



CITY OF KIRKLAND
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MEMORANDUM

To: Planning Commission

From: Dorian Collins, AICP, Senior Planner
Adam Weinstein, AICP, Planning and Building Director

Date: December 17, 2018

Subject: Amendments to the Design Guidelines for the Totem Lake Business District and the Design Guidelines for Pedestrian-Oriented Business Districts, File CAM18-00196

Recommendation

Receive briefing and discuss potential amendments to the following documents, adopted in accordance with Section 3.30 of the Kirkland Municipal Code:

- Design Guidelines for the Totem Lake Business District
- Design Guidelines for Pedestrian-Oriented Business Districts

Provide direction to staff regarding changes to both documents for consideration by City Council. The City Council is expected to consider and take action on the revised guidelines in February 2019.

Background

As part of the 2018-2020 Planning Work Program, the City Council directed staff and the Planning Commission to review and update the design guidelines for the Totem Lake Business District. Design guidelines are used by the City in the design review process to ensure that new development enhances the aesthetic character of the surrounding area and meets primary urban design and functional objectives (e.g., ensuring that buildings are interesting and attractive at the pedestrian level). The design guidelines for the Totem Lake Business District have not been updated since 2004, when they were developed to accompany an update of the plan for Totem Lake. Many changes to regulations in Totem Lake have occurred since that time, and the plan for the Totem Lake Business District and Urban Center had another significant update in December of 2015. As a result, many graphics and references contained in both the [Design Guidelines for Pedestrian Oriented Business Districts](#), which provides direction for the Totem Lake Business District Core subarea, and the [Design Guidelines for the Totem Lake Neighborhood](#), which guides the remainder of the business district, are out of date.

Project Summary

The scope of work for changes to the design guidelines includes:

- Updates to graphics and outdated text references
- Improvements to clarity and effectiveness of guidelines
- New guidelines to incorporate recommendations from the Totem Lake Enhancement and Multimodal Transportation Network Plan

Draft changes to the guidelines are shown in Attachments 1 and 2 to this memorandum. New text is shown as underlined, and deleted text is shown as ~~strikethrough~~. Notes are included to indicate where revised or new graphics are proposed.

Attachments

1. Draft Amendments to Design Guidelines for the Totem Lake Business District
2. Draft Amendments to the Design Guidelines for Pedestrian-Oriented Business Districts

cc: CAM18-00196

The City of Kirkland



Design Guidelines

Business District

For Totem Lake Neighborhood



Adopted by the City Council
pursuant to Kirkland Municipal Code
Section 3.30.040, Ordinance **4052**.



Jim Lauinger,
Mayor



Eric Shields,
Director,
Planning & Community
Development

Design Guidelines for Totem Lake Business DistrictNeighborhood

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Introduction

This document sets forth a series of Design Guidelines, adopted by Section 3.30.04X of the Kirkland Municipal Code that will be used by the City in the Design Board Review (DBR) process for development in the Totem Lake Business District Neighborhood. The Totem Lake Business District Neighborhood encompasses the Business District Core Totem Center and the adjacent land within the district's neighborhood boundaries. ~~At this time,~~ Design Guidelines governing development in the Business District Core Totem Center are contained in the document titled, Design Guidelines for Pedestrian-Oriented Business Districts, Section 3.30.040 of the Municipal Code.

Other documents that should be referred to during design review are the Totem Lake Business District Neighborhood Plan goals and policies contained in the Comprehensive Plan, and the TL Use Zone Charts found in the Kirkland Zoning Code and the Totem Lake Urban Center Enhancement and Multimodal Transportation Network Plan, approved by the Kirkland City Council on May 15, 2018.

Purpose of the Design Guidelines

~~This document sets forth a series of Design Guidelines, adopted by Section 3.30 of the Kirkland Municipal Code, that will be used by the City in the Design Board Review (DBR) process. For projects required to be reviewed by the Design Review Board, the Board will use these guidelines in association with the Design Regulations of the Kirkland Zoning Code. To the extent that the standards of the Design Guidelines or Design Regulations address the same issue but are not generally consistent or contain different levels of specificity, the Design Review Board will determine which standard results in superior design.~~ For Administrative Design Review (ADR), the Planning Official will use these guidelines when necessary to interpret the Design Regulations. They are also intended to assist project applicants and their architects by providing graphic examples of the intent of the City's guidelines and regulations.

The Design Guidelines do not set a particular style of architecture or design theme. They are intended to establish a greater sense of quality, unity, and conformance with Kirkland's physical assets and civic identity. These guidelines are not intended to slow or restrict development, but rather to add consistency and predictability to the permit review process.

Urban Design Goals

Urban design goals and objectives for the desired future development of the area were adopted in 201502 as part of the Totem Lake Business District Neighborhood Plan:

~~**Urban Design Framework Goal:** *Provide a sense of neighborhood identity. The Totem Lake Neighborhood is comprised of distinct areas separated by built features, such as I-405. Urban design policies seek to establish visual connections between these areas, create effective transitions within and around the neighborhood, and provide a collective identity for the neighborhood.*~~

~~**Design Goals TL-21-TL-24**~~

- ~~• **Ensure that public and private development contribute to a coherent and attractive neighborhood identity.**~~
- ~~• **Develop gateway features that strengthen the character and identity of the neighborhood.**~~
- ~~• **Develop a new landscaped boulevard that provides a green visual connection between the four quadrants of the neighborhood through enhanced landscape public amenities.**~~
- ~~• **Provide interconnected streetscape improvements throughout the neighborhood that contribute to a sense of neighborhood identity and enhance visual quality.**~~

The Totem Lake Business District is comprised of distinct areas separated by major transportation corridors, such as I-405, NE 124th Street. Urban design policies seek to establish visual and non-motorized connections between these areas, create effective transitions within and around the district, and provide a collective identity for Totem Lake.

Design Goals TL 21-TL-25

- Goal TL-21: Ensure that public and private development contributes to a coherent and attractive identity for the business district.
- Goal TL-22: Develop gateway features that strengthen the character and identity of the Business District.
- Goal TL-23: Develop a new landscaped boulevard, or “Circulator” that provides a green visual connection between the subareas of the business district through enhanced landscape and public amenities.
- Goal TL-24: Provide interconnected streetscape improvements throughout the business district that contribute to a sense of neighborhood identity and enhance visual quality.
- Goal TL-25: Provide effective transitions between the light industrial, commercial and higher density multifamily uses in the business district and single family residential areas surrounding the district.

Design Vision for Totem Lake Business DistrictNeighborhood

The Totem Lake Business DistrictNeighborhood will continue to evolve into an attractive urban center, ~~with Totem Center at its core~~ as a dense, compact community, with a mix of business, commercial and residential uses and a high level of transit and pedestrian activity. Outside of the Business District Coreits core, the Plan for the Totem Lake Business DistrictNeighborhood Plan envisions new connections between areas separated by built features such as I-405, and building design that promotes a sense of community identity and continuity throughout the districtneighborhood.

~~The Plan emphasizes the wide array of residential, retail, light industrial and office uses that surround the core, in the remainder of the Urban Center. This~~ The rich mix of uses in Totem Lake is accompanied by enhanced mobility within the neighborhood, ~~through maximized~~district. Maximized vehicular capacity ~~that occurs through improvements~~ infrastructure investment and an expanded bicycle and pedestrian network ~~connections that provides~~ additional opportunities for pedestrian-oriented development and placemaking. Local transit connections, an extensive non-motorized network and a local boulevard system will all combine to complement and support the regional system.

The Plan envisions an attractive and economically strong districtneighborhood in Totem Lake. It acknowledges the challenges to the creation of a single community identity posed by the area’s natural and built elements that split the districtneighborhood into four fairly distinct quadrants. Totem Lake is the City’s only neighborhood bisected by Interstate-405. Nevertheless, the use of design measures that address important elements of design, ~~will move the~~ districtneighborhood forward into a ~~more~~ cohesive and coherent community. Key design issues to be addressed include human and architectural scale, breaking up of building mass, attention to building detail and appropriate building orientation, ~~The identity and appeal of Totem Lake will be strengthened through the~~ establishment of continuous and interconnected walking and biking networks; ~~pedestrian connections, and consideration of building orientation, as well as~~ improvements in the public realm, ~~including publicly accessible spaces along the frontage of new development and in public gathering spaces within the right of way~~; gateway enhancements; public art; and streetscapes with coordinated ~~such as consistent~~ street lights, sidewalk design, landscaping ~~elements~~ and street furniture. Implementation of the Circulator street concept to connect subareas of the district, combined with the reduction of block sizes achieved through new streets oriented to local traffic and new through block connections, will improve circulation and simplify wayfinding for visitors.

Several areasdistricts within the districtneighborhood present unique opportunities for development. The Planning Concept Map (Figure 1) illustrates where these ~~focal points and~~ opportunities exist. Further discussion in this section presents the desired vision for each of these areas, as well as for the landscaped boulevard or “Circulator”, that should

tie the district area's otherwise separate elements together.

