SHEETS 131-159
 - ILLUMINATION EQUIPMENT, SEE ILLUMINATION SHEETS 64-73



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Call 811 before you dig.

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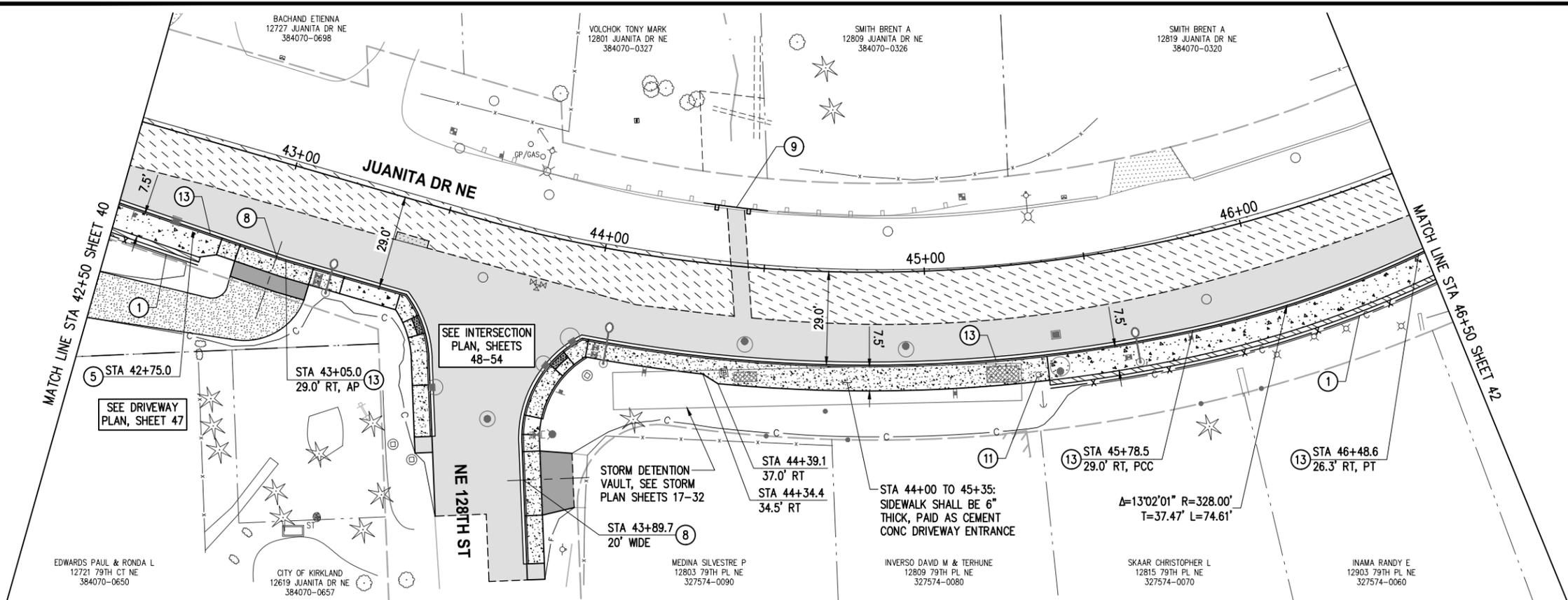
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JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

67\$ 72 67\$

ROADWAY PLAN & PROFILE

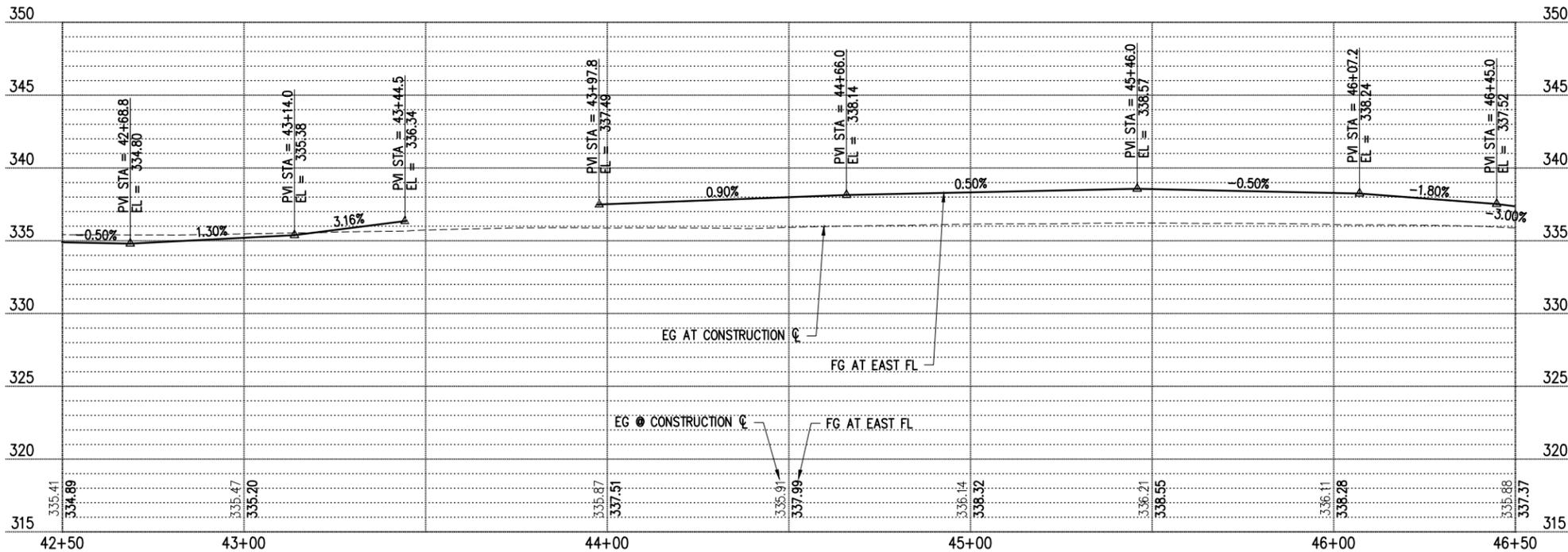
REFERENCE SHEET NO. SHEET 2) SHEETS



- GENERAL NOTES**
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 - REINSTALL MAILBOX WITH NEW WOOD POST & HARDWARE PER WSDOT STD PLAN H-70.10
 - CEMENT CONC DRIVEWAY ENTRANCE TYPE 1 PER WSDOT STD PLAN F-80.10
 - INSTALL GUARDRAIL PER WSDOT STD PLAN C-20.10. ALIGN FACE OF GUARDRAIL WITH BACK OF CURB OR EDGE OF PAVEMENT. CONNECT TO EXISTING GUARDRAIL; INSTALL GUARDRAIL ANCHOR AND END SECTION PER WSDOT STD PLAN C-23.70 WHERE NOTED.
 - RELOCATE/ADJUST/REMOVE UTILITY BY OTHERS, COORDINATE WITH UTILITY PURVEYOR
 - CEMENT CONC CURB AND GUTTER PER C.O.K. STD PLAN CK-R.17

- LEGEND**
- RIGHT OF WAY
 - - - TEMPORARY CONSTRUCTION EASEMENT
 - C- APPROX. CUT DAYLIGHT LINE
 - F- APPROX. FILL DAYLIGHT LINE
 - - - SAWCUT PER SITE PREP PLAN SHEETS 9-16
 - // BUTT JOINT PER SITE PREP PLAN SHEETS 9-16
 - X- FENCE, SEE WALL DETAILS FOR LOCATION
 - // WALL PER WALL PLAN SHEETS 55-63
 - CEMENT CONC SIDEWALK PER C.O.K. STD PLAN CK-R.23, MATCH STD WIDTH UNLESS OTHERWISE NOTED
 - CEMENT CONC CURB RAMP OR DRIVEWAY ENTRANCE, AS NOTED
 - ASPHALT DRIVEWAY, 3" COMMERCIAL HMA ON 2" CSTC
 - ASPHALT ROADWAY, 8" HMA ON 4" CSTC - SCHEDULE A
 - ASPHALT ROADWAY, 8" HMA ON 4" CSTC - SCHEDULE B
 - 2" GRIND & OVERLAY - SCHEDULE A
 - 2" GRIND & OVERLAY - SCHEDULE B
 - 2" CSTC DRIVEWAY/SHOULDER/MAILBOX CLUSTER PAD
 - STORM STRUCTURES, SEE STORM SHEETS 17-29
 - SIGN, SEE CHANNELIZATION & SIGNING SHEETS 82-89
 - WATER APPURTENANCES, SEE NUD WATER SHEETS 131-159



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JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

67\$ 72 67\$

ROADWAY PLAN & PROFILE

REFERENCE SHEET NO. SHEET 2) SHEETS

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GENERAL NOTES

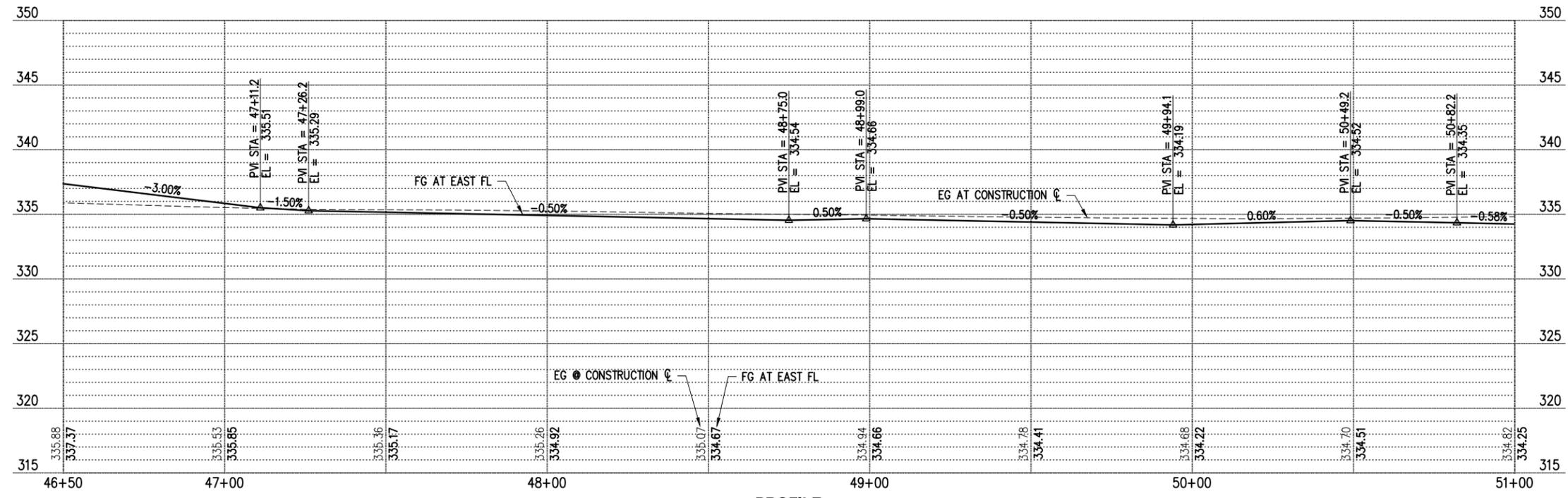
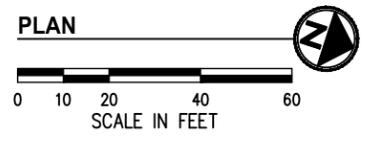
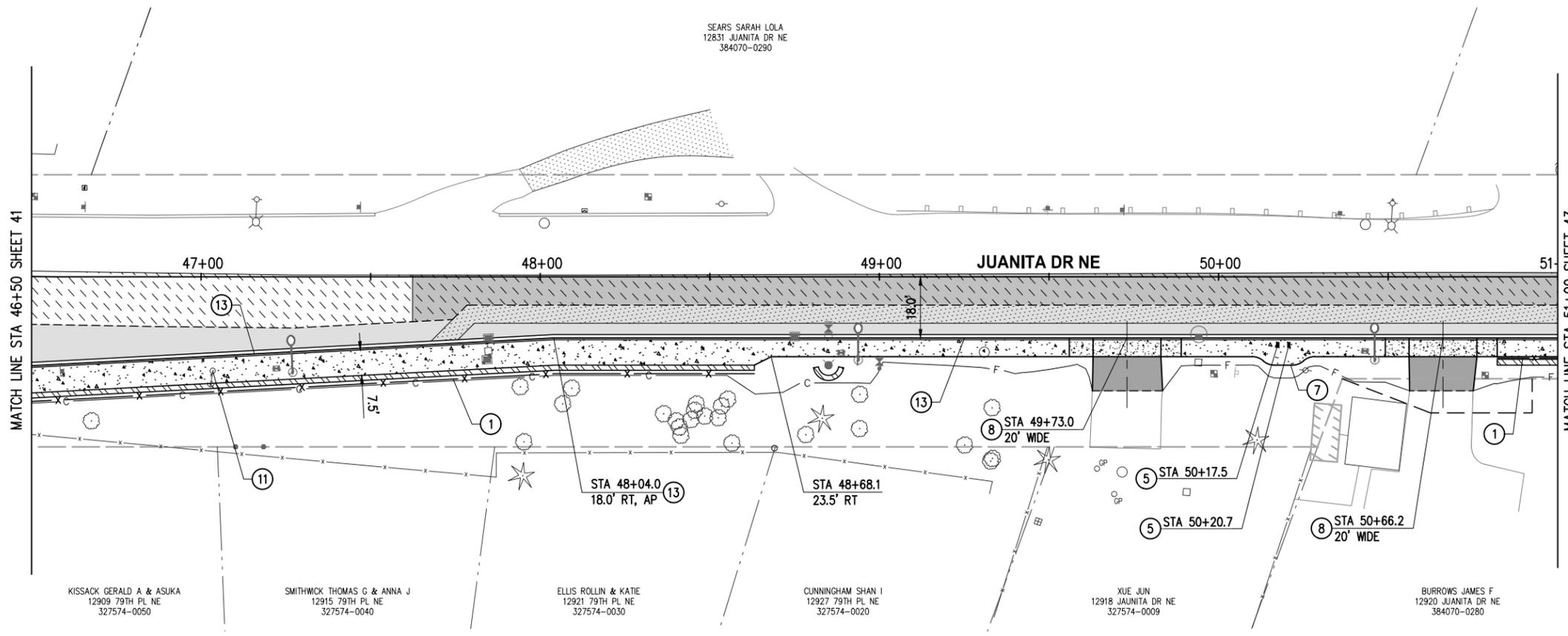
1. FOR ALIGNMENT & SURVEY CONTROL, SEE SHEETS 3-4.
2. FOR TYPICAL SECTIONS & PAVEMENT DEPTHS, SEE SHEETS 5-7.
3. FOR TREATMENT OF EXIST STORM STRUCTURES, SEE SHEETS 17-29.
4. FOR TREATMENT OF EXIST WATER APPURTENANCES AND SEWER STRUCTURES SEE SHEETS 131-159.
5. FOR LAYOUT & GRADING OF CURB RETURNS, RAMPS, AND DRIVEWAYS, SEE DRIVEWAY SHEET 47 AND INTERSECTION SHEETS 48-54.
6. FOR LANDSCAPE RESTORATION SEE SHEETS 74-81.
7. CONTRACTOR SHALL PROTECT ALL EXISTING FEATURES AND UTILITIES NOT SPECIFIED FOR REMOVAL.
8. AT PROPOSED LUMINAIRE LOCATIONS GRADE A FLAT AREA AROUND POLE (18" MIN FROM EDGE OF POLE).

CONSTRUCTION NOTES

- ① 3' CHAIN LINK SAFETY RAIL PER C.O.K. STD PLAN CK-R.51A
- ⑤ REINSTALL MAILBOX WITH NEW WOOD POST & HARDWARE PER WSDOT STD PLAN H-70.10
- ⑦ PEDESTRIAN PATH WIDENING PER DETAIL 1, SHEET 8
- ⑧ CEMENT CONC DRIVEWAY ENTRANCE TYPE 1 PER WSDOT STD PLAN F-80.10
- ⑪ RELOCATE/ADJUST/REMOVE UTILITY BY OTHERS, COORDINATE WITH UTILITY PURVEYOR
- ⑬ CEMENT CONC CURB AND GUTTER PER C.O.K. STD PLAN CK-R.17

LEGEND

- RIGHT OF WAY
- - - - TEMPORARY CONSTRUCTION EASEMENT
- C - - - APPROX. CUT DAYLIGHT LINE
- F - - - APPROX. FILL DAYLIGHT LINE
- - - - SAWCUT PER SITE PREP PLAN SHEETS 9-16
- /// BUTT JOINT PER SITE PREP PLAN SHEETS 9-16
- X - - - FENCE, SEE WALL DETAILS FOR LOCATION
- ▨ WALL PER WALL PLAN SHEETS 55-63
- ▤ CEMENT CONC SIDEWALK PER C.O.K. STD PLAN CK-R.23, MATCH STD WIDTH UNLESS OTHERWISE NOTED
- ▥ CEMENT CONC CURB RAMP OR DRIVEWAY ENTRANCE, AS NOTED
- ▧ ASPHALT DRIVEWAY, 3" COMMERCIAL HMA ON 2" CSTC
- ▨ ASPHALT ROADWAY, 8" HMA ON 4" CSTC - SCHEDULE A
- ▩ ASPHALT ROADWAY, 8" HMA ON 4" CSTC - SCHEDULE B
- 2" GRIND & OVERLAY - SCHEDULE A
- ▬ 2" GRIND & OVERLAY - SCHEDULE B
- ▭ 2" CSTC DRIVEWAY/SHOULDER/MAILBOX CLUSTER PAD
- STORM STRUCTURES, SEE STORM SHEETS 17-29
- SIGN, SEE CHANNELIZATION & SIGNING SHEETS 82-89
- WATER APPURTENANCES, SEE NUD WATER SHEETS 131-159

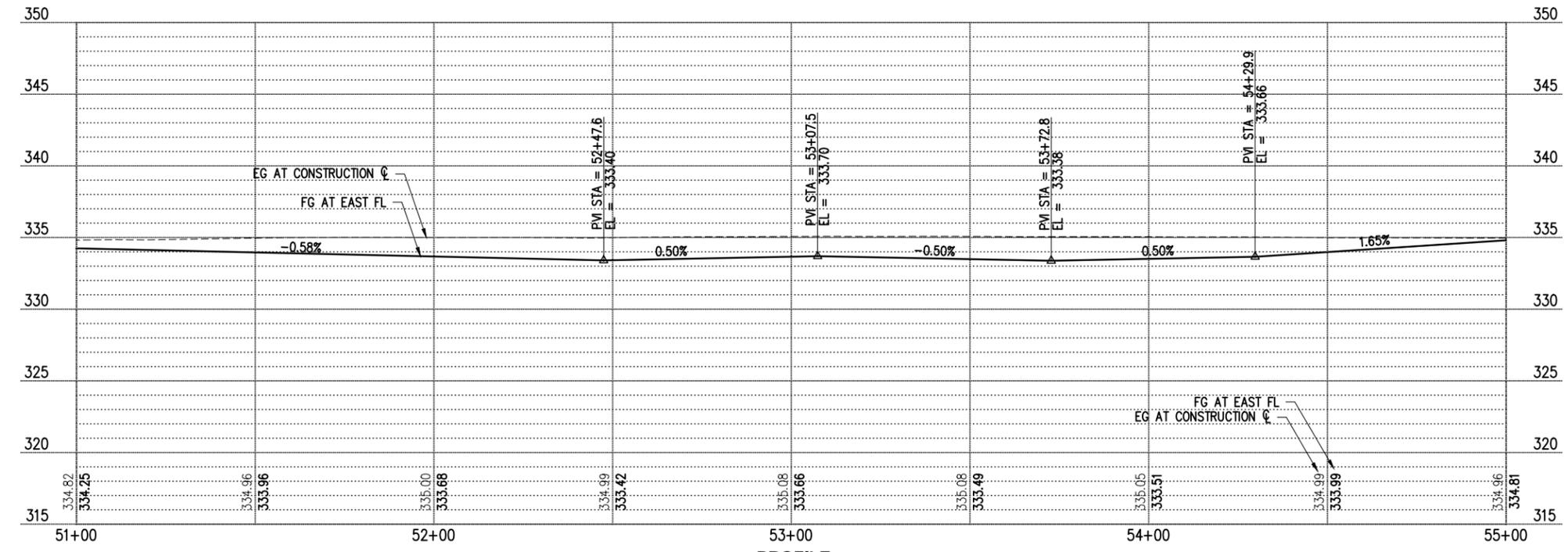
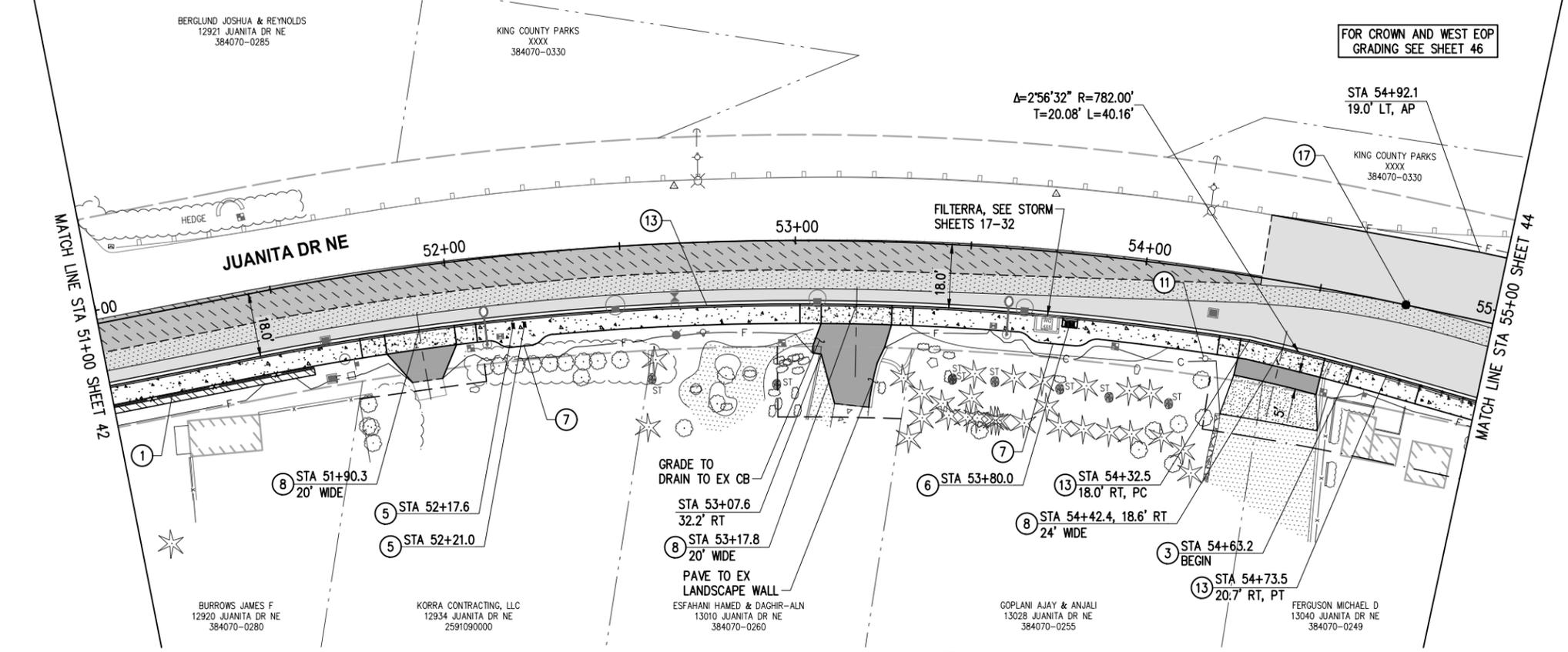


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JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS
67\$ 72 67\$
ROADWAY PLAN & PROFILE

REFERENCE SHEET NO.
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PROFILE

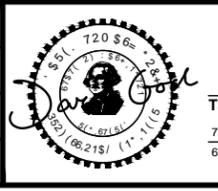
- GENERAL NOTES**
- FOR ALIGNMENT & SURVEY CONTROL, SEE SHEETS 3-4.
 - FOR TYPICAL SECTIONS & PAVEMENT DEPTHS, SEE SHEETS 5-7.
 - FOR TREATMENT OF EXIST STORM STRUCTURES, SEE SHEETS 17-29.
 - FOR TREATMENT OF EXIST WATER APPURTENANCES AND SEWER STRUCTURES SEE SHEETS 131-159.
 - FOR LAYOUT & GRADING OF CURB RETURNS, RAMP, AND DRIVEWAYS, SEE DRIVEWAY SHEET 47 AND INTERSECTION SHEETS 48-54.
 - FOR LANDSCAPE RESTORATION SEE SHEETS 74-81.
 - CONTRACTOR SHALL PROTECT ALL EXISTING FEATURES AND UTILITIES NOT SPECIFIED FOR REMOVAL.
 - AT PROPOSED LUMINAIRE LOCATIONS GRADE A FLAT AREA AROUND POLE (18" MIN FROM EDGE OF POLE).

- CONSTRUCTION NOTES**
- 3' CHAIN LINK SAFETY RAIL PER C.O.K. STD PLAN CK-R.51A
 - RAISED SIDEWALK AND SAFETY RAILING PER DETAIL 4, SHEET 8
 - REINSTALL MAILBOX WITH NEW WOOD POST & HARDWARE PER WSDOT STD PLAN H-70.10
 - MAILBOX CLUSTER PER C.O.K. STD PLAN CK-R.45A. INSTALL 4" CTSC PAD BELOW MAILBOX CLUSTER. CTSC PAD DIMENSIONS SHALL MATCH MAILBOX CLUSTER ROOF PERIMETER AND EXTEND TO BACK OF CURB
 - PEDESTRIAN PATH WIDENING PER DETAIL 1, SHEET 8
 - CEMENT CONC DRIVEWAY ENTRANCE TYPE 1 PER WSDOT STD PLAN F-80.10
 - RELOCATE/ADJUST/REMOVE UTILITY BY OTHERS, COORDINATE WITH UTILITY PURVEYOR
 - CEMENT CONC CURB AND GUTTER PER C.O.K. STD PLAN CK-R.17
 - PERPETUATE MONUMENT, INSTALL MONUMENT CASE AND COVER PER C.O.K. STD PLAN CK-R.03

- LEGEND**
- RIGHT OF WAY
 - TEMPORARY CONSTRUCTION EASEMENT
 - APPROX. CUT DAYLIGHT LINE
 - APPROX. FILL DAYLIGHT LINE
 - SAWCUT PER SITE PREP PLAN SHEETS 9-16
 - BUTT JOINT PER SITE PREP PLAN SHEETS 9-16
 - FENCE, SEE WALL DETAILS FOR LOCATION
 - WALL PER WALL PLAN SHEETS 55-63
 - CEMENT CONC SIDEWALK PER C.O.K. STD PLAN CK-R.23, MATCH STD WIDTH UNLESS OTHERWISE NOTED
 - CEMENT CONC CURB RAMP OR DRIVEWAY ENTRANCE, AS NOTED
 - ASPHALT DRIVEWAY, 3" COMMERCIAL HMA ON 2" CSTC
 - ASPHALT ROADWAY, 8" HMA ON 4" CSTC - SCHEDULE A
 - ASPHALT ROADWAY, 8" HMA ON 4" CSTC - SCHEDULE B
 - 2" GRIND & OVERLAY - SCHEDULE A
 - 2" GRIND & OVERLAY - SCHEDULE B
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 - STORM STRUCTURES, SEE STORM SHEETS 17-29
 - SIGN, SEE CHANNELIZATION & SIGNING SHEETS 82-89
 - WATER APPURTENANCES, SEE NUD WATER SHEETS 131-159



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JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

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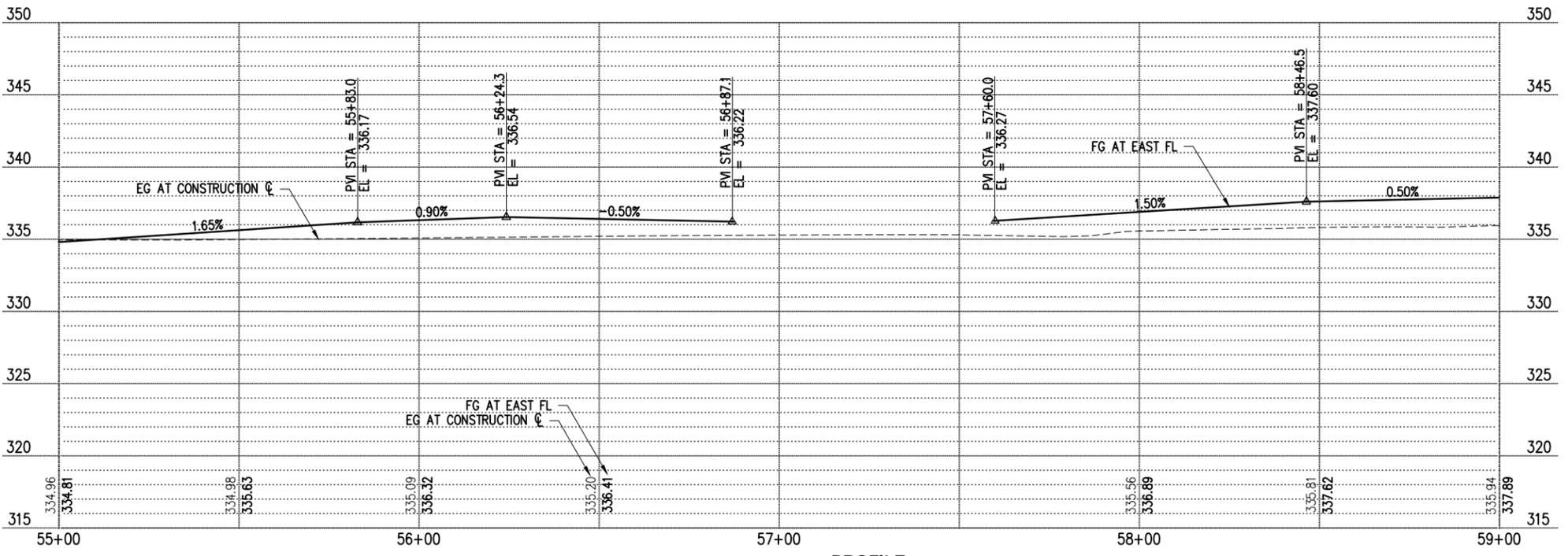
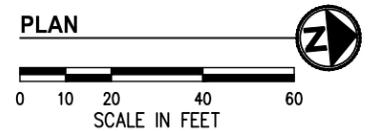
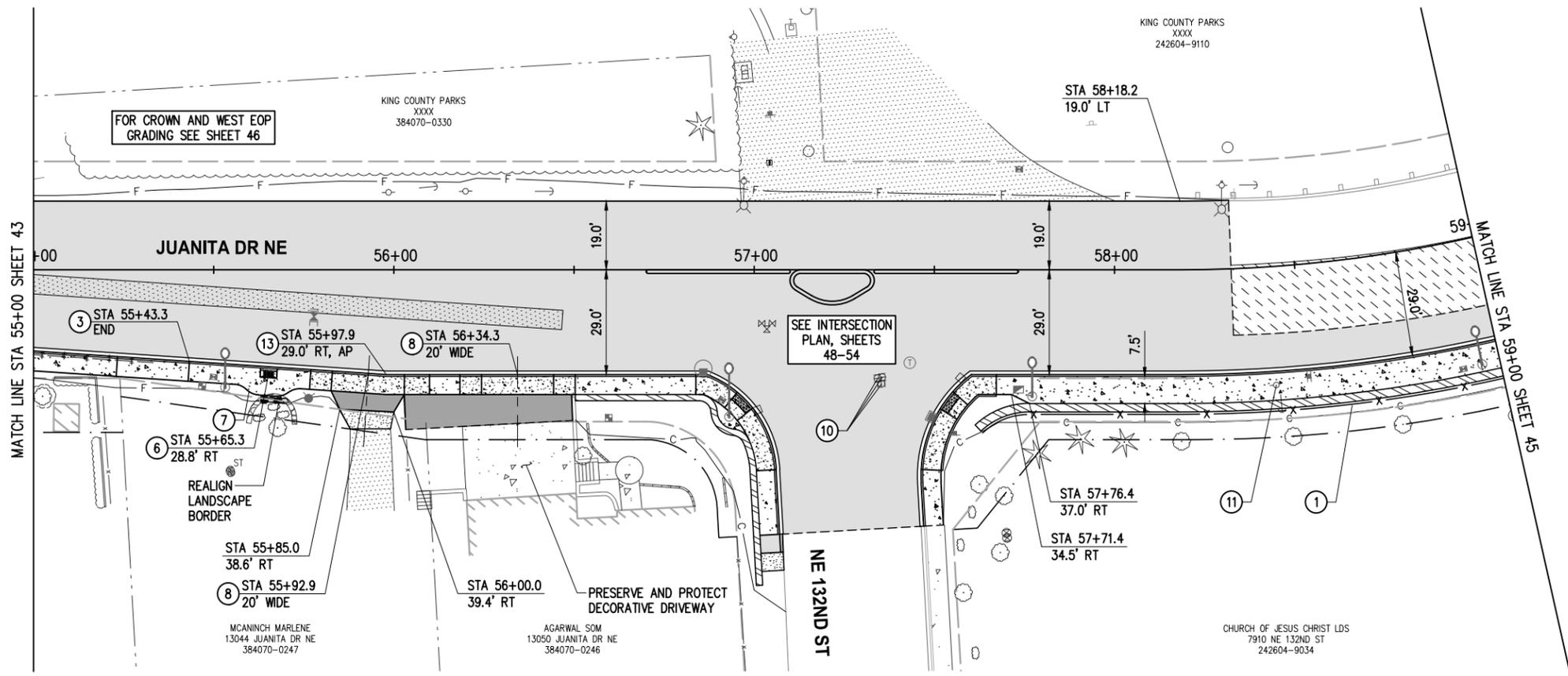
ROADWAY PLAN & PROFILE

REFERENCE SHEET NO.

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PROFILE

- ### GENERAL NOTES
- FOR ALIGNMENT & SURVEY CONTROL, SEE SHEETS 3-4.
 - FOR TYPICAL SECTIONS & PAVEMENT DEPTHS, SEE SHEETS 5-7.
 - FOR TREATMENT OF EXIST STORM STRUCTURES, SEE SHEETS 17-29.
 - FOR TREATMENT OF EXIST WATER APPURTENANCES AND SEWER STRUCTURES SEE SHEETS 131-159.
 - FOR LAYOUT & GRADING OF CURB RETURNS, RAMP, AND DRIVEWAYS, SEE DRIVEWAY SHEET 47 AND INTERSECTION SHEETS 48-54.
 - FOR LANDSCAPE RESTORATION SEE SHEETS 74-81.
 - CONTRACTOR SHALL PROTECT ALL EXISTING FEATURES AND UTILITIES NOT SPECIFIED FOR REMOVAL.
 - AT PROPOSED LUMINAIRE LOCATIONS GRADE A FLAT AREA AROUND POLE (18" MIN FROM EDGE OF POLE).

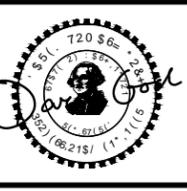
- ### CONSTRUCTION NOTES
- 3' CHAIN LINK SAFETY RAIL PER C.O.K. STD PLAN CK-R.51A
 - RAISED SIDEWALK AND SAFETY RAILING PER DETAIL 4, SHEET 8
 - MAILBOX CLUSTER PER C.O.K. STD PLAN CK-R.45A. INSTALL 4" CTSC PAD BELOW MAILBOX CLUSTER. CTSC PAD DIMENSIONS SHALL MATCH MAILBOX CLUSTER ROOF PERIMETER AND EXTEND TO BACK OF CURB
 - PEDESTRIAN PATH WIDENING PER DETAIL 1, SHEET 8
 - CEMENT CONC DRIVEWAY ENTRANCE TYPE 1 PER WSDOT STD PLAN F-80.10
 - ADJUST GAS VALVE TO FINISHED GRADE
 - RELOCATE/ADJUST/REMOVE UTILITY BY OTHERS, COORDINATE WITH UTILITY PURVEYOR
 - CEMENT CONC CURB AND GUTTER PER C.O.K. STD PLAN CK-R.17

- ### LEGEND
- RIGHT OF WAY
 - TEMPORARY CONSTRUCTION EASEMENT
 - APPROX. CUT DAYLIGHT LINE
 - APPROX. FILL DAYLIGHT LINE
 - SAWCUT PER SITE PREP PLAN SHEETS 9-16
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 - ASPHALT DRIVEWAY, 3" COMMERCIAL HMA ON 2" CSTC
 - ASPHALT ROADWAY, 8" HMA ON 4" CSTC - SCHEDULE A
 - ASPHALT ROADWAY, 8" HMA ON 4" CSTC - SCHEDULE B
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 - STORM STRUCTURES, SEE STORM SHEETS 17-29
 - SIGN, SEE CHANNELIZATION & SIGNING SHEETS 82-89
 - WATER APPURTENANCES, SEE NUD WATER SHEETS 131-159
 - ILLUMINATION EQUIPMENT, SEE ILLUMINATION SHEETS 64-73

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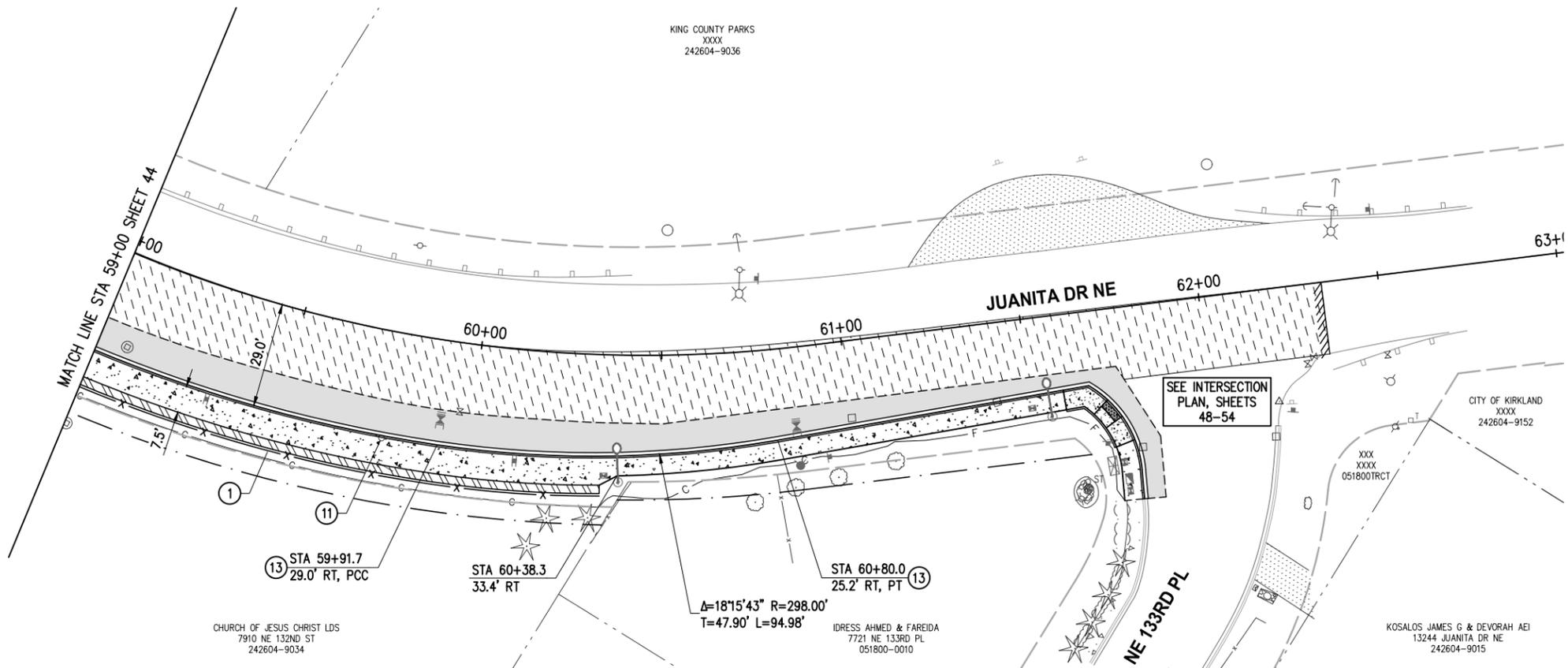
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 67\$ 72 67\$
ROADWAY PLAN & PROFILE

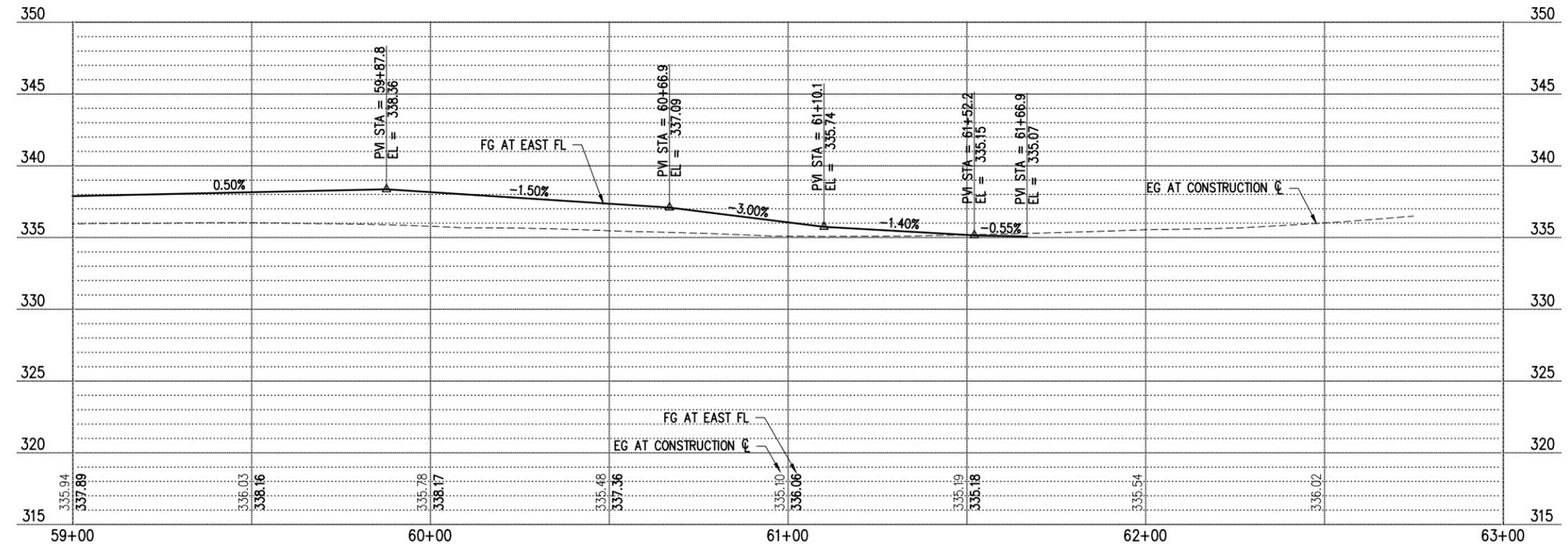
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PROFILE

GENERAL NOTES

1. FOR ALIGNMENT & SURVEY CONTROL, SEE SHEETS 3-4.
2. FOR TYPICAL SECTIONS & PAVEMENT DEPTHS, SEE SHEETS 5-7.
3. FOR TREATMENT OF EXIST STORM STRUCTURES, SEE SHEETS 17-29.
4. FOR TREATMENT OF EXIST WATER APPURTENANCES AND SEWER STRUCTURES SEE SHEETS 131-159.
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8. AT PROPOSED LUMINAIRE LOCATIONS GRADE A FLAT AREA AROUND POLE (18" MIN FROM EDGE OF POLE).

CONSTRUCTION NOTES

- 1 3' CHAIN LINK SAFETY RAIL PER C.O.K. STD PLAN CK-R.51A
- 11 RELOCATE/ADJUST/REMOVE UTILITY BY OTHERS, COORDINATE WITH UTILITY PURVEYOR
- 13 CEMENT CONC CURB AND GUTTER PER C.O.K. STD PLAN CK-R.17

LEGEND

- RIGHT OF WAY
- TEMPORARY CONSTRUCTION EASEMENT
- APPROX. CUT DAYLIGHT LINE
- APPROX. FILL DAYLIGHT LINE
- SAWCUT PER SITE PREP PLAN SHEETS 9-16
- BUTT JOINT PER SITE PREP PLAN SHEETS 9-16
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- ILLUMINATION EQUIPMENT, SEE ILLUMINATION SHEETS 64-73

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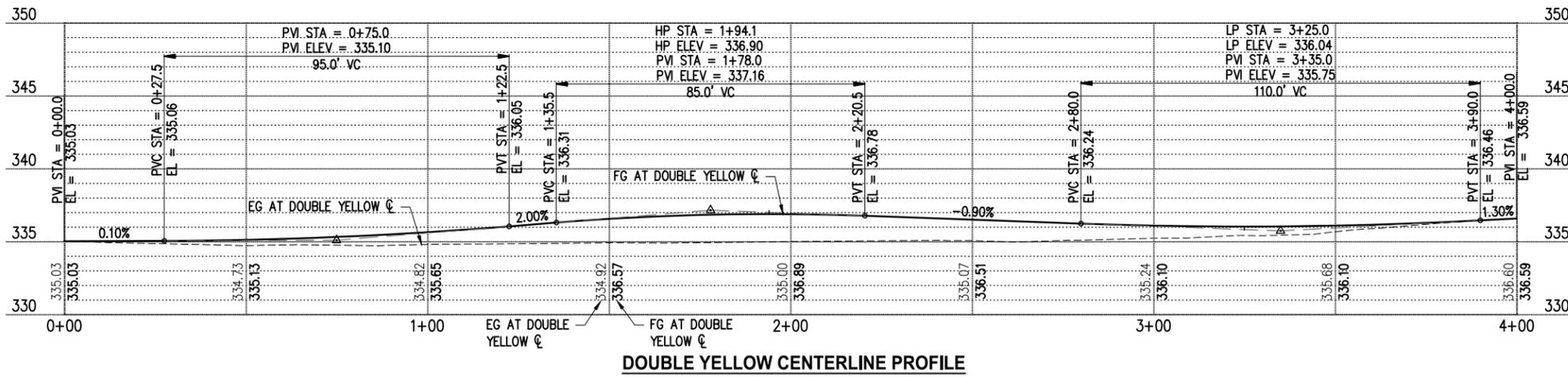
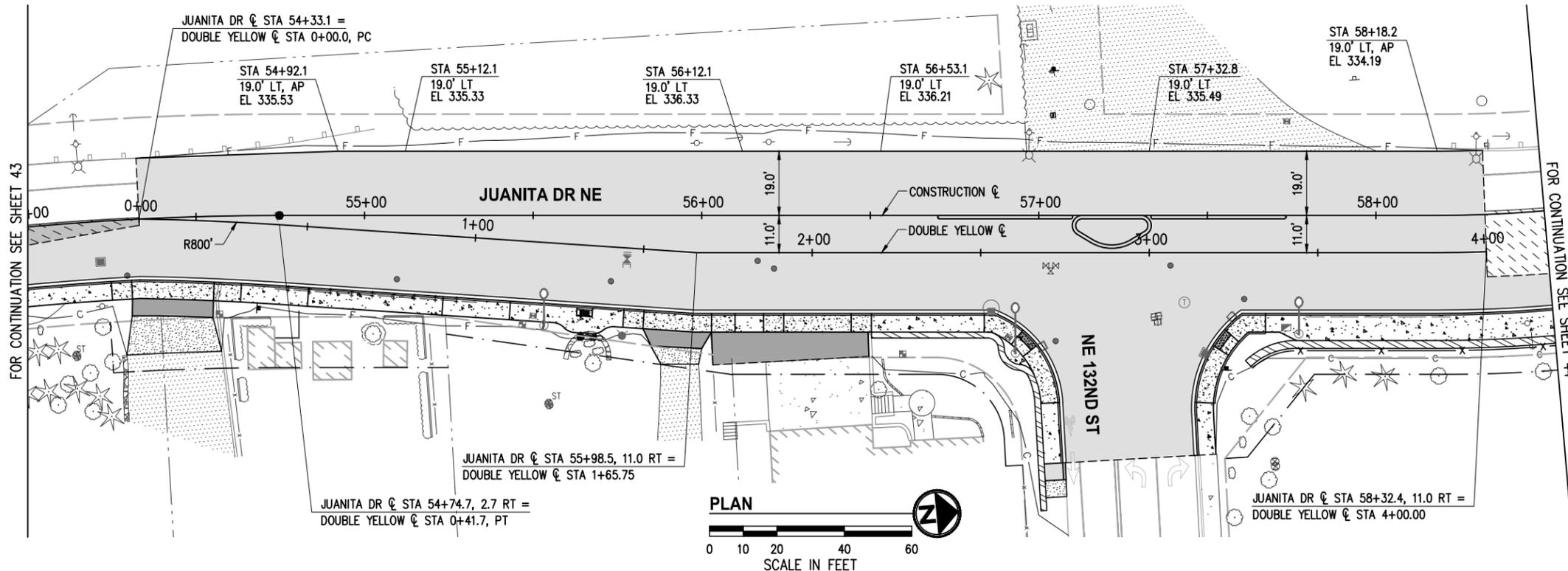
JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

67\$ 72 67\$

ROADWAY PLAN & PROFILE

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SEE SHEETS 43-44 FOR CONSTRUCTION NOTES



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JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

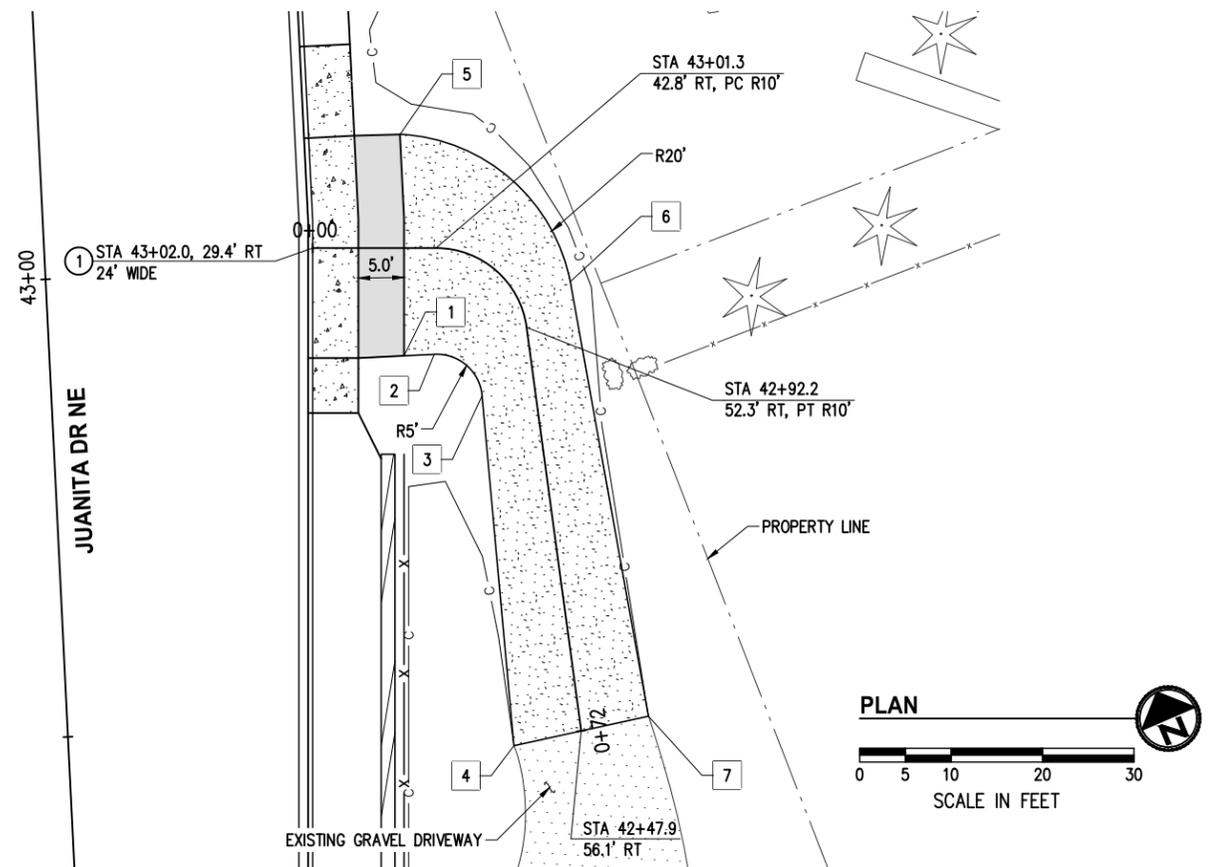
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ROADWAY PLAN & PROFILE

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POINT TABLE				
#	STATION	OFFSET	ELEV	DESCRIPTION
1	42+89.7	38.8' RT	335.85	ASPHALT CORNER
2	42+89.7	42.1' RT	336.68	PC
3	42+85.0	47.1' RT	338.39	PT
4	42+46.7	48.7' RT	342.13±	MATCH EX
5	43+13.9	39.5' RT	335.70	ASPHALT CORNER, PC
6	42+96.9	57.3' RT	338.18	PT
7	42+49.2	63.6' RT	342.85±	MATCH EX

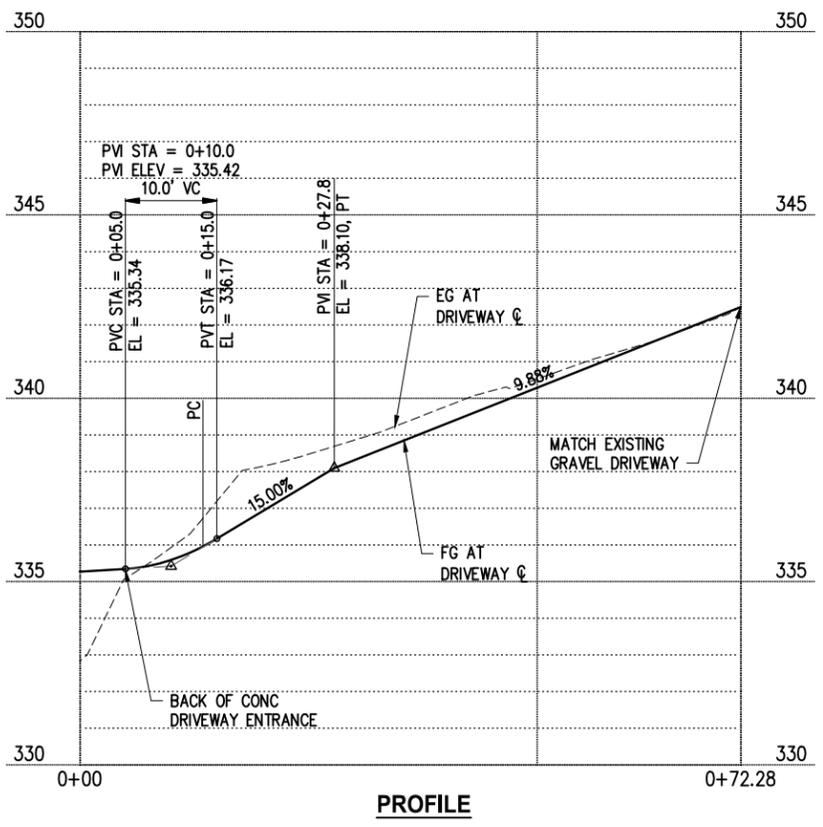


GENERAL NOTES

- SEE ROADWAY PLAN SHEETS 33-46 FOR CURB AND SIDEWALK GRADING.

CONSTRUCTION NOTES

- 6" CEMENT CONC DRIVEWAY ENTRANCE TYPE 1 PER WSDOT STD PLAN F-80.10, OVER 4" CSTC



LEGEND

- c --- APPROX CUT DAYLIGHT LINE
- x --- FENCE PER ROADWAY PLAN SHEETS 33-46
- [Hatched Box] RETAINING WALL PER PLAN SHEETS 55-63
- [Solid Grey Box] ASPHALT DRIVEWAY, 3" COMMERCIAL HMA OVER 2" CSTC
- [Dotted Box] 6" CEMENT CONC DRIVEWAY ENTRANCE, OVER 4" CSTC
- [Stippled Box] 2" CSTC DRIVEWAY

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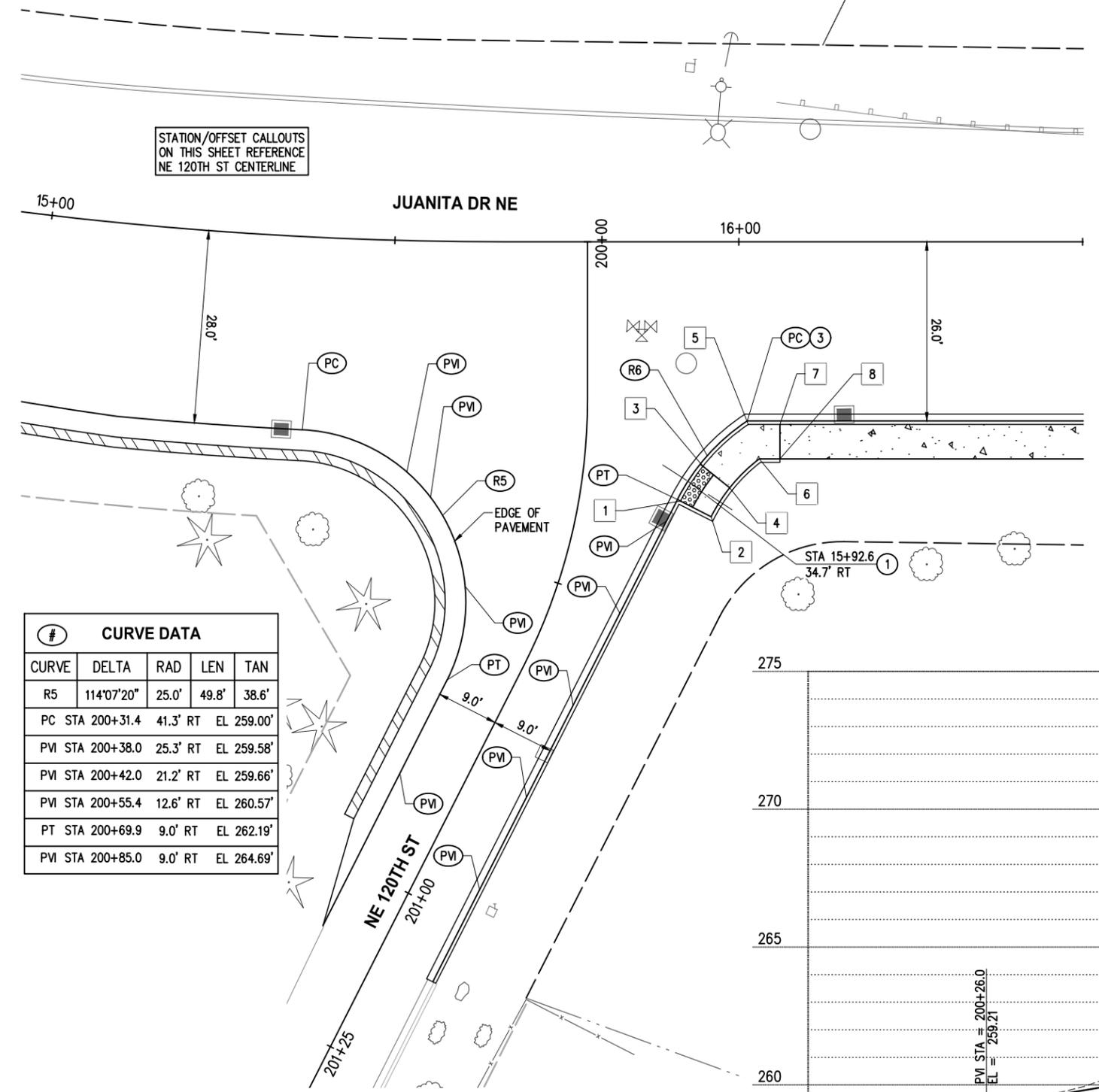
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DRIVEWAY PLAN & PROFILE

REFERENCE SHEET NO. SHEET 2) SHEETS

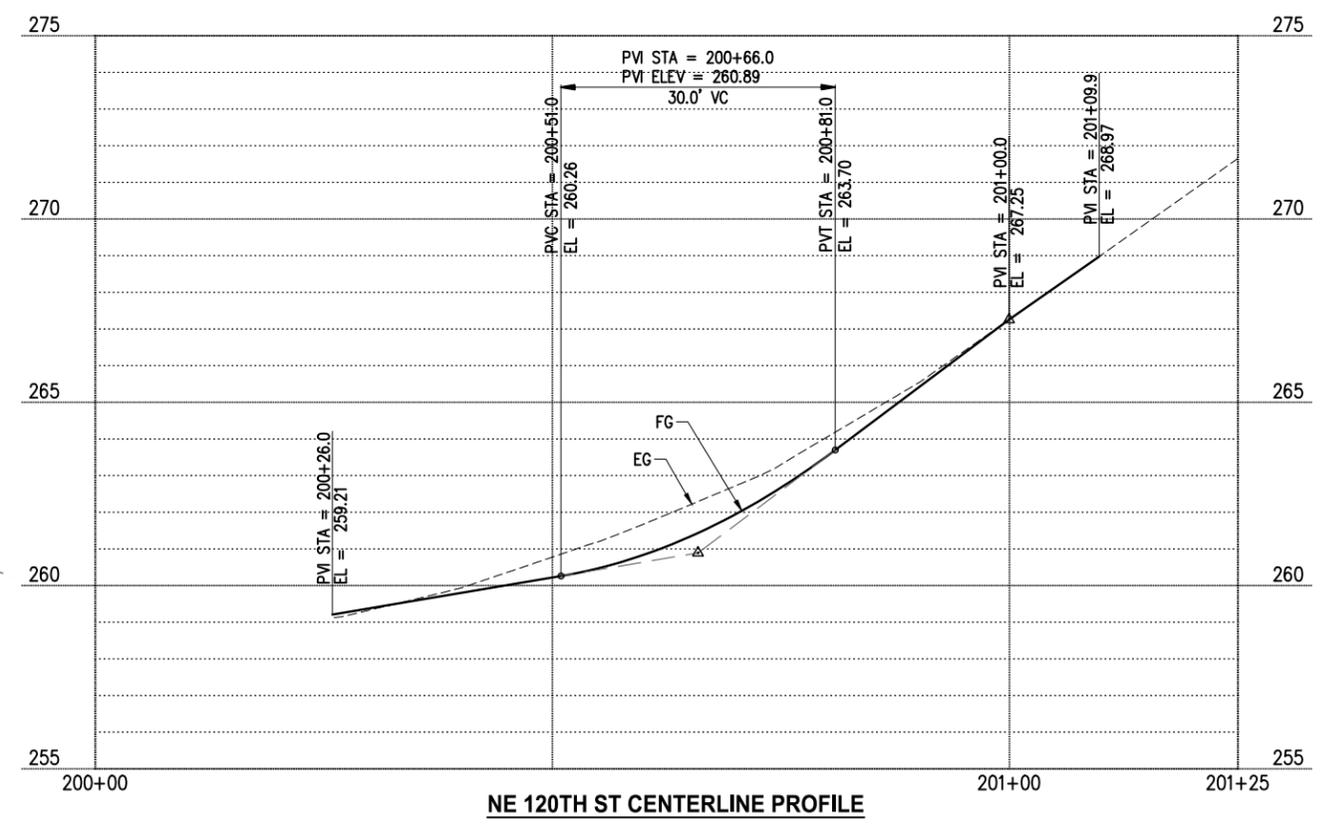
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#	CURVE DATA			
CURVE	DELTA	RAD	LEN	TAN
R5	114°07'20"	25.0'	49.8'	38.6'
PC STA 200+31.4 41.3' RT EL 259.00'				
PVI STA 200+38.0 25.3' RT EL 259.58'				
PVI STA 200+42.0 21.2' RT EL 259.66'				
PVI STA 200+55.4 12.6' RT EL 260.57'				
PT STA 200+69.9 9.0' RT EL 262.19'				
PVI STA 200+85.0 9.0' RT EL 264.69'				

#	STATION	OFFSET	ELEV	DESCRIPTION
1	200+35.5	14.6' LT	259.08	LANDING CORNER
2	200+36.8	19.4' LT	259.15	LANDING CORNER
3	200+30.9	17.2' LT	259.14	LANDING CORNER
4	200+33.1	21.5' LT	259.21	LANDING CORNER
5	200+25.7	23.4' LT	259.64	SPOT ELEV
6	200+29.5	25.3' LT	259.64	SPOT ELEV
7	200+25.6	28.0' LT	259.77	TOP OF RAMP
8	200+29.4	28.3' LT	259.85	TOP OF RAMP

#	CURVE	DELTA	RAD	LEN	TAN
	R6	29°29'20"	30.0'	15.4'	7.9'
	PC STA 200+25.3 23.3' LT EL 259.24'				
	PT STA 200+35.4 14.1' LT EL 259.08'				
	PVI STA 200+38.3 13.1' LT EL 259.04'				
	PVI STA 200+51.2 9.9' LT EL 259.64'				
	PVI STA 200+64.9 9.0' LT EL 260.99'				
	PVI STA 200+79.9 9.0' LT EL 263.09'				
	PVI STA 200+94.9 9.0' LT EL 265.94'				



- GENERAL NOTES**
- ALL RAMPS AND SIDEWALKS SHALL BE COMPLIANT WITH CURRENT ADA STANDARDS, OR TO 15' MAXIMUM RAMP LENGTH WHERE NOTED. COMPLIANCE SHALL BE CHECKED BY THE CONTRACTOR WHEN FORMS ARE SET AND WHEN CONCRETE IS POURED. ANY GRADING MODIFICATIONS REQUIRED FOR COMPLIANCE SHALL BE APPROVED BY THE ENGINEER.
 - SEE SHEETS 5-7 FOR TYPICAL SECTIONS & PAVEMENT DEPTHS. SEE ROADWAY SHEETS 33-46 FOR MAINLINE HORIZONTAL/VERTICAL GEOMETRY. SEE SITE PREP SHEETS 9-16 FOR SAWCUT/MATCH LINES.
 - FINAL RESTORATION AT BACK OF WALK SHALL BE PER ROADWAY SHEETS 33-46, WALL SHEETS 55-63, AND LANDSCAPE SHEETS 74-81, TO THE LIMITS DEFINED ON SITE PREP SHEETS 9-16.
 - CURB RAMP LOCATION SHOWN IS RAMP CENTER AT FACE OF CURB.
 - SEE ALIGNMENT & SURVEY CONTROL SHEET 3 FOR CONSTRUCTION CENTERLINE GEOMETRY.
 - PEDESTRIAN CURB SHALL BE INSTALLED PER C.O.K. STD PLAN CK-R.17A
 - ALL CURB INCLUDING PEDESTRIAN CURBS TO BE POURED PRIOR TO RAMPS/LANDINGS

- CONSTRUCTION NOTES**
- MODIFIED PARALLEL CURB RAMP PER WSDOT STD PLAN F-40.12 AND DETAIL 2, SHEET 8
 - CURB RETURN RADIUS PER DETAIL 2, SHEET 8

LEGEND

- PROPERTY LINE
- RIGHT OF WAY
- CURB AND GUTTER PER C.O.K. STD PLAN CK-R.17
- RETAINING WALL PER PLAN SHEETS 55-63
- CEMENT CONCRETE SIDEWALK PER C.O.K. STD PLAN CK-R.23, OVER 4" CSTC
- CEMENT CONC CURB RAMP AS NOTED, OVER 4" CSTC
- TRUNCATED DOME TEXTILE WARNING SURFACE PER C.O.K. STD PLAN CK-R.25B

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CITY OF KIRKLAND WASHINGTON

CITY OF KIRKLAND
 DEPARTMENT OF PUBLIC WORKS
 123 FIFTH AVENUE KIRKLAND, WA 98033
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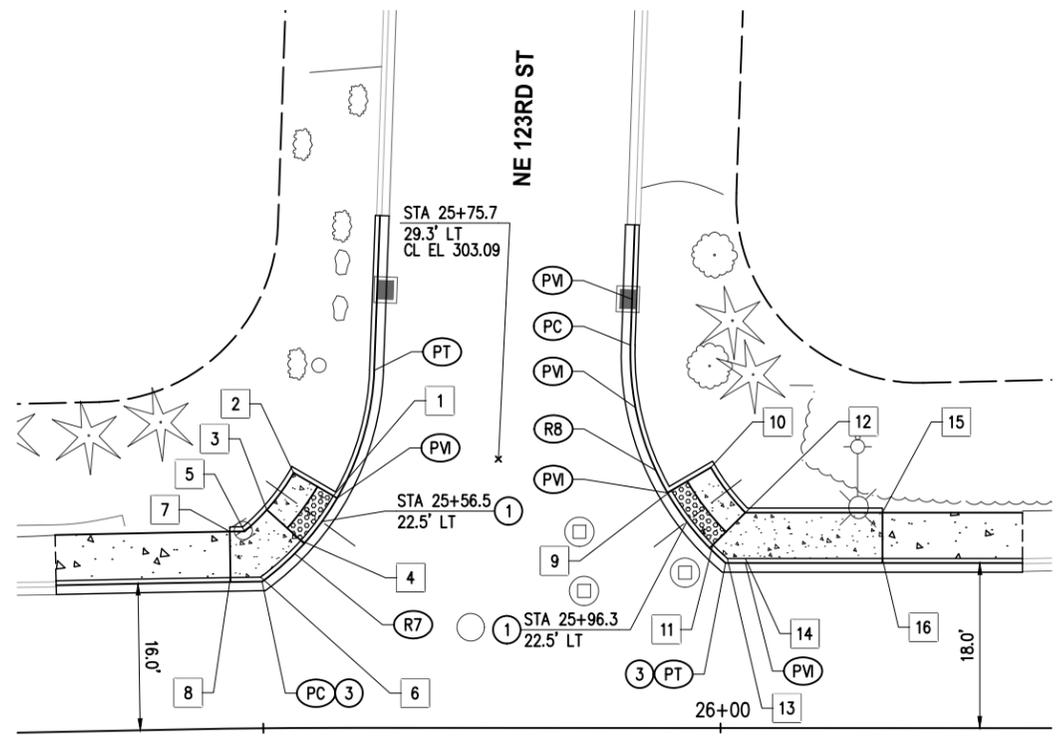
JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

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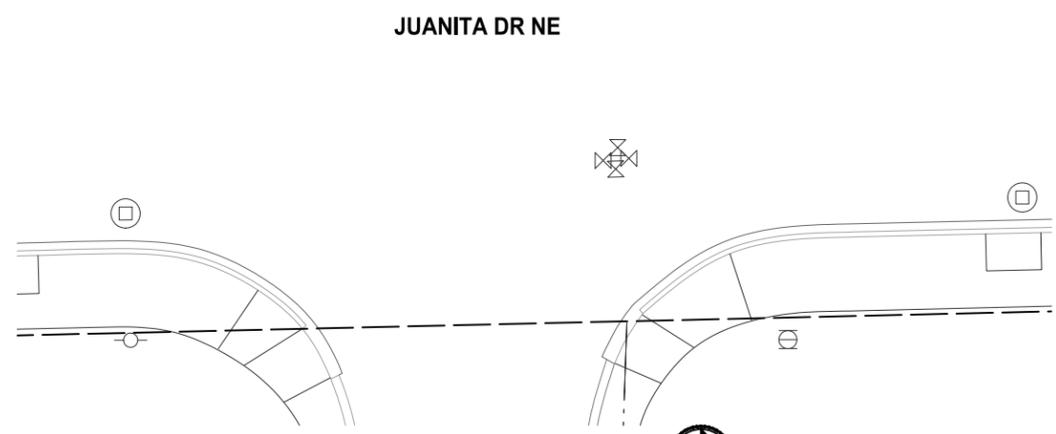
INTERSECTION PLAN

REFERENCE SHEET NO. SHEET 2) SHEETS

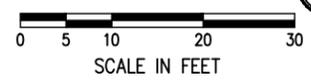
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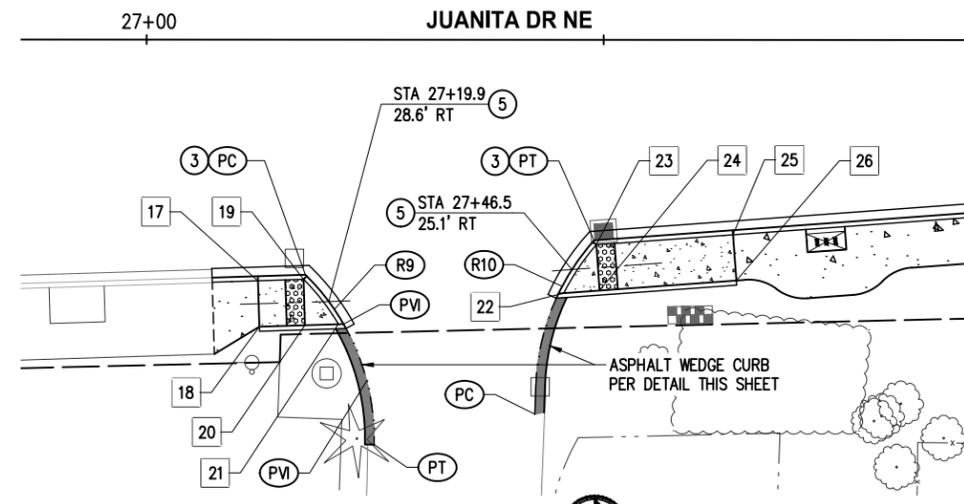


#	CURVE DATA
R7	51°35'20" 30.0' 27.0' 14.5'
PC	STA 25+50.0 16.0' LT EL 302.35'
PVI	STA 25+58.1 25.1' LT EL 302.44'
PT	STA 25+62.1 39.0' LT EL 302.64'

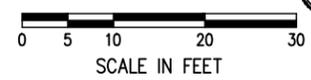
#	CURVE DATA
R8	51°14'50" 30.0' 26.8' 14.4'
PM	STA 25+90.3 46.8' LT EL 302.71'
PC	STA 25+90.1 41.8' LT EL 302.81'
PVI	STA 25+90.7 34.9' LT EL 302.95'
PVI	STA 25+94.2 25.7' LT EL 303.83'
PT	STA 26+00.6 18.0' LT EL 304.00'
PM	STA 26+02.7 18.0' LT EL 304.17'

#	POINT TABLE
1	25+57.7 25.3' LT 302.44 LANDING CORNER
2	25+53.5 27.8' LT 302.52 LANDING CORNER
3	25+50.6 23.8' LT 302.44 LANDING CORNER
4	25+54.2 20.5' LT 302.37 LANDING CORNER
5	25+48.2 21.5' LT 302.61 SPOT ELEV
6	25+49.9 16.5' LT 302.61 SPOT ELEV
7	25+46.6 21.5' LT 302.69 TOP OF RAMP
8	25+46.6 16.5' LT 302.62 TOP OF RAMP
9	25+94.6 25.9' LT 303.83 LANDING CORNER
10	25+98.9 28.4' LT 303.88 LANDING CORNER
11	25+99.2 20.0' LT 303.95 LANDING CORNER
12	26+02.7 23.5' LT 304.00 LANDING CORNER
13	26+00.7 18.5' LT 304.00 SPOT ELEV

#	POINT TABLE
14	26+02.7 18.5' LT 304.09 SPOT ELEV
15	26+17.7 23.5' LT 305.54 TOP OF RAMP
16	26+17.7 18.5' LT 305.46 TOP OF RAMP
17	27+12.2 26.3' RT 309.70 TOP OF RAMP
18	27+12.3 31.3' RT 309.77 TOP OF RAMP
19	27+17.2 26.1' RT 309.54 LANDING CORNER
20	27+17.3 31.1' RT 309.62 LANDING CORNER
21	27+21.0 31.0' RT 309.59 LANDING CORNER
22	27+45.6 27.6' RT 310.98 LANDING CORNER
23	27+49.2 22.4' RT 310.92 LANDING CORNER
24	27+49.6 27.3' RT 311.00 LANDING CORNER
25	27+64.2 21.3' RT 312.10 TOP OF RAMP
26	27+64.5 26.2' RT 312.17 TOP OF RAMP



PLAN



#	CURVE DATA
R9	47°04'40" 25.0' 20.6' 10.9'
PC	STA 27+17.4 25.6' RT EL 309.54'
PVI	STA 27+21.6 31.0' RT EL 309.59'
PM	STA 27+24.1 37.2' RT EL 310.64'
PT	STA 27+24.9 44.2' RT EL 311.34±

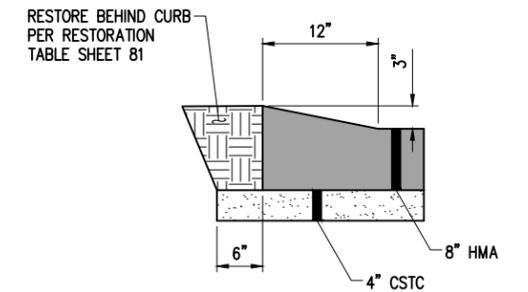
#	CURVE DATA
R10	39°53'10" 25.0' 17.4' 9.1'
PC	STA 27+42.5 40.8' RT EL 311.24±
PT	STA 27+49.0 21.9' RT EL 310.91'

GENERAL NOTES

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- PEDESTRIAN CURB SHALL BE INSTALLED PER C.O.K. STD PLAN CK-R.17A
- ALL CURB INCLUDING PEDESTRIAN CURBS TO BE POURED PRIOR TO RAMPS/LANDINGS

CONSTRUCTION NOTES

- MODIFIED PARALLEL CURB RAMP PER WSDOT STD PLAN F-40.12 AND DETAIL 2, SHEET 8
- CURB RETURN RADIUS PER DETAIL 2, SHEET 8
- SINGLE DIRECTION CURB RAMP PER WSDOT STD PLAN F-40.16



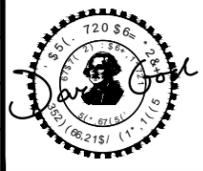
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LEGEND

- PROPERTY LINE
- RIGHT OF WAY
- CURB AND GUTTER PER C.O.K. STD PLAN CK-R.17
- RETAINING WALL PER PLAN SHEETS 55-63
- CEMENT CONCRETE SIDEWALK PER C.O.K. STD PLAN CK-R.23, OVER 4" CSTC
- CEMENT CONC CURB RAMP AS NOTED, OVER 4" CSTC
- TRUNCATED DOME TEXTILE WARNING SURFACE PER C.O.K. STD PLAN CK-R.25B



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DEPARTMENT OF PUBLIC WORKS
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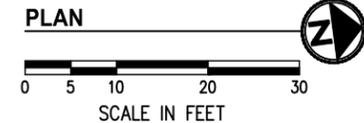
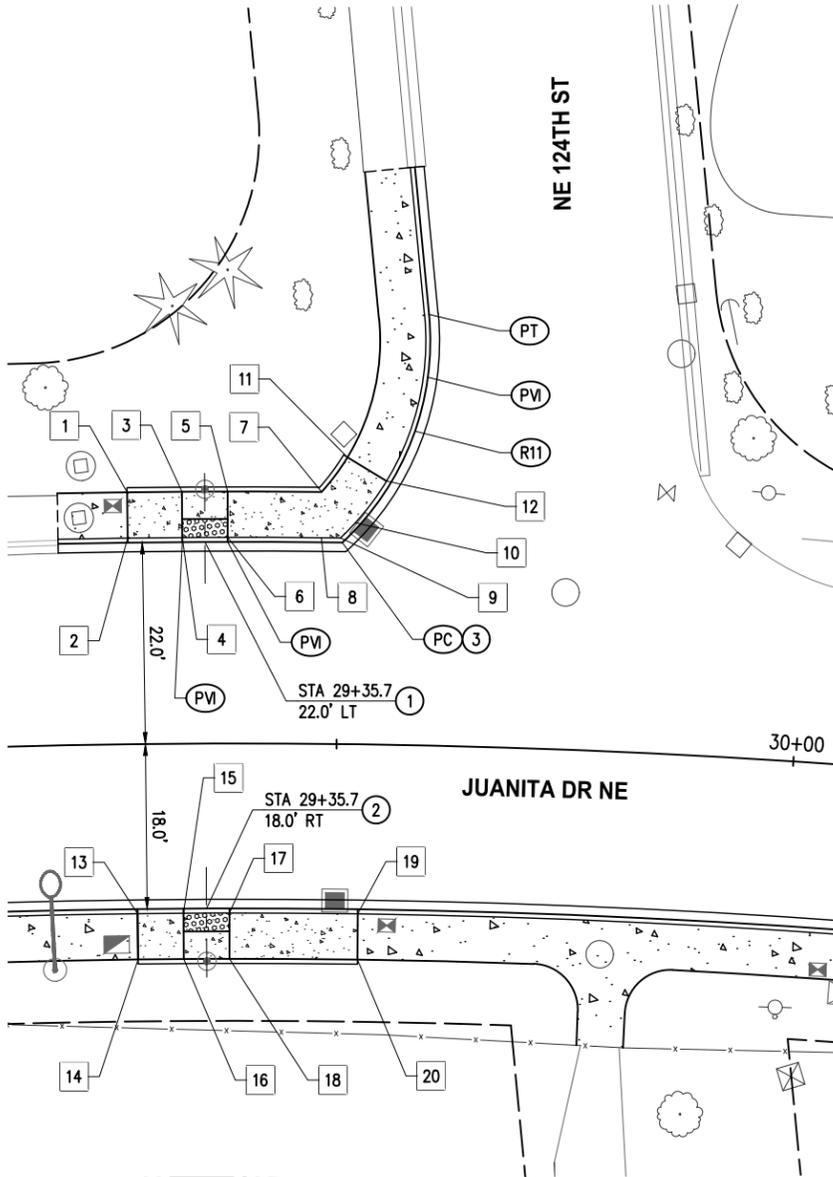
JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

NE 123RD ST & JUANITA

INTERSECTION PLAN

REFERENCE SHEET NO. SHEET 2) SHEETS

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#	CURVE DATA
	CURVE DELTA RAD LEN TAN
	R11 52°42'10" 30.0' 27.6' 14.9'
	PVI STA 29+33.3 22.0' LT EL 321.28'
	PVI STA 29+38.1 22.0' LT EL 321.35'
	PC STA 29+50.4 22.0' LT EL 321.93'
	PVI STA 29+59.1 40.2' LT EL 322.91'
	PT STA 29+59.1 47.0' LT EL 323.09'

#	STATION	OFFSET	ELEV	DESCRIPTION
1	29+27.4	27.5' LT	321.54	TOP OF RAMP
2	29+27.4	22.5' LT	321.46	TOP OF RAMP
3	29+33.3	27.5' LT	321.36	LANDING CORNER
4	29+33.3	22.5' LT	321.28	LANDING CORNER
5	29+38.1	27.5' LT	321.44	LANDING CORNER
6	29+38.1	22.5' LT	321.35	LANDING CORNER
7	29+48.1	27.5' LT	322.46	SPOT ELEV
8	29+48.1	22.5' LT	322.38	SPOT ELEV
9	29+50.2	22.5' LT	322.46	SPOT ELEV
10	29+51.7	24.2' LT	322.53	SPOT ELEV

#	STATION	OFFSET	ELEV	DESCRIPTION
11	29+50.8	31.3' LT	322.94	TOP OF RAMP
12	29+55.0	28.8' LT	322.86	TOP OF RAMP
13	29+28.0	18.5' RT	319.28	TOP OF RAMP
14	29+28.0	23.5' RT	319.36	TOP OF RAMP
15	29+33.1	18.5' RT	319.00	LANDING CORNER
16	29+33.1	23.5' RT	319.08	LANDING CORNER
17	29+38.3	18.5' RT	319.08	LANDING CORNER
18	29+38.3	23.5' RT	319.16	LANDING CORNER
19	29+52.6	18.5' RT	320.13	TOP OF RAMP
20	29+52.6	23.5' RT	320.21	TOP OF RAMP

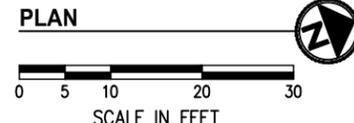
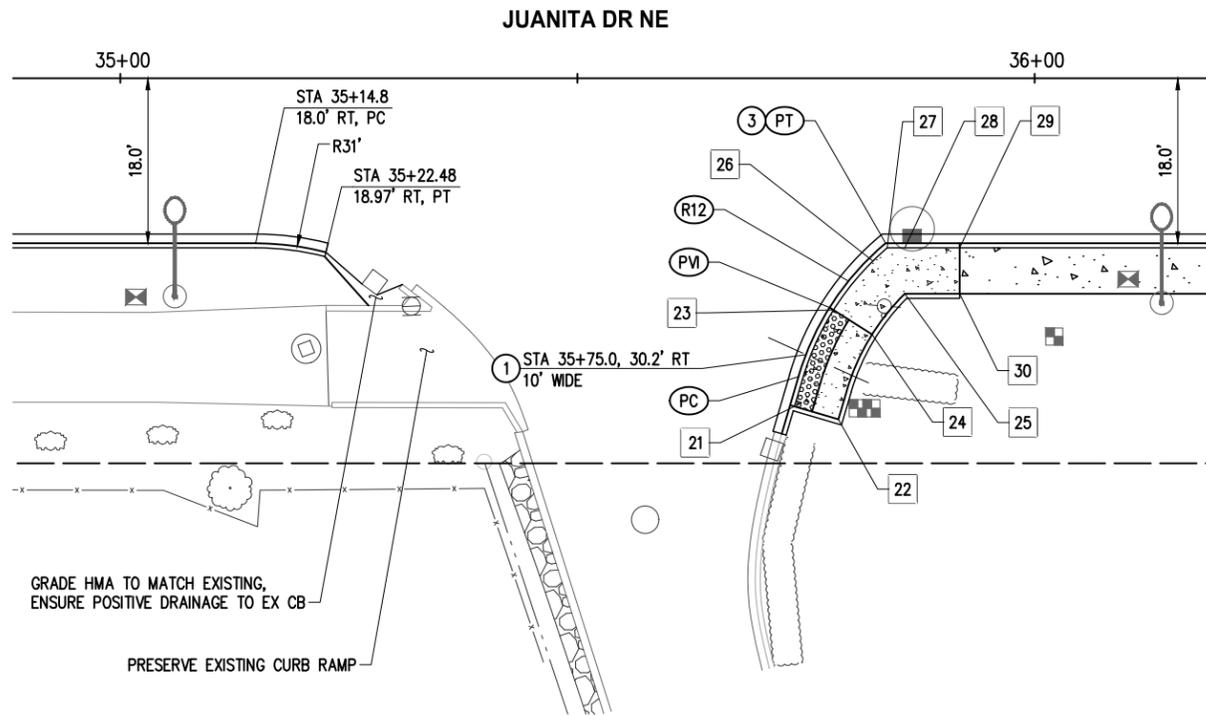
#	STATION	OFFSET	ELEV	DESCRIPTION
21	35+73.7	35.8' RT	332.89	LANDING CORNER
22	35+78.5	37.2' RT	332.96	LANDING CORNER
23	35+78.0	25.3' RT	333.01	LANDING CORNER
24	35+82.2	27.9' RT	333.08	LANDING CORNER
25	35+85.8	23.5' RT	333.51	SPOT ELEV
26	35+82.3	19.9' RT	333.31	SPOT ELEV
27	35+83.9	18.5' RT	333.47	SPOT ELEV
28	35+85.8	18.5' RT	333.57	SPOT ELEV
29	35+91.8	18.5' RT	333.88	TOP OF RAMP
30	35+91.8	23.5' RT	333.96	TOP OF RAMP

GENERAL NOTES

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7. ALL CURB INCLUDING PEDESTRIAN CURBS TO BE POURED PRIOR TO RAMPS/LANDINGS

CONSTRUCTION NOTES

1. MODIFIED PARALLEL CURB RAMP PER WSDOT STD PLAN F-40.12 AND DETAIL 2, SHEET 8
2. PARALLEL CURB RAMP PER WSDOT STD PLAN F-40.12
3. CURB RETURN RADIUS PER DETAIL 2, SHEET 8



#	CURVE DATA
	CURVE DELTA RAD LEN TAN
	R12 33°41'30" 30.0' 17.6' 9.1'
	PC STA 35+74.2 32.5' RT EL 332.65'
	PVI STA 35+77.6 25.0' RT EL 333.01'
	PT STA 35+83.7 18.0' RT EL 333.28'

LEGEND

- PROPERTY LINE
- - - RIGHT OF WAY
- ==== CURB AND GUTTER PER C.O.K. STD PLAN CK-R.17
- //// RETAINING WALL PER PLAN SHEETS 55-63
- ▣ CEMENT CONCRETE SIDEWALK PER C.O.K. STD PLAN CK-R.23, OVER 4" CSTC
- ▨ CEMENT CONC CURB RAMP AS NOTED, OVER 4" CSTC
- ☑ TRUNCATED DOME TEXTILE WARNING SURFACE PER C.O.K. STD PLAN CK-R.25B



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BID DOCUMENT

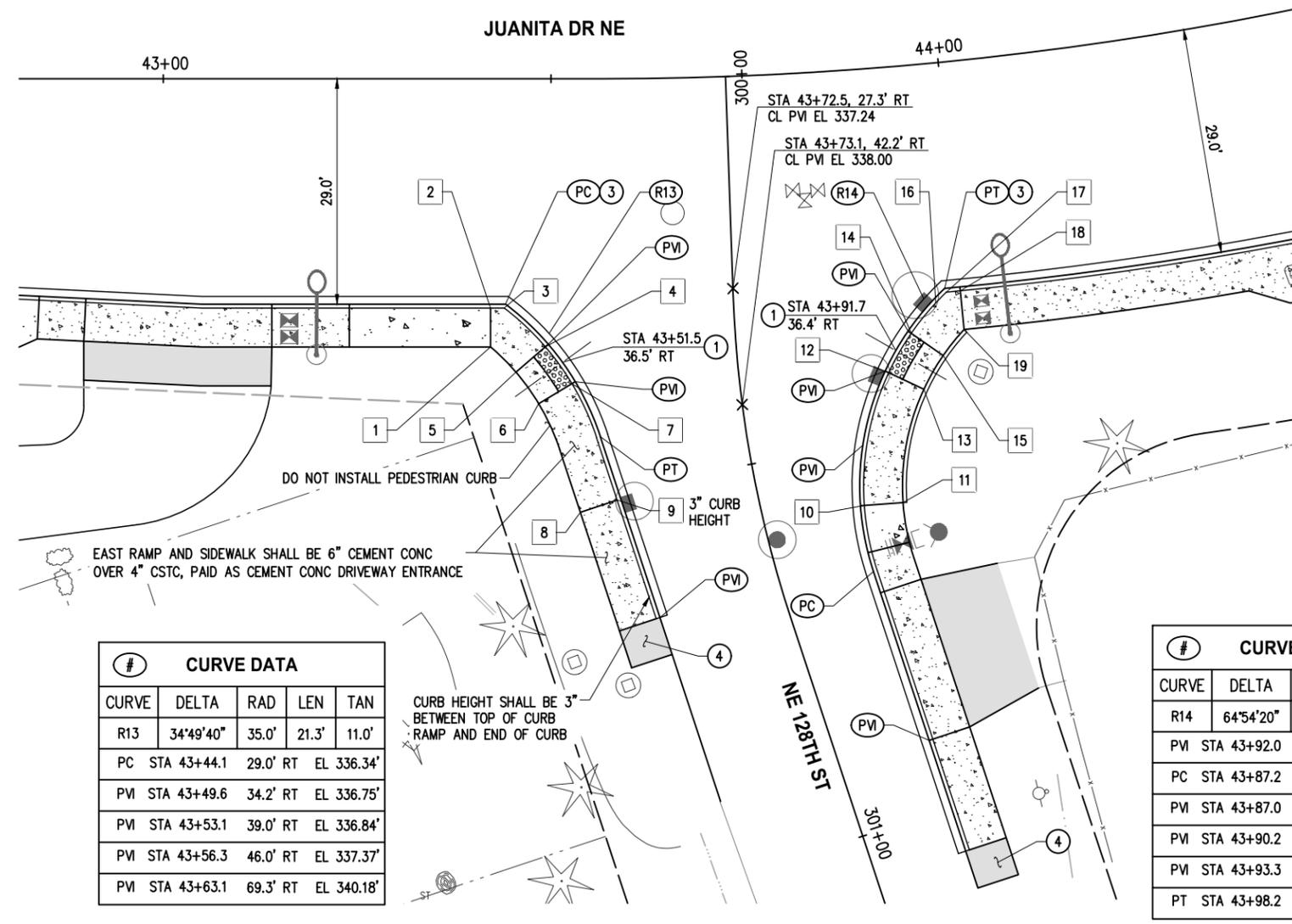


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**JUANITA DRIVE MULTIMODAL,
INTERSECTION & SAFETY IMPROVEMENTS**
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INTERSECTION PLAN

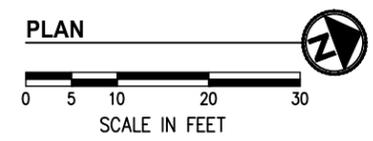
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#	CURVE DATA																																			
	<table border="1"><thead><tr><th>CURVE</th><th>DELTA</th><th>RAD</th><th>LEN</th><th>TAN</th></tr></thead><tbody><tr><td>R13</td><td>34°49'40"</td><td>35.0'</td><td>21.3'</td><td>11.0'</td></tr><tr><td>PC</td><td>STA 43+44.1</td><td>29.0' RT</td><td>EL 336.34'</td><td></td></tr><tr><td>PVI</td><td>STA 43+49.6</td><td>34.2' RT</td><td>EL 336.75'</td><td></td></tr><tr><td>PVI</td><td>STA 43+53.1</td><td>39.0' RT</td><td>EL 336.84'</td><td></td></tr><tr><td>PVI</td><td>STA 43+56.3</td><td>46.0' RT</td><td>EL 337.37'</td><td></td></tr><tr><td>PVI</td><td>STA 43+63.1</td><td>69.3' RT</td><td>EL 340.18'</td><td></td></tr></tbody></table>	CURVE	DELTA	RAD	LEN	TAN	R13	34°49'40"	35.0'	21.3'	11.0'	PC	STA 43+44.1	29.0' RT	EL 336.34'		PVI	STA 43+49.6	34.2' RT	EL 336.75'		PVI	STA 43+53.1	39.0' RT	EL 336.84'		PVI	STA 43+56.3	46.0' RT	EL 337.37'		PVI	STA 43+63.1	69.3' RT	EL 340.18'	
CURVE	DELTA	RAD	LEN	TAN																																
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	<table border="1"><thead><tr><th>CURVE</th><th>DELTA</th><th>RAD</th><th>LEN</th><th>TAN</th></tr></thead><tbody><tr><td>R14</td><td>64°54'20"</td><td>35.0'</td><td>39.6'</td><td>22.3'</td></tr><tr><td>PVI</td><td>STA 43+92.0</td><td>86.8' RT</td><td>EL 342.78'</td><td></td></tr><tr><td>PC</td><td>STA 43+87.2</td><td>64.6' RT</td><td>EL 340.52'</td><td></td></tr><tr><td>PVI</td><td>STA 43+87.0</td><td>48.3' RT</td><td>EL 338.57'</td><td></td></tr><tr><td>PVI</td><td>STA 43+90.2</td><td>39.0' RT</td><td>EL 337.89'</td><td></td></tr><tr><td>PVI</td><td>STA 43+93.3</td><td>34.1' RT</td><td>EL 337.80'</td><td></td></tr><tr><td>PT</td><td>STA 43+98.2</td><td>29.0' RT</td><td>EL 337.49'</td><td></td></tr></tbody></table>	CURVE	DELTA	RAD	LEN	TAN	R14	64°54'20"	35.0'	39.6'	22.3'	PVI	STA 43+92.0	86.8' RT	EL 342.78'		PC	STA 43+87.2	64.6' RT	EL 340.52'		PVI	STA 43+87.0	48.3' RT	EL 338.57'		PVI	STA 43+90.2	39.0' RT	EL 337.89'		PVI	STA 43+93.3	34.1' RT	EL 337.80'		PT	STA 43+98.2	29.0' RT	EL 337.49'	
CURVE	DELTA	RAD	LEN	TAN																																					
R14	64°54'20"	35.0'	39.6'	22.3'																																					
PVI	STA 43+92.0	86.8' RT	EL 342.78'																																						
PC	STA 43+87.2	64.6' RT	EL 340.52'																																						
PVI	STA 43+87.0	48.3' RT	EL 338.57'																																						
PVI	STA 43+90.2	39.0' RT	EL 337.89'																																						
PVI	STA 43+93.3	34.1' RT	EL 337.80'																																						
PT	STA 43+98.2	29.0' RT	EL 337.49'																																						



POINT TABLE				
#	STATION	OFFSET	ELEV	DESCRIPTION
1	43+42.2	34.5' RT	336.87	TOP OF RAMP
2	43+42.2	29.5' RT	336.80	TOP OF RAMP
3	43+43.9	29.5' RT	336.79	SPOT ELEV
4	43+49.3	34.5' RT	336.75	LANDING CORNER
5	43+45.5	37.7' RT	336.80	LANDING CORNER
6	43+48.4	41.8' RT	336.89	LANDING CORNER
7	43+52.7	39.2' RT	336.84	LANDING CORNER
8	43+53.8	55.8' RT	338.65	TOP OF RAMP
9	43+58.3	54.2' RT	338.58	TOP OF RAMP
10	43+86.7	56.0' RT	339.99	TOP OF RAMP
11	43+91.1	56.0' RT	340.06	TOP OF RAMP
12	43+90.6	39.2' RT	337.89	LANDING CORNER
13	43+94.6	41.7' RT	337.94	LANDING CORNER
14	43+93.7	34.4' RT	337.80	LANDING CORNER
15	43+97.2	37.6' RT	337.85	LANDING CORNER
16	43+96.9	30.8' RT	338.01	SPOT ELEV
17	43+98.3	29.5' RT	338.01	SPOT ELEV
18	44+00.0	29.5' RT	338.01	TOP OF RAMP
19	44+00.0	34.5' RT	338.09	TOP OF RAMP

- ### GENERAL NOTES
1. ALL RAMPS AND SIDEWALKS SHALL BE COMPLIANT WITH CURRENT ADA STANDARDS, OR TO 15' MAXIMUM RAMP LENGTH WHERE NOTED. COMPLIANCE SHALL BE CHECKED BY THE CONTRACTOR WHEN FORMS ARE SET AND WHEN CONCRETE IS POURED. ANY GRADING MODIFICATIONS REQUIRED FOR COMPLIANCE SHALL BE APPROVED BY THE ENGINEER.
 2. SEE SHEETS 5-7 FOR TYPICAL SECTIONS & PAVEMENT DEPTHS. SEE ROADWAY SHEETS 33-46 FOR MAINLINE HORIZONTAL/VERTICAL GEOMETRY. SEE SITE PREP SHEETS 9-16 FOR SAWCUT/MATCH LINES.
 3. FINAL RESTORATION AT BACK OF WALK SHALL BE PER ROADWAY SHEETS 33-46, WALL SHEETS 55-63, AND LANDSCAPE SHEETS 74-81, TO THE LIMITS DEFINED ON SITE PREP SHEETS 9-16.
 4. CURB RAMP LOCATION SHOWN IS RAMP CENTER AT FACE OF CURB.
 5. SEE ALIGNMENT & SURVEY CONTROL SHEET 3 FOR CONSTRUCTION CENTERLINE GEOMETRY.
 6. PEDESTRIAN CURB SHALL BE INSTALLED PER C.O.K. STD PLAN CK-R.17A
 7. ALL CURB INCLUDING PEDESTRIAN CURBS TO BE POURED PRIOR TO RAMPS/LANDINGS

- ### CONSTRUCTION NOTES
1. MODIFIED PARALLEL CURB RAMP PER WSDOT STD PLAN F-40.12 AND DETAIL 2, SHEET 8
 3. CURB RETURN RADIUS PER DETAIL 2, SHEET 8
 4. HMA RAMP, 2" COMMERCIAL HMA OVER 2" CSTC

- ### LEGEND
- PROPERTY LINE
 - RIGHT OF WAY
 - ==== CURB AND GUTTER PER C.O.K. STD PLAN CK-R.17
 - /// RETAINING WALL PER PLAN SHEETS 55-63
 - ▭ CEMENT CONCRETE SIDEWALK PER C.O.K. STD PLAN CK-R.23, OVER 4" CSTC
 - ▭ CEMENT CONC CURB RAMP AS NOTED, OVER 4" CSTC
 - ▭ TRUNCATED DOME TEXTILE WARNING SURFACE PER C.O.K. STD PLAN CK-R.25B