MAP TO BE REPLACED – SEE NEXT PAGE

Development should be coordinated to provide a focal point for pedestrian activity in the larger, mixed-use villages

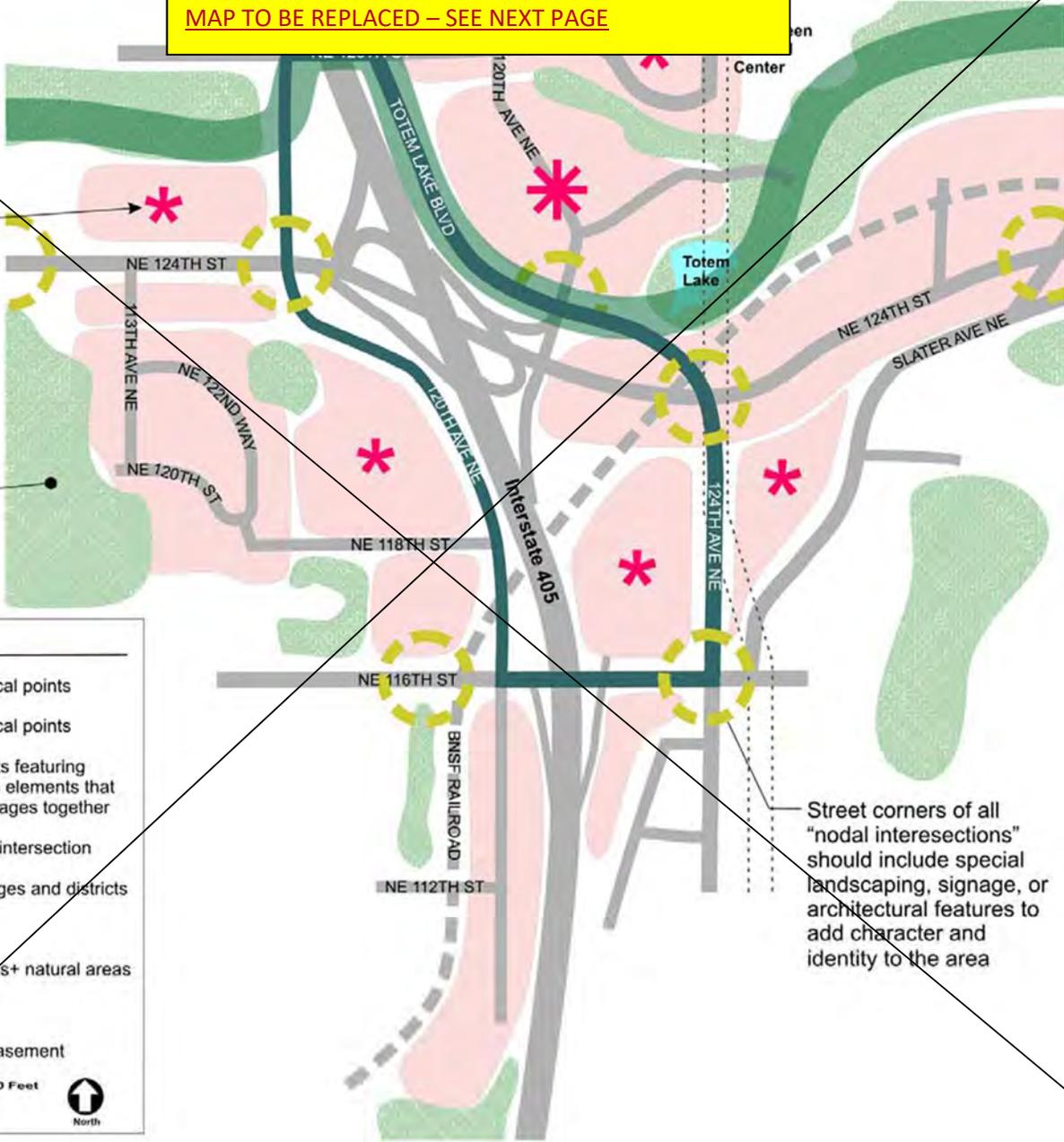
The wooded hillsides and natural areas that surround the Totem Lake area are a major character-defining feature

Street corners of all "nodal intersections" should include special landscaping, signage, or architectural features to add character and identity to the area

Legend

-  Major village focal points
-  Minor village focal points
-  Circulator streets featuring common design elements that tie individual villages together
-  Gateway/nodal intersection
-  Totem lake villages and districts
-  Greeway
-  Wooded hillsides+ natural areas
-  Railroad
-  Transmission easement

0 250 500 1000 Feet 



Vision for District TL 5

Bordered by Interstate 405, the BNSF Railroad, 124th Avenue NE, and NE 116th Street, tThe Totem Lake Plan envisions the TL5 zone area as a planned, lively mixed-use district. The potential for land assembly in the district, as well as its location adjacent to the freeway and Cross Kirkland Corridor (CKC) create an opportunity for substantial redevelopment. The western portion of the site is situated at an elevation somewhat lower than the freeway, enabling greater building heights with minimal impacts on surrounding development. The district's frontage along the CKC brings recreation and commuter users to the area, opportunities for connectivity to areas to the west, and provides opportunities for public gathering spaces.

The Plan envisions the expansion of the network of local access roads within the district, and designates through-block pathways roads connecting the new streets both north/south street to the east and west and from the east. Vehicular, pedestrian and bicycle access to properties within and beyond the district would be improved, and reliance on major arterial routes would be reduced. This network would be the foundation for an attractive grid of streets, wide sidewalks, and a supporting combination of commercial, office, and residential uses. The focal point of the village will be the spine of 123rd Avenue 120th Place-NE – extending from NE

116th Street over the CKCBNSF Railroad to NE 124th Street. Much of the road would resemble a “main street” with its storefronts, street trees, wide sidewalks, and on-street parking. Other notable features would be the taller office or residential buildings on visible sites bordering I-405 and a cluster of residential uses surrounding a small park site.

124th Avenue NE would be upgraded with a wider sidewalks, and street trees protected bicycle facilities, landscaping and wayfinding elements. Since the focus of retail activity will be on interior streets within the district building orientation may be largely to these interior streets. Building frontages along 124th Avenue will be important, but the street will also be defined by landscaping, lighting and wider sidewalks. Driveways- would be consolidated and coordinated with the internal street grid and properties on the east side of 124th Avenue NE. Storefronts would be clustered around major entry points to the development providing a welcoming entry. Also, building design and landscaping at the southeast corner of the village are important, as they will function as a major gateway to the village.

Parking would be provided in strategically located surface parking lots and within structures above, below, or behind commercial retail uses. Parking areas located adjacent to surrounding arterials would feature landscaping and other design features to maintain visual continuity along the street. Parking structures would either contain commercial retail uses at ground level or a combination of landscaping and architectural elements enhance the pedestrian environment.

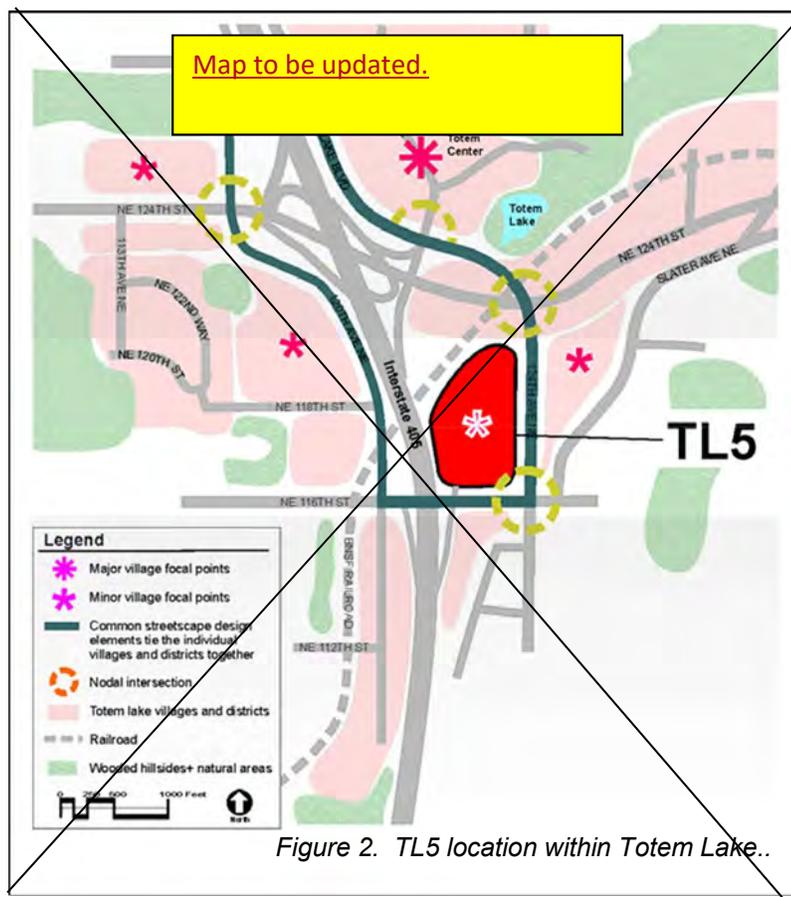
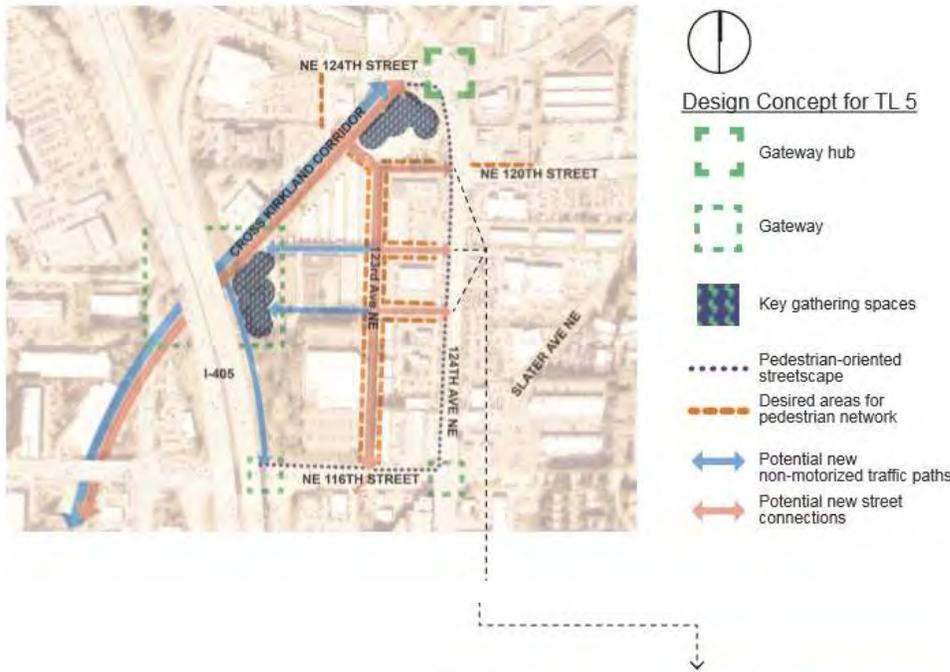


Figure 2. TL5 location within Totem Lake..

Taller residential or office buildings in the area bordering I-405 would create a striking skyline for the village. While the buildings may stand out from other village structures, they would be configured in a way that complements the village. Easy pedestrian connections, landscaping, and common streetscape features link the structures to the village's diversity of commercial/retail uses and amenities. As in other mixed use areas within the business district, developments will have publicly accessible spaces at their primary frontage, which contribute to the character and pedestrian-oriented quality of the area. These public spaces will be enhanced through including public art, water features, and distinctive landscaping that will lend a civic quality to the spaces and create opportunities for both passive and interactive elements.



New figures



Alta Planning + Design

~~Other village buildings would generally be between one story and six stories tall. The buildings would use a variety of materials and colors and modulated walls and rooflines to reduce their architectural scale. Storefronts would contain attractive details that provide interest at a pedestrian scale. Residential uses would feature prominent building entries and individual balconies and typically be clustered around a courtyard or small park area.~~

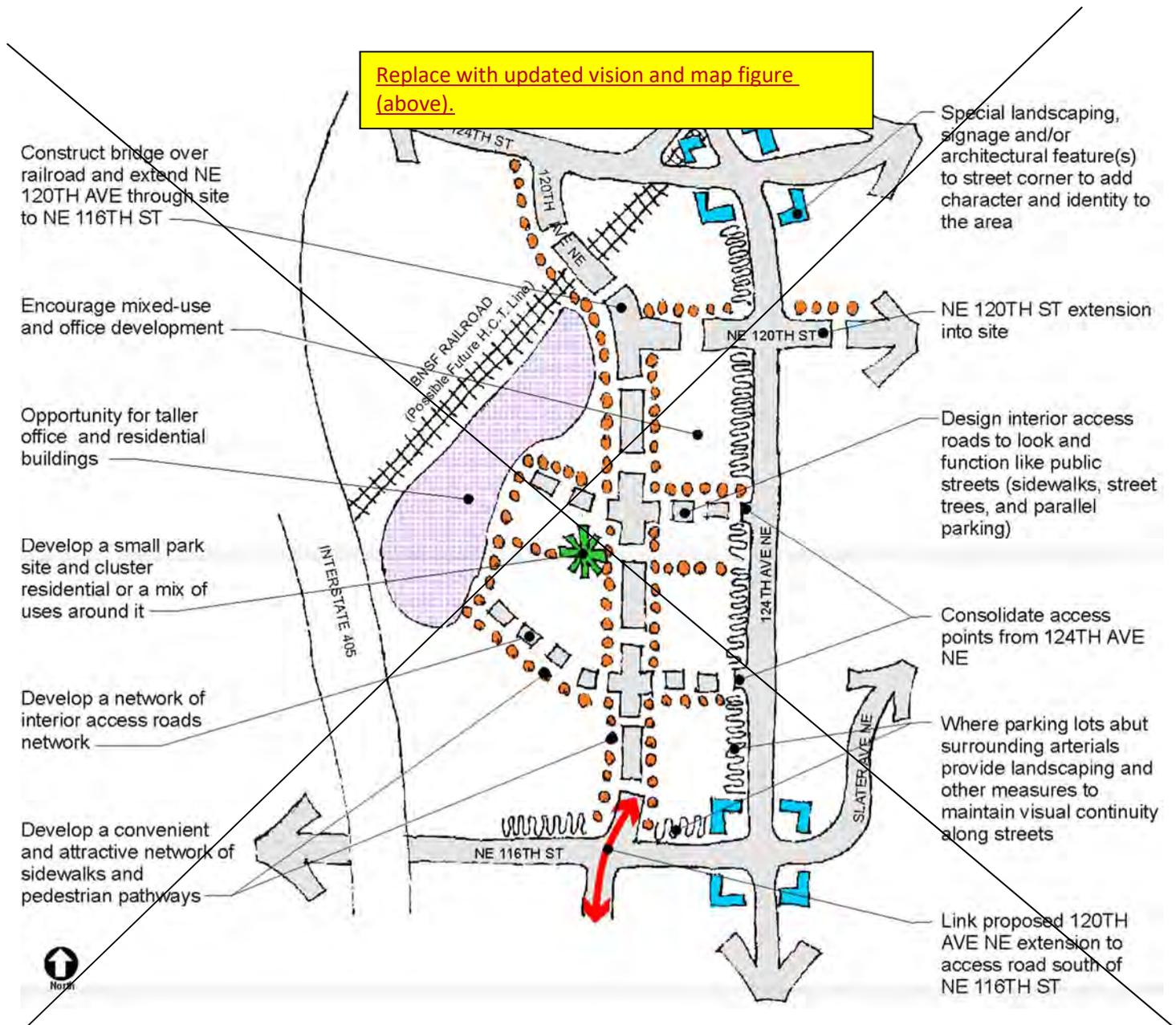


Figure 3. Redevelopment concept for TL5.