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BID DOCUMENT

KPG PSOMAS
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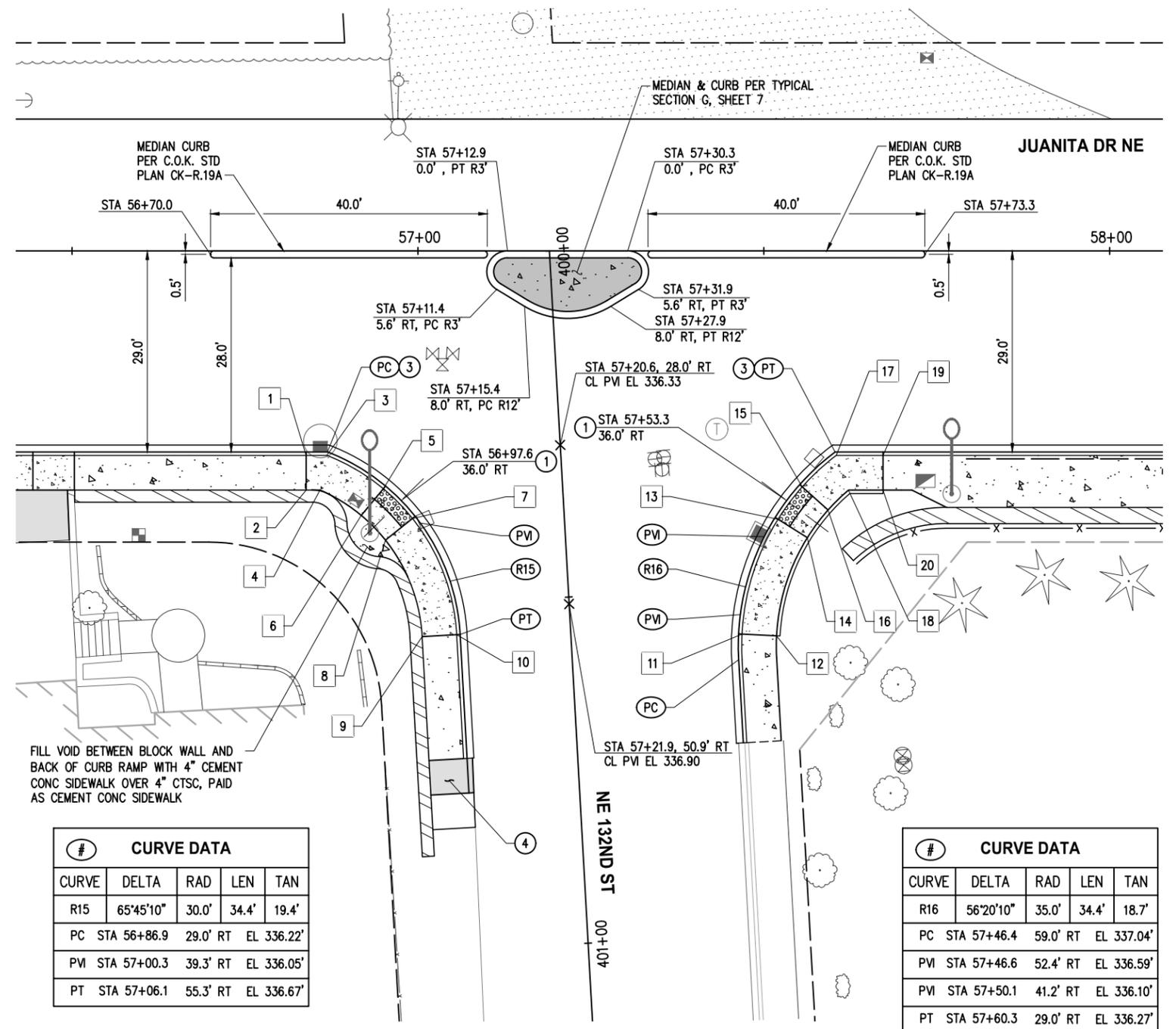
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JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

1(7+ 67 -8\$1,7\$
INTERSECTION PLAN

REFERENCE SHEET NO.
SHEET 2)
SHEETS

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POINT TABLE				
#	STATION	OFFSET	ELEV	DESCRIPTION
1	56+83.8	29.5' RT	336.74	TOP OF RAMP
2	56+83.8	34.5' RT	336.81	TOP OF RAMP
3	56+86.8	29.5' RT	336.68	SPOT ELEV
4	56+85.8	34.5' RT	336.68	SPOT ELEV
5	56+95.0	34.3' RT	336.12	LANDING CORNER
6	56+91.8	38.2' RT	336.19	LANDING CORNER
7	56+99.2	38.6' RT	336.05	LANDING CORNER
8	56+95.3	41.7' RT	336.13	LANDING CORNER
9	57+00.6	55.6' RT	337.24	TOP OF RAMP
10	57+05.6	55.3' RT	337.17	TOP OF RAMP
11	57+46.9	55.3' RT	337.28	TOP OF RAMP
12	57+51.8	55.5' RT	337.36	TOP OF RAMP
13	57+52.1	38.7' RT	336.13	LANDING CORNER
14	57+56.3	41.4' RT	336.20	LANDING CORNER
15	57+55.6	34.0' RT	336.20	LANDING CORNER
16	57+59.3	37.4' RT	336.27	LANDING CORNER
17	57+60.5	29.5' RT	336.60	SPOT ELEV
18	57+62.2	34.5' RT	336.58	SPOT ELEV
19	57+67.2	29.5' RT	336.88	TOP OF RAMP
20	57+67.2	34.5' RT	336.96	TOP OF RAMP

- GENERAL NOTES**
- ALL RAMPS AND SIDEWALKS SHALL BE COMPLIANT WITH CURRENT ADA STANDARDS, OR TO 15' MAXIMUM RAMP LENGTH WHERE NOTED. COMPLIANCE SHALL BE CHECKED BY THE CONTRACTOR WHEN FORMS ARE SET AND WHEN CONCRETE IS POURED. ANY GRADING MODIFICATIONS REQUIRED FOR COMPLIANCE SHALL BE APPROVED BY THE ENGINEER.
 - SEE SHEETS 5-7 FOR TYPICAL SECTIONS & PAVEMENT DEPTHS. SEE ROADWAY SHEETS 33-46 FOR MAINLINE HORIZONTAL/VERTICAL GEOMETRY. SEE SITE PREP SHEETS 9-16 FOR SAWCUT/MATCH LINES.
 - FINAL RESTORATION AT BACK OF WALK SHALL BE PER ROADWAY SHEETS 33-46, WALL SHEETS 55-63, AND LANDSCAPE SHEETS 74-81, TO THE LIMITS DEFINED ON SITE PREP SHEETS 9-16.
 - CURB RAMP LOCATION SHOWN IS RAMP CENTER AT FACE OF CURB.
 - SEE ALIGNMENT & SURVEY CONTROL SHEET 3 FOR CONSTRUCTION CENTERLINE GEOMETRY.
 - PEDESTRIAN CURB SHALL BE INSTALLED PER C.O.K. STD PLAN CK-R.17A
 - ALL CURB INCLUDING PEDESTRIAN CURBS TO BE POURED PRIOR TO RAMPS/LANDINGS

- CONSTRUCTION NOTES**
- MODIFIED PARALLEL CURB RAMP PER WSDOT STD PLAN F-40.12 AND DETAIL 2, SHEET 8
 - CURB RETURN RADIUS PER DETAIL 2, SHEET 8
 - HMA RAMP, 2" COMMERCIAL HMA OVER 2" CSTC

LEGEND

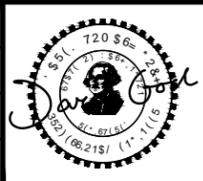
---	PROPERTY LINE
- - - - -	RIGHT OF WAY
=====	CURB AND GUTTER PER C.O.K. STD PLAN CK-R.17
/	RETAINING WALL PER PLAN SHEETS 55-63
▨	CEMENT CONCRETE SIDEWALK PER C.O.K. STD PLAN CK-R.23, OVER 4" CSTC
▩	CEMENT CONC CURB RAMP AS NOTED, OVER 4" CSTC
▧	TRUNCATED DOME TEXTILE WARNING SURFACE PER C.O.K. STD PLAN CK-R.25B
▤	STAMPED CEMENT CONC MEDIAN PER TYPICAL SECTION G, SHEET 7

#	CURVE DATA				
CURVE	DELTA	RAD	LEN	TAN	
R15	65°45'10"	30.0'	34.4'	19.4'	
PC	STA 56+86.9	29.0' RT	EL 336.22'		
PM	STA 57+00.3	39.3' RT	EL 336.05'		
PT	STA 57+06.1	55.3' RT	EL 336.67'		

#	CURVE DATA				
CURVE	DELTA	RAD	LEN	TAN	
R16	56°20'10"	35.0'	34.4'	18.7'	
PC	STA 57+46.4	59.0' RT	EL 337.04'		
PVI	STA 57+46.6	52.4' RT	EL 336.59'		
PVI	STA 57+50.1	41.2' RT	EL 336.10'		
PT	STA 57+60.3	29.0' RT	EL 336.27'		



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-HIHUVRQ \$YHQXH
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6HDWUOH :HCKPQL60MHH

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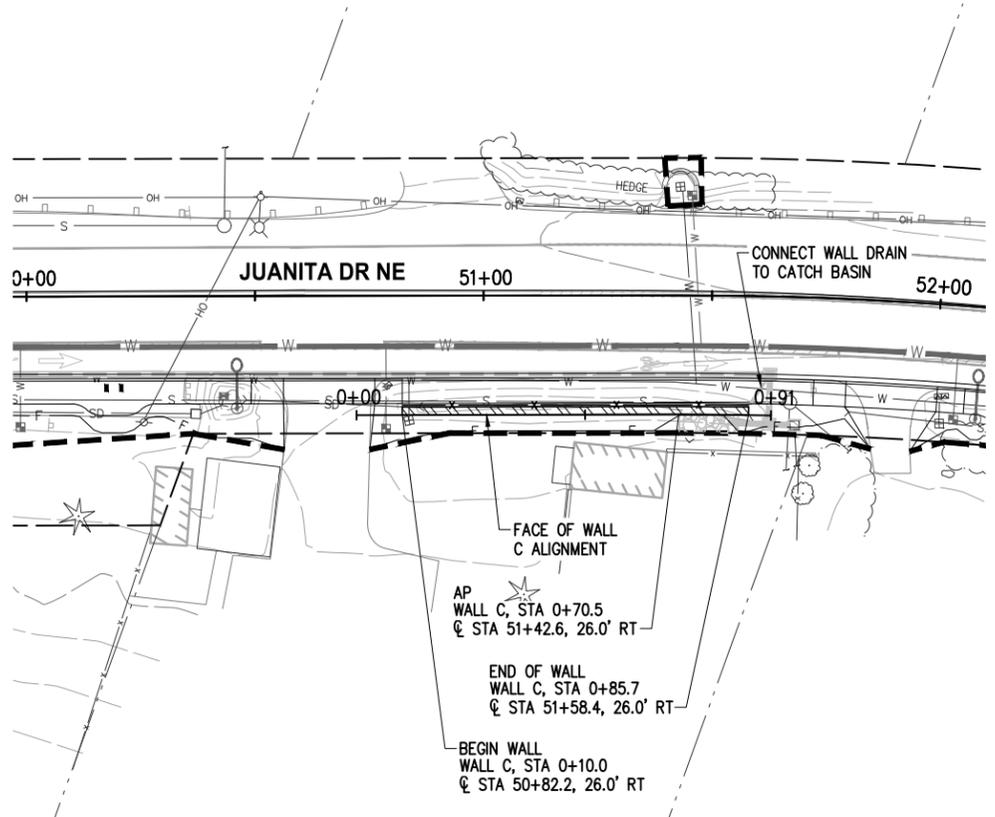
JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

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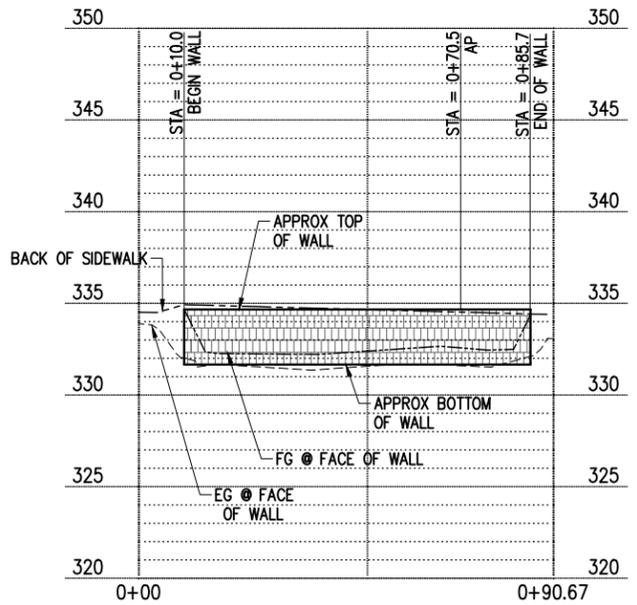
INTERSECTION PLAN

REFERENCE SHEET NO. SHEET 2) SHEETS

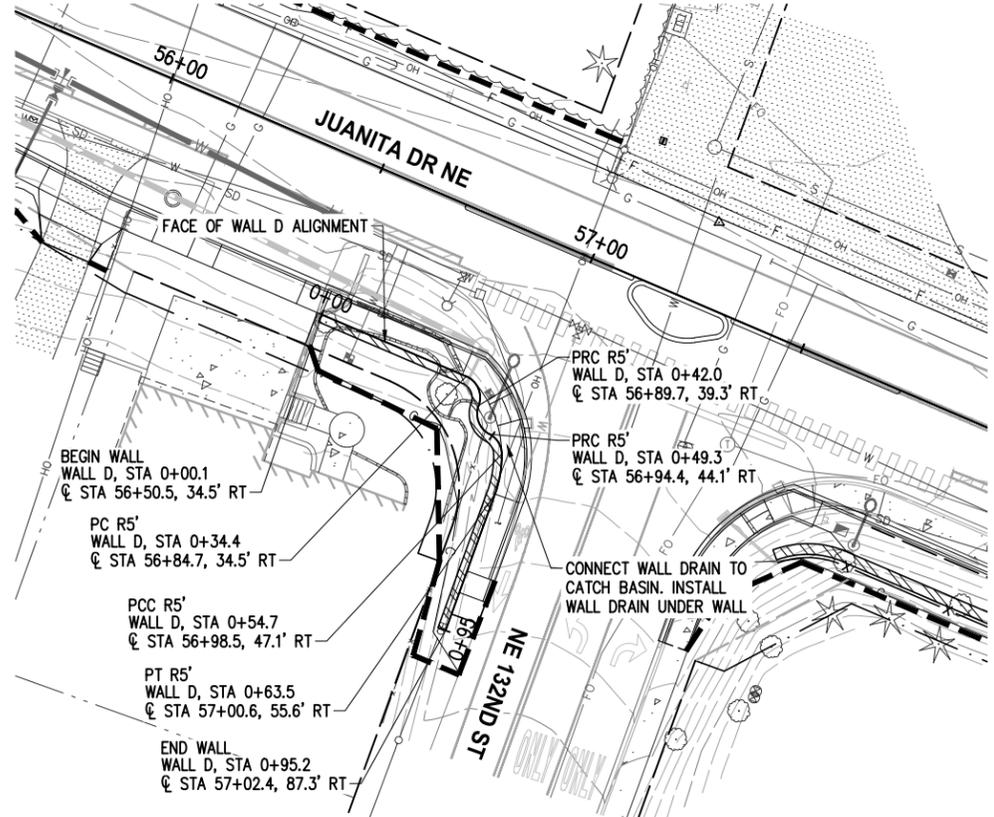
2FWREHU 30 '\$5(. *2&+ . 2352-(&76? .5./\$1'? -8\$1.7\$ '5.9(. 17(56(&7.21 6\$)(7<?'(6,*1?5\$.1*6?&2175\$&&7? :\$// B02B%/2&. '* :/DIRXW 1DPH :\$///



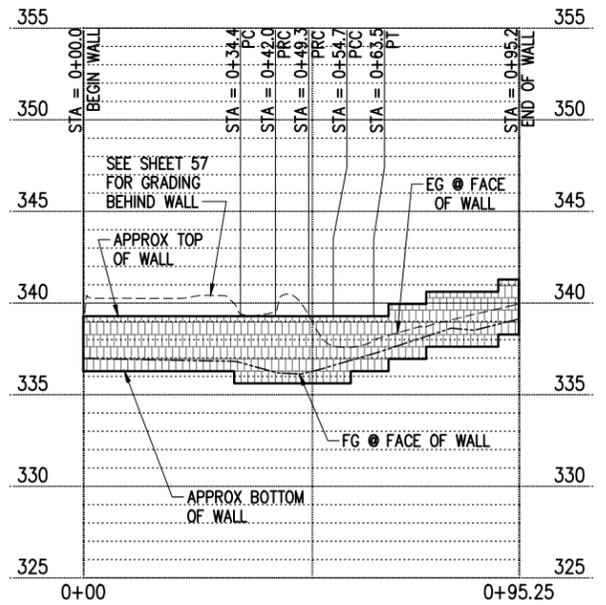
PLAN
SCALE IN FEET



MODULAR BLOCK FILL WALL C PROFILE
4V:1H



PLAN
SCALE IN FEET



MODULAR BLOCK FILL WALL D PROFILE
4V:1H

GENERAL NOTES

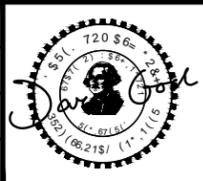
1. ELEVATIONS HAVE BEEN PROVIDED FOR USE AS A GUIDE FOR CONSTRUCTION. THE RETAINING WALL SHALL BE CONSTRUCTED PER MANUFACTURER'S RECOMMENDATIONS EXCEPT AS ADJUSTED IN THE FIELD BY THE ENGINEER.
2. CONTRACTOR SHALL PROTECT EXISTING UTILITIES WHILE EXCAVATING FOR WALL. THIS MAY REQUIRE TEMPORARY SHORING DEPENDING ON THE CONSTRUCTION METHODS EMPLOYED BY THE CONTRACTOR.
3. LOCATIONS NOTED ARE TO BOTTOM FACE OF WALL, SEE MODULAR BLOCK WALL DETAILS SHEET 57.

LEGEND

- RIGHT OF WAY
- - - TEMPORARY CONSTRUCTION EASEMENT
- [Hatched Box] MODULAR BLOCK WALL
- - - CLEARING & GRUBBING LIMITS PER SHEETS 9-16
- [Circle with X] STORM STRUCTURE PER SHEETS 17-29
- - - STORM PIPE PER SHEETS 17-29
- - - WATER MAIN PER SHEETS 133-145



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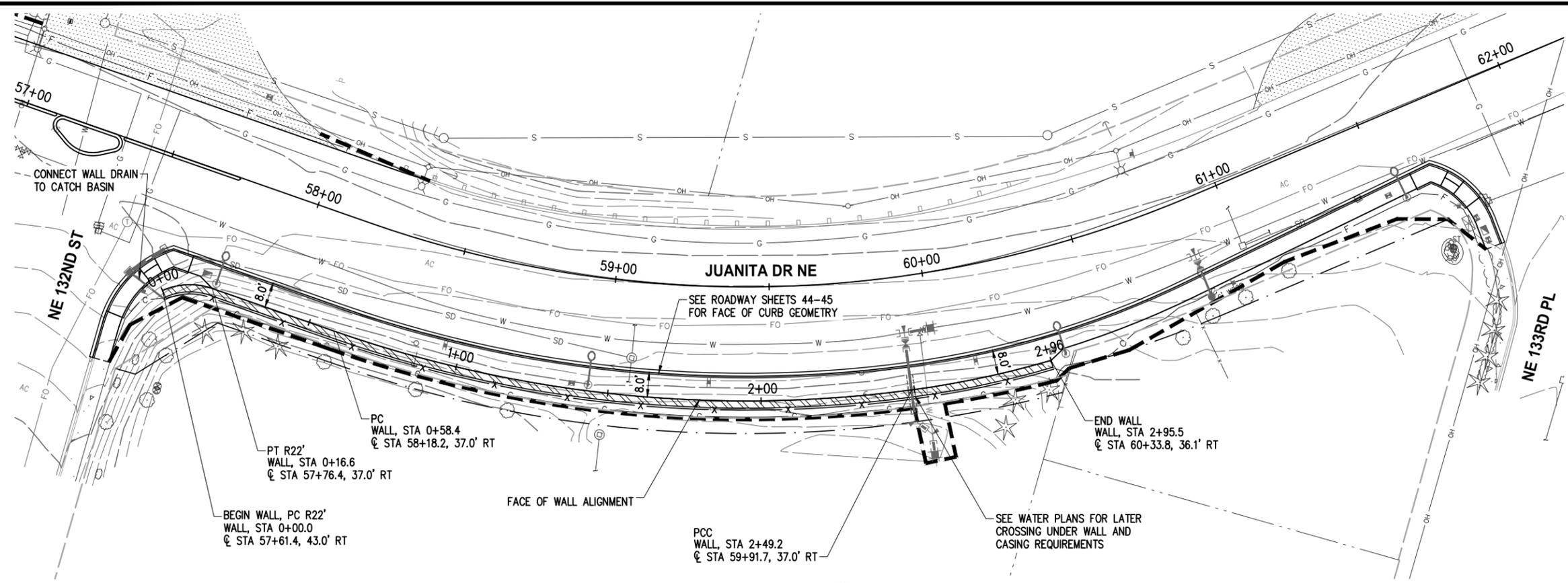
JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

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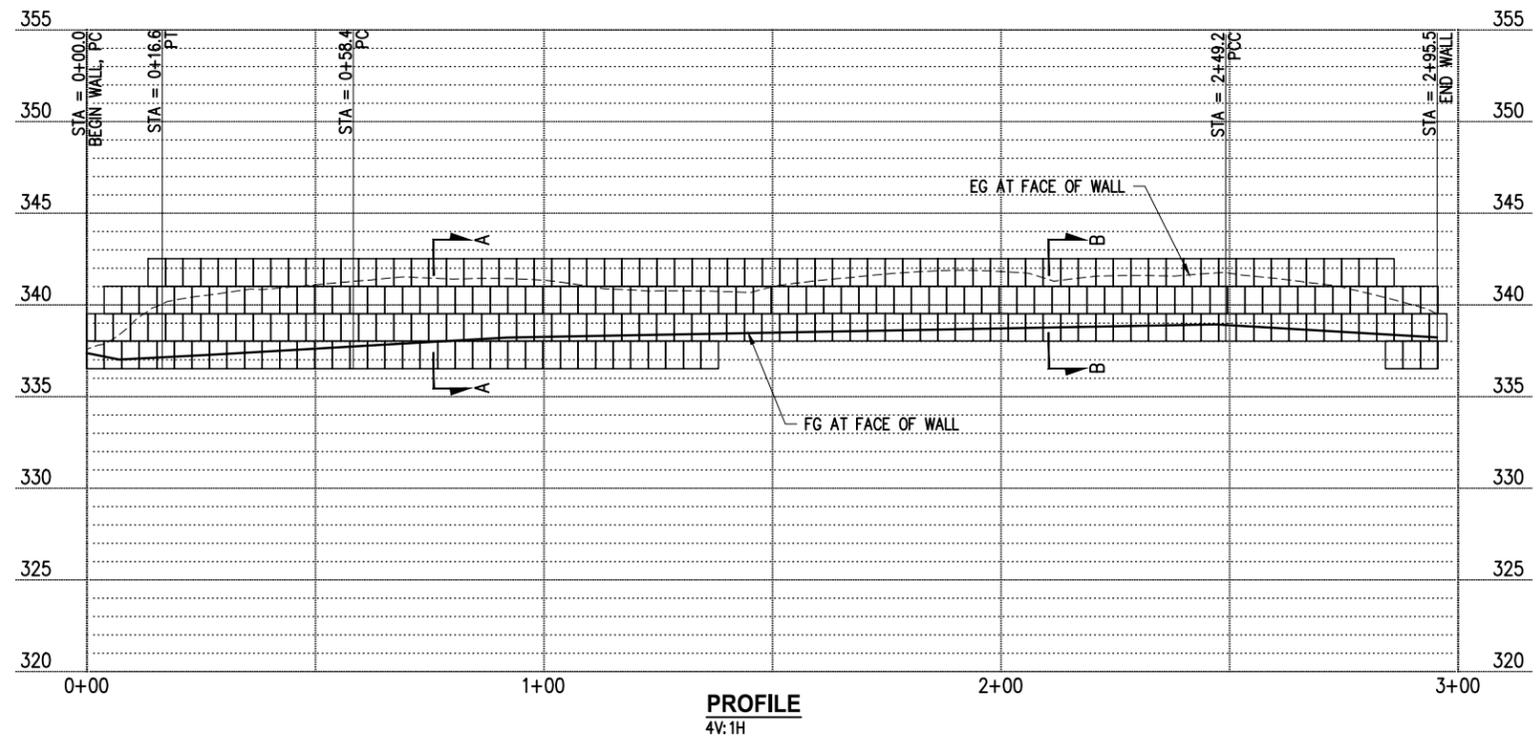
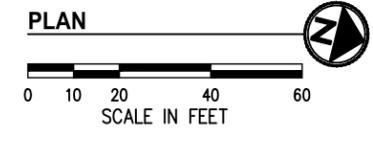
MODULAR BLOCK WALLS

REFERENCE SHEET NO.
SHEET 2)
SHEETS

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- GENERAL NOTES**
- ELEVATIONS HAVE BEEN PROVIDED FOR USE AS A GUIDE FOR CONSTRUCTION. THE RETAINING WALL SHALL BE CONSTRUCTED PER MANUFACTURER'S RECOMMENDATIONS EXCEPT AS ADJUSTED IN THE FIELD BY THE ENGINEER.
 - CONTRACTOR SHALL PROTECT EXISTING UTILITIES WHILE EXCAVATING FOR WALL. THIS MAY REQUIRE TEMPORARY SHORING DEPENDING ON THE CONSTRUCTION METHODS EMPLOYED BY THE CONTRACTOR.
 - EMBEDMENT SHALL BE A MINIMUM OF 6" BENEATH THE FACE OF THE WALL. INCREASE EMBEDMENT WHERE SLOPES ARE PRESENT IN FRONT OF WALL SO THAT THERE IS AT LEAST 5 FEET OF FLAT GROUND IN FRONT OF BASE OF WALL.
 - BLOCK WALL SHALL BE REDI-ROCK BLOCKS OR APPROVED EQUAL PROVIDED THEY MEET THE SPECIFICATIONS PROVIDED IN THE GRAVITY BLOCK WALL SPECIFICATIONS. BLOCKS SHALL BE A COLOR FROM MANUFACTURER'S STANDARD COLORS AND FINISH SHALL BE AS APPROVED BY THE OWNER.
 - FOR GRAVITY BLOCK WALL DETAILS SEE SHEET 59.



- LEGEND**
- RIGHT OF WAY
 - - - TEMPORARY CONSTRUCTION EASEMENT
 - ▨ STRUCTURAL GRAVITY BLOCK WALL
 - CLEARING & GRUBBING LIMITS PER SHEETS 9-16
 - ⊙ STORM STRUCTURE PER SHEETS 17-29
 - STORM PIPE PER SHEETS 17-29
 - W WATER MAIN PER SHEETS 133-145

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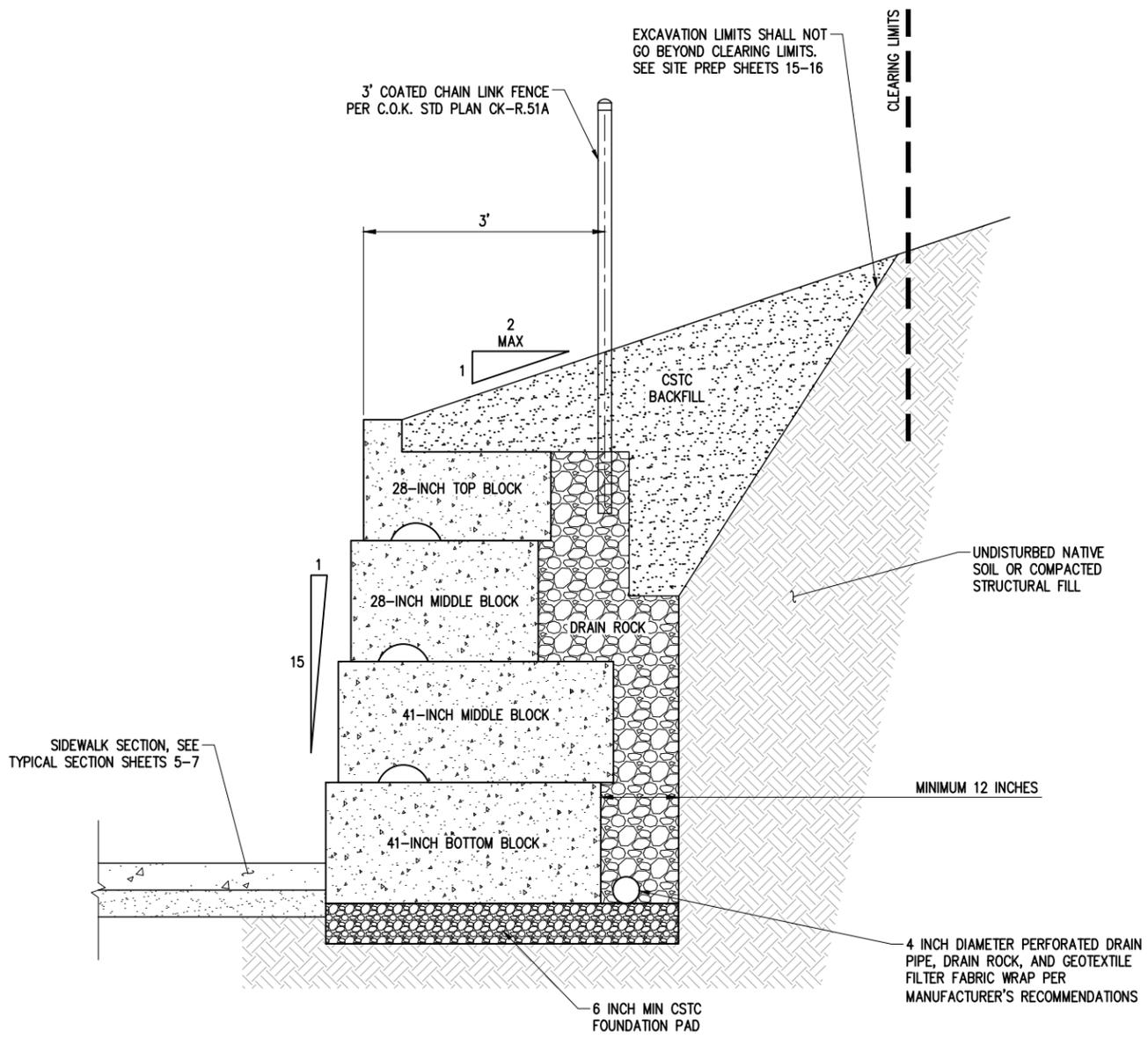
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 INTERSECTION & SAFETY IMPROVEMENTS**

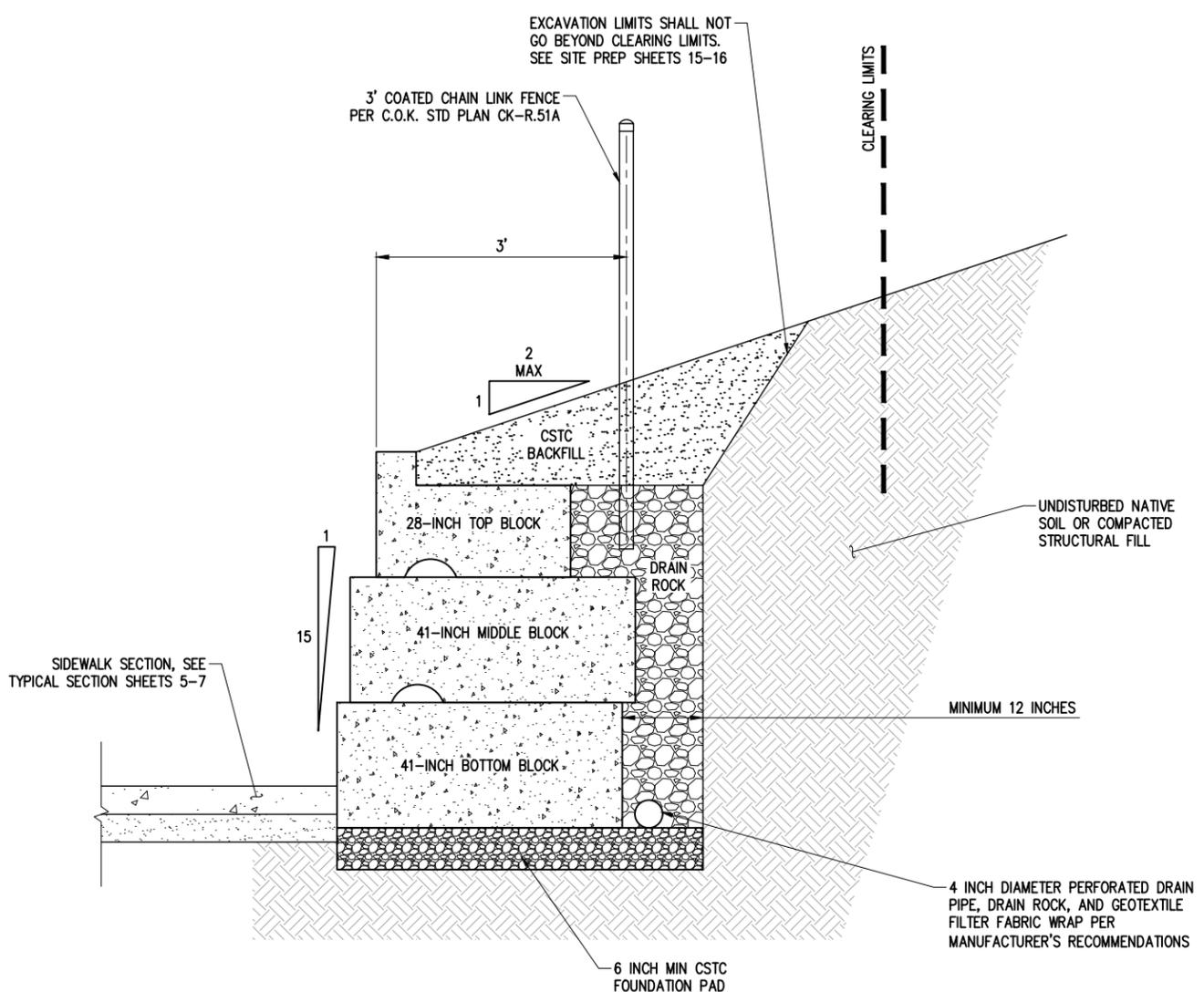
GRAVITY BLOCK WALL

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SHEET
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GRAVITY BLOCK WALL SECTION A-A
NTS



GRAVITY BLOCK WALL SECTION B-B
NTS

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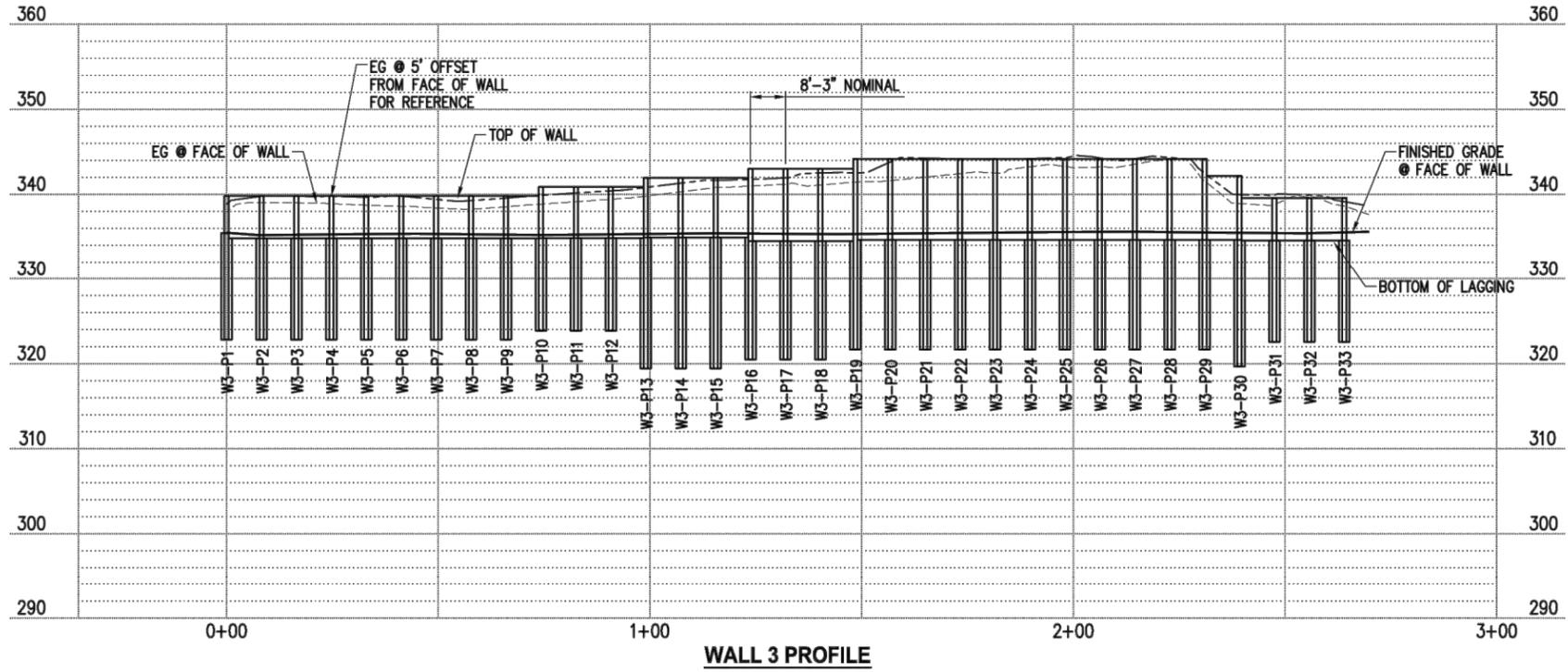
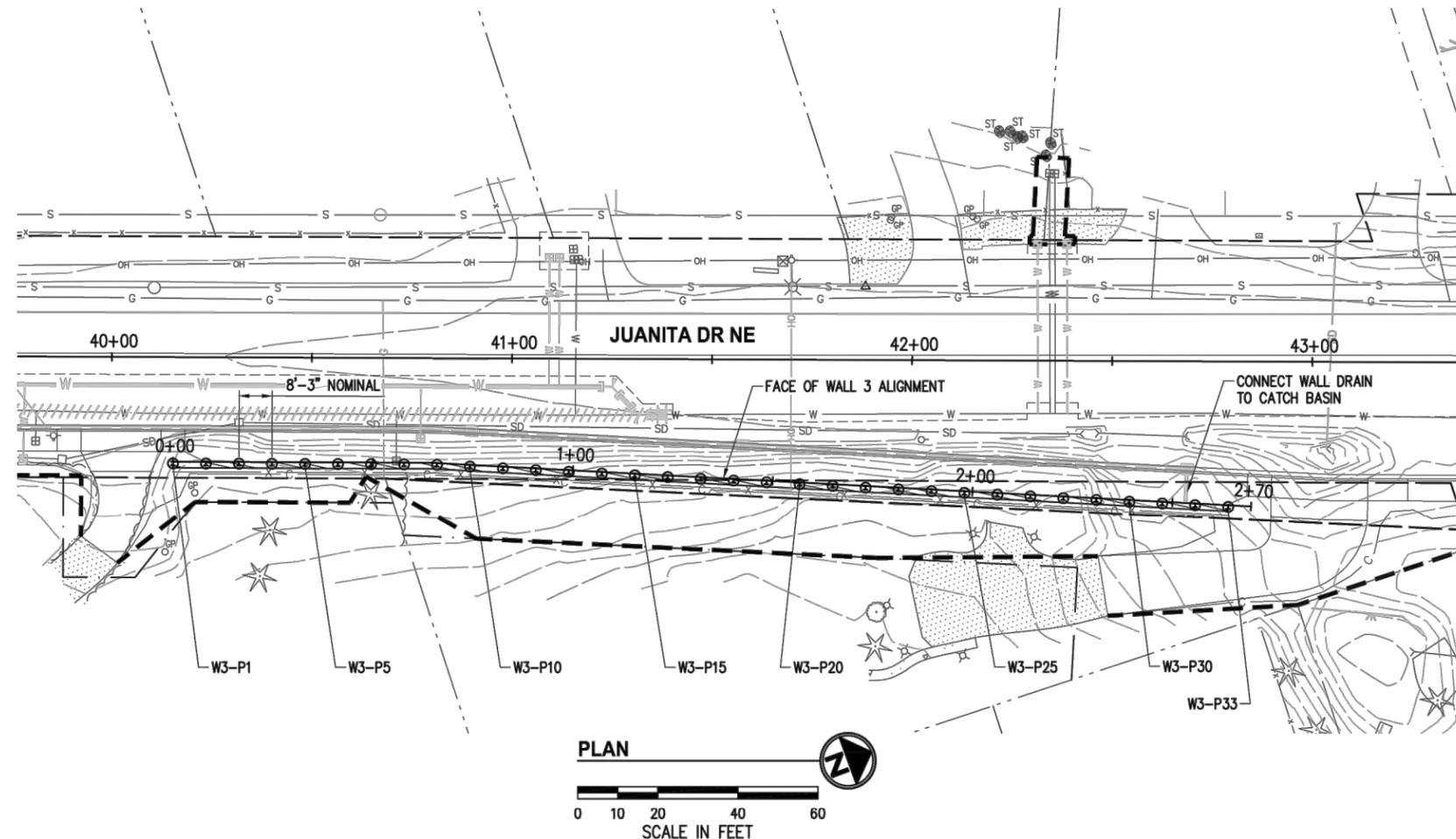
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**JUANITA DRIVE MULTIMODAL,
INTERSECTION & SAFETY IMPROVEMENTS**

6 (&7, 216
GRAVITY BLOCK WALL

REFERENCE SHEET NO.
SHEET
2)
SHEETS

October 11, 2023 - 7:40 PM - ERICK.OLSON - K:\PROJECTS\KIRKLAND\19065 - JUANITA DRIVE INTERSECTION & SAFETY IMPROVEMENTS\DRAWINGS\CONTRACT\19065WALL01_SOLDR_PILE.DWG - Layout Name: (3) WALL 3



GENERAL NOTES

1. FOR SOLDIER PILE WALL DETAILS SEE SHEET 62.
2. FOR PILE SCHEDULE SEE SHEET 63.
3. FOR INSTALLATION OF WATER SERVICES AND SEWER LATERALS UNDER WALL SEE WATER & SEWER PLANS SHEETS 135-147.

LEGEND

- RIGHT OF WAY
- TEMPORARY CONSTRUCTION EASEMENT
- SOLDIER PILE WALL
- CLEARING & GRUBBING LIMITS PER SHEETS 9-16
- STORM STRUCTURE PER SHEETS 17-29
- STORM PIPE PER SHEETS 17-29
- WATER MAIN PER SHEETS 135-147

DATE	NO.	REVISION	BY



UPI NO.:	FED. AID PROJ. NO.:
SURVEY NO.:	FIELD BOOK(S):
HORZ. DATUM: WSPCS, N ZONE, NAD 83/11	VERT. DATUM: NAVD 88
DESIGNED BY: MR	DRAWN BY: MR

BID DOCUMENT

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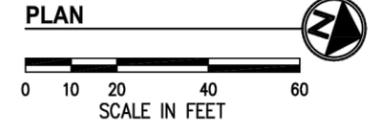
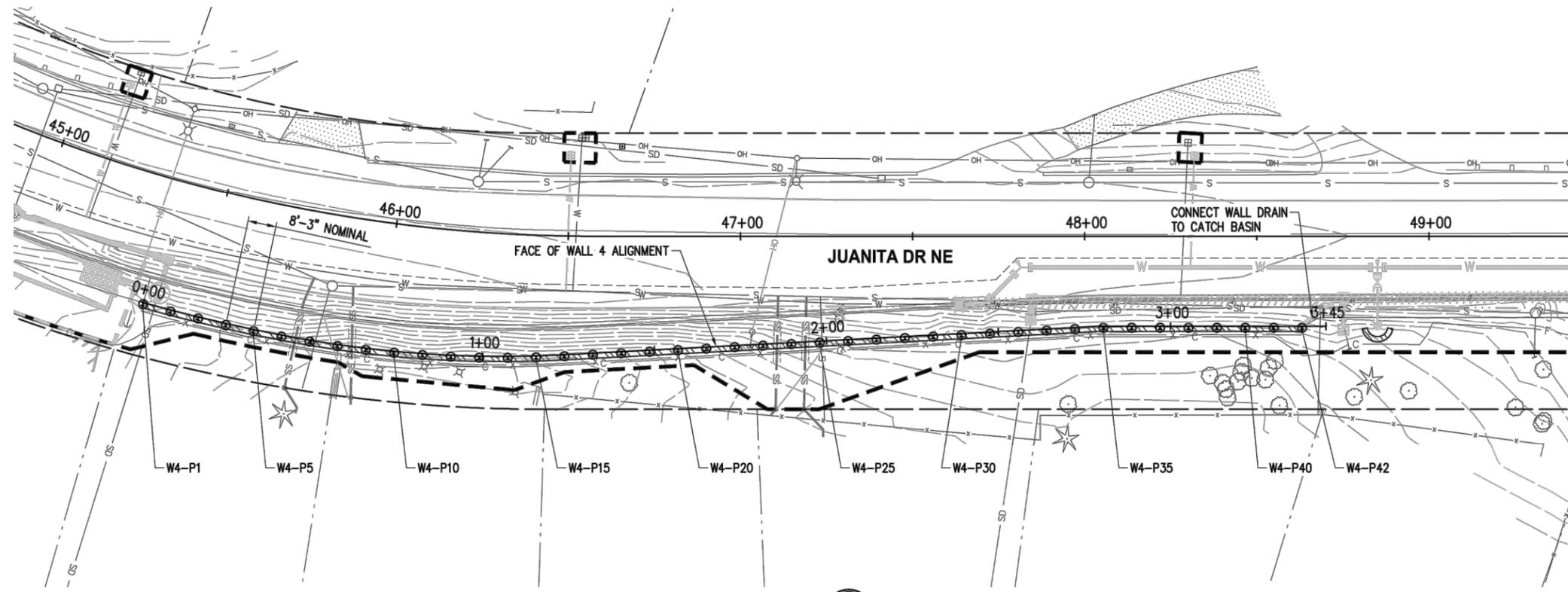
FUNDING NO. 19065

**JUANITA DRIVE MULTIMODAL,
INTERSECTION & SAFETY IMPROVEMENTS**

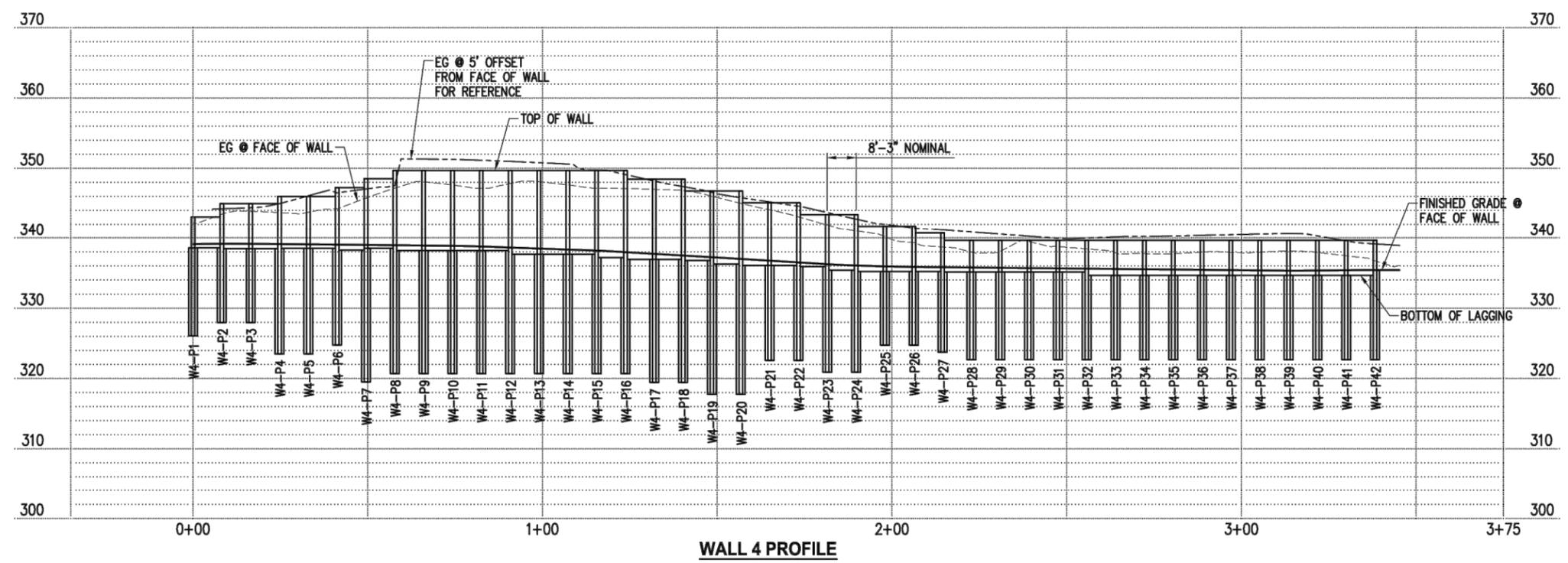
**WALL 3
SOLDIER PILE WALL**

REFERENCE SHEET NO.
SHEET 60 OF 161 SHEETS

October 11, 2023 - 7:40 PM - ERICK.OLSON - K:\PROJECTS\KIRKLAND\19065 - JUANITA DRIVE INTERSECTION & SAFETY IMPROVEMENTS\CONTRACT\19065WALL01_SOLDR_PILE.DWG - Layout Name: (4) WALL 4



- GENERAL NOTES**
- FOR SOLDIER PILE WALL DETAILS SEE SHEET 62.
 - FOR PILE SCHEDULE SEE SHEET 63.
 - FOR INSTALLATION OF WATER SERVICES AND SEWER LATERALS UNDER WALL SEE WATER & SEWER PLANS SHEETS 135-147.



- LEGEND**
- RIGHT OF WAY
 - TEMPORARY CONSTRUCTION EASEMENT
 - SOLDIER PILE WALL
 - CLEARING & GRUBBING LIMITS PER SHEETS 9-16
 - STORM STRUCTURE PER SHEETS 17-29
 - STORM PIPE PER SHEETS 17-29
 - WATER MAIN PER SHEETS 135-147

DATE	NO.	REVISION	BY



UPI NO.:	FED. AID PROJ. NO.:
SURVEY NO.:	FIELD BOOK(S):
HORZ. DATUM: WSPCS, N ZONE, NAD 83/11	VERT. DATUM: NAVD 88
DESIGNED BY: MR	DRAWN BY: MR

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FUNDING NO. 19065

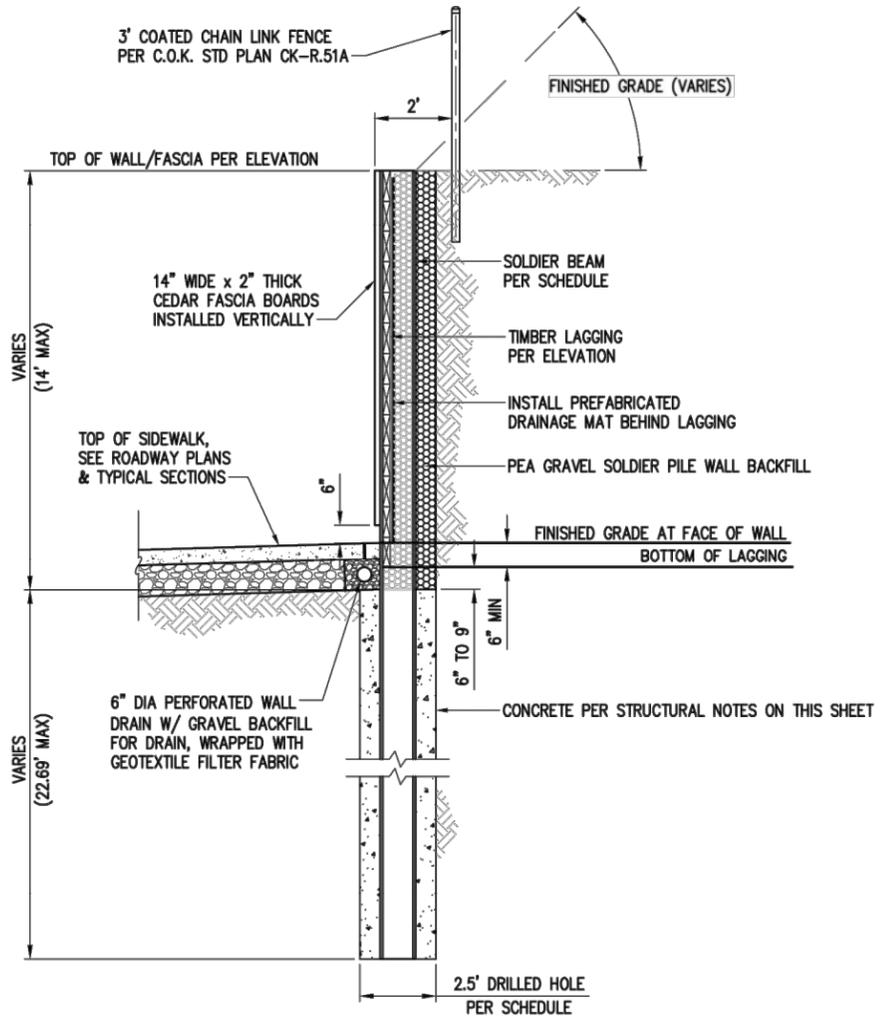
**JUANITA DRIVE MULTIMODAL,
INTERSECTION & SAFETY IMPROVEMENTS**

WALL 4
SOLDIER PILE WALL

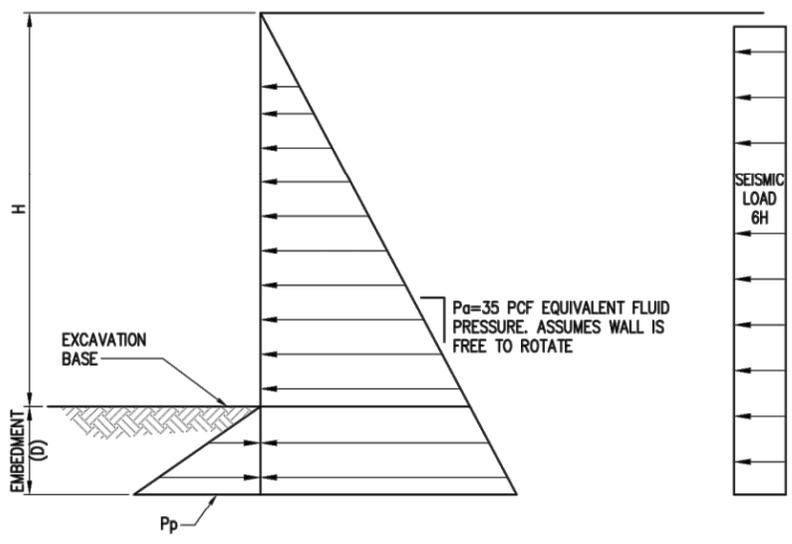
REFERENCE SHEET NO.

SHEET 61 OF 161 SHEETS

October 11, 2023 - 7:40 PM - ERICK.OLSON - K:\PROJECTS\KIRKLAND\19065 - JUANITA DRIVE INTERSECTION & SAFETY IMPROVEMENTS\CONTRACT\19065\WALL_DET_SOLDR_PILE.DWG - Layout Name: WALL_DET01_SP

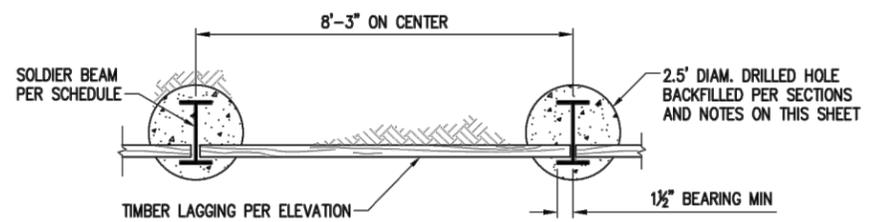


DRILLED CANTILEVER CROSS SECTION A
NTS

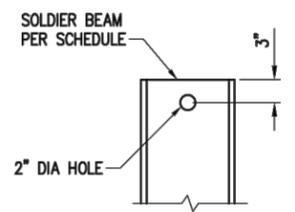


NOTES
 $P_p = 350 D$ PCF IN UNDISTURBED GLACIALLY CONSOLIDATED SOIL
 $H_1 =$ HEIGHT OF EXPOSED SOLDIER PILE IN FEET
 PASSIVE PRESSURE ACTS OVER 3X THE PILE WIDTH IN UNDISTURBED GLACIALLY CONSOLIDATED SOIL
 ACTIVE PRESSURE ACTS OVER 1X THE PILE WIDTH

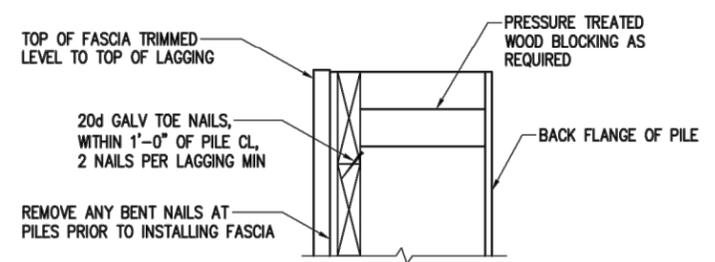
PRESSURE DIAGRAM DETAIL 1
NTS



LAGGING DETAIL 2
NTS



PILE LIFTING HOLE DETAIL 3
NTS



NOTES
 1. ALL NAILS SHALL BE GALVANIZED.
 2. FASCIA BOARD SHALL BE FULL AND ROUGH SAWN WESTERN CEDAR #2.

FASCIA DETAIL 4
NTS

GENERAL STRUCTURAL NOTES

DESIGN CRITERIA
 APPLICABLE BUILDING NOTES AND CODES
 INTERNATIONAL BUILDING CODE, IBC 2012 EDITION WITH STATE OF WASHINGTON AMENDMENTS,
 EXCEPT WHERE OTHER CODES ARE MORE RESTRICTIVE.

SOLDIER PILES
 WORK SHALL COMPLY WITH WSDOT STANDARD SPECIFICATIONS 6-16, UNLESS OTHERWISE NOTED IN THE DRAWINGS.

CONCRETE
 DESIGN STRENGTH
 CAST-IN-PLACE CONCRETE, UNLESS OTHERWISE NOTED
 $f'_c = 4000$ psi AT 28 DAYS

SOLDIER PILE BACKFILL (ABOVE CONCRETE)
 CAST-IN-PLACE LEAN MIX CONCRETE, UNLESS OTHERWISE NOTED
 $f'_c = 500$ psi AT 28 DAYS

STEEL
 MATERIAL
 ALL STRUCTURAL STEEL SHALL CONFORM TO THE FOLLOWING ASTM DESIGNATIONS UNLESS NOTES OTHERWISE ON THE DRAWINGS. SUBMIT SHOP DRAWINGS:

WIDE FLANGE SHAPES ASTM A992 OR ASTM A572, GRADE 50

SPECIAL INSPECTION
 THE FOLLOWING ITEMS REQUIRE SPECIAL INSPECTION PER IBC SECTION 1704:

- SOLDIER PILES, SPECIAL GRADING, EXCAVATION
- *FOUNDATION EXCAVATIONS
- *BACKFILL BEHIND STRUCTURAL WALLS
- *SHAFT EXCAVATION

MISCELLANEOUS
 SUBMIT ALL REQUIRED SHOP DRAWINGS AND RECEIVE THEIR SATISFACTORY REVIEW FROM THE ENGINEER, PRIOR TO FABRICATION.

COORDINATE AND VERIFY ALL DIMENSIONS WITH GENERAL AND CIVIL DRAWINGS AND CONDITIONS AT THE PROJECT SITE PRIOR TO STARTING WORK AND NOTIFY THE ENGINEER IMMEDIATELY OF ANY DISCREPANCIES.

PROVIDE TEMPORARY ERECTION BRACING AND SHORING AS REQUIRED FOR STABILITY OF THE STRUCTURE AND ADJACENT STRUCTURES DURING ALL PHASES OF CONSTRUCTION. REFER TO WSDOT STANDARD SPECIFICATIONS FOR INFORMATION NOT CONTAINED IN THESE GENERAL NOTES.

DATE	NO.	REVISION	BY



UPI NO.:	FED. AID PROJ. NO.:
SURVEY NO.:	FIELD BOOK(S):
HORZ. DATUM: WSPCS, N ZONE, NAD 83/11	VERT. DATUM: NAVD 88
DESIGNED BY: MR	DRAWN BY: MR



BID DOCUMENT

CITY OF KIRKLAND WASHINGTON

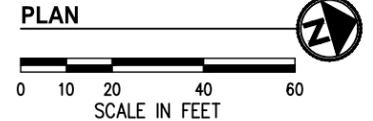
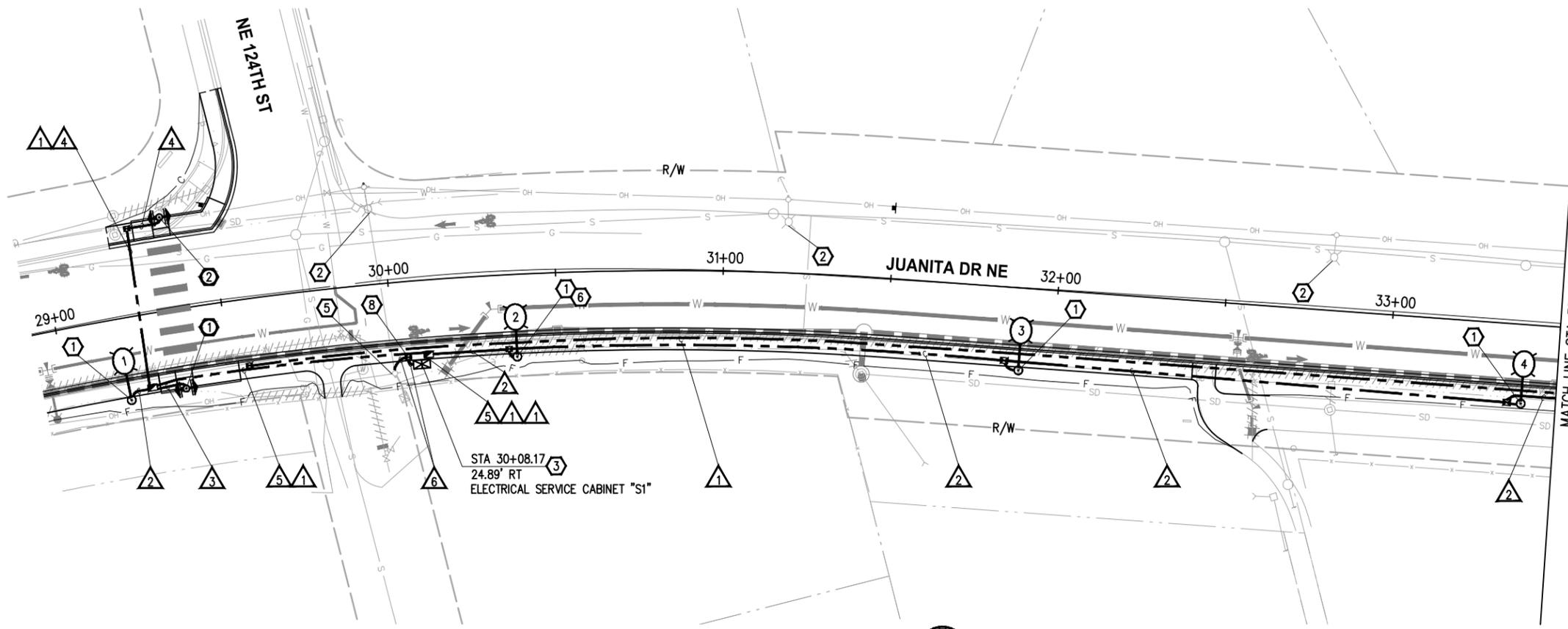
CITY OF KIRKLAND
 DEPARTMENT OF PUBLIC WORKS
 123 FIFTH AVENUE KIRKLAND, WA 98033
 (425) 587-3800 www.kirklandwa.gov

FUNDING NO. 19065

JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

DETAILS
SOLDIER PILE WALL

REFERENCE SHEET NO.
SHEET 62 OF 161 SHEETS



- ### GENERAL NOTES
1. ALL WORK SHALL BE COMPLETED IN ACCORDANCE WITH THE WSDOT/APWA STANDARD PLANS, STANDARD SPECIFICATIONS, CITY OF KIRKLAND STANDARDS, SPECIAL PROVISIONS, AND THESE PLANS.
 2. THE LOCATION OF ALL FEATURES TO BE INSTALLED BY THE CONTRACTOR ARE SHOWN FOR GRAPHICAL REPRESENTATION ONLY. FINAL LOCATIONS SHALL BE IDENTIFIED BY THE CONTRACTOR AND VERIFIED BY THE ENGINEER IN THE FIELD PRIOR TO INSTALLATION.
 3. NUMBER OF CONDUIT BENDS BETWEEN PULL POINTS SHALL NOT BE MORE THAN THE EQUIVALENT OF FOUR QUARTER BENDS (360 DEGREES TOTAL). IF NUMBER OF BENDS EXCEEDS 360 DEGREES, CONTRACTOR SHALL INSTALL ADDITIONAL JUNCTION BOXES, AS REQUIRED.
 4. UTILITY LOCATION PRIOR TO CONSTRUCTION SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. CONFLICTS SHALL BE BROUGHT TO THE ATTENTION OF THE ENGINEER FOR RESOLUTION.
 5. ALL NEW JUNCTION BOXES PLACED IN THE SIDEWALK SHALL HAVE SKID RESISTANT LIDS AND FRAMES. JUNCTION BOXES AND VAULTS SHALL NOT BE PLACED IN ADA CURB RAMP OR ADA CURB RAMP LANDINGS.
 6. CONTRACTOR SHALL CONTACT CITY OF KIRKLAND ENGINEER TO COORDINATE ALL WORK ON CITY-OWNED SYSTEMS.
 7. THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING A CLEARANCE ZONE OF 15' AROUND OVERHEAD PRIMARY POWER LINES DURING CONSTRUCTION. COORDINATE WORK WITH PUGET SOUND ENERGY.
 8. CONTRACTOR SHALL COORDINATE WITH THE PUGET SOUND ENERGY REPRESENTATIVE REGARDING THE ELECTRICAL SERVICE CONNECTION AND EXISTING PSE POLES.
 9. FOR TREATMENT OF EXISTING UTILITIES, SEE SITE PREPARATION & TESC PLAN, SHEETS 9-16.
 10. FOR ILLUMINATION ONE LINE DIAGRAMS, SEE SHEET 73.
 11. EXISTING OR HIGHER ILLUMINATION LEVELS SHALL BE MAINTAINED BY USING EXISTING OR TEMPORARY ILLUMINATION UNTIL THE NEW SYSTEM IS OPERATIONAL.

WIRE NOTES

RACEWAY/ CONDUIT SIZE	CONDUCTORS	COMMENTS
1 2"		SPARE
2 2"	2-#8(ILL)	
3 2"	4-#10(RRFB), 1-2CS	
4 2"	2-#10(RRFB), 1-2CS	
5 2"	2-#8(ILL), 2-#10(RRFB)	
6 3"	3-#3/0	FEEDER, NO GROUND

- #### NOTES:
1. ALL PVC CONDUIT CONTAINING CONDUCTORS SHALL CONTAIN GROUND WIRE (NOT SHOWN). WIRE SIZE SHALL MATCH THE LARGEST CONDUCTOR (MIN #8 AWG OR AS OTHERWISE NOTED IN THE WIRE NOTES). CONDUITS THAT DO NOT CONTAIN ELECTRICAL CONDUCTORS SHALL INCLUDE A DETECTABLE PULL TAPE AND SHALL BE LABELED "CITY OF KIRKLAND"
 2. ALL CONDUIT SHALL BE RIGID PVC SCH 80.

RRFB POLE NOTES

1. CONSTRUCT RRFB POLE FOUNDATION PER RRFB SCHEDULE, THIS SHEET. FURNISH AND INSTALL RRFB POLE BASE, POLE, SIGNS, RRFB LIGHT BARS, CONTROLLER, AND APS STYLE PEDESTRIAN PUSH BUTTON PER DETAIL, SHEET 72. CONFIRM FOUNDATION LOCATION WITH THE ENGINEER PRIOR TO EXCAVATION. APS PUSH BUTTON CONTROL FACE SHALL BE INSTALLED PARALLEL TO THE CROSSWALK SERVED. THE ORIENTATION OF PUSH BUTTON SHALL BE VERIFIED IN THE FIELD BY THE ENGINEER PRIOR TO INSTALLATION. SEE SPECIAL PROVISIONS.
2. CONSTRUCT RRFB POLE FOUNDATION PER RRFB SCHEDULE, THIS SHEET. FURNISH AND INSTALL RRFB POLE BASE, POLE, SIGNS, RRFB LIGHT BARS, AND APS STYLE PEDESTRIAN PUSH BUTTON PER DETAIL, SHEET 72. CONFIRM FOUNDATION LOCATION WITH THE ENGINEER PRIOR TO EXCAVATION. APS PUSH BUTTON CONTROL FACE SHALL BE INSTALLED PARALLEL TO THE CROSSWALK SERVED. THE ORIENTATION OF PUSH BUTTON SHALL BE VERIFIED IN THE FIELD BY THE ENGINEER PRIOR TO INSTALLATION. SEE SPECIAL PROVISIONS.

RRFB SCHEDULE

POLE #	STATION	OFFSET	POLE TYPE	SIGNS	FOUNDATION
1	29+36	23.8' RT	FB	(2) W11-2, (1) W16-PL, (1) W16-PR	WSDOT STD PLAN J-20.11
2	29+36	27.8' LT	FB	(2) W11-2, (1) W16-PL, (1) W16-PR	WSDOT STD PLAN J-20.11

LEGEND

EXISTING	NEW	DESCRIPTION
		ROADWAY LUMINAIRE
		JUNCTION BOX TYPE 1, 2, 8
		ELECTRICAL SERVICE CABINET
		CONDUIT
		APS PEDESTRIAN PUSH BUTTON
		RECTANGULAR RAPID FLASHING BEACON (RRFB) ASSEMBLY AND POLE
		WIRE NOTE
		CONSTRUCTION NOTE
		POLE NOTE

CONSTRUCTION NOTES

1. CONSTRUCT LUMINAIRE POLE FOUNDATION PER LUMINAIRE SCHEDULE, THIS SHEET. FURNISH AND INSTALL ROADWAY LUMINAIRE POLE, LUMINAIRE ARM, LUMINAIRE, AND ALL ASSOCIATED EQUIPMENT, CONDUIT, AND WIRING PER WSDOT STD PLANS J-28.10, J-28.24, J-28.30, J-28.40, J-28.50, AND J-28.70, EXCEPT CONDUIT BETWEEN JUNCTION BOX AND LUMINAIRE FOUNDATION SHALL BE 2" SCH 80 AND WIRING SHALL BE #8. SEE SPECIAL PROVISIONS.
2. EXISTING POLE MOUNTED LUMINAIRE TO BE REMOVED BY OTHERS.
3. CONSTRUCT ELECTRICAL SERVICE CABINET FOUNDATION PER COK DETAIL CK-TS.11. CONTRACTOR SHALL FURNISH AND INSTALL TINY ELECTRICAL SERVICE CABINET PER COK DETAIL CK-TS.10. VERIFY FINAL LOCATION WITH THE ENGINEER IN THE FIELD. COORDINATE ELECTRICAL SERVICE METER BASE INSTALLATION WITH PSE REPRESENTATIVE. SEE SPECIAL PROVISIONS.
5. LOCATION OF EXISTING TRANSFORMER. CONTRACTOR SHALL FURNISH AND INSTALL CONDUIT RISER ON POLE PER PSE HANDBOOK. INSTALL WIRING TO TOP OF RISER AND COIL 30'. PROVIDE 20' OF SPARE 3" CONDUIT FOR POWER COMPANY USE. COORDINATE FEEDER WIRE TERMINATION WITH PSE REPRESENTATIVE. SEE SPECIAL PROVISIONS.
6. EXISTING UTILITY TO BE ABANDONED. CUT AND CAP PIPE AFTER SERVICE IS DISCONNECTED TO ALLOW FOR LUMINAIRE FOUNDATION INSTALLATION.
8. CONTRACTOR TO INSTALL HAND HOLE WITH FARGO DISCONNECT AS CLOSE TO CABINET AS POSSIBLE. COORDINATE WITH CITY OF KIRKLAND ENGINEER.

LUMINAIRE SCHEDULE

LUMINAIRE #	STATION	OFFSET	LUMINAIRE TYPE	LUMINAIRE ARM	MOUNTING HEIGHT	BASE	CIRCUIT #	FOUNDATION	COMMENTS
1	29+19	24.5' RT	125W, 3K, 60LED, 970mA, IV, B3-U0-G3, 240V	8'	35'	FIXED	1	TYPE B FOUNDATION PER WSDOT J-28.30	
2	30+37	24.5' RT	110W, 3K, 60LED, 900mA, III, B3-U0-G3, 240V	8'	35'	FIXED	1	TYPE B FOUNDATION PER WSDOT J-28.30	
3	31+90	24.5' RT	110W, 3K, 60LED, 900mA, III, B3-U0-G3, 240V	8'	35'	FIXED	1	TYPE B FOUNDATION PER WSDOT J-28.30	
4	33+40	23.6' RT	110W, 3K, 60LED, 900mA, III, B3-U0-G3, 240V	8'	35'	FIXED	1	TYPE B FOUNDATION PER WSDOT J-28.30	

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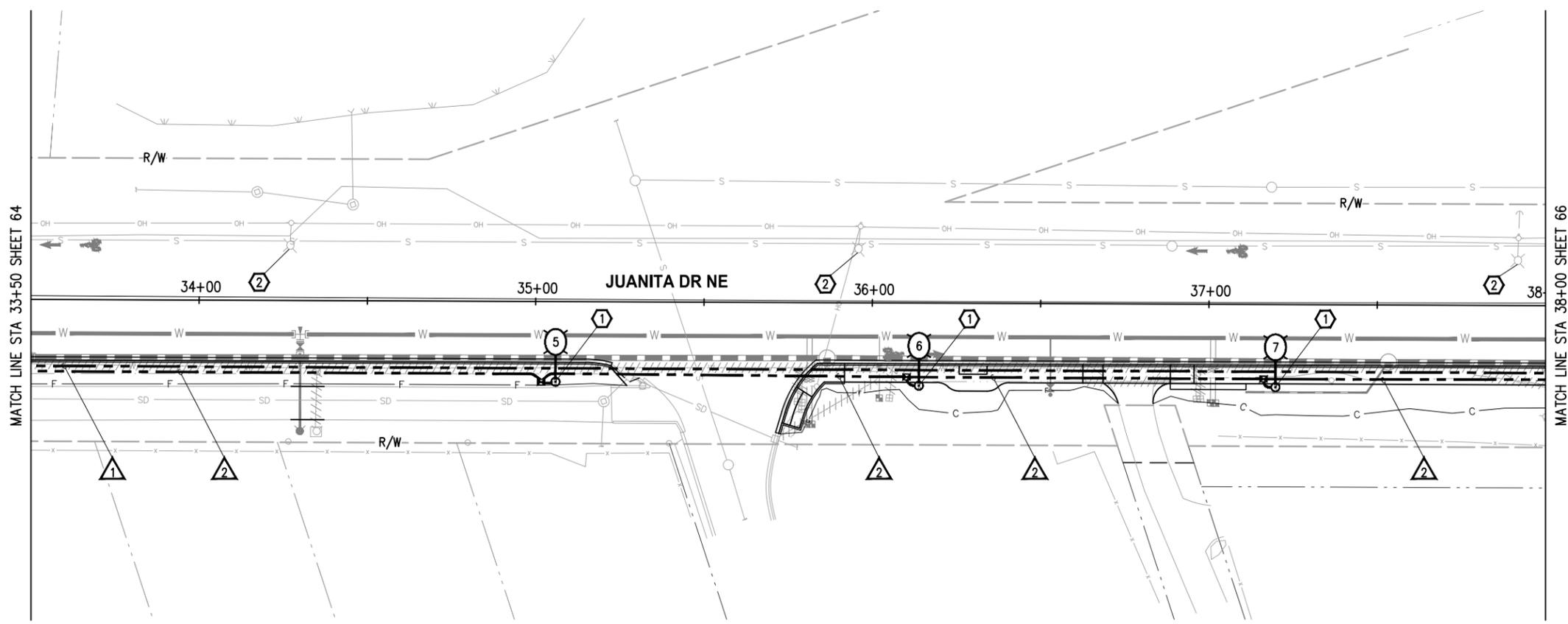


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CITY OF KIRKLAND WASHINGTON
 CITY OF KIRKLAND
 DEPARTMENT OF PUBLIC WORKS
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 (425) 587-3800 www.kirklandwa.gov
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JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS
 67\$ 72 67\$
ILLUMINATION PLAN
 REFERENCE SHEET NO.
 SHEET 2) SHEETS

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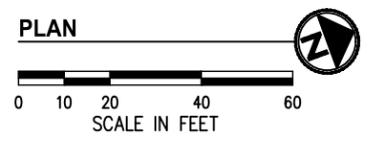
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- ② EXISTING POLE MOUNTED LUMINAIRE TO BE REMOVED BY OTHERS.

WIRE NOTES

▲	RACEWAY/ CONDUIT SIZE	CONDUCTORS	COMMENTS
1	2"		SPARE
2	2"	2-#8(ILL)	
3	2"	4-#10(RRFB), 1-2CS	
4	2"	2-#10(RRFB), 1-2CS	
5	2"	2-#8(ILL), 2-#10(RRFB)	
6	3"	3-#3/0	FEEDER, NO GROUND

- NOTES:**
1. ALL PVC CONDUIT CONTAINING CONDUCTORS SHALL CONTAIN GROUND WIRE (NOT SHOWN). WIRE SIZE SHALL MATCH THE LARGEST CONDUCTOR (MIN #8 AWG OR AS OTHERWISE NOTED IN THE WIRE NOTES). CONDUITS THAT DO NOT CONTAIN ELECTRICAL CONDUCTORS SHALL INCLUDE A DETECTABLE PULL TAPE AND SHALL BE LABELED "CITY OF KIRKLAND"
 2. ALL CONDUIT SHALL BE RIGID PVC SCH 80.



LEGEND

EXISTING	NEW	DESCRIPTION
		ROADWAY LUMINAIRE
		JUNCTION BOX TYPE 1, 2, 8
		ELECTRICAL SERVICE CABINET
		CONDUIT
		APS PEDESTRIAN PUSH BUTTON
		RECTANGULAR RAPID FLASHING BEACON (RRFB) ASSEMBLY AND POLE
		WIRE NOTE
		CONSTRUCTION NOTE
		POLE NOTE

LUMINAIRE SCHEDULE

LUMINAIRE #	STATION	OFFSET	LUMINAIRE TYPE	LUMINAIRE ARM	MOUNTING HEIGHT	BASE	CIRCUIT #	FOUNDATION	COMMENTS
5	35+06	23.8' RT	110W, 3K, 60LED, 900mA, III, B3-U0-G3, 240V	8'	35'	FIXED	1	TYPE B FOUNDATION PER WSDOT J-28.30	
6	36+14	24.5' RT	110W, 3K, 60LED, 900mA, III, B3-U0-G3, 240V	8'	35'	FIXED	1	TYPE B FOUNDATION PER WSDOT J-28.30	
7	37+20	24.5' RT	110W, 3K, 60LED, 900mA, III, B3-U0-G3, 240V	8'	35'	FIXED	1	TYPE B FOUNDATION PER WSDOT J-28.30	

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CITY OF KIRKLAND WASHINGTON

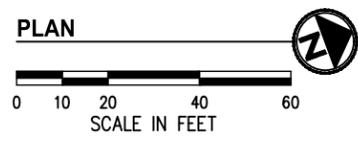
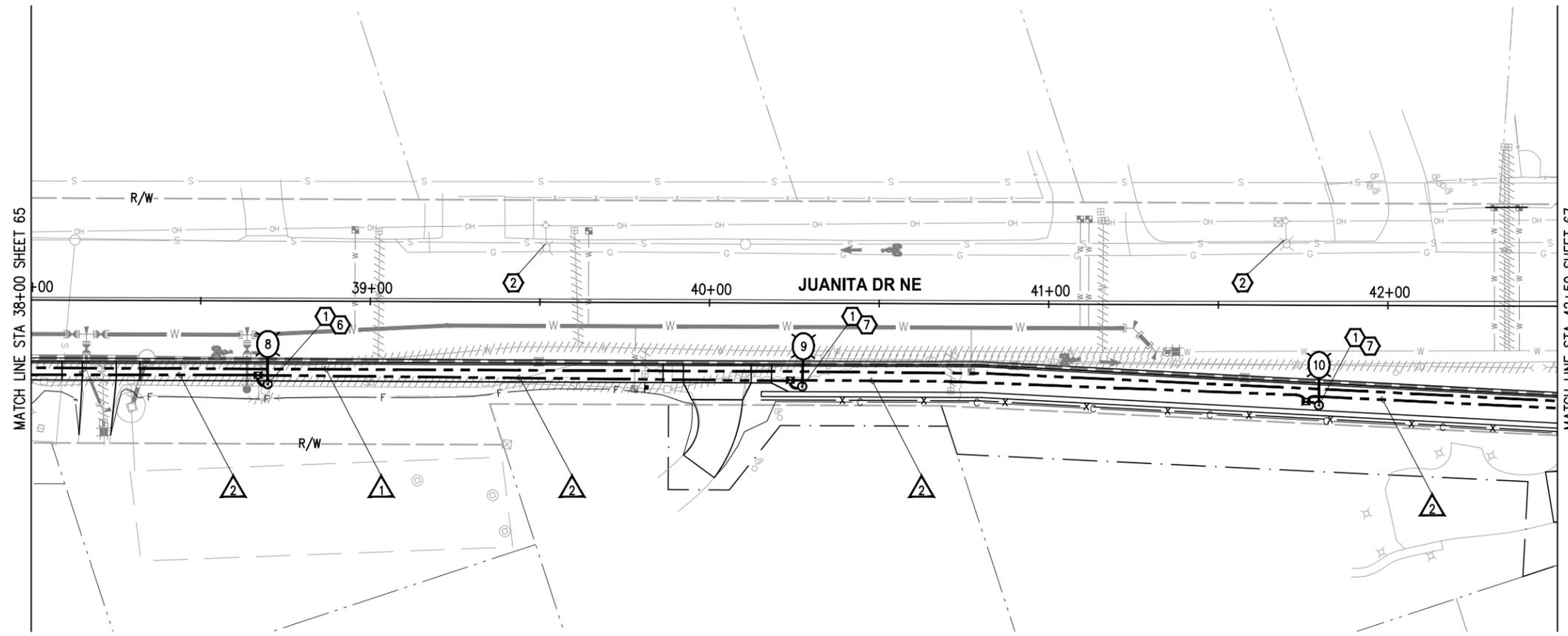
CITY OF KIRKLAND
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123 FIFTH AVENUE KIRKLAND, WA 98033
(425) 587-3800 www.kirklandwa.gov

JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

ILLUMINATION PLAN

67\$ 72 67\$

REFERENCE SHEET NO. SHEET 2) SHEETS



WIRE NOTES

#	RACEWAY/ CONDUIT SIZE	CONDUCTORS	COMMENTS
1	2"		SPARE
2	2"	2-#8(ILL)	
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 - ALL CONDUIT SHALL BE RIGID PVC SCH 80.

LUMINAIRE SCHEDULE

LUMINAIRE #	STATION	OFFSET	LUMINAIRE TYPE	LUMINAIRE ARM	MOUNTING HEIGHT	BASE	CIRCUIT #	FOUNDATION	COMMENTS
8	38+70	24.5' RT	110W, 3K, 60LED, 900mA, III, B3-U0-G3, 240V	8'	35'	FIXED	1	TYPE B FOUNDATION PER WSDOT J-28.30	
9	40+27	24.5' RT	110W, 3K, 60LED, 900mA, III, B3-U0-G3, 240V	8'	35'	FIXED	1	TYPE B FOUNDATION PER WSDOT J-28.30	HAND HOLE TO FACE ROADWAY
10	41+80	29.4' RT	110W, 3K, 60LED, 900mA, III, B3-U0-G3, 240V	8'	35'	FIXED	1	TYPE B FOUNDATION PER WSDOT J-28.30	HAND HOLE TO FACE ROADWAY

LEGEND

EXISTING	NEW	DESCRIPTION
		ROADWAY LUMINAIRE
		JUNCTION BOX TYPE 1, 2, 8
		ELECTRICAL SERVICE CABINET
		CONDUIT
		APS PEDESTRIAN PUSH BUTTON
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		WIRE NOTE
		CONSTRUCTION NOTE
		POLE NOTE

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- EXISTING POLE MOUNTED LUMINAIRE TO BE REMOVED BY OTHERS.
- EXISTING UTILITY TO BE ABANDONED. CUT AND CAP PIPE AFTER SERVICE IS DISCONNECTED TO ALLOW FOR LUMINAIRE FOUNDATION INSTALLATION.
- INSTALL LUMINAIRE FOUNDATION CENTERED BETWEEN SOLDIER PILE FOUNDATIONS.



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BID DOCUMENT

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CITY OF KIRKLAND WASHINGTON

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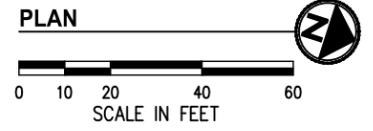
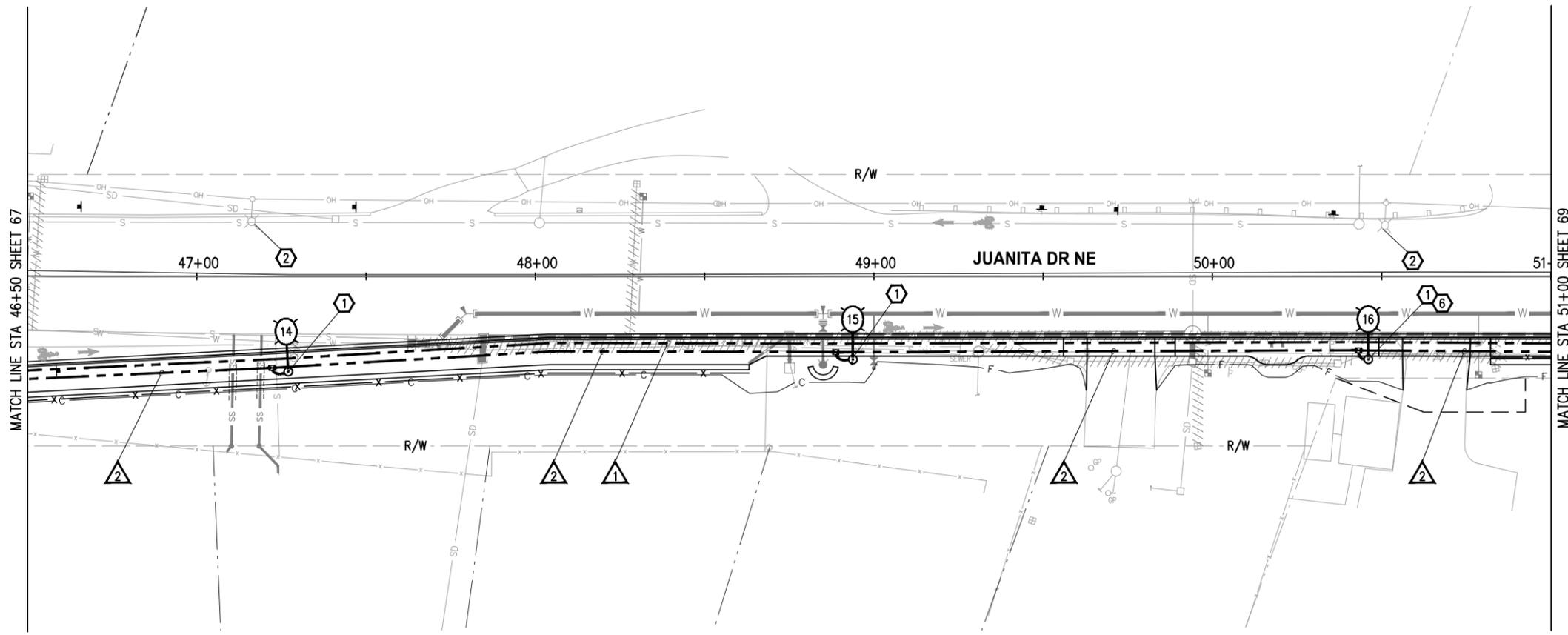
JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

67\$ 72 67\$

ILLUMINATION PLAN

REFERENCE SHEET NO.
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WIRE NOTES

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1	2"		SPARE
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6	3"	3-#3/0	FEEDER, NO GROUND

- NOTES:**
- ALL PVC CONDUIT CONTAINING CONDUCTORS SHALL CONTAIN GROUND WIRE (NOT SHOWN). WIRE SIZE SHALL MATCH THE LARGEST CONDUCTOR (MIN #8 AWG OR AS OTHERWISE NOTED IN THE WIRE NOTES). CONDUITS THAT DO NOT CONTAIN ELECTRICAL CONDUCTORS SHALL INCLUDE A DETECTABLE PULL TAPE AND SHALL BE LABELED "CITY OF KIRKLAND"
 - ALL CONDUIT SHALL BE RIGID PVC SCH 80.

GENERAL NOTES

- ALL WORK SHALL BE COMPLETED IN ACCORDANCE WITH THE WSDOT/APWA STANDARD PLANS, STANDARD SPECIFICATIONS, CITY OF KIRKLAND STANDARDS, SPECIAL PROVISIONS, AND THESE PLANS.
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- NUMBER OF CONDUIT BENDS BETWEEN PULL POINTS SHALL NOT BE MORE THAN THE EQUIVALENT OF FOUR QUARTER BENDS (360 DEGREES TOTAL). IF NUMBER OF BENDS EXCEEDS 360 DEGREES, CONTRACTOR SHALL INSTALL ADDITIONAL JUNCTION BOXES, AS REQUIRED.
- UTILITY LOCATION PRIOR TO CONSTRUCTION SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. CONFLICTS SHALL BE BROUGHT TO THE ATTENTION OF THE ENGINEER FOR RESOLUTION.
- ALL NEW JUNCTION BOXES PLACED IN THE SIDEWALK SHALL HAVE SKID RESISTANT LIDS AND FRAMES. JUNCTION BOXES AND VAULTS SHALL NOT BE PLACED IN ADA CURB RAMP OR ADA CURB RAMP LANDINGS.
- CONTRACTOR SHALL CONTACT CITY OF KIRKLAND ENGINEER TO COORDINATE ALL WORK ON CITY-OWNED SYSTEMS.
- THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING A CLEARANCE ZONE OF 15' AROUND OVERHEAD PRIMARY POWER LINES DURING CONSTRUCTION. COORDINATE WORK WITH PUGET SOUND ENERGY.
- CONTRACTOR SHALL COORDINATE WITH THE PUGET SOUND ENERGY REPRESENTATIVE REGARDING THE ELECTRICAL SERVICE CONNECTION AND EXISTING PSE POLES.
- FOR TREATMENT OF EXISTING UTILITIES, SEE SITE PREPARATION & TESC PLAN, SHEETS 9-16.
- FOR ILLUMINATION ONE LINE DIAGRAMS, SEE SHEET 73.
- EXISTING OR HIGHER ILLUMINATION LEVELS SHALL BE MAINTAINED BY USING EXISTING OR TEMPORARY ILLUMINATION UNTIL THE NEW SYSTEM IS OPERATIONAL.

CONSTRUCTION NOTES

- 1** CONSTRUCT LUMINAIRE POLE FOUNDATION PER LUMINAIRE SCHEDULE, THIS SHEET. FURNISH AND INSTALL ROADWAY LUMINAIRE POLE, LUMINAIRE ARM, LUMINAIRE, AND ALL ASSOCIATED EQUIPMENT, CONDUIT, AND WIRING PER WSDOT STD PLANS J-28.10, J-28.24, J-28.30, J-28.40, J-28.50, AND J-28.70, EXCEPT CONDUIT BETWEEN JUNCTION BOX AND LUMINAIRE FOUNDATION SHALL BE 2" SCH 80 AND WIRING SHALL BE #8. SEE SPECIAL PROVISIONS.
- 2** EXISTING POLE MOUNTED LUMINAIRE TO BE REMOVED BY OTHERS.
- 6** EXISTING UTILITY TO BE ABANDONED. CUT AND CAP PIPE AFTER SERVICE IS DISCONNECTED TO ALLOW FOR LUMINAIRE FOUNDATION INSTALLATION.

LEGEND

EXISTING	NEW	DESCRIPTION
		ROADWAY LUMINAIRE
		JUNCTION BOX TYPE 1, 2, 8
		ELECTRICAL SERVICE CABINET
		CONDUIT
		APS PEDESTRIAN PUSH BUTTON
		RECTANGULAR RAPID FLASHING BEACON (RRFB) ASSEMBLY AND POLE
		WIRE NOTE
		CONSTRUCTION NOTE
		POLE NOTE

LUMINAIRE SCHEDULE

LUMINAIRE #	STATION	OFFSET	LUMINAIRE TYPE	LUMINAIRE ARM	MOUNTING HEIGHT	BASE	CIRCUIT #	FOUNDATION	COMMENTS
14	47+27	28.1' RT	125W, 3K, 60LED, 970mA, IV, B3-U0-G3, 240V	8'	35'	FIXED	1	TYPE B FOUNDATION PER WSDOT J-28.30	HAND HOLE TO FACE ROADWAY
15	48+94	24.5' RT	125W, 3K, 60LED, 970mA, IV, B3-U0-G3, 240V	8'	35'	FIXED	1	TYPE B FOUNDATION PER WSDOT J-28.30	
16	50+46	24.5' RT	125W, 3K, 60LED, 970mA, IV, B3-U0-G3, 240V	8'	35'	FIXED	1	TYPE B FOUNDATION PER WSDOT J-28.30	

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KPG PSOMAS

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CITY OF KIRKLAND
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123 FIFTH AVENUE KIRKLAND, WA 98033
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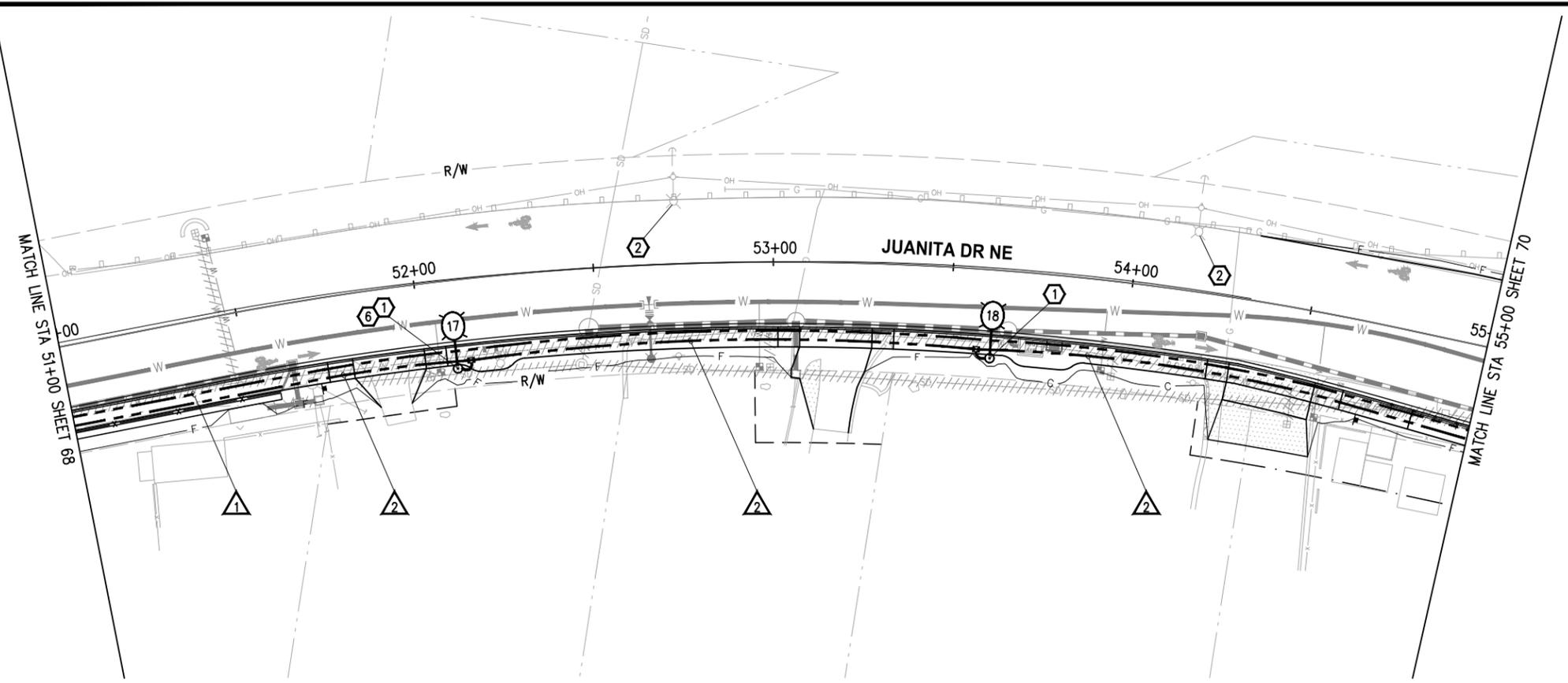
**JUANITA DRIVE MULTIMODAL,
INTERSECTION & SAFETY IMPROVEMENTS**

67\$ 72 67\$

ILLUMINATION PLAN

REFERENCE SHEET NO.	
SHEET	2)
SHEETS	

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3. NUMBER OF CONDUIT BENDS BETWEEN PULL POINTS SHALL NOT BE MORE THAN THE EQUIVALENT OF FOUR QUARTER BENDS (360 DEGREES TOTAL). IF NUMBER OF BENDS EXCEEDS 360 DEGREES, CONTRACTOR SHALL INSTALL ADDITIONAL JUNCTION BOXES, AS REQUIRED.
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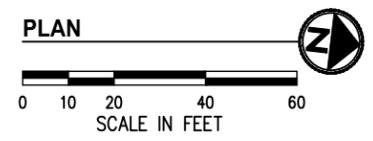
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WIRE NOTES

#	RACEWAY/ CONDUIT SIZE	CONDUCTORS	COMMENTS
1	2"		SPARE
2	2"	2-#8(ILL)	
3	2"	4-#10(RRFB), 1-2CS	
4	2"	2-#10(RRFB), 1-2CS	
5	2"	2-#8(ILL), 2-#10(RRFB)	
6	3"	3-#3/0	FEEDER, NO GROUND

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LEGEND

EXISTING	NEW	DESCRIPTION
		ROADWAY LUMINAIRE
		JUNCTION BOX TYPE 1, 2, 8
		ELECTRICAL SERVICE CABINET
		CONDUIT
		APS PEDESTRIAN PUSH BUTTON
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		WIRE NOTE
		CONSTRUCTION NOTE
		POLE NOTE

LUMINAIRE SCHEDULE

LUMINAIRE #	STATION	OFFSET	LUMINAIRE TYPE	LUMINAIRE ARM	MOUNTING HEIGHT	BASE	CIRCUIT #	FOUNDATION	COMMENTS
17	52+10	24.5' RT	125W, 3K, 60LED, 970mA, IV, B3-U0-G3, 240V	8'	35'	FIXED	1	TYPE B FOUNDATION PER WSDOT J-28.30	
18	53+62	24.5' RT	110W, 3K, 60LED, 900mA, III, B3-U0-G3, 240V	8'	35'	FIXED	1	TYPE B FOUNDATION PER WSDOT J-28.30	



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CITY OF KIRKLAND
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JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

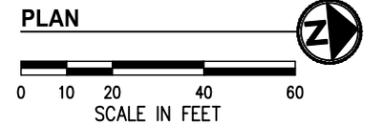
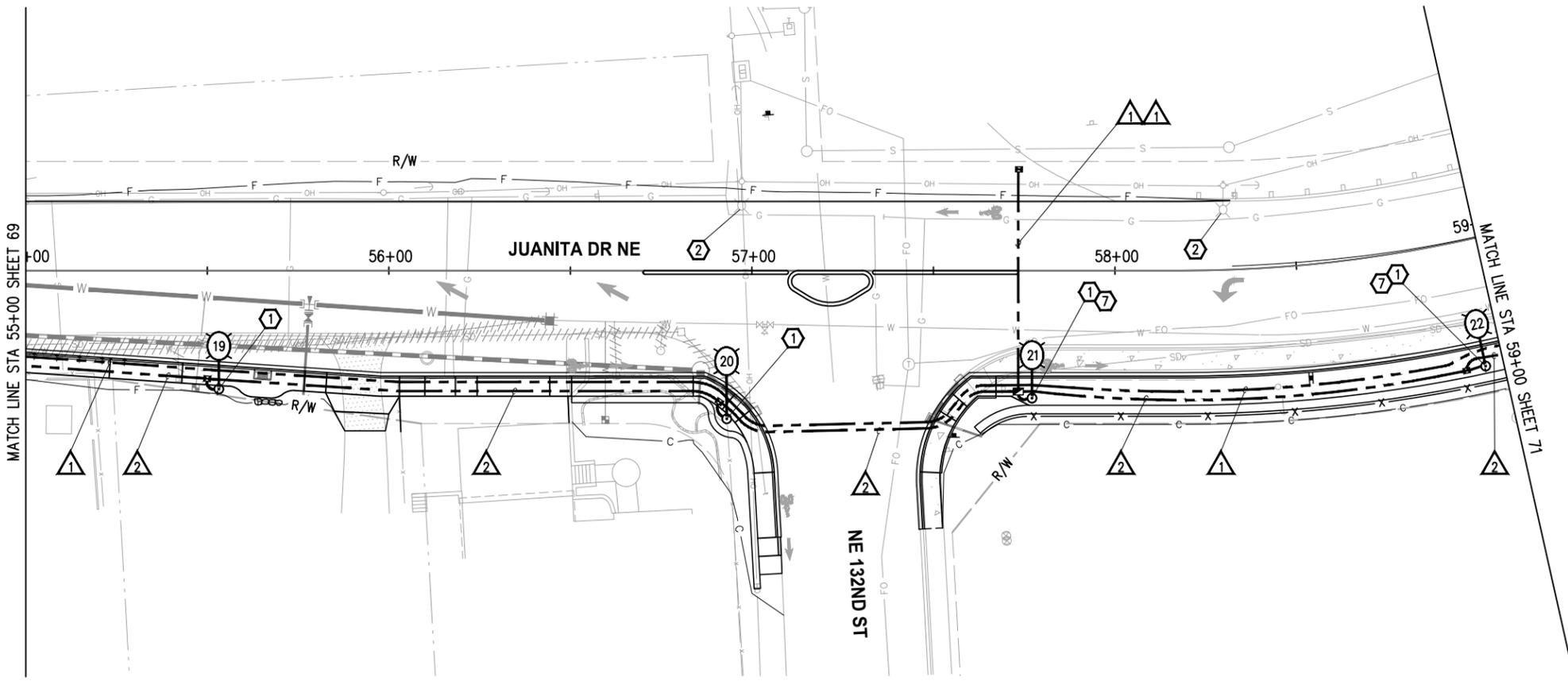
67\$ 72 67\$

ILLUMINATION PLAN

REFERENCE SHEET NO.