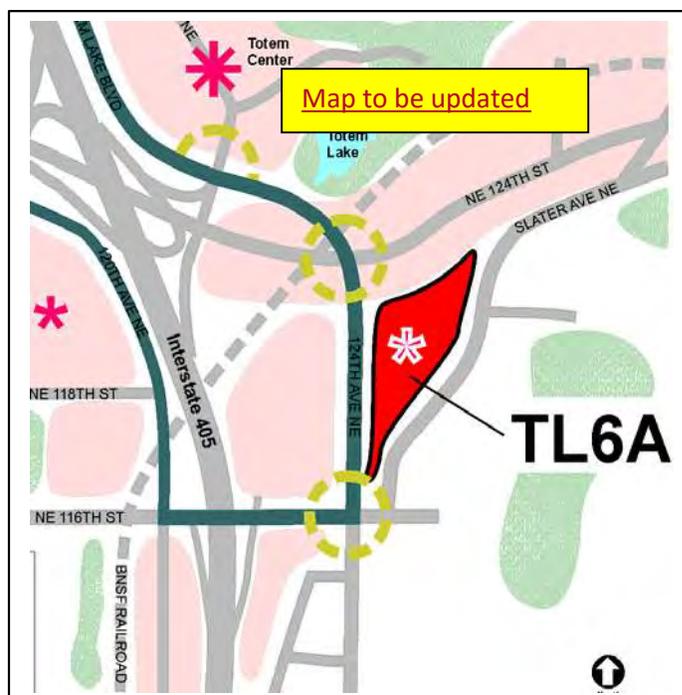
Vision for District TL 6A

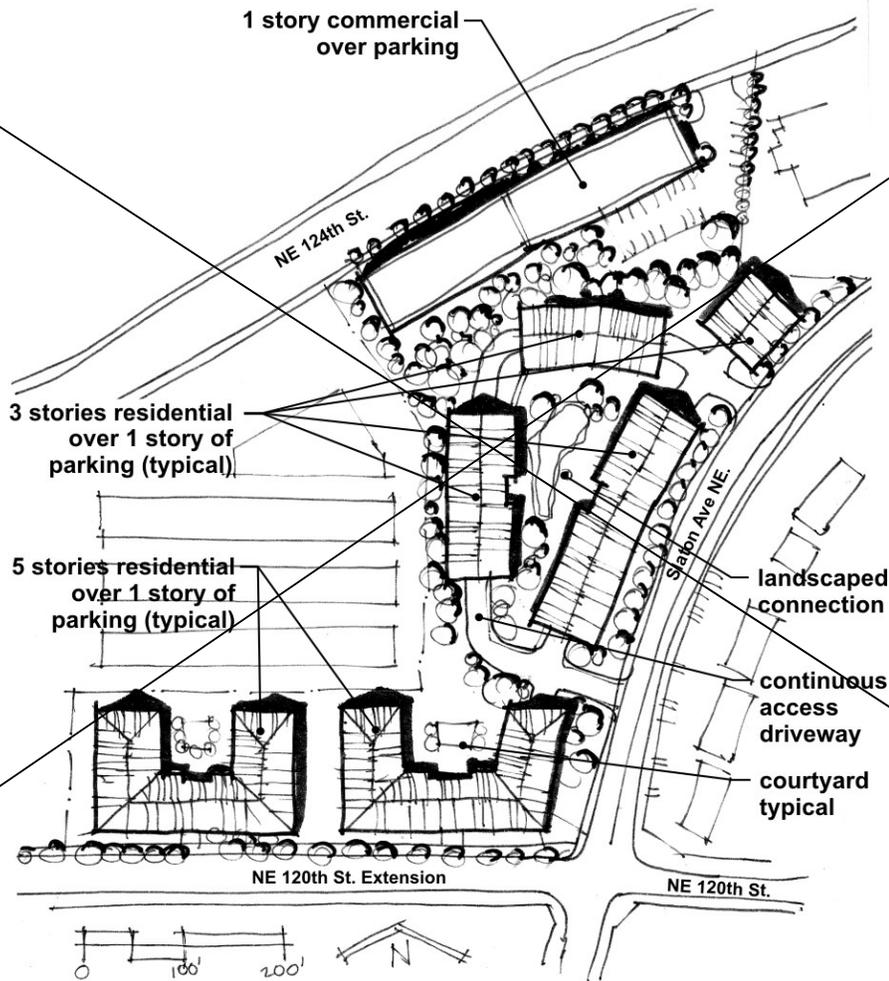
The TL6A district, located at the eastern edge of the Totem Lake Business District ~~Neighborhood~~, would feature an attractive mix of commercial uses along 124th Avenue NE and NE 124th Street, developed with ~~and~~ terraced multi-family or office uses on uphill sites towards Slater Avenue NE. ~~The extension of NE 120th Street would provide~~ a convenient east-west connection, linking the Lake Washington Institute of Technology and residential neighborhood to the east to the core of the business district. Developments along NE 120th Street will provide publicly accessible spaces along their primary frontage, enhancing the streetscape for pedestrians. ~~for both pedestrians and motorists between 124th Avenue NE and Slater Avenue NE.~~

~~Both 124th Avenue NE will and NE 124th Street would be significantly upgraded, with wide sidewalks, protected bicycle facilities, landscaping and wayfinding elements featuring landscaped medians in areas that don't conflict with site access, better street lights, sidewalks, and planting strips. While both corridors are likely to remain automobile-oriented in their use mix, they would be designed to be more accessible for the pedestrian.~~

Auto dealers may will remain clustered along both 124th Avenue NE and NE 124th Street. Adjacent to the dealerships, attractive landscaping strips along the sidewalks with seasonal plantings and low level signage will be provided. While some surface parking areas may will remain on the lots, many of the dealers will incorporate some structured parking to accommodate their vehicular stock. Other sites along 124th Avenue NE and NE 124th Street will retain a mix of commercial uses.

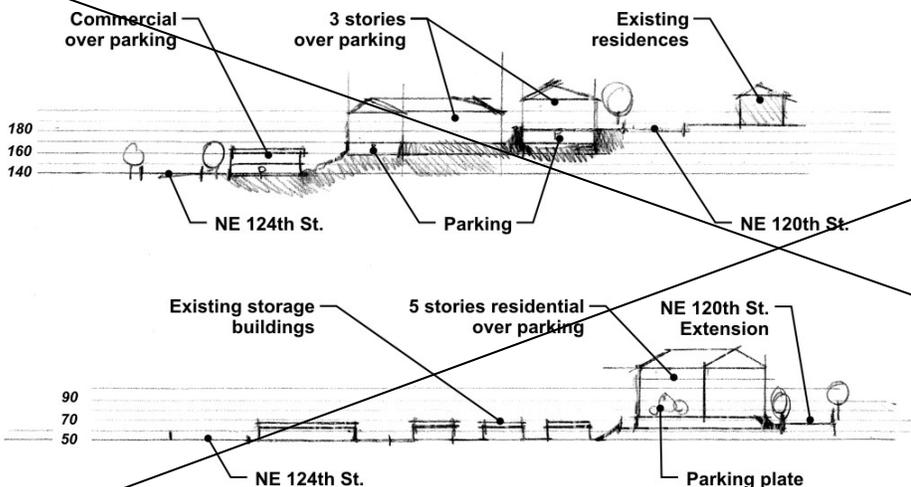
~~Multi-family residential uses will be concentrated on the uphill portion of the district, adjacent to Slater Avenue NE.~~ In areas where significant elevation change exists from the east to west, Individual buildings will be able to stairstep down the hillside, following the natural earth form and creating a dramatic visual setting. The topography also allows parking areas to be hidden under buildings. Buildings can be designed to cluster around small courtyard courtyards and useable open spaces. A system of pathways will connect buildings within the district to the surrounding streets and to adjacent properties in some areas.





Delete graphic as it is too specific and outdated.

Figure 5. Redevelopment concept incorporating multi-family uses along Slater Avenue NE and planned NE 120th Avenue extension. Note how residential buildings are configured towards the street and around common open spaces. The section drawings above illustrate how development can take advantage of slopes.



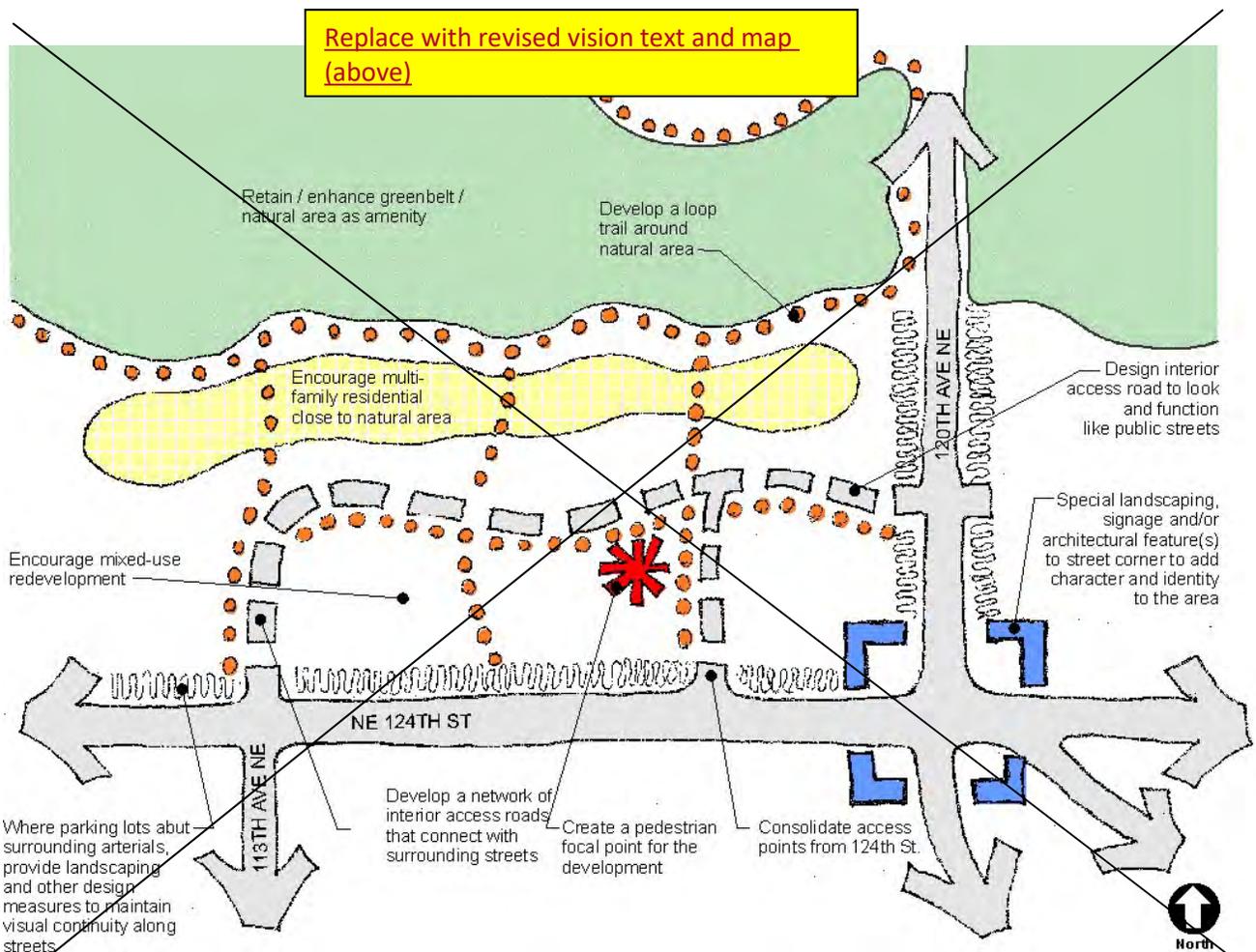
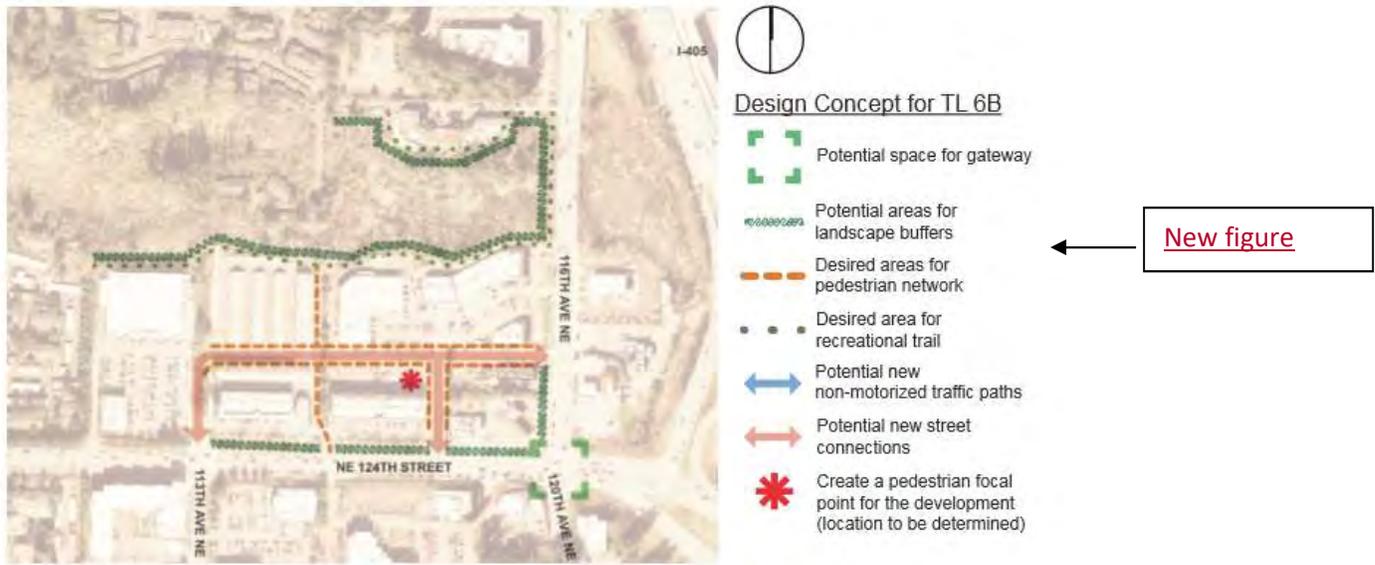


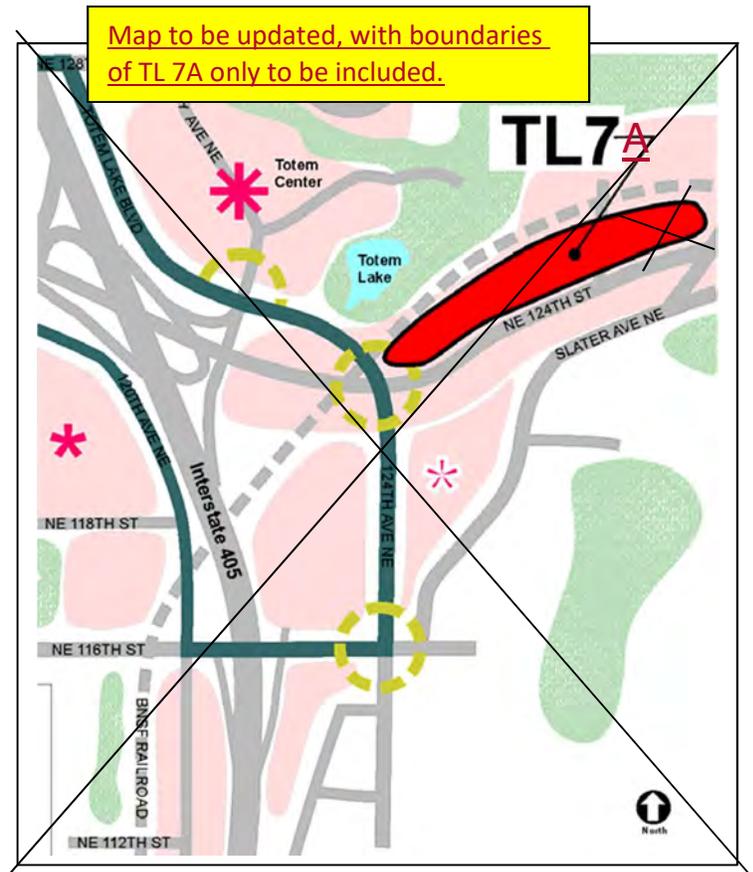
Figure 7. TL 6B Village Design Concept.

Vision for TL 7A

Located at the eastern edge of the Gateway Hub, just southeast of the lake itself, a key gateway to the City from the east, the TL 7A subarea district lies between the north side of the NE 124th Street arterial, and the CKC just on the southeastern edge of Totem Center. The Totem Lake gateway hub includes the westernmost tip of the zone. Site design in this area responds to its prominence at the north end of the CKC Connector overpass as a major route for bicycle and pedestrian commuters and recreational users and development includes pedestrian connections from NE 124th Street to the CKC. A gateway feature at the district's eastern boundary, as

well as attractive landscaping, street lighting and signage throughout the area will provide an inviting image at the entrance to the neighborhood and City.

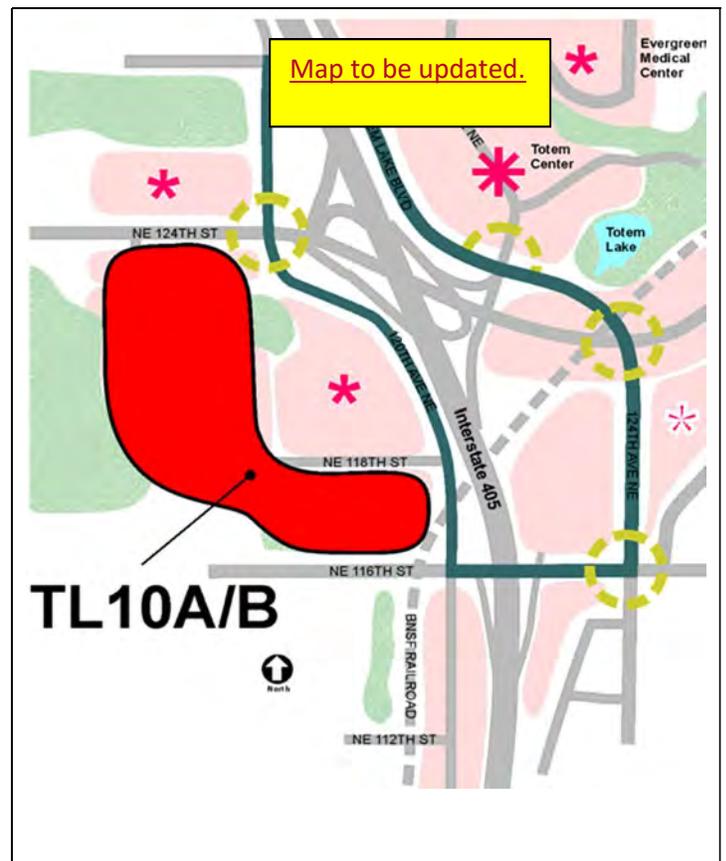
The district is ideally located to feature a combination of uses and business oriented to the City and greater region. Large parcels in the district are particularly well-suited to display for automobile sales.