SHEET 2)

SHEETS



GENERAL NOTES

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WIRE NOTES

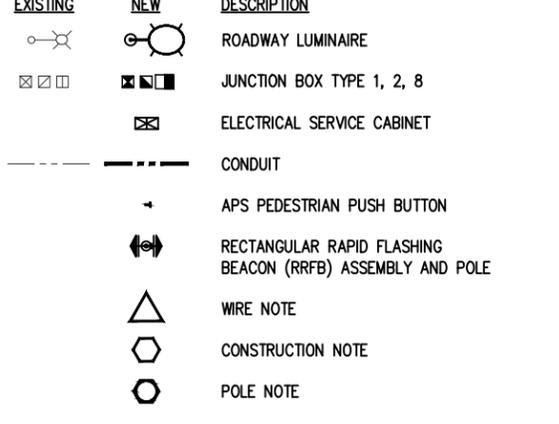
#	RACEWAY/ CONDUIT SIZE	CONDUCTORS	COMMENTS
1	2"		SPARE
2	2"	2-#8(ILL)	
3	2"	4-#10(RRFB), 1-2CS	
4	2"	2-#10(RRFB), 1-2CS	
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- ② EXISTING POLE MOUNTED LUMINAIRE TO BE REMOVED BY OTHERS.
- ⑦ INSTALL LUMINAIRE FOUNDATION CENTERED BETWEEN SOLDIER PILE FOUNDATIONS.

LEGEND



LUMINAIRE SCHEDULE

LUMINAIRE #	STATION	OFFSET	LUMINAIRE TYPE	LUMINAIRE ARM	MOUNTING HEIGHT	BASE	CIRCUIT #	FOUNDATION	COMMENTS
19	55+53	32.5' RT	110W, 3K, 60LED, 900mA, III, B3-U0-G3, 240V	8'	35'	FIXED	1	TYPE B FOUNDATION PER WSDOT J-28.30	
20	56+93	40.7' RT	110W, 3K, 60LED, 900mA, III, B3-U0-G3, 240V	12'	35'	FIXED	1	TYPE B FOUNDATION PER WSDOT J-28.30	HAND HOLE TO FACE ROADWAY
21	57+77	35.0' RT	110W, 3K, 60LED, 900mA, III, B3-U0-G3, 240V	8'	35'	FIXED	1	TYPE B FOUNDATION PER WSDOT J-28.30, EXCEPT FOUNDATION DEPTH SHALL BE 9'	HAND HOLE TO FACE ROADWAY
22	58+95	35.0' RT	110W, 3K, 60LED, 900mA, III, B3-U0-G3, 240V	8'	35'	FIXED	1	TYPE B FOUNDATION PER WSDOT J-28.30, EXCEPT FOUNDATION DEPTH SHALL BE 9'	HAND HOLE TO FACE ROADWAY



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CITY OF KIRKLAND WASHINGTON
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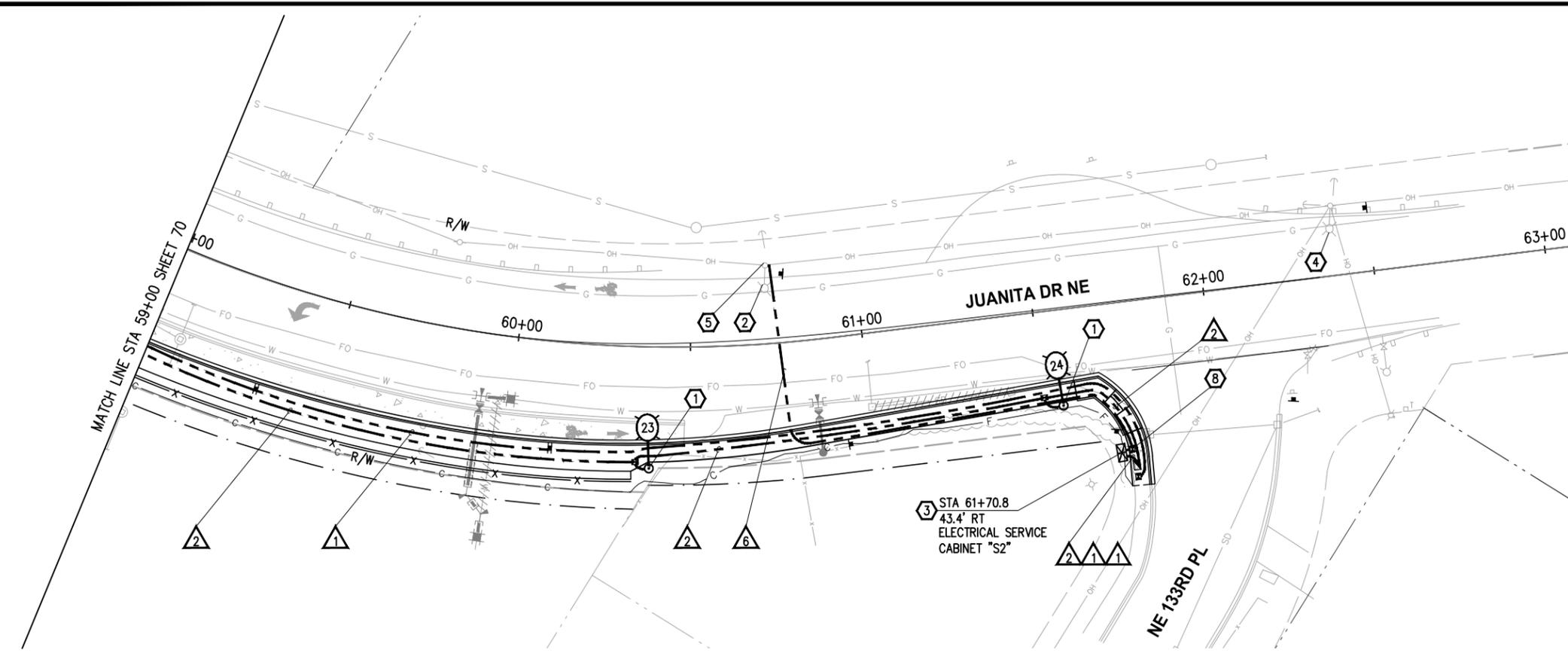
JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

67\$ 72 67\$

ILLUMINATION PLAN

REFERENCE SHEET NO. SHEET 2) SHEETS

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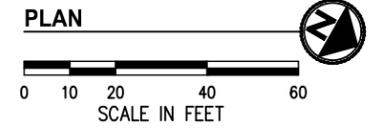
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1	2"		SPARE
2	2"	2-#8(ILL)	
3	2"	4-#10(RRFB), 1-2CS	
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LUMINAIRE SCHEDULE

LUMINAIRE #	STATION	OFFSET	LUMINAIRE TYPE	LUMINAIRE ARM	MOUNTING HEIGHT	BASE	CIRCUIT #	FOUNDATION	COMMENTS
23	60+39	35.2' RT	110W, 3K, 60LED, 900mA, III, B3-U0-G3, 240V	8'	35'	FIXED	1	TYPE B FOUNDATION PER WSDOT J-28.30, EXCEPT FOUNDATION DEPTH SHALL BE 9'	
24	61+56	27.8' RT	110W, 3K, 60LED, 900mA, III, B3-U0-G3, 240V	8'	35'	FIXED	1	TYPE B FOUNDATION PER WSDOT J-28.30, EXCEPT FOUNDATION DEPTH SHALL BE 9'	



LEGEND

EXISTING	NEW	DESCRIPTION
		ROADWAY LUMINAIRE
		JUNCTION BOX TYPE 1, 2, 8
		ELECTRICAL SERVICE CABINET
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- EXISTING POLE MOUNTED LUMINAIRE TO BE REMOVED BY OTHERS.
- CONSTRUCT ELECTRICAL SERVICE CABINET FOUNDATION PER COK DETAIL CK-TS.11. CONTRACTOR SHALL FURNISH AND INSTALL TINY ELECTRICAL SERVICE CABINET PER COK DETAIL CK-TS.10. VERIFY FINAL LOCATION WITH THE ENGINEER IN THE FIELD. COORDINATE ELECTRICAL SERVICE METER BASE INSTALLATION WITH PSE REPRESENTATIVE. SEE SPECIAL PROVISIONS.
- EXISTING POLE MOUNTED LUMINAIRE TO REMAIN.
- LOCATION OF EXISTING TRANSFORMER. CONTRACTOR SHALL FURNISH AND INSTALL CONDUIT RISER ON POLE PER PSE HANDBOOK. INSTALL WIRING TO TOP OF RISER AND COIL 30'. PROVIDE 20' OF SPARE 3" CONDUIT FOR POWER COMPANY USE. COORDINATE FEEDER WIRE TERMINATION WITH PSE REPRESENTATIVE. SEE SPECIAL PROVISIONS.
- CONTRACTOR TO INSTALL HAND HOLE WITH FARGO DISCONNECT AS CLOSE TO CABINET AS POSSIBLE. COORDINATE WITH CITY OF KIRKLAND ENGINEER.

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CITY OF KIRKLAND WASHINGTON

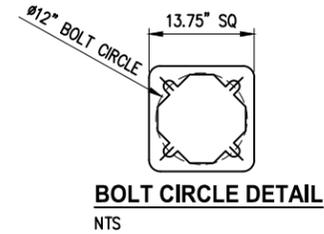
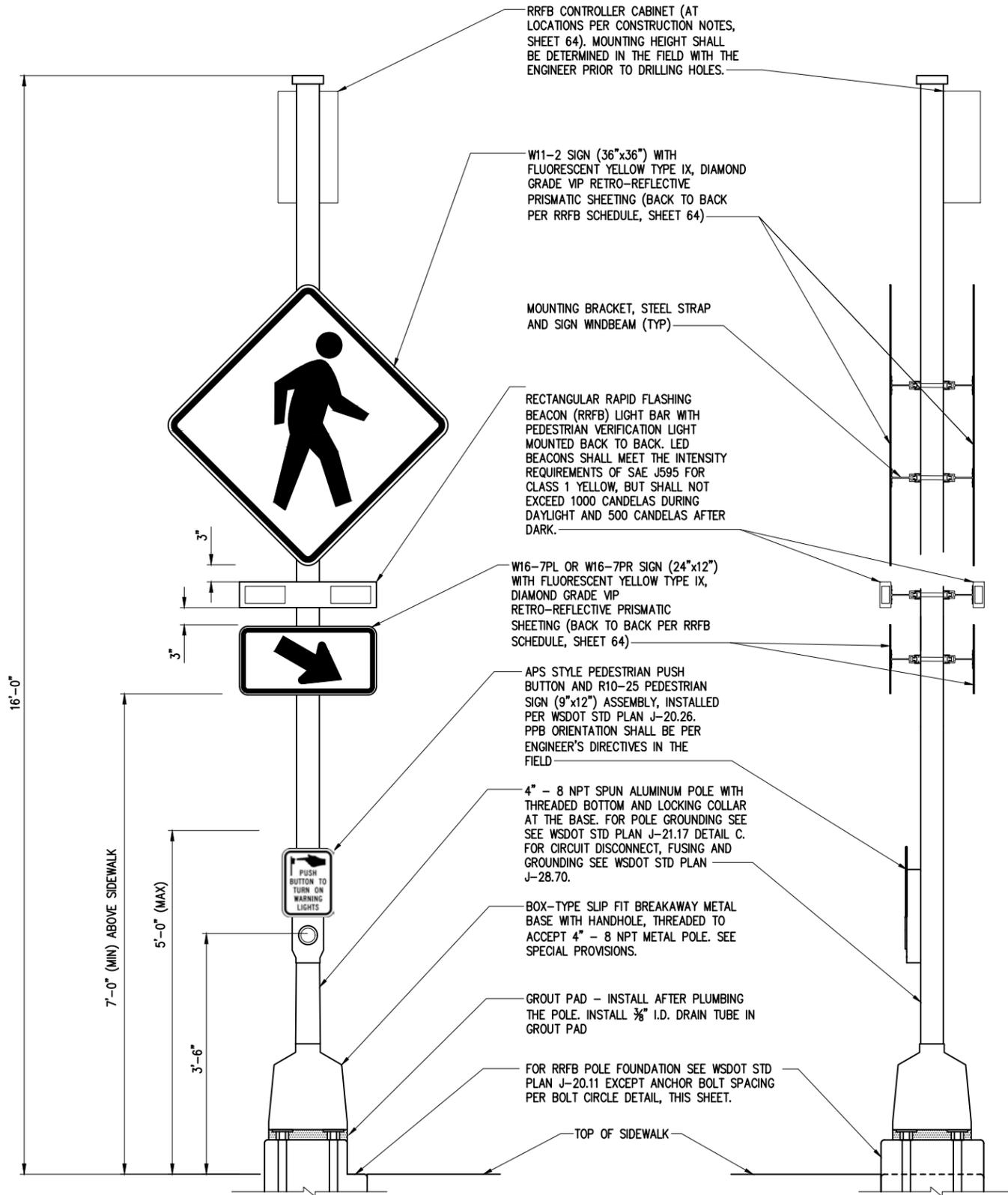
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JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

67\$ 72 67\$

ILLUMINATION PLAN

REFERENCE SHEET NO.
 SHEET
 2)
 SHEETS



RRFB POLE DETAIL
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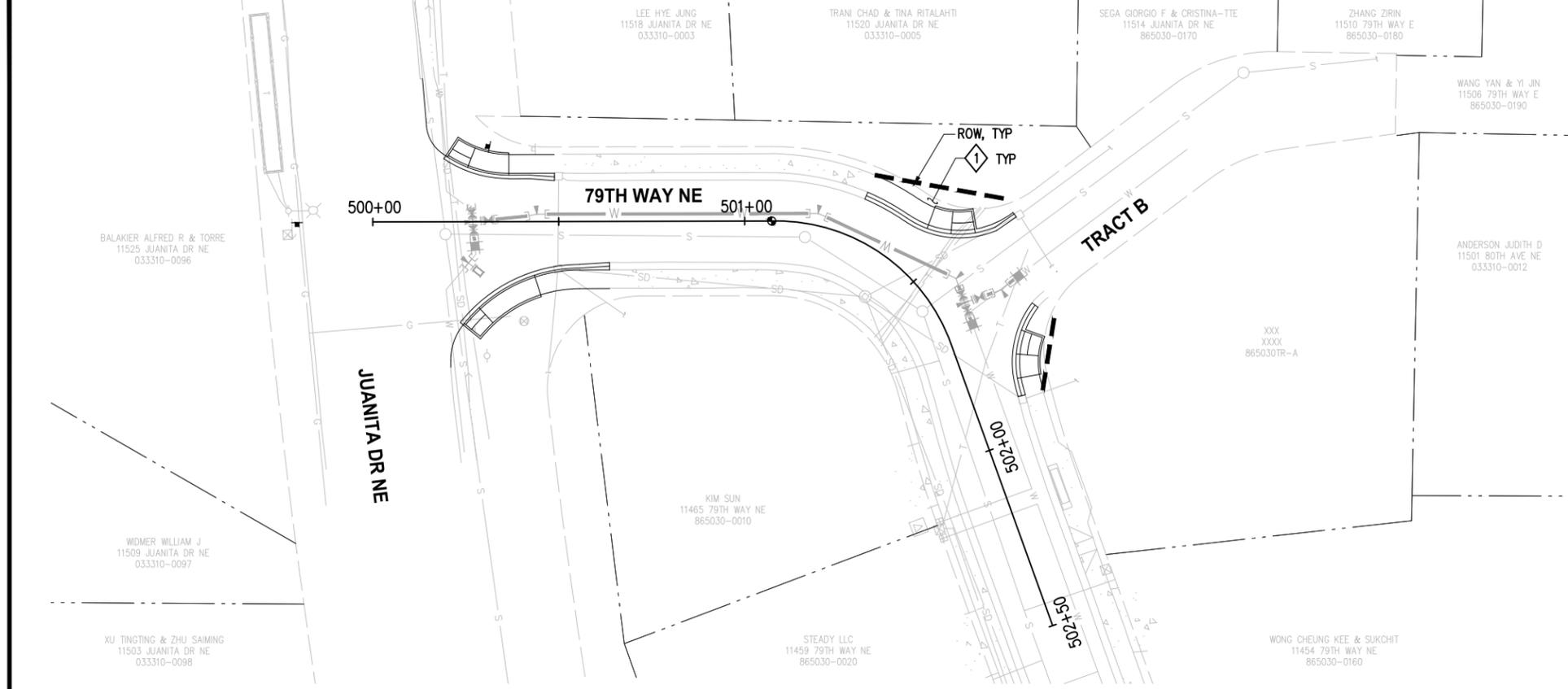
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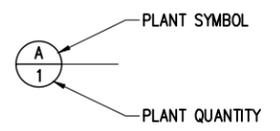
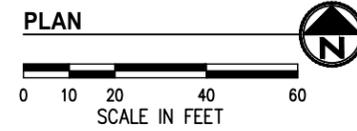
JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

ILLUMINATION DETAILS

REFERENCE SHEET NO.
SHEET
2)
SHEETS



CONSTRUCTION NOTES
 1) BACK OF WALK RESTORATION PER RESTORATION TABLE SHEET 81.



SEE SHEET 81 FOR LANDSCAPE SCHEDULE, DETAILS AND GENERAL NOTES

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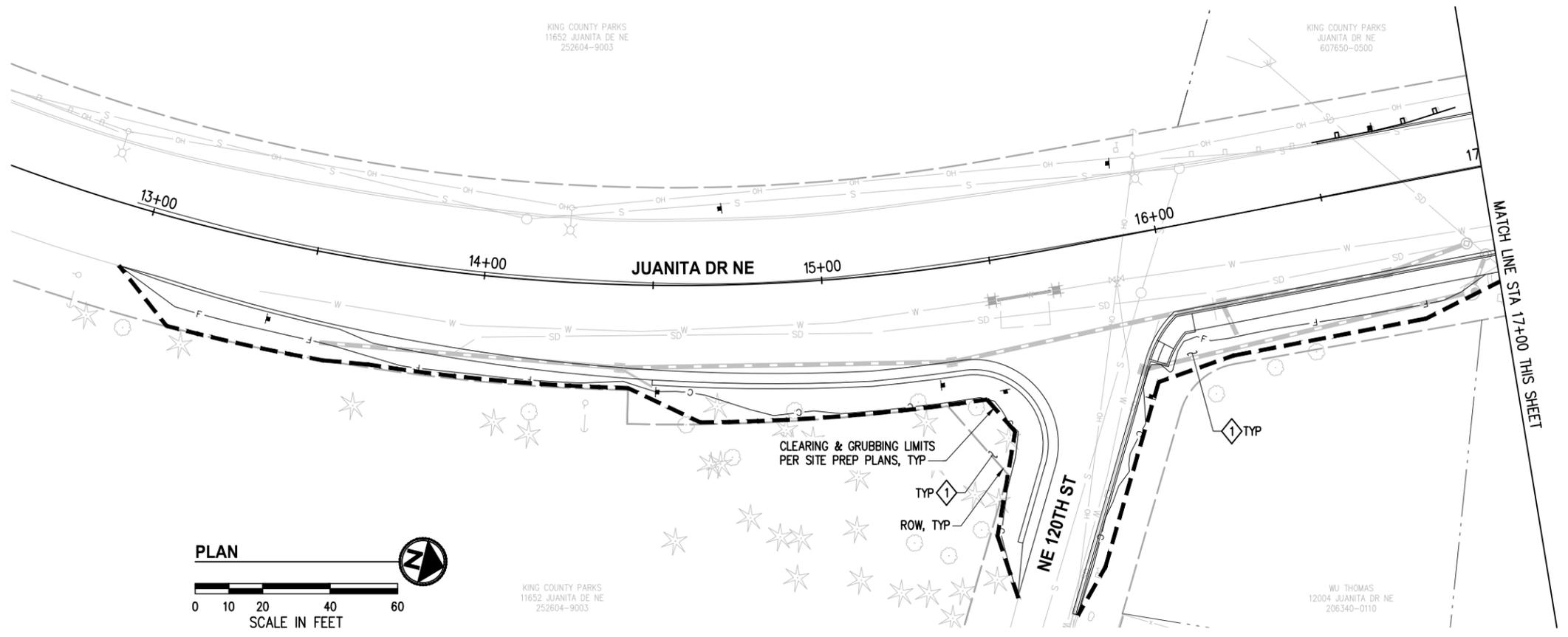
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JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

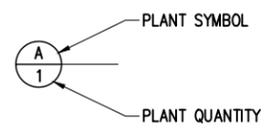
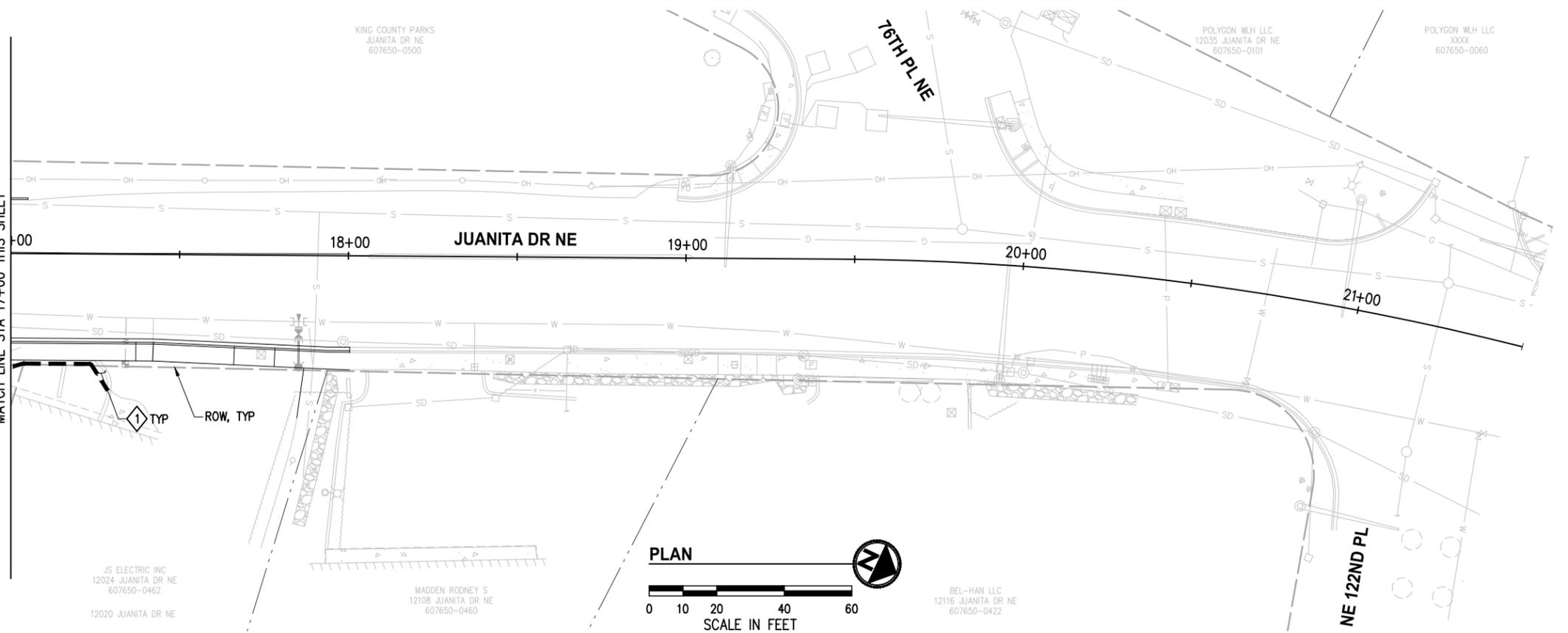
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LANDSCAPE RESTORATION PLAN

REFERENCE SHEET NO. SHEET 2) SHEETS



CONSTRUCTION NOTES
 ① BACK OF WALK RESTORATION PER RESTORATION TABLE SHEET 81.



SEE SHEET 81 FOR LANDSCAPE SCHEDULE, DETAILS AND GENERAL NOTES

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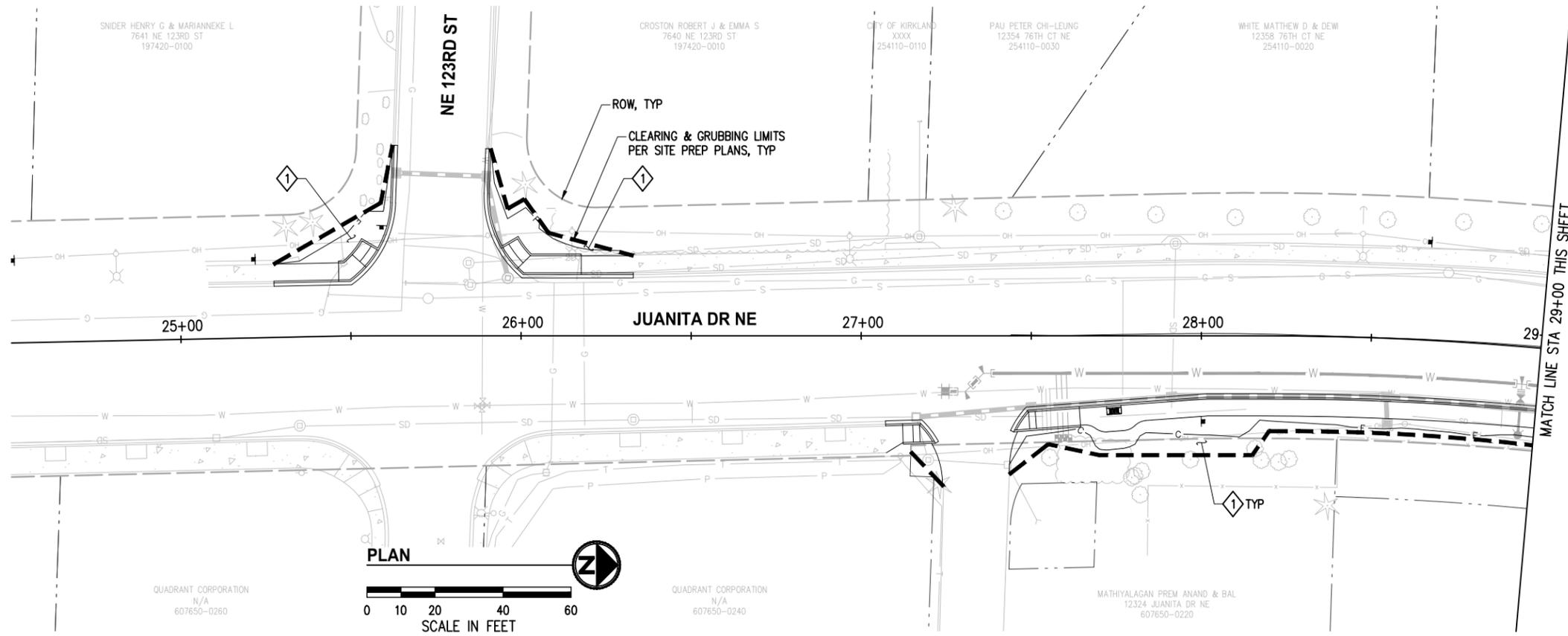
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JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

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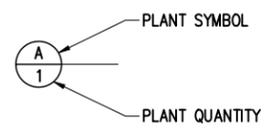
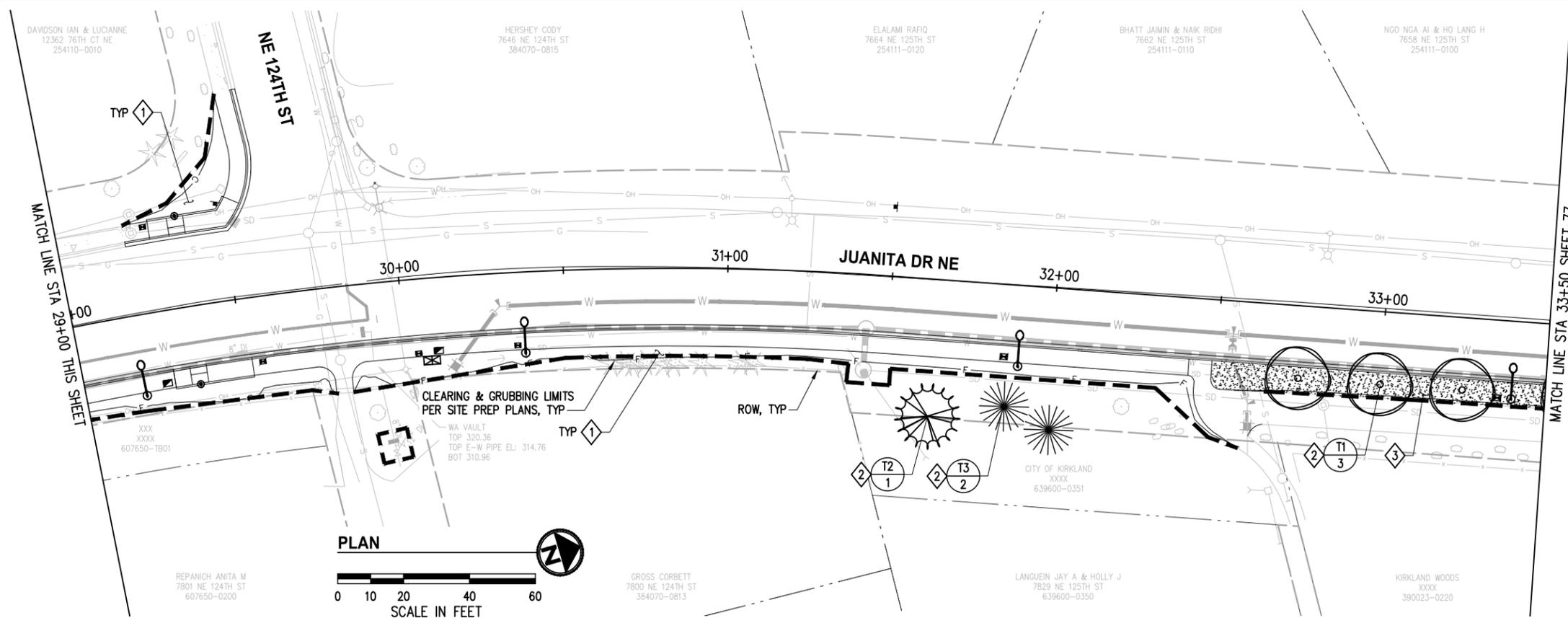
LANDSCAPE RESTORATION PLAN

REFERENCE SHEET NO.	
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CONSTRUCTION NOTES

- ① BACK OF WALK RESTORATION PER RESTORATION TABLE SHEET 81.
- ② PLANT TREE PER DETAILS, SHEET 81. INSTALL TREE WATERING BAG SYSTEM, ONE EACH PER TREE, TYP.
- ③ SEEDED LAWN INSTALLATION; INSTALL 4" TOPSOIL TYPE A AND HYDROSEED WITH SEEDED LAWN MIX.



SEE SHEET 81 FOR LANDSCAPE SCHEDULE, DETAILS AND GENERAL NOTES

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JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

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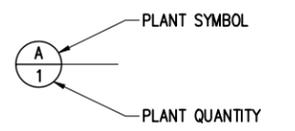
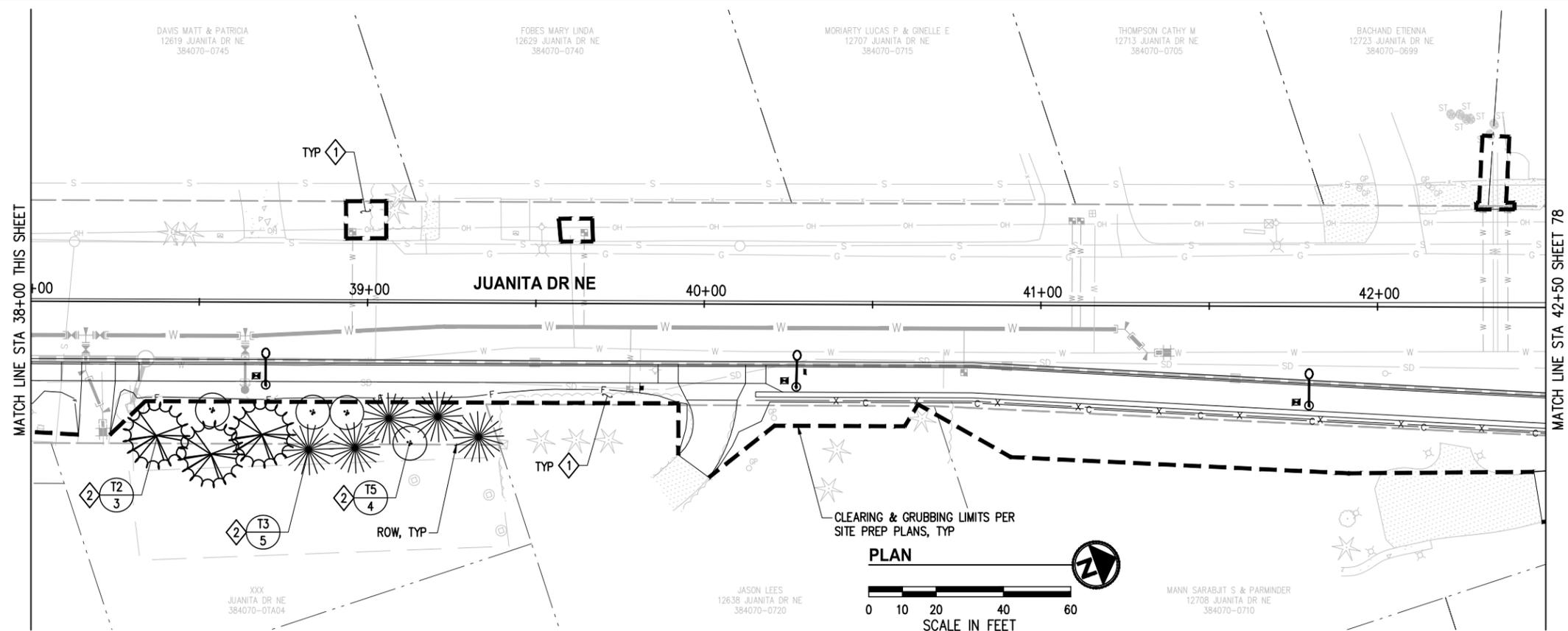
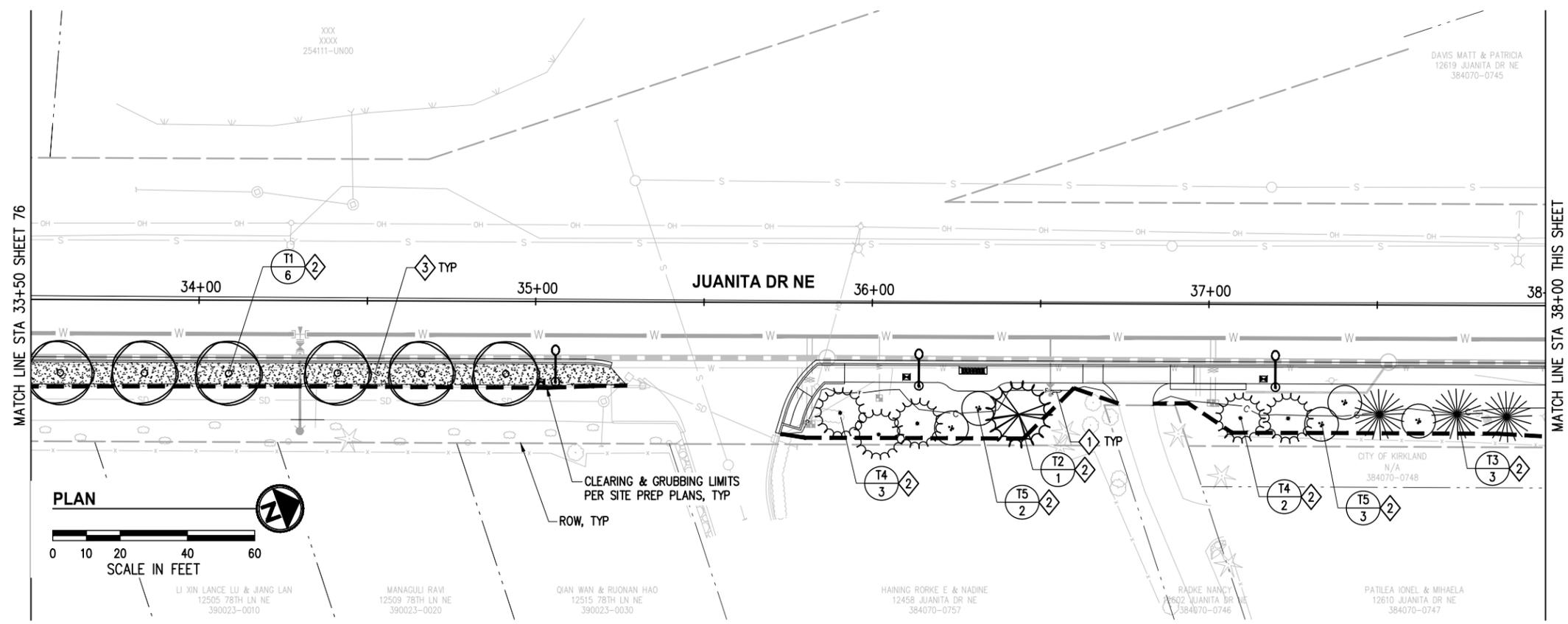
LANDSCAPE RESTORATION PLAN

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CONSTRUCTION NOTES

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- ② PLANT TREE PER DETAILS, SHEET 81. INSTALL TREE WATERING BAG SYSTEM, ONE EACH PER TREE, TYP.
- ③ SEEDED LAWN INSTALLATION; INSTALL 4" TOPSOIL TYPE A AND HYDROSEED WITH SEEDED LAWN MIX.



SEE SHEET 81 FOR LANDSCAPE SCHEDULE, DETAILS AND GENERAL NOTES

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JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

67\$ 72 67\$

LANDSCAPE RESTORATION PLAN

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TREE SCHEDULE

SYMBOL	BOTANICAL NAME/ COMMON NAME	SIZE	QTY*	REMARKS
	T1 NYSSA SYLVATICA 'DAVID ODOM'/ AFTERBURNER TUPELO ²	2.5" CAL. 12-14' HT.	9	B&B; WELL-BRANCHED
	T2 PINUS CONTORTA 'CONTORTA'/ SHORE PINE ¹	7'-8' HT.	8	B&B; WELL-BRANCHED
	T3 THUJA PLICATA/ WESTERN RED CEDAR ¹	7'-8' HT.	13	B&B; WELL-BRANCHED
	T4 TSUGA MERTENSIANA/ MOUNTAIN HEMLOCK ¹	7'-8' HT.	9	B&B; WELL-BRANCHED
	T5 ACER CIRCINATUM/ VINE MAPLE ²	6'-7' HT.	16	3-5 CANES MIN. 1' MIN. CAL. PER CANE

¹ (30) TREES TO BE PLANTED ALONG THE PROJECT CORRIDOR TO MITIGATE FOR (10) LANDMARK TREE REMOVALS (3:1 RATIO).
² (25) TREES TO BE PLANTED ALONG THE PROJECT CORRIDOR TO MITIGATE FOR (25) OF (91) SIGNIFICANT TREE REMOVALS (1:1 RATIO). AN ADDITIONAL 66 TREES WILL BE MITIGATED THROUGH FEE-IN-LIEU PAID TO THE CITY FORESTRY ACCOUNT.

RESTORATION TABLE

EXISTING CONDITIONS	RESTORATION
GRAVEL	2" CSTC*
LAWN / UNIMPROVED AREA	SEDED LAWN OVER 2" TOPSOIL TYPE A*
LANDSCAPED AREA / MULCH	2" BARK OR WOOD CHIP MULCH OVER 4" TOPSOIL TYPE A*

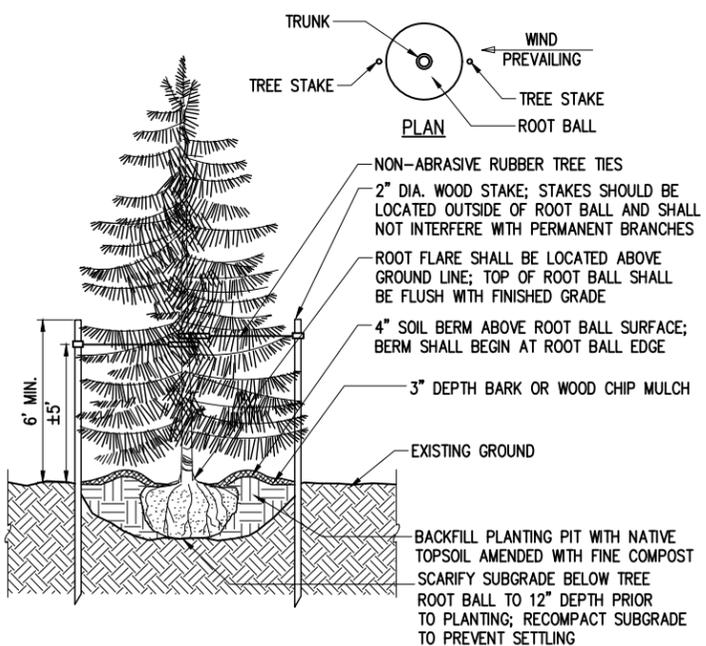
*INCREASE DEPTH OF CSTC/TOPSOIL AS REQUIRED TO BRING FINISHED GRADE FLUSH WITH ADJACENT SURFACES

GENERAL NOTES

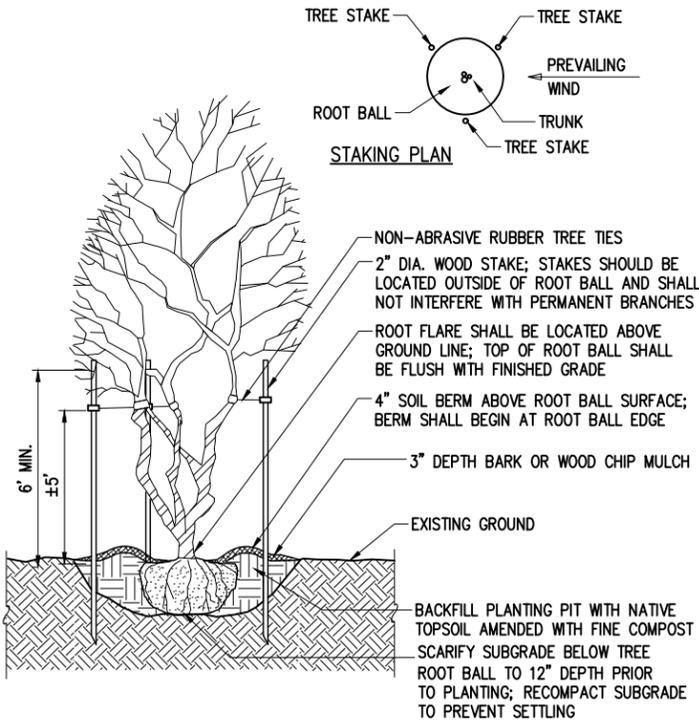
1. ALL PLANT MATERIALS SHALL MEET THE AMERICAN STANDARD FOR NURSERY STOCK, ANSI Z60.1 MOST CURRENT VERSION.
2. PLANT, MAINTAIN AND WARRANTY AS PER SPECIAL PROVISIONS.
3. DO NOT SUBSTITUTE SPECIES WITHOUT THE APPROVAL OF ENGINEER.
4. INSTALL 2" MULCH OVER ALL DISTURBED AREAS NOT BEING PLANTED OR SEDED, UNLESS OTHERWISE NOTED.
5. PROPERTY RESTORATION TO BE DONE AS DIRECTED BY THE PROJECT ENGINEER.
6. ALL SINGLE-STEM DECIDUOUS TREES SHALL MEET WSDOT "STREET TREE GRADE" STANDARD AND SHALL BRANCH AT MIN. 6' ABOVE ROOTBALL.

TREE PLANTING NOTES

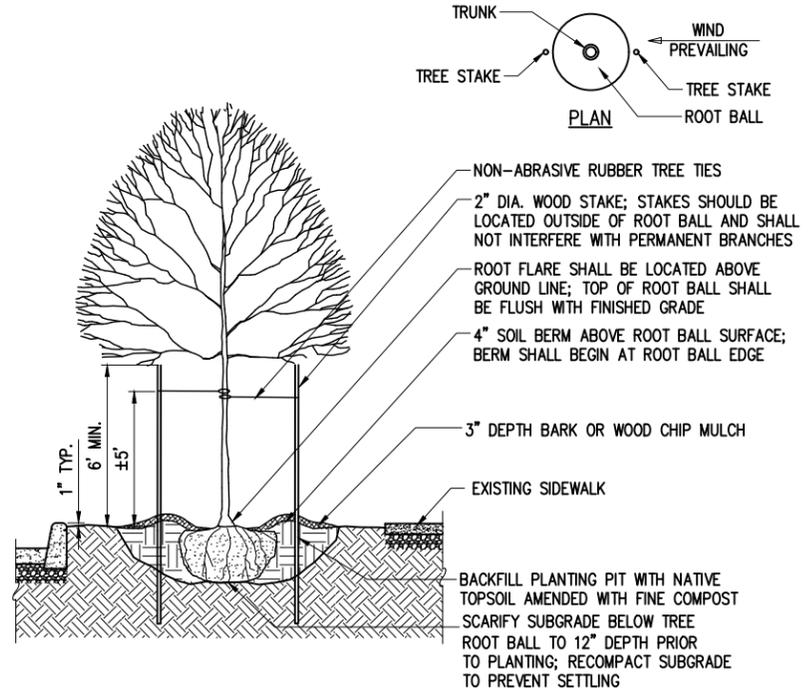
1. TREE PIT SHALL NOT BE LESS THAN (3) TIMES ROOTBALL DIAMETER.
2. CUT ALL TIES AND REMOVE BURLAP FROM ROOTBALL.
3. REMOVE ALL PLASTIC AND TWINE.
4. BACKFILL TOPSOIL AROUND ROOT BALL IN 6" LIFTS AND LIGHTLY RECOMPACT EACH LIFT; AFTER BACKFILLING, WATER IN ROOT BALL TO SETTLE THE SOIL.
5. PLANT TREES WITH ROOT FLARE VISIBLE; DO NOT COVER TOP OF ROOTBALL WITH SOIL.



TYPICAL EVERGREEN TREE PLANTING DETAIL
NTS



TYPICAL MULTI-STEM TREE PLANTING DETAIL
NTS



TYPICAL DECIDUOUS TREE PLANTING DETAIL
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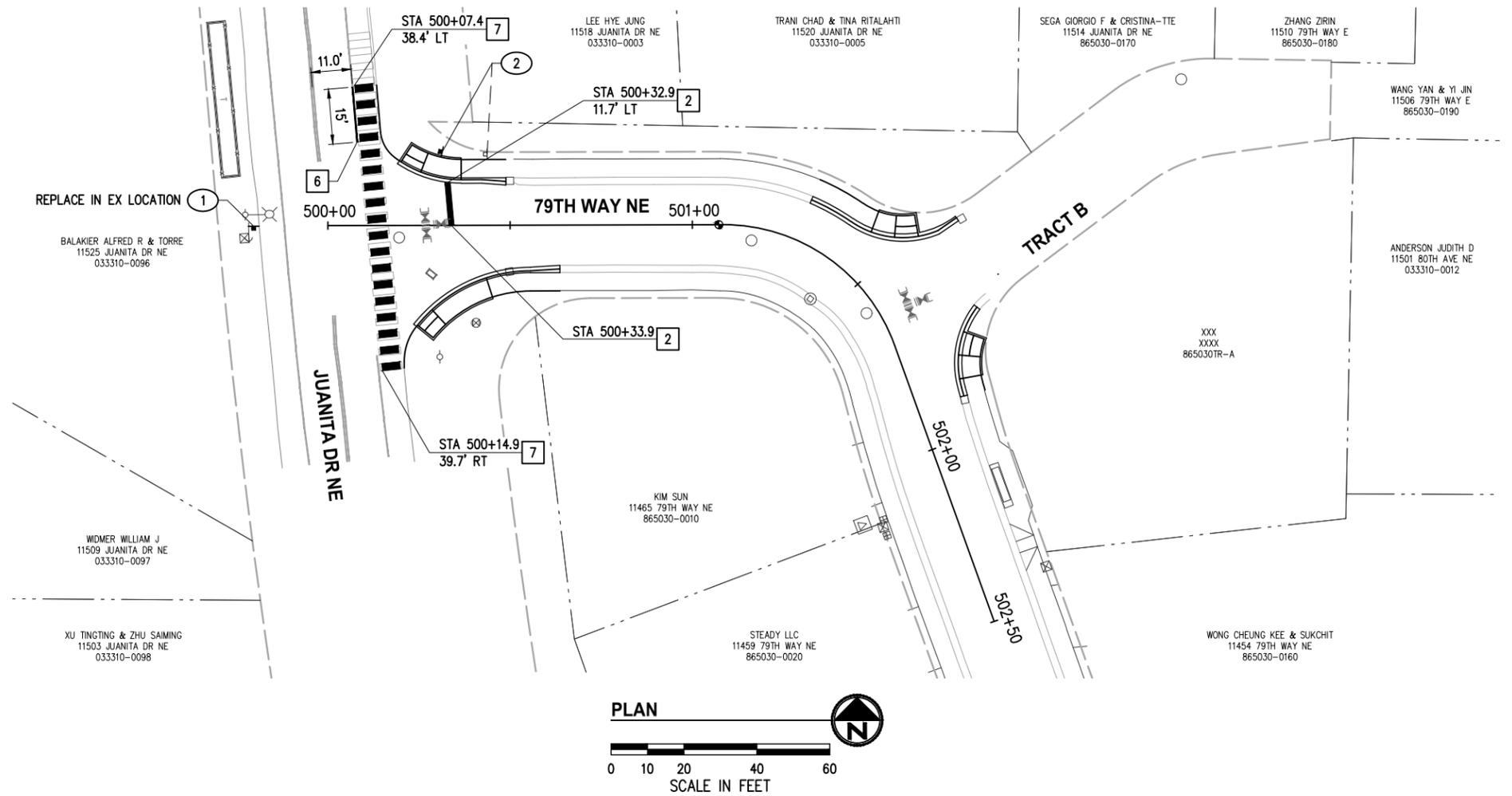
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**JUANITA DRIVE MULTIMODAL,
INTERSECTION & SAFETY IMPROVEMENTS**

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LANDSCAPE RESTORATION PLAN

REFERENCE SHEET NO.
SHEET 2) SHEETS



GENERAL NOTES

- ALL EXISTING STRIPING, MARKINGS, AND SIGNS IN CONFLICT WITH THE IMPROVEMENTS SHALL BE REMOVED.
- CONTRACTOR SHALL PRESERVE AND PROTECT ALL SIGNS NOT SPECIFICALLY CALLED OUT FOR REMOVAL OR RELOCATION, UNLESS DIRECTED OTHERWISE.
- ALL SYMBOLS SHALL BE CENTERED IN LANE.
- ALL MARKING SHALL BE 4' CLEAR FROM CROSSWALKS.
- ALL SIGNAGE AND MARKINGS SHALL CONFORM TO THE LATEST EDITION OF THE MUTCD.
- SIGNS LISTED IN SIGN SCHEDULE NOTED TO BE NEW, RELOCATED, OR REPLACED SHALL BE ALL NEW AND INSTALLED PER C.O.K. STD PLAN CK-R.43, UNLESS DIRECTED/NOTED OTHERWISE.
- STREET NAME SIGNS SHALL BE INSTALLED PER C.O.K. STD PLAN CK-R.44.
- CALLOUTS ARE TO CENTER OF LINE UNLESS OTHERWISE NOTED.

CONSTRUCTION NOTES

- 2 THERMOPLASTIC STOP BAR PER C.O.K. STD PLAN CK-R.28
- 3 PAINTED 6" WHITE EDGE LINE
- 6 PAINTED 8" WHITE GORE STRIPE PER C.O.K. STD PLAN CK-R.31
- 7 THERMOPLASTIC BIKE LANE CONFLICT ZONE MARKINGS PER C.O.K. STD PLAN CK-R.36A AND CK-R.36C

SIGN SCHEDULE					
SIGN NO.	STATION	OFFSET	DESIGNATION	SIZE	REMARKS
1	-	-	STREET NAME SIGNS	VARIES	REPLACE
2	500+31.4	20.1' LT	R1-1, STREET SIGNS	30X30	RELOCATE/REPLACE

LEGEND

- EXISTING SIGN
- PROPOSED SIGN
- SIGN SCHEDULE NOTE



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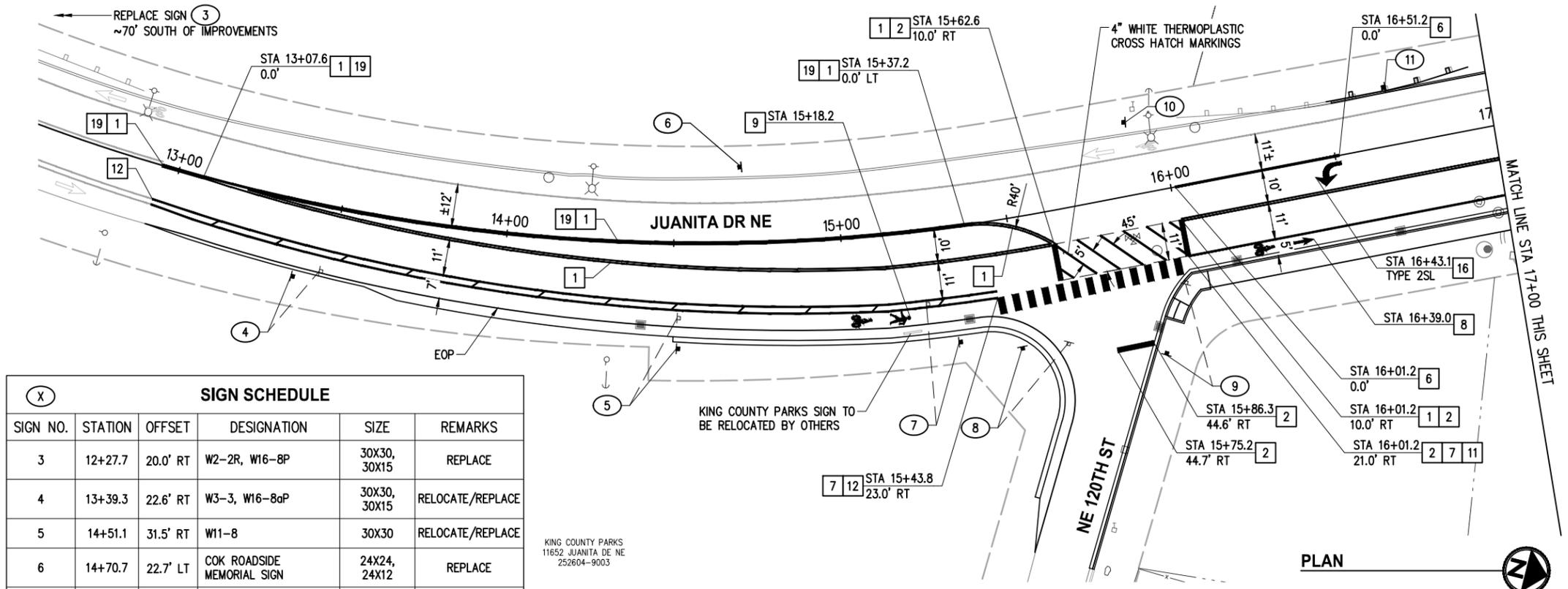


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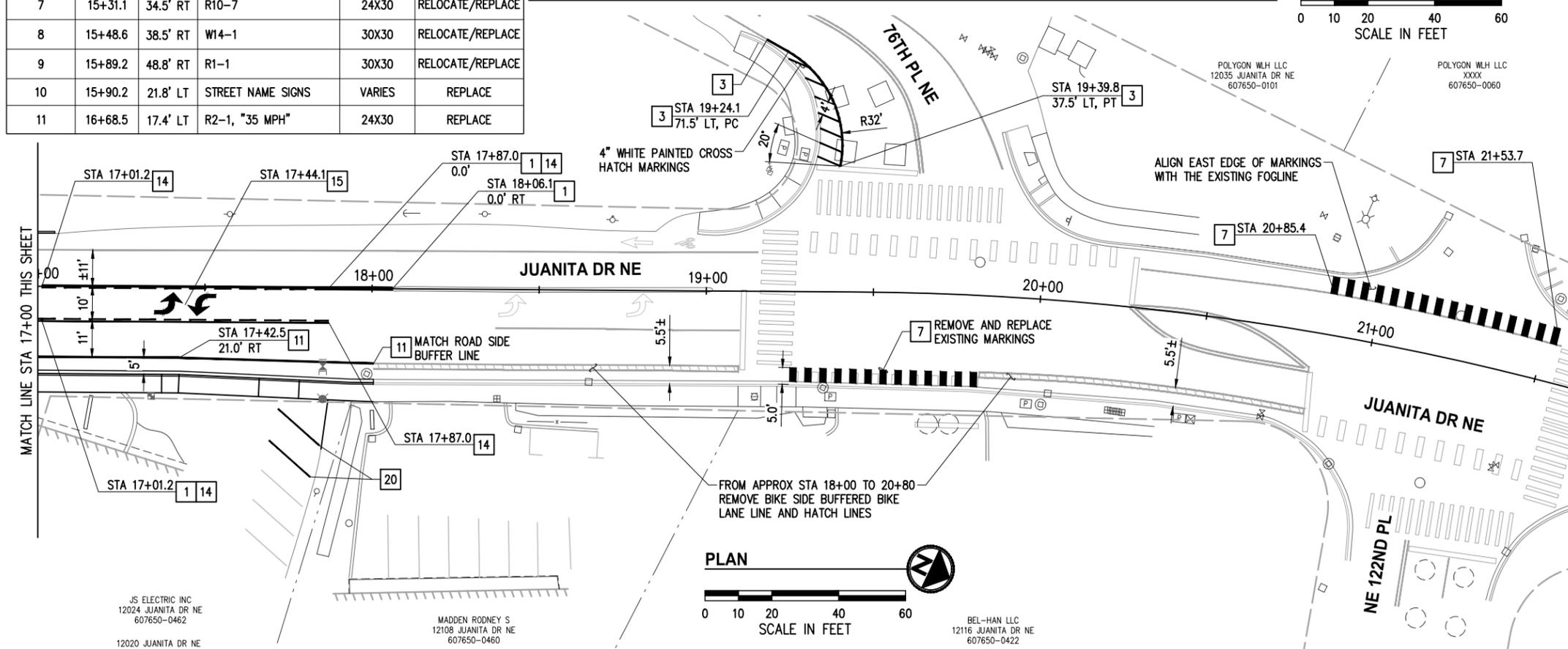


JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS
 67\$ 72 67\$
CHANNELIZATION & SIGNING PLAN

REFERENCE SHEET NO.
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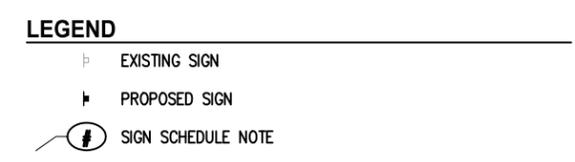


SIGN SCHEDULE					
SIGN NO.	STATION	OFFSET	DESIGNATION	SIZE	REMARKS
3	12+27.7	20.0' RT	W2-2R, W16-8P	30X30, 30X15	REPLACE
4	13+39.3	22.6' RT	W3-3, W16-8aP	30X30, 30X15	RELOCATE/REPLACE
5	14+51.1	31.5' RT	W11-8	30X30	RELOCATE/REPLACE
6	14+70.7	22.7' LT	COK ROADSIDE MEMORIAL SIGN	24X24, 24X12	REPLACE
7	15+31.1	34.5' RT	R10-7	24X30	RELOCATE/REPLACE
8	15+48.6	38.5' RT	W14-1	30X30	RELOCATE/REPLACE
9	15+89.2	48.8' RT	R1-1	30X30	RELOCATE/REPLACE
10	15+90.2	21.8' LT	STREET NAME SIGNS	VARIES	REPLACE
11	16+68.5	17.4' LT	R2-1, "35 MPH"	24X30	REPLACE

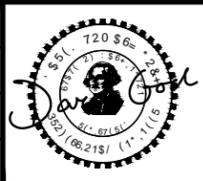


- ### GENERAL NOTES
- ALL EXISTING STRIPING, MARKINGS, AND SIGNS IN CONFLICT WITH THE IMPROVEMENTS SHALL BE REMOVED.
 - CONTRACTOR SHALL PRESERVE AND PROTECT ALL SIGNS NOT SPECIFICALLY CALLED OUT FOR REMOVAL OR RELOCATION, UNLESS DIRECTED OTHERWISE.
 - ALL SYMBOLS SHALL BE CENTERED IN LANE.
 - ALL MARKING SHALL BE 4' CLEAR FROM CROSSWALKS.
 - ALL SIGNAGE AND MARKINGS SHALL CONFORM TO THE LATEST EDITION OF THE MUTCD.
 - SIGNS LISTED IN SIGN SCHEDULE NOTED TO BE NEW, RELOCATED, OR REPLACED SHALL BE ALL NEW AND INSTALLED PER C.O.K. STD PLAN CK-R.43, UNLESS DIRECTED/NOTED OTHERWISE.
 - STREET NAME SIGNS SHALL BE INSTALLED PER C.O.K. STD PLAN CK-R.44.
 - CALLOUTS ARE TO CENTER OF LINE UNLESS OTHERWISE NOTED.

- ### CONSTRUCTION NOTES
- PAINTED DOUBLE YELLOW CENTERLINE PER C.O.K. STD PLAN CK-R.31
 - THERMOPLASTIC STOP BAR PER C.O.K. STD PLAN CK-R.28
 - PAINTED 6" WHITE EDGE LINE
 - PAINTED 8" WHITE GORE STRIPE PER C.O.K. STD PLAN CK-R.31
 - THERMOPLASTIC BIKE LANE CONFLICT ZONE MARKINGS PER C.O.K. STD PLAN CK-R.36A AND CK-R.36C
 - THERMOPLASTIC BICYCLE LANE MARKINGS PER C.O.K. STD PLAN CK-R.34
 - THERMOPLASTIC BICYCLE AND PEDESTRIAN LANE MARKINGS PER C.O.K. STD PLAN CK-R.34B
 - PAINTED 6" WHITE EDGE LINE FOR BIKE LANE PER C.O.K. STD PLAN CK-R.35
 - PROFILED THERMOPLASTIC 6" WHITE ROAD-SIDE LANE LINE PER WSDOT STD PLAN M-20.20. PAINTED 6" WHITE BIKE-SIDE BUFFERED BIKE LANE LINE AND PAINTED 4" WHITE HATCH LINES PER C.O.K. STD PLAN CK-R.35A. CALLOUT IS TO CENTER OF BIKE-SIDE LINE
 - PAINTED TWO-WAY LEFT TURN LANE LINE PER C.O.K. STD PLAN CK-R.31
 - THERMOPLASTIC TWO-WAY LEFT TURN ARROWS, SPACING PER C.O.K. STD PLAN CK-R.30, TRAFFIC ARROW PER WSDOT STD PLAN M-24.40
 - THERMOPLASTIC TRAFFIC ARROW PER WSDOT STD PLAN M-24.40, TYPE AS NOTED
 - CENTERLINE RUMBLE STRIP PER WSDOT STD PLAN M-65.10-03
 - REPLACE PAVEMENT MARKINGS, MATCH EXISTING



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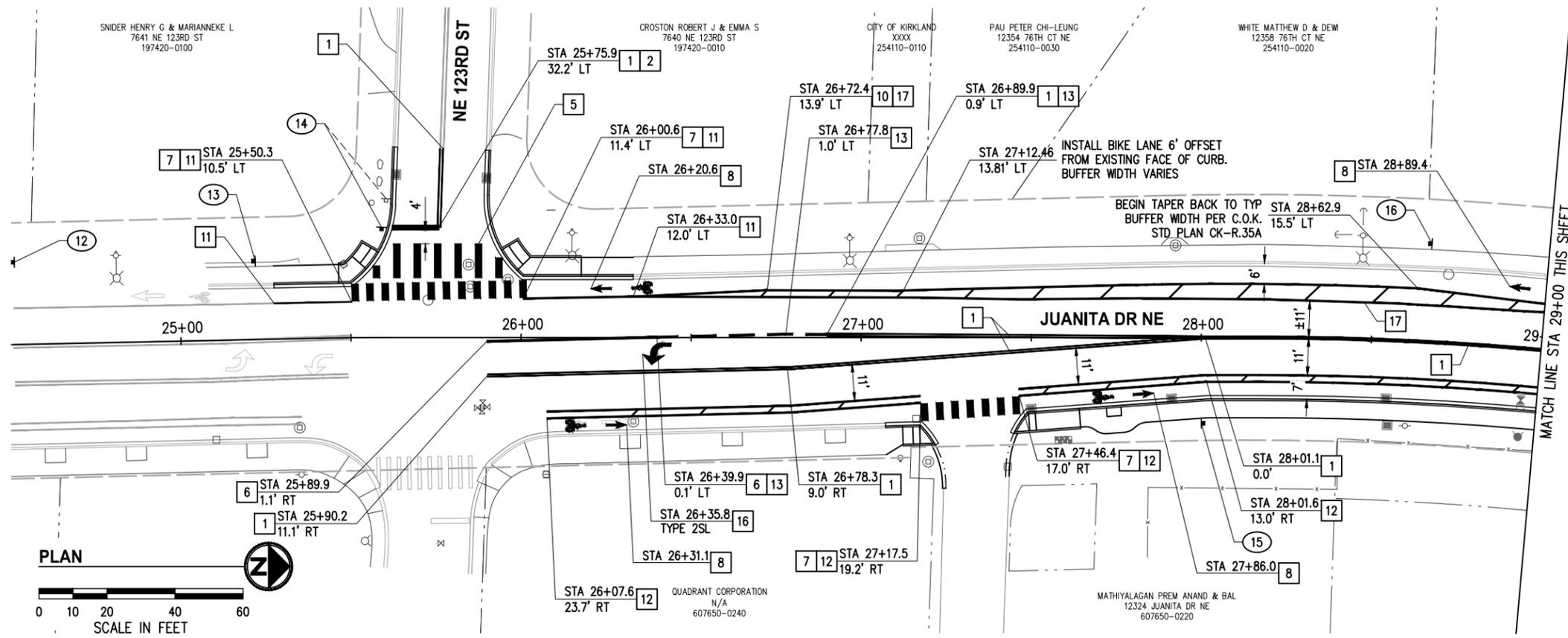
JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

67\$ 72 67\$

CHANNELIZATION & SIGNING PLAN

REFERENCE SHEET NO. SHEET 2) SHEETS

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GENERAL NOTES

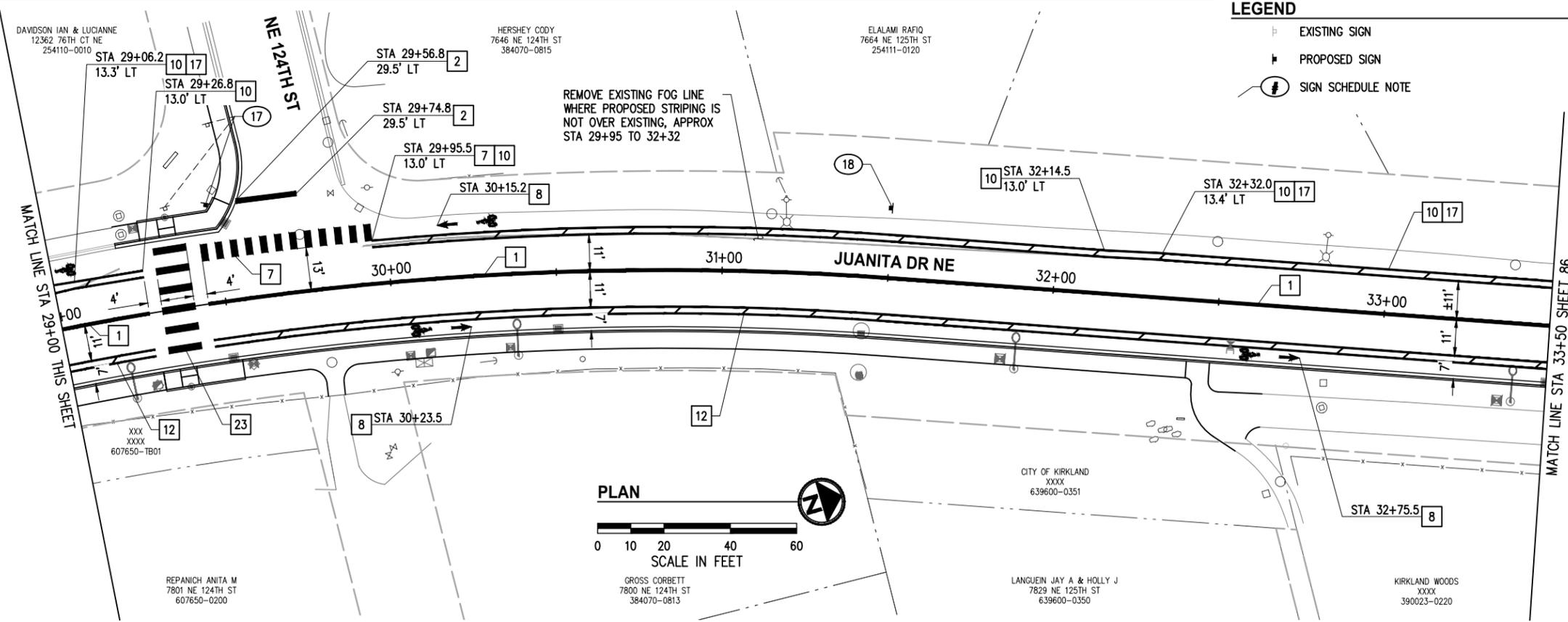
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CONSTRUCTION NOTES

- PAINTED DOUBLE YELLOW CENTERLINE PER C.O.K. STD PLAN CK-R.31
- THERMOPLASTIC STOP BAR PER C.O.K. STD PLAN CK-R.28
- THERMOPLASTIC CROSSWALK PER C.O.K. STD PLAN CK-R.28, CENTERED ON RAMP OPENINGS
- PAINTED 8" WHITE GORE STRIPE PER C.O.K. STD PLAN CK-R.31
- THERMOPLASTIC BIKE LANE CONFLICT ZONE MARKINGS PER C.O.K. STD PLAN CK-R.36A AND CK-R.36C
- THERMOPLASTIC BICYCLE LANE MARKINGS PER C.O.K. STD PLAN CK-R.34
- PAINTED 6" WHITE BUFFERED BIKE LANE LINES AND PAINTED 4" WHITE HATCH LINES PER C.O.K. STD PLAN CK-R.35A. CALLOUT IS TO CENTER OF BIKE-SIDE LINE
- PAINTED 6" WHITE EDGE LINE FOR BIKE LANE PER C.O.K. STD PLAN CK-R.35
- PROFIED THERMOPLASTIC 6" WHITE ROAD-SIDE LANE LINE PER WSDOT STD PLAN M-20.20. PAINTED 6" WHITE BIKE-SIDE BUFFERED BIKE LANE LINE AND PAINTED 4" WHITE HATCH LINES PER C.O.K. STD PLAN CK-R.35A. CALLOUT IS TO CENTER OF BIKE-SIDE LINE
- PAINTED WHITE DOTTED EXTENSION LINE PER WSDOT STD PLAN M-20.10
- THERMOPLASTIC TRAFFIC ARROW PER WSDOT STD PLAN M-24.40, TYPE AS NOTED
- PAINT ROAD-SIDE LANE LINE OF THE BUFFERED BIKE LANE OVER EXISTING FOG LINE
- THERMOPLASTIC CROSSWALK PER C.O.K. STD PLAN CK-R.28A, CENTERED ON RAMP OPENINGS

LEGEND

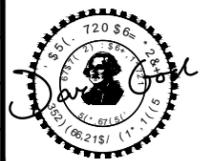
- EXISTING SIGN
- PROPOSED SIGN
- SIGN SCHEDULE NOTE



SIGN SCHEDULE					
SIGN NO.	STATION	OFFSET	DESIGNATION	SIZE	REMARKS
12	24+51.5	24.2' LT	W11-8	30X30	REPLACE
13	25+22.3	23.0' LT	R10-7	24X30	REPLACE
14	25+59.1	32.3' LT	R1-1	30X30	RELOCATE/REPLACE
15	28+00.0	25.2' RT	W11-2, W16-9P	30X30 24X12	NEW
16	28+66.4	28.8' LT	W3-3	30X30	REPLACE
17	29+48.3	30.1' LT	R1-1, STREET NAME SIGNS	30X30 VARIES	RELOCATE/REPLACE
18	31+50.0	21.0' LT	W11-2, W16-9P	30X30 24X12	NEW



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BID DOCUMENT

Tacoma
 -HIIHVVRQ \$YHQXH
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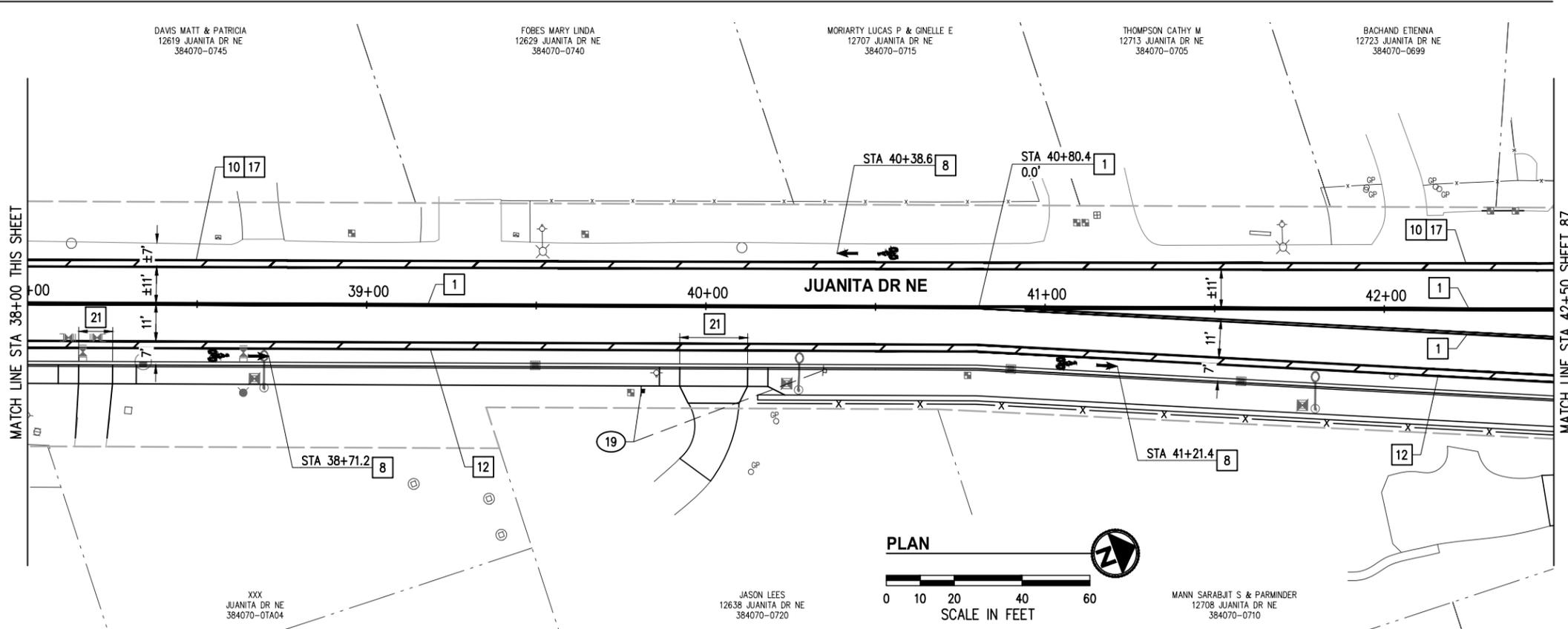
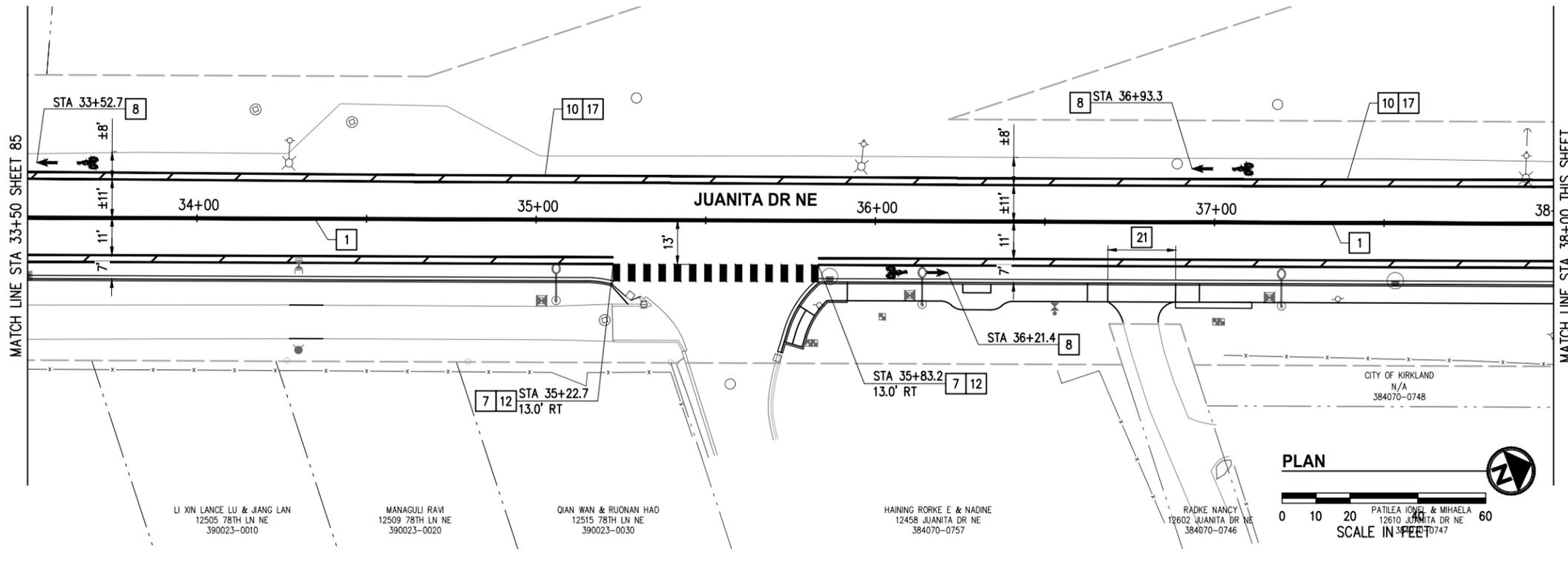
JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

67\$ 72 67\$

CHANNELIZATION & SIGNING PLAN

REFERENCE SHEET NO.
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SHEETS

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GENERAL NOTES

1. ALL EXISTING STRIPING, MARKINGS, AND SIGNS IN CONFLICT WITH THE IMPROVEMENTS SHALL BE REMOVED.
2. CONTRACTOR SHALL PRESERVE AND PROTECT ALL SIGNS NOT SPECIFICALLY CALLED OUT FOR REMOVAL OR RELOCATION, UNLESS DIRECTED OTHERWISE.
3. ALL SYMBOLS SHALL BE CENTERED IN LANE.
4. ALL MARKING SHALL BE 4' CLEAR FROM CROSSWALKS.
5. ALL SIGNAGE AND MARKINGS SHALL CONFORM TO THE LATEST EDITION OF THE MUTCD.
6. SIGNS LISTED IN SIGN SCHEDULE NOTED TO BE NEW, RELOCATED, OR REPLACED SHALL BE ALL NEW AND INSTALLED PER C.O.K. STD PLAN CK-R.43, UNLESS DIRECTED/NOTED OTHERWISE.
7. STREET NAME SIGNS SHALL BE INSTALLED PER C.O.K. STD PLAN CK-R.44.
8. CALLOUTS ARE TO CENTER OF LINE UNLESS OTHERWISE NOTED.

CONSTRUCTION NOTES

- 1 PAINTED DOUBLE YELLOW CENTERLINE PER C.O.K. STD PLAN CK-R.31
- 3 PAINTED 6" WHITE EDGE LINE
- 7 THERMOPLASTIC BIKE LANE CONFLICT ZONE MARKINGS PER C.O.K. STD PLAN CK-R.36A AND CK-R.36C
- 8 THERMOPLASTIC BICYCLE LANE MARKINGS PER C.O.K. STD PLAN CK-R.34
- 10 PAINTED 6" WHITE BUFFERED BIKE LANE LINES AND PAINTED 4" WHITE HATCH LINES PER C.O.K. STD PLAN CK-R.35A. CALLOUT IS TO CENTER OF BIKE-SIDE LINE
- 12 PROFILED THERMOPLASTIC 6" WHITE ROAD-SIDE LANE LINE PER WSDOT STD PLAN M-20.20. PAINTED 6" WHITE BIKE-SIDE BUFFERED BIKE LANE LINE AND PAINTED 4" WHITE HATCH LINES PER C.O.K. STD PLAN CK-R.35A. CALLOUT IS TO CENTER OF BIKE-SIDE LINE
- 17 PAINT ROAD-SIDE LANE LINE OF THE BUFFERED BIKE LANE OVER EXISTING FOG LINE
- 21 NON-PROFILED THERMOPLASTIC 6" WHITE ROAD-SIDE LANE LINE ACROSS DRIVEWAY OPENINGS. DRIVEWAY WIDTH AS NOTED IN ROADWAY PLANS SEE SHEETS 33-46

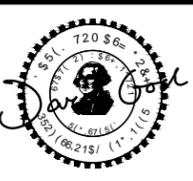
SIGN SCHEDULE					
SIGN NO.	STATION	OFFSET	DESIGNATION	SIZE	REMARKS
19	39+81.0	25.0' RT	W2-2, W16-8	30X30, VARIES	RELOCATE/REPLACE

LEGEND

- (X) EXISTING SIGN
- ▮ PROPOSED SIGN
- Ⓢ SIGN SCHEDULE NOTE



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KPG PSOMAS

BID DOCUMENT

Tacoma
 -HIHUVRO \$YHQXH
 7DFRPD :\$
 6HDWVOH :HCKPQZ.6mHH

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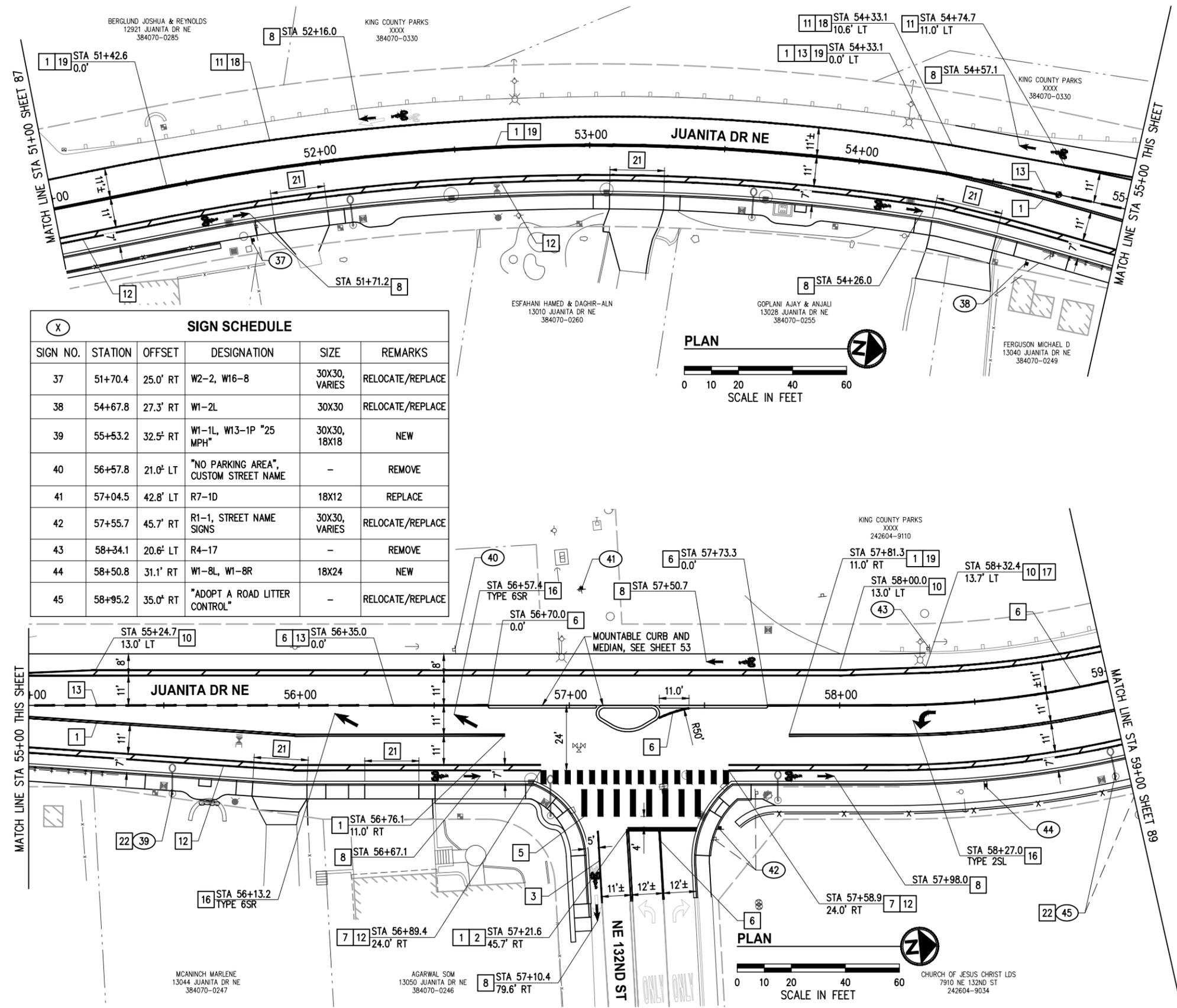
**JUANITA DRIVE MULTIMODAL,
 INTERSECTION & SAFETY IMPROVEMENTS**

67\$ 72 67\$

CHANNELIZATION & SIGNING PLAN

REFERENCE SHEET NO.	
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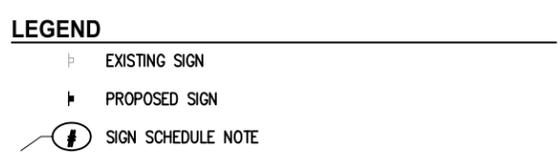


SIGN SCHEDULE

SIGN NO.	STATION	OFFSET	DESIGNATION	SIZE	REMARKS
37	51+70.4	25.0' RT	W2-2, W16-8	30X30, VARIES	RELOCATE/REPLACE
38	54+67.8	27.3' RT	W1-2L	30X30	RELOCATE/REPLACE
39	55+53.2	32.5' RT	W1-1L, W13-1P "25 MPH"	30X30, 18X18	NEW
40	56+57.8	21.0' LT	"NO PARKING AREA", CUSTOM STREET NAME	-	REMOVE
41	57+04.5	42.8' LT	R7-1D	18X12	REPLACE
42	57+55.7	45.7' RT	R1-1, STREET NAME SIGNS	30X30, VARIES	RELOCATE/REPLACE
43	58+34.1	20.6' LT	R4-17	-	REMOVE
44	58+50.8	31.1' RT	W1-8L, W1-8R	18X24	NEW
45	58+95.2	35.0' RT	"ADOPT A ROAD LITTER CONTROL"	-	RELOCATE/REPLACE

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- PAINTED DOUBLE YELLOW CENTERLINE PER C.O.K. STD PLAN CK-R.31
 - THERMOPLASTIC STOP BAR PER C.O.K. STD PLAN CK-R.28
 - PAINTED 6" WHITE EDGE LINE
 - THERMOPLASTIC CROSSWALK PER C.O.K. STD PLAN CK-R.28, CENTERED ON RAMP OPENINGS
 - PAINTED 8" WHITE GORE STRIPE PER C.O.K. STD PLAN CK-R.31
 - THERMOPLASTIC BIKE LANE CONFLICT ZONE MARKINGS PER C.O.K. STD PLAN CK-R.36A AND CK-R.36C
 - THERMOPLASTIC BICYCLE LANE MARKINGS PER C.O.K. STD PLAN CK-R.34
 - PAINTED 6" WHITE BUFFERED BIKE LANE LINES AND PAINTED 4" WHITE HATCH LINES PER C.O.K. STD PLAN CK-R.35A. CALLOUT IS TO CENTER OF BIKE-SIDE LINE
 - PAINTED 6" WHITE EDGE LINE FOR BIKE LANE PER C.O.K. STD PLAN CK-R.35
 - PROFILED THERMOPLASTIC 6" WHITE ROAD-SIDE LANE LINE PER WSDOT STD PLAN M-20.20. PAINTED 6" WHITE BIKE-SIDE BUFFERED BIKE LANE LINE AND PAINTED 4" WHITE HATCH LINES PER C.O.K. STD PLAN CK-R.35A. CALLOUT IS TO CENTER OF BIKE-SIDE LINE
 - PAINTED WHITE DOTTED EXTENSION LINE PER WSDOT STD PLAN M-20.10
 - THERMOPLASTIC TRAFFIC ARROW PER WSDOT STD PLAN M-24.40, TYPE AS NOTED
 - PAINT ROAD-SIDE LANE LINE OF THE BUFFERED BIKE LANE OVER EXISTING FOG LINE
 - PAINT BIKE LANE LINE OVER EXISTING FOG LINE
 - CENTERLINE RUMBLE STRIP PER WSDOT STD PLAN M-65.10-03
 - NON-PROFILED THERMOPLASTIC 6" WHITE ROAD-SIDE LANE LINE ACROSS DRIVEWAY OPENINGS. DRIVEWAY WIDTH AS NOTED IN ROADWAY PLANS SEE SHEETS 33-46
 - INSTALL SIGN ON LUMINAIRE PER WSDOT STD PLAN G-30.10



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Call 811 before you dig.

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BID DOCUMENT

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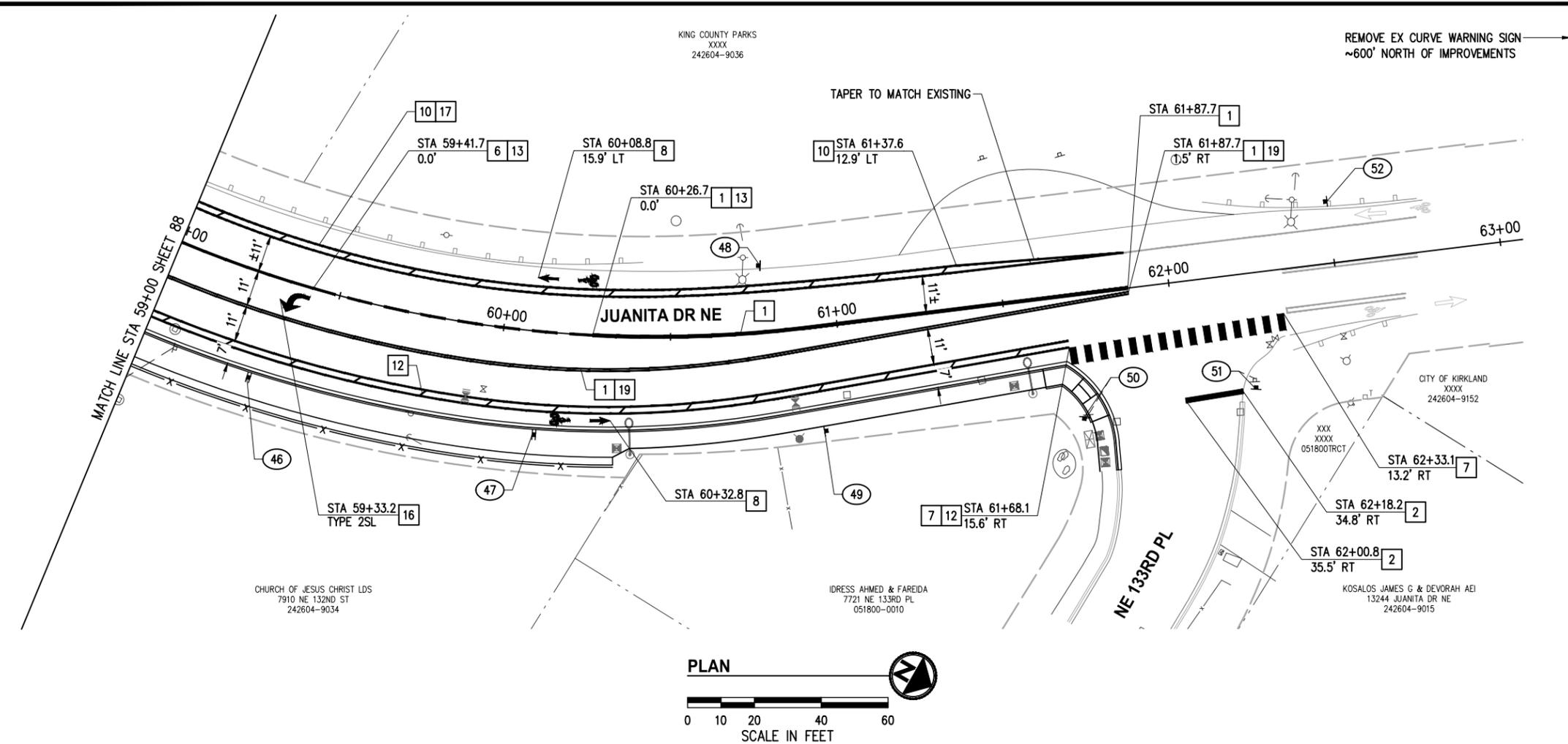
JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

67\$ 72 67\$

CHANNELIZATION & SIGNING PLAN

REFERENCE SHEET NO.
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 - PAINT ROAD-SIDE LANE LINE OF THE BUFFERED BIKE LANE OVER EXISTING FOG LINE
 - CENTERLINE RUMBLE STRIP PER WSDOT STD PLAN M-65.10-03

SIGN SCHEDULE					
SIGN NO.	STATION	OFFSET	DESIGNATION	SIZE	REMARKS
46	59+30.8	31.1' RT	W1-8L, W1-8R	18X24	NEW
47	60+11.4	30.9' RT	W1-8L, W1-8R	18X24	NEW
48	60+78.6	19.9' LT	W2-2L, W16-8P	30X30, VARIES	REPLACE
49	60+92.4	31.3' RT	W1-8L	18X24	NEW
50	61+70.1	36.6' RT	W14-2	30X30	RELOCATE/REPLACE
51	62+22.1	34.7' RT	R1-1, STREET NAME SIGNS	30X30, VARIES	RELOCATE/REPLACE
52	62+50.0	18.4' LT	W1-1R, W13-1P *25MPH*	30X30, 18X18	NEW

- ### LEGEND
- EXISTING SIGN
 - PROPOSED SIGN
 - SIGN SCHEDULE NOTE



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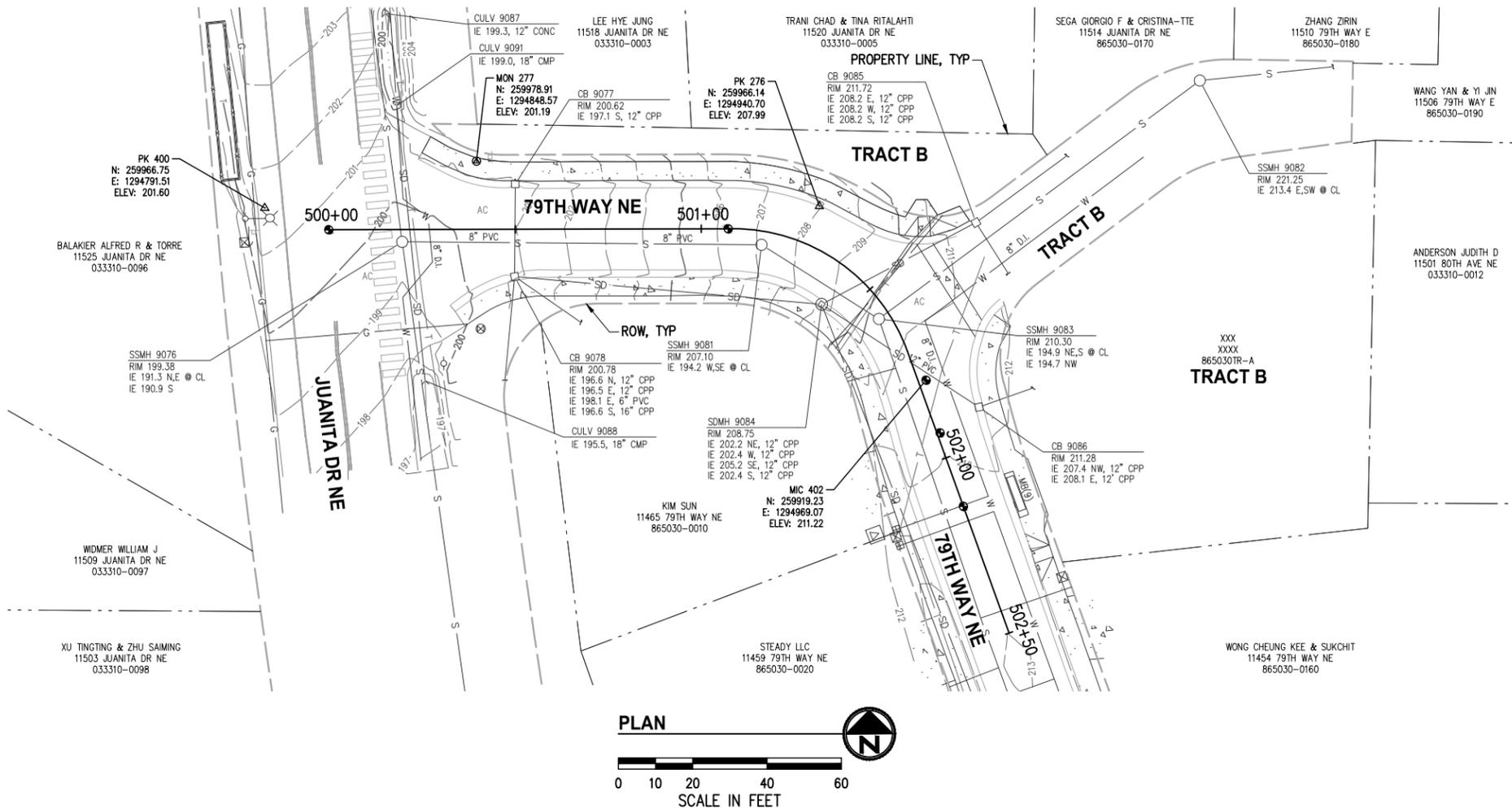


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JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS
 67\$ 72 67\$
CHANNELIZATION & SIGNING PLAN
 REFERENCE SHEET NO. SHEET 2) SHEETS

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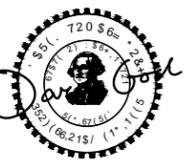
NOTES

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3. FIELD SURVEY: KPG, OCTOBER, NOVEMBER, DECEMBER 2019. LICENSEE MICHAEL R. BOWEN, P.L.S. NO. 29294/RONALD D. REICHEL, P.L.S. NO. 38015.
4. CONTOUR INTERVAL = 1 FOOT, ±0.5 FOOT PER NATIONAL MAPPING STANDARDS. CONTOURS DERIVED FROM DIRECT FIELD OBSERVATIONS.
5. STORM AND SEWER CONNECTIONS HAVE BEEN DRAWN FROM CENTER OF LID TO CENTER OF LID.
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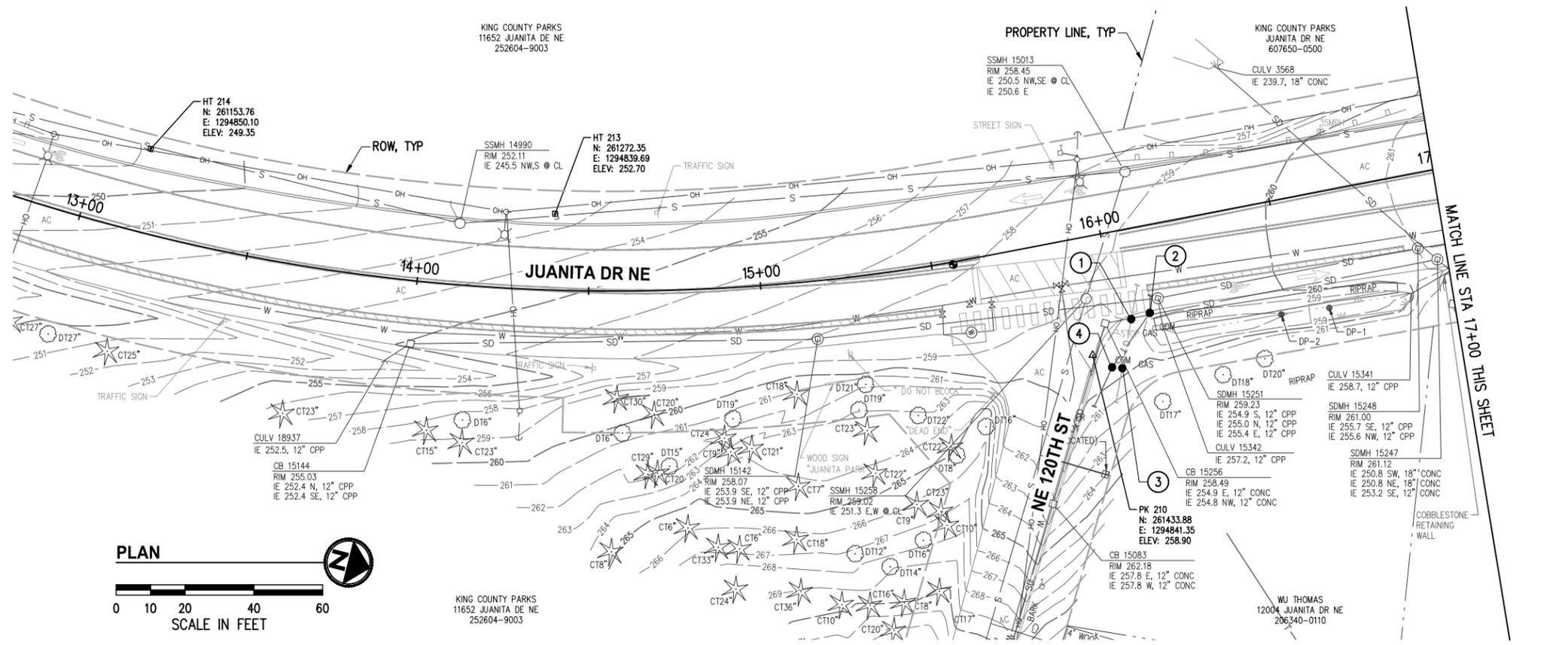
**JUANITA DRIVE MULTIMODAL,
INTERSECTION & SAFETY IMPROVEMENTS**

67\$ 72 67\$

EXISTING CONDITIONS & POTHOLE PLAN

REFERENCE SHEET NO.
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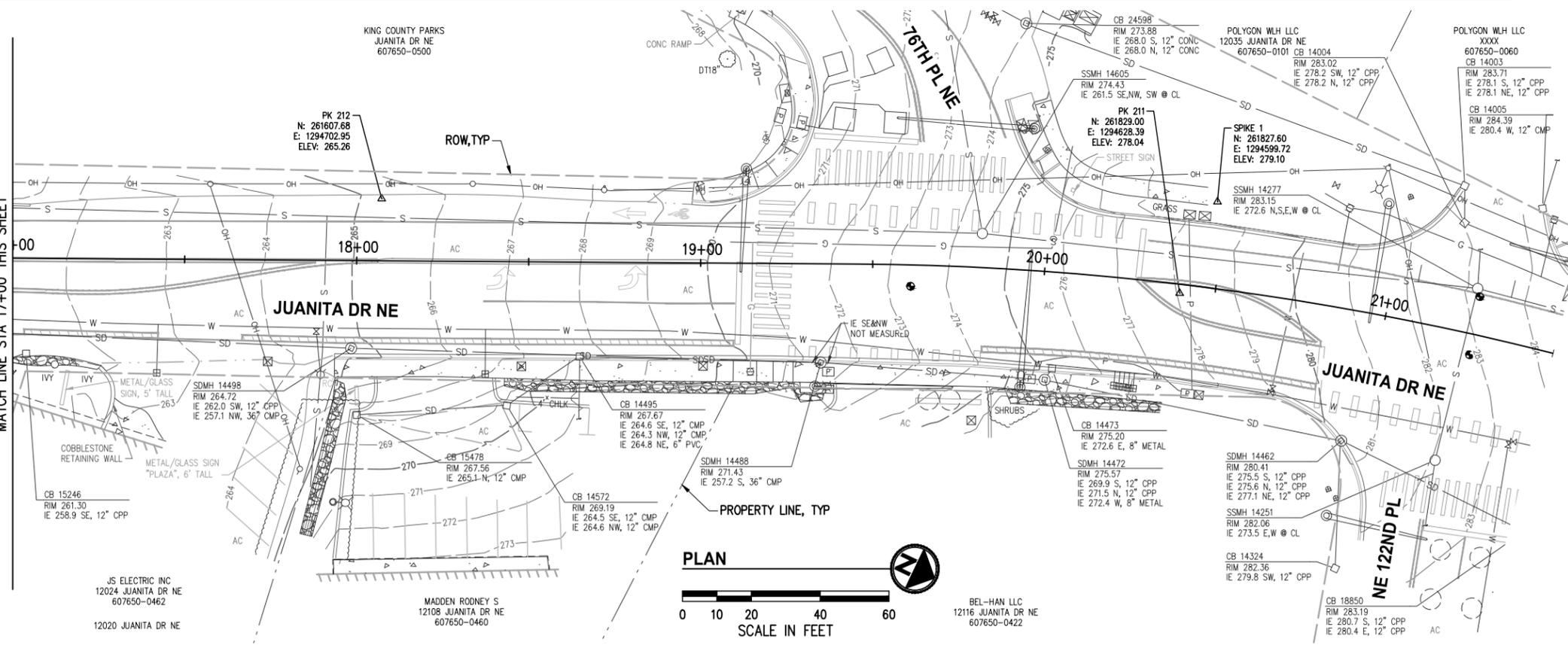
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POTHOLES						
#	STATION	OFFSET	DESCRIPTION	TOP UTIL DEPTH (IN)	BOT UTIL DEPTH (IN)	ASPHALT DEPTH (IN)
1	16+04.1	25.9' RT	PSE GAS 2" STW	36.0	38.0	N/A
2	16+09.8	25.2' RT	ZIPLY 1" DB	32.0	34.0	N/A
3	15+98.9	39.4' RT	PSE GAS 2" STW	46.0	48.0	N/A
4	15+96.0	38.6' RT	ZIPLY 1" DB	16.0	17.0	N/A

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KPG PSOMAS

BID DOCUMENT

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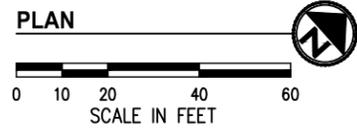
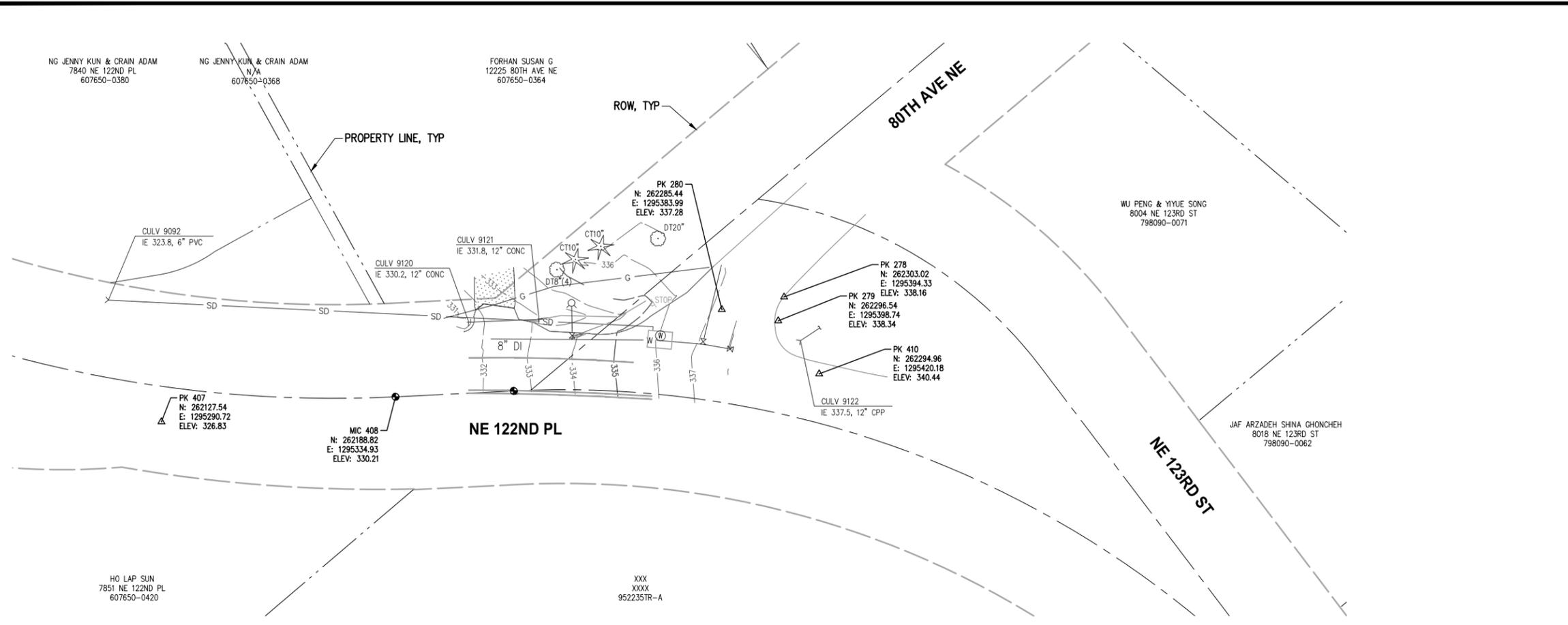
JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

67\$ 72 67\$

EXISTING CONDITIONS & POT HOLE PLAN

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SHEET 2)
SHEETS

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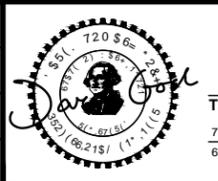


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KPG PSOMAS

BID DOCUMENT

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CITY OF KIRKLAND WASHINGTON

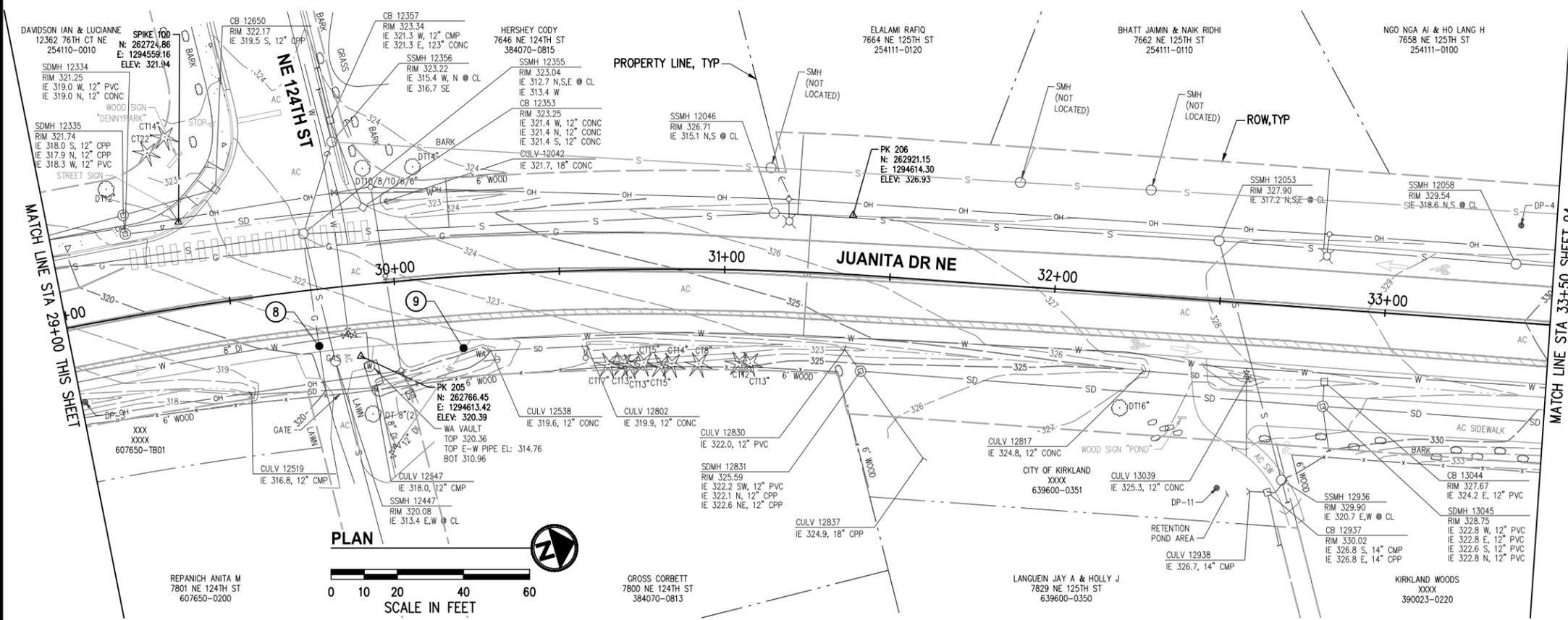
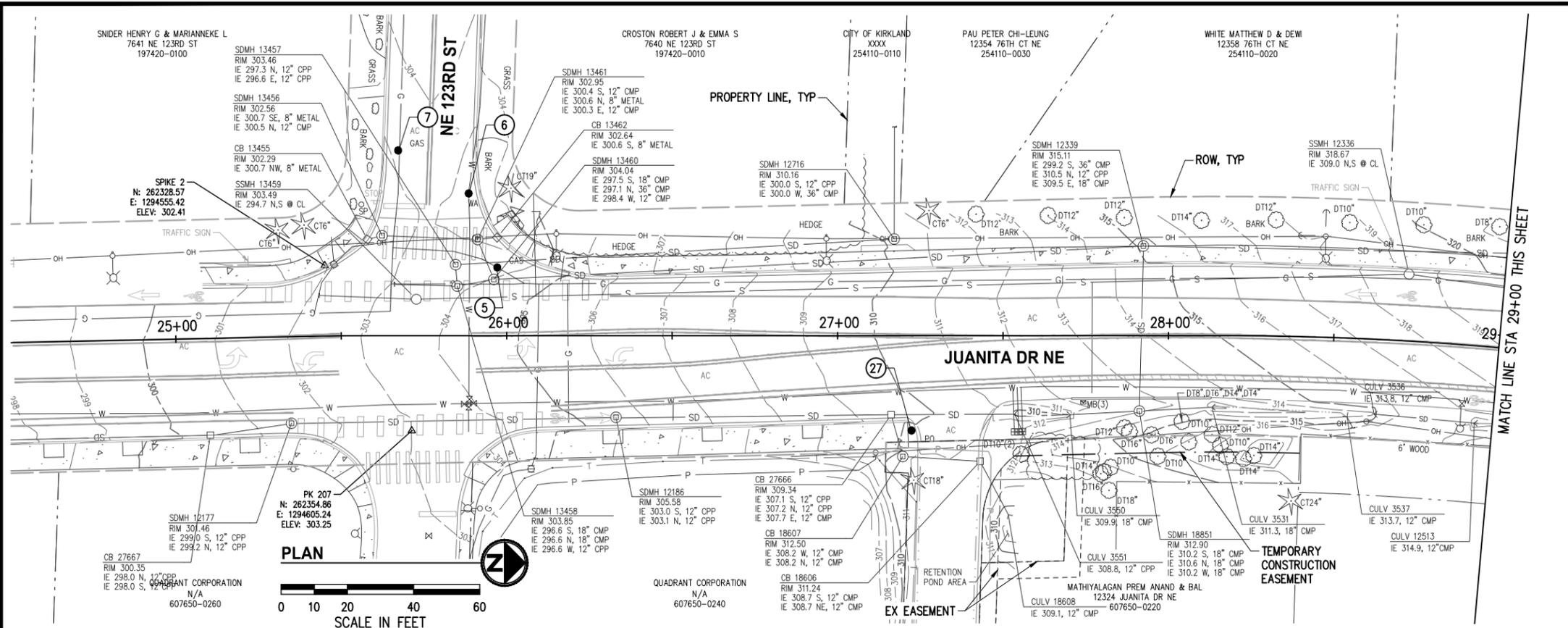
CITY OF KIRKLAND
DEPARTMENT OF PUBLIC WORKS
123 FIFTH AVENUE KIRKLAND, WA 98033
(425) 587-3800 www.kirklandwa.gov

JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

1(1' 3/ 7+ \$9(1(**EXISTING CONDITIONS & POTHOLE PLAN**

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- NOTES**
- THE PURPOSE OF THIS TOPOGRAPHIC SURVEY IS FOR CIVIL ENGINEERING DESIGN. THIS IS NOT A BOUNDARY SURVEY. SOURCES OF BOUNDARY INFORMATION AS SHOWN INCLUDE FIELD-TIED MONUMENTATION, PLATS, COUNTY RECORDS OF SURVEY, AND AUDITOR INDEXING INFORMATION.
 - THE LOCATIONS OF EXISTING UNDERGROUND UTILITY SYSTEMS, AS SHOWN HEREON, ARE TAKEN FROM UTILITY LOCATE PAINT MARKS OR AS-BUILT PLANS AND ARE SHOWN IN AN APPROXIMATE WAY ONLY.

THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK, AND AGREES TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE OCCASIONED BY THE CONTRACTOR'S FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES. ALL LOCATOR SERVICES SHOULD BE CONTACTED PRIOR TO ANY CONSTRUCTION OR SUBSURFACE EXPLORATION. CALL 1-800-424-5555.
 - FIELD SURVEY: KPG, OCTOBER, NOVEMBER, DECEMBER 2019. LICENSEE MICHAEL R. BOWEN, P.L.S. NO. 29294/RONALD D. REICHEL, P.L.S. NO. 38015.
 - CONTOUR INTERVAL = 1 FOOT, ±0.5 FOOT PER NATIONAL MAPPING STANDARDS. CONTOURS DERIVED FROM DIRECT FIELD OBSERVATIONS.
 - STORM AND SEWER CONNECTIONS HAVE BEEN DRAWN FROM CENTER OF LID TO CENTER OF LID.
 - THE LOCATIONS AND DIMENSIONS OF UNDERGROUND VAULTS HAVE NOT BEEN VERIFIED AND ARE APPROXIMATE.

POTHOLES						
#	STATION	OFFSET	DESCRIPTION	TOP UTIL DEPTH (IN)	BOT UTIL DEPTH (IN)	ASPHALT DEPTH (IN)
5	25+97.2	20.5' LT	PSE GAS 5/8" GAS	22.0	24.0	N/A
6	25+88.5	42.8' LT	NUD WATER 8" DI	35.0	43.0	N/A
7	25+67.2	55.8' LT	PSE GAS 2" STW	31.0	32.0	8"
8	29+75.2	17.1' RT	PSE GAS 2" STW	37.0	39.0	10"
9	30+19.5	21.7' RT	NUD WATER	33.0	-	8"
27	27+22.3	28.6' RT	PSE POWER (2)-2" PVC, (1)-4" PVC 8" WIDE DUCT	36.0	40.0	N/A

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KPG PSOMAS

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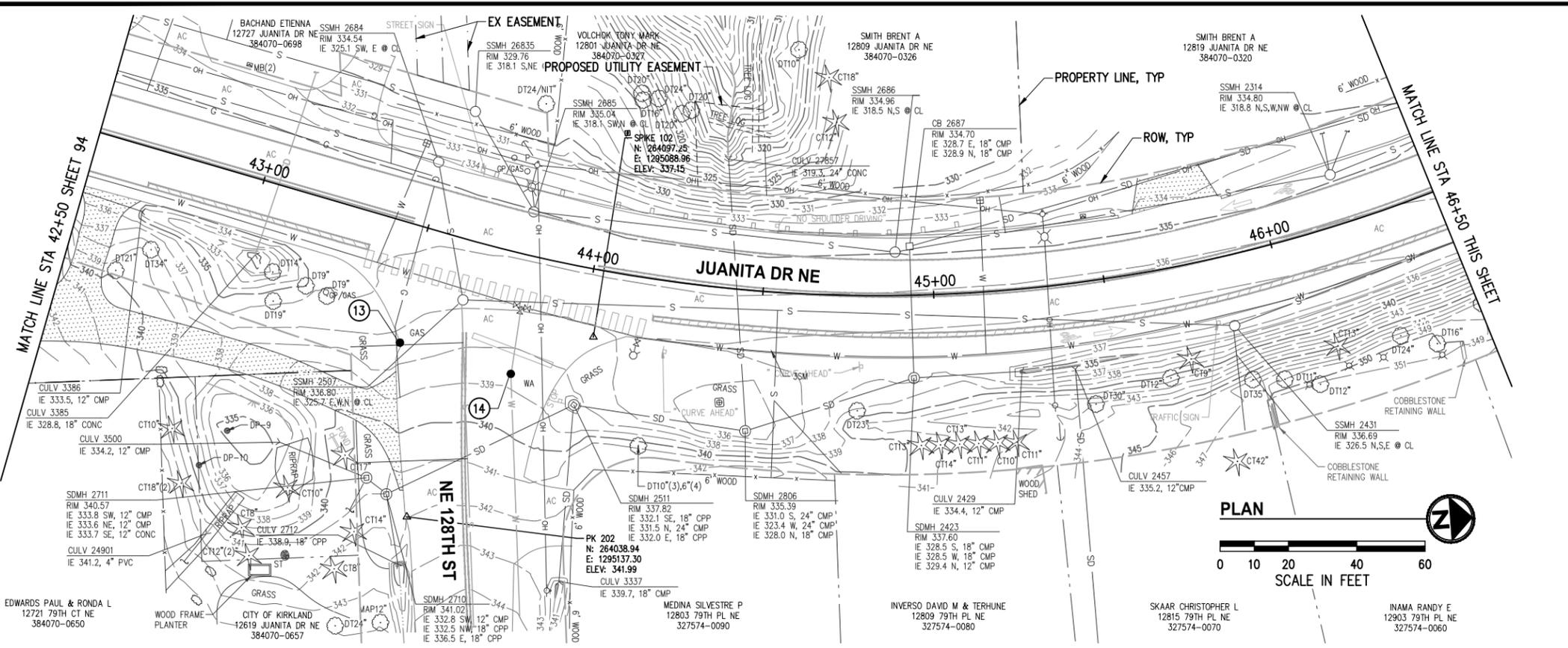
JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

67\$ 72 67\$

EXISTING CONDITIONS & POT HOLE PLAN

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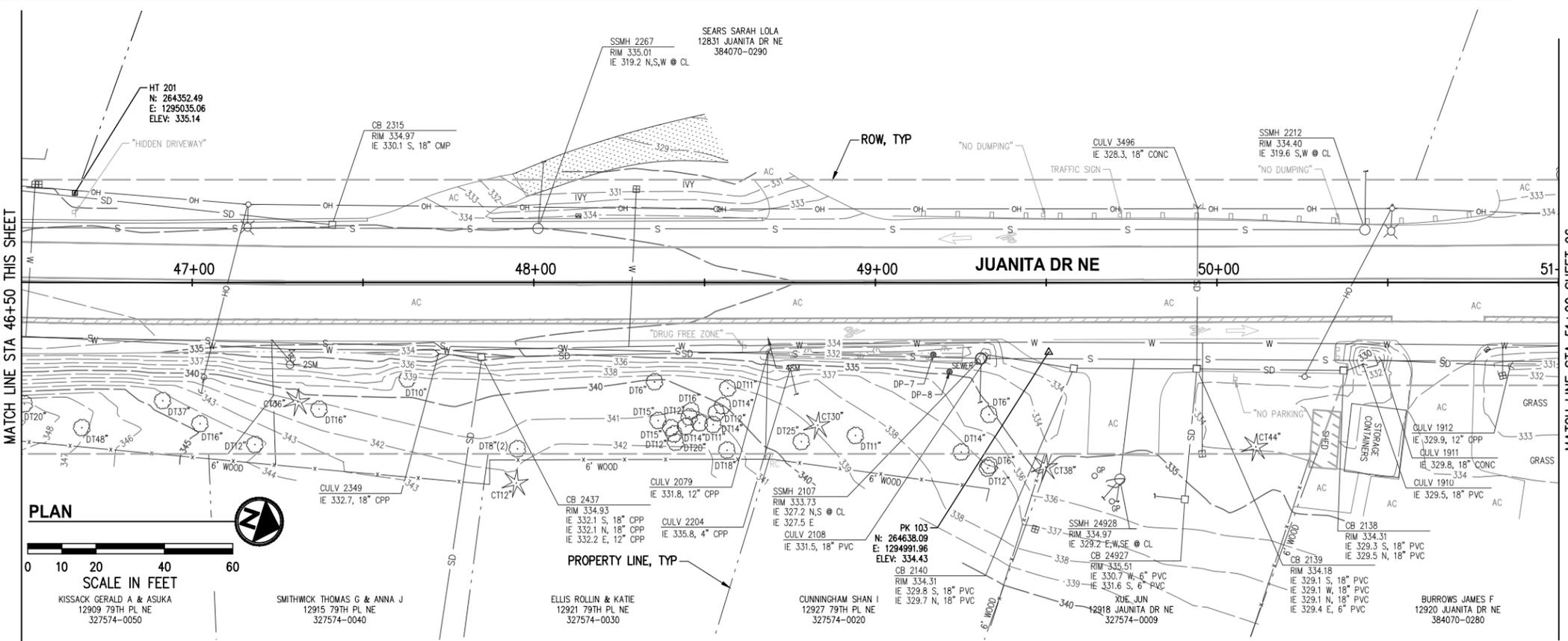
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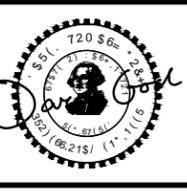


POTHoles						
#	STATION	OFFSET	DESCRIPTION	TOP UTIL DEPTH (IN)	BOT UTIL DEPTH (IN)	ASPHALT DEPTH (IN)
13	43+51.4	34.4' RT	PSE GAS 2" PE	36.0	38.0	N/A
14	43+83.1	34.9' RT	NUD WATER 8" PVC	40.0	48.0	6"

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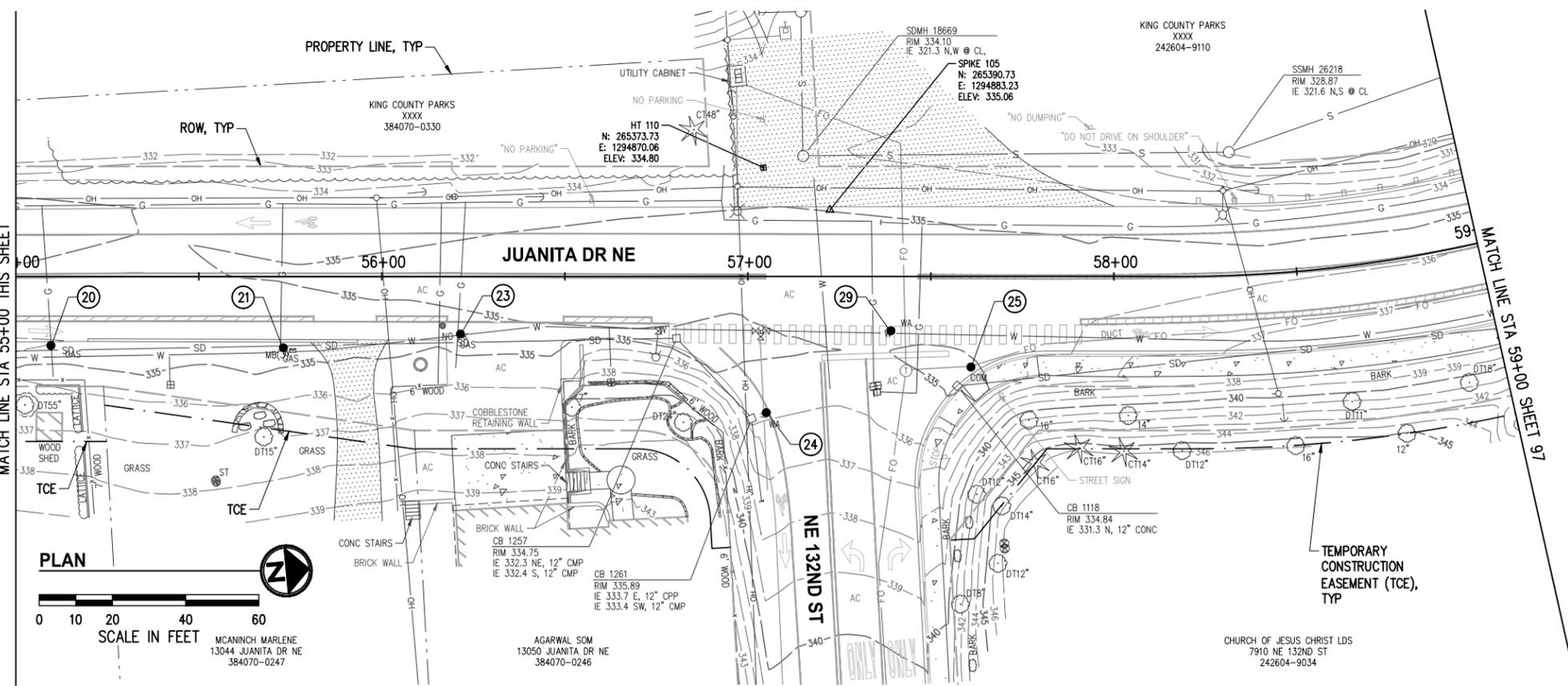
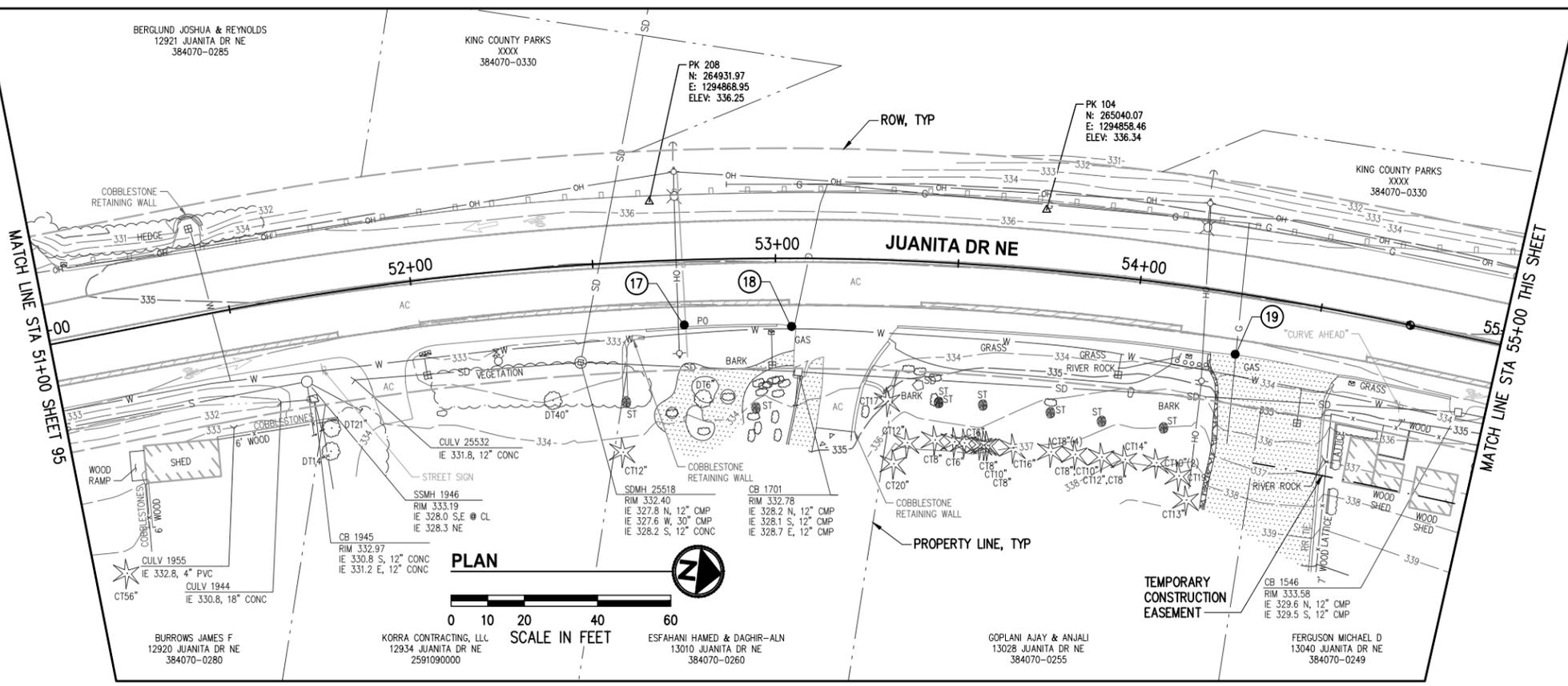
JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

67\$ 72 67\$

EXISTING CONDITIONS & POTHOLE PLAN

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POTHOLES						
#	STATION	OFFSET	DESCRIPTION	TOP UTIL DEPTH (IN)	BOT UTIL DEPTH (IN)	ASPHALT DEPTH (IN)
17	52+74.5	17.8' RT	PSE POWER 3" PVC	60.0	63.0	N/A
18	53+04.5	18.6' RT	PSE GAS 5/8" PE	31.0	32.0	N/A
19	54+28.7	16.5' RT	PSE GAS 5/8" PE	34.0	35.0	N/A
20	55+09.6	18.8' RT	PSE GAS 5/8" PE	17.0	18.0	N/A
21	55+73.1	19.4' RT	PSE GAS 5/8" PE	29.0	30.0	N/A
23	56+21.5	15.7' RT	PSE GAS 5/8" PE	32.0	34.0	6"
24	57+05.0	37.1' RT	NUD WATER 12" DI	36.0	48.0	6"
25	57+60.9	24.7' RT	ZIPLY FIBER DUCT	43.0	53.0	6"
29	57+39.1	14.8' RT	NUD WATER 12" DI	50.0	62.0	8"

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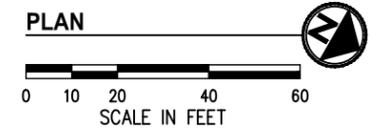
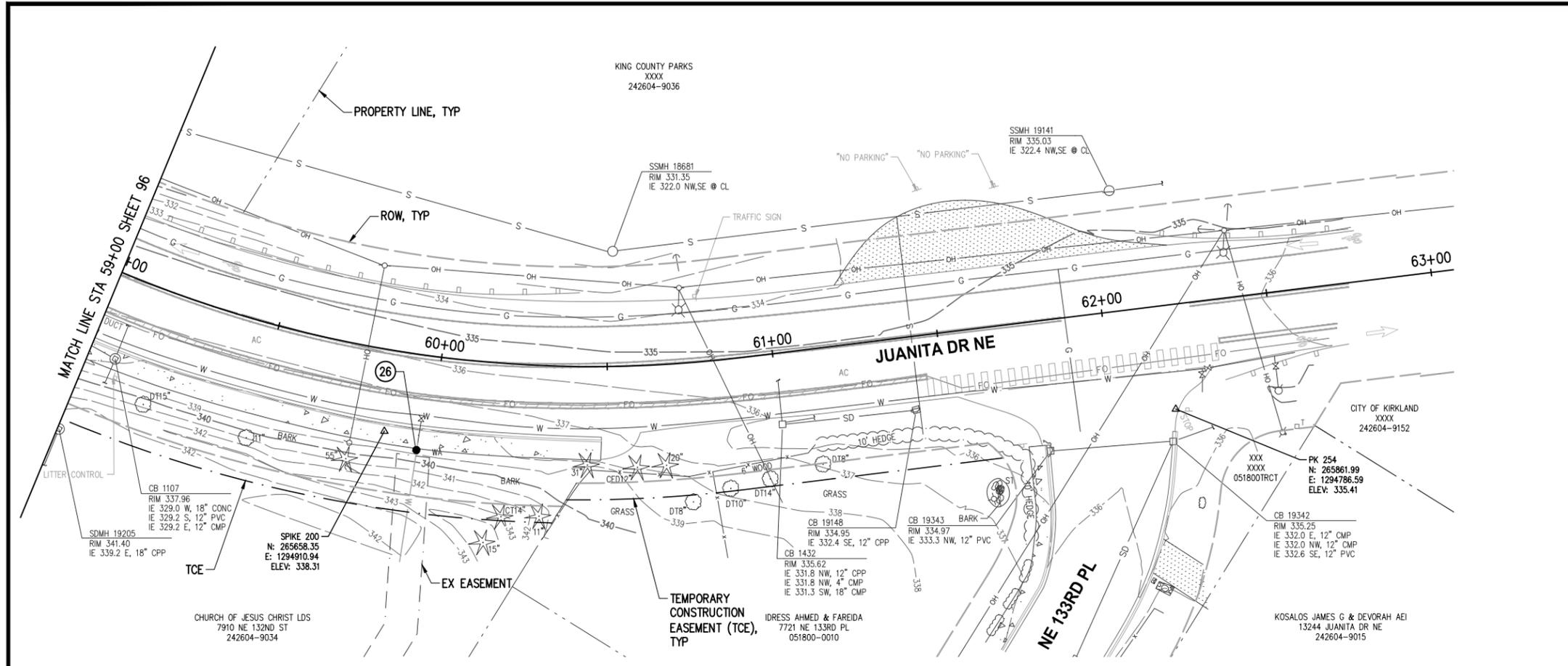
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67\$ 72 67\$

EXISTING CONDITIONS & POT HOLE PLAN

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POTHOLES						
#	STATION	OFFSET	DESCRIPTION	TOP UTIL DEPTH (IN)	BOT UTIL DEPTH (IN)	ASPHALT DEPTH (IN)
26	59+96.1	28.6' RT	NUD WATER 8" DI	48.0	56.0	N/A

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811
Know what's below.
Call 811 before you dig.

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KPG PSOMAS
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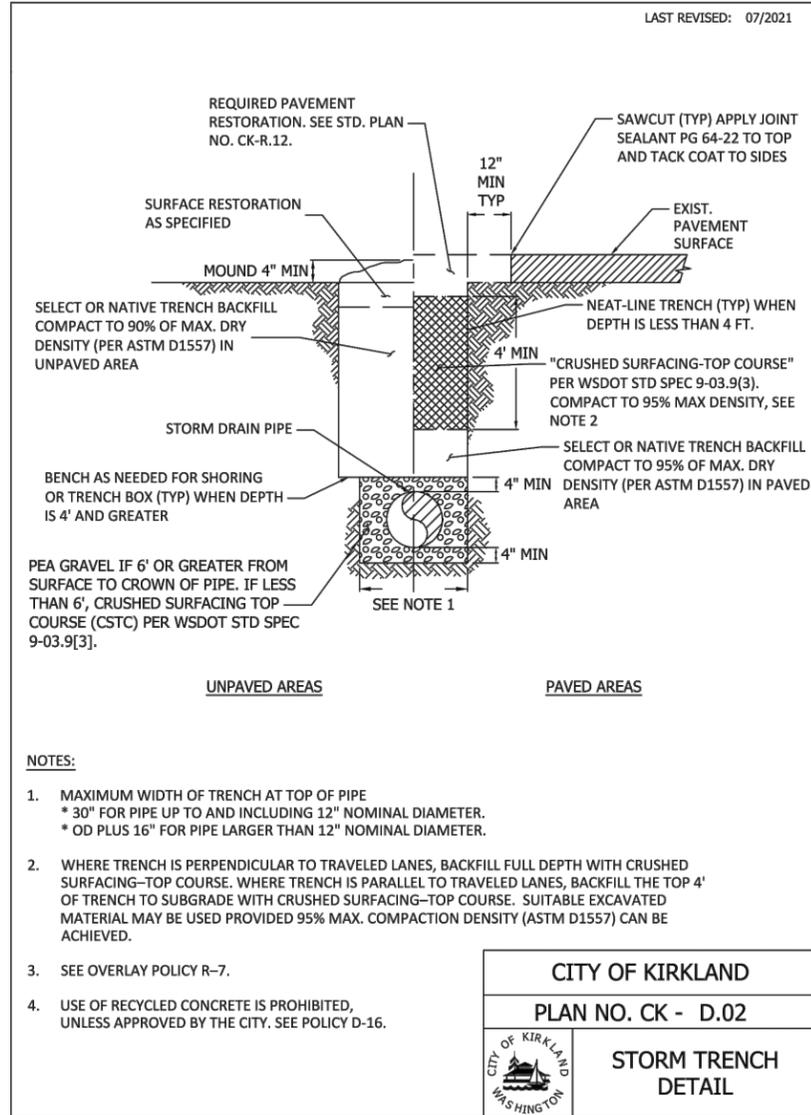
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INTERSECTION & SAFETY IMPROVEMENTS**

67\$ 72 67\$

EXISTING CONDITIONS & POT HOLE PLAN

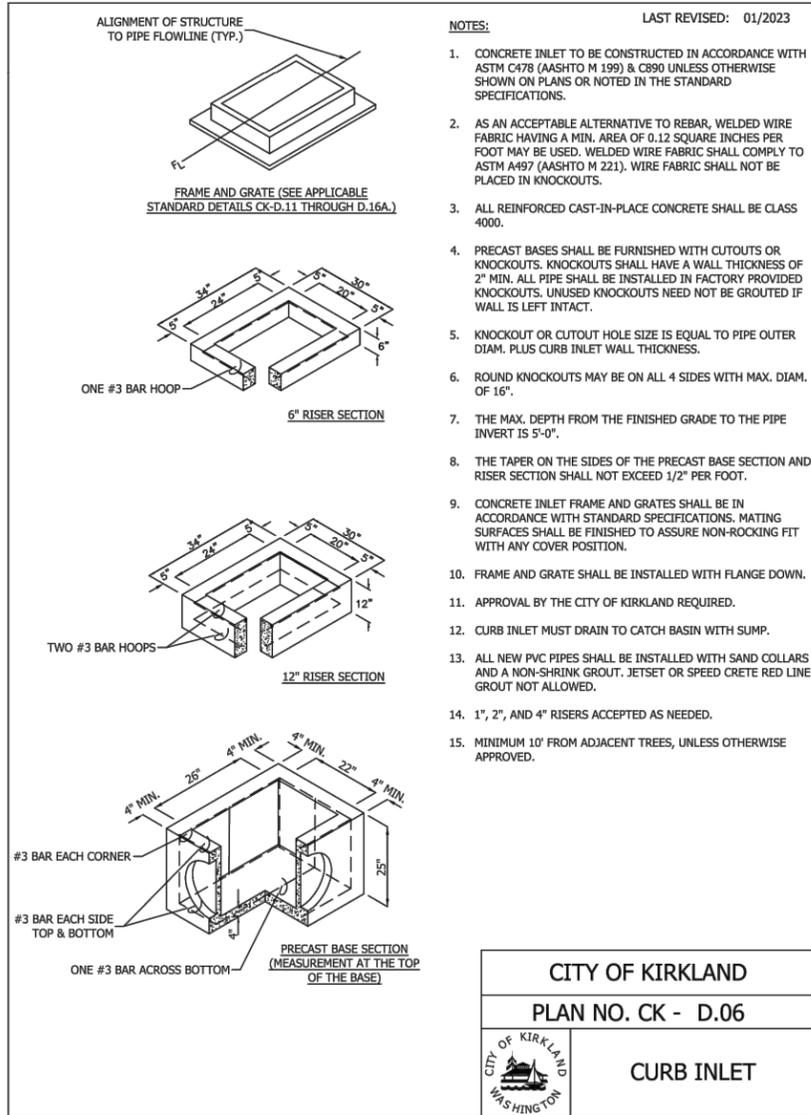
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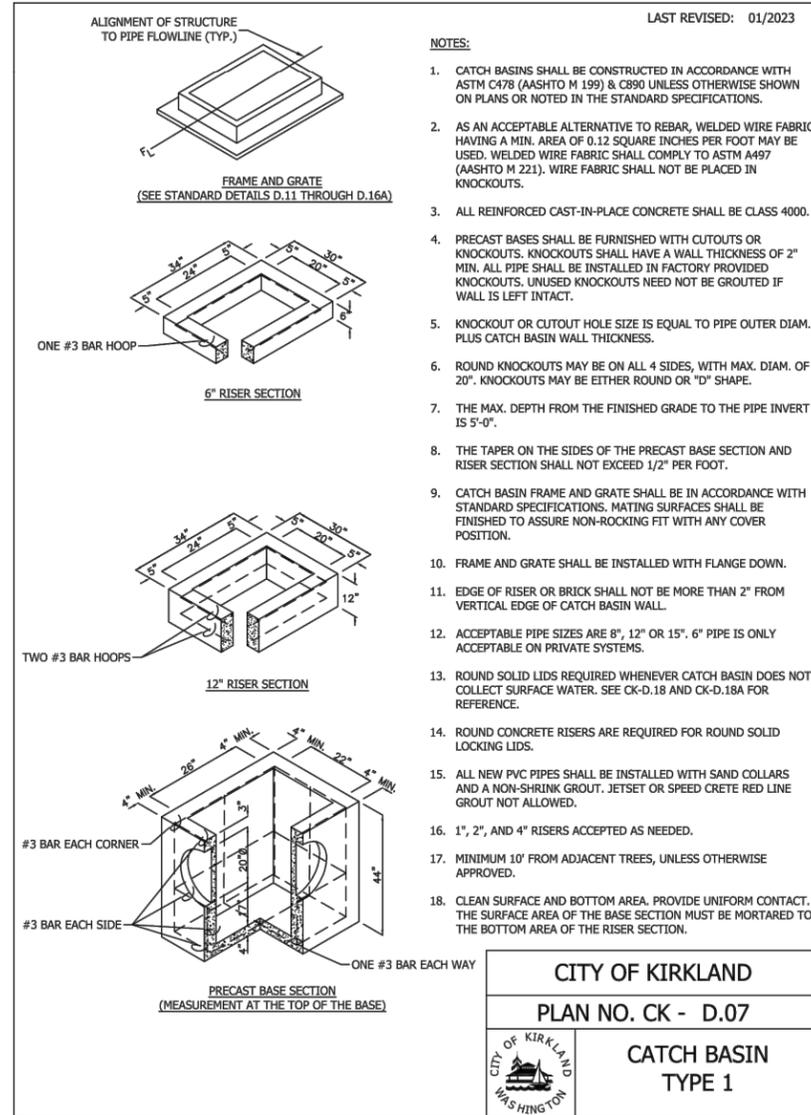
NOTES:
 1. MAXIMUM WIDTH OF TRENCH AT TOP OF PIPE
 * 30" FOR PIPE UP TO AND INCLUDING 12" NOMINAL DIAMETER.
 * OD PLUS 16" FOR PIPE LARGER THAN 12" NOMINAL DIAMETER.
 2. WHERE TRENCH IS PERPENDICULAR TO TRAVELED LANES, BACKFILL FULL DEPTH WITH CRUSHED SURFACING-TOP COURSE. WHERE TRENCH IS PARALLEL TO TRAVELED LANES, BACKFILL THE TOP 4" OF TRENCH TO SUBGRADE WITH CRUSHED SURFACING-TOP COURSE. SUITABLE EXCAVATED MATERIAL MAY BE USED PROVIDED 95% MAX. COMPACTION DENSITY (ASTM D1557) CAN BE ACHIEVED.
 3. SEE OVERLAY POLICY R-7.
 4. USE OF RECYCLED CONCRETE IS PROHIBITED, UNLESS APPROVED BY THE CITY. SEE POLICY D-16.

CITY OF KIRKLAND	
PLAN NO. CK - D.02	
CITY OF KIRKLAND WASHINGTON	STORM TRENCH DETAIL



NOTES:
 1. CONCRETE INLET TO BE CONSTRUCTED IN ACCORDANCE WITH ASTM C478 (AASHTO M 199) & C890 UNLESS OTHERWISE SHOWN ON PLANS OR NOTED IN THE STANDARD SPECIFICATIONS.
 2. AS AN ACCEPTABLE ALTERNATIVE TO REBAR, WELDED WIRE FABRIC HAVING A MIN. AREA OF 0.12 SQUARE INCHES PER FOOT MAY BE USED. WELDED WIRE FABRIC SHALL COMPLY TO ASTM A497 (AASHTO M 221). WIRE FABRIC SHALL NOT BE PLACED IN KNOCKOUTS.
 3. ALL REINFORCED CAST-IN-PLACE CONCRETE SHALL BE CLASS 4000.
 4. PRECAST BASES SHALL BE FURNISHED WITH CUTOUTS OR KNOCKOUTS. KNOCKOUTS SHALL HAVE A WALL THICKNESS OF 2" MIN. ALL PIPE SHALL BE INSTALLED IN FACTORY PROVIDED KNOCKOUTS. UNUSED KNOCKOUTS NEED NOT BE GROUTED IF WALL IS LEFT INTACT.
 5. KNOCKOUT OR CUTOUT HOLE SIZE IS EQUAL TO PIPE OUTER DIAM. PLUS CURB INLET WALL THICKNESS.
 6. ROUND KNOCKOUTS MAY BE ON ALL 4 SIDES WITH MAX. DIAM. OF 16".
 7. THE MAX. DEPTH FROM THE FINISHED GRADE TO THE PIPE INVERT IS 5'-0".
 8. THE TAPER ON THE SIDES OF THE PRECAST BASE SECTION AND RISER SECTION SHALL NOT EXCEED 1/2" PER FOOT.
 9. CONCRETE INLET FRAME AND GRATES SHALL BE IN ACCORDANCE WITH STANDARD SPECIFICATIONS. MATING SURFACES SHALL BE FINISHED TO ASSURE NON-ROCKING FIT WITH ANY COVER POSITION.
 10. FRAME AND GRATE SHALL BE INSTALLED WITH FLANGE DOWN.
 11. APPROVAL BY THE CITY OF KIRKLAND REQUIRED.
 12. CURB INLET MUST DRAIN TO CATCH BASIN WITH SUMP.
 13. ALL NEW PVC PIPES SHALL BE INSTALLED WITH SAND COLLARS AND A NON-SHRINK GROUT. JETSET OR SPEED CRETE RED LINE GROUT NOT ALLOWED.
 14. 1", 2", AND 4" RISERS ACCEPTED AS NEEDED.
 15. MINIMUM 10' FROM ADJACENT TREES, UNLESS OTHERWISE APPROVED.

CITY OF KIRKLAND	
PLAN NO. CK - D.06	
CITY OF KIRKLAND WASHINGTON	CURB INLET

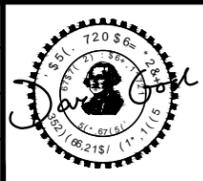


NOTES:
 1. CATCH BASINS SHALL BE CONSTRUCTED IN ACCORDANCE WITH ASTM C478 (AASHTO M 199) & C890 UNLESS OTHERWISE SHOWN ON PLANS OR NOTED IN THE STANDARD SPECIFICATIONS.
 2. AS AN ACCEPTABLE ALTERNATIVE TO REBAR, WELDED WIRE FABRIC HAVING A MIN. AREA OF 0.12 SQUARE INCHES PER FOOT MAY BE USED. WELDED WIRE FABRIC SHALL COMPLY TO ASTM A497 (AASHTO M 221). WIRE FABRIC SHALL NOT BE PLACED IN KNOCKOUTS.
 3. ALL REINFORCED CAST-IN-PLACE CONCRETE SHALL BE CLASS 4000.
 4. PRECAST BASES SHALL BE FURNISHED WITH CUTOUTS OR KNOCKOUTS. KNOCKOUTS SHALL HAVE A WALL THICKNESS OF 2" MIN. ALL PIPE SHALL BE INSTALLED IN FACTORY PROVIDED KNOCKOUTS. UNUSED KNOCKOUTS NEED NOT BE GROUTED IF WALL IS LEFT INTACT.
 5. KNOCKOUT OR CUTOUT HOLE SIZE IS EQUAL TO PIPE OUTER DIAM. PLUS CATCH BASIN WALL THICKNESS.
 6. ROUND KNOCKOUTS MAY BE ON ALL 4 SIDES, WITH MAX. DIAM. OF 20". KNOCKOUTS MAY BE EITHER ROUND OR "D" SHAPE.
 7. THE MAX. DEPTH FROM THE FINISHED GRADE TO THE PIPE INVERT IS 5'-0".
 8. THE TAPER ON THE SIDES OF THE PRECAST BASE SECTION AND RISER SECTION SHALL NOT EXCEED 1/2" PER FOOT.
 9. CATCH BASIN FRAME AND GRATE SHALL BE IN ACCORDANCE WITH STANDARD SPECIFICATIONS. MATING SURFACES SHALL BE FINISHED TO ASSURE NON-ROCKING FIT WITH ANY COVER POSITION.
 10. FRAME AND GRATE SHALL BE INSTALLED WITH FLANGE DOWN.
 11. EDGE OF RISER OR BRICK SHALL NOT BE MORE THAN 2" FROM VERTICAL EDGE OF CATCH BASIN WALL.
 12. ACCEPTABLE PIPE SIZES ARE 8", 12" OR 15". 6" PIPE IS ONLY ACCEPTABLE ON PRIVATE SYSTEMS.
 13. ROUND SOLID LIDS REQUIRED WHENEVER CATCH BASIN DOES NOT COLLECT SURFACE WATER. SEE CK-D.18 AND CK-D.18A FOR REFERENCE.
 14. ROUND CONCRETE RISERS ARE REQUIRED FOR ROUND SOLID LOCKING LIDS.
 15. ALL NEW PVC PIPES SHALL BE INSTALLED WITH SAND COLLARS AND A NON-SHRINK GROUT. JETSET OR SPEED CRETE RED LINE GROUT NOT ALLOWED.
 16. 1", 2", AND 4" RISERS ACCEPTED AS NEEDED.
 17. MINIMUM 10' FROM ADJACENT TREES, UNLESS OTHERWISE APPROVED.
 18. CLEAN SURFACE AND BOTTOM AREA. PROVIDE UNIFORM CONTACT. THE SURFACE AREA OF THE BASE SECTION MUST BE MORTARED TO THE BOTTOM AREA OF THE RISER SECTION.

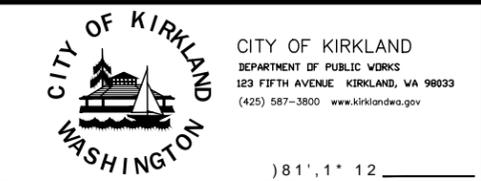
CITY OF KIRKLAND	
PLAN NO. CK - D.07	
CITY OF KIRKLAND WASHINGTON	CATCH BASIN TYPE 1



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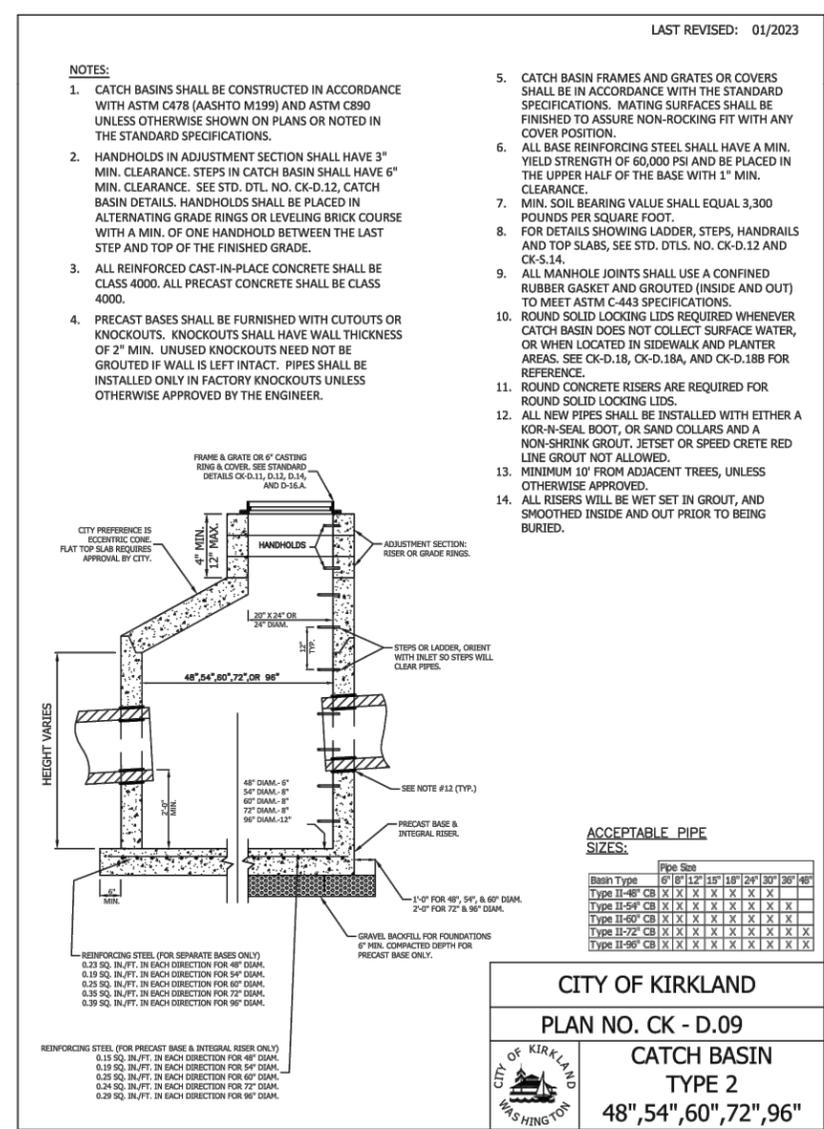
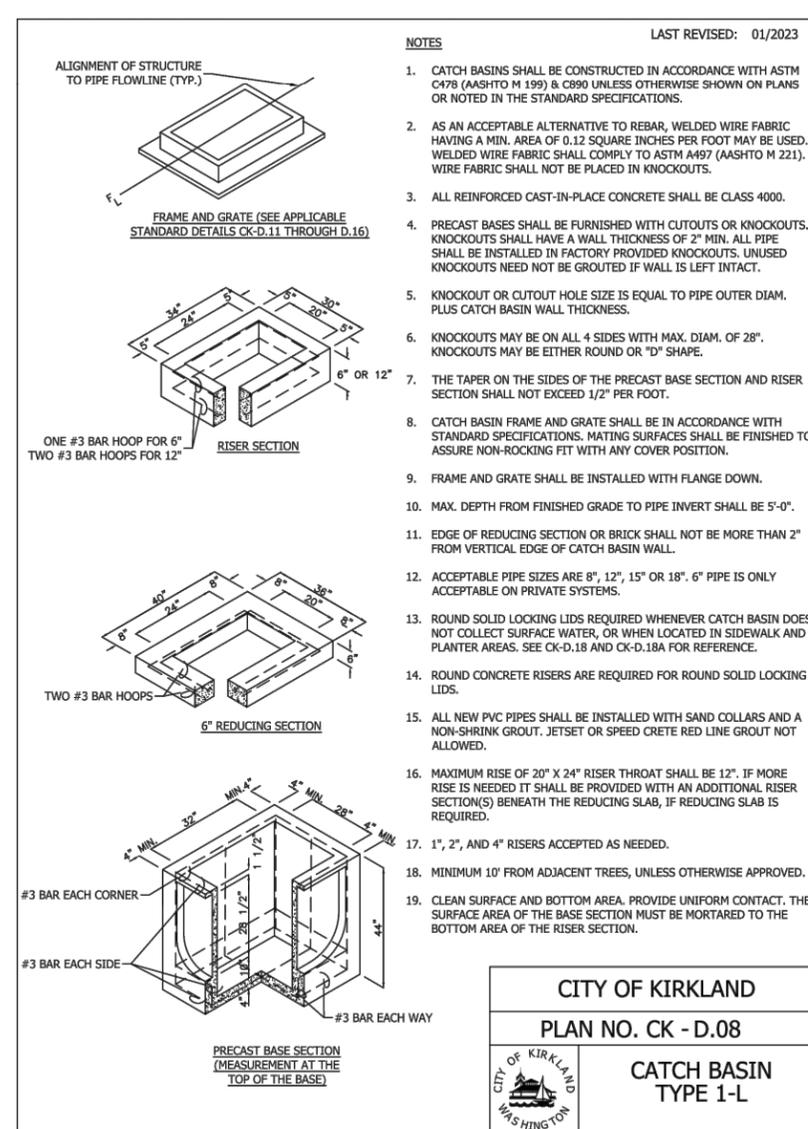
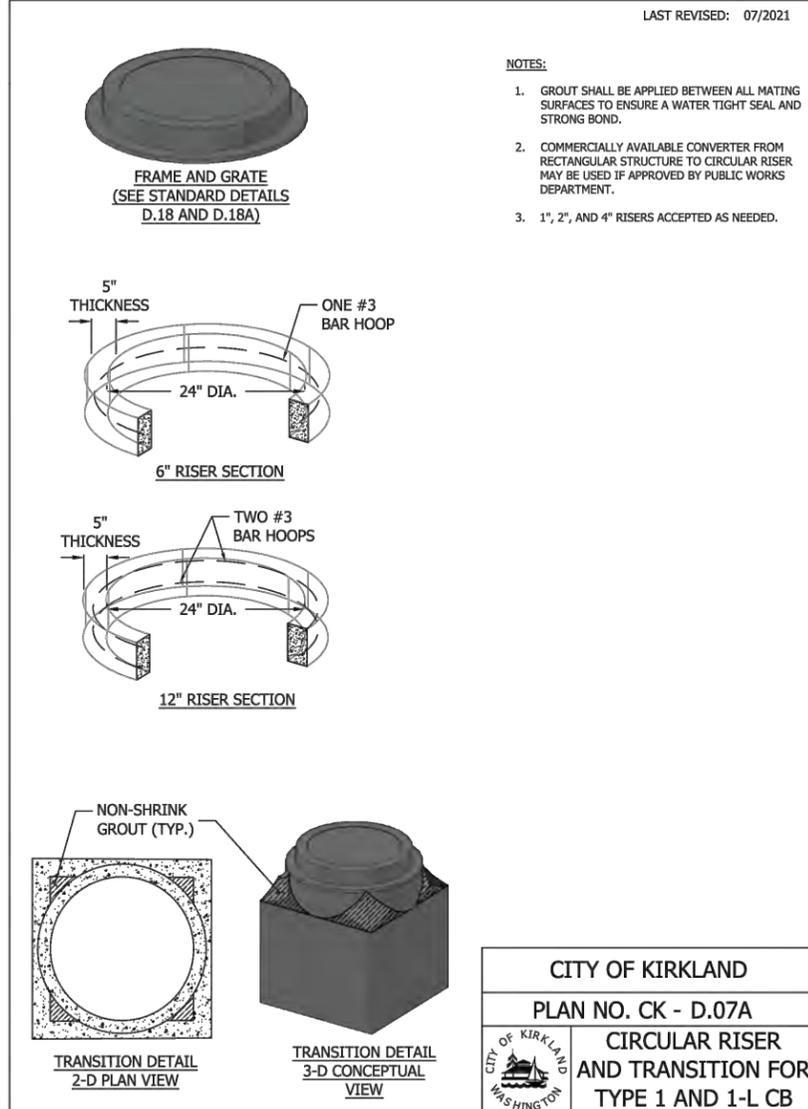


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JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS
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STANDARD DETAILS

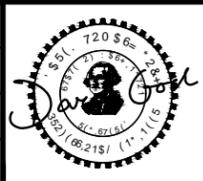
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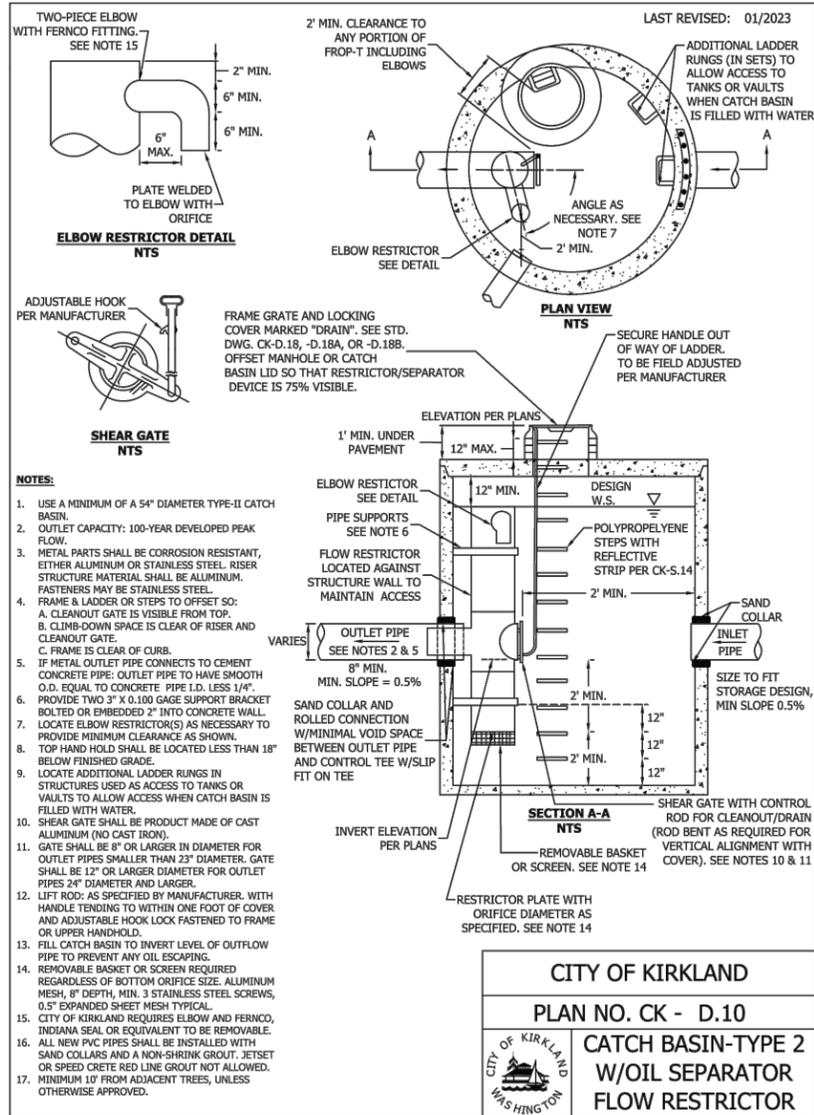
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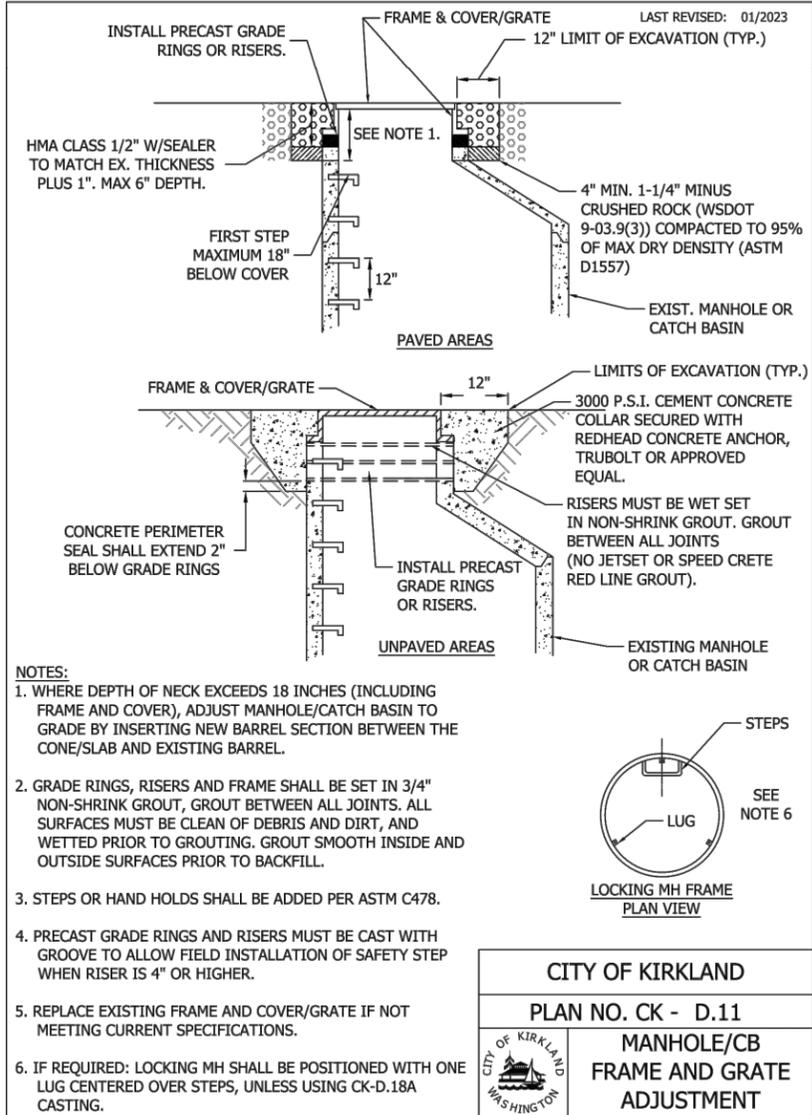
**JUANITA DRIVE MULTIMODAL,
INTERSECTION & SAFETY IMPROVEMENTS**

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STANDARD DETAILS

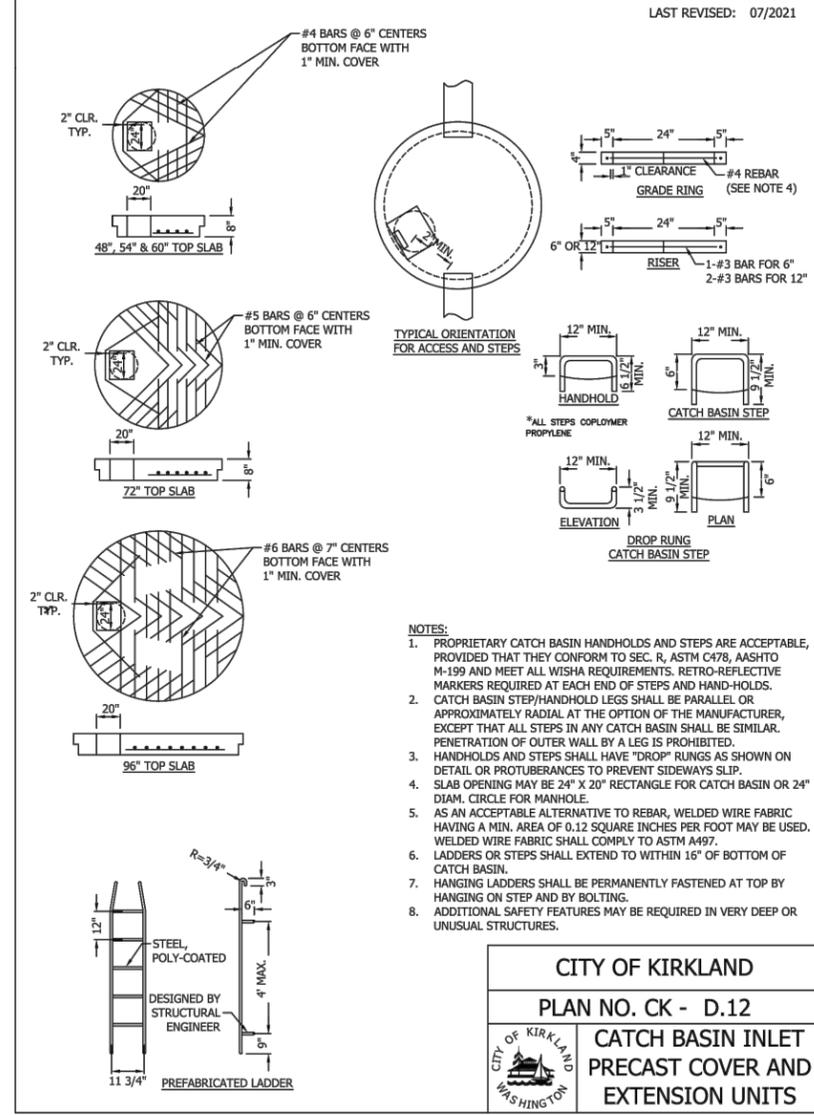
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CITY OF KIRKLAND
PLAN NO. CK - D.10
CATCH BASIN-TYPE 2
W/OIL SEPARATOR
FLOW RESTRICTOR



CITY OF KIRKLAND
PLAN NO. CK - D.11
MANHOLE/CB
FRAME AND GRATE
ADJUSTMENT

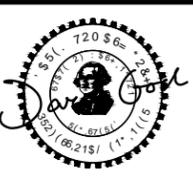


CITY OF KIRKLAND
PLAN NO. CK - D.12
CATCH BASIN INLET
PRECAST COVER AND
EXTENSION UNITS

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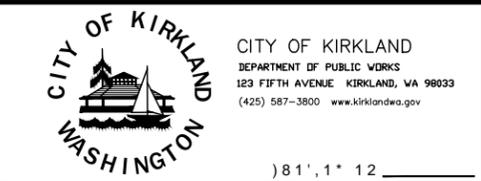


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JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

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LAST REVISED: 01/2023

PLAN

ELEVATION

NOTES:

- USE EAST JORDAN IRON WORKS OR EQUAL TWO BOLT LOCK CAPABILITY THAT MEETS WSDOT SPEC. MANUFACTURER SUBJECT TO APPROVAL BY CITY.
- USE WITH TWO LOCKING BOLTS 5/8"-11 NC STAINLESS TYPE 304 STEEL SOCKET HEAD (ALLEN HEAD) BOLTS, 2" LONG. FRAMES SHALL INCLUDE THREADS AS DROP-OUT REPLACEABLE NUTS.
- MATERIAL IS DUCTILE IRON ASTM A536 GRADE 80-55-06.
- "OUTFALL TO STREAM DUMP NO POLLUTANTS" MAY BE LOCATED ON BORDER AREA.
- SHALL CONFORM TO SEC. 7.05 OF THE STANDARD SPECIFICATIONS.
- WELDING IS NOT PERMITTED.
- EDGES SHALL HAVE 0.125" RADIUS, 0.125" CHAMFER OR COMPLETE DEBURRING.
- USE A BI-DIRECTIONAL VANED GRATE AT ANY LOW POINT OR WHEN FLOWS COME FROM MULTIPLE DIRECTIONS.
- NO EXPANSION MATERIAL IN THE FLOW LINE, WHERE CONCRETE COMES TO FRAME.
- FRAME AND COVER SHALL BE H-20 LOADING RATED IF INSTALLED IN ROADWAY.
- MUST BE MADE IN USA.

CITY OF KIRKLAND	
PLAN NO. CK - D.14	
	VANED GRATE FOR CATCH BASIN AND INLET

LAST REVISED: 01/2022

PLAN

SECTION B-B

SECTION A-A

HOOD DETAIL

NOTES:

- FRAME AND COVER SHALL BE EAST JORDAN IRON WORKS OR EQUAL, SUBJECT TO APPROVAL BY CITY. MATERIAL SHALL CONFORM TO SECTION 9-05.15(2) OF THE STANDARD SPECIFICATION.
- PATTERN ON TOP SURFACE SHALL SPECIFY THE FISH LOGO AND DUMP NO POLLUTANTS (NO DIAMOND PATTERN).
- BOLT, WASHER, AND NUT SHALL BE GALVANIZED OR CORROSION RESISTANT. BOLTS SHALL BE INSERTED INTO THE FACE OF THE HOOD WITH WASHER AND NUT SECURED TO THE BACK SIDE OF THE HOOD.
- USE APPROPRIATE GRATE DEPENDING ON THE DIRECTION OF FLOW.
- NO HORIZONTAL CROSS BAR IN THE OPENING.
- 18" X 24" VANED OR BI-VANED LID. APPLICATION OF THIS DETAIL NOT TO REPLACE FUNCTION OF CK-D.14.
- MUST BE MADE IN THE USA.
- TROWELED EDGE MUST BE IN CONTACT WITH FRAME (RATHER THAN EXPANSION JOINT).

CITY OF KIRKLAND	
PLAN NO. CK - D.15	
	OPEN CURB FACE FRAME AND GRATE DETAILS

LAST REVISED: 07/2021

PLAN

SECTION B-B

SECTION A-A

NOTES:

- FRAME AND COVER SHALL BE EAST JORDAN IRON WORKS OR EQUAL, SUBJECT TO APPROVAL BY CITY. SEE CK-D.15.
- PATTERN ON TOP SHALL SPECIFY FISH LOGO AND DUMP NO POLLUTANTS (NO DIAMOND PATTERN).
- CASTING MUST BE SET 0.5" BELOW FINAL ROAD/GUTTER GRADE.
- HOOD SHALL MATCH TOP OF CURB ELEVATION.
- NO HORIZONTAL CROSS BAR IN THE OPENING.
- TROWELED EDGE MUST BE IN CONTACT WITH FRAME (RATHER THAN EXPANSION JOINT).
- MUST BE MADE IN THE USA.

CITY OF KIRKLAND	
PLAN NO. CK - D.16	
	THROUGH-CURB INLET FRAME AND GRATE WITH VERTICAL CURB INSTALLATION

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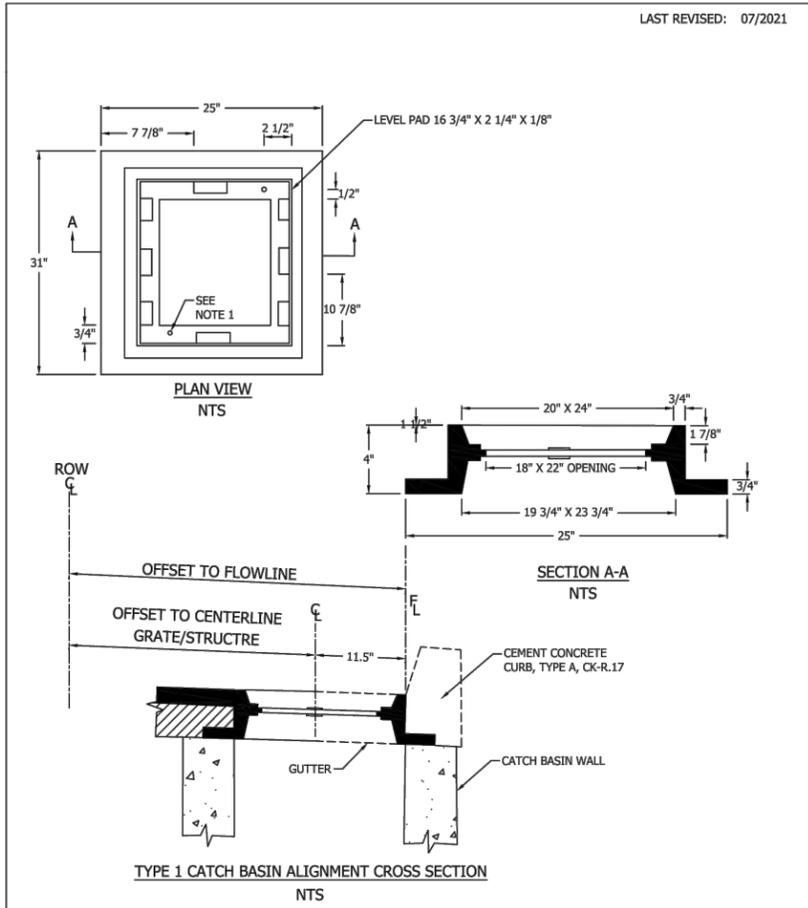
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**JUANITA DRIVE MULTIMODAL,
INTERSECTION & SAFETY IMPROVEMENTS**

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STANDARD DETAILS

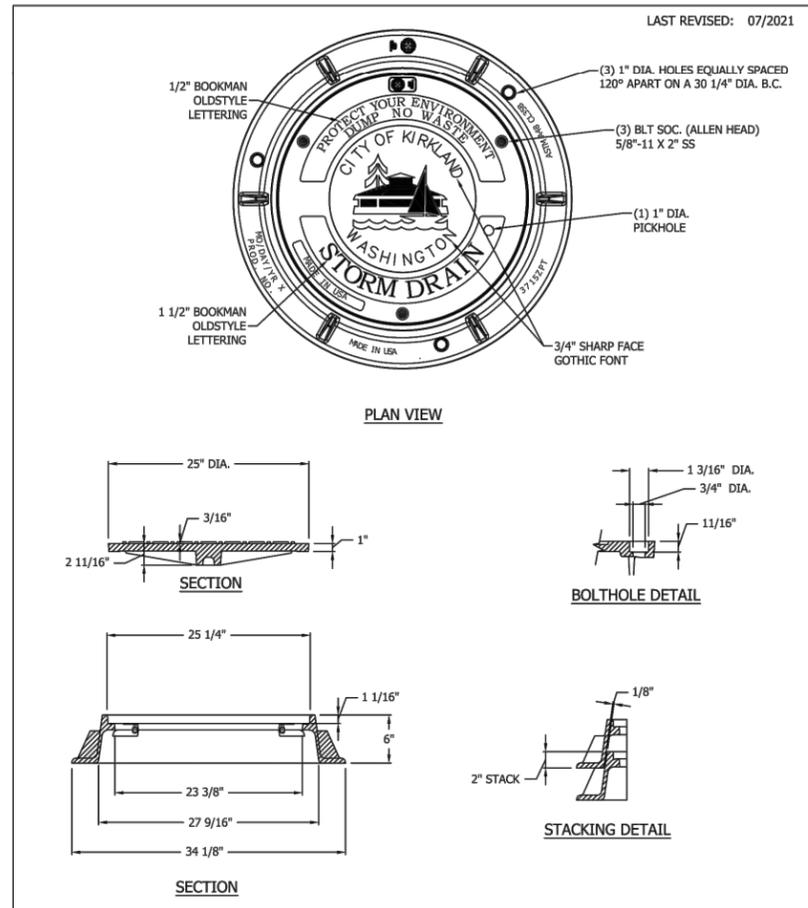
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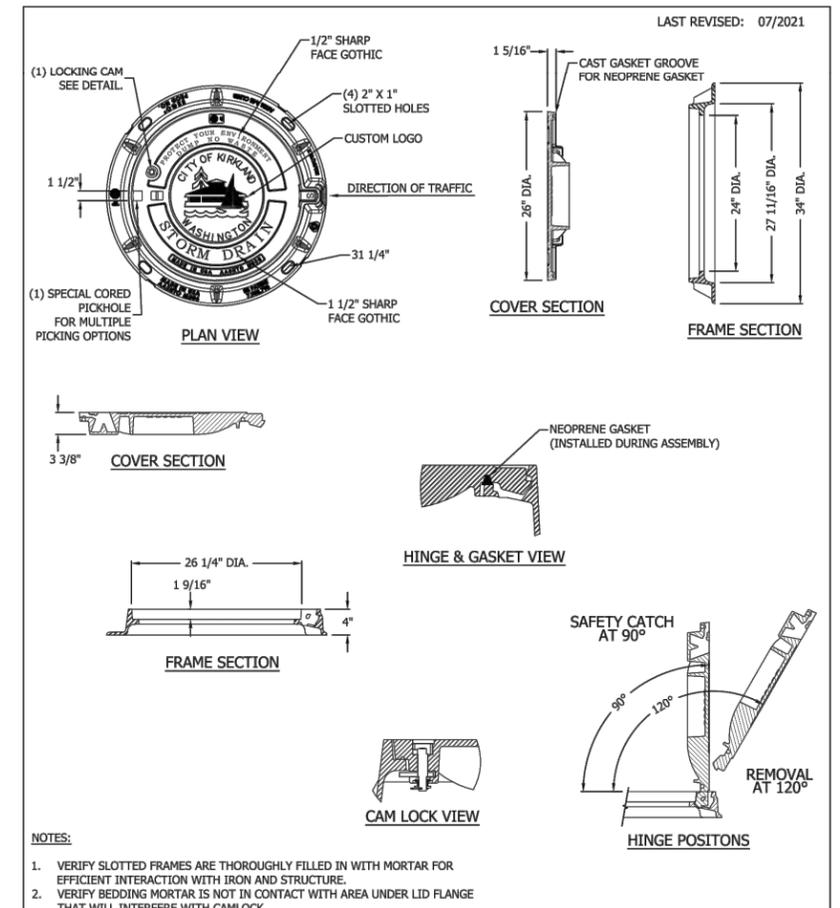
NOTE:
 1. FRAME MATERIAL IS CAST IRON PER ASTM A48 CLASS 30.
 2. SET FRAME TO GRADE AND CONSTRUCT ROAD AND GUTTER TO BE FLUSH WITH FRAME.
 3. BACK OF FRAME SHALL BE IN FLOWLINE OF GUTTER.
 4. MUST BE MADE IN THE USA.

CITY OF KIRKLAND
PLAN NO. CK - D.16A
STANDARD FRAME WITH CURB INSTALLATION



NOTE:
 1. COVERS SHALL BE GRAY IRON, LOCKING, WITH A MINIMUM WEIGHT OF 141 LBS.
 2. MINIMUM WEIGHT OF FRAME SHALL BE 134 LBS.
 3. PRODUCT SUPPLIED BY EJ GROUP, INC., APPROVED EQUAL.
 4. CITY OF KIRKLAND LOGO REQUIRED
 5. THIS SPEC SHOULD NOT BE USED IN THE ROADWAY.
 6. MUST BE MADE IN THE USA.

CITY OF KIRKLAND
PLAN NO. CK - D.18
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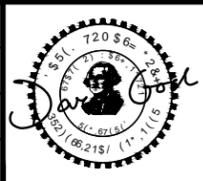


NOTE:
 1. VERIFY SLOTTED FRAMES ARE THOROUGHLY FILLED IN WITH MORTAR FOR EFFICIENT INTERACTION WITH IRON AND STRUCTURE.
 2. VERIFY BEDDING MORTAR IS NOT IN CONTACT WITH AREA UNDER LID FLANGE THAT WILL INTERFERE WITH CAMLOCK.
 3. INSTALL PLUG IN LOCK HOLE TO KEEP LOCK FREE OF FOREIGN MATERIAL.
 4. 24 INCH MANHOLE LID IS FITTED WITH AN INFILTRATION PLUG LOCATED IN THE HINGE HOUSING OF THE FRAME. VERIFY PLUG IS PROPERLY INSTALLED BEFORE INSTALLING THE FRAME.
 5. REQUIRED ON ALL ARTERIALS, COLLECTORS OR ANY TIME THAT THE IRON WILL BE WITHIN THE TRAVEL LANE.
 6. LID SHALL BE MARKED "STORM DRAIN".
 7. CITY OF KIRKLAND LOGO REQUIRED.
 8. LID MUST BE COVERED WITH TAR PAPER BEFORE OVERLAY.
 9. PRODUCT SUPPLIED BY EAST JORDAN IRON WORKS, OR APPROVED EQUAL.
 10. FRAME AND COVER SHALL BE H-20 LOADING RATED AND BE AT MINIMUM 7\"/>

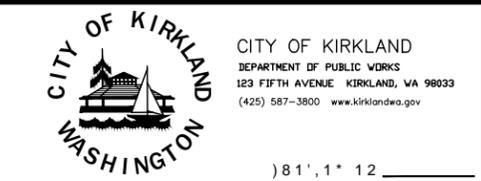
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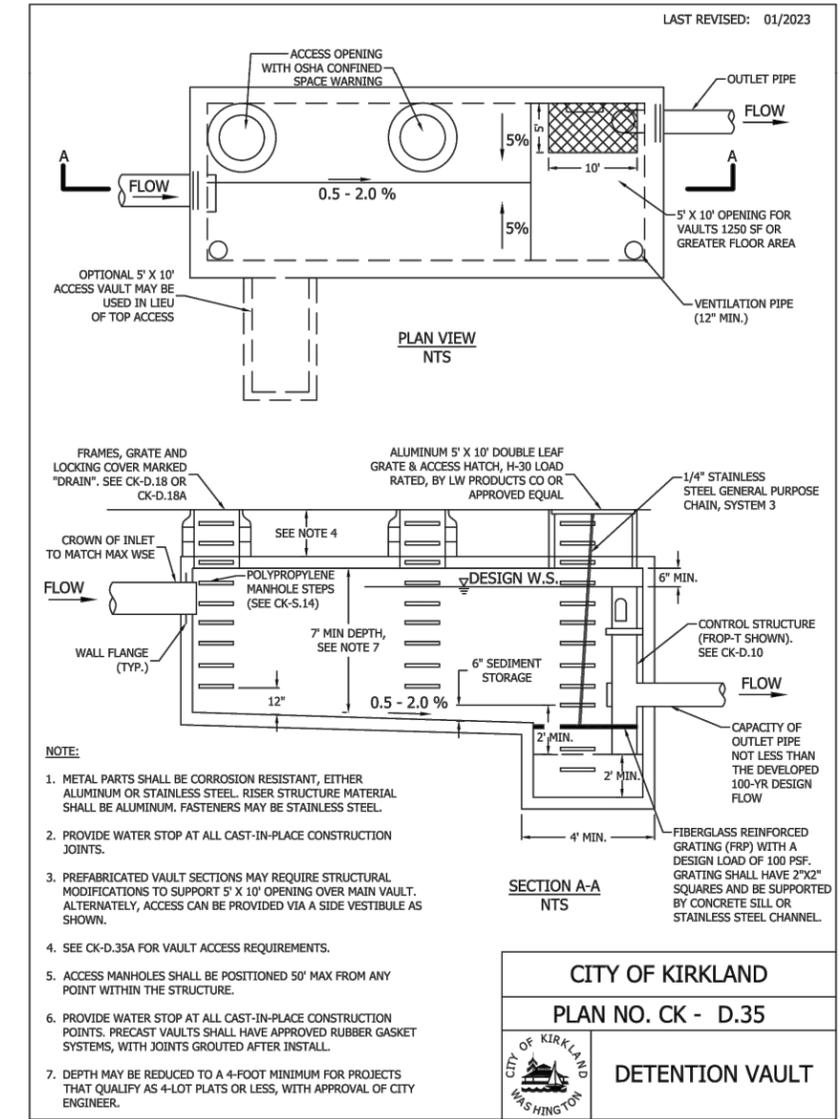
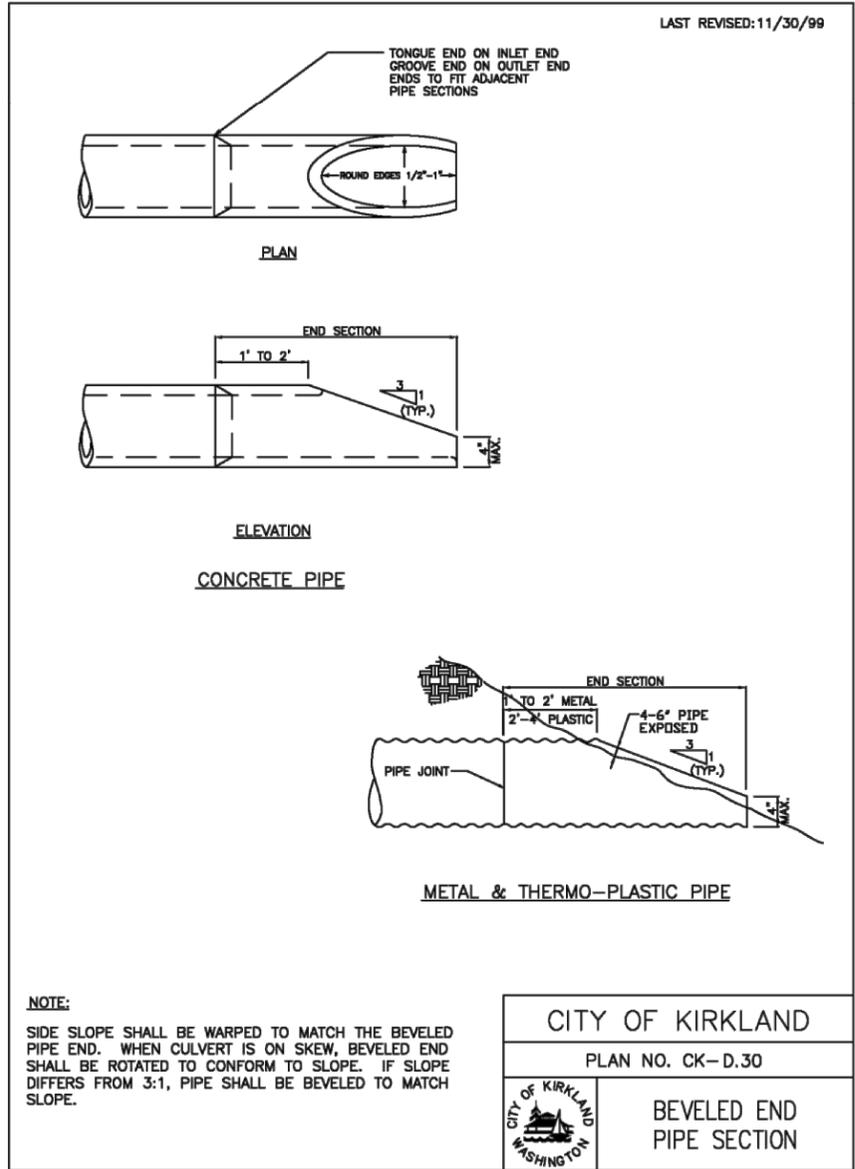
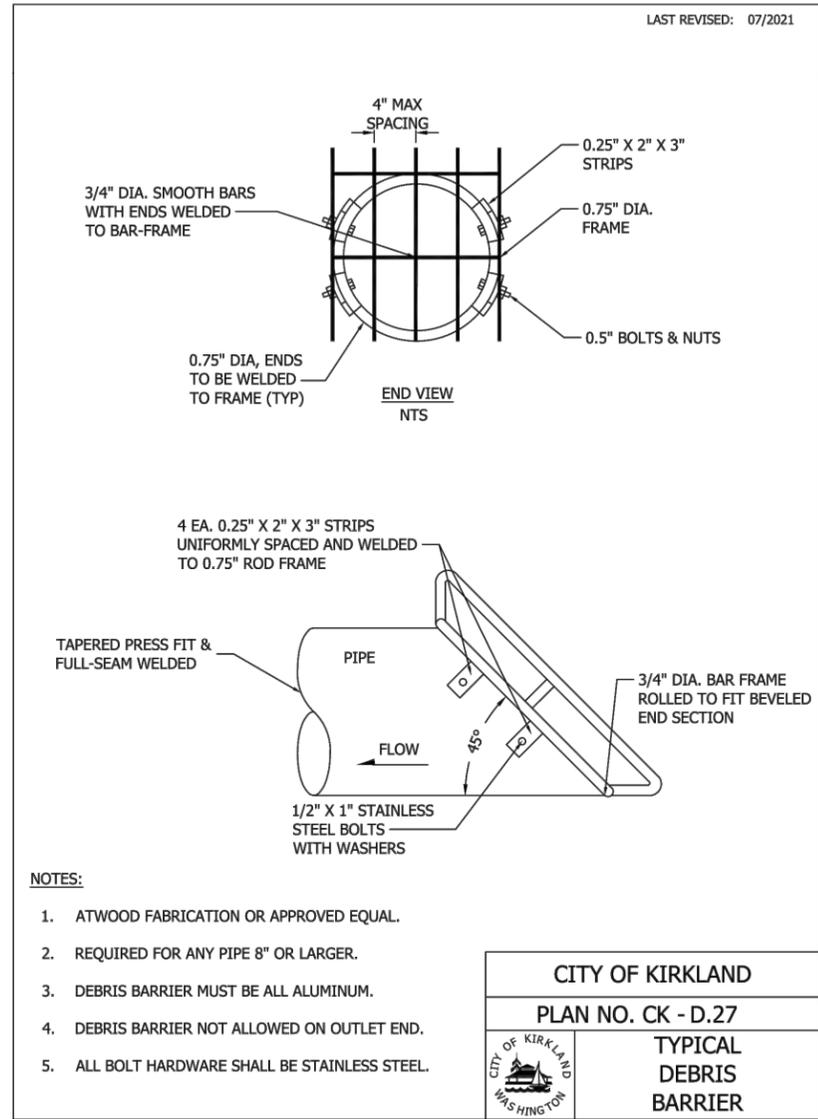
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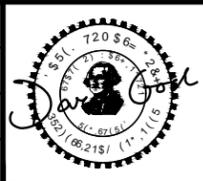
JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS
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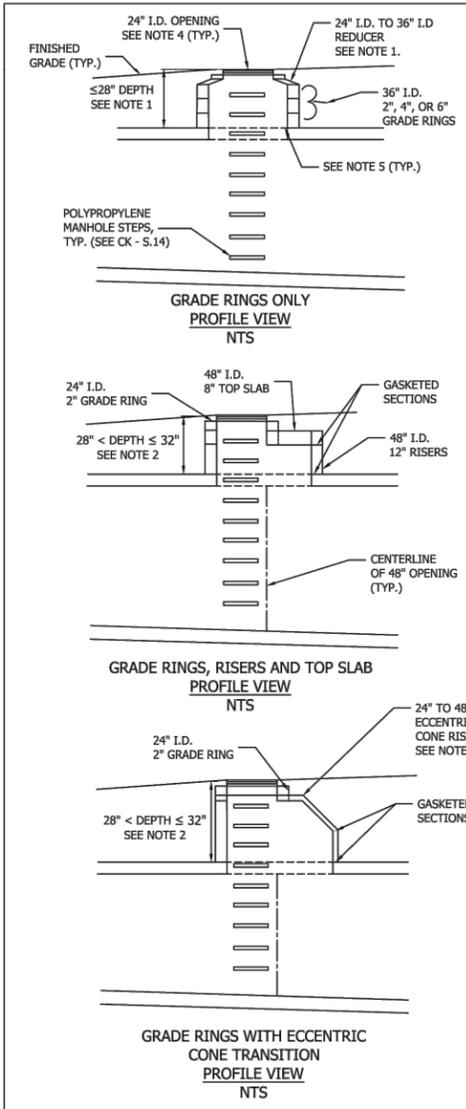
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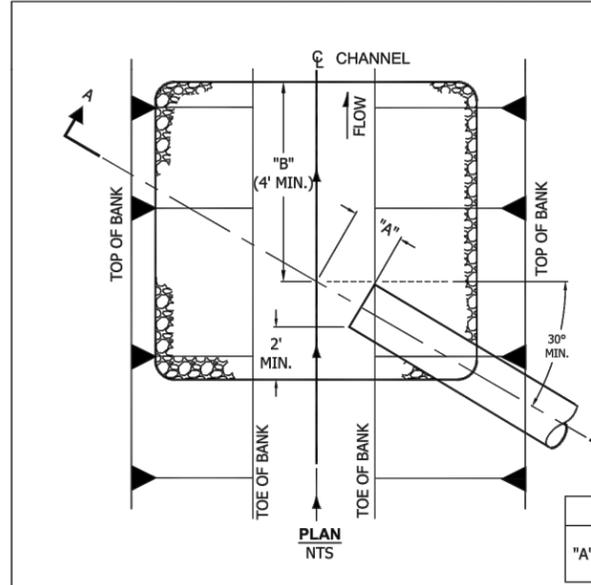
LAST REVISED: 01/2023

NOTE:

- 24 INCH ACCESS LID SET ON REDUCER ATOP 36 INCH GRADE RINGS TO BE USED WHEN DEPTH TO VAULT OPENING IS 28 INCHES OR LESS FROM THE SURFACE.
- TRANSITION TO 48 INCH ACCESS TO BE USED WHEN DEPTH TO VAULT OPENING IS GREATER THAN 28 INCHES FROM THE SURFACE. FOR DEPTHS BETWEEN 28 INCHES AND 32 INCHES, USE COMBINATION OF RISERS AND TOP SLAB. FOR DEPTHS GREATER THAN 32 INCHES, USE ECCENTRIC CONE TRANSITION.
- REFER TO MANUFACTURER FOR HOW ACCESS RISERS AND TOP SLAB SHALL BE STRUCTURALLY SUPPORTED.
- REFER TO CK-D.18, CK-D.18A, AND CK-D.18B FOR APPROPRIATE 24 INCH ACCESS CASTING.
- BOND FROM ACCESS RISERS, GRADE RINGS, OR CONE TO VAULT SHALL BE WET SET IN 3/4" NON-SHRINK GROUT. GROUT BETWEEN ALL JOINTS (NO JETSET OR SPEED CRETE RED LINE GROUT). ALL SURFACES MUST BE CLEAN OF DEBRIS AND DIRT, AND WETTED PRIOR TO GROUTING. GROUT SMOOTH INSIDE AND OUTSIDE SURFACE PRIOR TO BACKFILL.
- MUST BE ECCENTRIC CONE. CONCENTRIC CONE IS NOT ALLOWED.

CITY OF KIRKLAND
 PLAN NO. CK - D.35A

 VAULT ACCESS

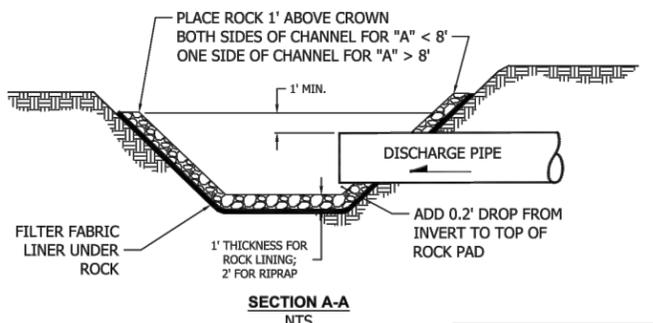


LAST REVISED: 07/2021

NOTE: "A" IS DISTANCE FROM END OF PIPE TO CENTER OF CHANNEL. "B" IS DISTANCE DOWNSTREAM FROM "A" TO END OF OUTFALL PROTECTION.