Vision for TL 10A and TL 10B

The I-405 Corporate Center in TL 10A is a business park that serves as a model of coordinated efforts in signage and building design for the areas in transition to the south. New development in the area will continue to complement existing structures.

–District TL 10B to the south provides the link between the established Corporate Center and the evolving office park area in TL 10D and TL 10E to the south. Development in this partially wooded area provides a mix of housing and office uses. The topography and vegetation in the area enable taller residential buildings to be well situated to avoid impacts to the residential areas to the west, while providing a significant housing resource for the business district neighborhood and the city.

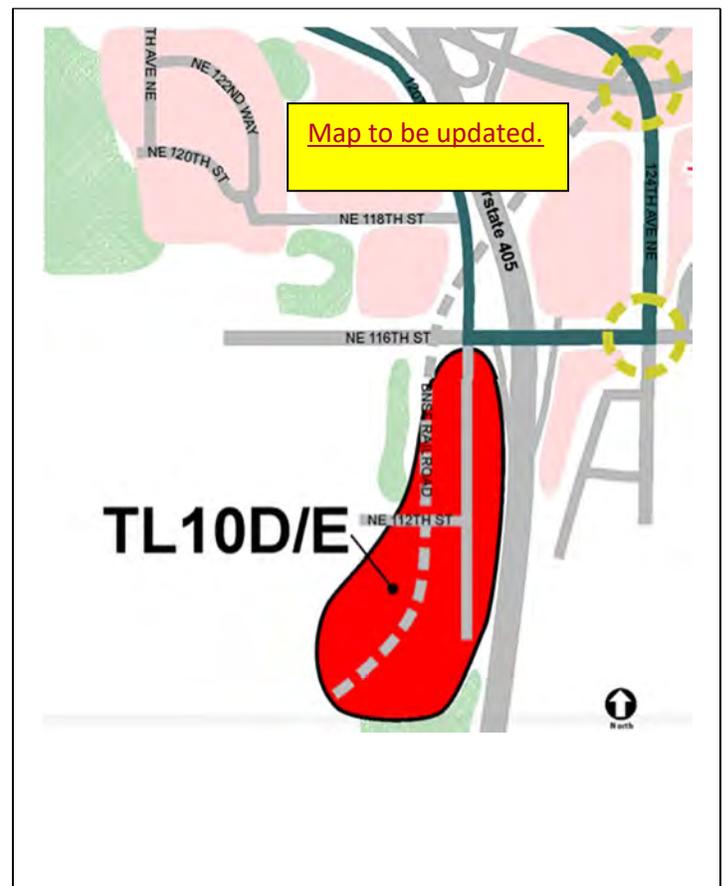


Vision for TL 10D and TL 10E

Visibility and proximity to I-405, as well as the land's elevation below the freeway to the east and the residential areas to the west, provide for substantial redevelopment opportunities throughout these districts. As uses in the area convert from manufacturing and light industrial to office, high-tech and residential (within the western perimeter of TL 10D), development in the area can begin to create a more cohesive and distinct visual image. Supportive service and retail uses will add to the area's appeal for workers and residents, and reduce the need for travel outside the district.

Taller buildings can be accommodated here with minimal visual impacts to territorial views from the freeway. Consideration of elements that produce distinctive roof forms and minimize mass at upper levels will contribute to a skyline that is visually interesting.

A gateway to the Totem Lake Business District is located at the south end of the area, along the CKC. The CKC runs through the area providing opportunities for non-motorized transportation and public open space for employees and residents. Building design along the corridor should be sensitive to and benefit from the corridor's use as a transportation corridor for commuters.

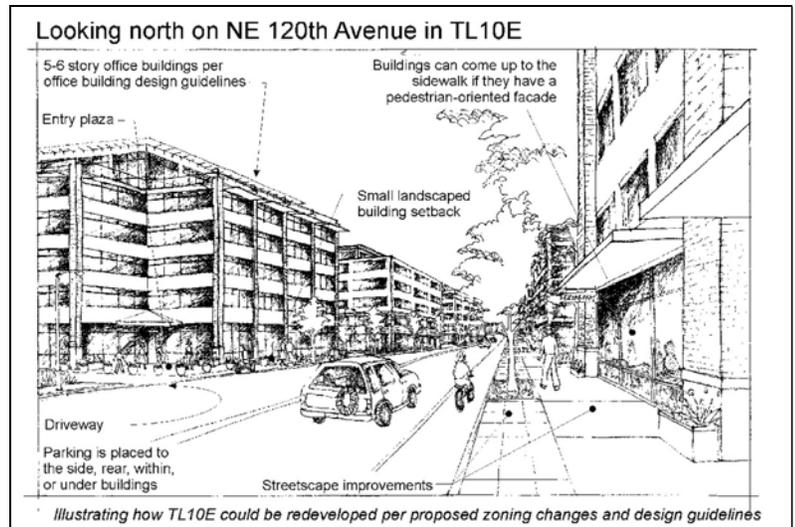


and recreational users. Design should acknowledge the high visibility of buildings in this area and incorporate measures to address parking garages, blank walls and pedestrian access between the CKC and adjacent structures.

Common streetscape elements aimed at the creation of a pleasant pedestrian-oriented environment will be very important in this area. An enhanced grid of major pedestrian sidewalks will contribute to the pedestrian experience throughout the district.

A successful residential community within the western portion of the area (TL 10D) will provide a close-in housing location for Totem Lake employees and add evening activity to the district. The CKC and a Attractive walkways to connect residents within TL 10D to points east and north will be important to ensure the success of the mix of uses throughout the area.

Support for shared and coordinated signage throughout the district will help to minimize visual clutter and contribute to the visual identity of the area.



Vision for Large-Site Development

Larger sites within the Totem Lake Business District neighborhood present opportunities for master planning to provide coordinated development. Within TL 4B for example, a vibrant mixed-use center could be created, combining retail, office and residential uses. While parcels in this area and others in the business district neighborhood can provide an attractive face along the major traffic corridors including the Totem Lake Circulator, many are large enough to provide their own interior vehicular and pedestrian pathways, as well as gathering areas focal points for pedestrians. These may include a plaza area surrounded by shops, or wide sidewalk areas along an interior access street.

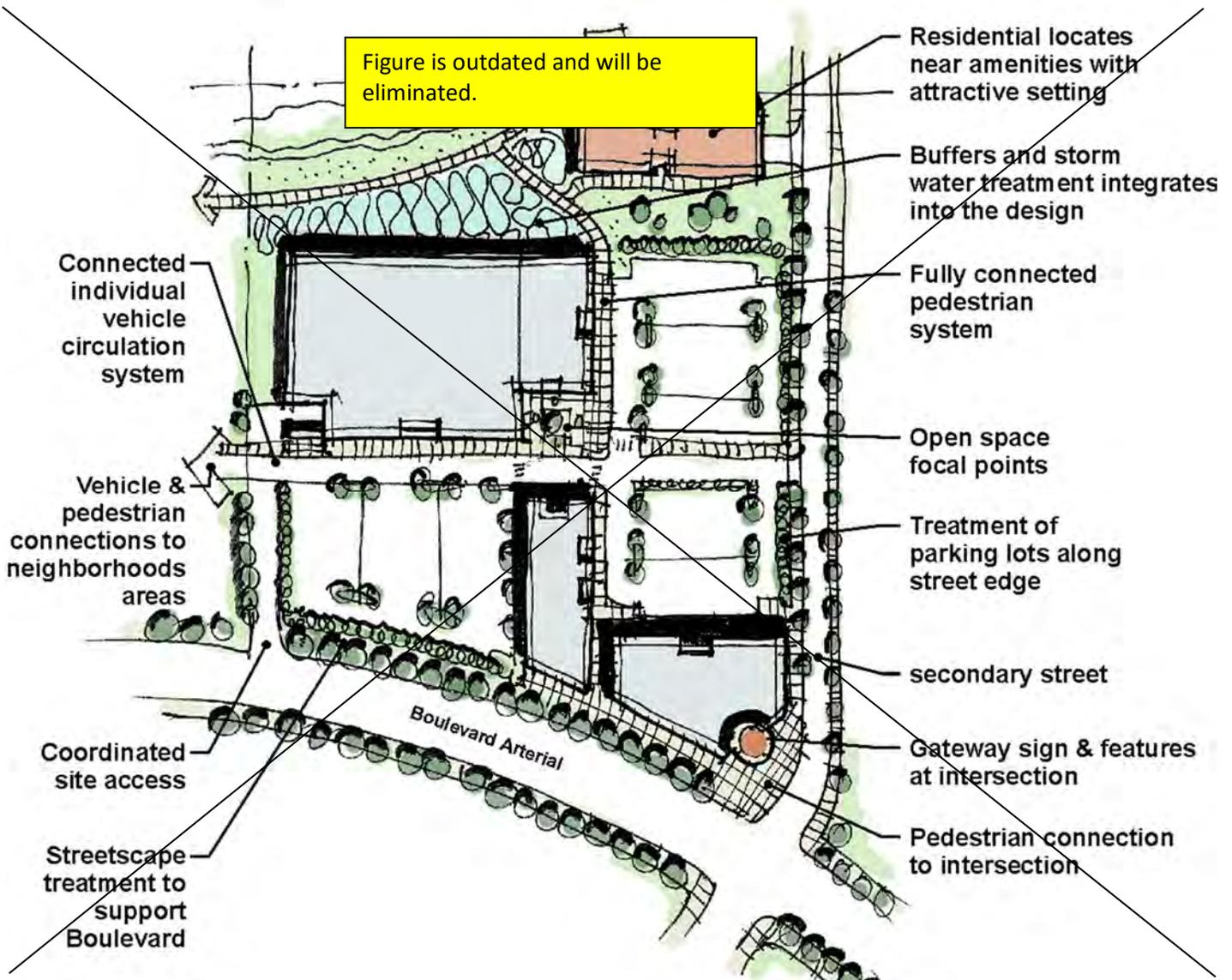


Figure 8. Conceptual design guidelines for large site development in Totem Lake.