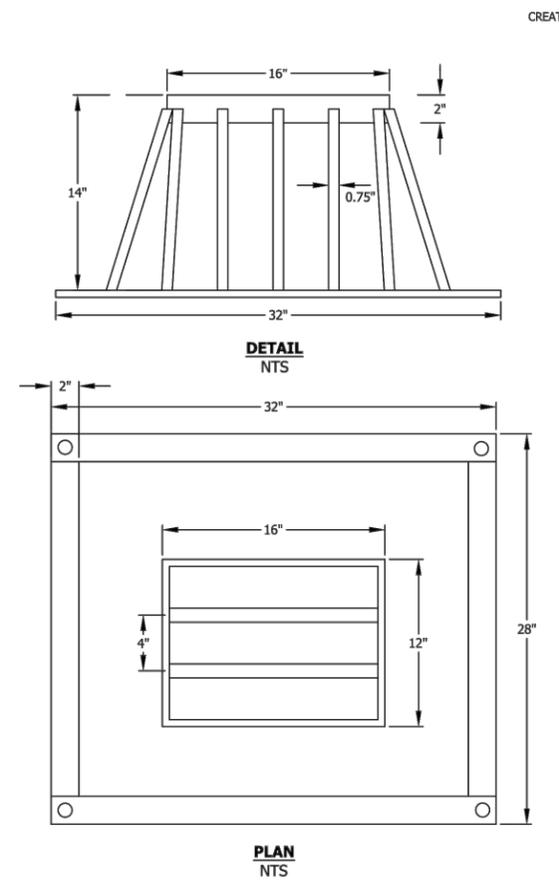
IF DISCHARGE VELOCITY IS LESS THAN OR EQUAL TO 5 FPS, USE ROCK LINING. IF DISCHARGE VELOCITY IS GREATER THAN 5 BUT EQUAL OR LESS THAN 10 FPS, USE RIPRAP. (TO BE DETERMINED BY ENGINEER)

REQUIRED DIMENSIONS	
"A"+"B"	8' FOR ROCK LINING
	12' FOR RIP RAP



CITY OF KIRKLAND
 PLAN NO. CK - D.43

 OUTFALL PROTECTION



CREATED: 1/2019

NOTES

- TO BE PLACED ON TOP OF VERTICAL OVERFLOW STRUCTURES, I.E., WITHIN BIORETENTION CELLS
- 3/8" x 2 FLAT BAR
- 3/4" ROUND BAR
- 4-5/8" HOLE FOR MOUNTING
- ALL MATERIAL 6061 ALUMINUM
- MUST BE ANCHORED TO CONCRETE IN REMOVABLE FASHION WITH CORROSION-RESISTANT HARDWARE
- PRODUCT: ATWOOD FABRICATION OR EQUAL

CITY OF KIRKLAND
 PLAN NO. CK- D.44

 TYPE 1 CB DEBRIS (BIRD) CAGE

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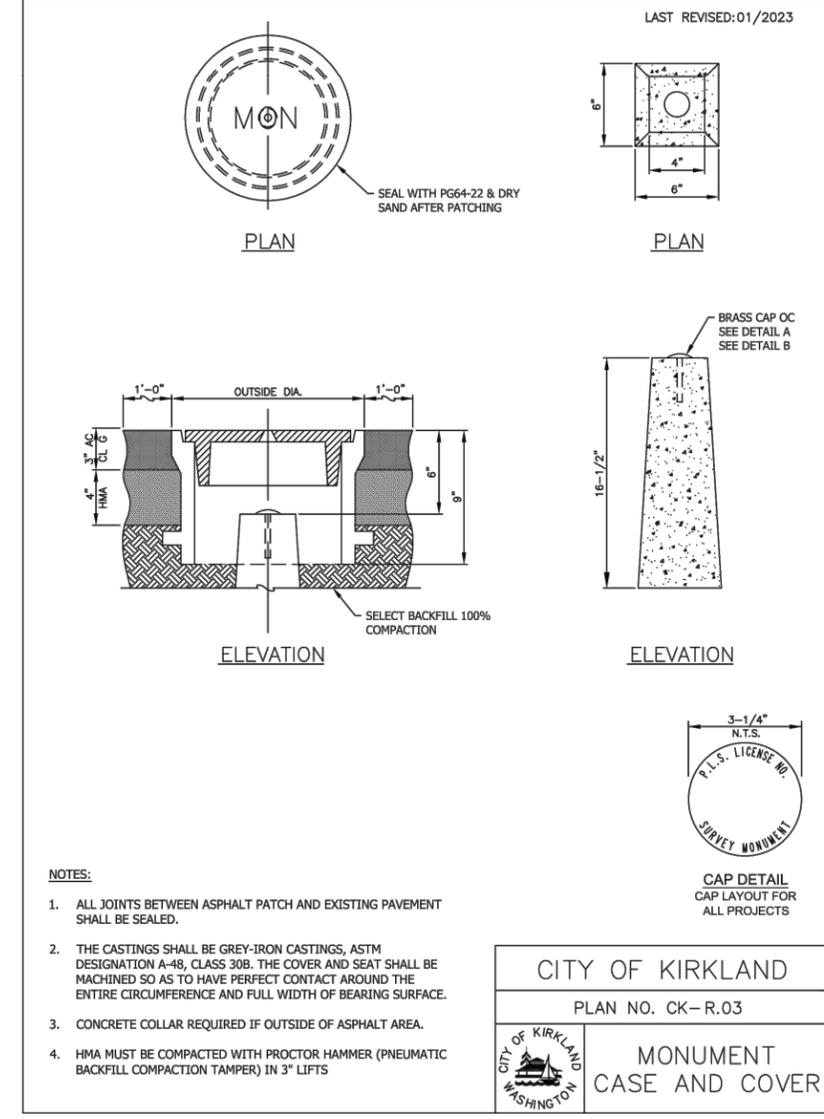
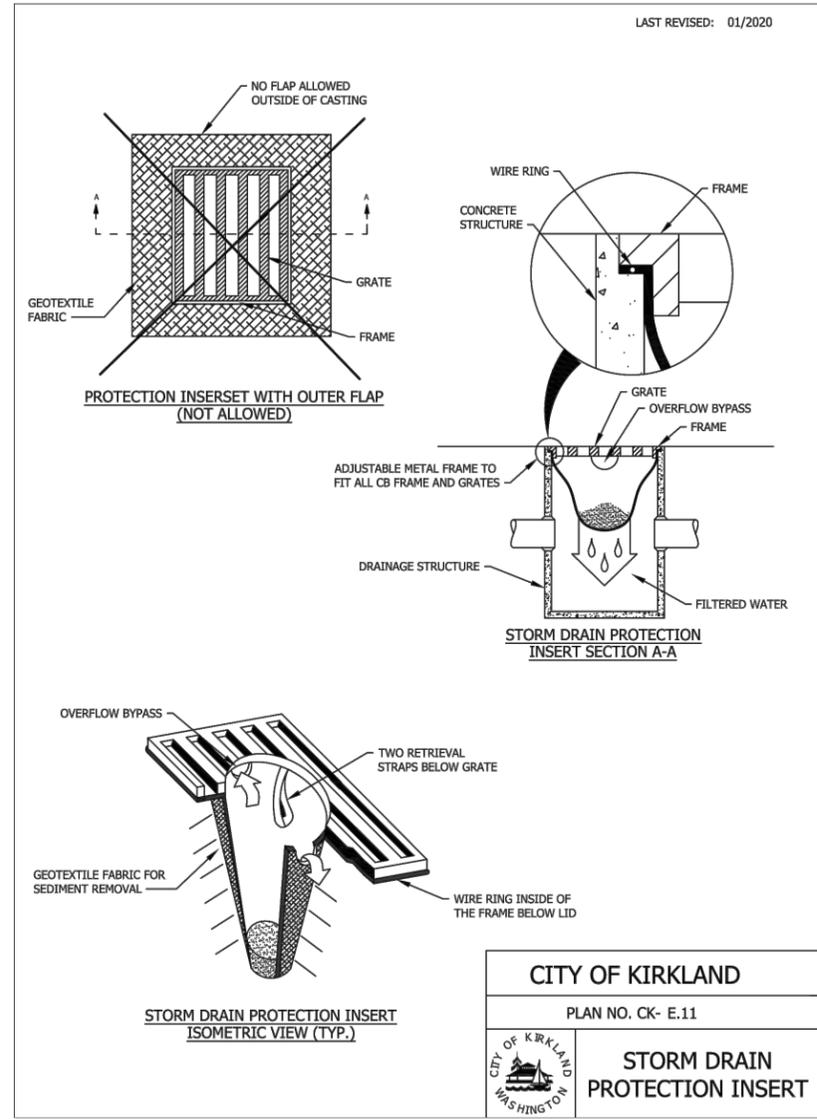
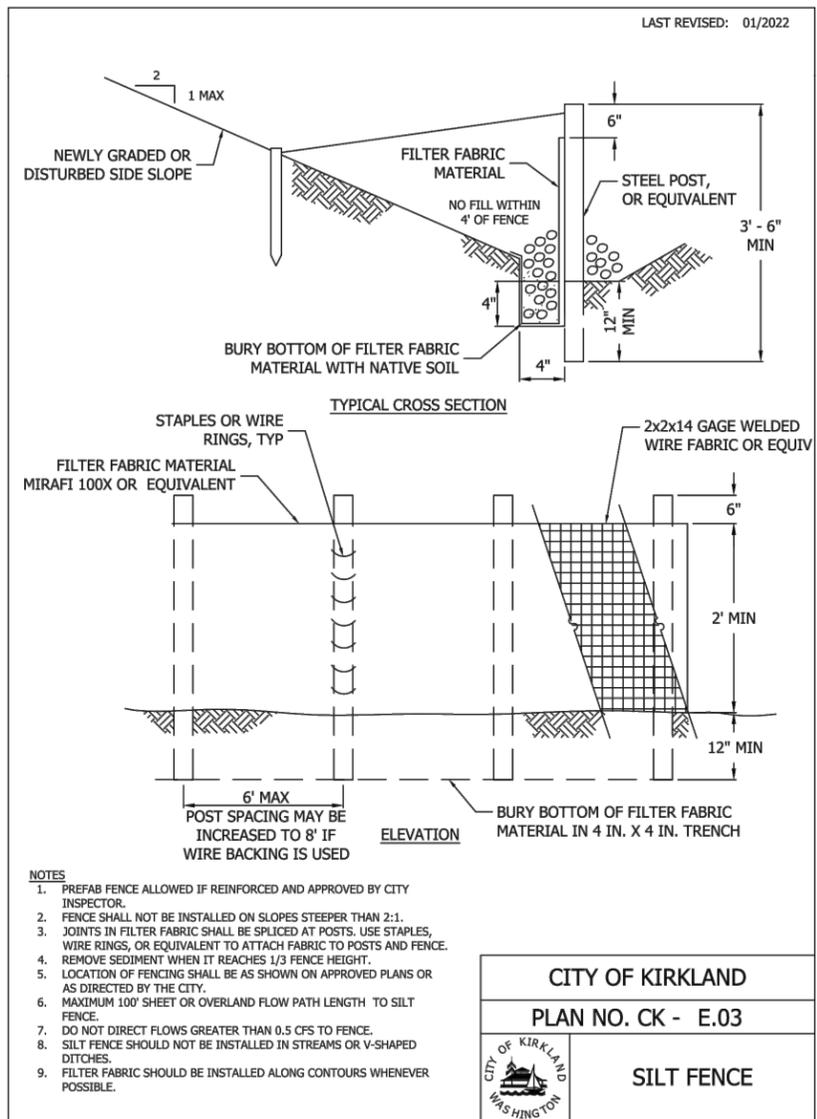
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STANDARD DETAILS

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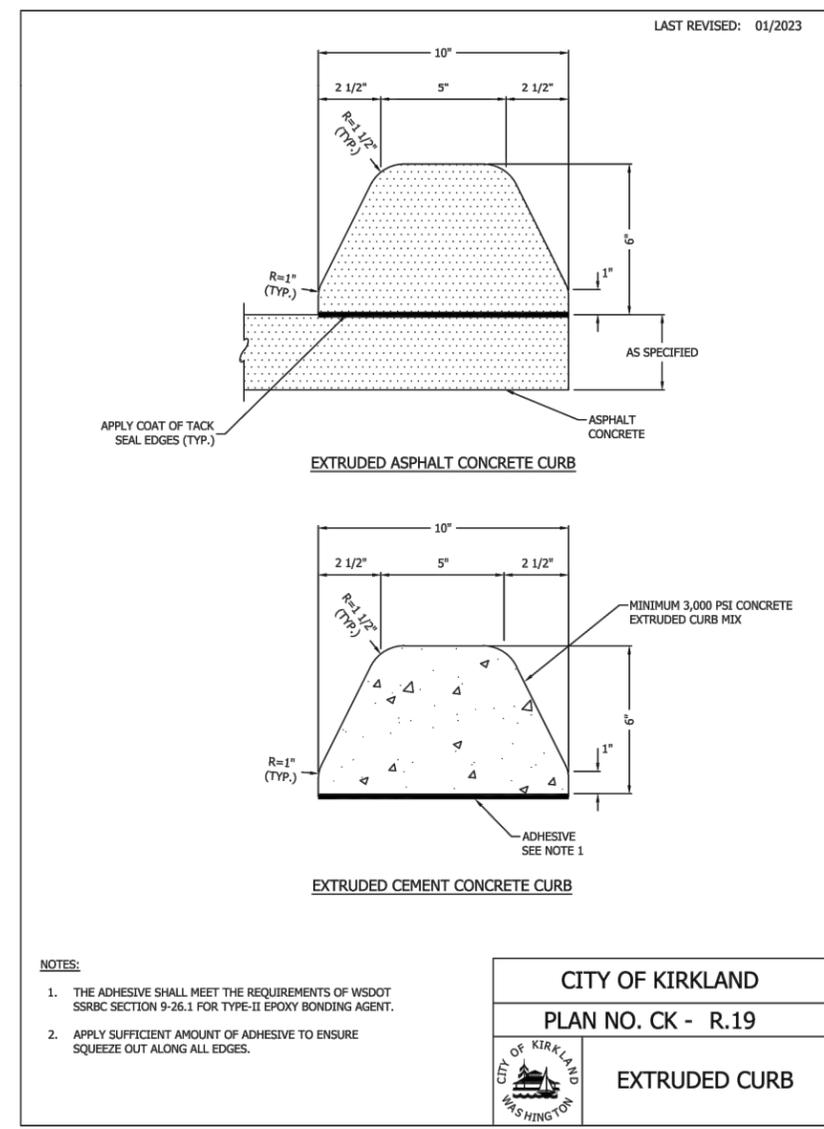
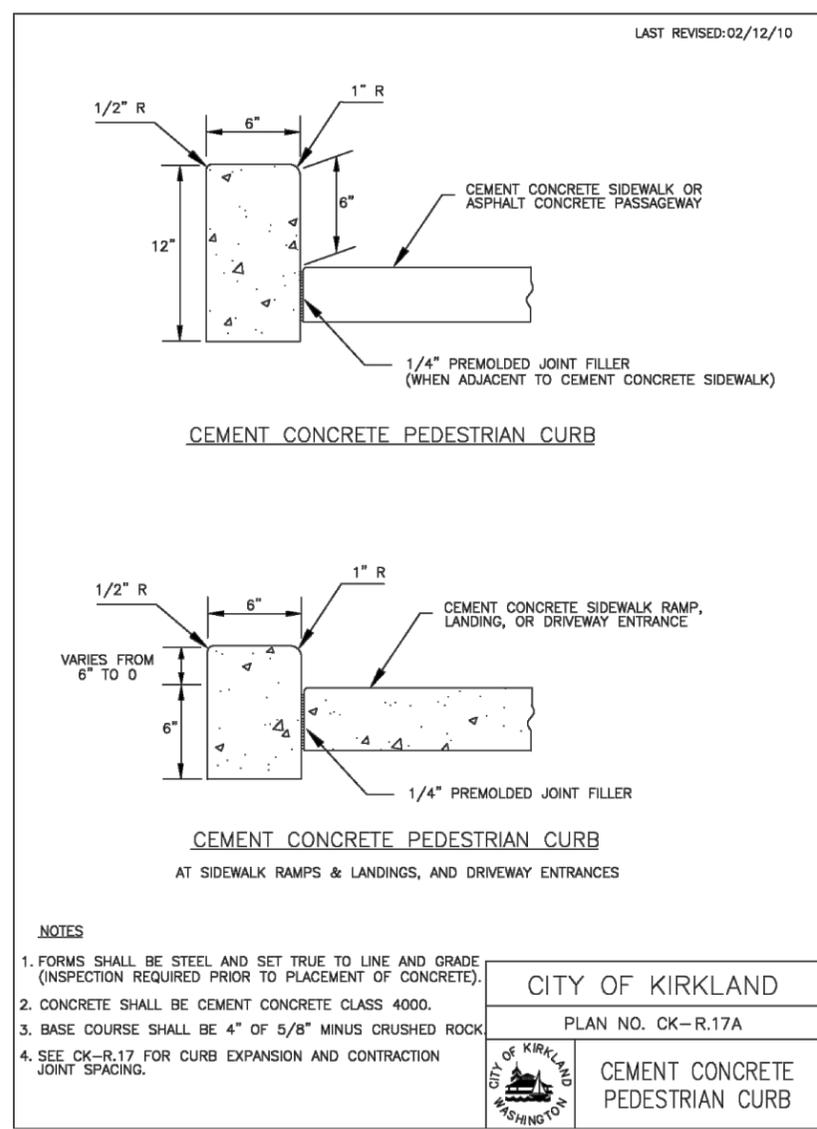
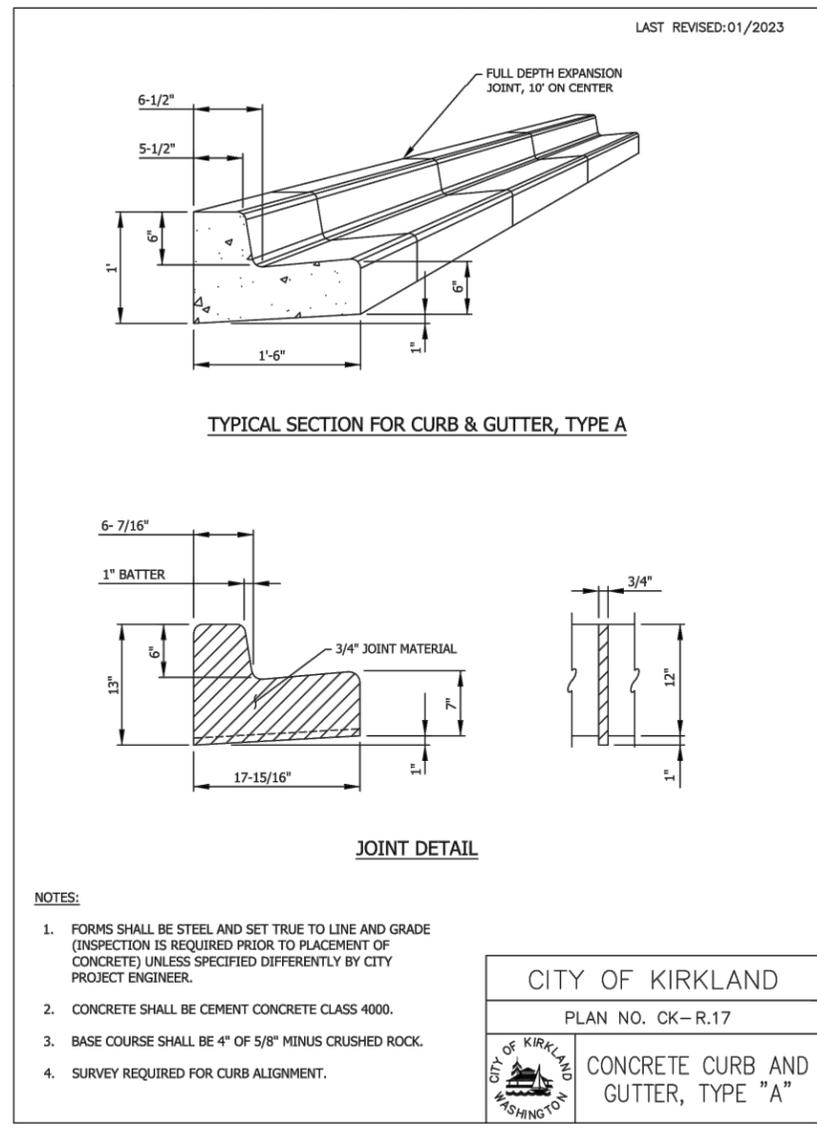
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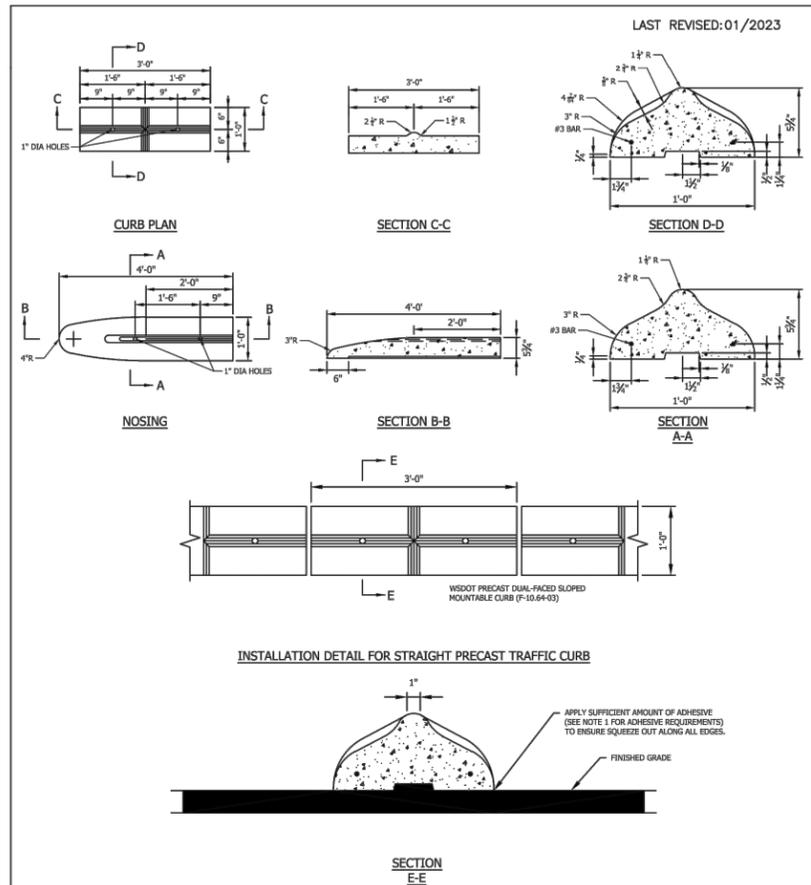
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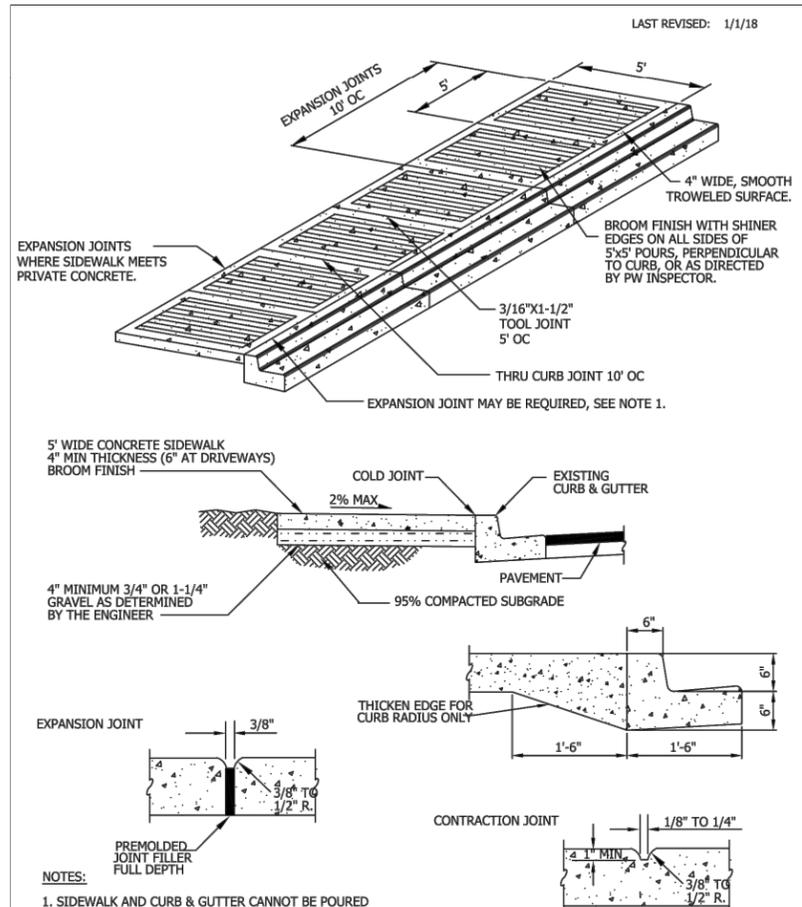


- NOTES:**
1. THE ADHESIVE SHALL MEET THE REQUIREMENTS OF SECTION 9.26(1) OF THE WSDOT STANDARD SPECIFICATION. USE APPROPRIATE ADHESIVE TYPE FOR EXISTING CONDITIONS.
 2. MEDIAN CURB SHALL BE PAINTED. PAINT SHALL MEET SECTION 9.34.2 OF THE WSDOT STANDARD SPECIFICATION.

CITY OF KIRKLAND

PLAN NO. CK-R.19A

MEDIAN CURB

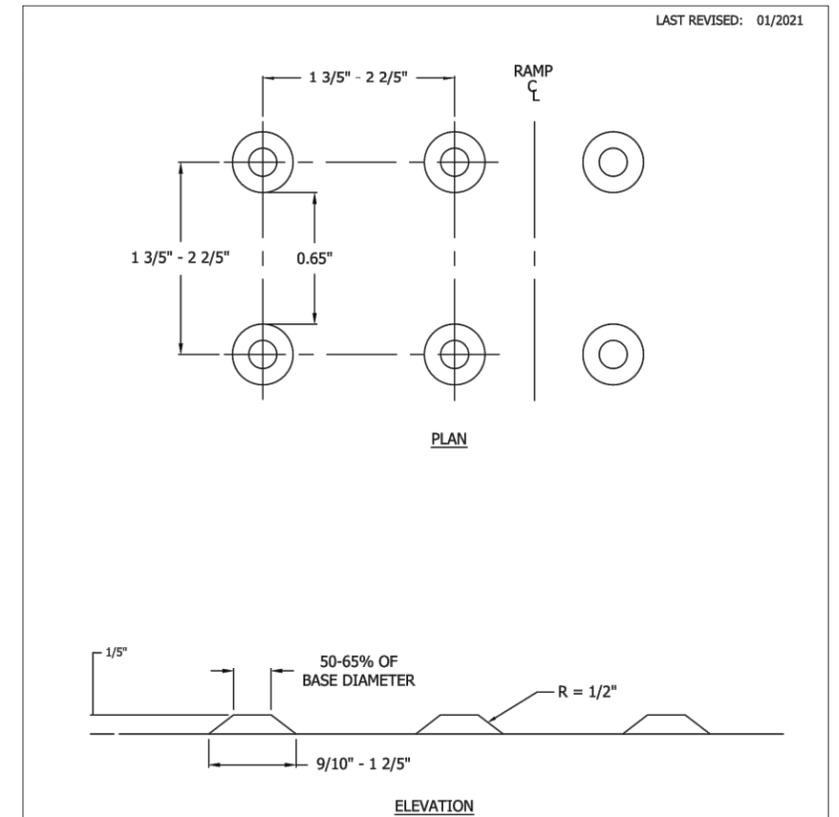


- NOTES:**
1. SIDEWALK AND CURB & GUTTER CANNOT BE POURED MONOLITHICALLY. EXPANSION JOINT WILL BE REQUIRED WHEN CONCRETE SIDEWALK IS SURROUNDED BY OTHER HARD SURFACES (E.G., DRIVEWAY); OR AS DIRECTED BY PW INSPECTOR.
 2. CONCRETE SHALL BE CEMENT CONCRETE CLASS 4000 PSI MINIMUM, WITH AIR ENTRAINMENT. NO COLOR OR TINT SHALL BE ADDED.
 3. FORMS SHALL BE SET TRUE TO LINE AND GRADE AND SHALL BE STEEL UNLESS OTHERWISE APPROVED BY INSPECTOR.
 4. SIDEWALK SHALL NOT BE POURED IN THE RAIN. SEE POLICY R-8, PLACING CONCRETE OR ASPHALT IN ADVERSE WEATHER CONDITIONS.

CITY OF KIRKLAND

PLAN NO. CK- R.23

SIDEWALK SECTION



- NOTE:**
1. THE DETECTABLE WARNING PATTERN SHALL BE FORMED BY ADDING A MANUFACTURED MATERIAL BEFORE THE CONCRETE HAS CURED.
 2. THE TWO-FOOT WIDE DETECTABLE WARNING PATTERN AREA ON THE RAMP SHALL BE YELLOW AND SHALL MATCH THE COLOR OF "STANDARD INTERSTATE YELLOW" PAINT AS SPECIFIED IN FORMULA K-2-83. EMBOSSING THE WET CONCRETE OR INSTALLING MASONRY OF CERAMIC TILES MUST BE APPROVED BY CITY ENGINEER.

CITY OF KIRKLAND

PLAN NO. CK - R.25B

TRUNCATED DOME TEXTILE WARNING SURFACE

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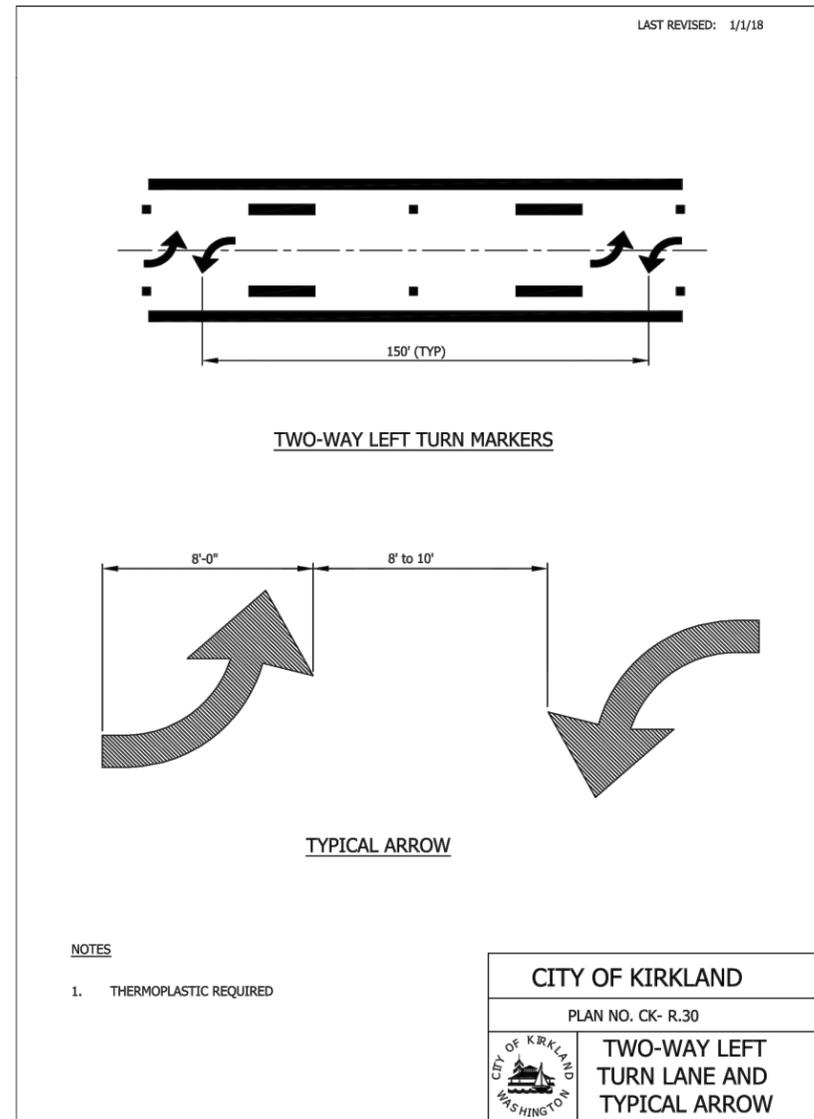
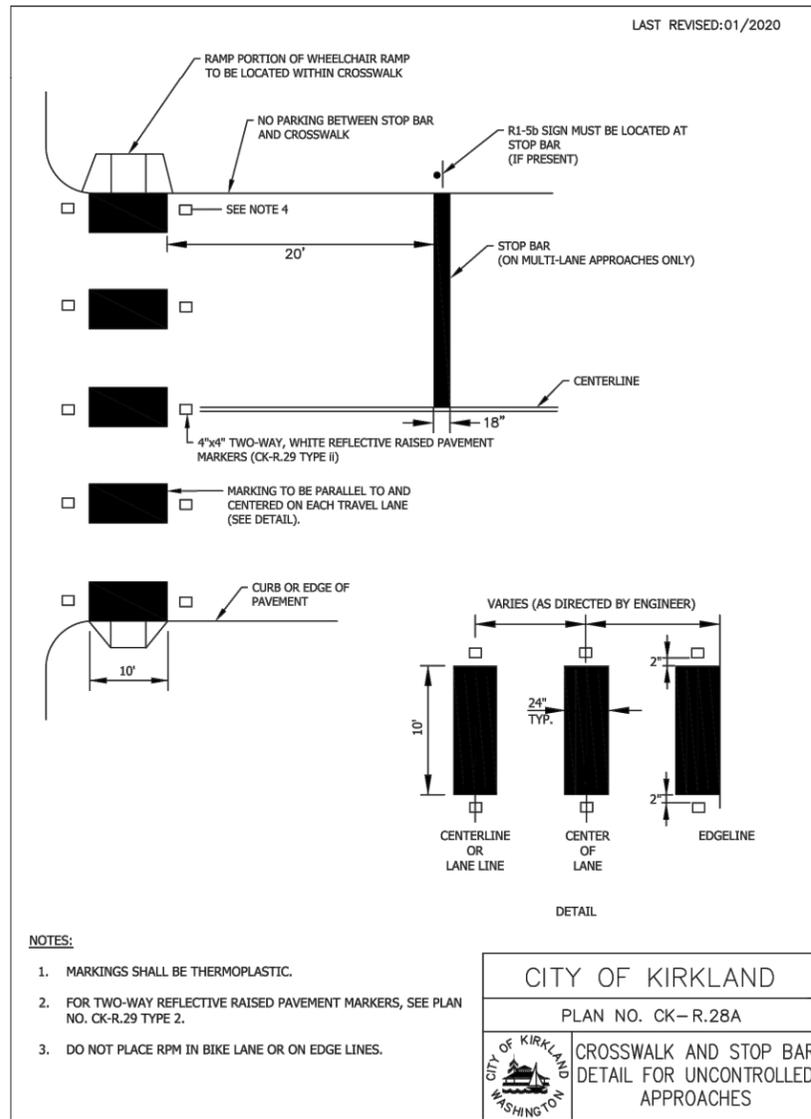
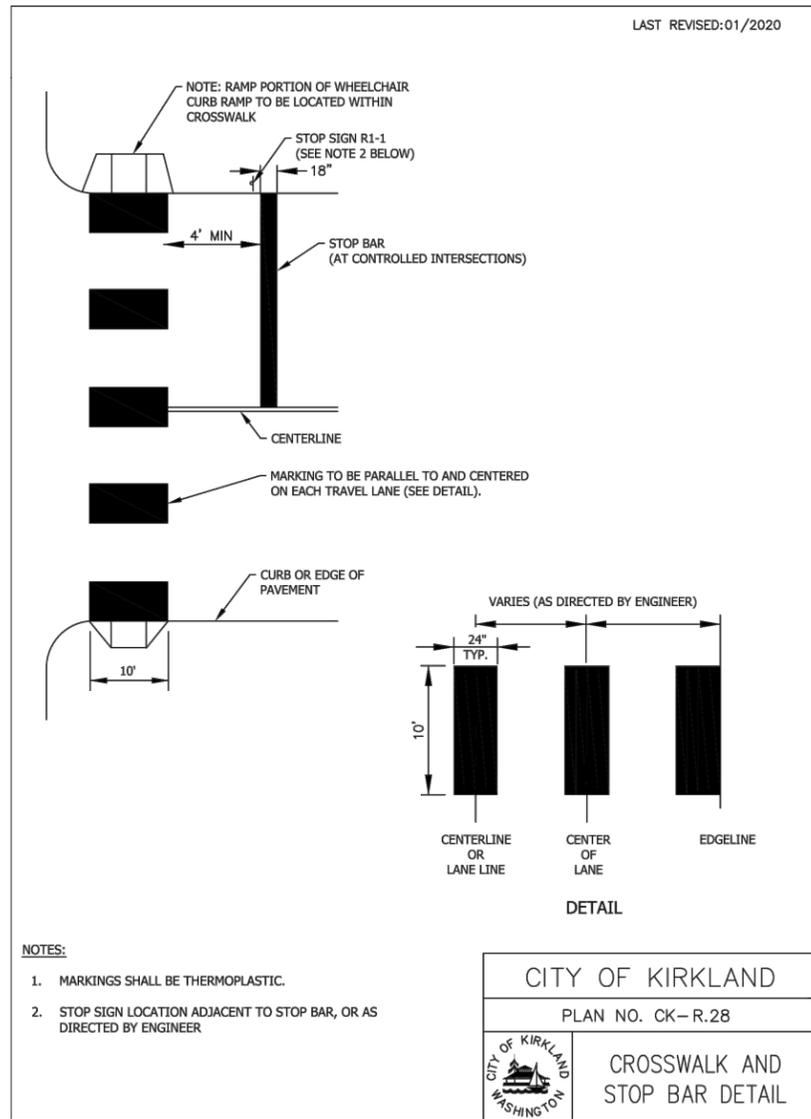
JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

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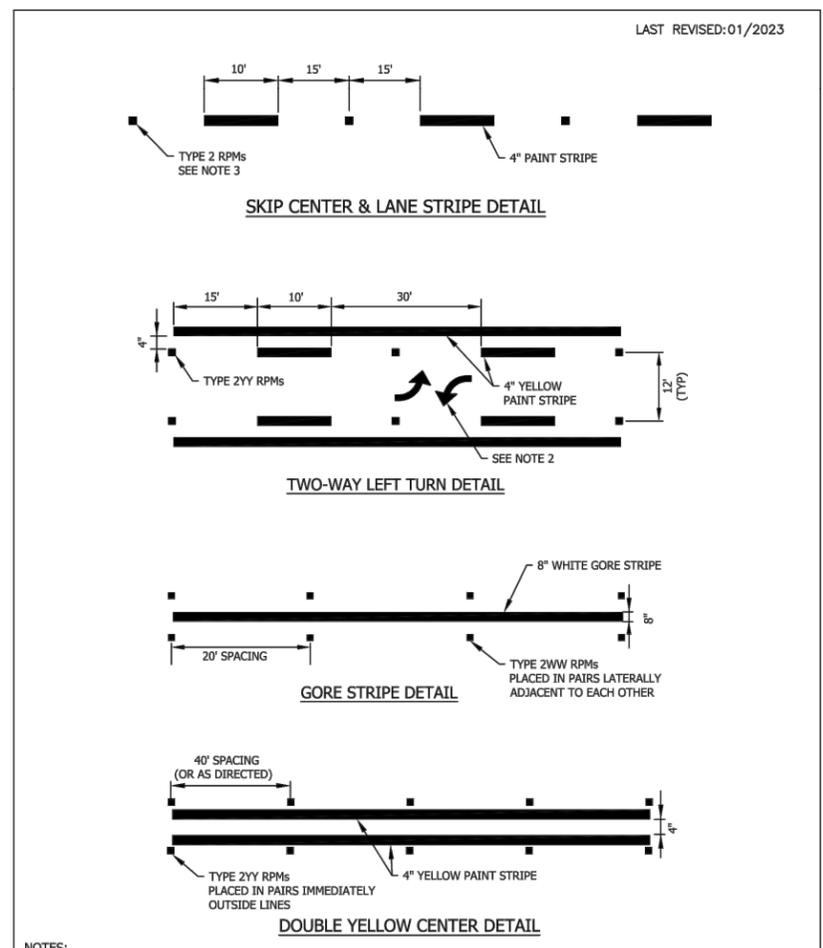
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**JUANITA DRIVE MULTIMODAL,
 INTERSECTION & SAFETY IMPROVEMENTS**

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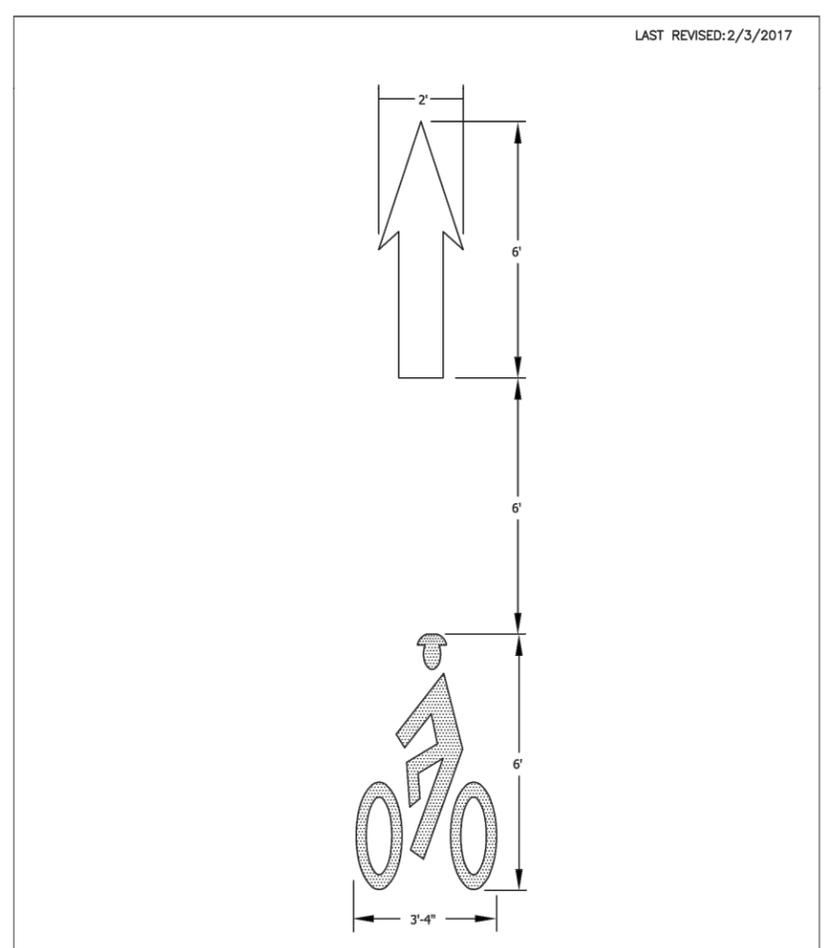
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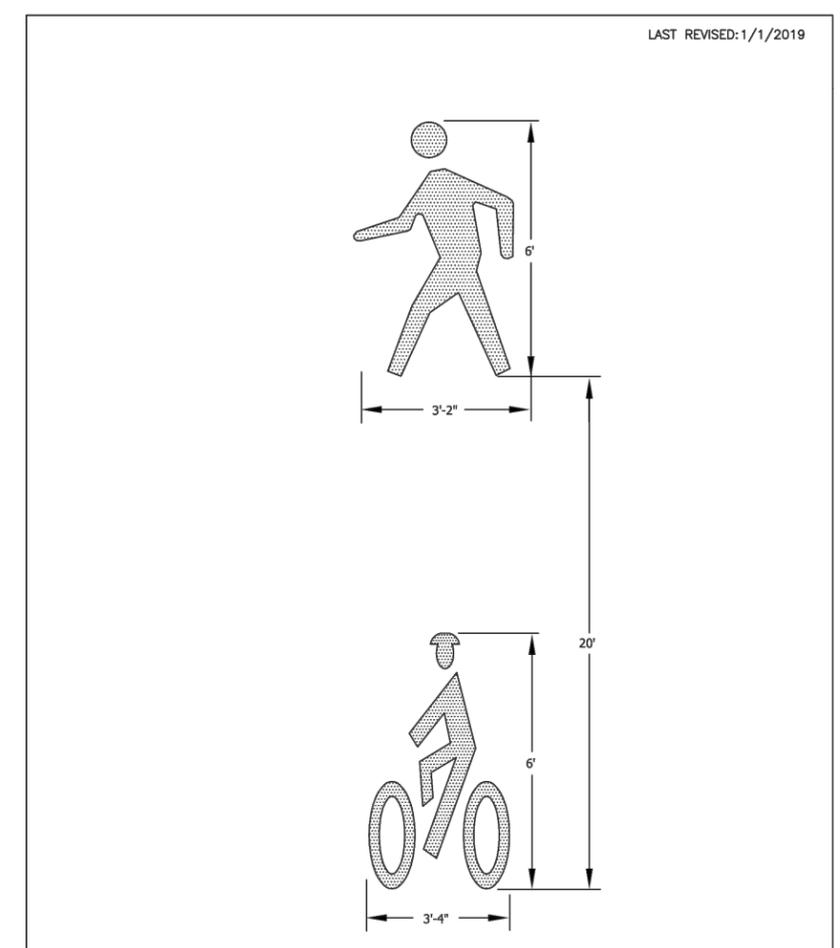
- NOTES:**
1. MATCH EXISTING PAVEMENT MARKING DIMENSIONS.
 2. SEE CK-R.30 FOR TWO-WAY LEFT TURN ARROW PLACEMENT.
 3. RAISED PAVEMENT MARKER BODY AND LENS COLOR SHALL CONFORM TO THE COLOR OF THE MARKING FOR WHICH THEY SUPPLEMENT, SUBSTITUTE FOR, OR SERVE AS A POSITIONING GUIDE FOR.

CITY OF KIRKLAND	
PLAN NO. CK-R.31	
	PAVEMENT MARKING DETAIL



- NOTES:**
1. BIKE LANE SYMBOLS AND ARROW MATERIAL SHALL BE 90 MILL, PREFORMED, SKID RESISTANT THERMOPLASTIC.
 2. BICYCLE SYMBOL FACES ROADWAY CENTERLINE.

CITY OF KIRKLAND	
PLAN NO. CK-R.34	
	BICYCLE LANE MARKINGS



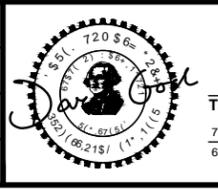
- NOTES:**
1. BIKE AND PEDESTRIAN LANE SYMBOLS MATERIAL SHALL BE 90 MILL, PERFORMED, SKID RESISTANT THERMOPLASTIC.
 2. BICYCLE AND PEDESTRIAN SYMBOLS FACES ROADWAY CENTERLINE.

CITY OF KIRKLAND	
PLAN NO. CK-R.34B	
	BICYCLE AND PEDESTRIAN LANE MARKINGS

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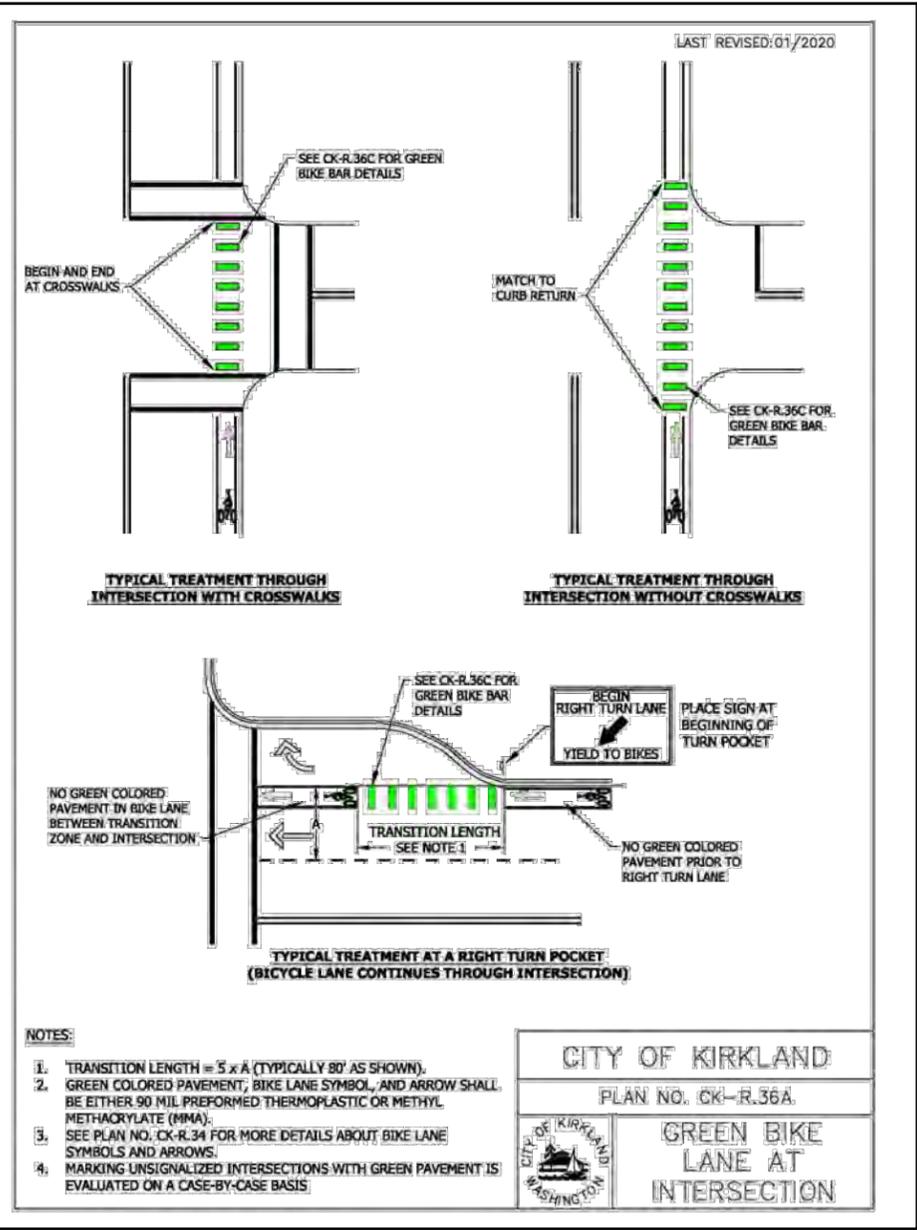
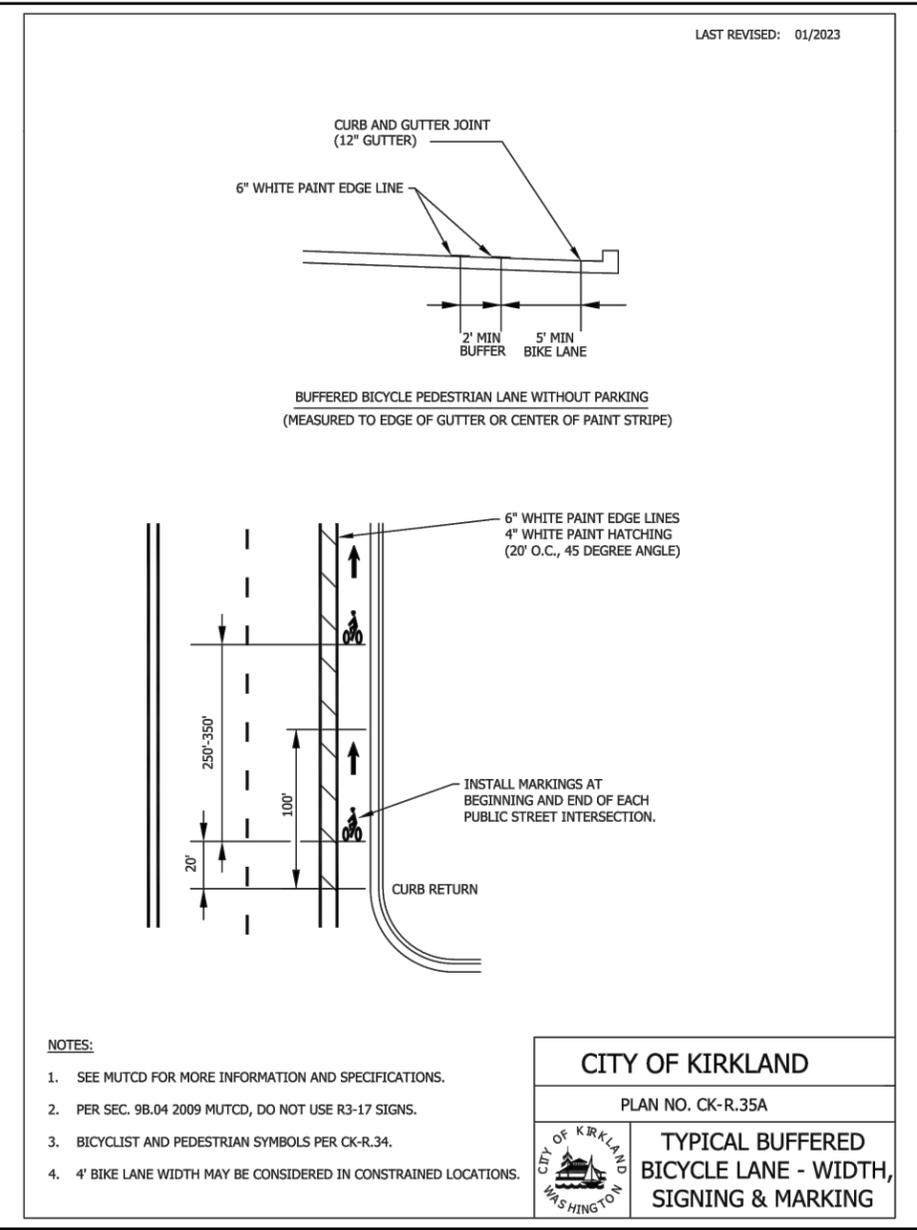
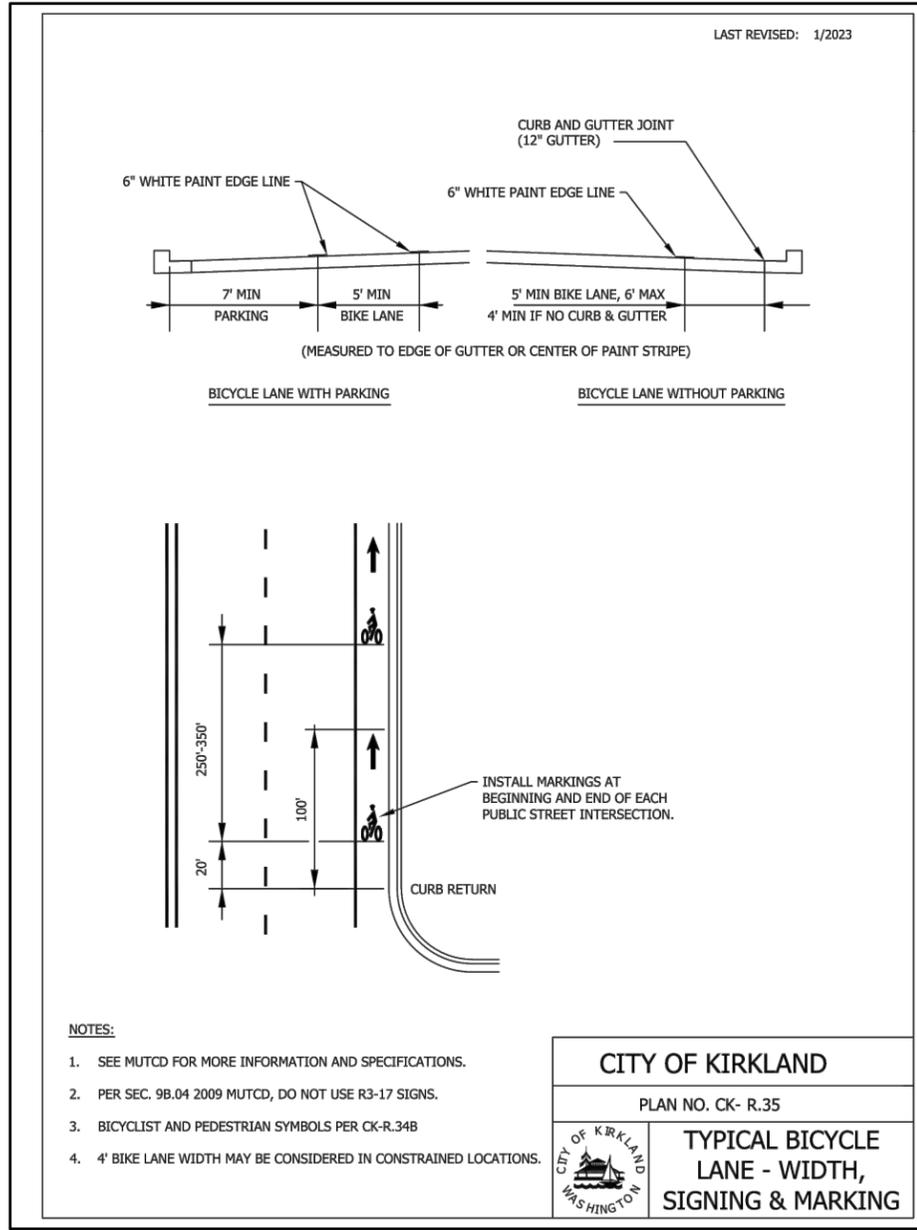
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**JUANITA DRIVE MULTIMODAL,
INTERSECTION & SAFETY IMPROVEMENTS**

&, 7 < 2) ., 5. / \$1'
STANDARD DETAILS

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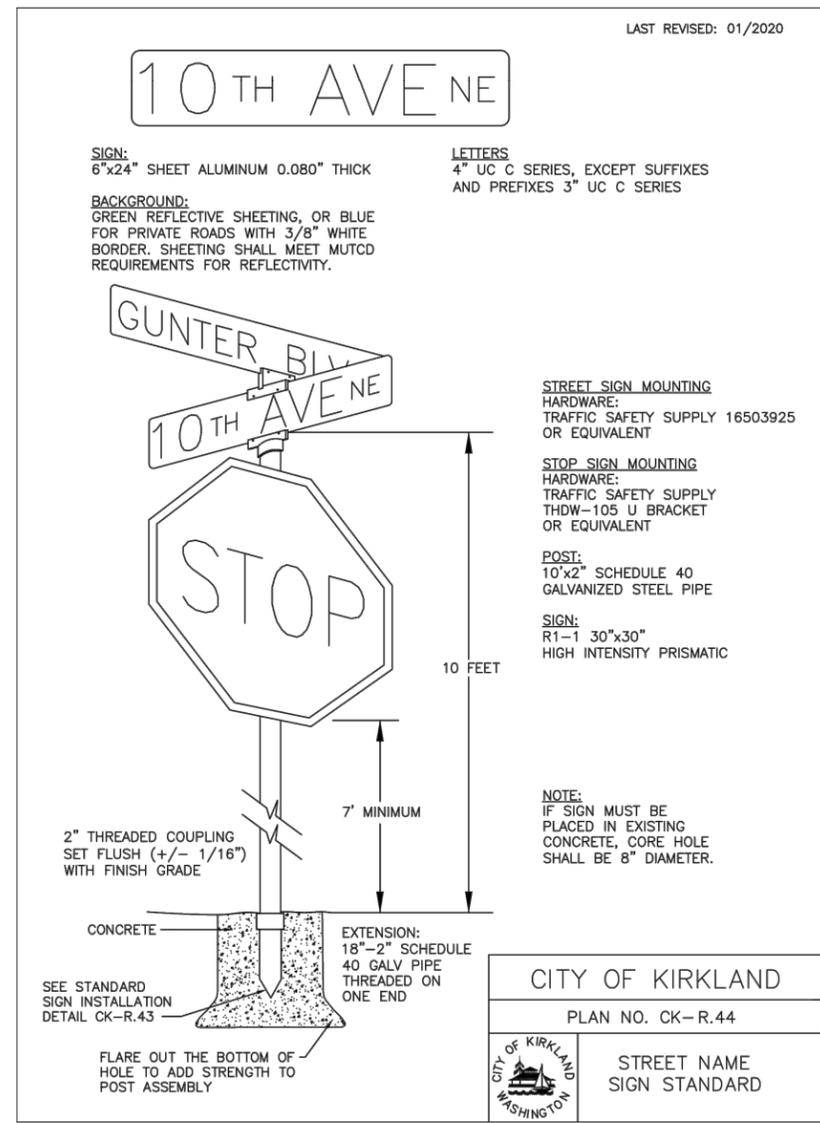
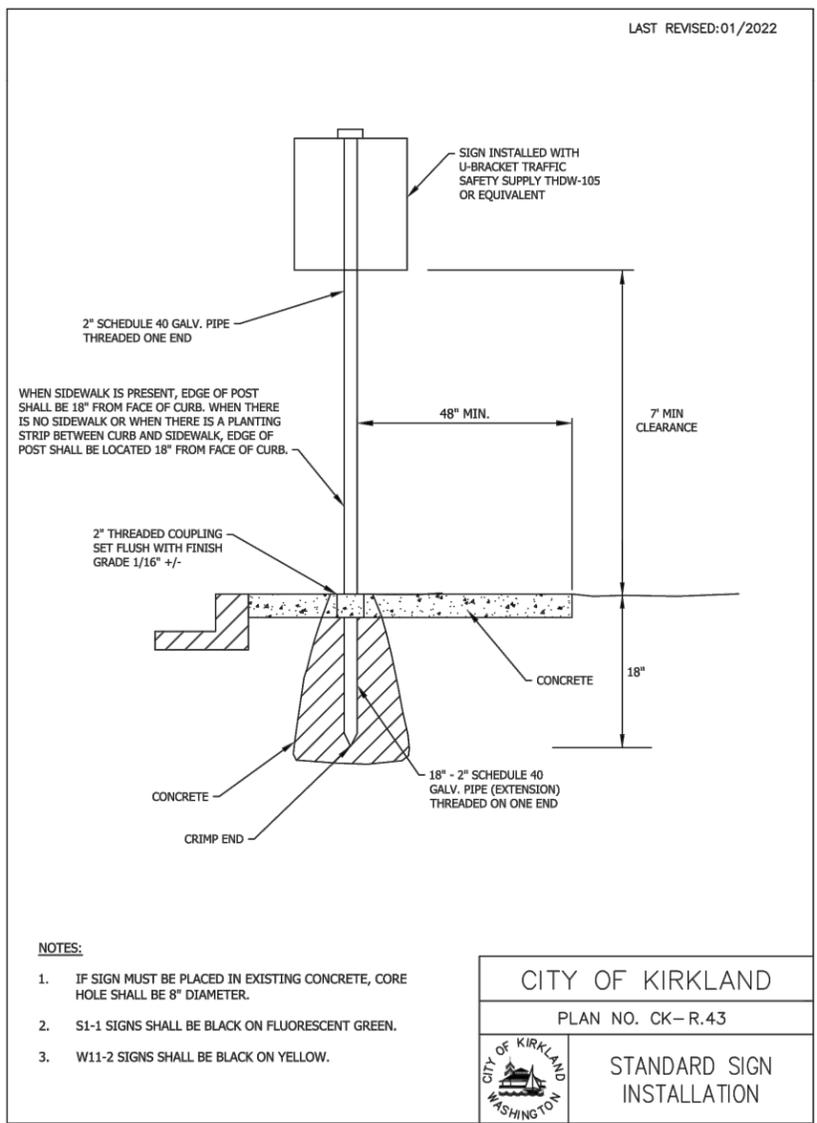
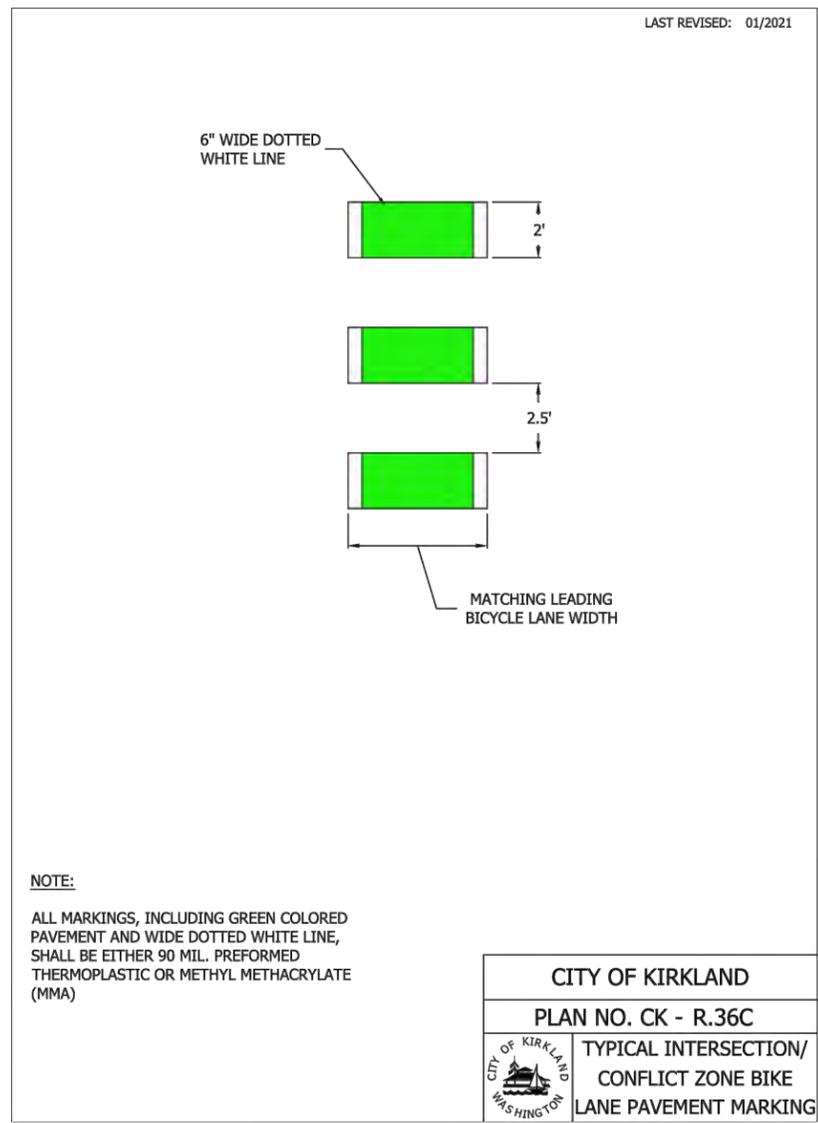
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JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS
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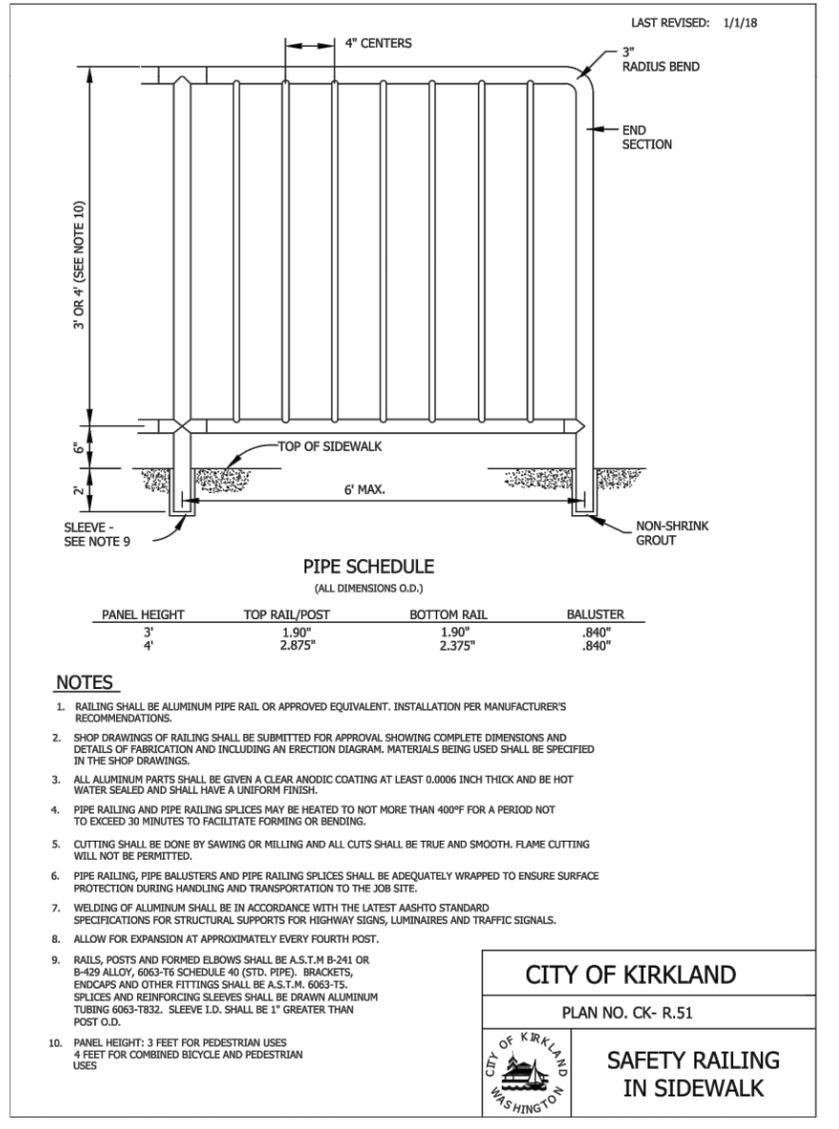
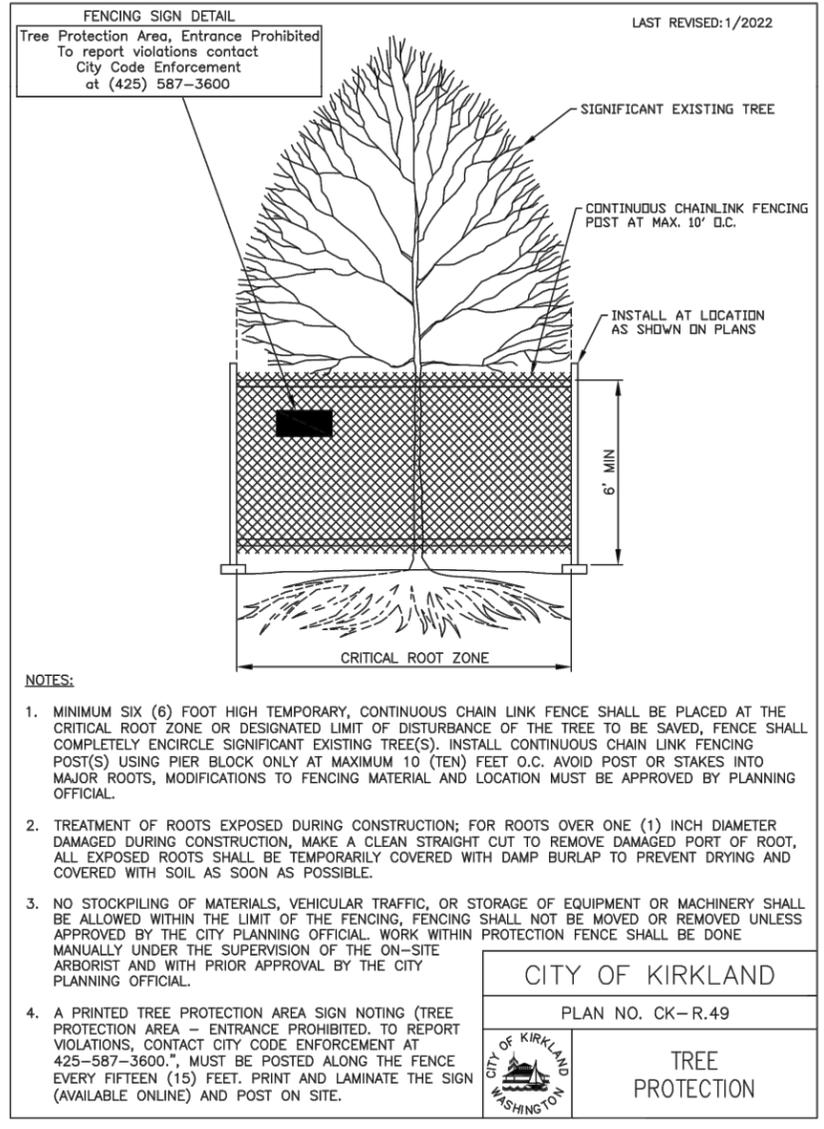
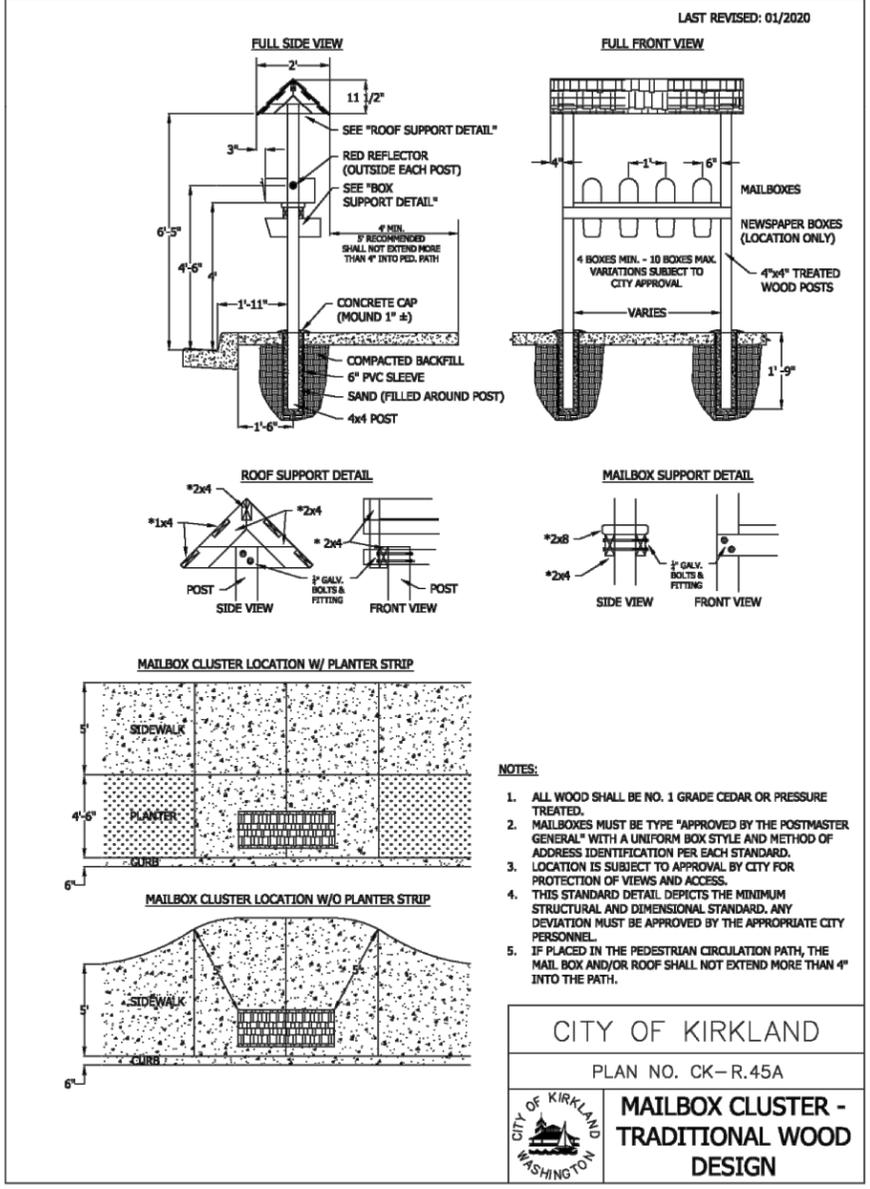
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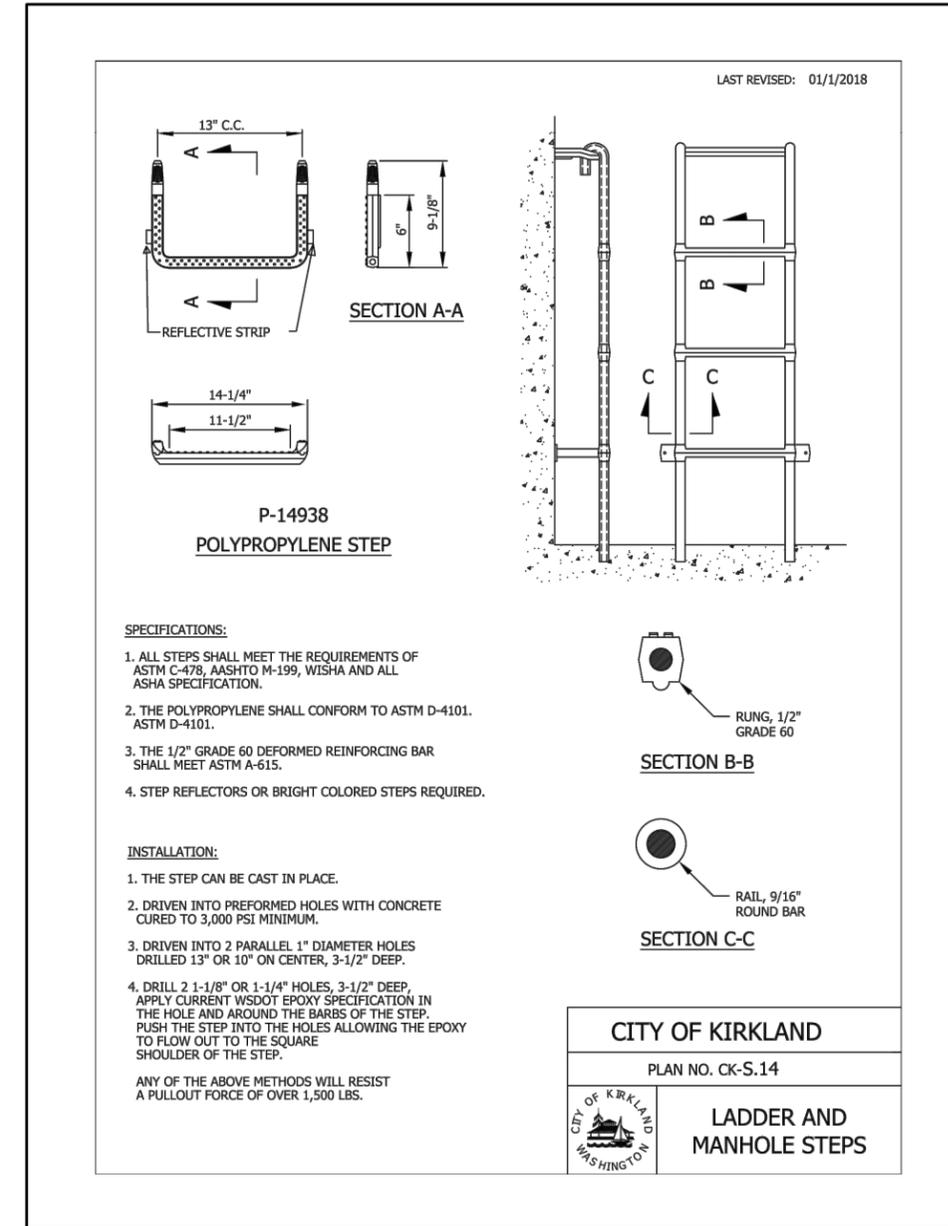
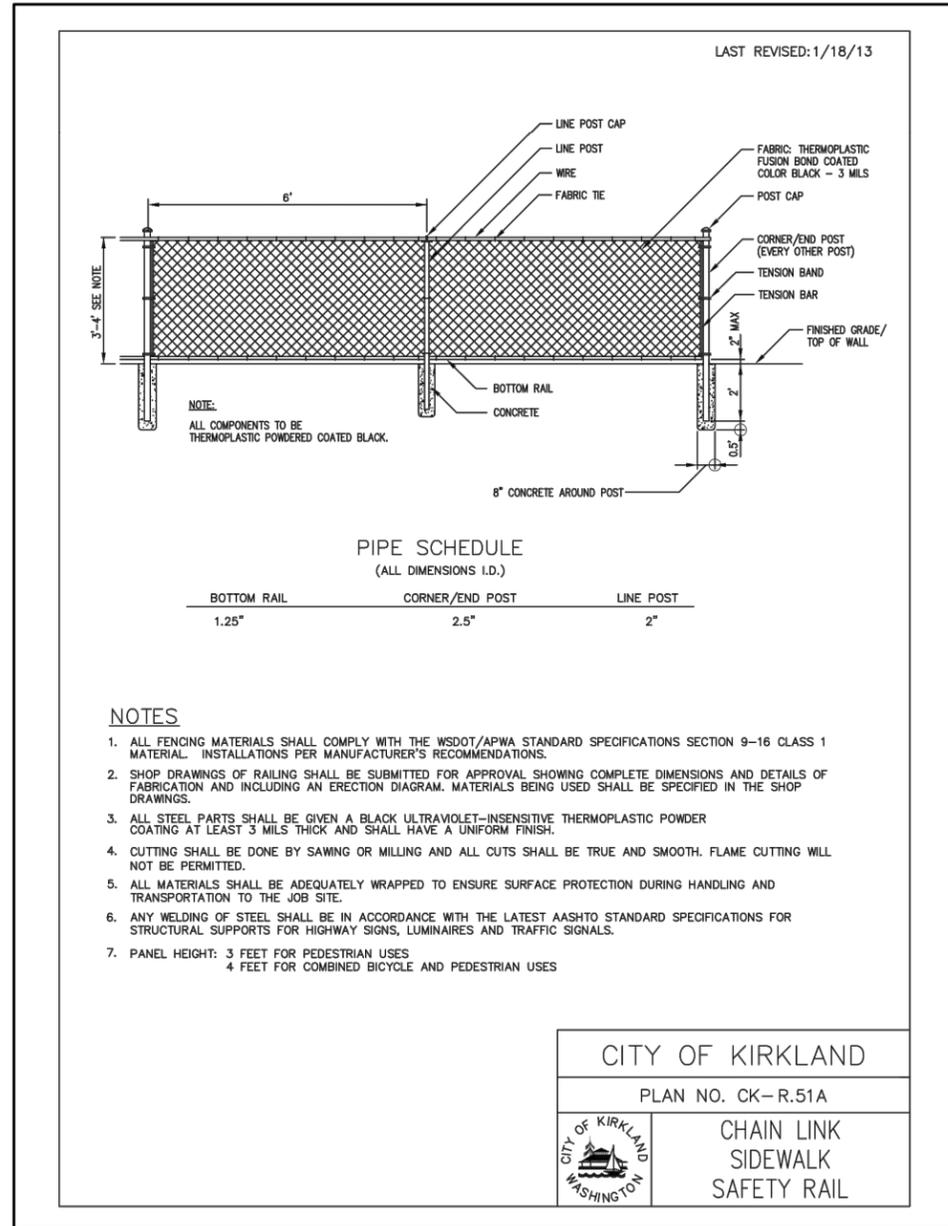
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JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

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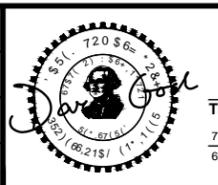
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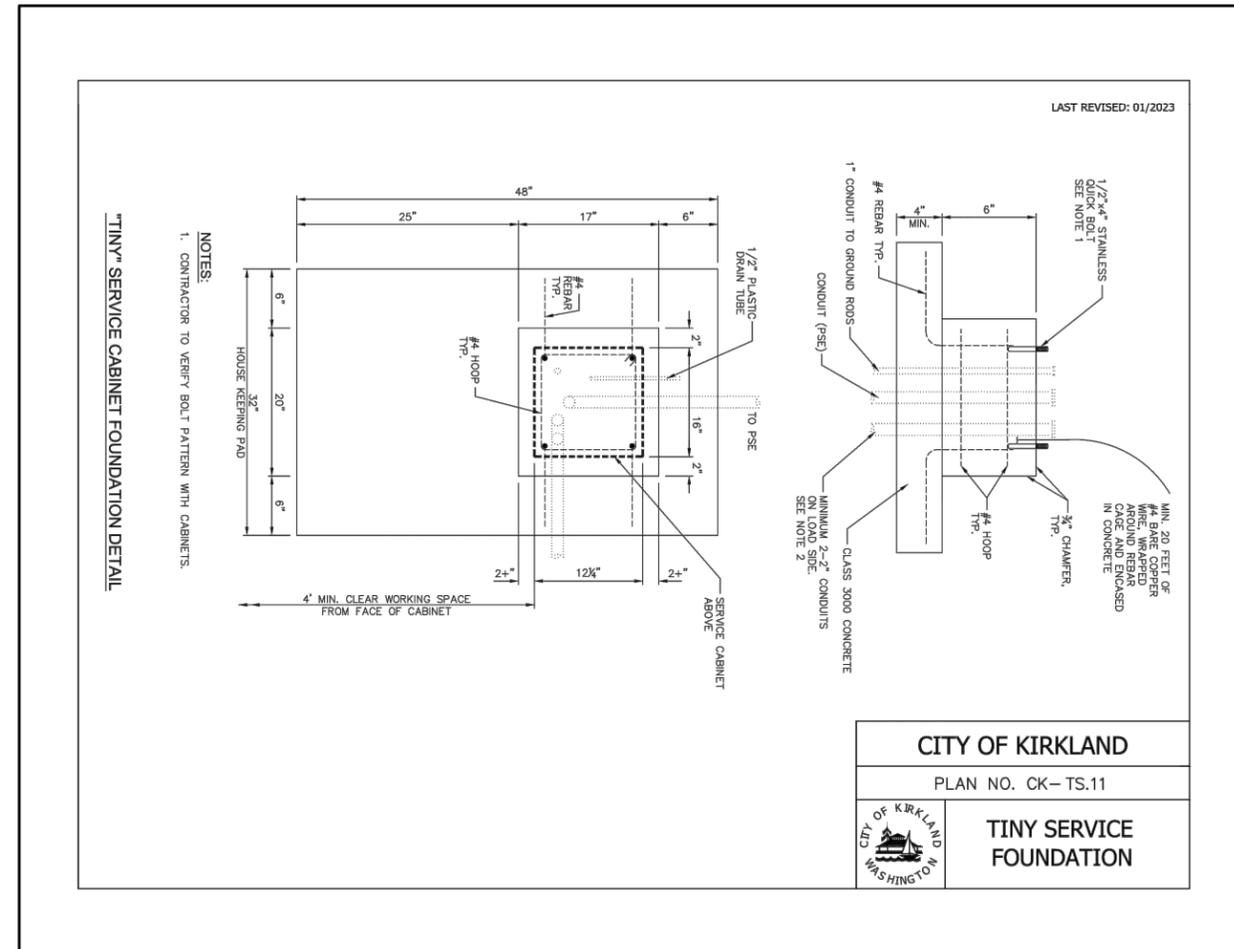
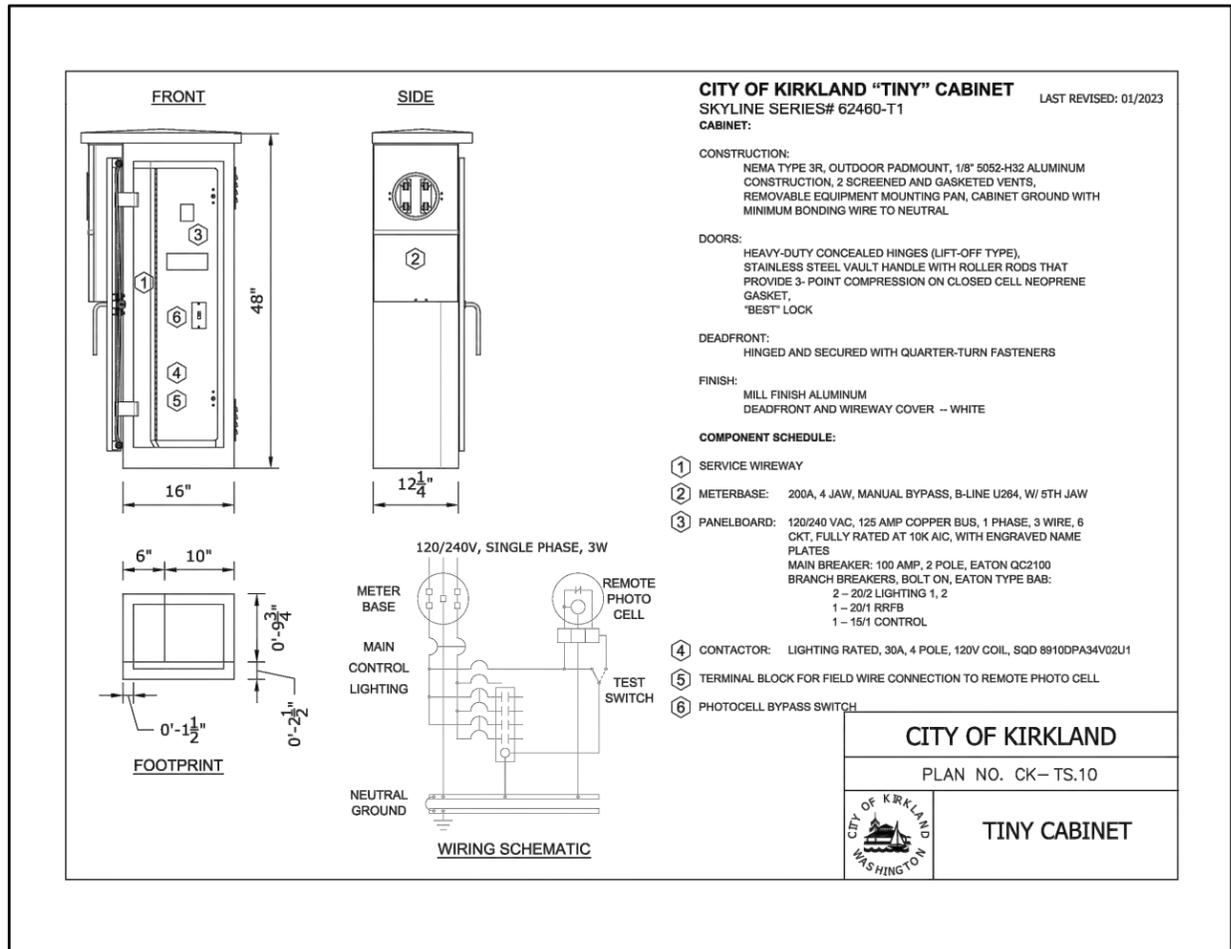
JUANITA DRIVE MULTIMODAL,
INTERSECTION & SAFETY IMPROVEMENTS

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STANDARD DETAILS

REFERENCE SHEET NO.	
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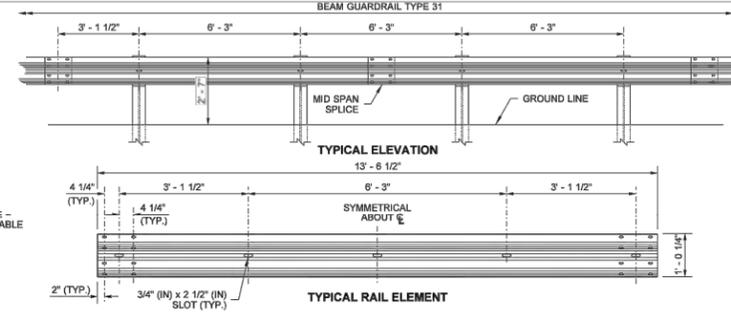
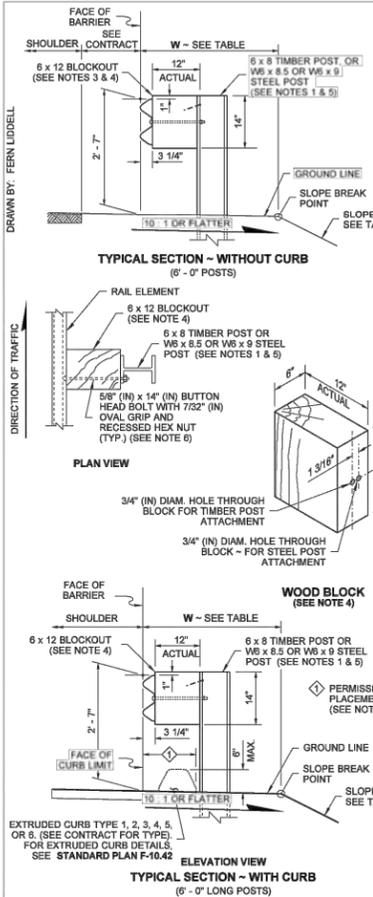
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**JUANITA DRIVE MULTIMODAL,
INTERSECTION & SAFETY IMPROVEMENTS**

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STANDARD DETAILS

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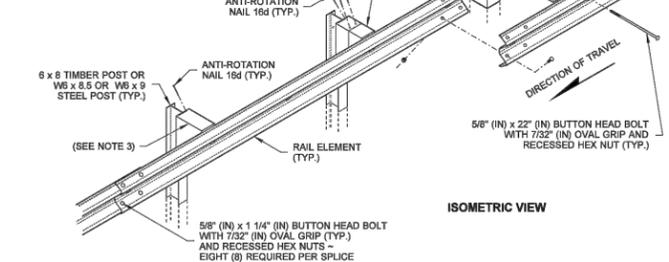
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SLOPE/EMBANKMENT TABLE FOR 6', 8', 9', AND 11' LONG POSTS

POST LENGTH	SLOPE	W (FT)
6-FOOT	2H: 1V OR FLATTER	2.5 MIN.
6-FOOT	1H: 1V OR FLATTER	4.0 MIN.
8-FOOT	STEEPER THAN 2H: 1V TO 1H: 1V	2.5 MIN.
8-FOOT	2H: 1V	0 (FACE OF BARRIER AT SLOPE BREAK POINT)
*9-FOOT	STEEPER THAN 2H: 1V TO 1.5H: 1V	0 (FACE OF BARRIER AT SLOPE BREAK POINT)
*11-FOOT	STEEPER THAN 1.5H: 1V TO 1H: 1V	0 (FACE OF BARRIER AT SLOPE BREAK POINT)

* MAY BE USED ONLY IF SHOWN IN CONTRACT DOCUMENTS



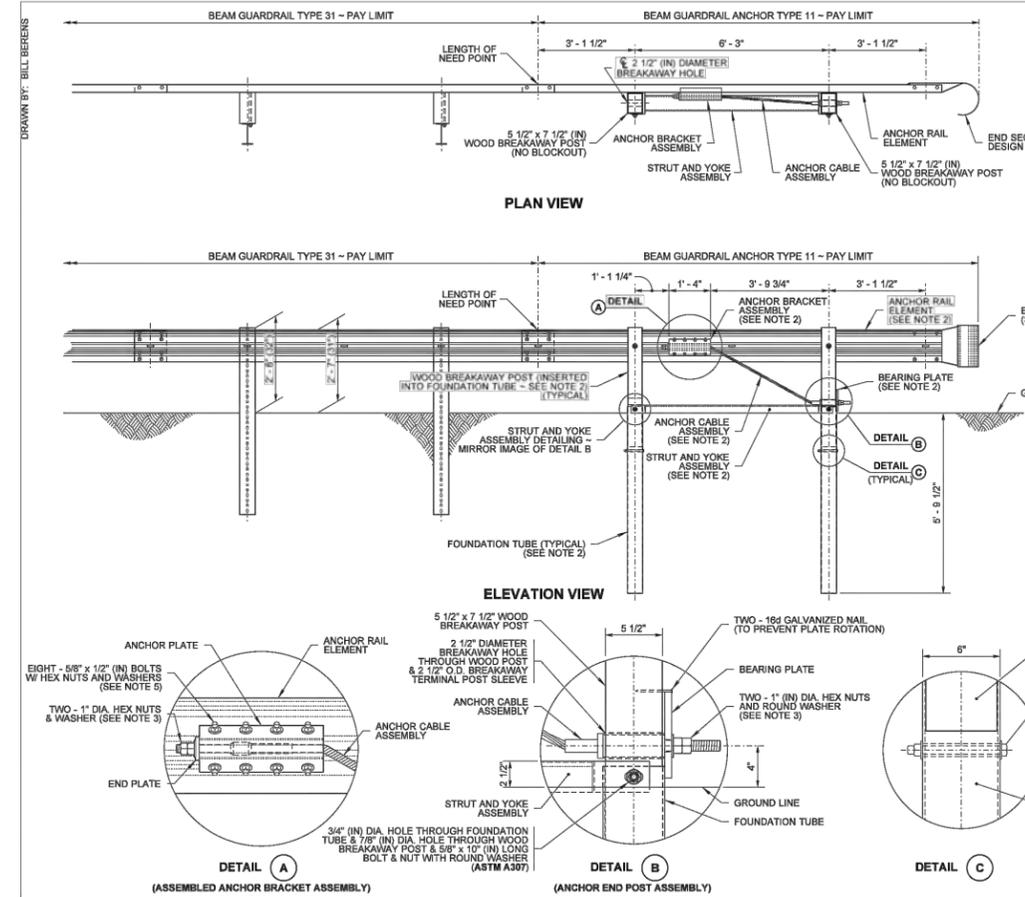
- NOTES**
- Refer to **Standard Plan C-1b** for additional details not shown on this plan.
 - Extend shoulder pavement to provide a base for the extruded curb. See Contract Plans for exceptions to distances shown.
 - Use a single block or combination of blocks (no more than two (2) to achieve the actual 12" (in) offset. See **Standard Specification, Section 9-16.3(2)**. Wood blocks shall be secured to the posts with anti-rotation nails. If combination blocks are used, the adjacent blocks shall be toenailed with two 16d galvanized nails to prevent block rotation.
 - Wood blocks are shown. Blocks of an approved alternative material may be used. See **Standard Specification, Section 9-16.3(2)**.
 - All posts for any standard barrier run shall be of the same type: timber or steel.
 - Attach blockouts to steel posts using bolt holes on approaching traffic side of post web.
 - Anti-rotation holes in steel posts are not required when using blocks with anti-rotation features (e.g., routed blocks).

**BEAM GUARDRAIL TYPE 31
STANDARD PLAN C-20.10-08**

SHEET 1 OF 1 SHEET

APPROVED FOR PUBLICATION
Mark Calvert
STATE DESIGN ENGINEER
Sep 8, 2022

Washington State Department of Transportation



- NOTES**
- For typical rail element and post details not shown on this plan refer to **Standard Plans C-20.10** and **C-1b**.
 - For additional details not shown, see **Sheet 2** of this Plan.
 - Fasten the Anchor Cable using two 1" (in) nuts and washer, at both ends of cable. Outside nut shall be torqued against inside nut a minimum of 100 ft.-lbs.
 - It is permissible to fabricate the anchor plate from 1/4" (in) thick plates welded to equal strength and dimensions as shown.
 - Eight 5/8" x 1 1/2" (in) machine bolts with hex nut and washer. Place washer on face side of rail.
 - Galvanizing of Anchor metal components shall be in accordance with **Standard Specification Section 9-16.3(6)**.

**BEAM GUARDRAIL (TYPE 31)
ANCHOR TYPE 11**

STANDARD PLAN C-23.70-00

SHEET 1 OF 2 SHEETS

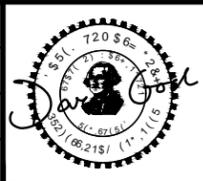
APPROVED FOR PUBLICATION
Mark Calvert
STATE DESIGN ENGINEER
Aug 22, 2022

Washington State Department of Transportation

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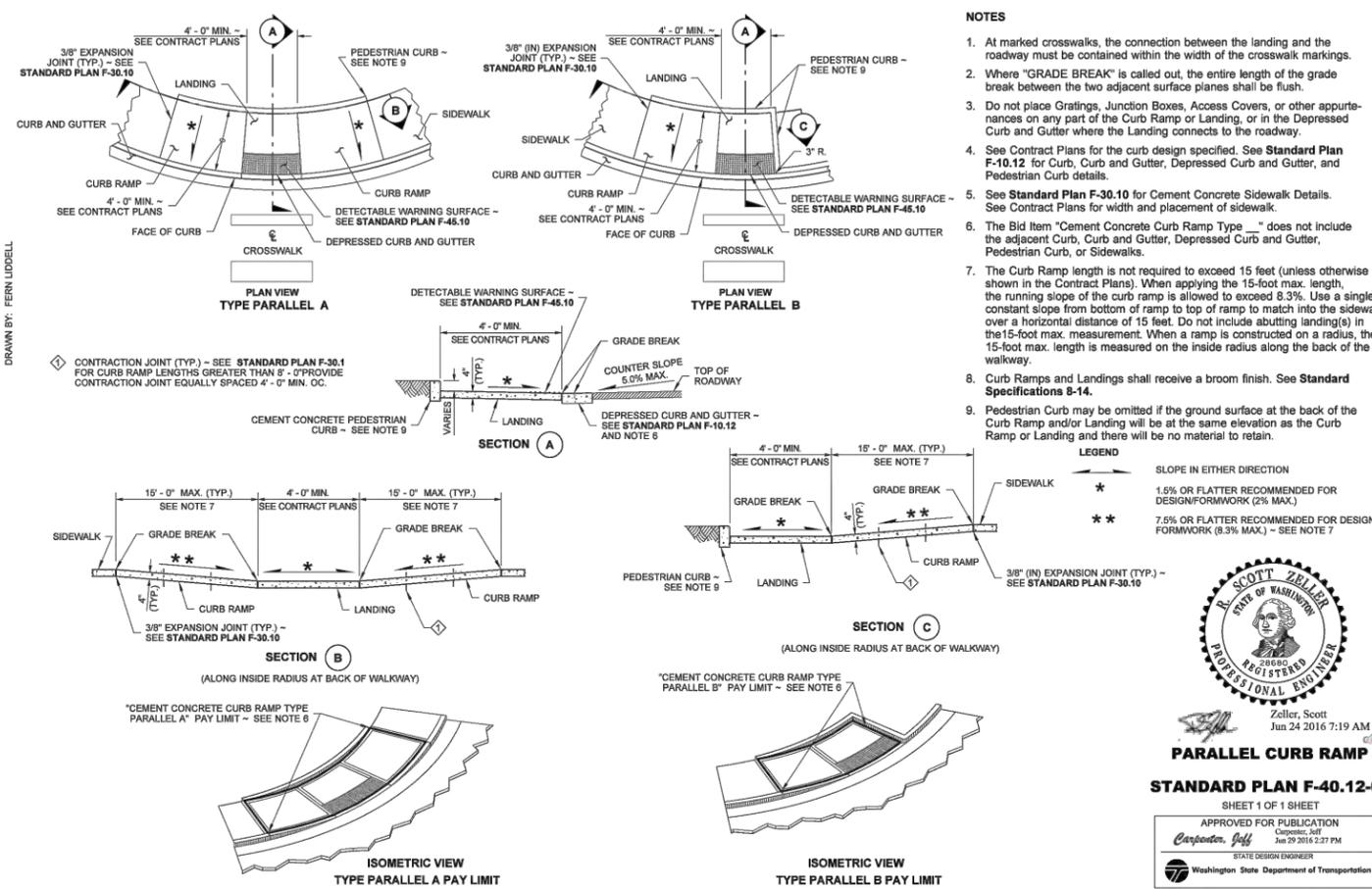
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**JUANITA DRIVE MULTIMODAL,
INTERSECTION & SAFETY IMPROVEMENTS**

WSDOT STANDARD PLANS

REFERENCE SHEET NO. SHEET 2) SHEETS

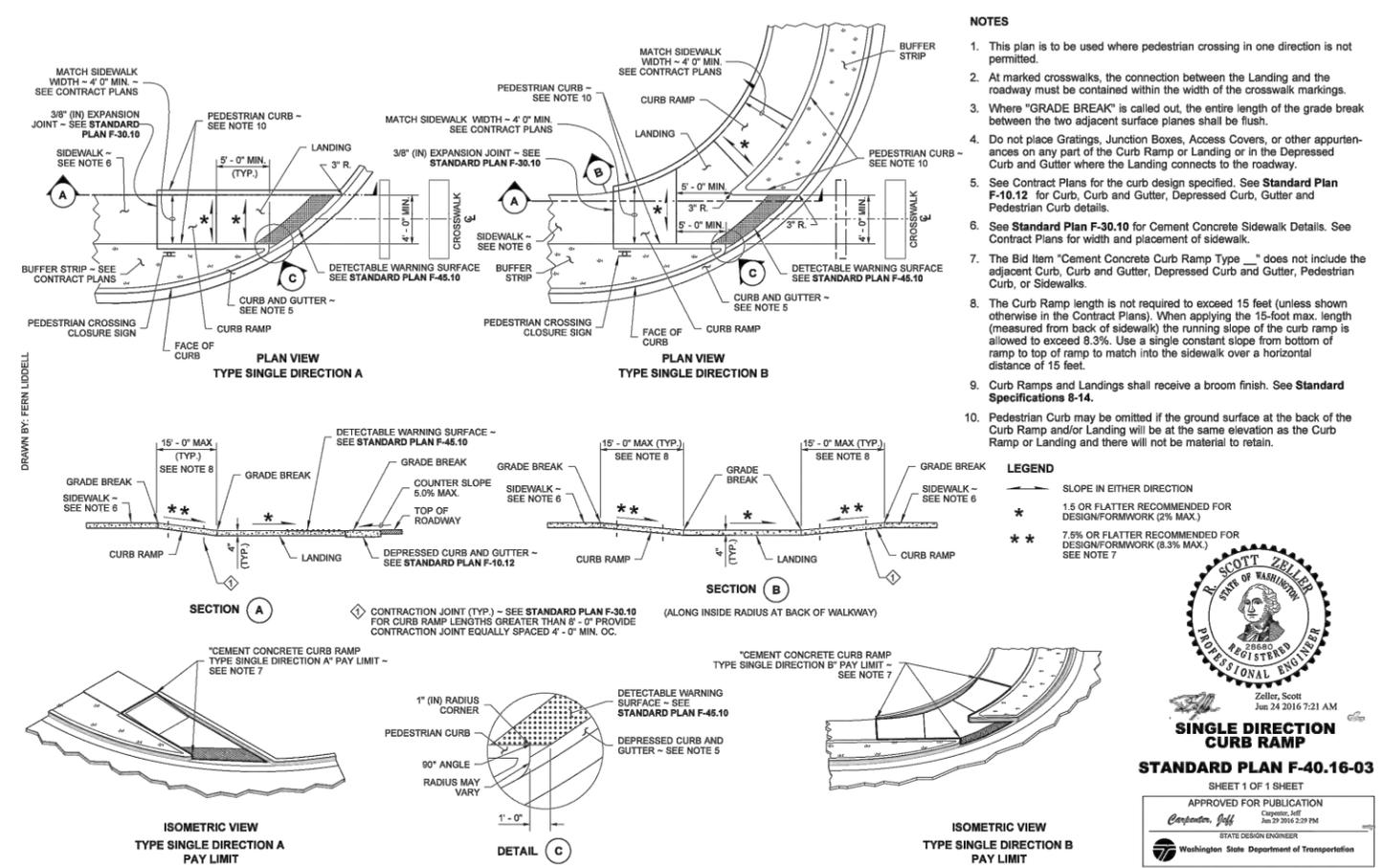
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- NOTES**
- At marked crosswalks, the connection between the landing and the roadway must be contained within the width of the crosswalk markings.
 - Where "GRADE BREAK" is called out, the entire length of the grade break between the two adjacent surface planes shall be flush.
 - Do not place Gratings, Junction Boxes, Access Covers, or other appurtenances on any part of the Curb Ramp or Landing, or in the Depressed Curb and Gutter where the Landing connects to the roadway.
 - See Contract Plans for the curb design specified. See **Standard Plan F-10.12** for Curb, Curb and Gutter, Depressed Curb and Gutter, and Pedestrian Curb details.
 - See **Standard Plan F-30.10** for Cement Concrete Sidewalk Details. See Contract Plans for width and placement of sidewalk.
 - The Bid Item "Cement Concrete Curb Ramp Type ___" does not include the adjacent Curb, Curb and Gutter, Depressed Curb and Gutter, Pedestrian Curb, or Sidewalks.
 - The Curb Ramp length is not required to exceed 15 feet (unless otherwise shown in the Contract Plans). When applying the 15-foot max. length, the running slope of the curb ramp is allowed to exceed 8.3%. Use a single constant slope from bottom of ramp to top of ramp to match into the sidewalk over a horizontal distance of 15 feet. Do not include abutting landing(s) in the 15-foot max. measurement. When a ramp is constructed on a radius, the 15-foot max. length is measured on the inside radius along the back of the walkway.
 - Curb Ramps and Landings shall receive a broom finish. See **Standard Specifications 8-14**.
 - Pedestrian Curb may be omitted if the ground surface at the back of the Curb Ramp and/or Landing will be at the same elevation as the Curb Ramp or Landing and there will be no material to retain.



Zeller, Scott
Jun 24 2016 7:19 AM
PARALLEL CURB RAMP
STANDARD PLAN F-40.12-03
SHEET 1 OF 1 SHEET



- NOTES**
- This plan is to be used where pedestrian crossing in one direction is not permitted.
 - At marked crosswalks, the connection between the Landing and the roadway must be contained within the width of the crosswalk markings.
 - Where "GRADE BREAK" is called out, the entire length of the grade break between the two adjacent surface planes shall be flush.
 - Do not place Gratings, Junction Boxes, Access Covers, or other appurtenances on any part of the Curb Ramp or Landing or in the Depressed Curb and Gutter where the Landing connects to the roadway.
 - See Contract Plans for the curb design specified. See **Standard Plan F-10.12** for Curb, Curb and Gutter, Depressed Curb, Gutter and Pedestrian Curb details.
 - See **Standard Plan F-30.10** for Cement Concrete Sidewalk Details. See Contract Plans for width and placement of sidewalk.
 - The Bid Item "Cement Concrete Curb Ramp Type ___" does not include the adjacent Curb, Curb and Gutter, Depressed Curb and Gutter, Pedestrian Curb, or Sidewalks.
 - The Curb Ramp length is not required to exceed 15 feet (unless shown otherwise in the Contract Plans). When applying the 15-foot max. length (measured from back of sidewalk) the running slope of the curb ramp is allowed to exceed 8.3%. Use a single constant slope from bottom of ramp to top of ramp to match into the sidewalk over a horizontal distance of 15 feet.
 - Curb Ramps and Landings shall receive a broom finish. See **Standard Specifications 8-14**.
 - Pedestrian Curb may be omitted if the ground surface at the back of the Curb Ramp and/or Landing will be at the same elevation as the Curb Ramp or Landing and there will be no material to retain.



Zeller, Scott
Jun 24 2016 7:21 AM
SINGLE DIRECTION CURB RAMP
STANDARD PLAN F-40.16-03
SHEET 1 OF 1 SHEET

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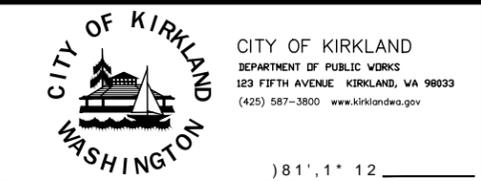


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KPG PSOMAS
Tacoma
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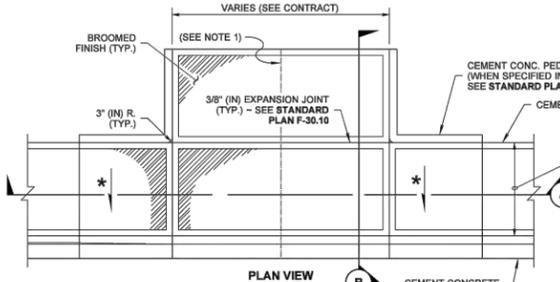


JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

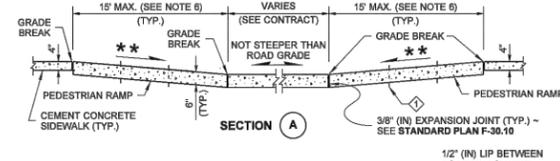
WSDOT STANDARD PLANS

REFERENCE SHEET NO.
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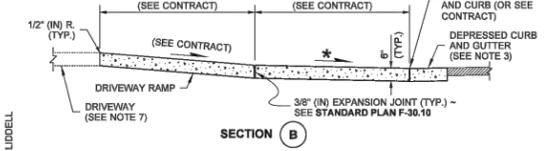
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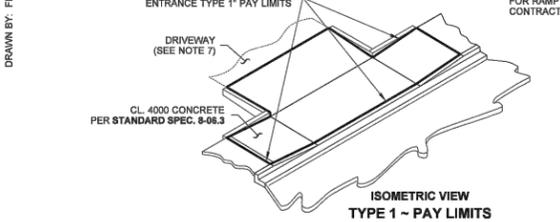
PLAN VIEW TYPE 1



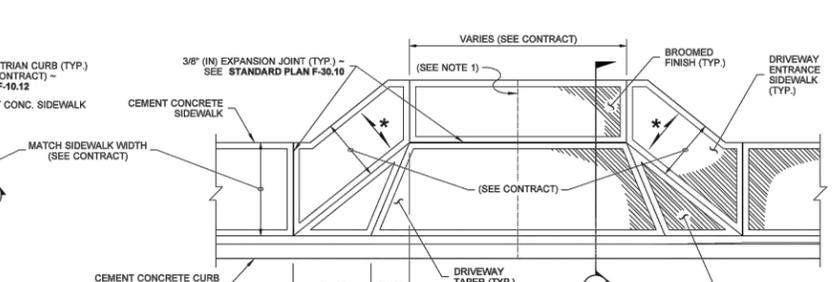
SECTION A



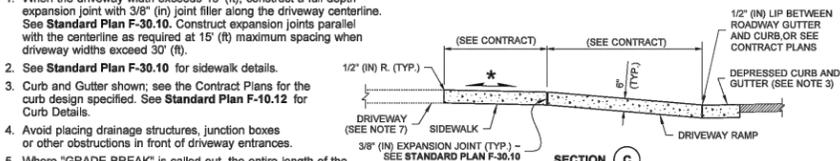
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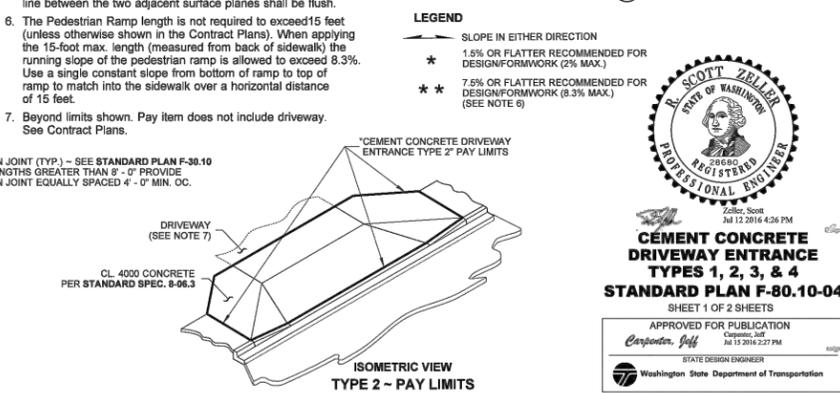
ISOMETRIC VIEW TYPE 1 - PAY LIMITS



PLAN VIEW TYPE 2



SECTION C



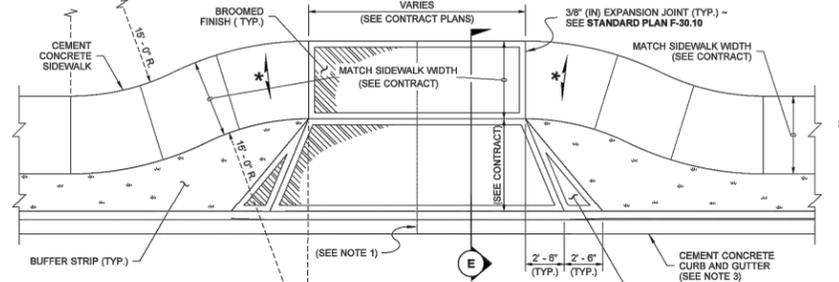
ISOMETRIC VIEW TYPE 2 - PAY LIMITS

- NOTES**
- When the driveway width exceeds 15' (ft), construct a full depth expansion joint with 3/8" (in) joint filler along the driveway centerline. See Standard Plan F-30.10. Construct expansion joints parallel with the centerline as required at 15' (ft) maximum spacing when driveway widths exceed 30' (ft).
 - See Standard Plan F-30.10 for sidewalk details.
 - Curb and Gutter shown; see the Contract Plans for the curb design specified. See Standard Plan F-10.12 for Curb Details.
 - Avoid placing drainage structures, junction boxes or other obstructions in front of driveway entrances.
 - Where "GRADE BREAK" is called out, the entire length of the line between the two adjacent surface planes shall be flush.
 - The Pedestrian Ramp length is not required to exceed 15 feet (unless otherwise shown in the Contract Plans). When applying the 15-foot max. length (measured from back of sidewalk) the running slope of the pedestrian ramp is allowed to exceed 8.3%. Use a single constant slope from bottom of ramp to top of ramp to match into the sidewalk over a horizontal distance of 15 feet.
 - Beyond limits shown. Pay Item does not include driveway. See Contract Plans.
- LEGEND**
- SLOPE IN EITHER DIRECTION
 - 1.5% OR FLATTER RECOMMENDED FOR DESIGN/FORMWORK (2% MAX.)
 - 7.5% OR FLATTER RECOMMENDED FOR DESIGN/FORMWORK (8.3% MAX.) (SEE NOTE 6)
- CONTRACTION JOINT (TYP.) - SEE STANDARD PLAN F-30.10 FOR RAMP LENGTHS GREATER THAN 9' - 0" PROVIDE CONTRACTION JOINT EQUALLY SPACED 4' - 0" MIN. OC.**

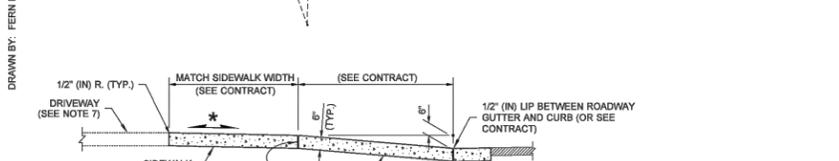


CEMENT CONCRETE DRIVEWAY ENTRANCE TYPES 1, 2, 3, & 4
STANDARD PLAN F-80.10-04

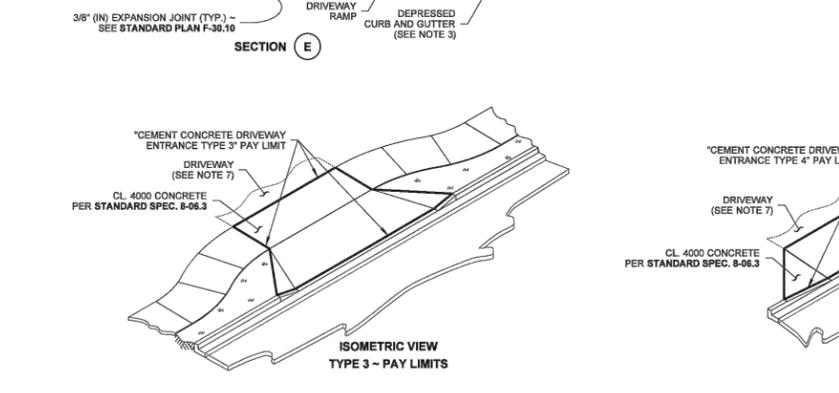
SHEET 1 OF 2 SHEETS
APPROVED FOR PUBLICATION
Carpenter, Jeff
MAY 13 2016 2:27 PM
STATE DESIGN ENGINEER
Washington State Department of Transportation



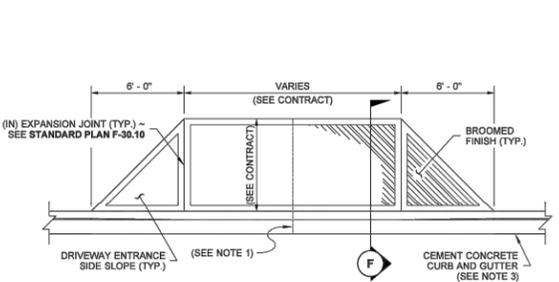
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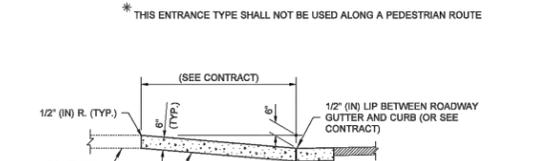
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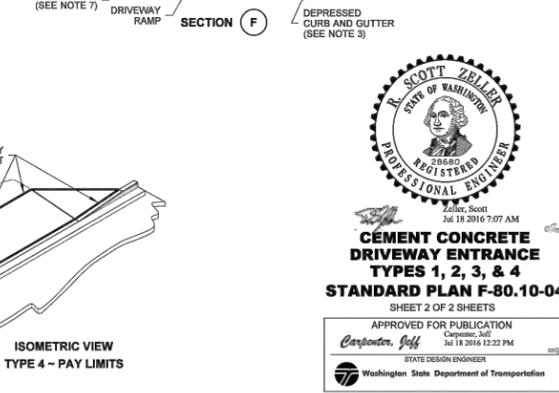
ISOMETRIC VIEW TYPE 3 - PAY LIMITS



PLAN VIEW TYPE 4*



SECTION F



ISOMETRIC VIEW TYPE 4 - PAY LIMITS



CEMENT CONCRETE DRIVEWAY ENTRANCE TYPES 1, 2, 3, & 4
STANDARD PLAN F-80.10-04

SHEET 2 OF 2 SHEETS
APPROVED FOR PUBLICATION
Carpenter, Jeff
MAY 13 2016 12:22 PM
STATE DESIGN ENGINEER
Washington State Department of Transportation

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KPG PSOMAS
Tacoma
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BID DOCUMENT

CITY OF KIRKLAND WASHINGTON

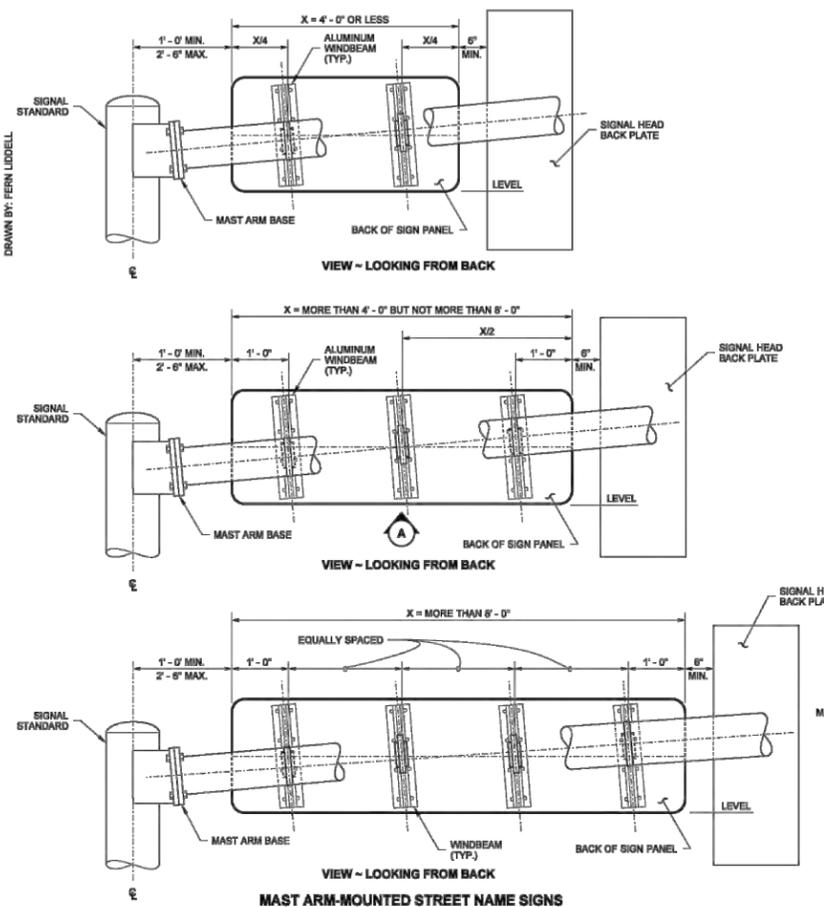
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123 FIFTH AVENUE KIRKLAND, WA 98033
(425) 587-3800 www.kirklandwa.gov

JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

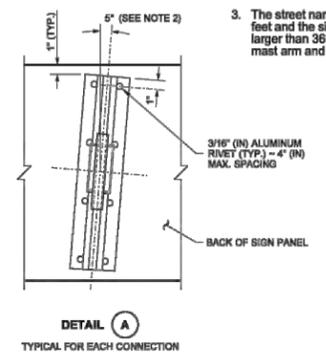
WSDOT STANDARD PLANS

REFERENCE SHEET NO.
SHEET 2)
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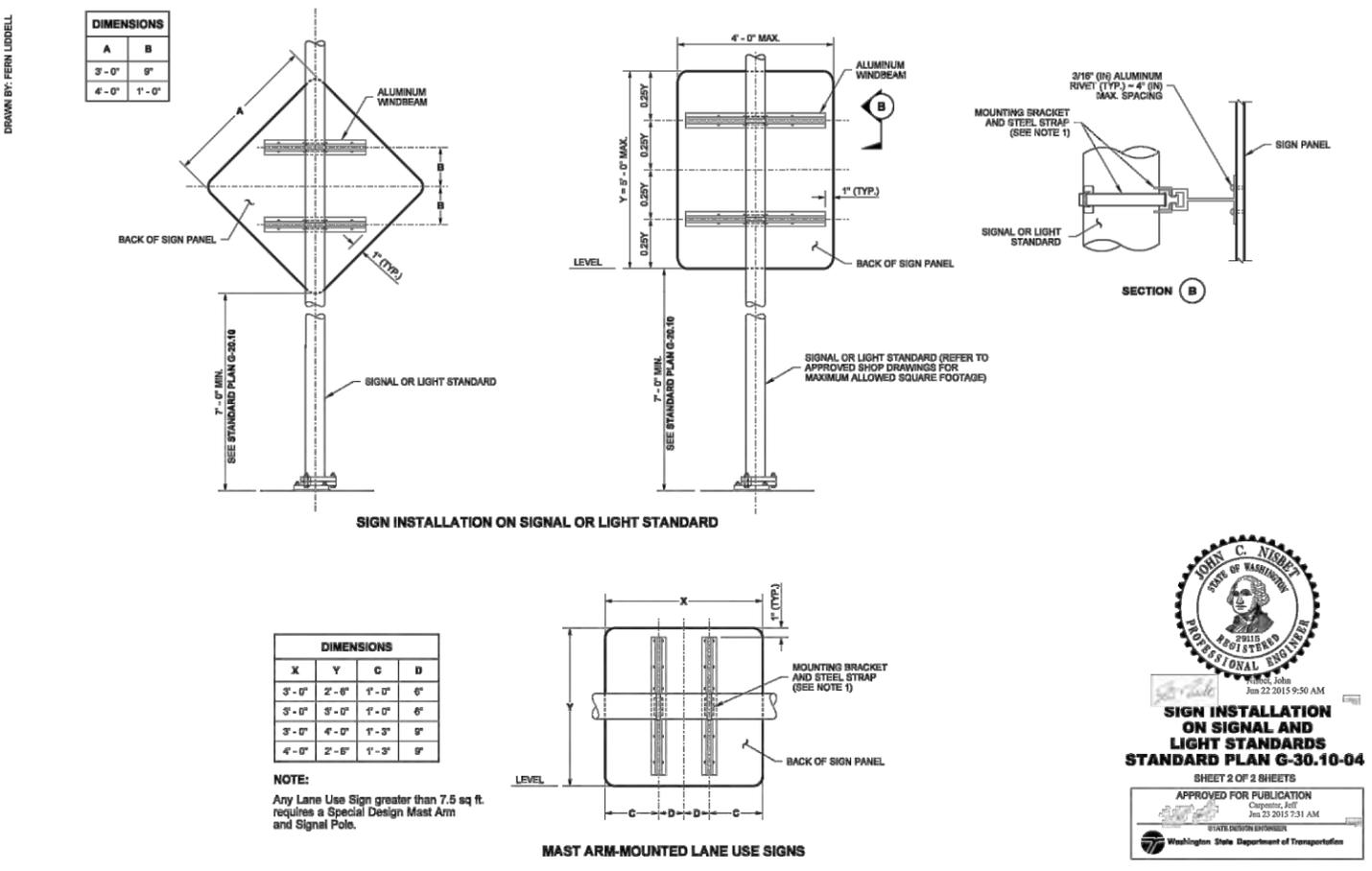
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- NOTES**
- Mounting brackets with steel straps shall be a stainless steel band and buckle system product or an approved equal. Mounting brackets shall be universal channel clamps; steel straps shall be 3/4" (in) wide and 0.030" (in) thick.
 - All signs installed on mast arms or standards (poles) require windbeams. All signs shall be installed with horizontal edges level. A skewed windbeam is required only when the sign is mounted within 12" (in) of the mast arm base (see Detail "A").
 - The street name sign shall be a maximum of 36 square feet and the sign height is a maximum of 3' (ft); signs larger than 36 square feet require a special design mast arm and signal pole.



SIGN INSTALLATION ON SIGNAL AND LIGHT STANDARDS
STANDARD PLAN G-30.10-04
 SHEET 1 OF 2 SHEETS
 APPROVED FOR PUBLICATION
 Carpenter, Jeff
 Jan 23 2015 7:31 AM
 STATE ENGINEER
 Washington State Department of Transportation

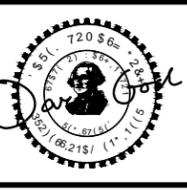


SIGN INSTALLATION ON SIGNAL AND LIGHT STANDARDS
STANDARD PLAN G-30.10-04
 SHEET 2 OF 2 SHEETS
 APPROVED FOR PUBLICATION
 Carpenter, Jeff
 Jan 23 2015 7:31 AM
 STATE ENGINEER
 Washington State Department of Transportation

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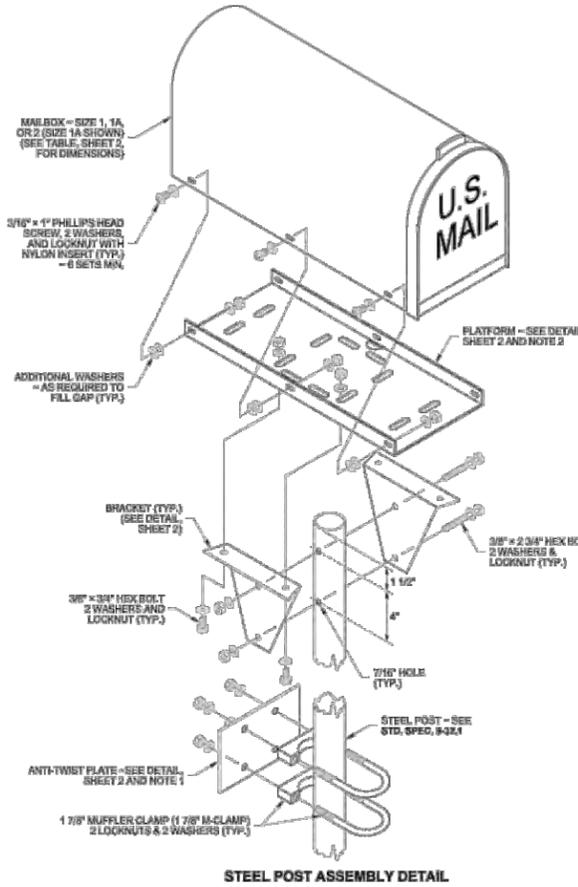


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JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS
WSDOT STANDARD PLANS
 REFERENCE SHEET NO. SHEET 2) SHEETS

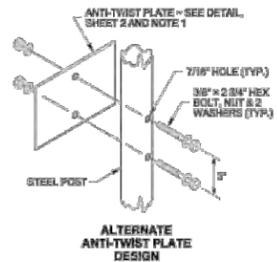
DRAWN BY: MARK SUZKA



SIZE / TYPE	QUANTITY	WASHERS	LOCKNUTS
3/8" DIAM. x 2 3/4" BOLT	2	4	2
3/8" DIAM. x 3/4" BOLT	4	8	4
3/16" DIAM. x 1" SCREW	6	12	6
1 7/8" M-CLAMP	2	4	4

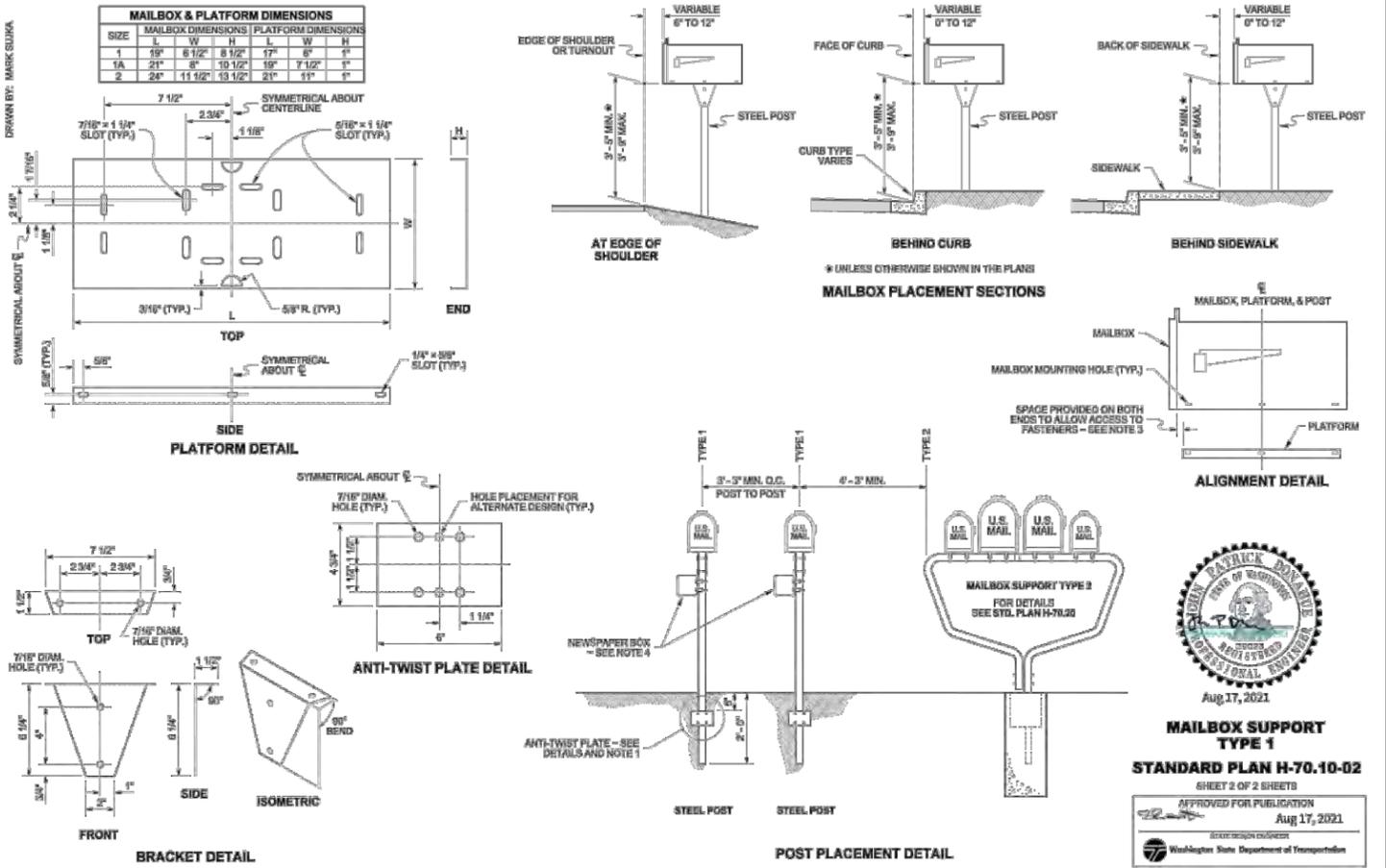
NOTES

1. The anchoring system shall meet MASH crash test criteria. The anti-twist plate anchoring system shown on this plan is deemed MASH compliant by WSDOT. The V-Wing socket and wedge assembly in a concrete base shown on Std. Plan H-70.20 is also deemed MASH compliant by WSDOT and may be substituted in lieu of the anti-twist plate designs shown. Other MASH compliant anchoring systems manufactured by or recommended by the Type 1 support manufacturer are allowed to be used in lieu of the anti-twist plate or V-wing socket and wedge assembly.
2. The platform design shown on this plan features slots that accommodate several types of mailbox supports; only those slots necessary for assembling the type being installed are required. An adjustable platform may be used in lieu of this design, but it must fit the bracket design shown on this plan. Brackets are required for all single-post installations. Field drilling may be necessary.
3. Center the mailbox on the platform to ensure space for the mailbox door to open and to allow space for installing the fasteners (see ALIGNMENT DETAIL, Sheet 2). Spacing of mailbox mounting holes varies among manufacturers. Attachment of the mailbox to the platform may require drilling additional holes through the platform to fit the platform.
4. Attach a newspaper box to a steel post with two 1 7/8" (n) Muffler Clamps spaced 4" (n) apart. Field drill 7/16" (n) holes in the newspaper box to fit. Newspaper boxes must not extend beyond the front of the mailbox when the mailbox door is closed.
5. A Type 2 Support (Standard Plan H-70.20) is required when 2 or more mailboxes are to be installed on one support.



Aug 17, 2021

MAILBOX SUPPORT TYPE 1
STANDARD PLAN H-70.10-02
 SHEET 1 OF 2 SHEETS
 APPROVED FOR PUBLICATION
 Aug 17, 2021
 DESIGNER: [Signature]
 WASHINGTON State Department of Transportation



Aug 17, 2021

MAILBOX SUPPORT TYPE 1
STANDARD PLAN H-70.10-02
 SHEET 2 OF 2 SHEETS
 APPROVED FOR PUBLICATION
 Aug 17, 2021
 DESIGNER: [Signature]
 WASHINGTON State Department of Transportation

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:6'27	:6'27



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 Tacoma
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BID DOCUMENT



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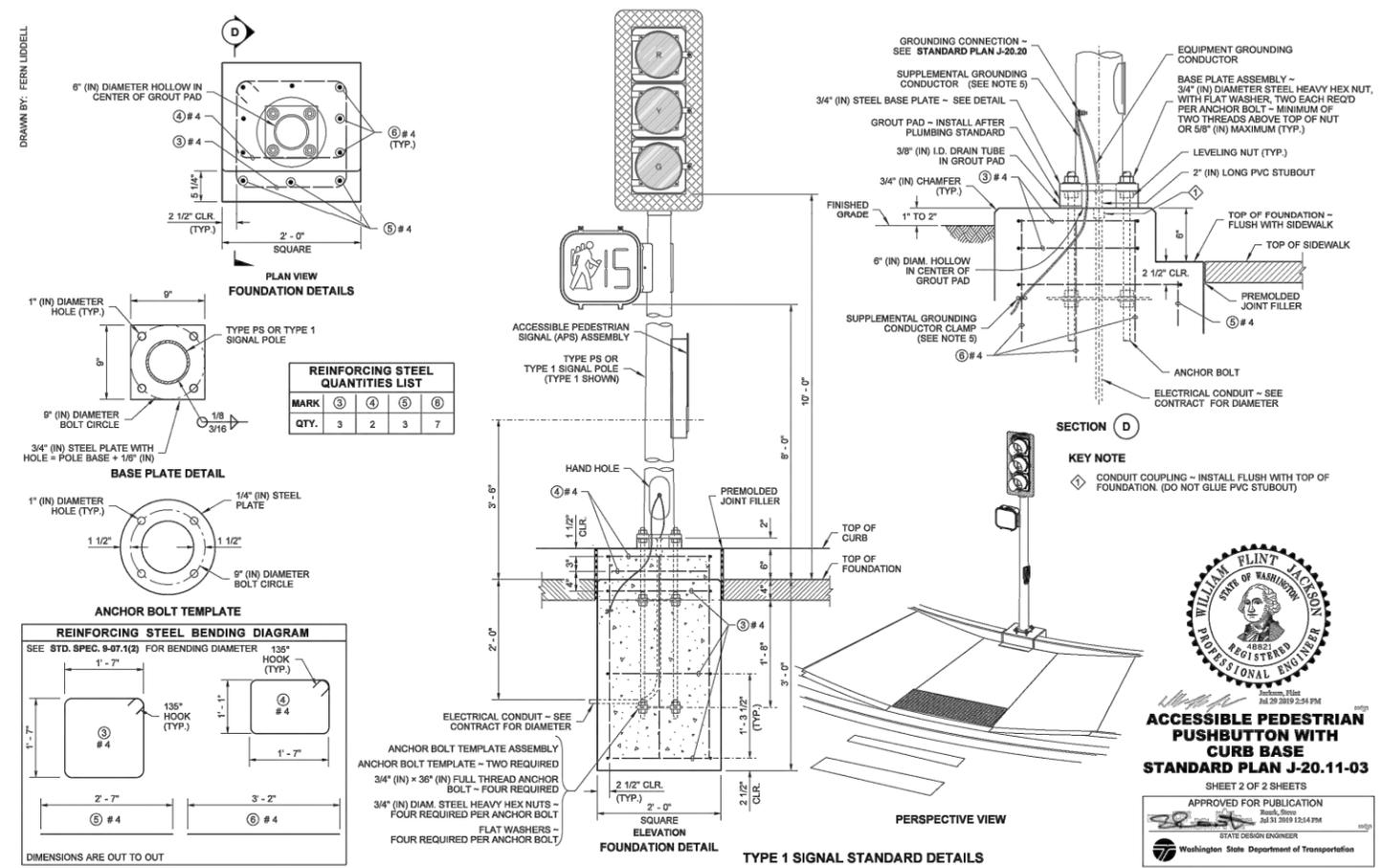
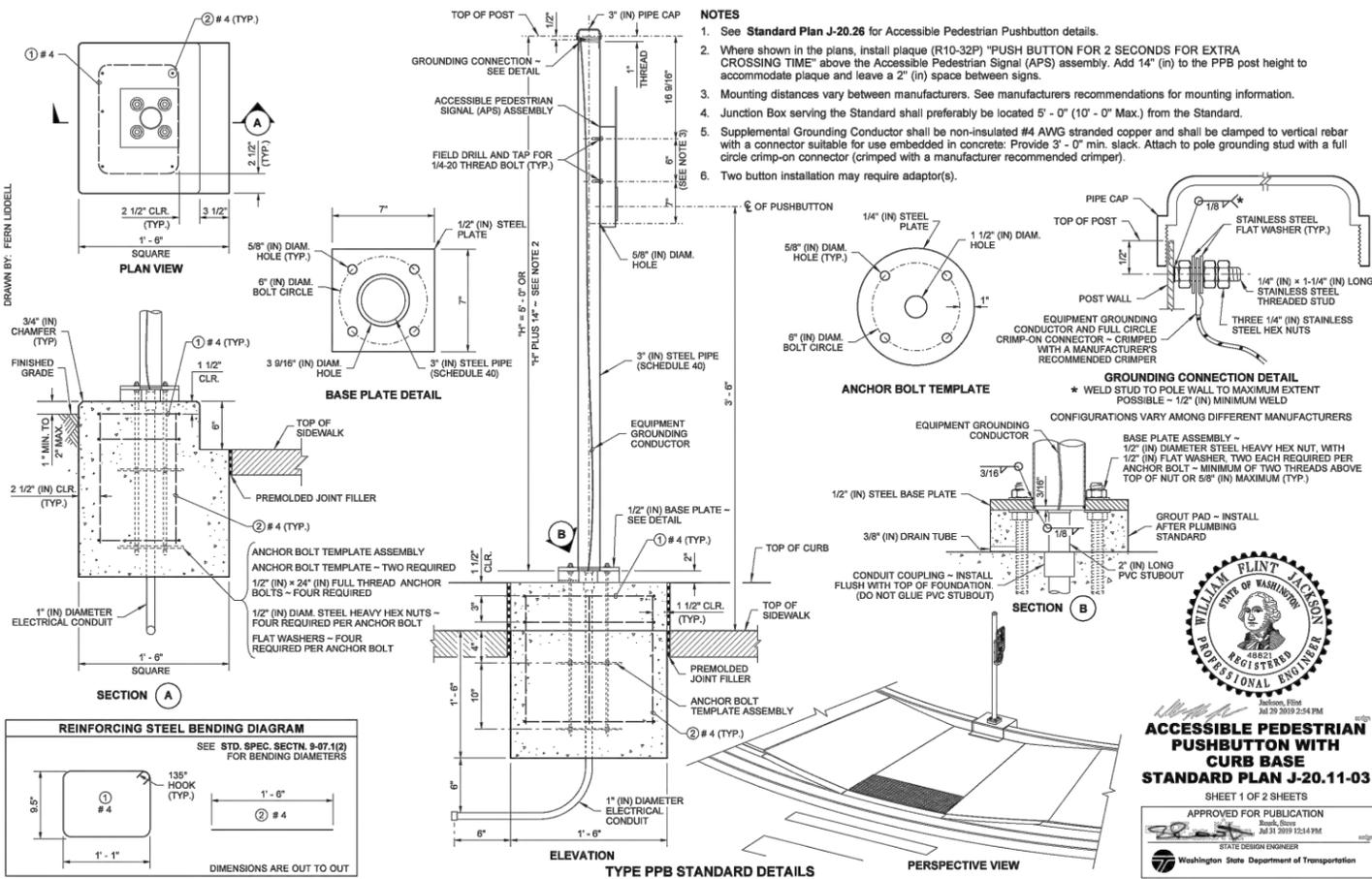
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JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

WSDOT STANDARD PLANS

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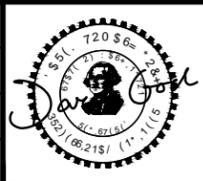
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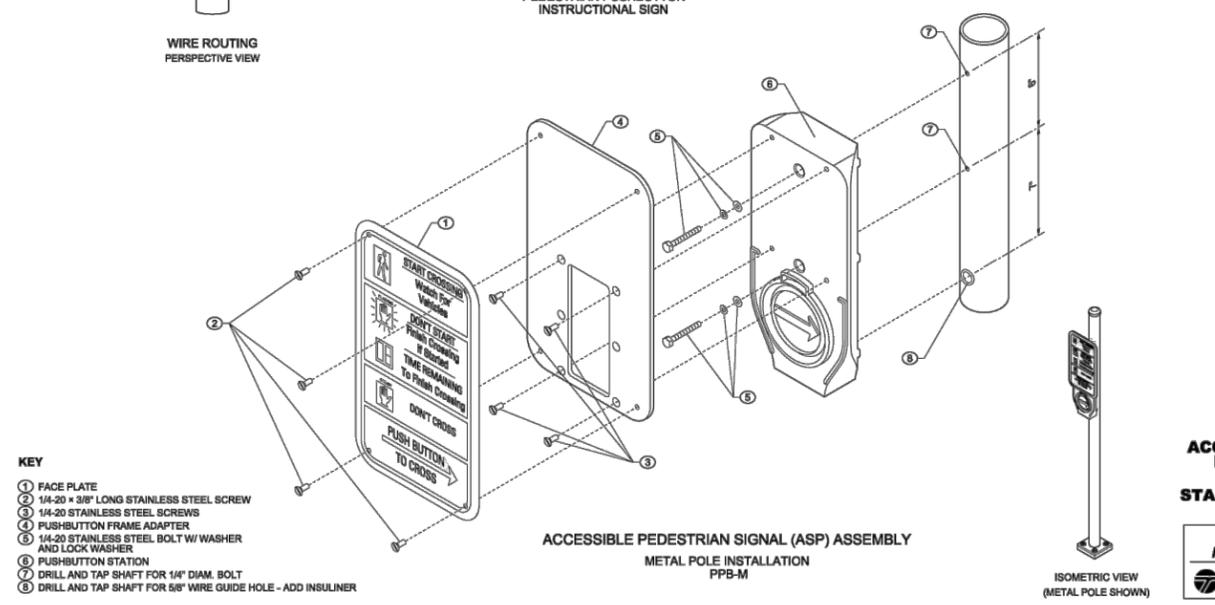
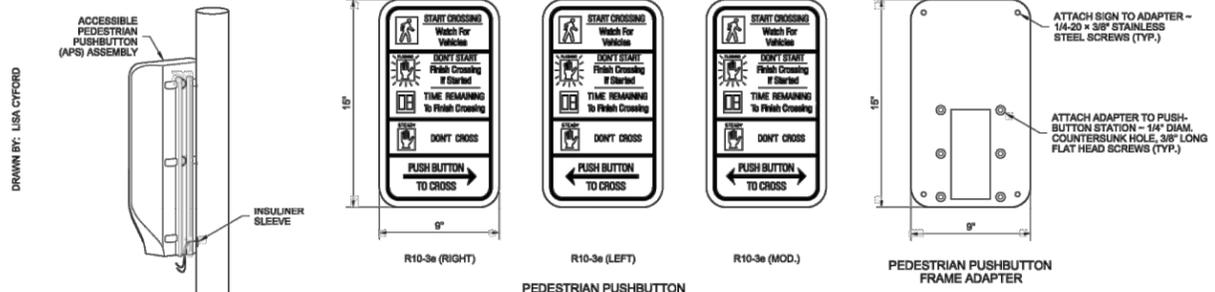


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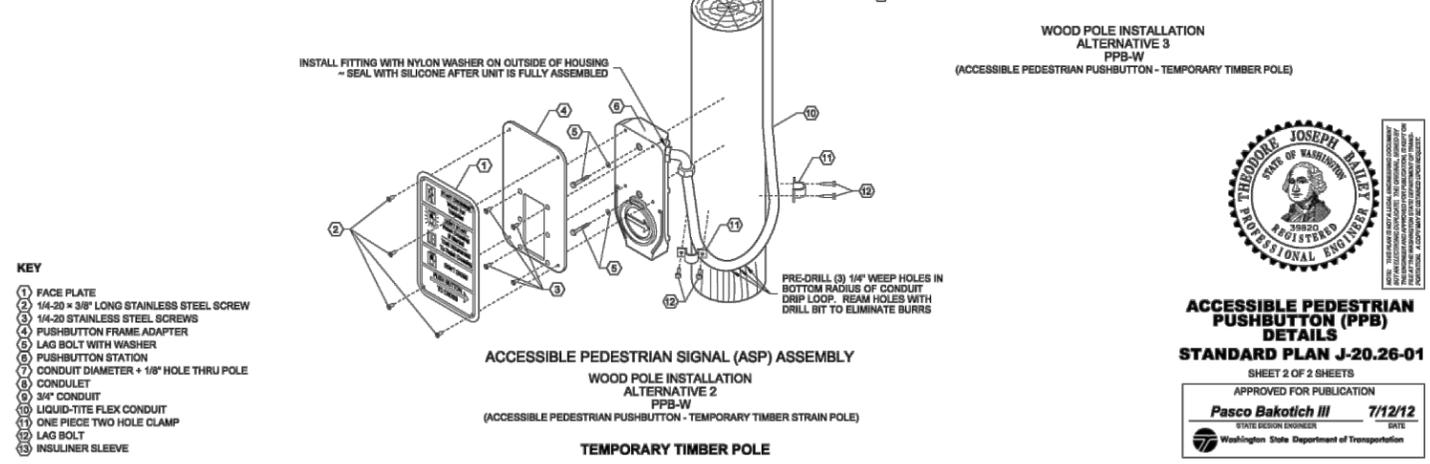
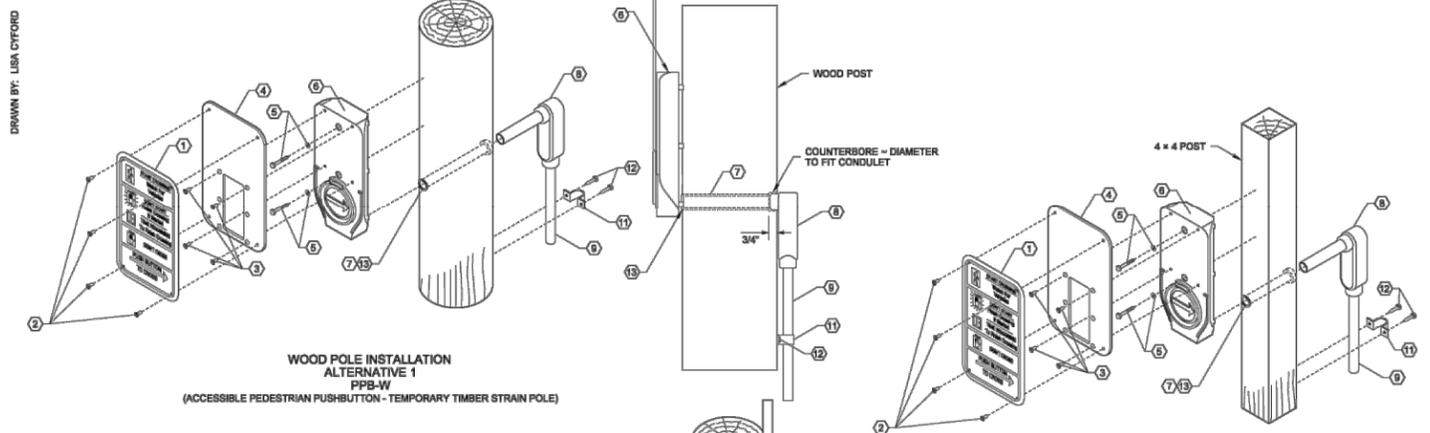
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WSDOT STANDARD PLANS
 REFERENCE SHEET NO. SHEET 2) SHEETS

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Professional Engineer Seal for Pasco Bakotich III, State of Washington, No. 38060, dated 7/12/12. Includes title 'ACCESSIBLE PEDESTRIAN PUSHBUTTON (PPB) DETAILS STANDARD PLAN J-20.26-01'.

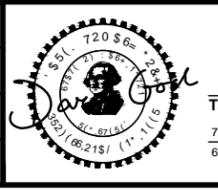


Professional Engineer Seal for Pasco Bakotich III, State of Washington, No. 38060, dated 7/12/12. Includes title 'ACCESSIBLE PEDESTRIAN PUSHBUTTON (PPB) DETAILS STANDARD PLAN J-20.26-01'.

Table with 4 columns and 10 rows, containing numerical data for various items.



Table with 2 columns and 4 rows, containing numerical data and percentages.



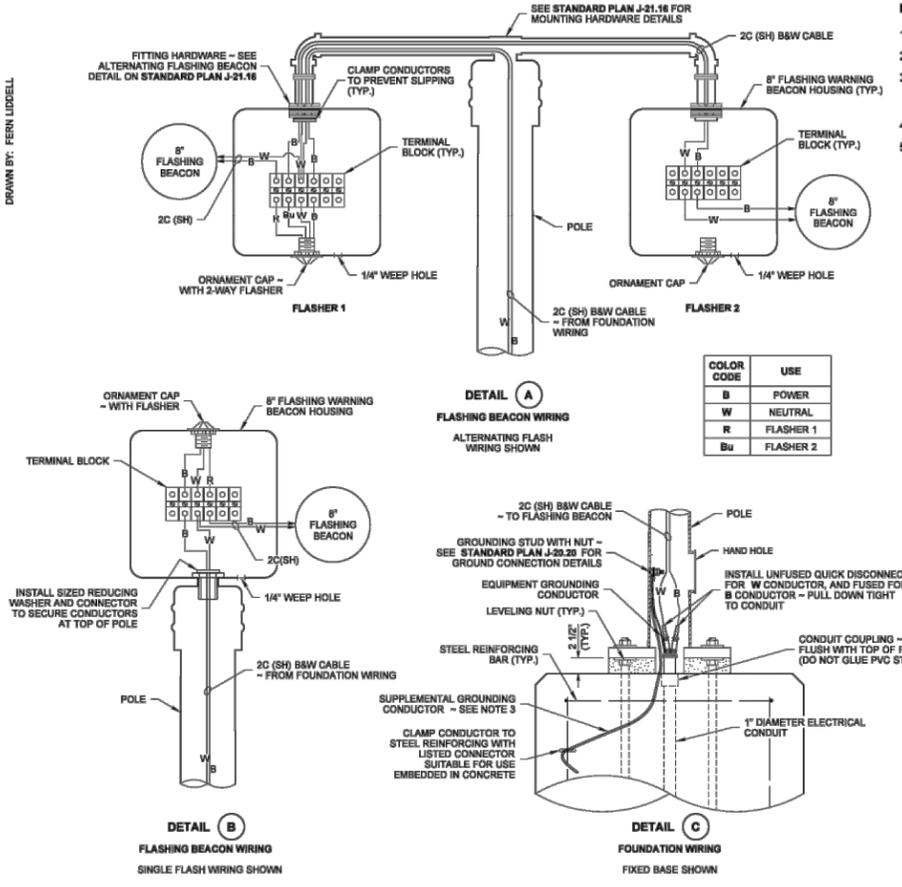
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JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS WSDOT STANDARD PLANS

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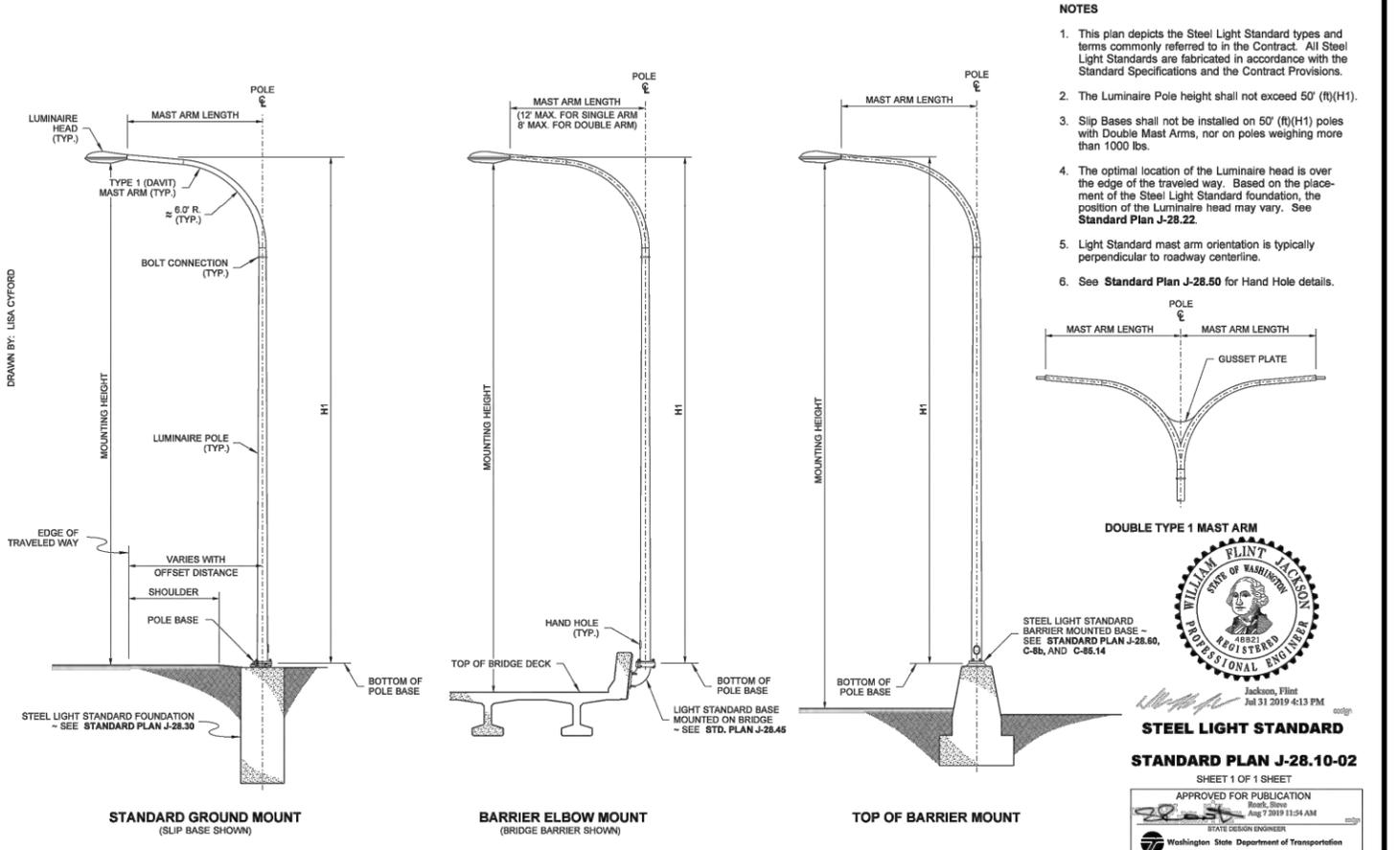
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NOTES

- See Standard Specification 9-29.3 for Cable Conductor requirements.
- See Standard Plan J-21.16 for Flashing Beacon Type 1 Signal Standard details.
- Supplemental Grounding Conductor shall be non-insulated #4 AWG stranded copper, provide 3' - 0" min. slack. Clamp to vertical steel reinforcing bar with listed connector suitable for use embedded in concrete.
- See Standard Plan J-21.10 for foundation requirements.
- Provide Cable Tie at wiring entering the Junction Box (for Slip Base Installations only) - See Detail A, Standard Plan J-28.70.

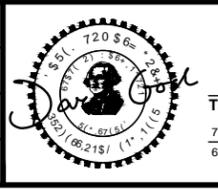
FLASHING BEACON TYPE 1 SIGNAL STANDARD ELECTRICAL DETAILS
 STANDARD PLAN J-21.17-01
 SHEET 1 OF 1 SHEET
 APPROVED FOR PUBLICATION
 Pasco Bakotich III 6/10/13
 STATE DESIGN ENGINEER
 Washington State Department of Transportation



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811
 Know what's below.
 Call 811 before you dig.

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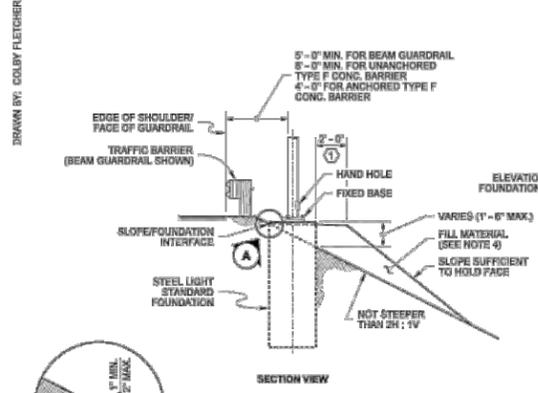
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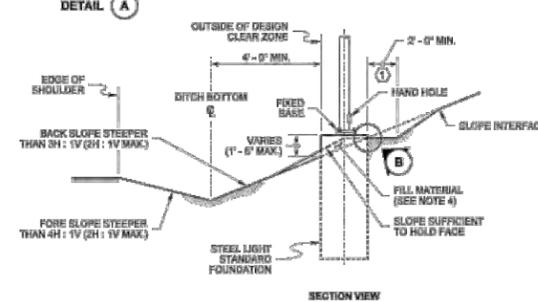
JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS
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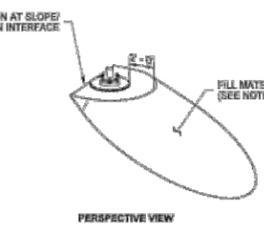


CASE E
SLOPES 2H:1V OR FLATTER
BEHIND TRAFFIC BARRIER



CASE G
ROADSIDE DITCH WITH FORE SLOPE
STEEPER THAN 4H:1V (2H:1V MAX.)

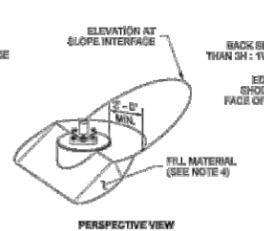
SLOPE	HEIGHT (SEE NOTE 3)
1.75H:1V	1' - 0 1/2"
1.50H:1V	2' - 0"
1.25H:1V	2' - 4 3/4"



CASE E & CASE F
MAINTENANCE PAD

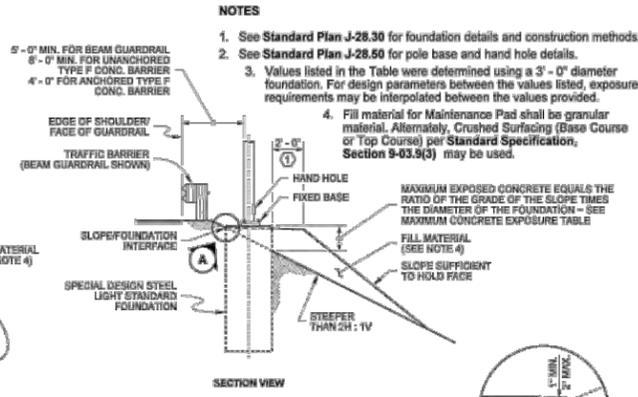
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① MAINTENANCE PAD - SLOPE TO DRAIN AWAY FROM THE FOUNDATION - NOT STEEPER THAN 5%

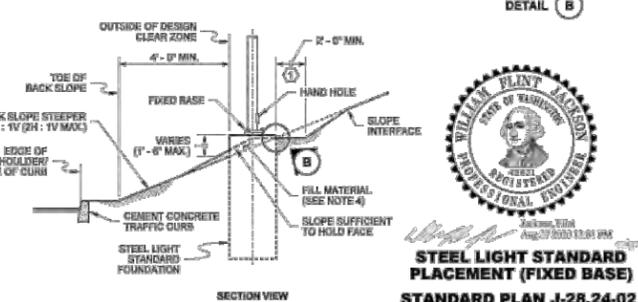


CASE G & CASE H
MAINTENANCE PAD

BACK SLOPES

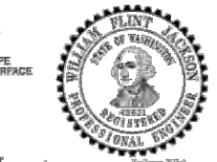


CASE F
SLOPES STEEPER THAN 2H:1V
BEHIND TRAFFIC BARRIER
(SPECIAL DESIGN FOUNDATION)



CASE H
CUT SECTION WITH BACK SLOPE
STEEPER THAN 3H:1V (2H:1V MAX.)

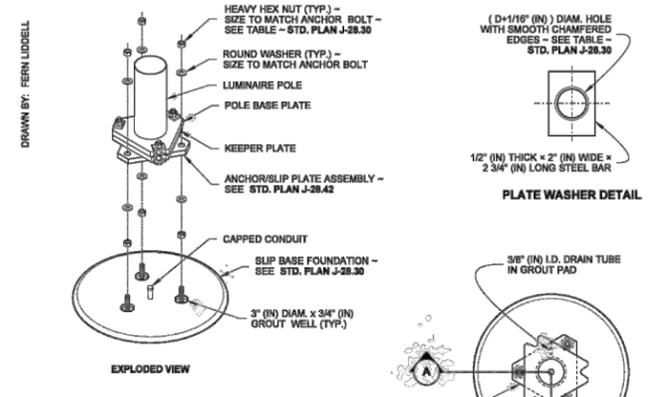
- NOTES**
- See Standard Plan J-28.30 for foundation details and construction methods.
 - See Standard Plan J-28.50 for pole base and hand hole details.
 - Values listed in the Table were determined using a 3' - 0" diameter foundation. For design parameters between the values listed, exposure requirements may be interpolated between the values provided.
 - Fill material for Maintenance Pad shall be granular material. Alternately, Crushed Surfacing (Base Course or Top Course) per Standard Specification, Section 9-03.9(3) may be used.



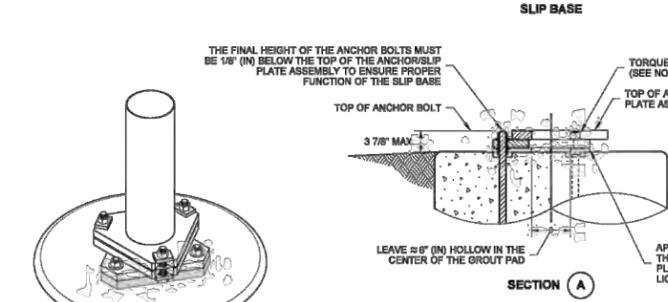
STEEL LIGHT STANDARD PLACEMENT (FIXED BASE)
STANDARD PLAN J-28.24-02
SHEET 1 OF 4 SHEET

APPROVED FOR PUBLICATION
Date: 2020.09.16
12:20:59 -0700
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DRAWN BY: FERN LIDDELL

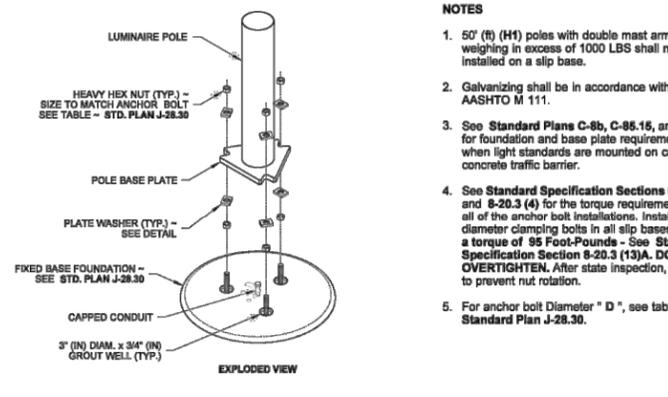


EXPLODED VIEW

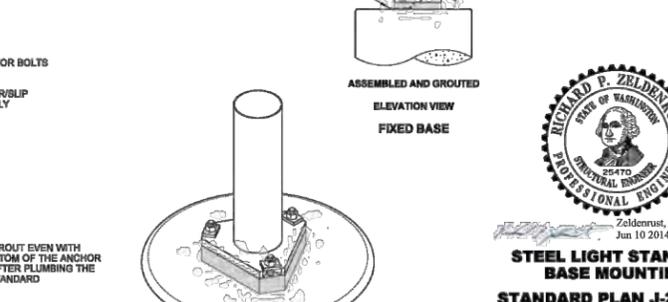


ASSEMBLED ISOMETRIC VIEW

THE FINAL HEIGHT OF THE ANCHOR BOLTS MUST BE 1/8" (IN) BELOW THE TOP OF THE ANCHOR/SLIP PLATE ASSEMBLY TO ENSURE PROPER FUNCTION OF THE SLIP BASE



EXPLODED VIEW



ASSEMBLED ISOMETRIC VIEW

APPLY GROUT EVEN WITH THE BOTTOM OF THE ANCHOR/SLIP PLATE AFTER PLUMBING THE LUMINAIRE POLE

- NOTES**
- 50' (ft) (H1) poles with double mast arms or poles weighing in excess of 1000 LBS shall not be installed on a slip base.
 - Galvanizing shall be in accordance with AASHTO M 111.
 - See Standard Plans C-8b, C-85.16, and J-28.50 for foundation and base plate requirements when light standards are mounted on cement concrete traffic barrier.
 - See Standard Specification Sections 8-03.3(33) and 8-20.3 (4) for the torque requirements for all of the anchor bolt installations. Install 1" (in) diameter clamping bolts in all slip bases to a torque of 95 Foot-Pounds - See Standard Specification Section 8-20.3 (13)A. DO NOT OVERTIGHTEN. After state inspection, burr threads to prevent nut rotation.
 - For anchor bolt Diameter "D", see table, Standard Plan J-28.30.



STEEL LIGHT STANDARD BASE MOUNTING
STANDARD PLAN J-28.40-02
SHEET 1 OF 1 SHEET

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Date: 2014.10.31
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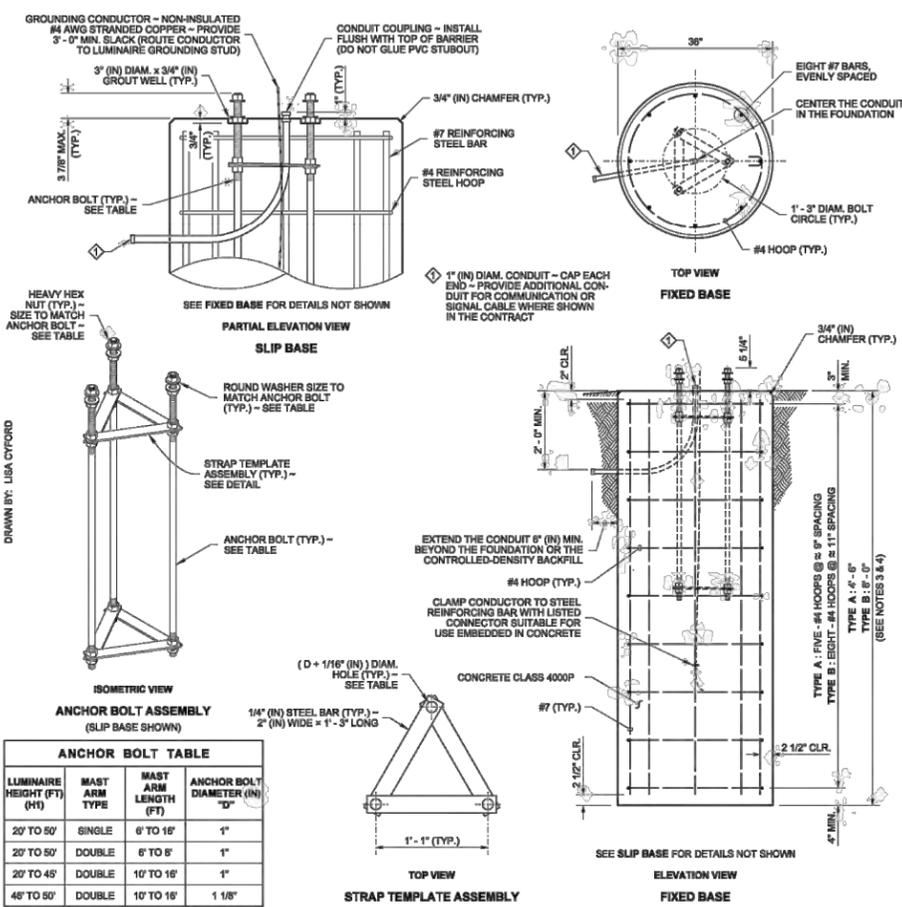
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JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS
WSDOT STANDARD PLANS

REFERENCE SHEET NO.
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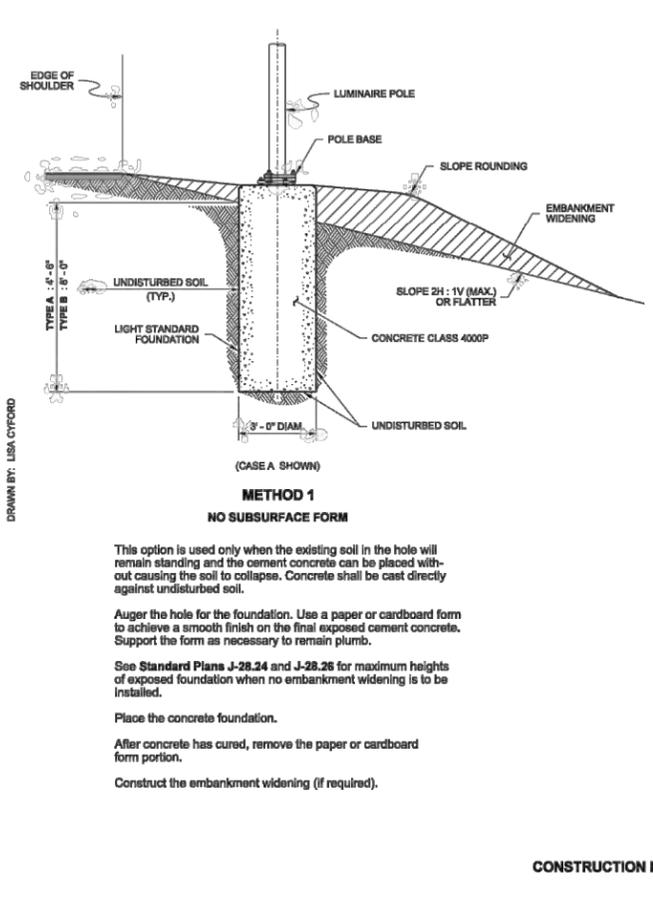


ANCHOR BOLT TABLE

LUMINAIRE HEIGHT (FT) (H1)	MAST ARM TYPE	MAST ARM LENGTH (FT)	ANCHOR BOLT DIAMETER (IN) "D"
20' TO 50'	SINGLE	6' TO 16'	1"
20' TO 50'	DOUBLE	6' TO 6'	1"
20' TO 45'	DOUBLE	10' TO 16'	1"
45' TO 50'	DOUBLE	10' TO 16'	1 1/8"

- NOTES**
- See Standard Plan J-28.40 for Luminaire Pole base mounting details.
 - The Strap Templates shall be held in place by nuts, 6" (in) from the top of the foundation and 3" (in) from the bottom of the anchor bolts. Eighteen heavy duty hex nuts and six round washers are required for a slip base assembly. Eighteen heavy duty hex nuts and six plate washers are required for a fixed base assembly.
 - Use Steel Light Standard Foundation Type A on level ground or slopes not exceeding 4H : 1V. Use Type B for slopes steeper than 4H : 1V, but not exceeding 2H : 1V. Slopes steeper than 2H : 1V shall require a special design.
 - These foundations are designed for a minimum of 2000 PSF (TYPE A) or 1500 PSF (TYPE B) allowable lateral bearing pressure for the soil. A special foundation shall be required for soil with allowable lateral bearing pressure lower than 1500 PSF.
 - The Luminaire Pole height shall not exceed 50' (H1).
 - Slip bases shall not be installed on 50' (H1) poles with Double Mast Arms, nor on poles weighing more than 1000 lbs.
 - Slip bases are required on poles installed inside the Design Clear Zone, and on poles installed behind traffic barrier that are within the traffic barrier deflection zone.
 - Foundations constructed within Media Filter Drains shall be increased in depth by the depth of the Media Filter Drain.
 - Exposed portions of the foundation shall be formed to create a Class 2 surface finish. All forming shall be removed upon completion of foundation construction.
 - For excavation, concrete placement, and backfill options, see METHOD 1 and METHOD 2 on Sheet 2 of 2.
 - The anchor bolts shall be high-strength steel, manufactured from ASTM F1554 Grade 105, with heavy hex nuts and hardened washers. Galvanize the anchor bolts according to ASTM F2329.
 - The foundation shall be grounded in accordance with the requirements of Standard Specification 8-20.3(4).
 - See Standard Plans C-8b and C-85.14 for steel light standards on traffic barrier.

Richard P. Zeldenzust
 Jun 10 2014 10:37 AM
STEEL LIGHT STANDARD FOUNDATION TYPES A & B
 STANDARD PLAN J-28.30-03
 SHEET 1 OF 2 SHEETS
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 Bakoch, Pasco
 Jun 11 2014 1:33 PM
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**METHOD 1
NO SUBSURFACE FORM**

This option is used only when the existing soil in the hole will remain standing and the cement concrete can be placed without causing the soil to collapse. Concrete shall be cast directly against undisturbed soil.

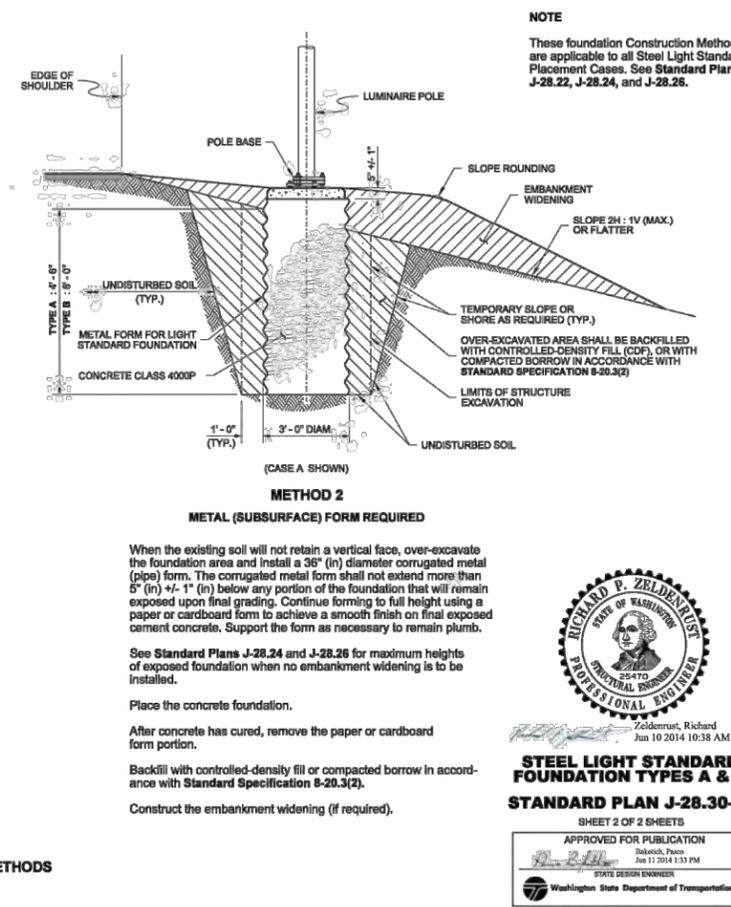
Auger the hole for the foundation. Use a paper or cardboard form to achieve a smooth finish on the final exposed cement concrete. Support the form as necessary to remain plumb.

See Standard Plans J-28.24 and J-28.26 for maximum heights of exposed foundation when no embankment widening is to be installed.

Place the concrete foundation.

After concrete has cured, remove the paper or cardboard form portion.

Construct the embankment widening (if required).



**METHOD 2
METAL (SUBSURFACE) FORM REQUIRED**

When the existing soil will not retain a vertical face, over-excavate the foundation area and install a 36" (in) diameter corrugated metal (pipe) form. The corrugated metal form shall not extend more than 6" (in) +/- 1" (in) below any portion of the foundation that will remain exposed upon final grading. Continue forming to full height using a paper or cardboard form to achieve a smooth finish on final exposed cement concrete. Support the form as necessary to remain plumb.

See Standard Plans J-28.24 and J-28.26 for maximum heights of exposed foundation when no embankment widening is to be installed.

Place the concrete foundation.

After concrete has cured, remove the paper or cardboard form portion.

Backfill with controlled-density fill or compacted borrow in accordance with Standard Specification 8-20.3(2).

Construct the embankment widening (if required).

Richard P. Zeldenzust
 Jun 10 2014 10:38 AM
STEEL LIGHT STANDARD FOUNDATION TYPES A & B
 STANDARD PLAN J-28.30-03
 SHEET 2 OF 2 SHEETS
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CONSTRUCTION METHODS

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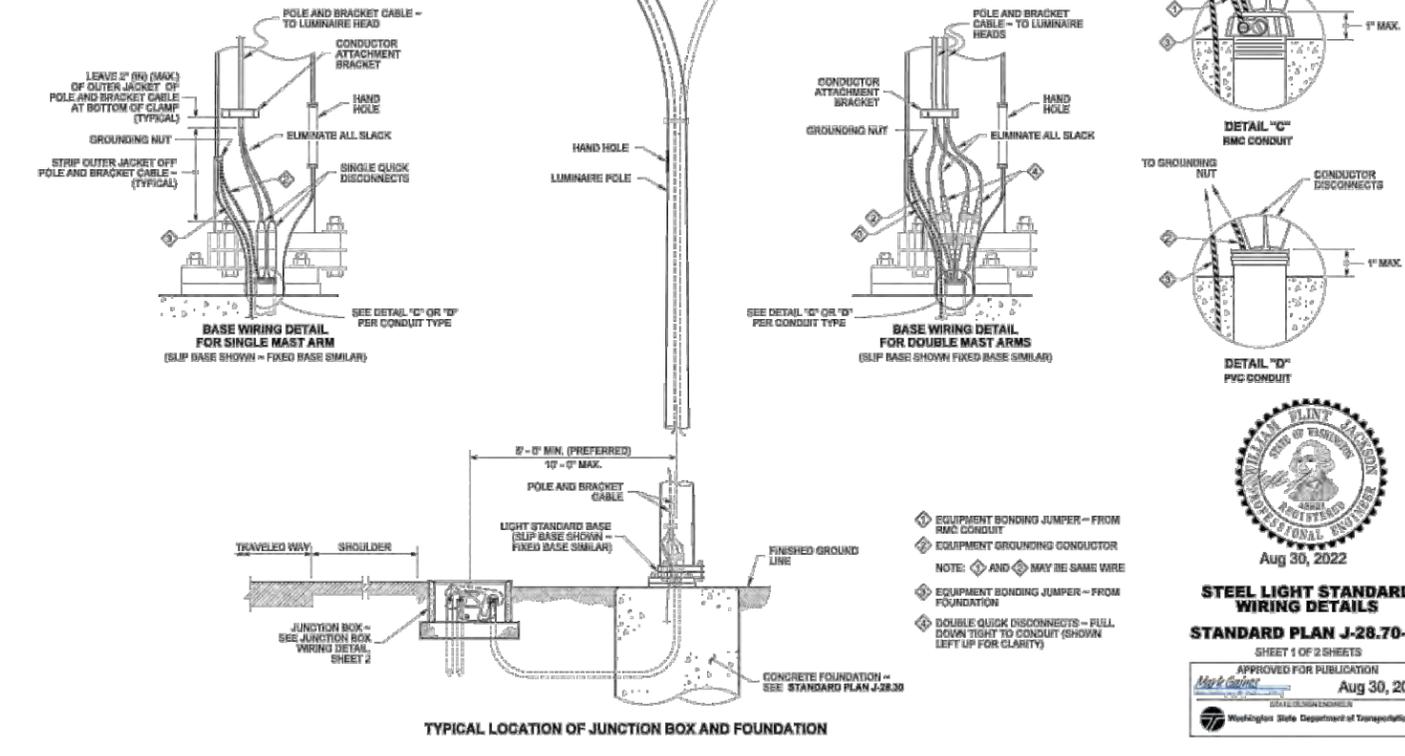
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JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

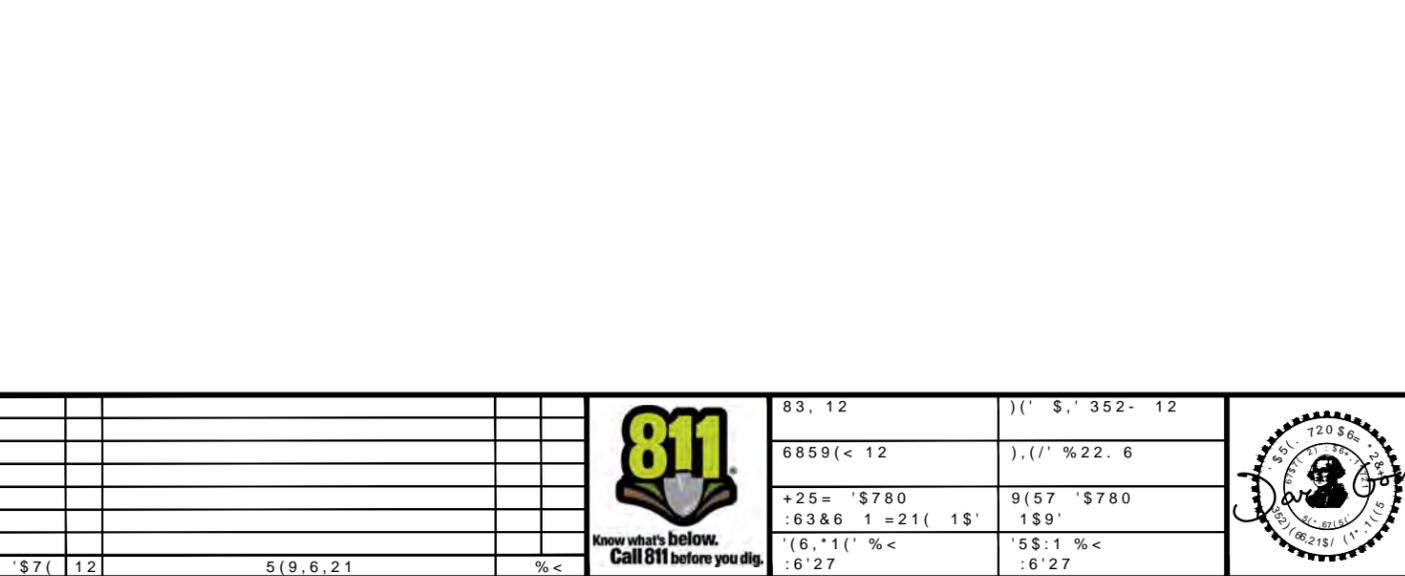
WSDOT STANDARD PLANS

REFERENCE SHEET NO. SHEET 2) SHEETS

DRAWN BY: FEIN LOBEL



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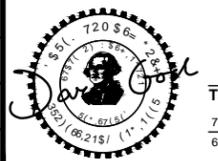


WILLIAM BLINT JACKSON
 NO. OF LICENSE 14350
 STATE OF WASHINGTON
 PROFESSIONAL ENGINEER
 Aug 30, 2022
STEEL LIGHT STANDARD WIRING DETAILS
STANDARD PLAN J-28.70-04
 SHEET 1 OF 2 SHEETS
 APPROVED FOR PUBLICATION
 Mark Gaines
 Aug 30, 2022
DATE REVISIONS

WILLIAM BLINT JACKSON
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 STATE OF WASHINGTON
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 Aug 30, 2022
STEEL LIGHT STANDARD WIRING DETAILS
STANDARD PLAN J-28.70-04
 SHEET 2 OF 2 SHEETS
 APPROVED FOR PUBLICATION
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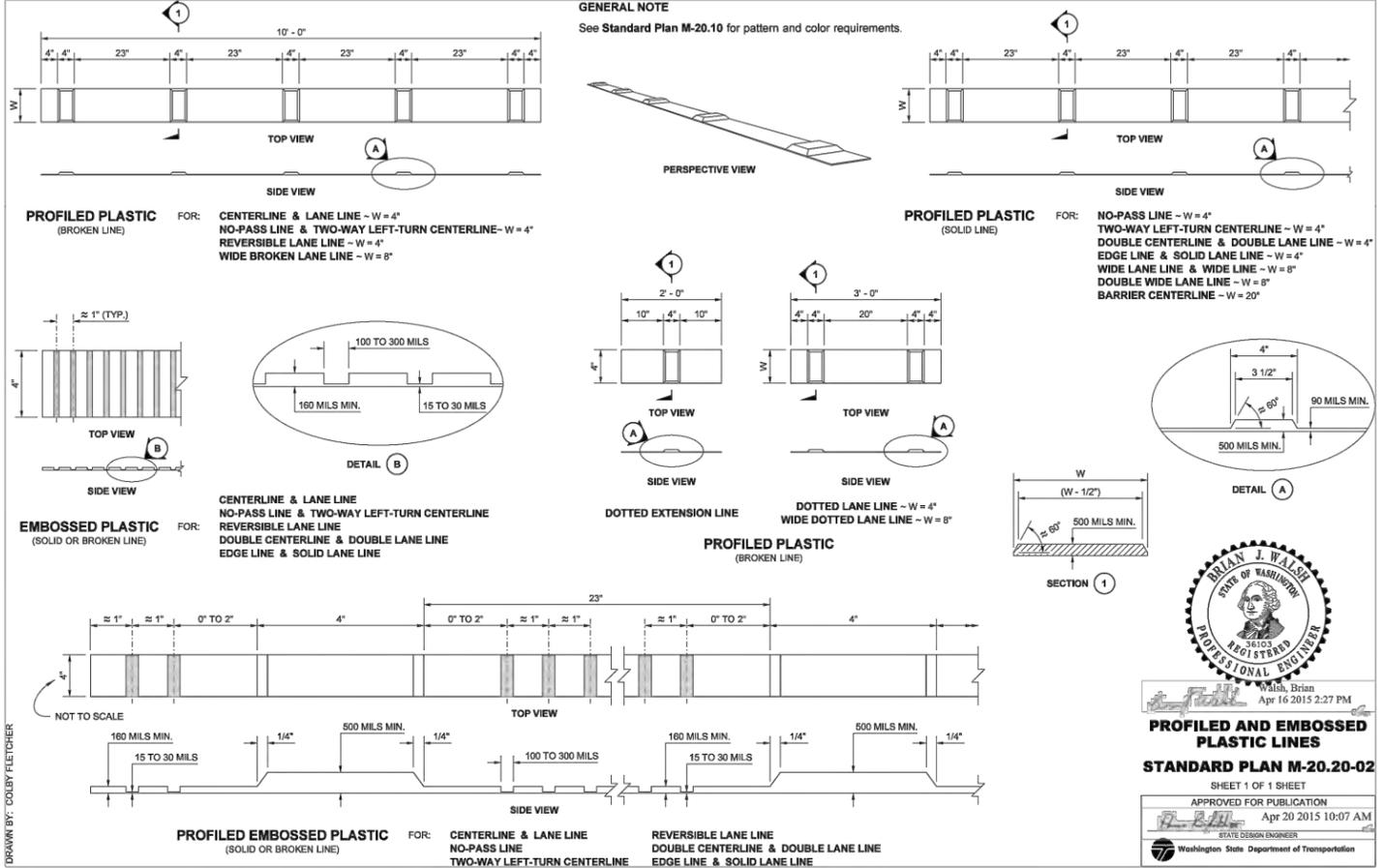
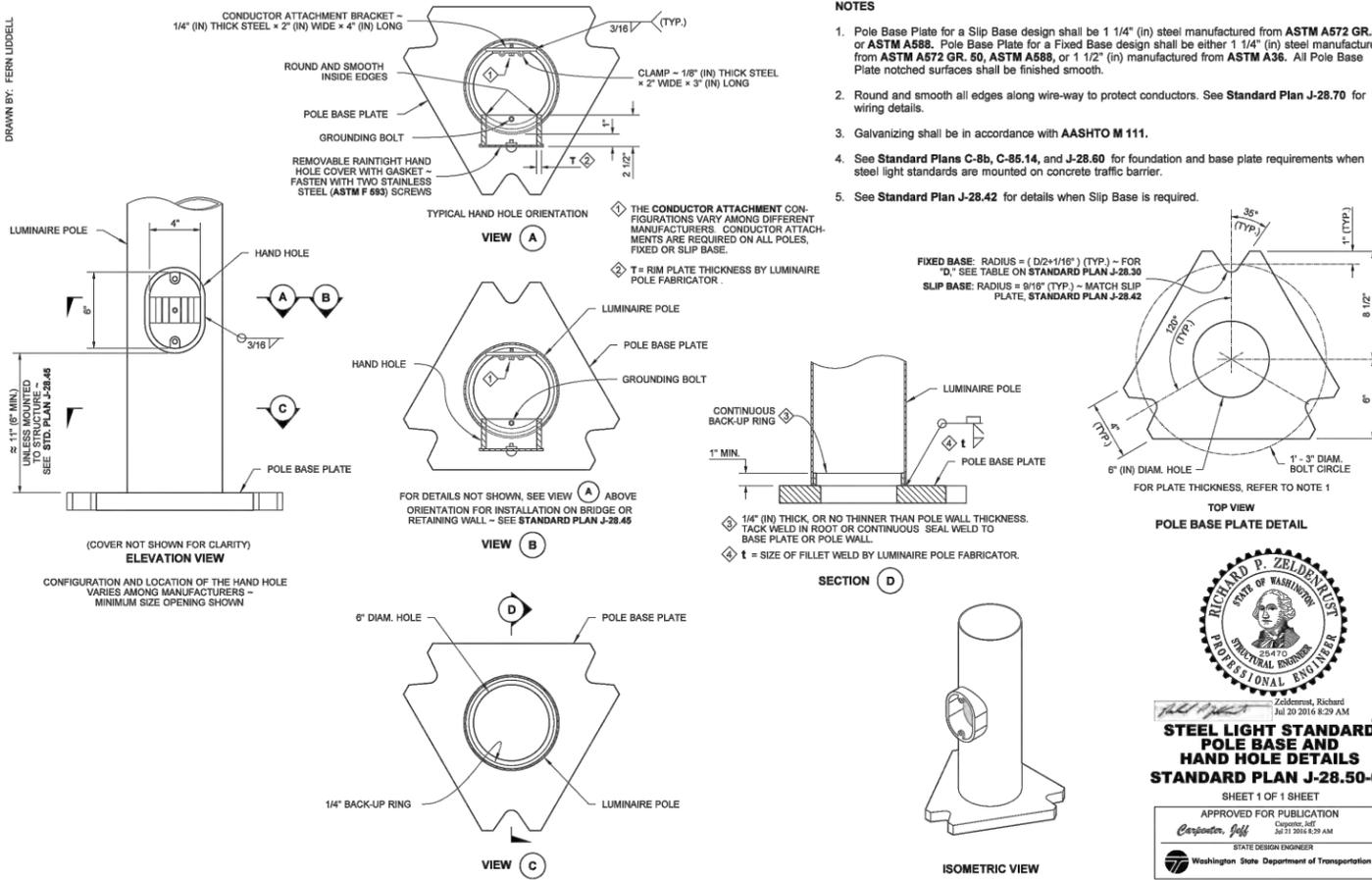
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WSDOT STANDARD PLANS

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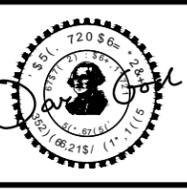
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KPG PSOMAS

BID DOCUMENT

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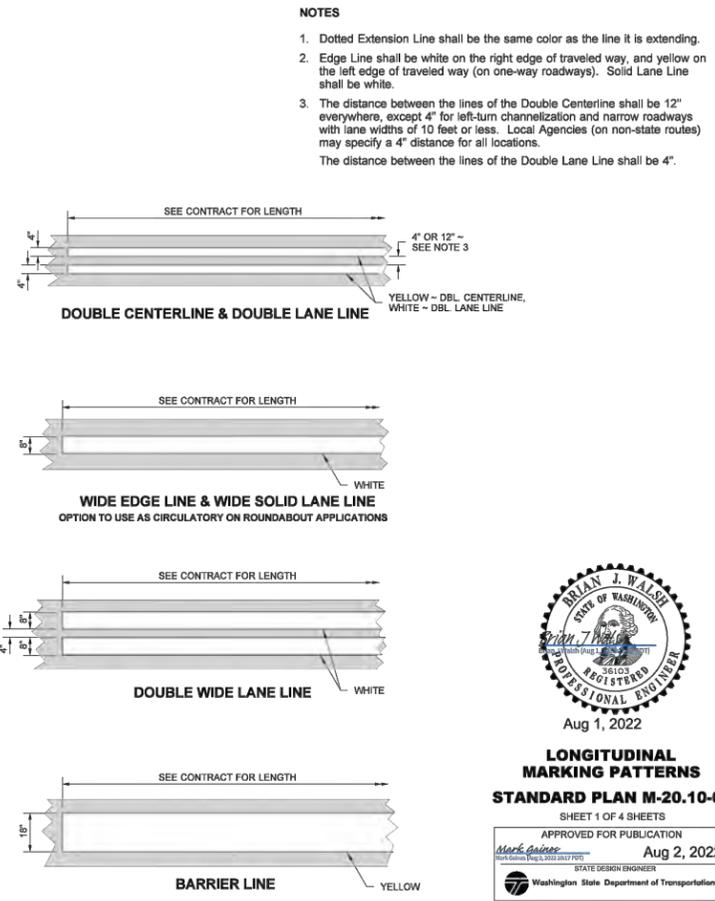
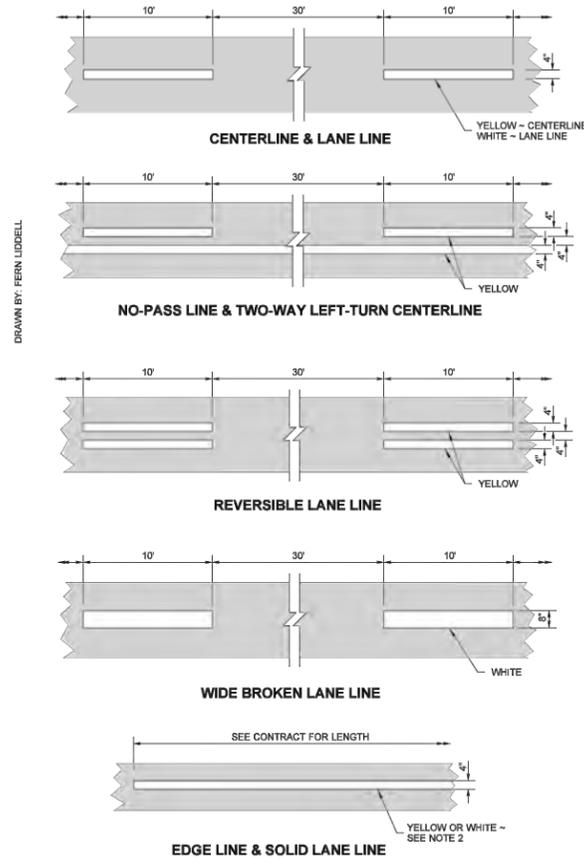
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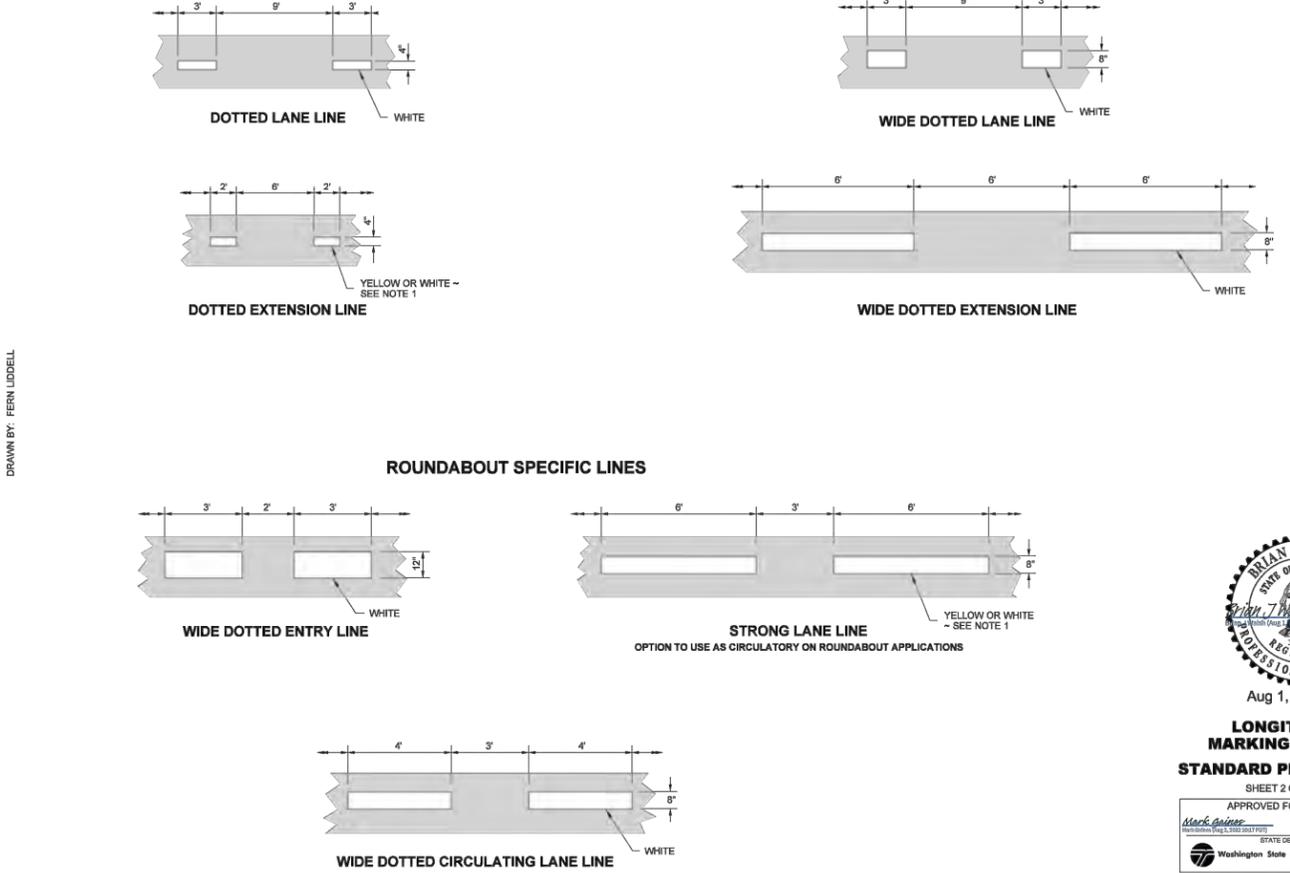
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- NOTES**
- Dotted Extension Line shall be the same color as the line it is extending.
 - Edge Line shall be white on the right edge of traveled way, and yellow on the left edge of traveled way (on one-way roadways). Solid Lane Line shall be white.
 - The distance between the lines of the Double Centerline shall be 12" everywhere, except 4" for left-turn channelization and narrow roadways with lane widths of 10 feet or less. Local Agencies (on non-state routes) may specify a 4" distance for all locations. The distance between the lines of the Double Lane Line shall be 4".