Vision for Landscaped Boulevard, the Totem Lake “Circulator”

The Totem Lake Business District Plan envisions the creation of a landscaped boulevard that links the four quadrants of the business district neighborhood with a recognizable character. Improvements including landscaping and public amenities will be provided by both public and private development to ensure a cohesive streetscape experience through enhanced landscape and public amenities. The boulevard will provide a hospitable environment for pedestrians and drivers through reducing scale, providing shade and seasonal interest and reducing noise levels. Improvements may include widened and meandering planting areas, continuous and clustered tree plantings and shrubbery, and plantings varying in seasonal color, texture and shape. Other elements, such as lighting, directional signs, benches, varying pavement texture, bike racks, transit shelters, interactive elements, s and-public art and water features will further enhance the route and experience.

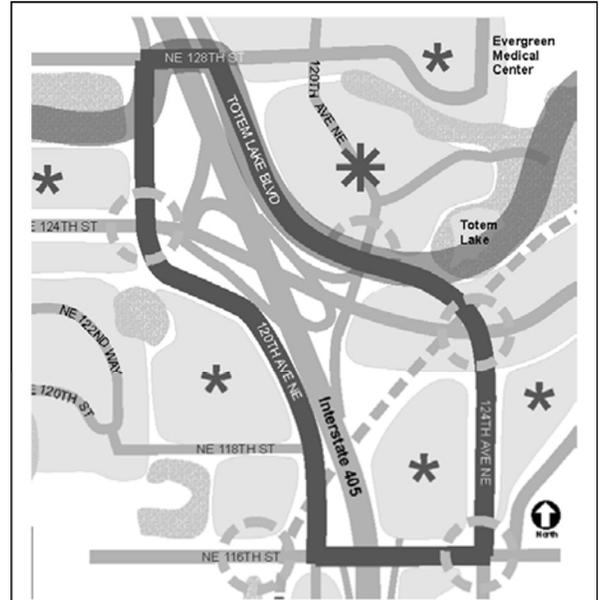


Figure 9. A series of landscaped boulevards (a.k.a. the Totem Lake Circulator) links the various quadrants of the Totem Lake area.

The boulevard will not only visually connect the district's neighborhood's separate areas, but will also help local circulation. In most areas, existing rights-of-way can be used to create the boulevard. In others, dedication may be necessary to provide the necessary improvements and amenities.

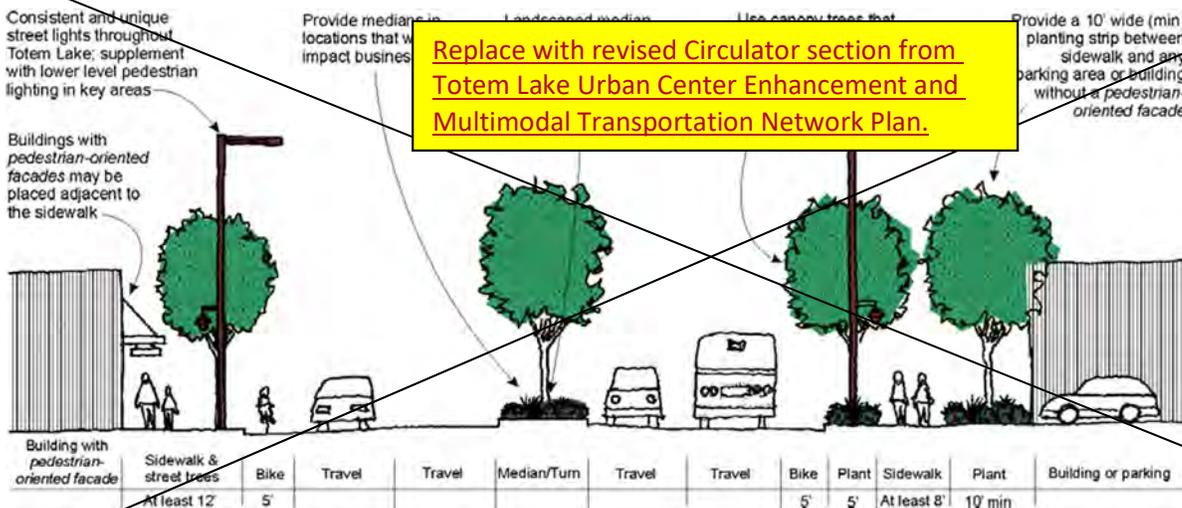


Figure 10. Conceptual design guidelines for large site development in Totem Lake



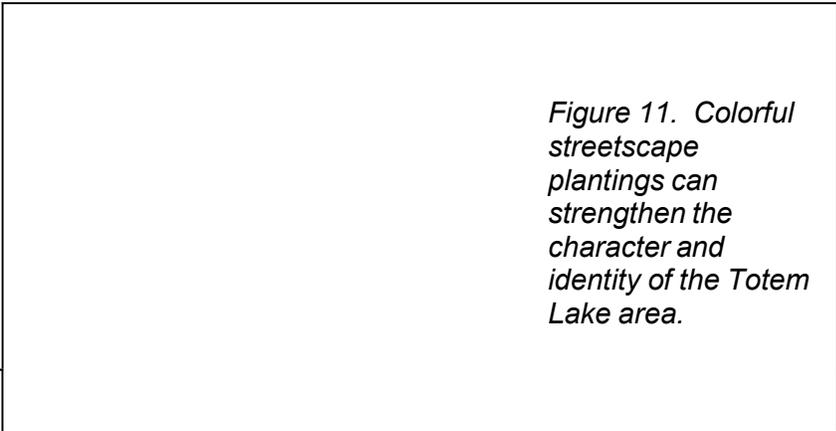


Figure 11. Colorful streetscape plantings can strengthen the character and identity of the Totem Lake area.

Design

Design Guidelines

The following design guidelines for the Totem Lake [Neighborhood Business District \(TLBD\)](#), outside of the [Business District Core \(BDC\)](#), (TLN) are intended to help guide the future development of the [districtneighborhood](#) toward the future vision described in the Totem Lake [Business District Plan](#) and elsewhere in this document. These guidelines include both [neighborhooddistrict](#)-wide measures and unique measures specific to individual districts or sites within Totem Lake.

Improvements to streets, parks and the development of new public facilities will create a dynamic setting for civic activities and private development.

1. Entry Gateway Features

The Comprehensive Plan calls for gateway features at the key entry points into neighborhoods and business districts.

Objectives

- To enhance the character and identity of the Totem Lake [Business District](#) [Neighborhood](#).

Guideline

Incorporate entry gateway features in new development in the vicinity of gateways/nodal intersections identified in the Concept Map (Figure 1). Gateway features should incorporate design elements associated with or desired in the [districtneighborhood](#),

depending on available space. Gateway features should include some or all of the following:

- Distinctive landscaping, including suggested common landscaping elements from the City's Urban Forester.
- Artwork (e.g. vertical sculpture incorporating historical information about Totem Lake).
- A gateway sign with the City logo.
- Multicolored masonry forming a base for an entry sign.
- Decorative lighting elements.
- [Elements identified to be provided at gateways to support wayfinding in the business district.](#)



2. Street Trees

Objectives

- To upgrade the character and identity of the Totem Lake Business DistrictNeighborhood.
- To enhance the pedestrian environment on the Totem Lake Business DistrictNeighborhood.
- To use trees that provide seasonal interest.
- To use trees appropriate to the urban environment of the Totem Lake Business DistrictNeighborhood.

Discussion

The repetition of trees bordering streets, internal roadways, and pathways can unify a community's landscape. Trees can add color, texture, and form to the urban environment. A strong street tree planting scheme can establish community identity and provide a respite from the weather and the built environment.