BRIAN J. WALSH
STATE OF WASHINGTON
REGISTERED PROFESSIONAL ENGINEER
36103
Aug 1, 2022

LONGITUDINAL MARKING PATTERNS
STANDARD PLAN M-20.10-04
SHEET 1 OF 4 SHEETS
APPROVED FOR PUBLICATION
Aug 2, 2022
Mark Gaines
STATE DESIGN ENGINEER
Washington State Department of Transportation



BRIAN J. WALSH
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36103
Aug 1, 2022

LONGITUDINAL MARKING PATTERNS
STANDARD PLAN M-20.10-04
SHEET 2 OF 4 SHEETS
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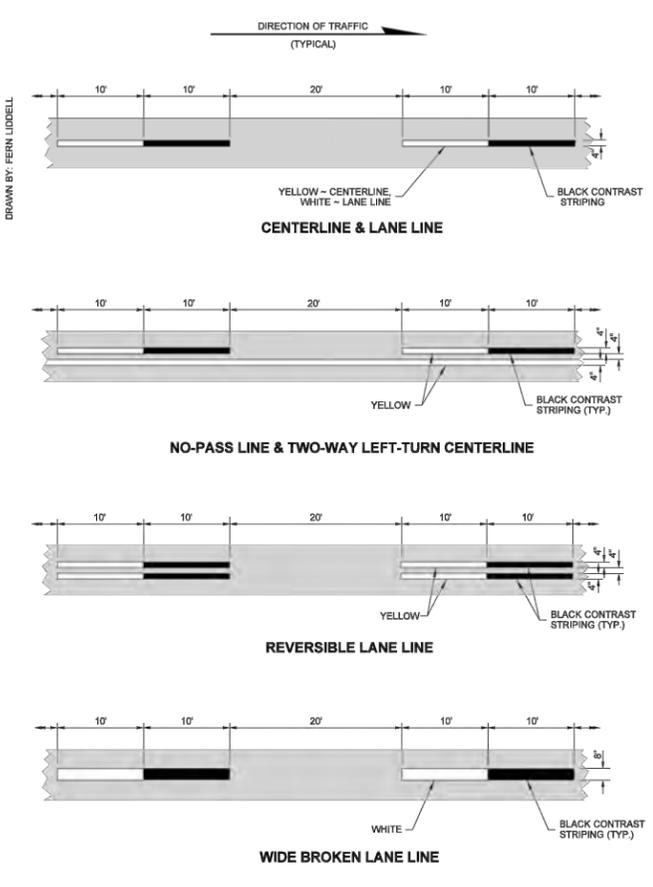
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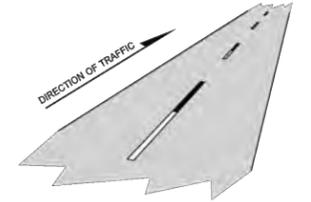
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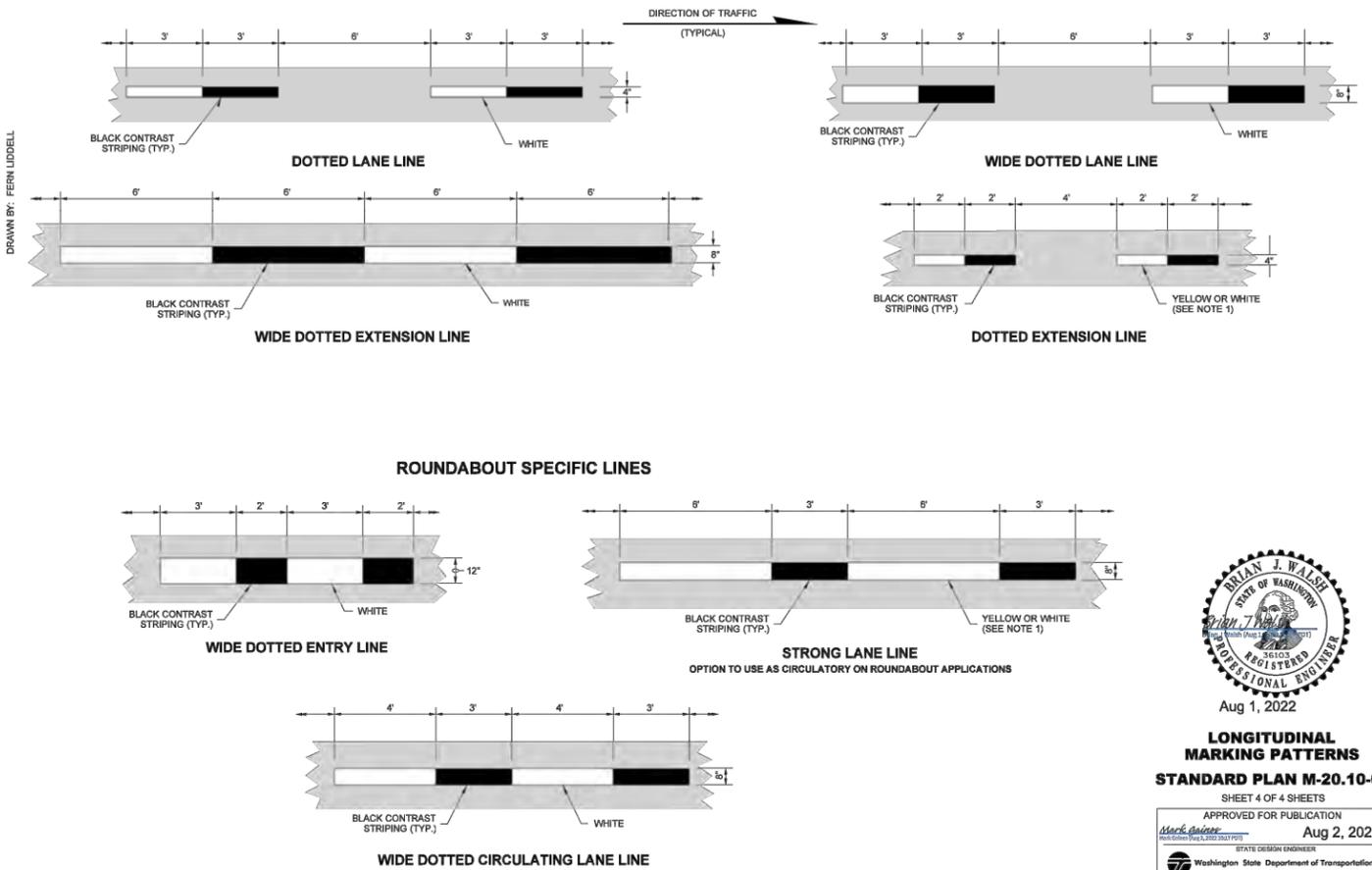
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NOTE
1. Dotted Extension Line shall be the same color as the line it is extending.



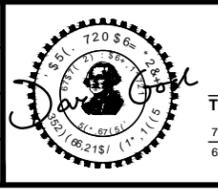
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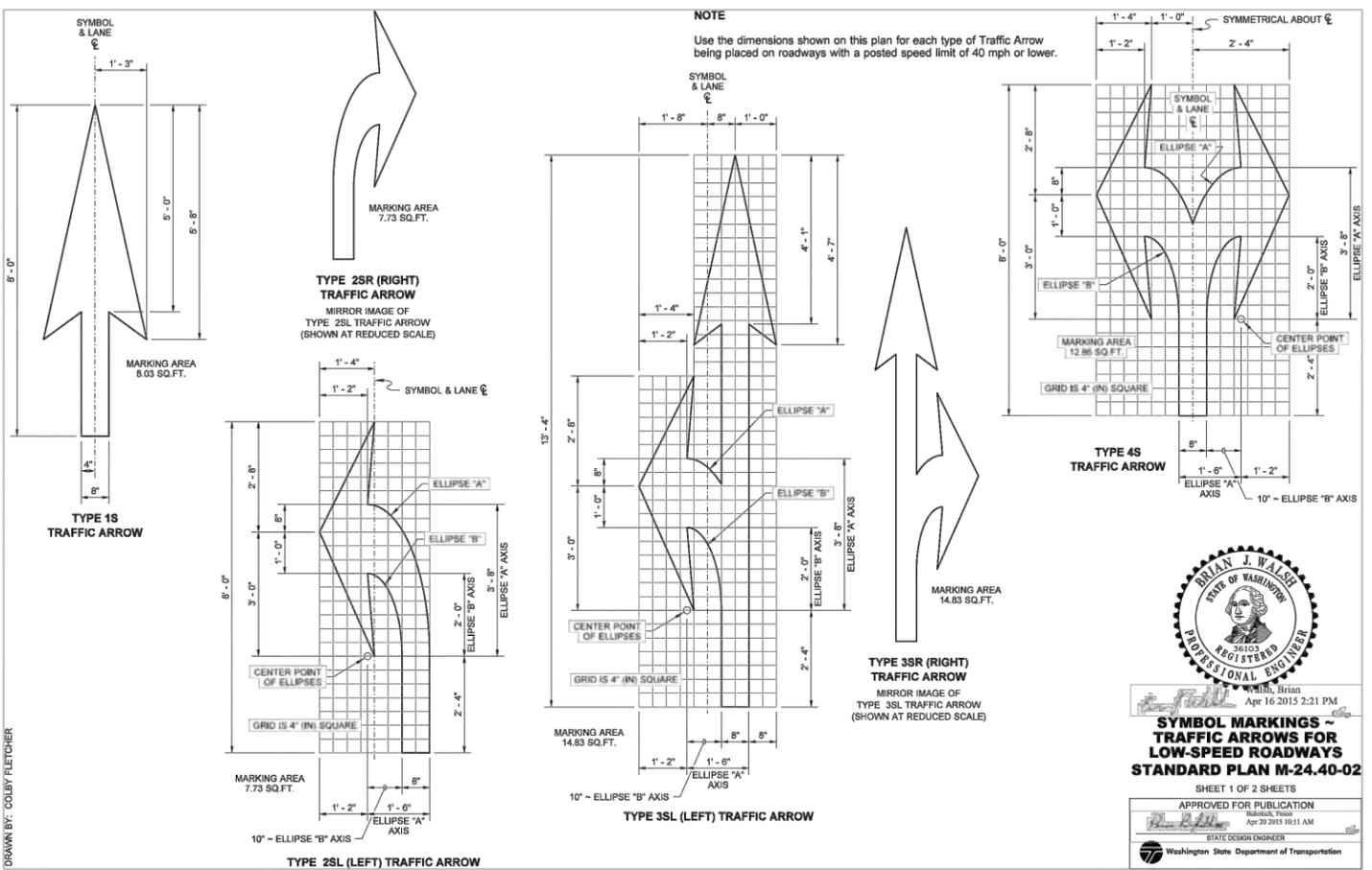
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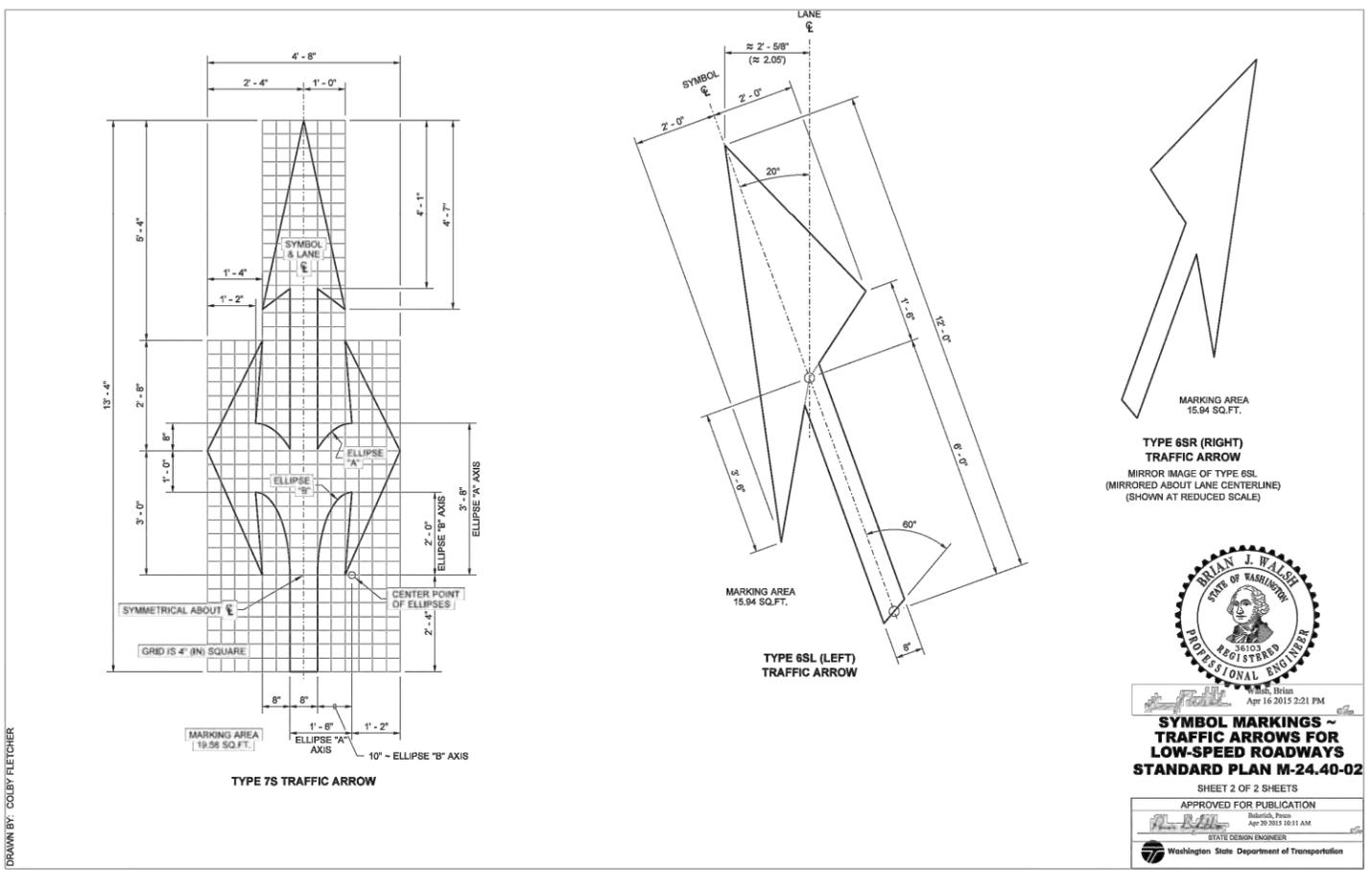
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SYMBOL MARKINGS - TRAFFIC ARROWS FOR LOW-SPEED ROADWAYS
STANDARD PLAN M-24.40-02
SHEET 1 OF 2 SHEETS
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Apr 20 2015 10:11 AM
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SYMBOL MARKINGS - TRAFFIC ARROWS FOR LOW-SPEED ROADWAYS
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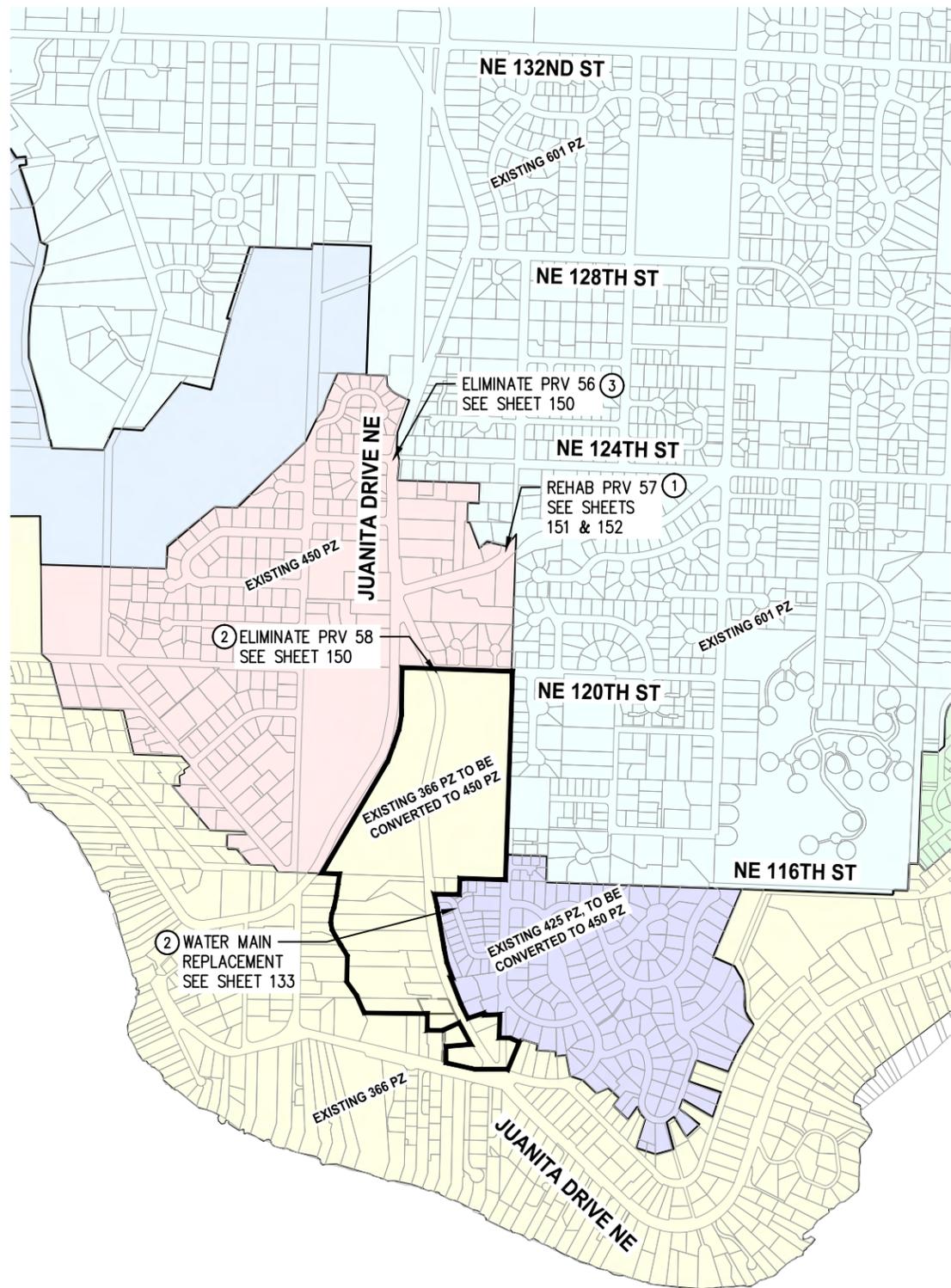
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JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

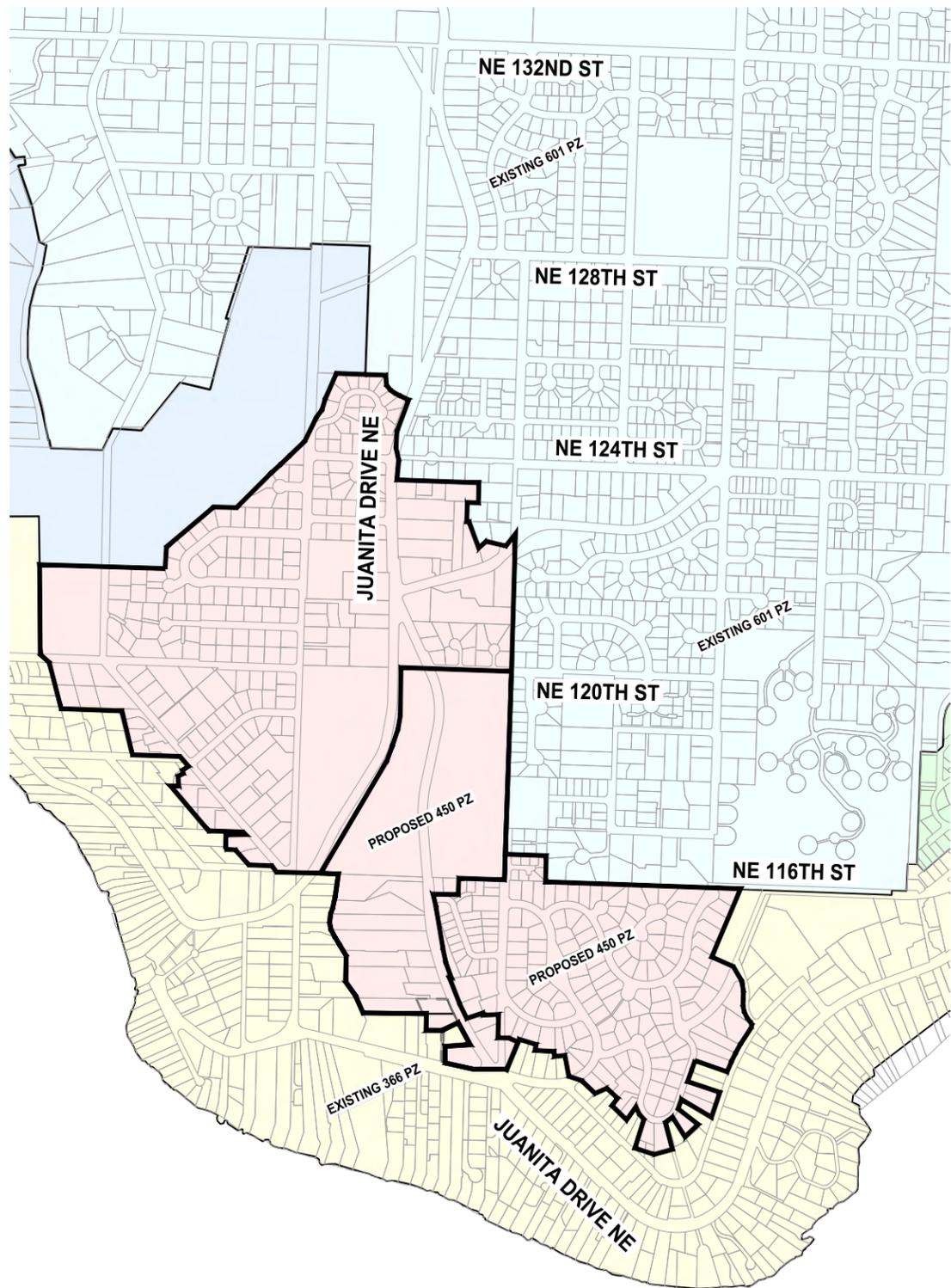
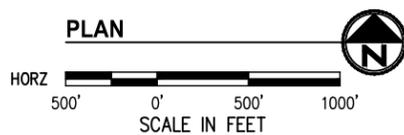
WSDOT STANDARD PLANS

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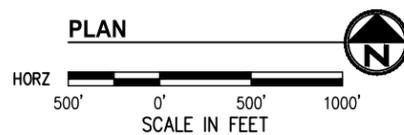
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CURRENT PRESSURE ZONE MAP (PRIOR TO CONSTRUCTION)



PROPOSED PRESSURE ZONE MAP (FOLLOWING PROJECT COMPLETION)



PROJECT SEQUENCE

THE PROPOSED WORK FOR THE PROJECT INCLUDES PRESSURE ZONE MODIFICATIONS. THE SEQUENCE OF CONSTRUCTION LISTED BELOW IS REQUIRED FOR DISTRICT OPERATIONAL PURPOSES TO MAINTAIN SERVICE FOR THEIR CUSTOMERS.

THE CONTRACTOR MUST SUBMIT A PHASING PLAN TO THE DISTRICT, PRIOR TO ISSUANCE OF NOTICE TO PROCEED, FOR APPROVAL. AFTER COMPLETION OF EACH SITE, THE CONTRACTOR MUST RECEIVE WRITTEN APPROVAL FROM THE DISTRICT PRIOR TO MOVING TO A SUBSEQUENT SITE. FULL RESTORATION MUST BE PERFORMED.

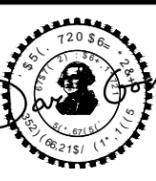
- ① PRV 57 MAIN LINE VALVE CUT IN AND SUBSEQUENT PRV REHABILITATION.
- ② JUANITA/79TH WATER MAIN AND PRV 58 ABANDONMENT, IN EITHER ORDER.
- ③ PRV 56 ABANDONMENT.

THE WATER MAIN REPLACEMENTS BETWEEN NE 123RD ST AND NE 133RD PL IN JUANITA DR DO NOT MODIFY PRESSURE ZONES. THE CONTRACTOR MAY SELECT THE PHASING FOR THIS WORK.

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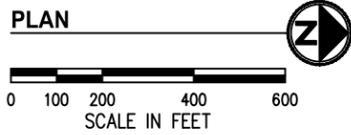
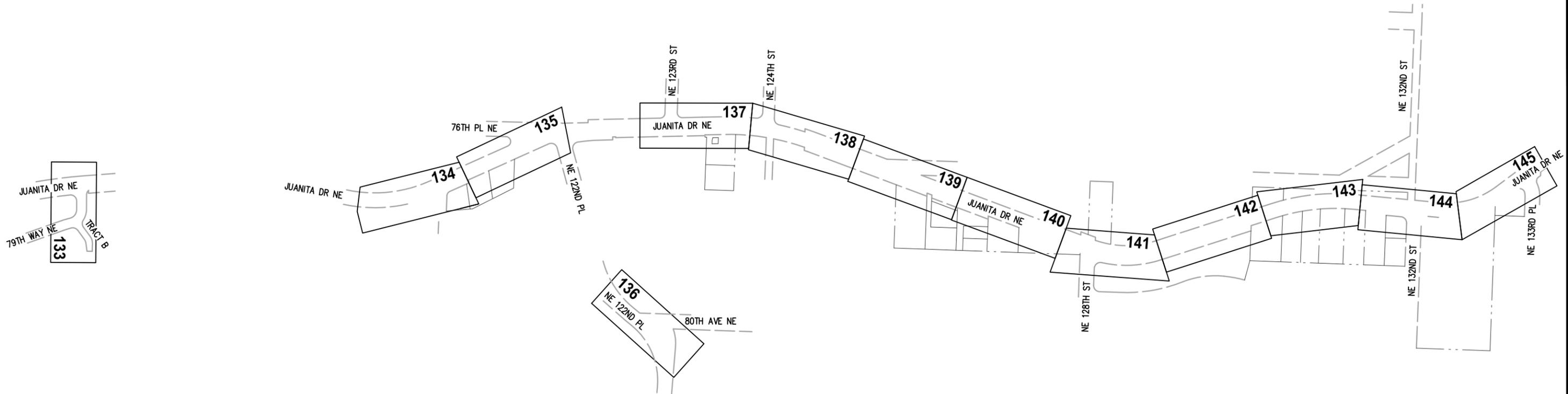
JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

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PRESSURE ZONE OVERVIEW

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GENERAL NOTES

1. ALL CONSTRUCTION AND MATERIALS SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS SPECIFIED IN THE CURRENT NORTSHORE UTILITY DISTRICT STANDARD SPECIFICATIONS AND STANDARD DETAILS.
2. THE APPROXIMATE LOCATIONS OF EXISTING UTILITIES ARE SHOWN ON THE PLANS FOR CONVENIENCE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFICATION OF UTILITY LOCATIONS SHOWN, FOR THE PROTECTION AND REPAIR OF DAMAGED UTILITIES AND FOR THE DISCOVERY OF POSSIBLE ADDITIONAL UTILITIES NOT SHOWN ON THE PLANS. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO HAVE LOCATED, BY THE APPROPRIATE UTILITY DISTRICTS OR COMPANIES, ALL UNDERGROUND UTILITIES PRIOR TO BEGINNING CONSTRUCTION. FOR UTILITY LOCATES IN KING COUNTY, CALL 1-800-424-5555 PRIOR TO DIGGING.
3. A PRE-CONSTRUCTION CONFERENCE WILL BE HELD AT THE DISTRICT OFFICE PRIOR TO START OF CONSTRUCTION.
4. THE CONTRACTOR SHALL NOTIFY NORTSHORE UTILITY DISTRICT A MINIMUM OF FIVE (5) DAYS IN ADVANCE OF BEGINNING CONSTRUCTION. CONSTRUCTION SHALL NOT BEGIN WITHOUT PRIOR WRITTEN NOTICE TO PROCEED BY THE DISTRICT.
5. THE CONTRACTOR SHALL NOT OPERATE AY VALVES OR MAKE ANY CONNECTIONS TO THE EXISTING WATER SYSTEM WITHOUT PRIOR APPROVAL FROM THE DISTRICT.

GENERAL WATER NOTES

1. FOR DEMOLITION AND RESTORATION ASSOCIATED WITH PROPOSED WATER SYSTEM SEE SITE PREP PLAN SHEETS 9-16 AND ROADWAY PLAN SHEETS 33-46.
2. CONSTRUCT NEW WATER MAIN BELOW EXISTING MAIN UNLESS OTHERWISE NOTED. CONTRACTOR TO COORDINATE TRANSITION OF SERVICE TO NEW WATER MAIN WITH NORTSHORE UTILITY DISTRICT IN ORDER TO RESTORE SERVICE WITH MINIMAL DISRUPTION.
3. EXISTING WATER MAIN AND APPURTENANCES SHALL BE ABANDONED IN PLACE, UNLESS OTHERWISE NOTED ON THE PLANS. CLOSE ALL VALVES AND REMOVE & DISPOSE OF EXISTING VALVE BOXES. THE PIPE SHALL BE CAPPED WATERTIGHT WITH FITTINGS IN ACCORDANCE WITH THE SPECIFICATIONS AT LOCATIONS WHERE CROSSINGS OR CONFLICTS REQUIRE REMOVAL OF SECTIONS OF ABANDONED PIPE. AC PIPES TO BE ABANDONED SHALL BE FILLED WITH CDF.
4. CONTRACTOR SHALL POTHOLE EXISTING WATER MAIN AT CONNECTIONS TO VERIFY HORIZONTAL & VERTICAL LOCATION AND PIPE MATERIAL PRIOR TO COMMENCING WATER MAIN INSTALLATION.
5. WATER MAIN PIPING SHALL BE RESTRAINED JOINT (RJ) PIPE. FIELD LOK GASKETS AND MEGALUG FITTINGS, OR APPROVED EQUAL.
6. SALVAGE EXISTING HYDRANTS AND DELIVER TO DISTRICT YARD IN A CLEAN AND OPERABLE CONDITION.
7. CONTRACTOR SHALL PLACE FOAM BLOCK, ETHAFOAM 220 OR EQUAL, BETWEEN UTILITIES WHERE THERE IS LESS THAN 1' SEPARATION.
8. WATER TRENCH SHALL BE PER NUD WATER STD DETAIL 12. SEWER TRENCH SHALL BE PER NUD SEWER STD DETAIL 11.
9. THRUST BLOCKING SHALL BE PER NUD STD DETAILS 1 & 2.
10. ALL SERVICE CONNECTIONS NORTH OF NE 124TH ST ARE ASSUMED TO HAVE A PRIVATE PRV DEVICE ON THE BACKSIDE OF THE METER. UNLESS OTHERWISE NOTED, A REPLACEMENT PRV DEVICE SHALL BE INSTALLED PER NUD STD DETAIL 21 AT THE TIME OF RECONNECTION. SEE SPECIFICATIONS FOR PRESSURE TESTING REQUIREMENTS.
11. WATER SERVICE INSTALLATION MAY BE COMPLETED BY EITHER OPEN TRENCH OR HOLE HOG. MEANS AND METHODS FOR WATER SERVICE INSTALLATION SHALL BE DETERMINED BY THE CONTRACTOR.

GENERAL SEWER NOTES

1. THE LOCATION SHOWN FOR SIDE SEWER STUBS IS APPROXIMATE. CONTRACTOR TO VERIFY THE LOCATION OF SIDE SEWER STUBS.
2. SEWER SERVICE SHALL NOT BE INTERRUPTED DURING CONSTRUCTION.
3. A TEMPORARY SEWER BYPASS PLAN SHALL BE SUBMITTED TO THE DISTRICT FOR REVIEW AND APPROVAL PRIOR TO BEGINNING ANY SEWER WORK.
4. EXISTING SIDE SEWERS MUST BE TEMPORARILY OR PERMANENTLY RECONNECTED BY THE END OF THE WORK DAY.



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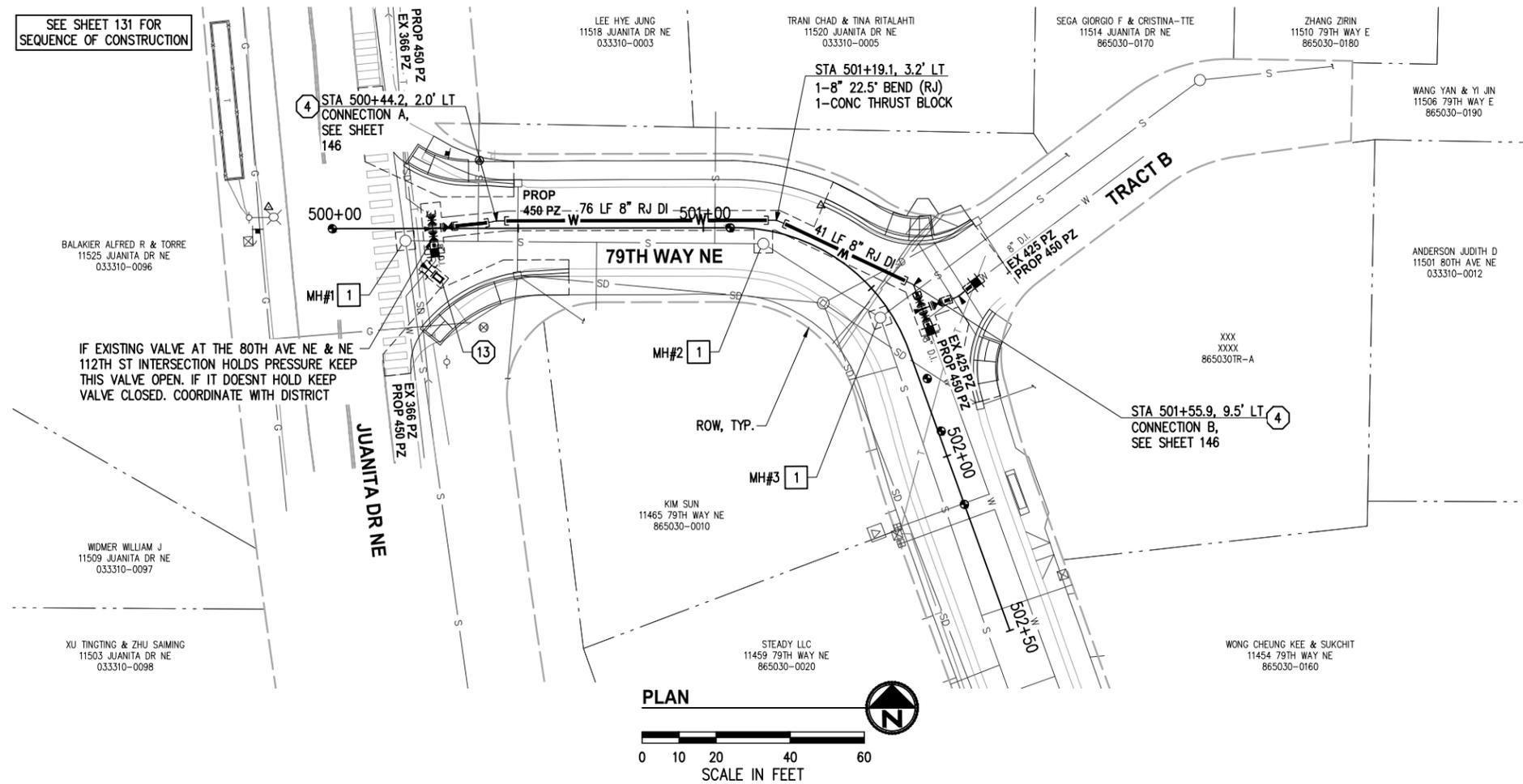
**JUANITA DRIVE MULTIMODAL,
 INTERSECTION & SAFETY IMPROVEMENTS**

SHEET KEY

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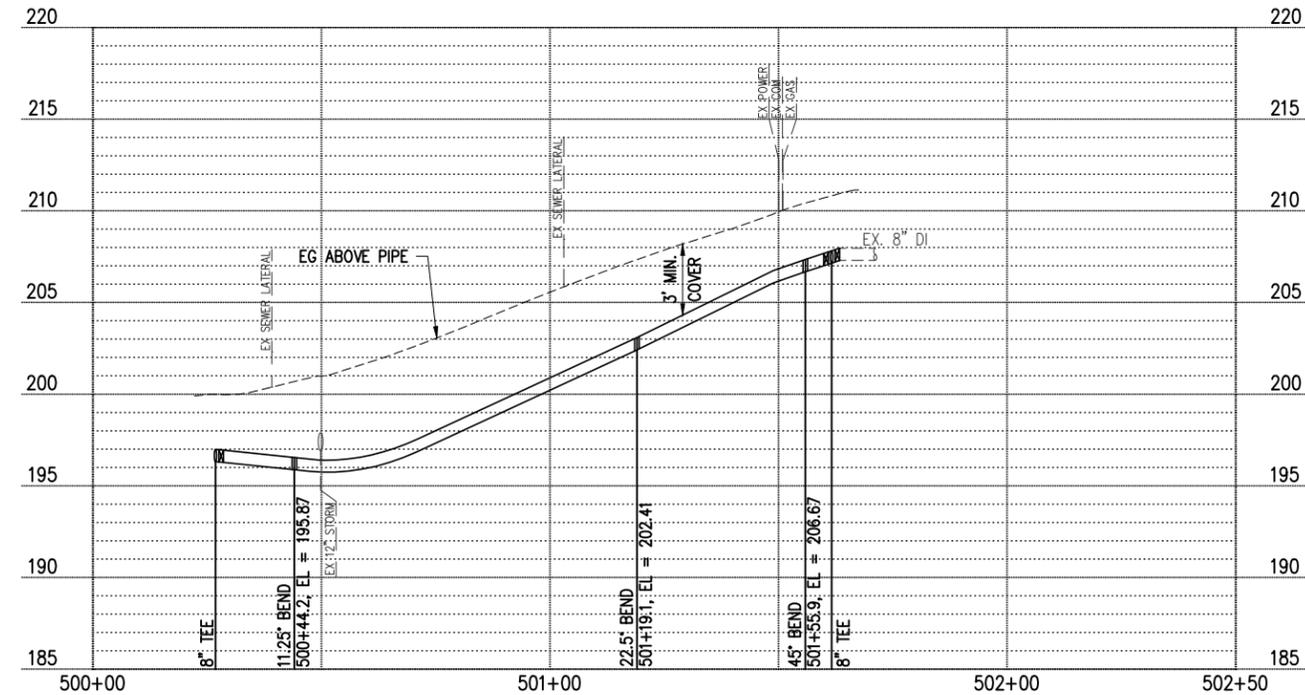
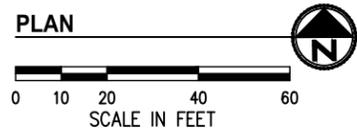
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SEE SHEET 131 FOR SEQUENCE OF CONSTRUCTION



- GENERAL NOTES**
- 1. FOR GENERAL NOTES SEE SHEET KEY, SHEET 132
- WATER CONSTRUCTION NOTES**
- 4 CONNECT TO EX WATER MAIN, SEE CONNECTION SHEETS 146-147
 - 13 2" BLOW-OFF ASSEMBLY PER NUD WATER STD DETAIL 10, EXCEPT INSTALL TAPPING TEE PER NUD STD DETAIL 20 IN LIEU OF DI TEE
- SEWER CONSTRUCTION NOTES**
- 1 REPLACE MANHOLE FRAME, COVER AND ADJUSTMENT RINGS FOR EXISTING MANHOLE PER NUD STD SEWER DETAILS 1 & 5

IF EXISTING VALVE AT THE 80TH AVE NE & NE 112TH ST INTERSECTION HOLDS PRESSURE KEEP THIS VALVE OPEN. IF IT DOESNT HOLD KEEP VALVE CLOSED. COORDINATE WITH DISTRICT



- LEGEND**
- ABANDON PIPE, SEE GENERAL WATER NOTE 4, SHEET 132
 - W WATER LINE
 - WATER VALVE AS NOTED
 - WATER METER
 - LONG SLEEVE COUPLING
 - FL x RJ ADAPTER
 - FIRE HYDRANT
 - CONC THRUST BLOCKS PER NUD STD DETAIL 1
 - CAP

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Call 811 before you dig.

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NORTHSHORE UTILITY DISTRICT
CITY OF KIRKLAND
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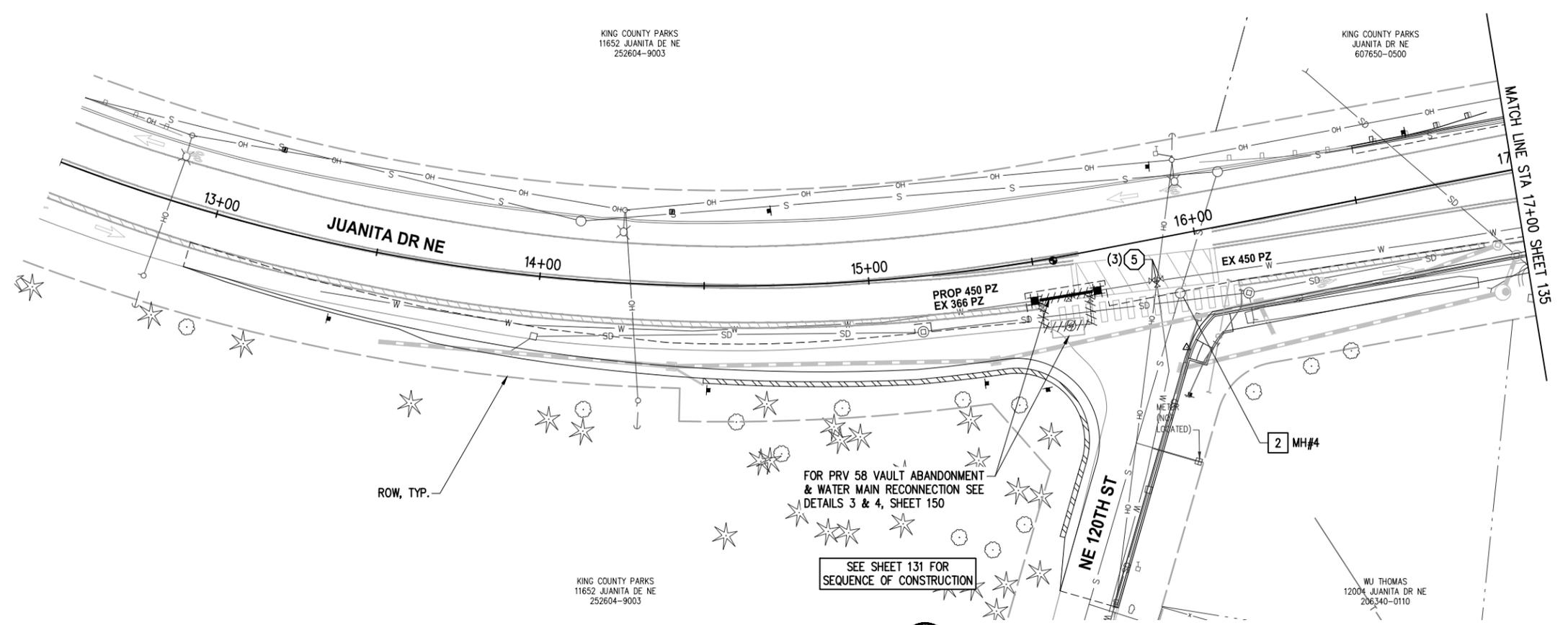
JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

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NUD WATER & SEWER PLAN & PROFILE

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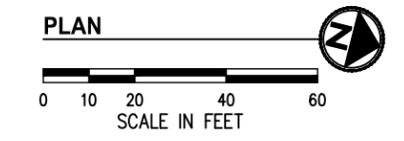
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KING COUNTY PARKS
JUANITA DR NE
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KING COUNTY PARKS
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252604-9003

WJ THOMAS
12004 JUANITA DR NE
206340-0110

- GENERAL NOTES**
- FOR GENERAL NOTES SEE SHEET KEY, SHEET 132
- WATER CONSTRUCTION NOTES**
- REPLACE VALVE BOX AND INSTALL OPERATING NUT EXTENSION (WHERE REQ'D) PER NUD WATER STD DETAIL 11, REMOVE AND DISPOSE OF EXISTING VALVE BOX
- SEWER CONSTRUCTION NOTES**
- REPLACE MANHOLE FRAME, COVER, CONE SECTION AND ADJUSTMENT RINGS FOR EXISTING MANHOLE. ADD MANHOLE SECTIONS AND MANHOLE STEPS AS NEEDED PER TABLE ON SHEET 156 ONTO EXISTING MANHOLE PER SEWER DETAIL 1, SHEET 155



- LEGEND**
- ABANDON PIPE, SEE GENERAL WATER NOTE 4, SHEET 132
 - WATER LINE
 - WATER VALVE AS NOTED
 - WATER METER
 - LONG SLEEVE COUPLING
 - FL x RJ ADAPTER
 - FIRE HYDRANT
 - CONC THRUST BLOCKS PER NUD STD DETAIL 1
 - CAP

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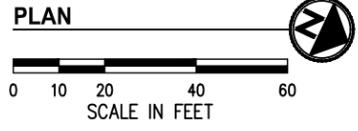
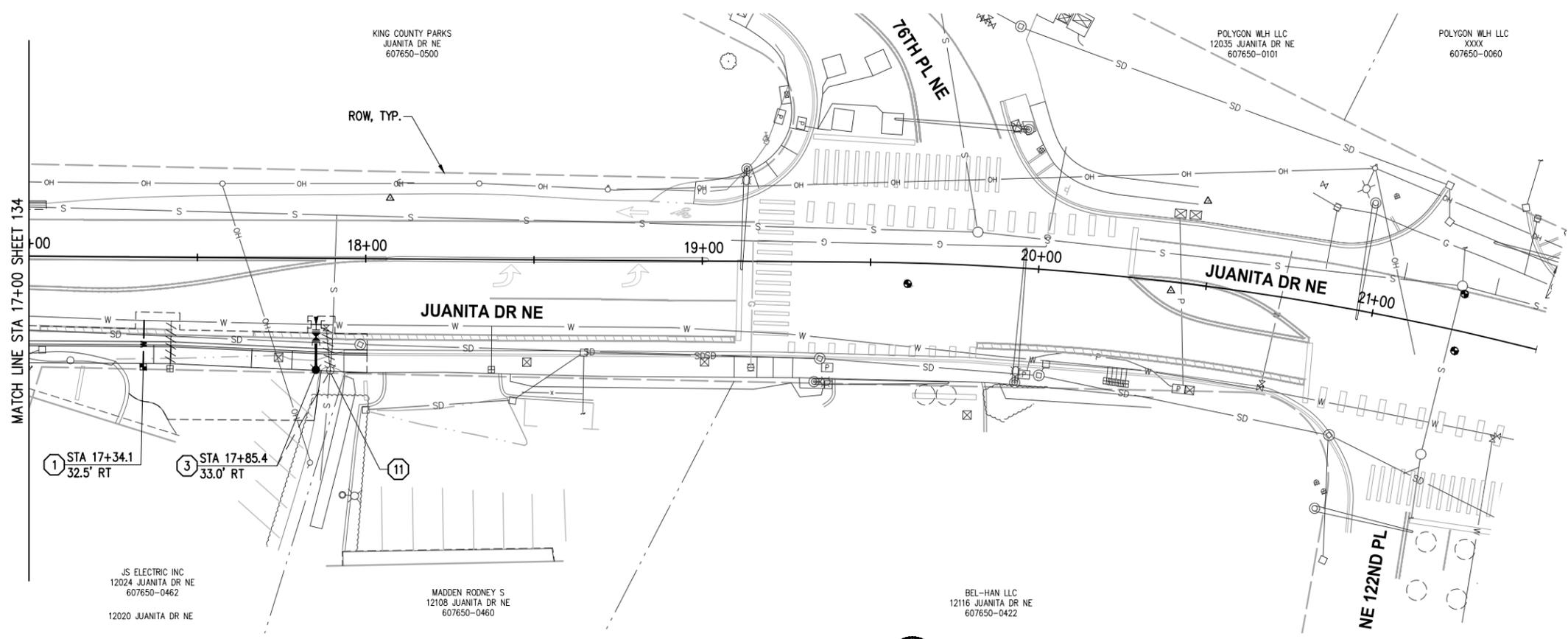
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NORTHSHORE UTILITY DISTRICT
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JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS
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NUD WATER & SEWER PLAN & PROFILE

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- ### GENERAL NOTES
- 1. FOR GENERAL NOTES SEE SHEET KEY, SHEET 132
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- ### WATER CONSTRUCTION NOTES
- (1) 1" SERVICE PER NUD WATER STD DETAILS 4 & 9, SEE WATER SERVICE TABLE SHEET 154, REMOVE AND DISPOSE OF EXISTING METER BOX AND APPURTENANCES. CLOSE CORP STOP AT MAIN OR ABANDON WITH MAIN. SALVAGE THE EXISTING METER AND COORDINATE WITH THE DISTRICT FOR REINSTALLATION INTO SERVICE SETTER. LOCATE AND RECONNECT THE EXISTING SERVICE LINE ON THE BACK SIDE OF THE METER
 - (3) HYDRANT ASSEMBLY PER NUD WATER STD DETAIL 3, EXCEPT INSTALL TAPPING TEE PER NUD STD DETAIL 20 IN LIEU OF DI TEE
 - (11) REMOVE HYDRANT ASSEMBLY, SEE GENERAL WATER NOTE 6 ON SHEET 132.

- ### LEGEND
- //// ABANDON PIPE, SEE GENERAL WATER NOTE 4, SHEET 132
 - W WATER LINE
 - ⊘ WATER VALVE AS NOTED
 - ⊘ WATER METER
 - LONG SLEEVE COUPLING
 - ⊘ FL x RJ ADAPTER
 - FIRE HYDRANT
 - ▲ CONC THRUST BLOCKS PER NUD STD DETAIL 1
 - CAP

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JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

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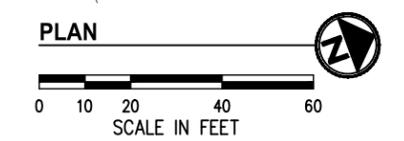
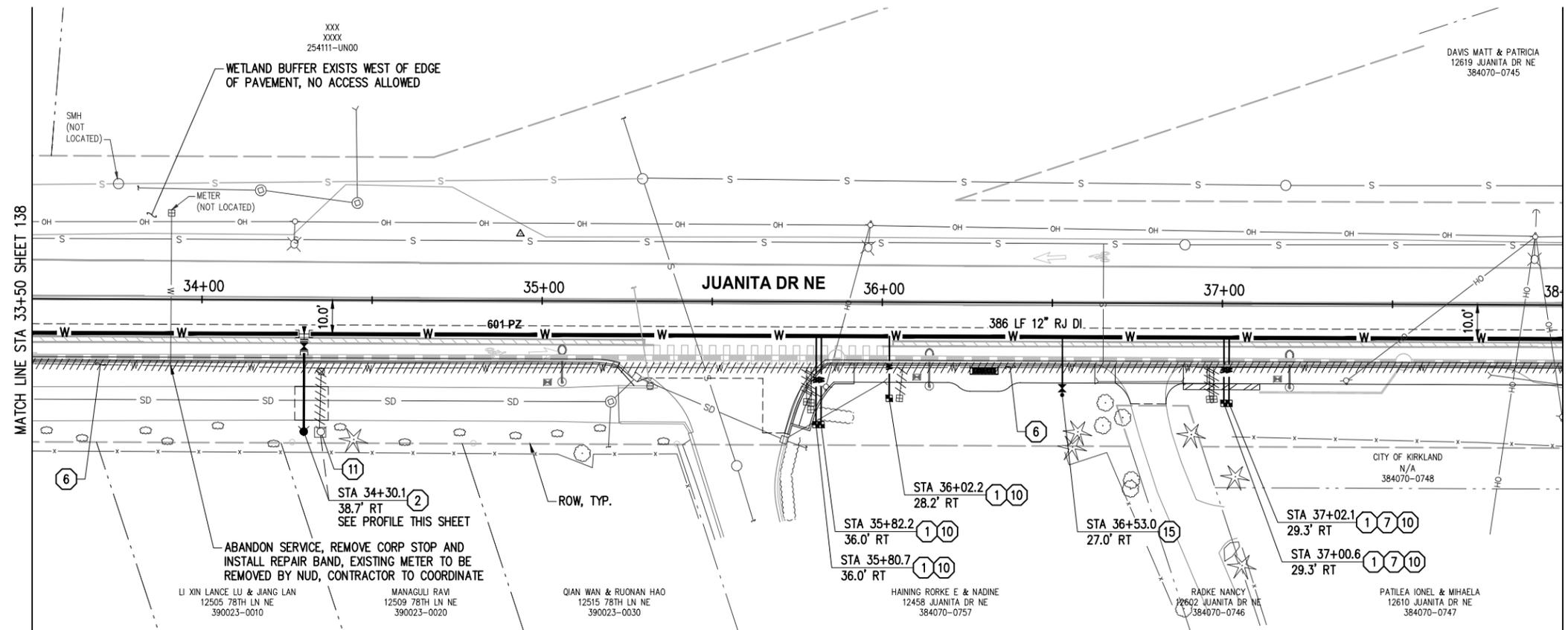
NUD WATER & SEWER PLAN & PROFILE

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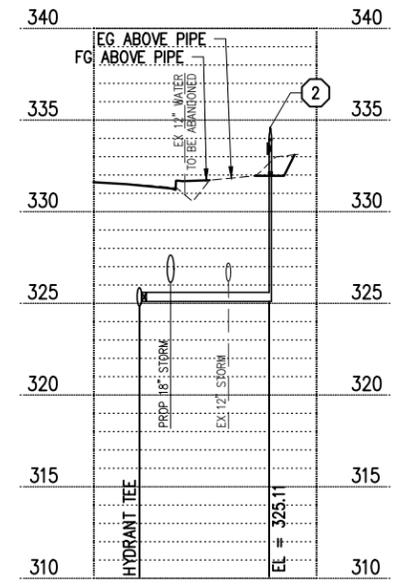
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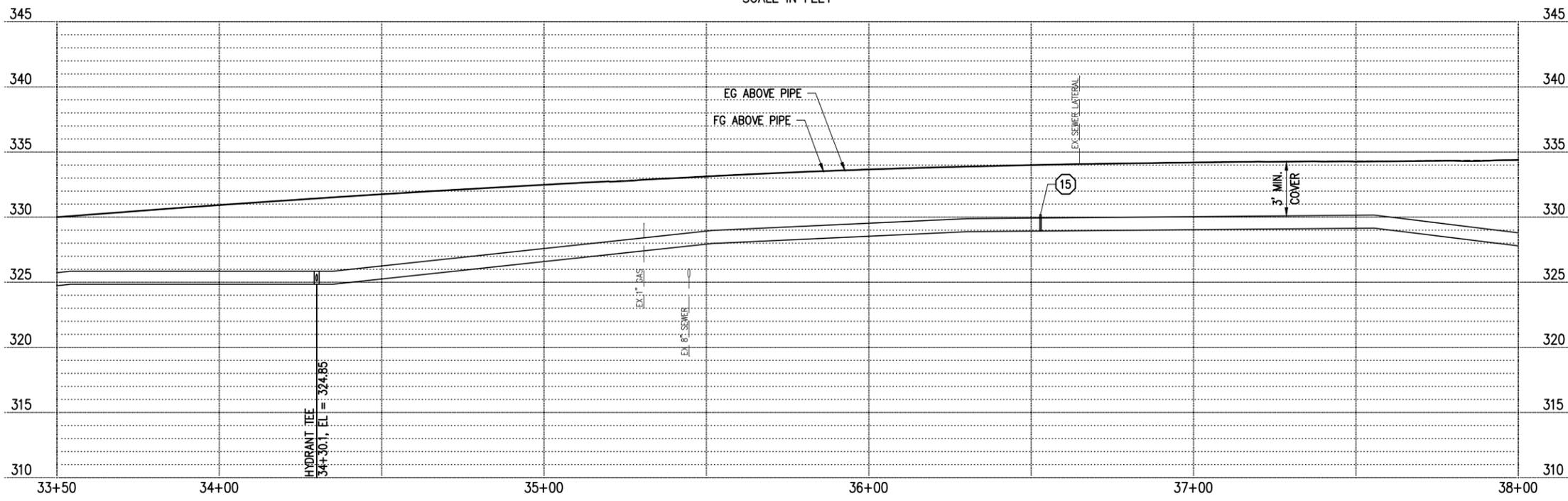
- ### GENERAL NOTES
- FOR GENERAL NOTES SEE SHEET KEY, SHEET 132
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 - HYDRANT ASSEMBLY PER NUD WATER STD DETAIL 3
 - REMOVE EXISTING WATER MAIN AS NEEDED TO INSTALL STORM LINE
 - INSTALL SERVICE UNDER BLOCK WALL PER DETAIL 1, SHEET 149
 - INSTALL PRESSURE REDUCING VALVE PER NUD STD DETAIL 21 BEHIND METER
 - REMOVE HYDRANT ASSEMBLY, SEE GENERAL WATER NOTE 6 ON SHEET 132.
 - 2" AIR & VACUUM RELIEF ASSEMBLY PER NUD WATER STD DETAIL 8



HYDRANT STA 34+30 PROFILE
HORIZ 1"=20', VERT 1"=5'

LEGEND

- ABANDON PIPE, SEE GENERAL WATER NOTE 4, SHEET 132
- WATER LINE
- WATER VALVE AS NOTED
- WATER METER
- LONG SLEEVE COUPLING
- FL x RJ ADAPTER
- FIRE HYDRANT
- CONC THRUST BLOCKS PER NUD STD DETAIL 1
- CAP



PROFILE
HORIZ 1"=20', VERT 1"=5'

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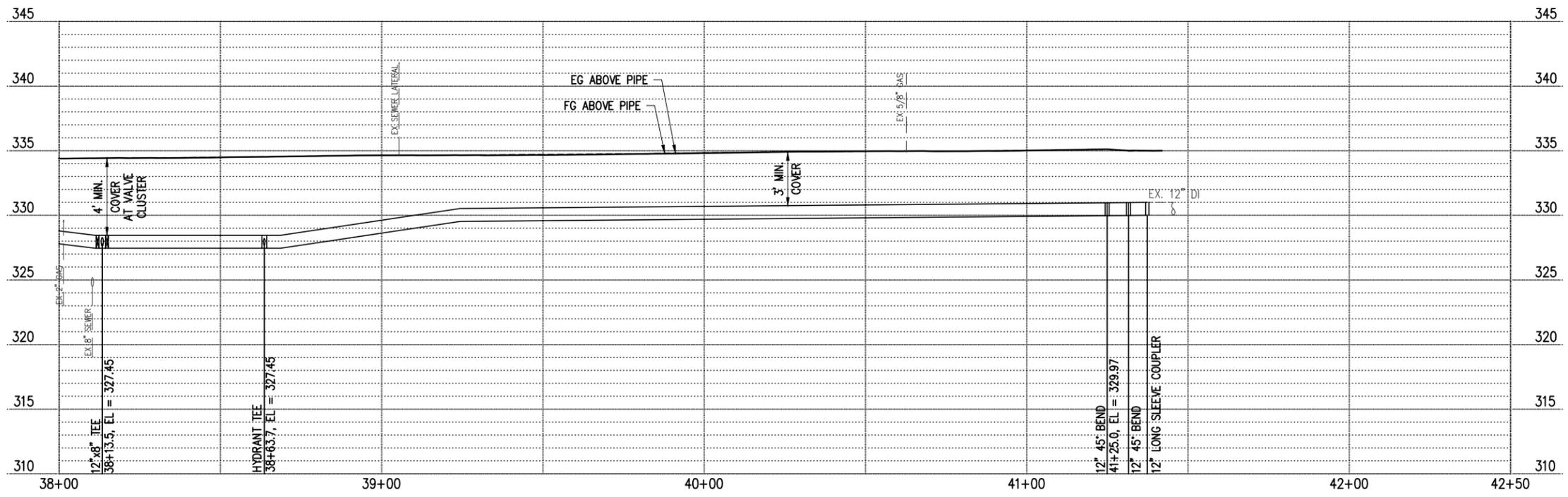
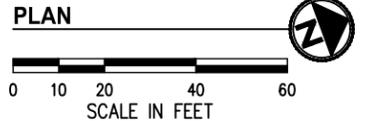
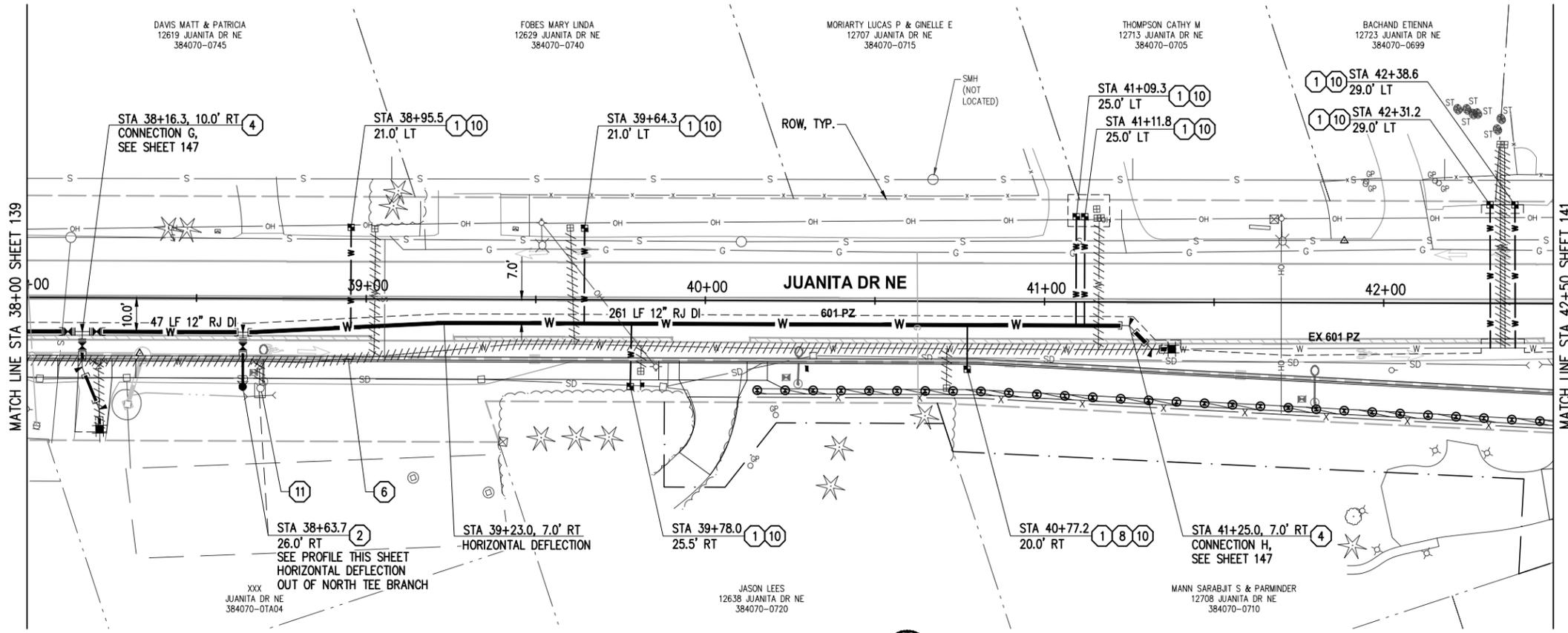
JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

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NUD WATER & SEWER PLAN & PROFILE

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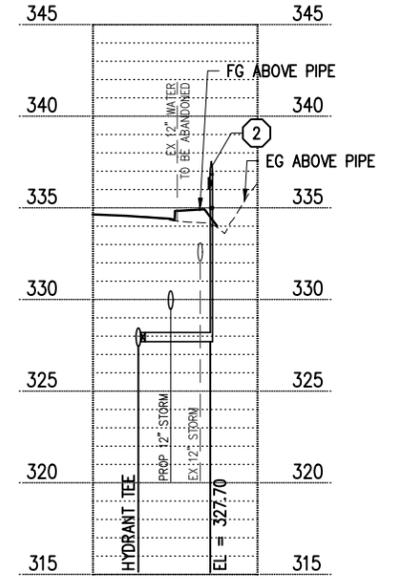
PROFILE
HORIZ 1"=20', VERT 1"=5'

GENERAL NOTES

1. FOR GENERAL NOTES SEE SHEET KEY, SHEET 132

WATER CONSTRUCTION NOTES

- (1) 1" SERVICE PER NUD WATER STD DETAILS 4 & 9, SEE WATER SERVICE TABLE SHEET 154, REMOVE AND DISPOSE OF EXISTING METER BOX AND APPURTENANCES. CLOSE CORP STOP AT MAIN OR ABANDON WITH MAIN. SALVAGE THE EXISTING METER AND COORDINATE WITH THE DISTRICT FOR REINSTALLATION INTO SERVICE SETTER. LOCATE AND RECONNECT THE EXISTING SERVICE LINE ON THE BACK SIDE OF THE METER
- (2) HYDRANT ASSEMBLY PER NUD WATER STD DETAIL 3
- (4) CONNECT TO EX WATER MAIN, SEE CONNECTION SHEETS 146-147
- (6) REMOVE EXISTING WATER MAIN AS NEEDED TO INSTALL STORM LINE
- (8) INSTALL SERVICE UNDER SOLDIER PILE WALL PER DETAIL 2, SHEET 149
- (10) INSTALL PRESSURE REDUCING VALVE PER NUD STD DETAIL 21 BEHIND METER
- (11) REMOVE HYDRANT ASSEMBLY, SEE GENERAL WATER NOTE 6 ON SHEET 132.



HYDRANT STA 38+64 PROFILE
HORIZ 1"=20', VERT 1"=5'

LEGEND

- ABANDON PIPE, SEE GENERAL WATER NOTE 4, SHEET 132
- WATER LINE
- WATER VALVE AS NOTED
- WATER METER
- LONG SLEEVE COUPLER
- FL x RJ ADAPTER
- FIRE HYDRANT
- CONC THRUST BLOCKS PER NUD STD DETAIL 1
- CAP



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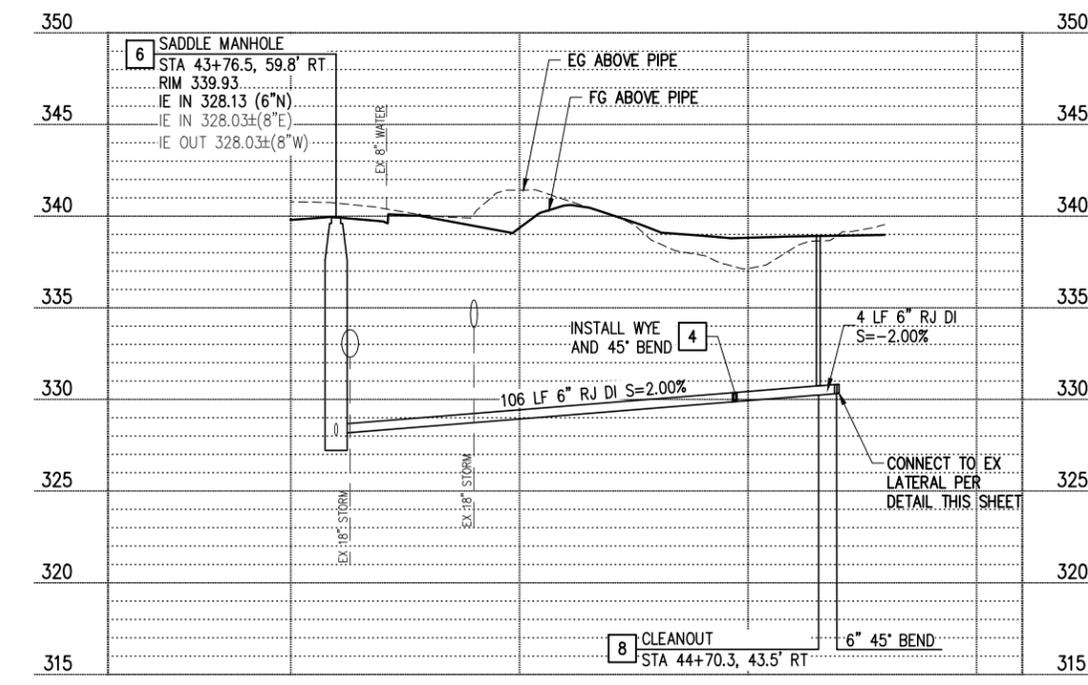
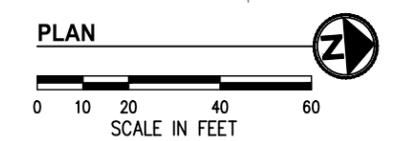
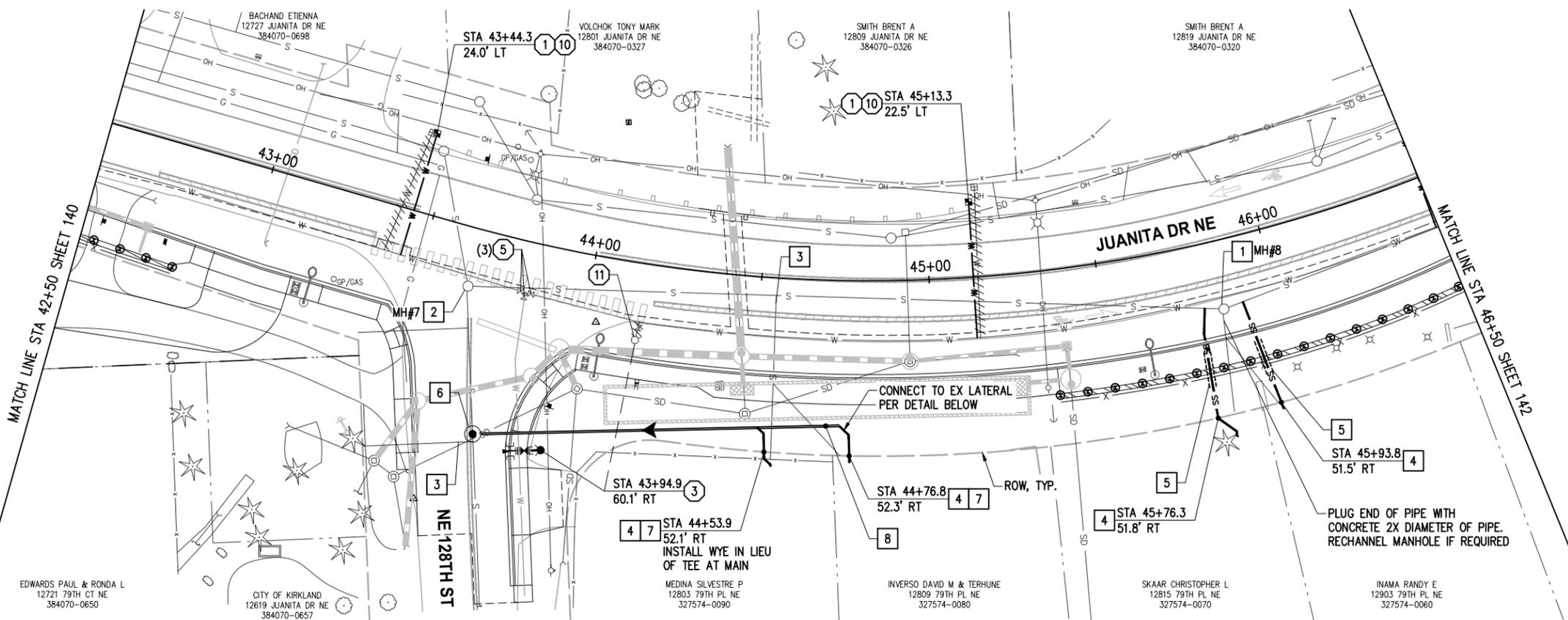
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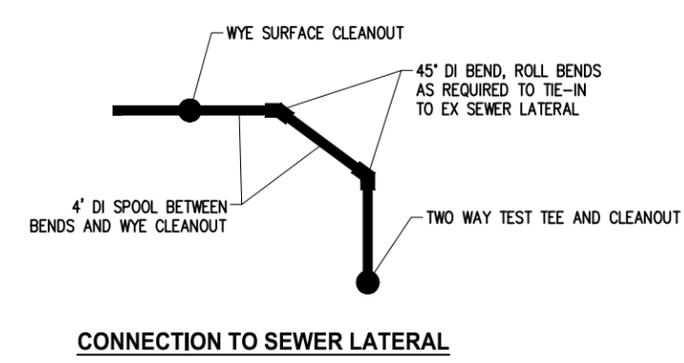
NUD WATER & SEWER PLAN & PROFILE

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SEWER PROFILE
HORIZ 1"=20', VERT 1"=5'



GENERAL NOTES

1. FOR GENERAL NOTES SEE SHEET KEY, SHEET 132

- ### WATER CONSTRUCTION NOTES
- ① 1" SERVICE PER NUD WATER STD DETAILS 4 & 9, SEE WATER SERVICE TABLE SHEET 154, REMOVE AND DISPOSE OF EXISTING METER BOX AND APPURTENANCES. CLOSE CORP STOP AT MAIN OR ABANDON WITH MAIN. SALVAGE THE EXISTING METER AND COORDINATE WITH THE DISTRICT FOR REINSTALLATION INTO SERVICE SETTER. LOCATE AND RECONNECT THE EXISTING SERVICE LINE ON THE BACK SIDE OF THE METER
 - ③ HYDRANT ASSEMBLY PER NUD WATER STD DETAIL 3, EXCEPT INSTALL TAPPING TEE PER NUD STD DETAIL 20 IN LIEU OF DI TEE
 - ⑤ REPLACE VALVE BOX AND INSTALL OPERATING NUT EXTENSION (WHERE REQ'D) PER NUD WATER STD DETAIL 11, REMOVE AND DISPOSE OF EXISTING VALVE BOX
 - ⑩ INSTALL PRESSURE REDUCING VALVE PER NUD STD DETAIL 21 BEHIND METER
 - ⑪ REMOVE HYDRANT ASSEMBLY, SEE GENERAL WATER NOTE 6 ON SHEET 132.

- ### SEWER CONSTRUCTION NOTES
- ① REPLACE MANHOLE FRAME, COVER AND ADJUSTMENT RINGS FOR EXISTING MANHOLE PER NUD STD SEWER DETAILS 1 & 5
 - ② REPLACE MANHOLE FRAME, COVER, CONE SECTION AND ADJUSTMENT RINGS FOR EXISTING MANHOLE. ADD MANHOLE SECTIONS AND MANHOLE STEPS AS NEEDED PER TABLE ON SHEET 156 ONTO EXISTING MANHOLE PER SEWER DETAIL 1, SHEET 155
 - ③ PERMANENTLY ABANDON EXISTING SIDE SEWER AT THE MAIN PER NUD STD SEWER DETAIL 8C
 - ④ INSTALL NEW SIDE SEWER STUB AND CONNECT TO EXISTING SIDE SEWER PER DETAIL 2, SHEET 155. POTHOLE EXISTING SIDE SEWER AND ALL UTILITY CROSSINGS (WATER, SEWER, POWER, TELEPHONE, GAS, ETC.) PRIOR TO COMMENCING SIDE SEWER INSTALLATION IN ORDER TO CONFIRM THE VERTICAL LOCATION AS SHOWN
 - ⑤ CENTER NEW DI SIDE SEWER BETWEEN SOLDIER PILES (SEE SHEETS 60-63) AND CONSTRUCT SIDE SEWER INSIDE STEEL CASING PER NUD STD SEWER DETAIL 10
 - ⑥ INSTALL SADDLE MANHOLE PER NUD STD SEWER DETAIL 2
 - ⑦ INSTALL TWO-WAY TEST TEE & CLEANOUT PER NUD STD SEWER DETAIL 9
 - ⑧ INSTALL WYE SURFACE CLEANOUT PER NUD STD SEWER DETAIL 9

- ### LEGEND
- ABANDON PIPE, SEE GENERAL WATER NOTE 4, SHEET 132
 - W — WATER LINE
 - ⊘ WATER VALVE AS NOTED
 - ⊠ WATER METER
 - ⊥ LONG SLEEVE COUPLING
 - ⊢ FL x RJ ADAPTER
 - FIRE HYDRANT
 - ▲ CONC THRUST BLOCKS PER NUD STD DETAIL 1
 - CAP



83, 12)(' \$, ' 352- 12
6859(< 12), (/ ' %22. 6
+25 = '\$780	9(57 '\$780
:63&6 1 =21(1\$'	1\$9'
(6, *1(' %<	'5\$:1 %<
**	**

KPG PSOMAS BID DOCUMENT
Tacoma - HIIHUVRQ SYHQXH
7DFRPD :\$
6HDWVVOH :HCPG6SMHH

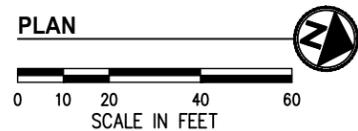
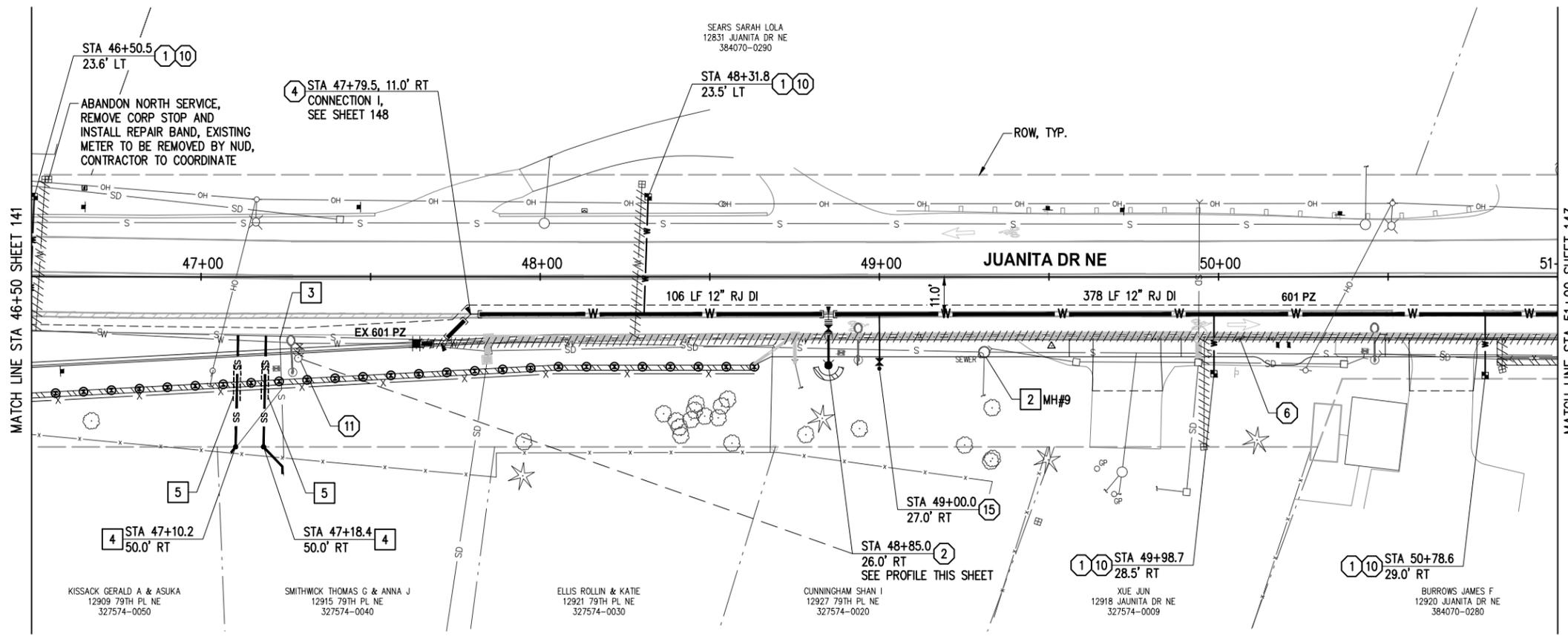
NORTHSHORE UTILITY DISTRICT
CITY OF KIRKLAND DEPARTMENT OF PUBLIC WORKS
123 FIFTH AVENUE KIRKLAND, WA 98033
(425) 587-3800 www.kirklandwa.gov

JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS
67\$ 72 67\$
NUD WATER & SEWER PLAN & PROFILE

REFERENCE SHEET NO.	
SHEET	2)
SHEETS	

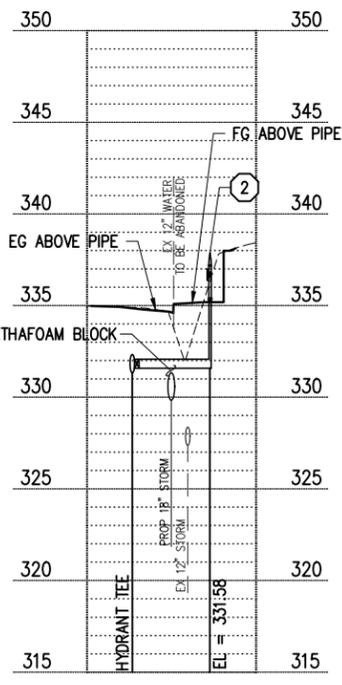
2FWREHU

67\$.75 1DPH .75 1.62175677 .75 .175667.21 6\$ (7-? (6, *1? 5\$.1? 6? & 2175677 .75 .1756667.21 -8\$1.7\$ '5.9 (.1756667.21 30 '\$5 (. *2&+ . 2352. (& 76? .5. /\$1'? 2FWREHU

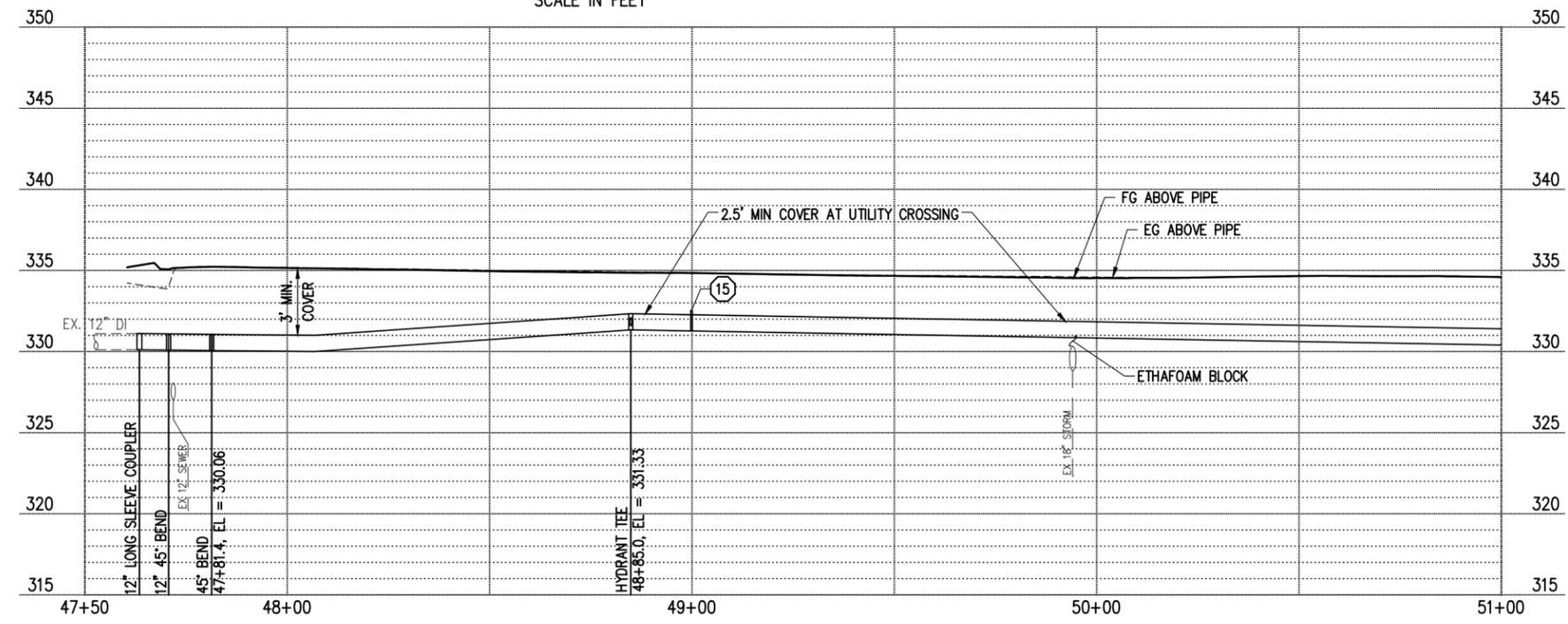


- GENERAL NOTES**
- FOR GENERAL NOTES SEE SHEET KEY, SHEET 132
- WATER CONSTRUCTION NOTES**
- 1" SERVICE PER NUD WATER STD DETAILS 4 & 9, SEE WATER SERVICE TABLE SHEET 154, REMOVE AND DISPOSE OF EXISTING METER BOX AND APPURTENANCES. CLOSE CORP STOP AT MAIN OR ABANDON WITH MAIN. SALVAGE THE EXISTING METER AND COORDINATE WITH THE DISTRICT FOR REINSTALLATION INTO SERVICE SETTER. LOCATE AND RECONNECT THE EXISTING SERVICE LINE ON THE BACK SIDE OF THE METER
 - HYDRANT ASSEMBLY PER NUD WATER STD DETAIL 3
 - CONNECT TO EX WATER MAIN, SEE CONNECTION SHEETS 146-147
 - REMOVE EXISTING WATER MAIN AS NEEDED TO INSTALL STORM LINE
 - INSTALL PRESSURE REDUCING VALVE PER NUD STD DETAIL 21 BEHIND METER
 - REMOVE HYDRANT ASSEMBLY, SEE GENERAL WATER NOTE 6 ON SHEET 132.
 - 2" AIR & VACUUM RELIEF ASSEMBLY PER NUD WATER STD DETAIL 8

- SEWER CONSTRUCTION NOTES**
- REPLACE MANHOLE FRAME, COVER, CONE SECTION AND ADJUSTMENT RINGS FOR EXISTING MANHOLE. ADD MANHOLE SECTIONS AND MANHOLE STEPS AS NEEDED PER TABLE ON SHEET 156 ONTO EXISTING MANHOLE PER SEWER DETAIL 1, SHEET 155
 - PERMANENTLY ABANDON EXISTING SIDE SEWER AT THE MAIN PER NUD STD SEWER DETAIL 8C
 - INSTALL NEW SIDE SEWER STUB AND CONNECT TO EXISTING SIDE SEWER PER DETAIL 2, SHEET 155. POTHOLE EXISTING SIDE SEWER AND ALL UTILITY CROSSINGS (WATER, SEWER, POWER, TELEPHONE, GAS, ETC.) PRIOR TO COMMENCING SIDE SEWER INSTALLATION IN ORDER TO CONFIRM THE VERTICAL LOCATION AS SHOWN
 - CENTER NEW DI SIDE SEWER BETWEEN SOLDIER PILES (SEE SHEETS 60-63) AND CONSTRUCT SIDE SEWER INSIDE STEEL CASING PER NUD STD SEWER DETAIL 10



HYDRANT STA 48+85 PROFILE
HORIZ 1"=20', VERT 1"=5'



PROFILE
HORIZ 1"=20', VERT 1"=5'

- LEGEND**
- ABANDON PIPE, SEE GENERAL WATER NOTE 4, SHEET 132
 - W WATER LINE
 - ⊗ WATER VALVE AS NOTED
 - ⊠ WATER METER
 - ⊡ LONG SLEEVE COUPLER
 - ⊞ FL x RJ ADAPTER
 - FIRE HYDRANT
 - ▲ CONC THRUST BLOCKS PER NUD STD DETAIL 1
 - CAP

811
Know what's below.
Call 811 before you dig.

83, 12) (' \$, ' 352- 12
6859 (< 12), (/ ' % 22. 6
+ 25 = ' \$ 780	9 (57 ' \$ 780
: 63 & 6 1 = 21 (1 \$'	1 \$ 9'
' (6, *1 (' % <	' 5 \$: 1 % <
..	..

KPG PSOMAS
Tacoma
-HIIHVURQ SYHOXH
ZDFRPD :\$
6HDWVWH :HCPPLGHH

BID DOCUMENT

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(425) 587-3800 www.kirklandwa.gov

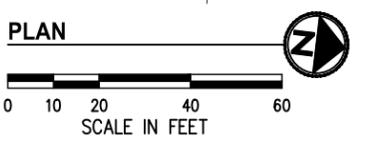
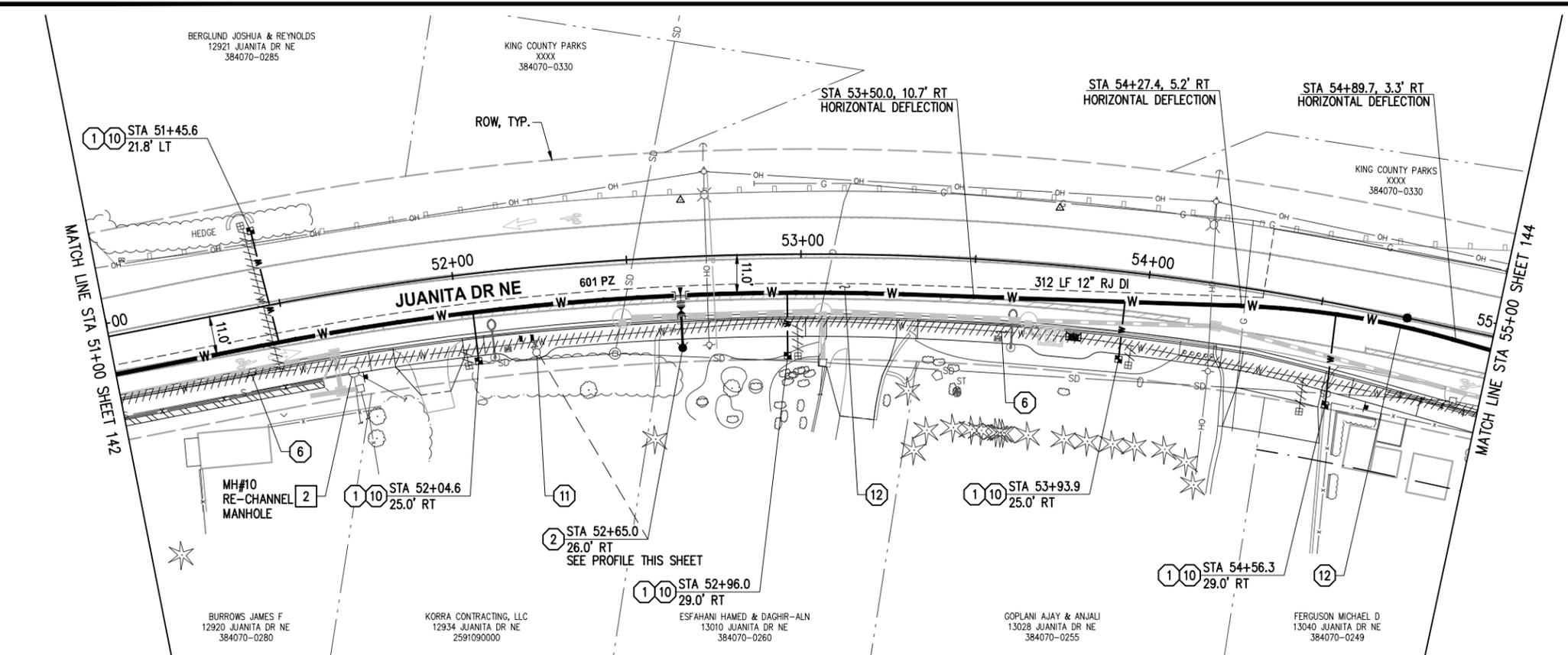
JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

67\$ 72 67\$

NUD WATER & SEWER PLAN & PROFILE

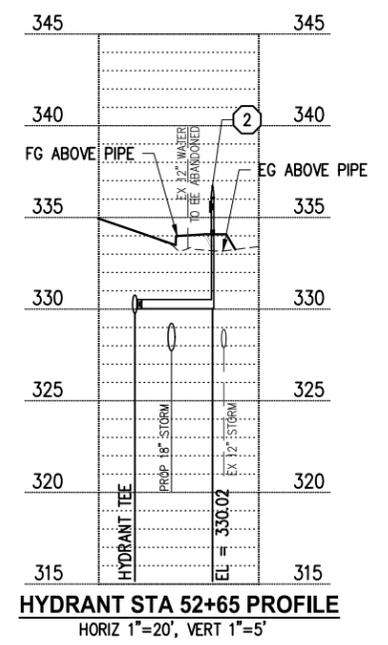
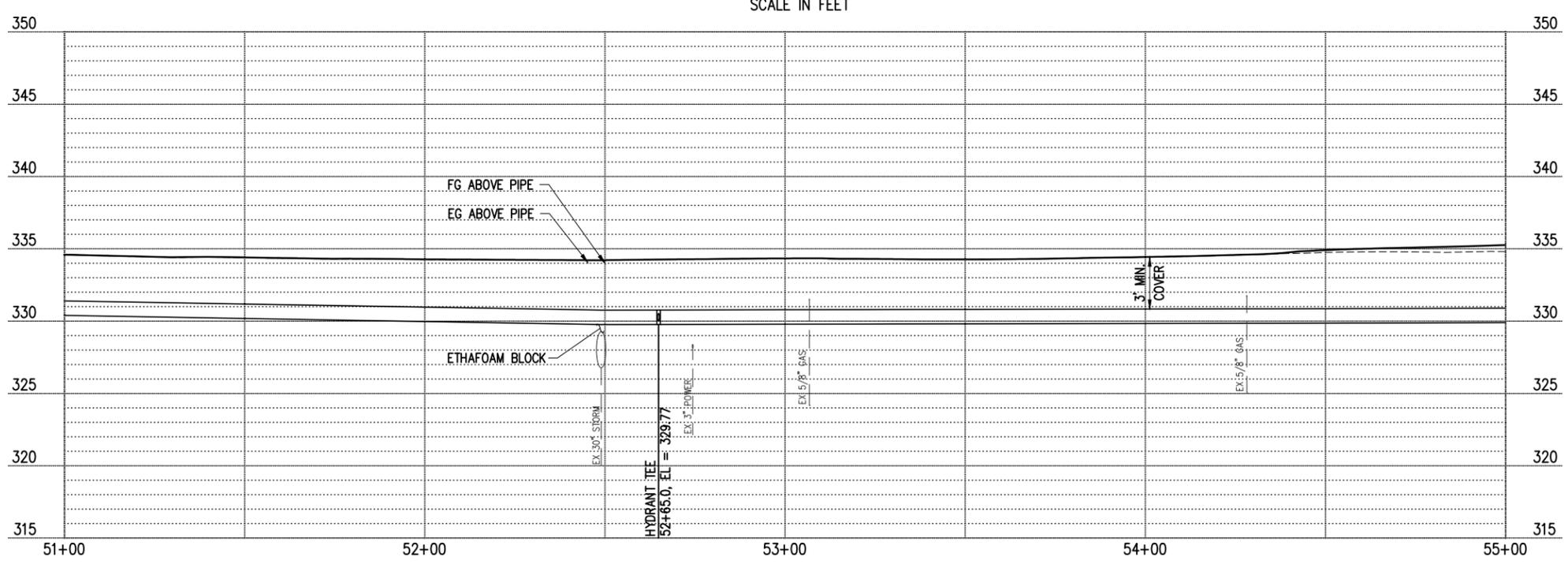
REFERENCE SHEET NO. SHEET 2) SHEETS

67\$:\$75 :DIRXW 1DPH :\$75 :\$5(. :2&+ . ?352.(&76?..5./\$1'? -8\$1,7\$ '5,9(,17(56(&7.21 6\$)(7<?'(6,*1?5\$:1*6?&2175\$&\$?? :\$75 :DIRXW 1DPH :\$75 :\$5(. :2&+ . ?352.(&76?..5./\$1'? -8\$1,7\$ '5,9(,17(56(&7.21 6\$)(7<?'(6,*1?5\$:1*6?&2175\$&\$?? :\$75 :DIRXW 1DPH :\$75 :\$5(. :2&+ . ?352.(&76?..5./\$1'? -8\$1,7\$ '5,9(,17(56(&7.21 6\$)(7<?'(6,*1?5\$:1*6?&2175\$&\$?? :\$75 :DIRXW 1DPH :\$75 :\$5(. :2&+ . ?352.(&76?..5./\$1'? -8\$1,7\$ '5,9(,17(56(&7.21 6\$)(7<?'(6,*1?5\$:1*6?&2175\$&\$??



- GENERAL NOTES**
- FOR GENERAL NOTES SEE SHEET KEY, SHEET 132
- WATER CONSTRUCTION NOTES**
- 1" SERVICE PER NUD WATER STD DETAILS 4 & 9, SEE WATER SERVICE TABLE SHEET 154, REMOVE AND DISPOSE OF EXISTING METER BOX AND APPURTENANCES. CLOSE CORP STOP AT MAIN OR ABANDON WITH MAIN. SALVAGE THE EXISTING METER AND COORDINATE WITH THE DISTRICT FOR REINSTALLATION INTO SERVICE SETTER. LOCATE AND RECONNECT THE EXISTING SERVICE LINE ON THE BACK SIDE OF THE METER
 - HYDRANT ASSEMBLY PER NUD WATER STD DETAIL 3
 - REMOVE EXISTING WATER MAIN AS NEEDED TO INSTALL STORM LINE
 - INSTALL PRESSURE REDUCING VALVE PER NUD STD DETAIL 21 BEHIND METER
 - REMOVE HYDRANT ASSEMBLY, SEE GENERAL WATER NOTE 6 ON SHEET 132.
 - DEFLECT WATER MAIN PER MANUFACTURERS RECOMMENDATIONS TO MAINTAIN ALIGNMENT

- SEWER CONSTRUCTION NOTES**
- REPLACE MANHOLE FRAME, COVER, CONE SECTION AND ADJUSTMENT RINGS FOR EXISTING MANHOLE. ADD MANHOLE SECTIONS AND MANHOLE STEPS AS NEEDED PER TABLE ON SHEET 156 ONTO EXISTING MANHOLE PER SEWER DETAIL 1, SHEET 155



- LEGEND**
- ABANDON PIPE, SEE GENERAL WATER NOTE 4, SHEET 132
 - W WATER LINE
 - Water Valve Symbols: WATER VALVE AS NOTED, WATER METER, LONG SLEEVE COUPLING, FL x RJ ADAPTER, FIRE HYDRANT
 - CONC THRUST BLOCKS PER NUD STD DETAIL 1
 - CAP

811
 Know what's below.
 Call 811 before you dig.

83, 12	352-12
6859(<12), (/) %22.6
+25 = '\$780	9(57 '\$780
:63&6 1 =21 (1\$'	1\$9'
(6,*1(' %<	'5\$:1 %<
**	**

KPG PSOMAS
 BID DOCUMENT
 Tacoma
 -HIIHVURQ SYHXH
 ZDFRPD :\$
 6HDWVOH :HCPGEMHH

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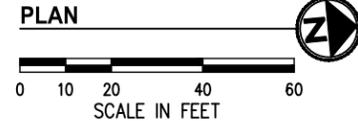
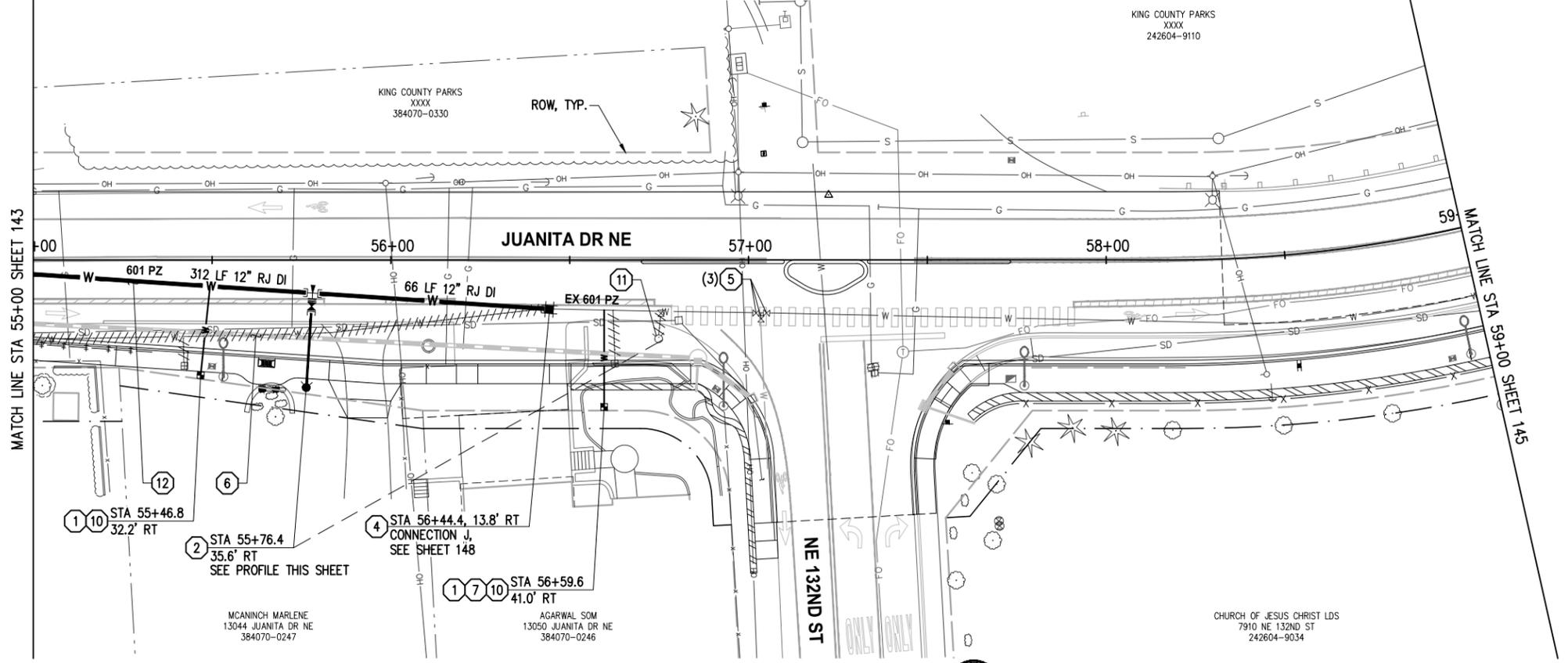
**JUANITA DRIVE MULTIMODAL,
 INTERSECTION & SAFETY IMPROVEMENTS**

67\$ 72 67\$

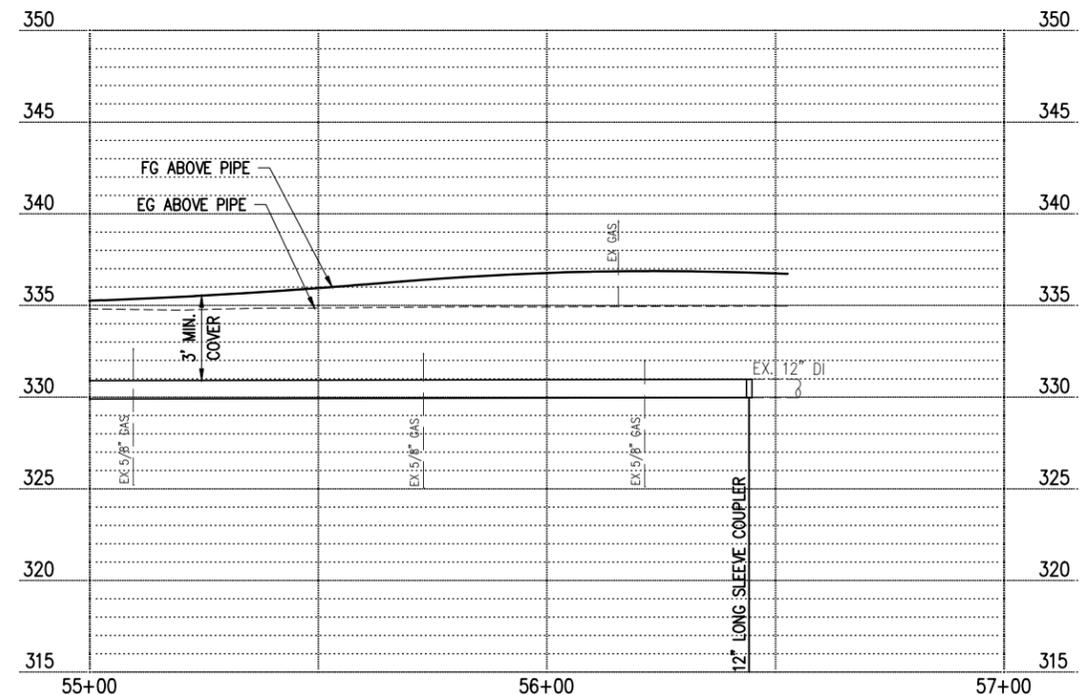
NUD WATER & SEWER PLAN & PROFILE

REFERENCE SHEET NO. SHEET 2) SHEETS

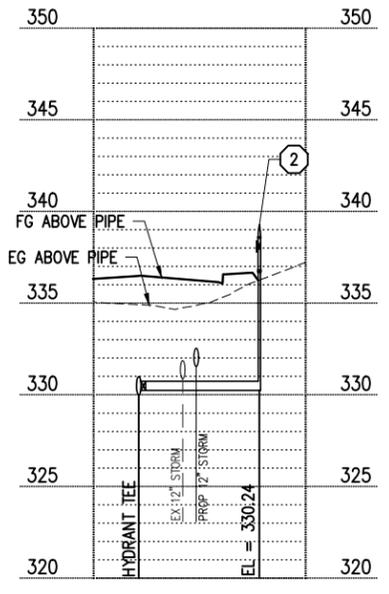
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- GENERAL NOTES**
- FOR GENERAL NOTES SEE SHEET KEY, SHEET 132
- WATER CONSTRUCTION NOTES**
- 1" SERVICE PER NUD WATER STD DETAILS 4 & 9, SEE WATER SERVICE TABLE SHEET 154, REMOVE AND DISPOSE OF EXISTING METER BOX AND APPURTENANCES. CLOSE CORP STOP AT MAIN OR ABANDON WITH MAIN. SALVAGE THE EXISTING METER AND COORDINATE WITH THE DISTRICT FOR REINSTALLATION INTO SERVICE SETTER. LOCATE AND RECONNECT THE EXISTING SERVICE LINE ON THE BACK SIDE OF THE METER
 - HYDRANT ASSEMBLY PER NUD WATER STD DETAIL 3
 - CONNECT TO EX WATER MAIN, SEE CONNECTION SHEETS 146-147
 - REPLACE VALVE BOX AND INSTALL OPERATING NUT EXTENSION (WHERE REQ'D) PER NUD WATER STD DETAIL 11, REMOVE AND DISPOSE OF EXISTING VALVE BOX
 - REMOVE EXISTING WATER MAIN AS NEEDED TO INSTALL STORM LINE
 - INSTALL SERVICE UNDER BLOCK WALL PER DETAIL 1, SHEET 149
 - INSTALL PRESSURE REDUCING VALVE PER NUD STD DETAIL 21 BEHIND METER
 - REMOVE HYDRANT ASSEMBLY, SEE GENERAL WATER NOTE 6 ON SHEET 132.
 - DEFLECT WATER MAIN PER MANUFACTURERS RECOMMENDATIONS TO MAINTAIN ALIGNMENT



PROFILE
HORIZ 1"=20', VERT 1"=5'



HYDRANT STA 55+76 PROFILE
HORIZ 1"=20', VERT 1"=5'

- LEGEND**
- ABANDON PIPE, SEE GENERAL WATER NOTE 4, SHEET 132
 - WATER LINE
 - WATER VALVE AS NOTED
 - WATER METER
 - LONG SLEEVE COUPLER
 - FL x RJ ADAPTER
 - FIRE HYDRANT
 - CONC THRUST BLOCKS PER NUD STD DETAIL 1
 - CAP

\$7	12	5(9,6,21	% <

	83, 12) (' \$, ' 352 - 12
	6859 (< 12), (/' %22. 6
	+25 = '\$780	9(57 '\$780
	:63&6 1 = 21 (1\$'	1\$9'
	(6, *1 (' % <	'5\$: 1 % <
Know what's below. Call 811 before you dig.		

KPG PSOMAS

Tacoma
-HIIHUVRQ SYHOXH
ZDFRPD :\$
6HDWUOH :HCPLESMHH

BID DOCUMENT

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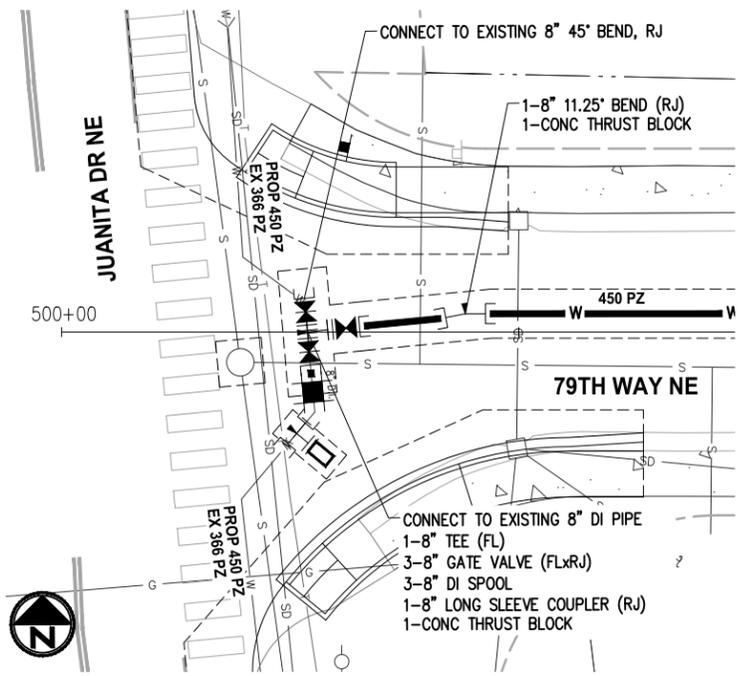
JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

67\$ 72 67\$

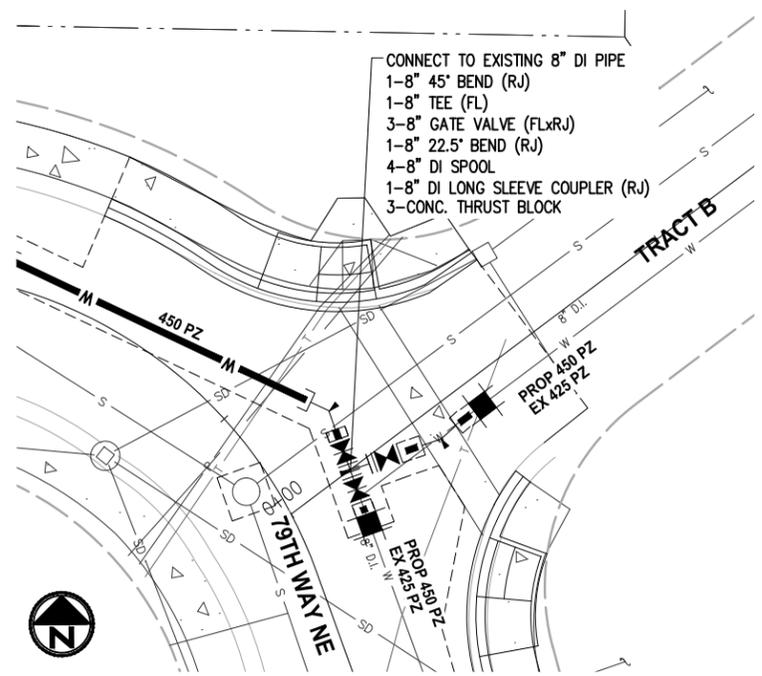
NUD WATER & SEWER PLAN & PROFILE

REFERENCE SHEET NO. SHEET 2) SHEETS

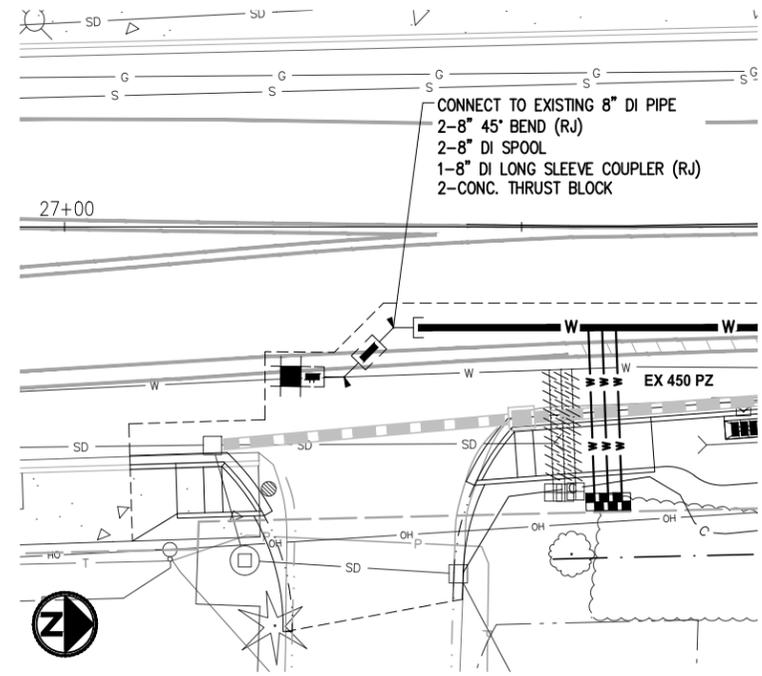
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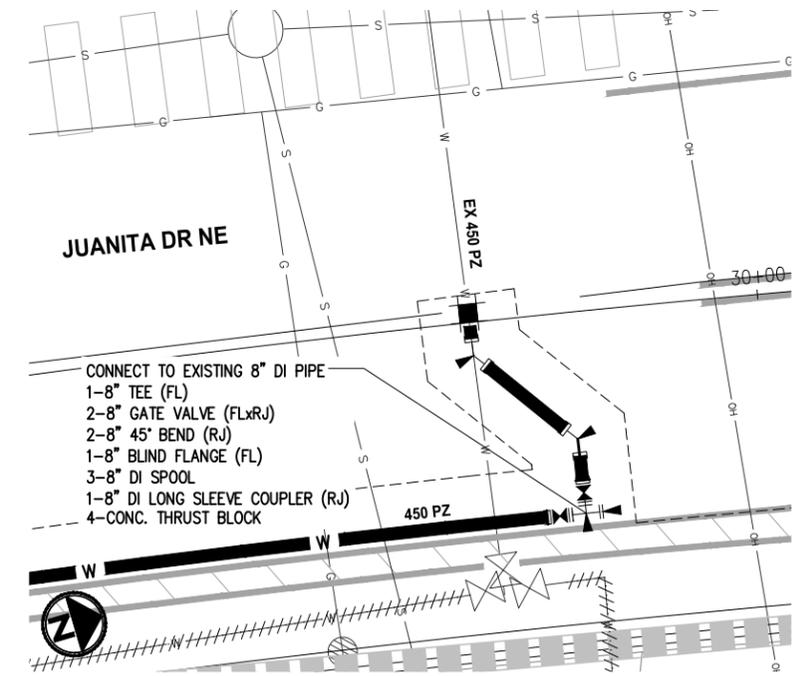
CONNECTION DETAIL A
HOR: 1"=10'



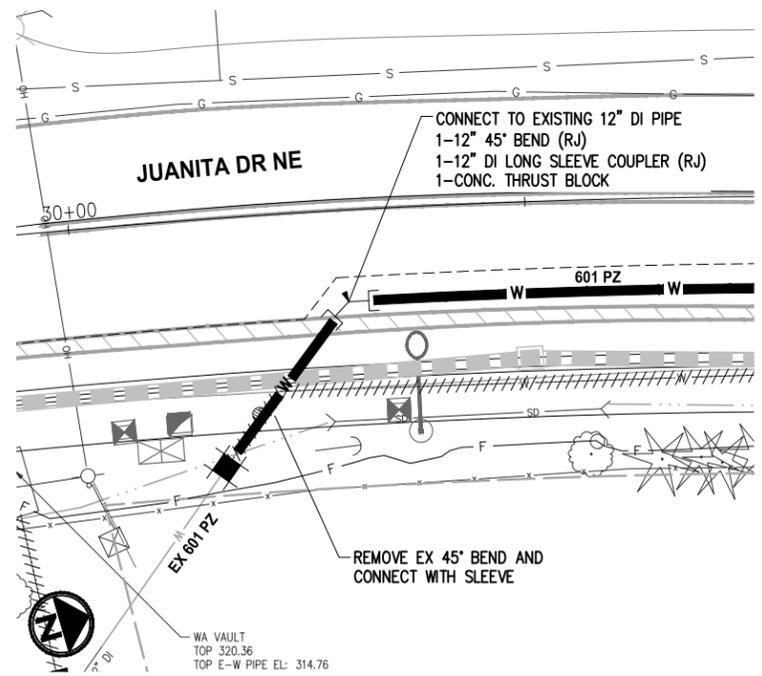
CONNECTION DETAIL B
HOR: 1"=10'



CONNECTION DETAIL C
HOR: 1"=10'



CONNECTION DETAIL D
HOR: 1"=5'



CONNECTION DETAIL E
HOR: 1"=10'

\$7(12	5(9,6,21	%<		



83, 12)(' \$,' 352- 12
6859(< 12),(/' %22. 6
+25= '\$780	9(57 '\$780
:63&6 1 =21(1\$'	1\$9'
'(6, *1(' %<	'5\$: 1 %<
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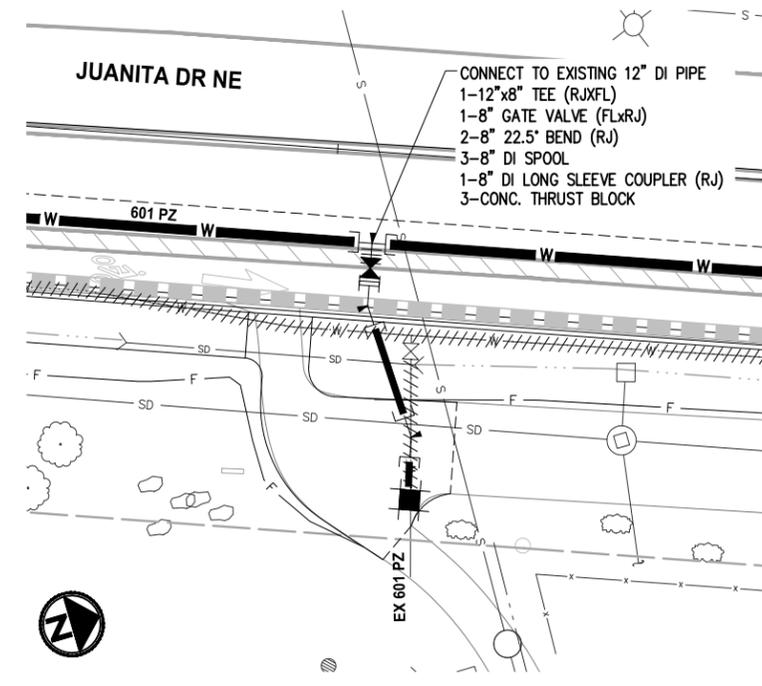


KPG PSOMAS BID DOCUMENT
 Tacoma - H11HUVRQ SYHXQX
 7DFRPD :\$
 6HDWVOH :HCPPL65MH

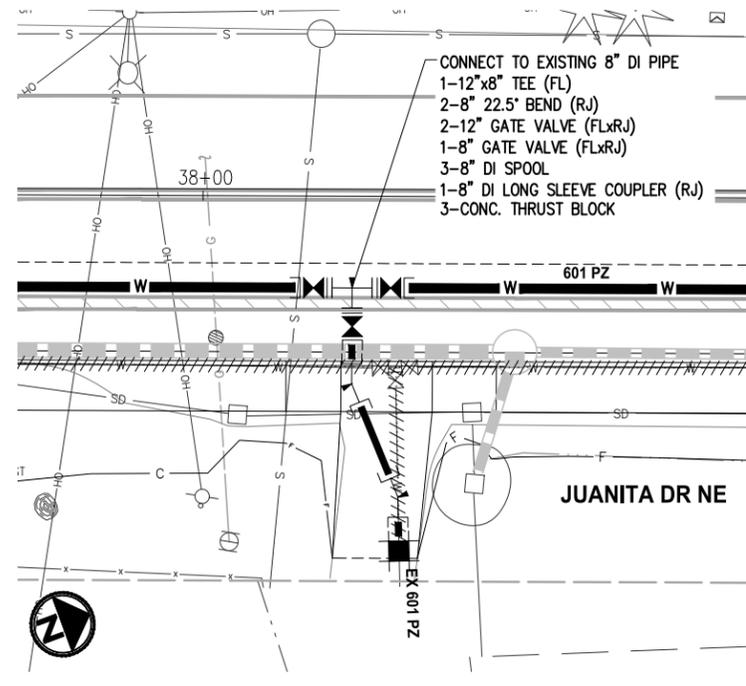
NORTHSHORE UTILITY DISTRICT WATER SEWER
 CITY OF KIRKLAND WASHINGTON
 CITY OF KIRKLAND
 DEPARTMENT OF PUBLIC WORKS
 123 FIFTH AVENUE KIRKLAND, WA 98033
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JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS
NUD WATER CONNECTIONS
 REFERENCE SHEET NO. SHEET 2) SHEETS

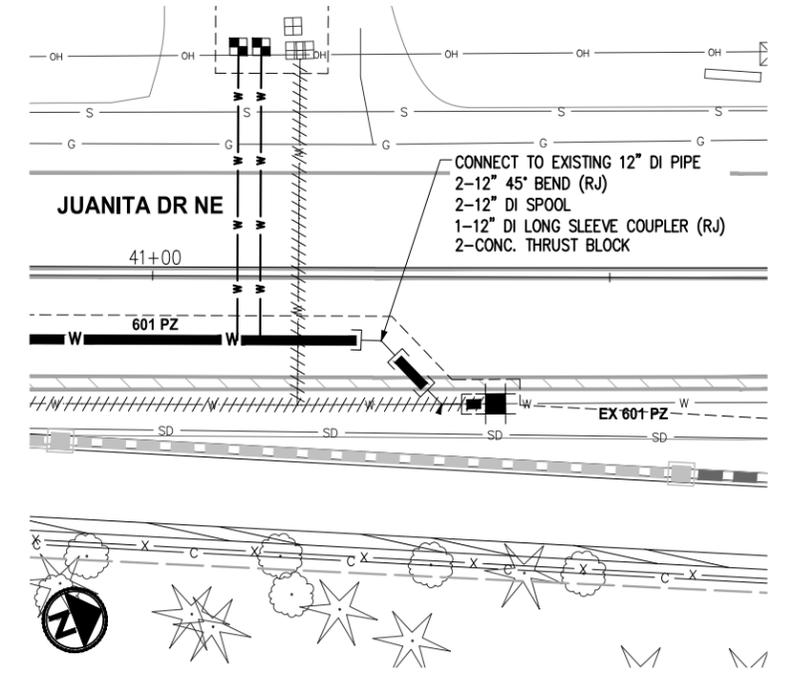
30 '\$5(. *2&+ . ?352-(&76?..5./.\$1'? -8\$1.7\$ '5.9(. 17(56(&7.21 6\$)(7<?'(6,*1?5\$.1*6?&2175\$&7? :.75 :.1' : /D\RXW 1DPH :75 :.1'



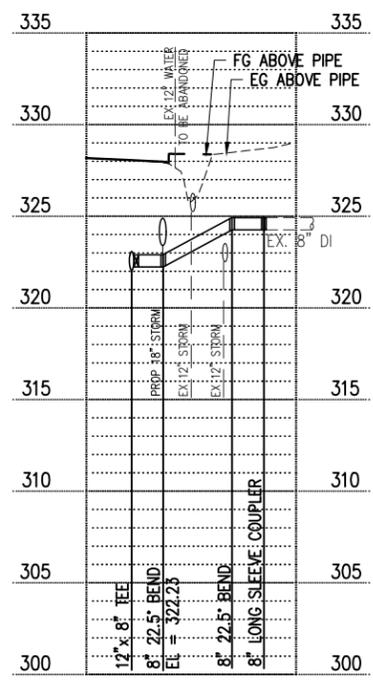
CONNECTION DETAIL F
HOR: 1"=10'



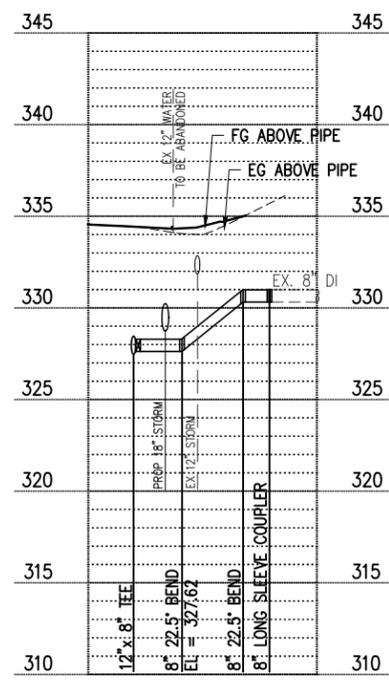
CONNECTION DETAIL G
HOR: 1"=10'



CONNECTION DETAIL H
HOR: 1"=10'



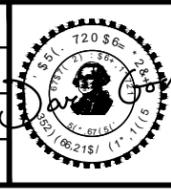
CONNECTION F PROFILE
HORIZ 1"=20', VERT 1"=5'



CONNECTION G PROFILE
HORIZ 1"=20', VERT 1"=5'



83, 12)(' \$,' 352- 12
6859(< 12), (/ ' %22. 6
+25 = '\$780	9(57 '\$780
:63&6 1 =21(1\$'	1\$9'
'(6, *1(' %<	'5\$:1 %<
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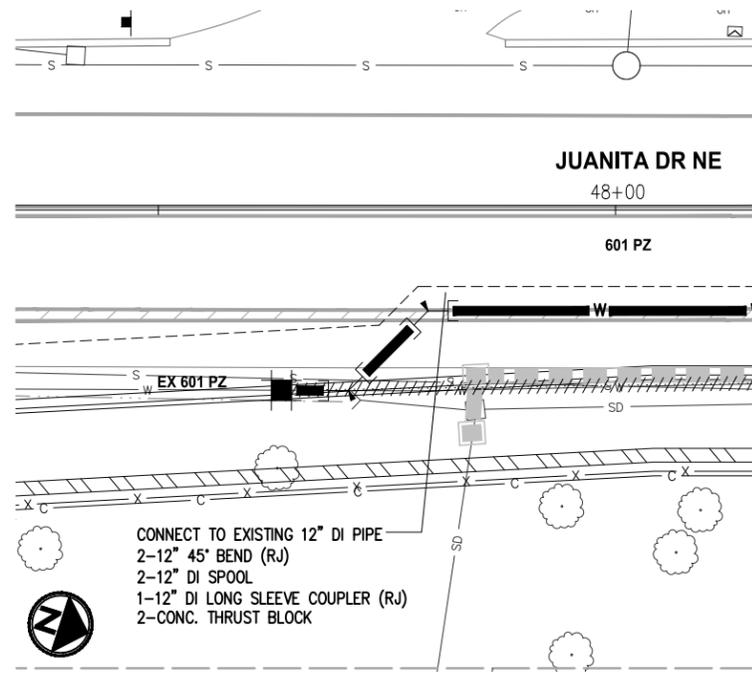
KPG PSOMAS BID DOCUMENT
 Tacoma - H1HUVRQ SYHQXH
 ZDFRPD :\$
 6HD\W\OH :HCP\6\6\HH

NORTHSHORE UTILITY DISTRICT
CITY OF KIRKLAND WASHINGTON
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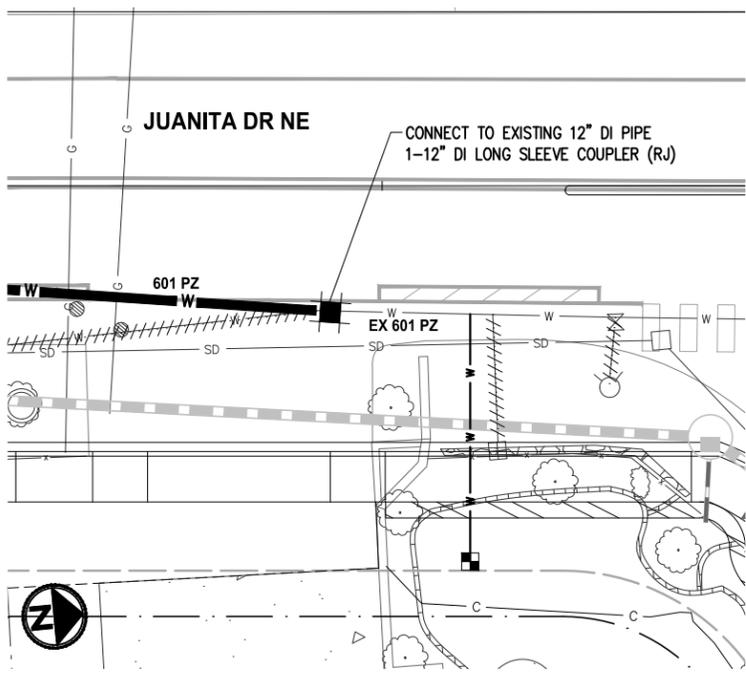
**JUANITA DRIVE MULTIMODAL,
 INTERSECTION & SAFETY IMPROVEMENTS**
 NUD WATER CONNECTIONS

REFERENCE SHEET NO.	
SHEET	2)
SHEETS	

30 '55(. '2&+ . ?352-(&76?..5./\$'1'? -8\$1,7\$ '5,9(. ,17(56(&7.21 6\$)(7<?'(6,*1?'5\$:.1*6?&2175\$&7? :\$75 ;,1* '.* /D\RXW 1DPH :75 ;,1*



CONNECTION DETAIL I
HORIZ: 1"=10'



CONNECTION DETAIL J
HORIZ: 1"=10'

\$7(12	5(9,6,21	%<	



83, 12)(' \$,' 352- 12
6859(< 12),(/' %22. 6
+25= '\$780	9(57 '\$780
:63&6 1 =21(1\$'	1\$9'
'(6, *1(' %<	'5\$:1 %<
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720 S.E.
 WASHINGTON, WA 98003
 (206) 462-1515 / (1) 216

KPG
PSOMAS

BID
DOCUMENT

Tacoma
-HIIHUVRQ SYHQXH
ZDFRPD :\$
6HDWQWOH :HCPQL6XHH

WATER SEWER

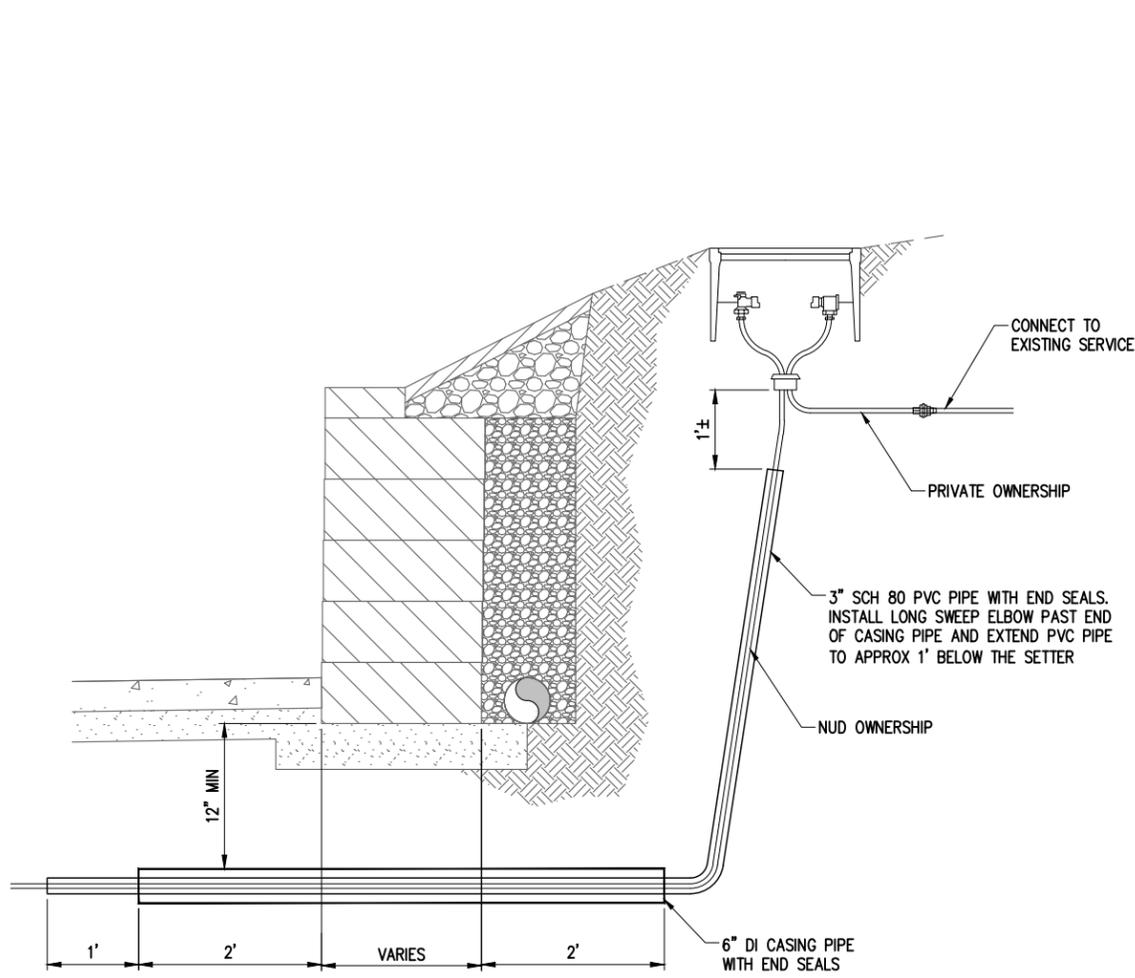
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DEPARTMENT OF PUBLIC WORKS
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(425) 587-3800 www.kirklandwa.gov

**JUANITA DRIVE MULTIMODAL,
INTERSECTION & SAFETY IMPROVEMENTS**

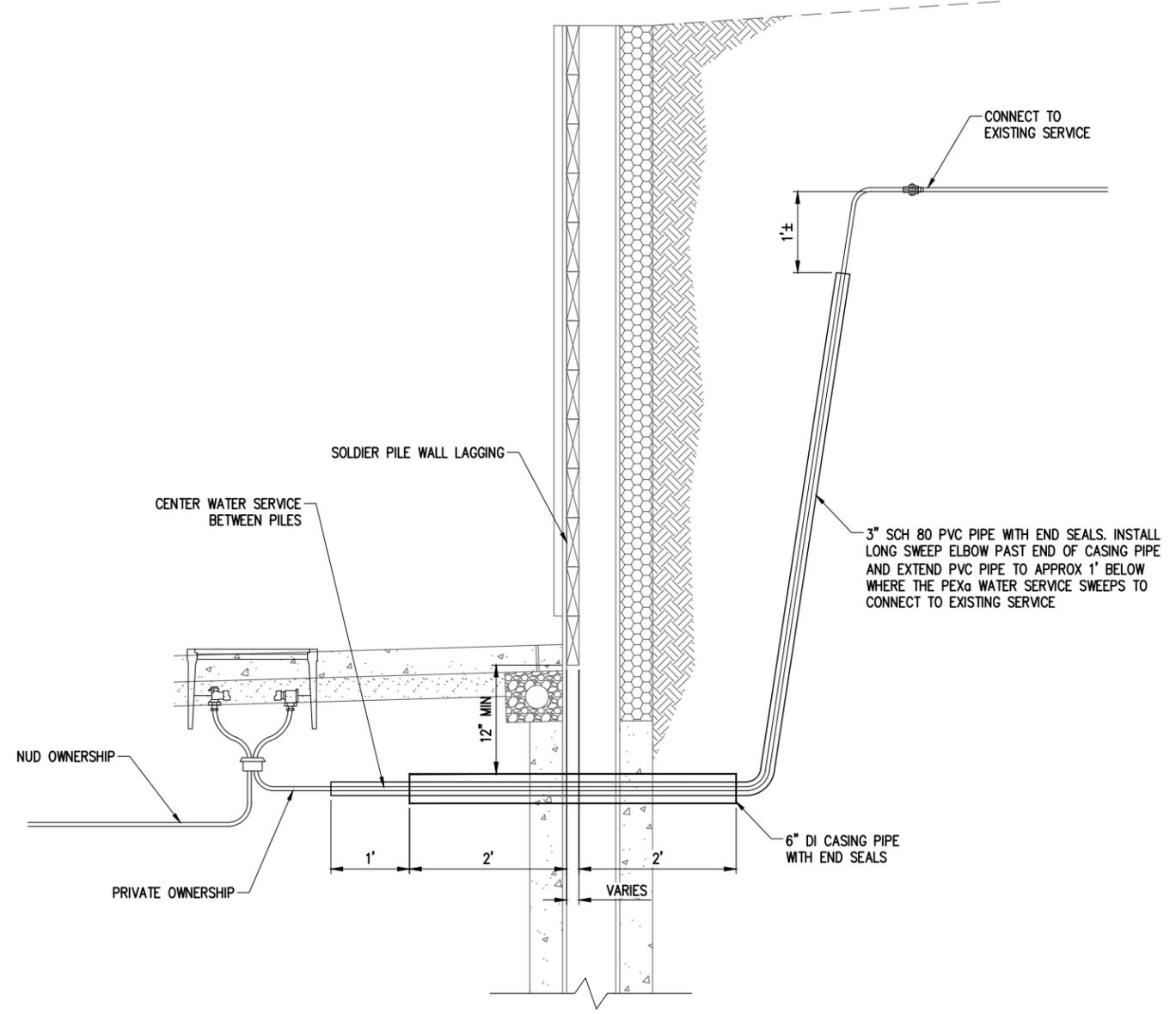
NUD WATER CONNECTIONS

REFERENCE SHEET NO.	
SHEET	
2)	
SHEETS	

0 9211,(672&.0\$1 . 3352-(&767..5./\$1' ? -8\$1,7\$ '5,9(. 17(56(&7,21 6\$)(7<?'(6, +1?'5\$.;1*6?&2175\$&7?? 18'(7 :* /DIRXW 1DPH 18' '7



SERVICE UNDER BLOCK WALL DETAIL 1
NTS



SERVICE UNDER SOLDIER PILE WALL DETAIL 2
NTS

12	
11	
10	
9	
8	
7	
6	
5	
4	
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2	
1	



83, 12)('\$, ' 352- 12
6859(< 12), (/ ' % 22. 6
+25= '\$780	9(57 '\$780
:63&6 1 =21(1\$'	1\$9'
'(6, '1(' %<	'5\$: 1 %<
..	..

KPG PSOMAS

BID DOCUMENT

Tacoma
-HIIHUVRO \$YHOXH
7DFRPD :\$
6HDVWOH :HCP&6KH H

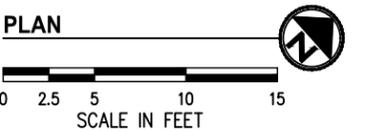
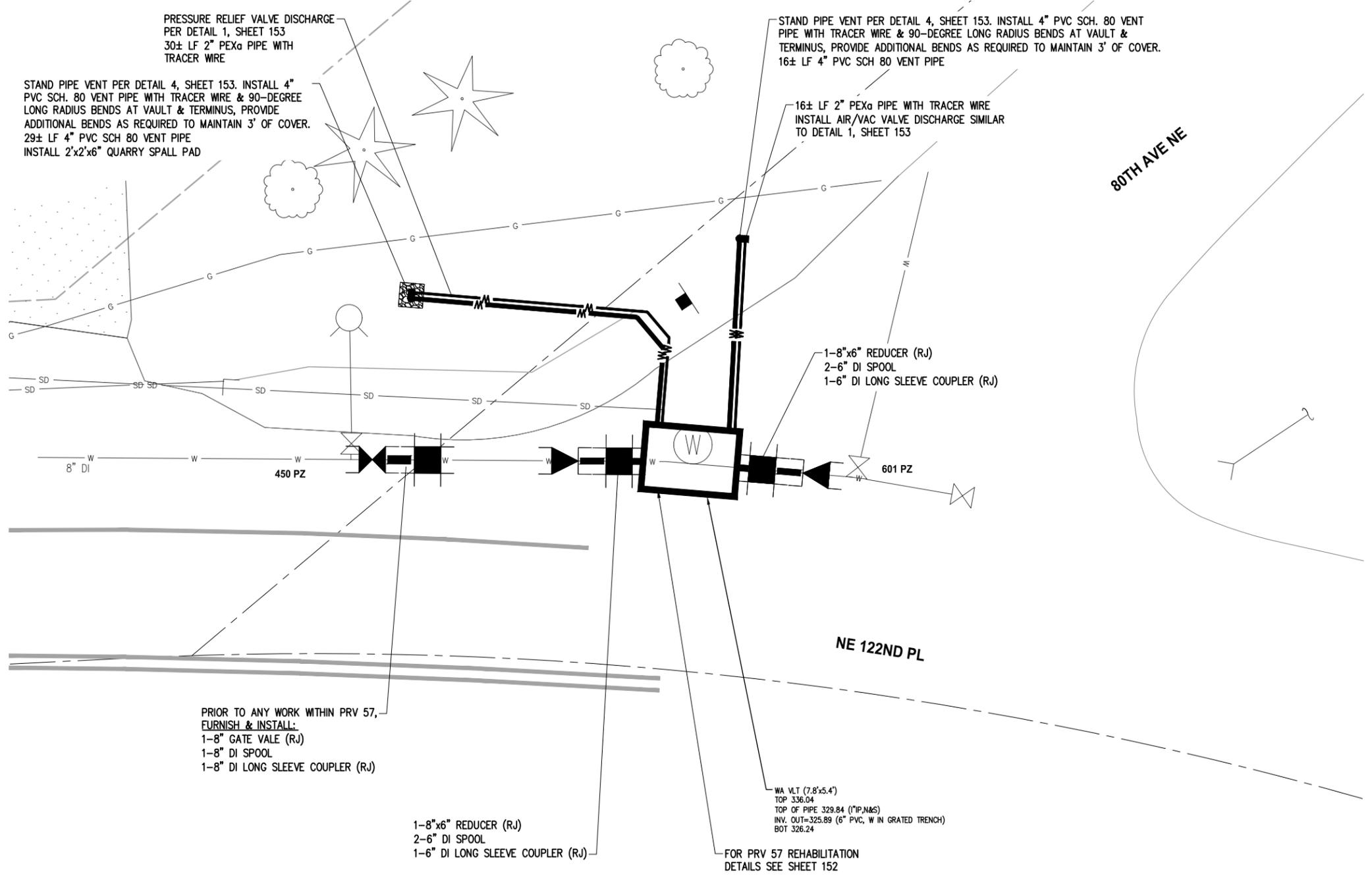
WATERSHORE UTILITY DISTRICT

CITY OF KIRKLAND WASHINGTON

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DEPARTMENT OF PUBLIC WORKS
123 FIFTH AVENUE KIRKLAND, WA 98033
(425) 587-3800 www.kirklandwa.gov

JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS		REFERENCE SHEET NO.
NUD WATER DETAILS		SHEET
		2)
		SHEETS

30 9211(.672&.0\$1 . 352.-(&76?..5./\$1' ? -8\$1,7\$ '5,9(.17(56(&7,21 6\$(7<'6,1'5\$:1'6'2175\$&7? :\$75B359 B3/\$1 '.* /DIRXW 1DPH 2FWREHU



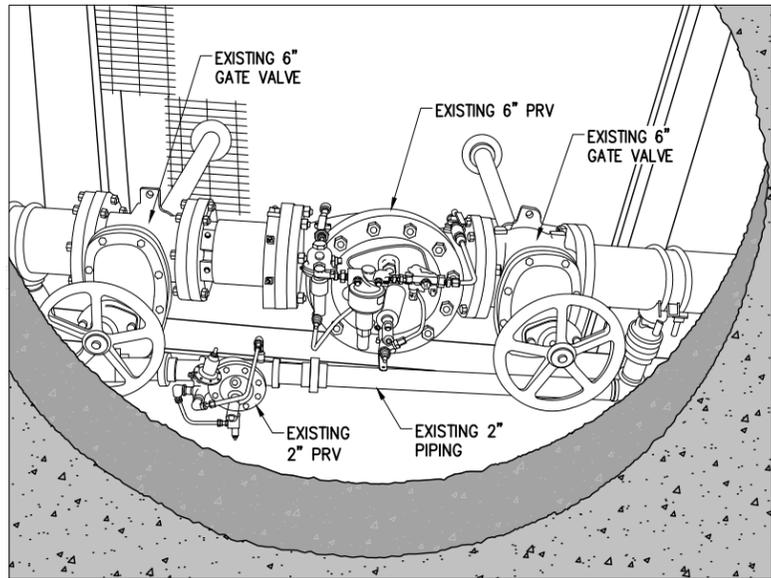
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811 Know what's below. Call 811 before you dig.	83, 12 6859(< 12 +25= '\$780 :63&6 1=21(1\$' '(6,'1(' %< ..)(' \$,' 352- 12 ,(/' %22. 6 9(57 '\$780 1\$9' '5\$: 1 %< ..	 KPG PSOMAS Tacoma -HIIHUVRQ SYHOXH ZDFRPD :\$ 6HDWVVOH :HCPUL6MH	BID DOCUMENT
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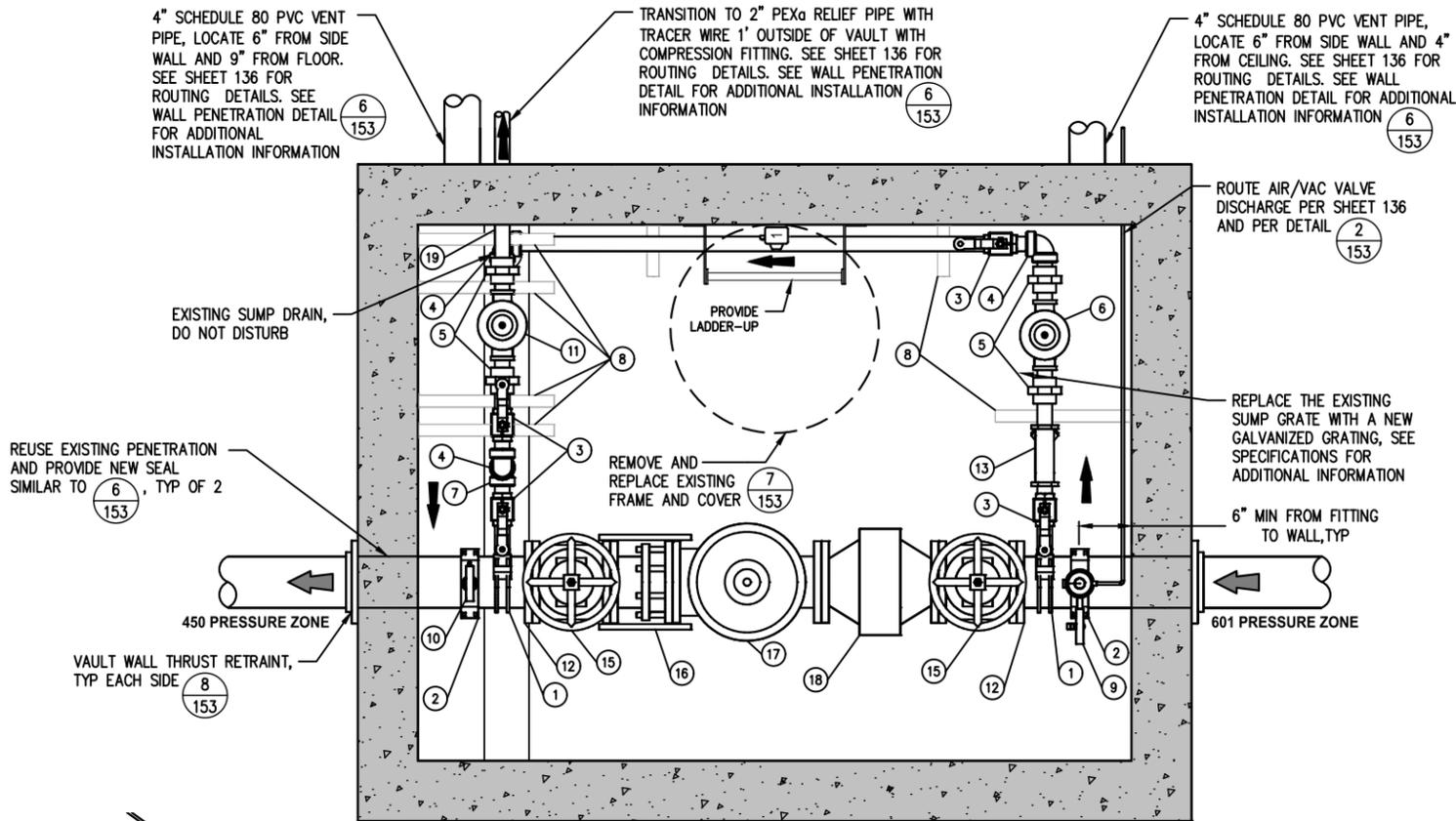
		CITY OF KIRKLAND DEPARTMENT OF PUBLIC WORKS 123 FIFTH AVENUE KIRKLAND, WA 98033 (425) 587-3800 www.kirklandwa.gov
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JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS		REFERENCE SHEET NO.
PRV STATION 57 REHABILITATION PLAN		SHEET 21
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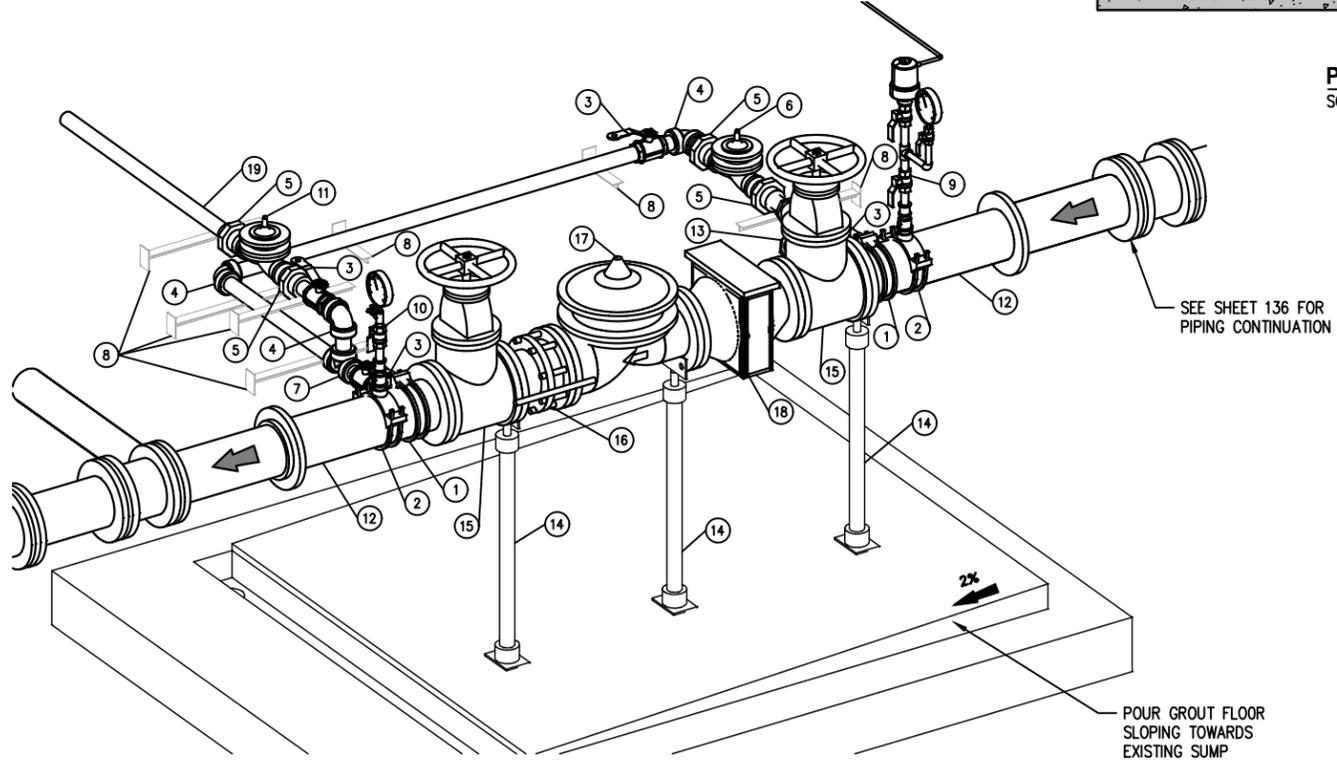
30 9211, (672&.0\$1 . 3352-(&76? . 5. / \$1' ? - 8\$1, 7\$ ' 5, 9 (. 17(56(&7, 21 6\$)(7<? (6, ' 1' 5\$: . 1' 6' & 2175\$ & 7? 18' (7 ' : : / DIRXW 1DPH 359 6'



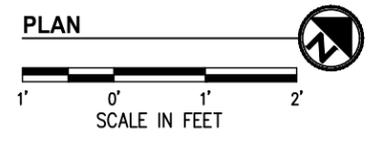
EXISTING PRV STATION 57 DETAIL
NTS



PRV STATION 57 PLAN DETAIL
SCALE: 1" = 1'



PRV STATION 57 OBLIQUE DETAIL
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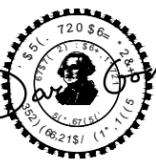
- NOTES**
- EXISTING PIPING, FITTINGS, AND APPURTENANCES TO BE REPLACED WITH NEW AS SHOWN UNLESS OTHERWISE NOTED. SEE THE MATERIAL LIST AND SPECIFICATIONS FOR ADDITIONAL INFORMATION.
 - PROVIDE 24 HOUR NOTICE TO THE DISTRICT SUCH THAT THE DISTRICT CAN SHUT OFF WATER SUPPLY AND REMOVE SALVAGABLE COMPONENTS.
 - PREPARE AND COAT/RECOAT ALL PIPING, VALVES, AND WALLS PER THE PROJECT SPECIFICATIONS.

MATERIAL LIST		
ITEM	QTY	DESCRIPTION
1	2	6" PIPE SADDLE WITH 2" IP OUTLET
2	2	6" PIPE SADDLE WITH 1" IP OUTLET
3	4	2" BRASS BALL VALVE W/ HAND LEVER
4	3	2" BRASS 90° BEND, FIP
5	4	2" BRASS UNION, FIP
6	1	EXISTING 2" PRESSURE REDUCING VALVE ASSEMBLY TO BE REUSED
7	1	2" BRASS TEE, FIP
8	MANY	PIPE SUPPORT (5/153)
9	1	AIR/VAC VALVE AND PRESSURE GAUGE (2/153)
10	1	HOSE BIBB AND PRESSURE GAUGE (3/153)
11	1	2" PRESSURE RELIEF VALVE
12	2	6" DI PIPE, FLxPE (6/153)
13	1	2" BRASS STRAINER
14	3	ADJUSTABLE PIPING SUPPORT, STANDON #S89
15	2	6" DI GATE VALVE, FLxFL
16	1	6" DISMANTLING JOINT, FLxFL
17	1	EXISTING 6" PRESSURE REDUCING VALVE, FLxFL TO BE REUSED
18	1	H6" STRAINER FLxFL
19	1	2" BRASS NIPPLE INSIDE VAULT, TRANSITION TO 2" POLYETHYLENE PIPE 1' OUTSIDE VAULT WITH FIPxPACK FITTING

PROVIDE 2" BRASS NIPPLES, MIP, LENGTH TO FIT, AS NECESSARY TO HAVE A COMPLETE AND FUNCTIONAL SYSTEM. INSTALL PIPE AND SUPPORTS AS NECESSARY TO PROVIDE A RIGID, SECURE, NON-SAGGING SYSTEM.



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-HIIHUVRQ SYHQXH
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BID DOCUMENT

NORTHSHORE UTILITY DISTRICT
CITY OF KIRKLAND
WASHINGTON

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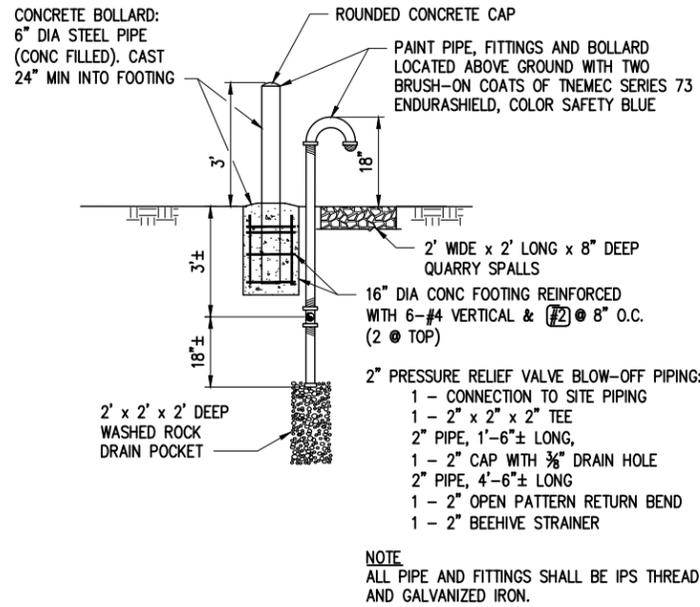
JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

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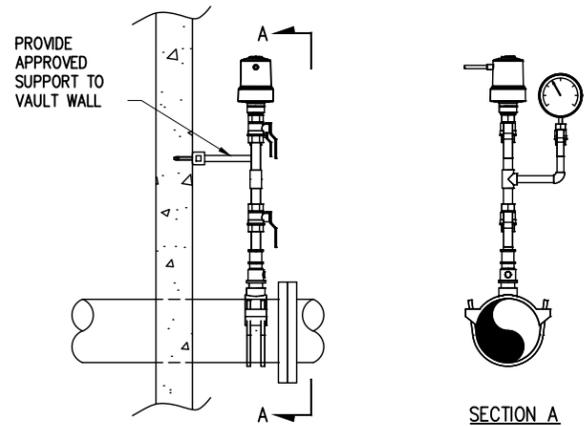
PRV STATION 57 REHABILITATION DETAILS

REFERENCE SHEET NO.
SHEET 21
SHEETS

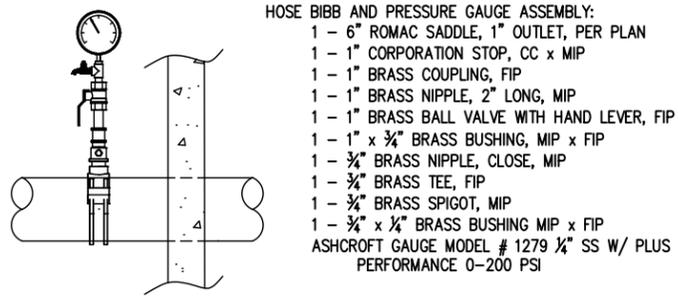
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PRESSURE RELIEF VALVE DISCHARGE DETAIL 1
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1" AIR/VAC VALVE & PRESSURE GAUGE DETAIL 2
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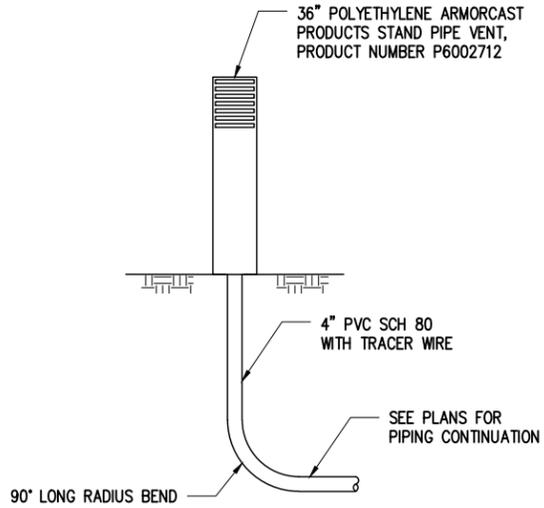


HOSE BIBB & PRESSURE GAUGE DETAIL 3
NTS

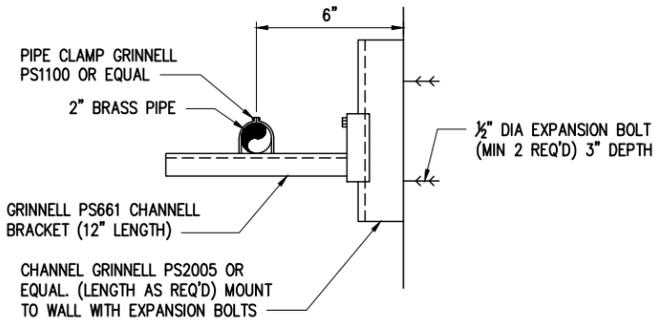
NOTE
ROUTE AIR/VAC VALVE TO VAULT EXTERIOR
PER PLAN. PROVIDE 1" COPPER PIPE INSIDE
VAULT AND TRANSITION TO 2" PEX PIPE 1'
OUTSIDE VAULT WITH COMPRESSION FITTING.
PROVIDE BRASS PIPE AND FITTINGS AS
NECESSARY TO HAVE A COMPLETE AND
FUNCTIONAL SYSTEM.

- AIR/VAC VALVE AND PRESSURE GAUGE ASSEMBLY:
1 - 6" ROMAC SADDLE, 1" OUTLET, PER PLAN
1 - 1" CORPORATION STOP, CC x MIP
1 - 1" BRASS COUPLING, FIP
1 - 1" BRASS NIPPLE, 2" LONG, MIP
1 - 1" BRASS BALL VALVE WITH HAND LEVER, FIP
1 - 1" BRASS NIPPLE, CLOSE, MIP
1 - 1" BRASS TEE, FIP
1 - 1" BRASS NIPPLE, 3" LONG, MIP
1 - 1" BRASS BALL VALVE WITH HAND LEVER, FIP
1 - 1" AIR/VAC VALVE, ARI D-040-C OR APPROVED EQUAL
- 1 - 1" x 3/4" BRASS BUSHING, MIP x FIP
1 - 3/4" BRASS NIPPLE, 4" LONG, MIP
1 - 3/4" BRASS 90° ELBOW, FIP
1 - 3/4" BRASS NIPPLE, 3" LONG, MIP
1 - 3/4" BRASS BALL VALVE WITH HAND LEVER, FIP
1 - 3/4" x 1/4" BRASS BUSHING, MIP x FIP
ASHCROFT GAUGE MODEL #1279 1/4" SS W/ PLUS
PERFORMANCE, 0-200 PSI, W/MAXIMUM POINTER

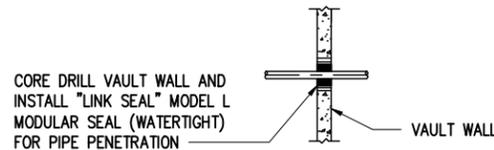
NOTE
VALVES IN VERTICAL ORIENTATION SHALL HAVE
HANDLES POINT DOWN.



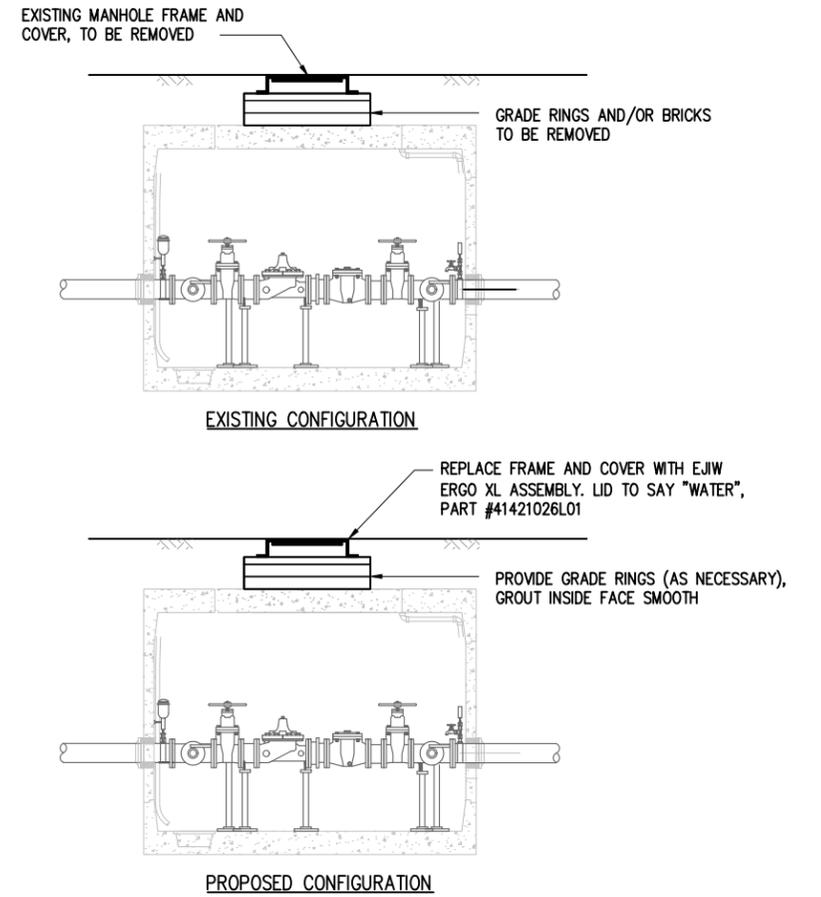
STAND PIPE VENT DETAIL 4
NTS



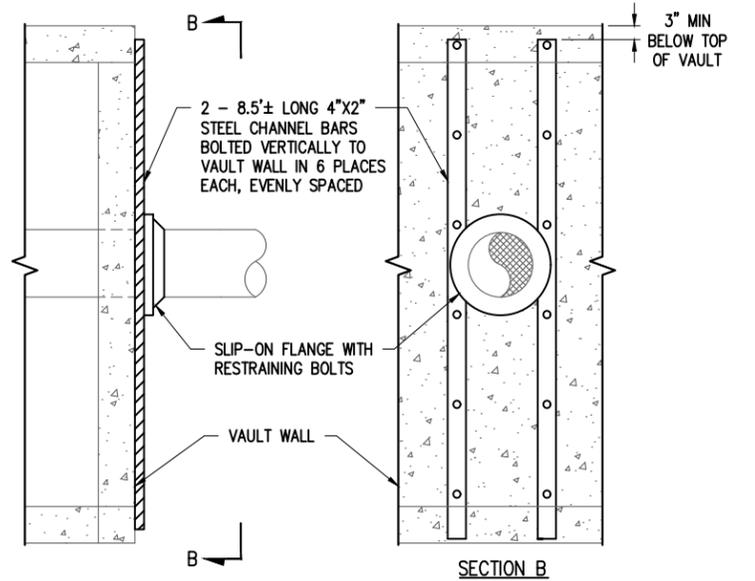
PIPE SUPPORT DETAIL 5
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WALL PENETRATION DETAIL 6
NTS



VAULT LID DETAIL 7
NTS



VAULT WALL THRUST RESTRAINT DETAIL 8
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KPG PSOMAS
BID DOCUMENT
Tacoma
-HIIHUVRQ SYHQXH
ZDFRPD :\$
6HDWQWH :HCPG6MHH



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**JUANITA DRIVE MULTIMODAL,
INTERSECTION & SAFETY IMPROVEMENTS**
35(6685(=21(&219(56,21
MISCELLANEOUS PRV DETAILS

REFERENCE SHEET NO.
SHEET 21
SHEETS

NUD WATER TABLE

NO.	PLAN NO.	EX METER, (IN)	SERVICE ADDRESS	SVC TYPE	NUD ACCT NO.	METER NO.	METER INSTALL DATE	OWNER NAME	OWNER CONTACT INFO	MTR LOC VERIF.	LOCATION NOTES
1	135	0.75	12020 Juanita Dr NE Kirkland WA 98034-3431	Poly	147702	43580306	10/20/2011	JS Electric % Sailors	23002 Barker Rd Bothell 98021-9268	X	IN LINE WITH FRONT DOOR IN ASPHALT
2	137	0.75	12316 Juanita Dr NE Kirkland WA 98034-2512	Poly	266968	43451608	9/1/2011	Puneet Konghot & Tulasi Atluri	4011 Carracci Lane San Jose 95135-1170	X	3 METERS LEFT OF DRIVE
3	137	0.75	12320 Juanita Dr NE Kirkland WA 98034-2512	Poly	266976	43451609	9/1/2011	Mark & Francine Kruschwitz	12320 Juanita Dr NE Kirkland 98034-2512	X	3 METERS LEFT OF DRIVE
4	137	0.75	12324 Juanita Dr NE Kirkland WA 98034-2512	Poly	266984	43451601	9/1/2011	Mathiyalagan / Balasubramanian	10001 NE 1st St #W114 Bellevue 98004-5675	X	3 METERS LEFT OF DRIVE
5	139	0.75	12450 Juanita Dr NE Kirkland WA 98034-2567	Poly	299744	43451600	9/1/2011	Cheri Vanderwel	20415 150th Ave SE Monroe 98272-9178	X	LEFT OF DR
6	139	0.75	12454 Juanita Dr NE Kirkland WA 98034-2567	Poly	146563	43451602	9/1/2011	Ngo/ Do	11322 NE 128th St Kirkland 98034-6331	X	TOP OF BANK BEHIND MAILBOXES
7	139	0.75	12458 Juanita Dr NE Kirkland WA 98034-2567	Poly	298884	43451603	9/1/2011	Rorke and Nadine Haining	12458 Juanita Dr NE Kirkland 98034-2567	X	LEFT OF D/WY NEAR POWER POLE
8	139	0.75	12602 Juanita Dr NE Kirkland WA 98034-2563	Poly	260816	43451599	9/1/2011	Nancy Radke	12603 80th Ave NE Kirkland 98034-2509	X	TOP OF BANK IN BANK OF MAILBOXES
9	139	0.75	12610 Juanita Dr NE Kirkland WA 98034-2563	Poly	260296	43451569	9/1/2011	Ionel & Mihaela Patilea	12610 Juanita Dr NE Kirkland 98034-2563	X	TOP OF BANK IN BANK OF MAILBOXES
10	140	0.75	12619 Juanita Dr NE Kirkland WA 98034-2564	Poly	253456	43452548	8/29/2011	Matt & Patricia Davis II	12619 Juanita Dr NE Kirkland 98034-2564	X	RT SIDE OF PROP
11	140	0.75	12629 Juanita Dr NE Kirkland WA 98034-2564	Poly	251769	43452538	8/29/2011	Mary Linda Fobes	12629 Juanita Dr NE Kirkland 98034-2564	X	5' RIGHT POWER POLE
12	140	1	12634 Juanita Dr NE Kirkland WA 98034-2563	Poly	146530	43523267	9/12/2011	D Lees	12634 Juanita Dr NE Kirkland 98034-2563	X	RT OF POWER POLE
13	140	0.75	12707 Juanita Dr NE Kirkland WA 98034-2562	Poly	249862	43452518	8/29/2011	Lucas & Ginelle Moriarty	12707 Juanita Dr NE Kirkland 98034-2562	X	2 METERS CTR OF DRIVES
14	140	0.75	12708 Juanita Dr NE Kirkland WA 98034-2561	Poly	253848	43451638	9/5/2011	Sarabjit Mann & Parminder Kaur Mann	12708 Juanita Dr NE Kirkland 98034-2561	X	RT PROP, TOP OF BANK
15	140	0.75	12713 Juanita Dr NE Kirkland WA 98034-2562	Poly	249854	43452579	8/29/2011	Cathy Thompson	1933 S 375th St Federal Way 98003-7567	X	2 METERS CTR OF DRIVES
16	140	0.75	12723 Juanita Dr NE Kirkland WA 98034-2562	Poly	252632	43452614	8/29/2011	Etienna Bachand	12723 Juanita Dr NE Kirkland 98034-2562	X	2 MTRS 20' RT OF DR BH FENCE
17	140	0.75	12727 Juanita Dr NE Kirkland WA 98034-2562	Poly	225748	43452616	8/29/2011	Etienna Bachand	12723 Juanita Dr NE Kirkland 98034-2562	X	2 MTRS 20' RT OF 12723 DR BH FENCE
18	141	0.75	12801 Juanita Dr NE Kirkland WA 98034-2519	Poly	247353	43452617	8/29/2011	Sylvia Volchok	2920 76th Ave SE Unit 401 Mercer Island 98040-2749	X	RT OF DRIVE 10'
19	141	0.75	12809 Juanita Dr NE Kirkland WA 98034-2519	Poly	206011	43452620	8/29/2011	Brent Smith	12819 Juanita Dr NE Kirkland 98034-2519	X	APPROX 18' S. OF POLE & 5' WEST
20	142	0.75	12819 Juanita Dr NE Kirkland WA 98034-2519	Poly	268216	43452621	8/29/2011	Brent Smith	12819 Juanita Dr NE Kirkland 98034-2519	X	RT PROP LINE
21	142	0.75	12831 Juanita Dr NE Kirkland WA 98034-2519	Poly	146522	43452618	8/29/2011	Sarah Sears	127 Cedars Dr Troy 59935-9829	X	CENTER OF PROP 15' LEFT MAILBOX & 7' BACK
22	142	?	12918 Juanita Dr NE Kirkland WA 98034-2519	Poly	326624	?	4/26/2019	Xue/Li	12933 79th Pl NE Kirkland 98034	X	ONLY SVC, NO MTR, LEFT CTR PROP
23	142	0.75	12920 Juanita Dr NE Kirkland WA 98034-2558	Poly	146514	43451637	9/5/2011	Jim Burrows	12920 Juanita Dr NE Kirkland 98034-2558	X	12' LEFT OF DRIVE
24	143	0.75	12921 Juanita Dr NE Kirkland WA 98034-2559	Poly	146506	43452619	8/29/2011	Josh & Reynolds Berglund	12921 Juanita Dr NE Kirkland 98034-2559	X	RT PROP, IN HEDGE
25	143	0.75	12934 Juanita Dr NE Kirkland WA 98034-2558	Poly	146498	43451636	9/5/2011	Korra Contracting, LLC	12934 Juanita Dr NE Kirkland 98034-2558	X	4' LEFT OF DRIVE
26	143	0.75	13010 Juanita Dr NE Kirkland WA 98034-2556	Poly	295992	43452636	9/6/2011	Hamed Esfahani & Zainab Daghir-Alnoor	13010 Juanita Dr NE Kirkland 98034-2556	X	10' RIGHT OF DR.
27	143	0.75	13028 Juanita Dr NE Kirkland WA 98034-2556	Poly	146480	43452637	9/6/2011	Ajay & Anjali Goplani	13028 Juanita Dr NE Kirkland 98034-2556	X	LEFT CENTER OF PROP
28	143	0.75	13040 Juanita Dr NE Kirkland WA 98034-2556	Poly	146472	43452638	9/6/2011	Michael Ferguson	13040 Juanita Dr NE Kirkland 98034-2556	X	IN DR LT SIDE OF PROP
29	144	0.75	13044 Juanita Dr NE Kirkland WA 98034-2556	Poly	146464	43452639	9/6/2011	Marlene McAninch	13044 Juanita Dr NE Kirkland 98034-2556	X	RT CENTER OF PROP
30	144	0.75	13050 Juanita Dr NE Kirkland WA 98034-2556	Poly	307312	43451628	9/6/2011	Som Agarwal	13050 Juanita Dr NE Kirkland 98034-2556	X	20' LEFT DR IN FRONT OF FENCE

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BID DOCUMENT

Tacoma - HHHVVRQ SYHOXH
ZDFRPD :\$
6HDVWOH :HCP66HH




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**JUANITA DRIVE MULTIMODAL,
INTERSECTION & SAFETY IMPROVEMENTS**

NUD WATER TABLE

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GENERAL NOTES

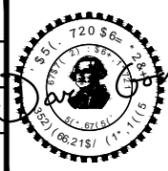
1. REMOVE ALL SSMH ADJUSTMENT MATERIAL (BRICK, CONC., ETC.) DOWN TO THE TOP OF THE PRECAST STRUCTURE AND INSTALL NEW CONCRETE ADJUSTMENT RINGS (PER NUD SEWER STD DETAIL 1) AND NEW FRAME & COVER (PER NUD SEWER STD DETAIL 5).
2. FOR SSMH WHERE SECTIONS ARE ALSO ADDED/REMOVED, REMOVE CONE AND INSTALL NEW SECTION AND NEW CONE PER NUD SEWER STD DETAIL 1 AND SEWER DETAIL 1, SHEET 155.

NUD SANITARY SEWER MANHOLE ADJUSTMENT TABLE

PLAN ID	MANHOLE ID	SURVEY NO.	SHEET	STATION	EX CHIMNEY HEIGHT (IN)	EG (FT)	FG (FT)	CHNG (IN)	PROP CHIMNEY HEIGHT (IN)	ADD MH SEC'N 1'	ADJ ONLY	FINAL CHIMNEY HEIGHT (IN)	FINAL FINISHED SURFACE	NOTES
1	1648	9076	133	500+19.6, 3.3' RT	18.0	199.38	199.38	0.00	18.0		X	18.0	ASPHALT	
2	1647	9081	133	501+16.9, 3.6' RT	17.0	207.10	207.10	0.00	17.0		X	17.0	ASPHALT	
3	1646	9083	133	501+58.0, 3.0' RT	25.0	210.30	210.30	0.00	25.0		X	25.0	ASPHALT	
4	6374	15258	134	15+92.4, 17.6' RT	12.5	259.02	259.02	0.00	12.5	-	-	24.5	ASPHALT	REMOVE 1' BARREL SECTION, REPLACE CONE, AND ADJUST TO FINISHED GRADE
5	6367	13459	137	25+72.7, 11.0' LT	24.0	303.49	303.49	0.00	24.0		X	24.0	ASPHALT	
6	5629	12447	138	29+79.7, 22.2' RT	6.0	320.08	321.22	13.68	19.7		X	19.7	CEMENT CONCRETE	
7	5622	2507	141	43+65.3, 17.4' RT	8.0	336.80	336.69	-1.32	6.7	X		18.7	ASPHALT	REMOVE 2' BARREL SECTION, REPLACE CONE, AND ADJUST TO FINISHED GRADE
8	5621	2431	141	45+84.4, 20.4' RT	10.0	336.69	337.87	14.16	24.2		X	24.2	ASPHALT	
9	5620	2107	142	49+30.9, 22.2' RT	14.0	333.73	335.06	15.96	30.0	X		18.0	CEMENT CONCRETE	REPLACE CONE AND ADJUST TO FINISHED GRADE
10	5619	1946	143	51+67.7, 23.0' RT	14.0	333.19	334.43	14.88	28.9	X		16.9	ASPHALT	REPLACE TOP SLAB AND ADJUST TO FINISHED GRADE. RE-CHANNEL STRUCTURE



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KPG PSOMAS

Tacoma
-HIIHUVRQ SYHOXH
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6HDWWOH :HCPG6MH

BID DOCUMENT



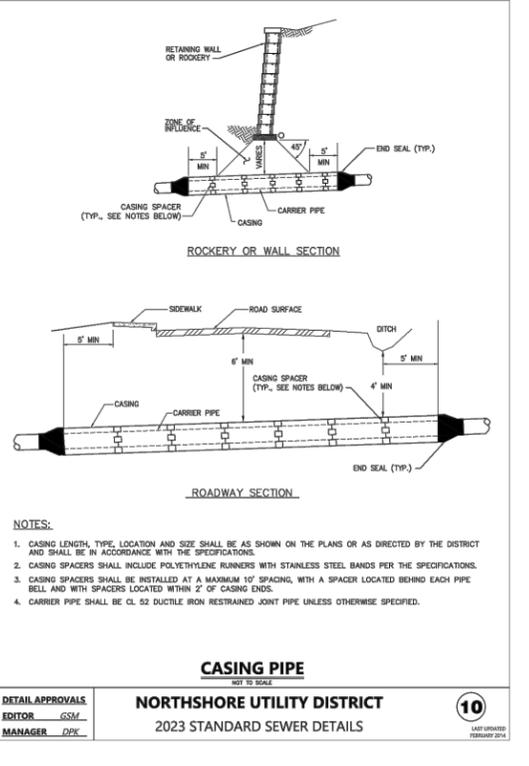
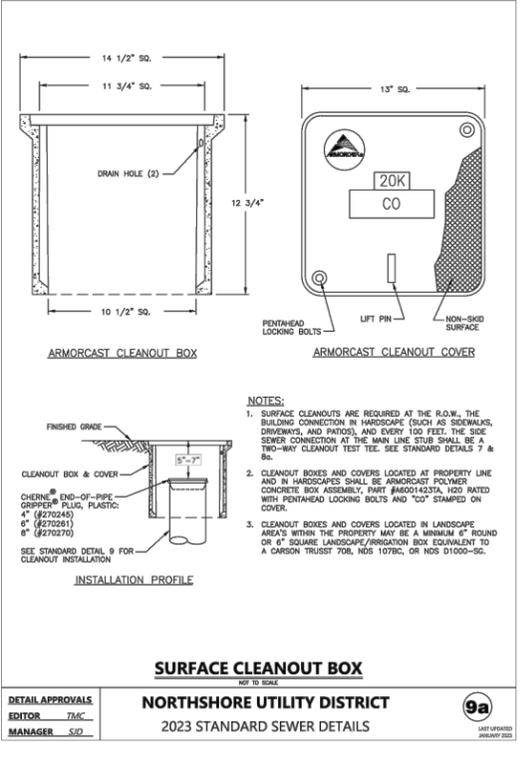
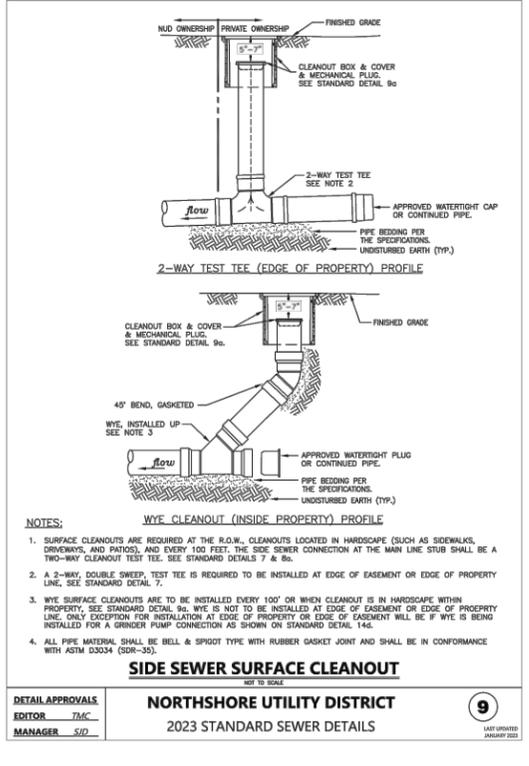
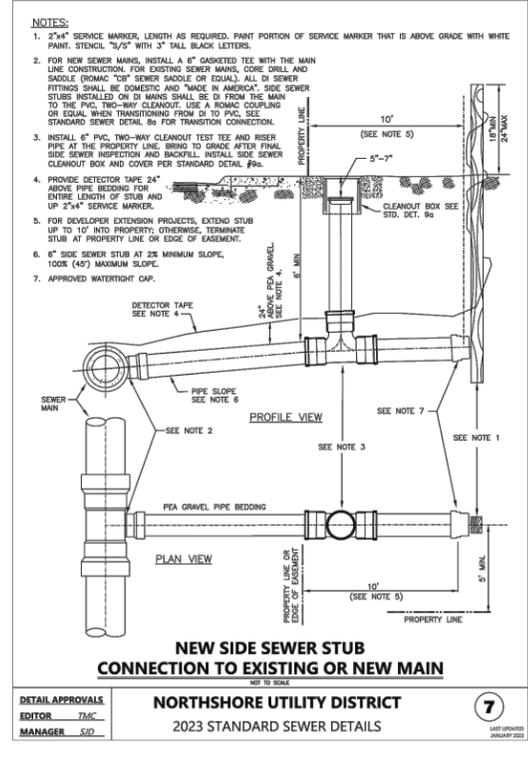
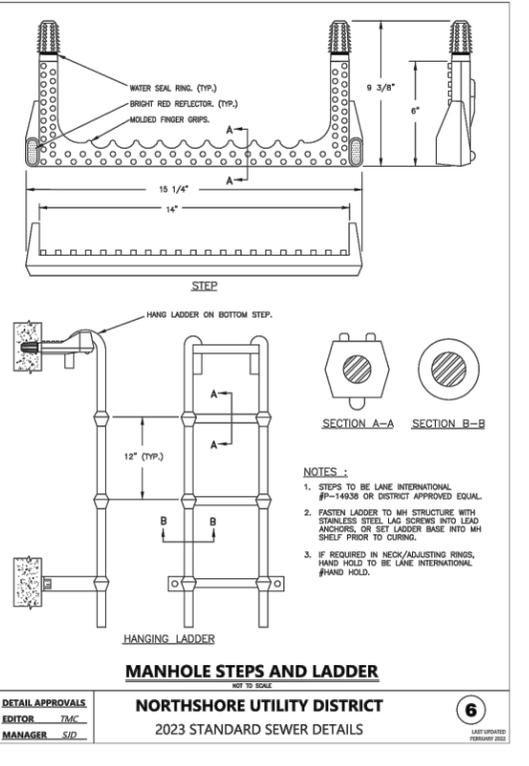
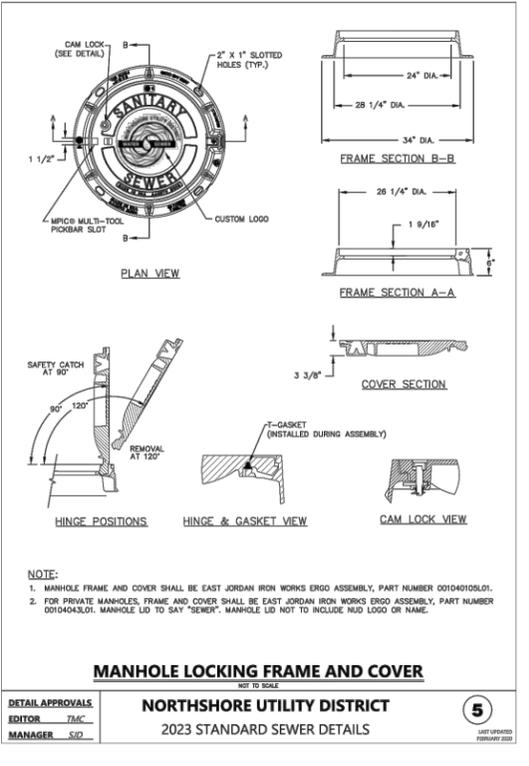
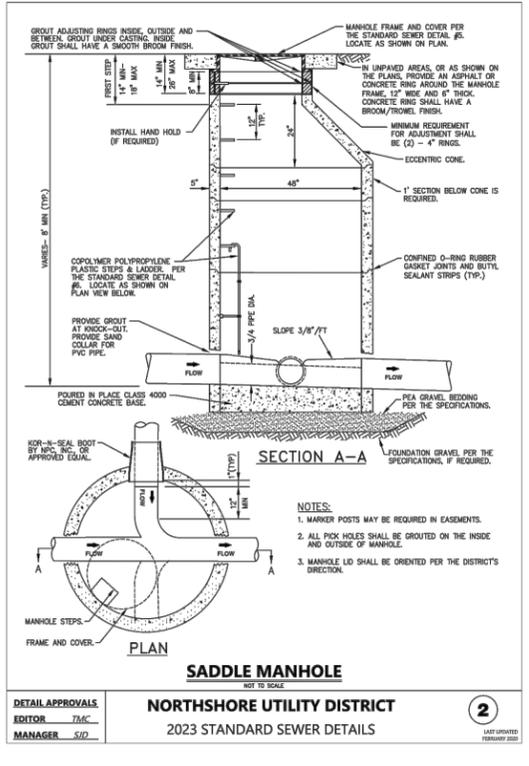
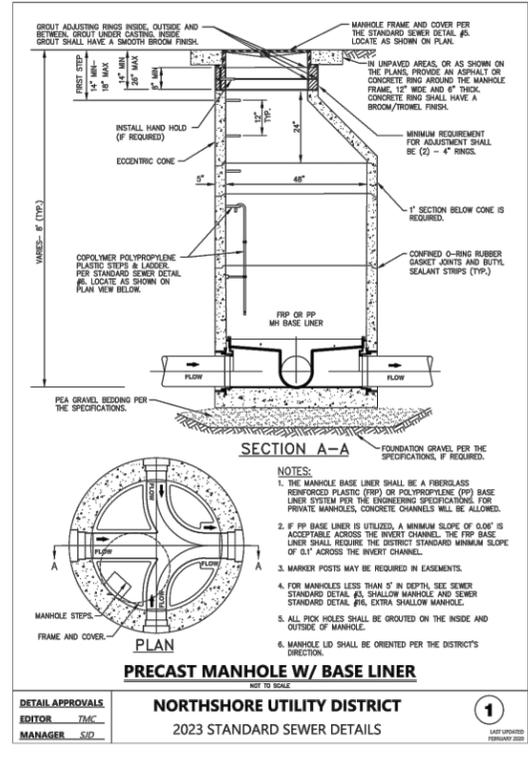
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**JUANITA DRIVE MULTIMODAL,
INTERSECTION & SAFETY IMPROVEMENTS**

NUD SEWER TABLE

REFERENCE SHEET NO.
SHEET 2) SHEETS

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:63 & 6 1 = 2 (1 \$'	1 \$9'
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18'	18'



KPG PSOMAS

BID DOCUMENT

Tacoma - HIIHUVRO SYHQX
7DFRPD : \$
6HDWQWH : HCPK65MH

NORTHSHORE UTILITY DISTRICT

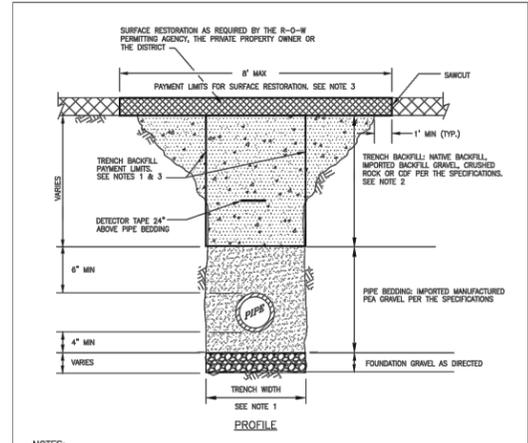
CITY OF KIRKLAND WASHINGTON

CITY OF KIRKLAND
DEPARTMENT OF PUBLIC WORKS
123 FIFTH AVENUE KIRKLAND, WA 98033
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JUANITA DRIVE MULTIMODAL, INTERSECTION & SAFETY IMPROVEMENTS

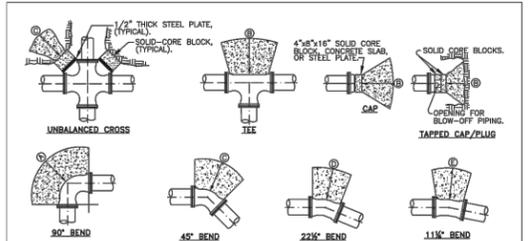
STANDARD DETAILS

REFERENCE SHEET NO. SHEET 2) SHEETS



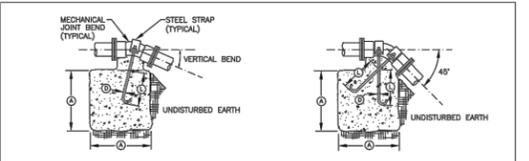
NOTES: 1. TRENCH WIDTH: MINIMUM: PIPE O.D. + 24" (12" EACH SIDE OF PIPE). MAXIMUM: 48" FOR 18" PIPE AND SMALLER. (1 1/2" X L.D.) + 18" FOR 18" PIPE AND LARGER. THE NEAT-LINE PAYMENT LIMITS FOR TRENCH BACKFILL MATERIALS SHALL BE BASED UPON THE MAXIMUM ALLOWABLE TRENCH WIDTH AS SHOWN ABOVE.

DETAIL APPROVALS: EDITOR: TMC, MANAGER: SID. NORTHSHORE UTILITY DISTRICT, 2023 STANDARD SEWER DETAILS, 11



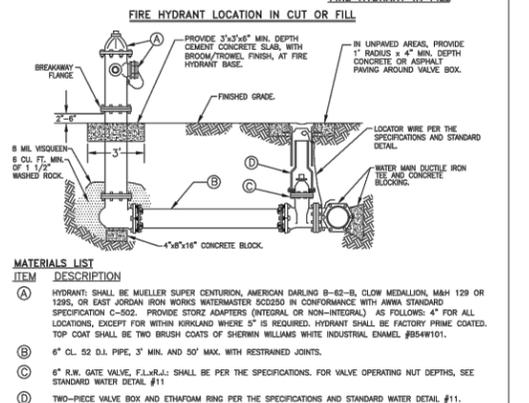
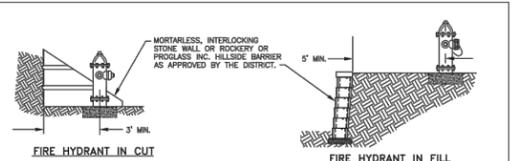
THRUST BLOCK SIZING FOR 250 PSI PRESSURE. Table with columns for Pipe Size, Thrust, and Thrust Block Dimensions. Includes a bearing capacity table for various soil types.

DETAIL APPROVALS: EDITOR: TMC, MANAGER: DPX. NORTHSHORE UTILITY DISTRICT, 2023 STANDARD WATER DETAILS, 1

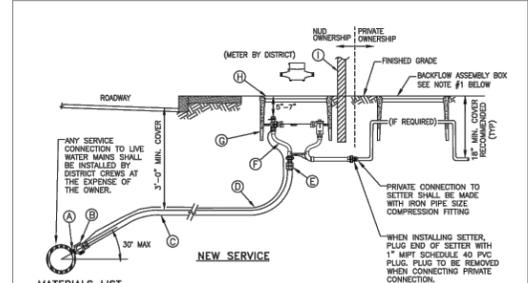


VERTICAL BLOCKING table with columns for Pipe Size, Vertical Bend, and Blocking Dimensions. Includes a typical cross-section diagram and notes on blocking area and materials.

DETAIL APPROVALS: EDITOR: TMC, MANAGER: SID. NORTHSHORE UTILITY DISTRICT, 2023 STANDARD WATER DETAILS, 2

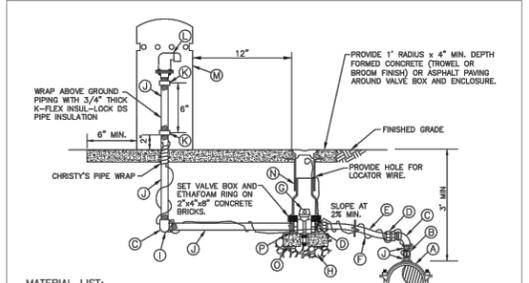


DETAIL APPROVALS: EDITOR: TMC, MANAGER: SID. NORTHSHORE UTILITY DISTRICT, 2023 STANDARD WATER DETAILS, 3



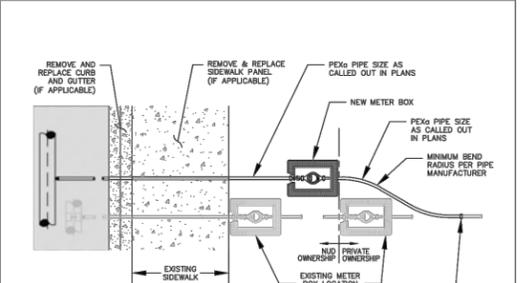
- MATERIALS LIST: A DIRECT TAP: DIRECT TAP OF DUCTILE IRON OR CAST IRON LARGER THAN 4" DIAMETER IS REQUIRED FOR 1" SERVICES. SADDLES WILL NOT BE ALLOWED ON DUCTILE IRON OR CAST IRON PIPE LARGER THAN 4" DIAMETER.

DETAIL APPROVALS: EDITOR: TMC, MANAGER: SID. NORTHSHORE UTILITY DISTRICT, 2023 STANDARD WATER DETAILS, 4



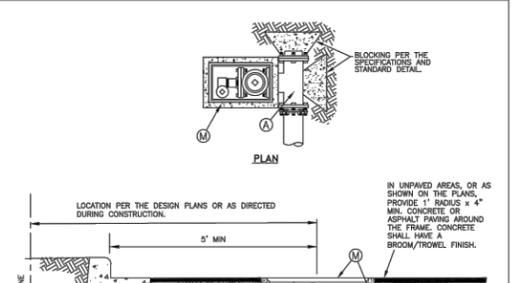
- MATERIALS LIST: A SADDLE: ALL SADDLES SHALL BE DUCTILE IRON CASTING WITH IP THREADS. FOR ALL PIPE MATERIAL OTHER THAN PVC PIPE, SADDLES SHALL BE SINGLE STRAP TYPE EQUAL TO FORD FC101, ROMAC 101NS, MUELLER DR15, OR DISTRICT APPROVED EQUAL.

DETAIL APPROVALS: EDITOR: TMC, MANAGER: SID. NORTHSHORE UTILITY DISTRICT, 2023 STANDARD WATER DETAILS, 8



- NOTES: 1. WATER SERVICE PER NUD STANDARD WATER DETAILS, #4 OR #5. 2. NEW METER TO BE RELOCATED TO LOCATION SHOWN ON PLANS.

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- MATERIALS LIST: A WATER MAIN TEE, R.I.J. PL. B 4"x8"x16" CONCRETE BLOCK.

DETAIL APPROVALS: EDITOR: TMC, MANAGER: SID. NORTHSHORE UTILITY DISTRICT, 2023 STANDARD WATER DETAILS, 10



83, 12) (' \$ ' 352- 12 6859(< 12), (/ ' % 22. 6 +25 = '\$780 9(57 '\$780 :63&6 1 = 21(1\$' 1\$9' ' (6, *1(' % < '5\$1' % < 18' 18'



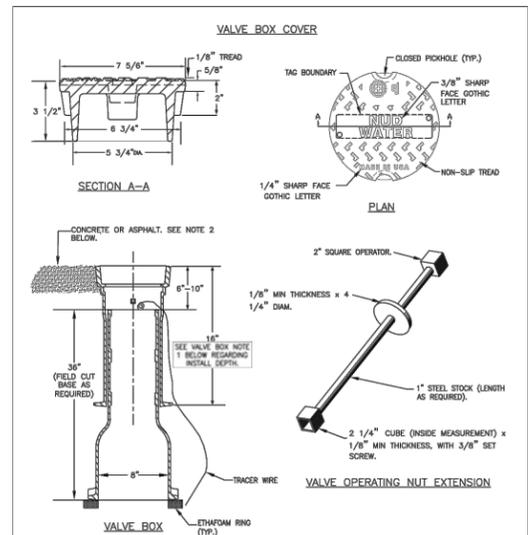
KPG PSOMAS Bid DOCUMENT Tacoma -HIIHVURQ SYHQX 7DFRPD :\$ 6HDWQOH :HCPKPEHMH



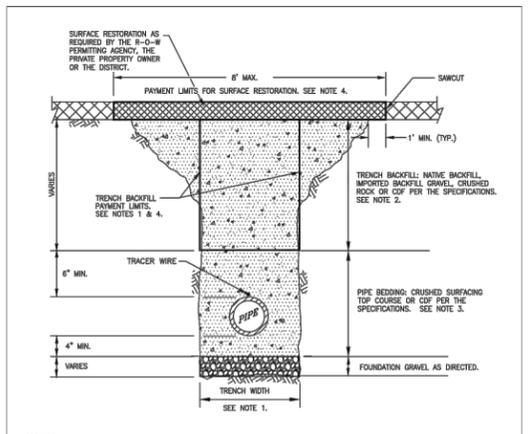
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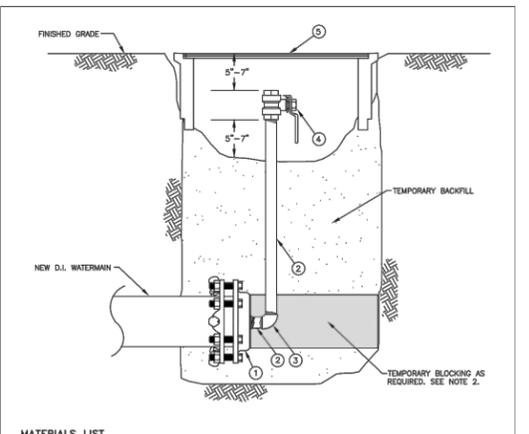
2FWREHU 30 9211.(672&.0\$1 . 9352-(876?..5.51' ? -8\$1.7\$ '5.9(.17(56(&7.21 6\$)(7<'6.1'7'5\$:.1'6?&2175\$&7? 18' '(7 B67' .: /DIRXW 1DPH 18



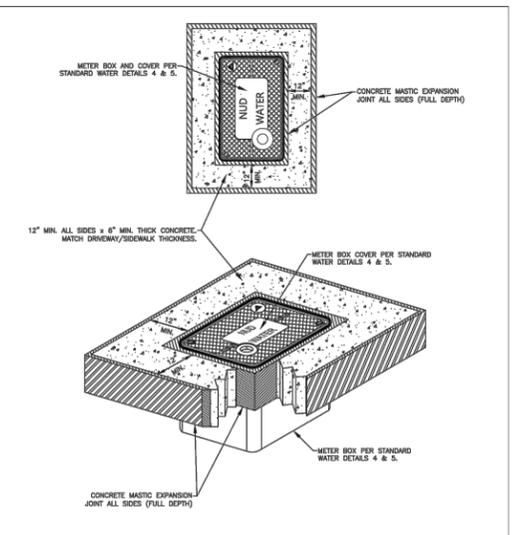
VALVE BOX & OPERATING NUT EXTENSION
 NOT TO SCALE
 DETAIL APPROVALS: NORTHSHORE UTILITY DISTRICT
 EDITOR: T/M/C
 MANAGER: S/D
 2023 STANDARD WATER DETAILS
 11
 LAST UPDATED: FEBRUARY 2023



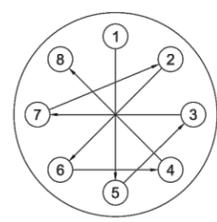
TYPICAL TRENCH SECTION & PAYMENT LIMITS
 NOT TO SCALE
 DETAIL APPROVALS: NORTHSHORE UTILITY DISTRICT
 EDITOR: T/M/C
 MANAGER: S/D
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 LAST UPDATED: JANUARY 2023



TEMPORARY BLOW-OFF ASSEMBLY FOR NEW WATERMAIN
 NOT TO SCALE
 DETAIL APPROVALS: NORTHSHORE UTILITY DISTRICT
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 MANAGER: S/D
 2023 STANDARD WATER DETAILS
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 LAST UPDATED: FEBRUARY 2023



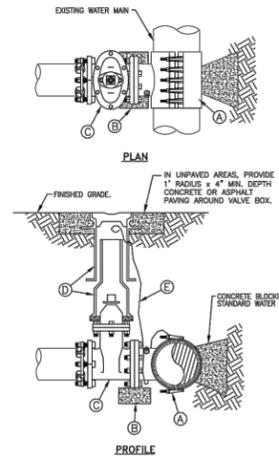
METER BOX BLOCK OUT FOR DRIVEWAYS AND SIDEWALKS
 NOT TO SCALE
 DETAIL APPROVALS: NORTHSHORE UTILITY DISTRICT
 EDITOR: T/M/C
 MANAGER: S/D
 2023 STANDARD WATER DETAILS
 18
 LAST UPDATED: APRIL 2023



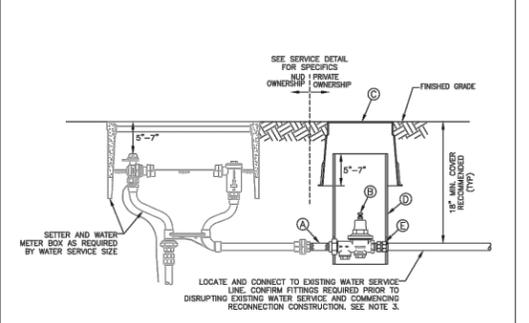
8 BOLT EXAMPLE

NO OF BOLTS	NOMINAL FLG SIZE	BOLT NUT TIGHTENING SEQUENCE
8	4", 6", 8"	1, 5, 3, 7, 2, 6, 4, 8
12	10", 12"	1, 7, 4, 10, 2, 8, 5, 11, 3, 9, 6, 12

BOLT TIGHTENING SEQUENCE
 NOT TO SCALE
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TAPPING TEE AND GATE VALVE
 NOT TO SCALE
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 EDITOR: T/M/C
 MANAGER: S/D
 2023 STANDARD WATER DETAILS
 20
 LAST UPDATED: JANUARY 2023



PRESSURE REDUCING VALVE (PRV)
 NOT TO SCALE
 DETAIL APPROVALS: NORTHSHORE UTILITY DISTRICT
 EDITOR: T/M/C
 MANAGER: S/D
 2023 STANDARD WATER DETAILS
 21
 LAST UPDATED: FEBRUARY 2023

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:63&6 1 =21(1\$'	1\$9'
'(6, *1(' % <	'5\$: 1 % <
18'	18'

KPG PSOMAS
 BID DOCUMENT
 Tacoma
 -HIHUVRQ SYHQX
 7DFRPD :\$
 6HDVQWH :HCP65MH

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