Guidelines

- Incorporate street trees along all streets, internal access roads, and pathways.
- Encourage developments to use street trees as a unifying feature of the development.
- Select and maintain tree species that will accommodate pedestrian and vehicular traffic, and maintain visibility into and through sites for safety purposes.

3. Street Corners

Objectives

- To enhance the appearance of highly visible locations.
- To upgrade the character and identity of the Totem Lake Business District ~~Neighborhood~~ and its individual districts.
- To enhance pedestrian access and safety.

Discussion

Street corners, especially along arterial corridors, provide special opportunities for visual punctuation and an enhanced pedestrian environment. Buildings on corner sites that incorporate architectural design elements create visual interest for the pedestrian and provide a sense of human proportion and scale.

Guidelines

- Encourage design treatments that emphasize street corners through the use of building location and design, plaza spaces, landscaping, distinctive architectural features, and/or signage. Street corners can be an excellent location for plazas, particularly where adjacent storefronts and building entries are provided. In auto-oriented areas, landscaping elements on street corners can enhance the character of the area and visual relief from pavement areas. Such landscaping elements should incorporate a variety of plant types and textures that add seasonal interest.
- Encourage all buildings located at or near street corner to incorporate special architectural elements that add visual interest and provide a sense of human proportion and scale. This could include a raised roofline, turret, corner balconies, bay windows, special awning or canopy design, and/or distinctive use of building materials (see the following examples).



Figure 14. This building uses a cropped corner with entry and decorative roofline, building materials, and details to provide

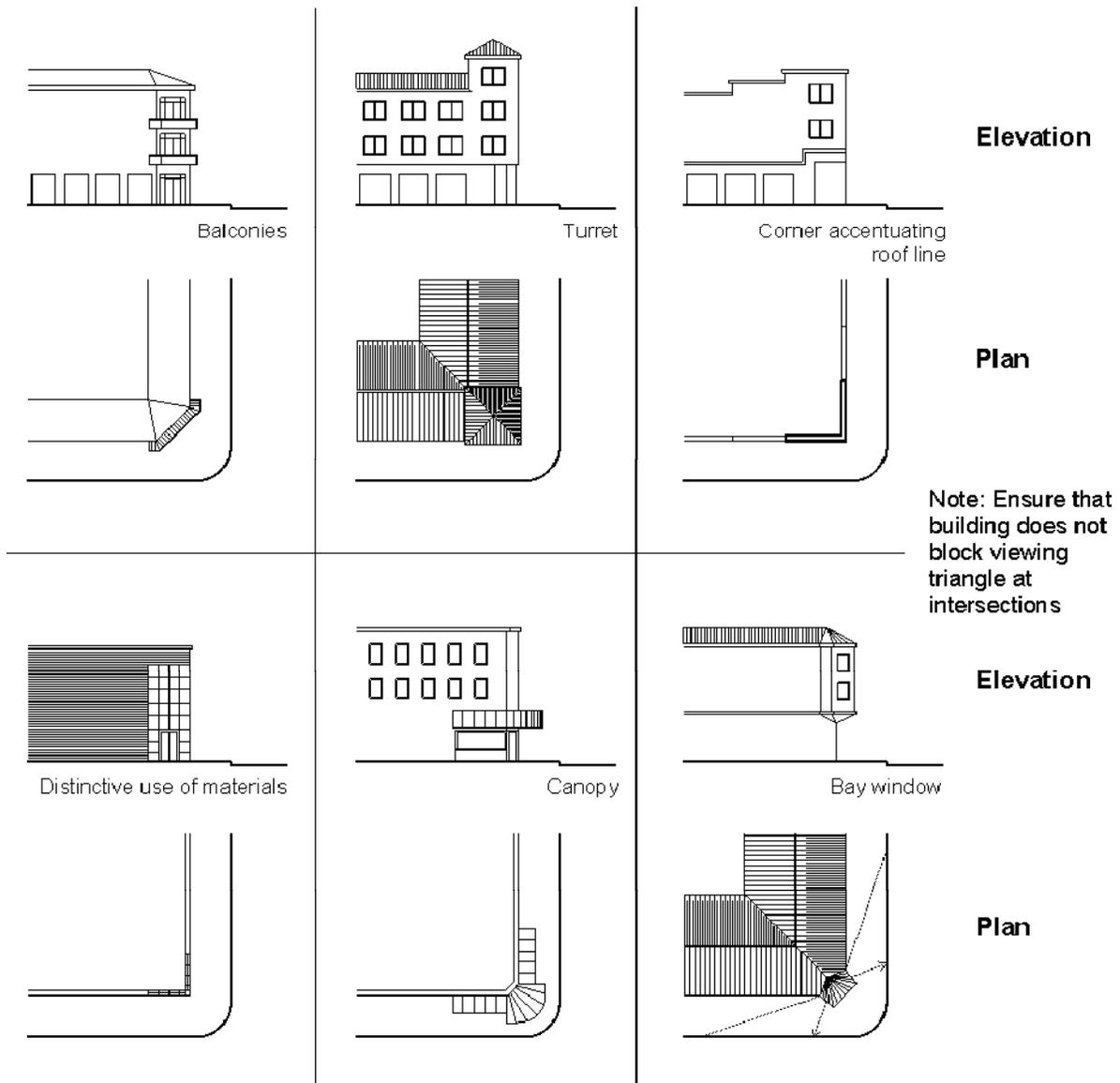


Figure 15. Desirable building elements for street corners.

4. Pedestrian-Friendly Building Fronts

Objectives

- To enhance the pedestrian environment within the Totem Lake Business District. ~~Neighborhood.~~
- To create safe and active sidewalks and pathways.

Guidelines

Incorporate transparent windows and doors and weather protection features along all non-residential facades adjacent to a sidewalk or internal pathway. Weather protection features could include awnings, canopies, marquees, or other permitted treatments.

Alternative treatments may be considered if they meet the objectives. For example, reduced transparency and weather protection levels may be considered if an alternative configuration provides other amenities above and beyond what is required by KZC Chapter 92 and the Design Guidelines, and if the building details and architectural treatments provide interest at close range and won't "deaden" the pedestrian environment or create a potential safety problem.



5. Building Location and Orientation

Objectives

- To enhance the character and identity of the Totem Lake Business District Neighborhood.
- To upgrade the appearance of streets within the Totem Lake Business District Neighborhood.
- To increase pedestrian circulation.
- Create focal points, particularly on large sites.
- To encourage development configurations that minimize negative impacts to adjacent single family residential areas.



Guidelines

- Locate and orient buildings toward streets, plazas or common open spaces, and major internal pathways, with parking to the side and/or rear.
- Configure buildings to create focal points of pedestrian activity. This is particularly important on large sites.
- Configure development to provide opportunities for coordinated pedestrian and vehicular access. Where there are no current opportunities for coordinated access, developments should provide the opportunity for future coordination, where desirable, should the adjacent site be redeveloped in the future.
- Site and orient multi-story buildings to minimize impacts to adjacent single family residents. For example, if a multi-story building is located near a single family property, provide landscaping elements and/or minimize windows and openings to protect the privacy of adjacent homes. Another consideration is to increase upper level building setbacks.

- Ensure Encourage vehicle sales uses to locate their showrooms towards the street (with parking to the side or rear):

- Allow designated vehicle display areas between a portion of the property street frontage if the display is integrated creatively with the landscaping. ~~This could include cars on a rock outcropping or on a discreet structure that allows a display vehicle to “float” over the landscaping.~~
- Allow increased signage through coordinated master sign plans.
- Allow modifications in perimeter landscaping adjacent to a street.
- Ensure that inventory areas located along



the perimeter are organized and landscaped.

- f. Encourage buildings located adjacent to any street to orient to the street. This includes pedestrian entries from the sidewalk and windows facing the street. Avoid fences or hedges that block visibility between buildings and the street. Exceptions may be considered consistent with the objectives and guidelines herein.

Special considerations in Districts TL 5, TL 6B and other Large Site Developments

1. TL 5: In this district where buildings may front on more than one street, first priority for building orientation should be to any designated pedestrian oriented street.
2. TL 6A: Residential buildings located adjacent to NE 120th Street should be oriented toward this street and to Slater Avenue NE. Common and/or individual entries and windows should face the street. Parking areas should not be located between the building and the street.
3. TL 6B: Single purpose residential buildings should be configured and oriented to take advantage of the greenbelt area to the north. For example, buildings could be arranged in a courtyard layout with the courtyard opening towards the greenbelt area.
4. TL 5, TL 6B and other Large Site Development: Where buildings front on both pedestrian-oriented streets and through-block interior pathways, building orientation may be to internal focal points, public gathering spaces and streets. Parking areas should not occupy the majority of a site's frontage.
5. TL 5, TL 6B and other Large Site Development: Where buildings are oriented to an interior open space or courtyard, primary building entries may orient to the open space provided there is direct visibility in to the open space from the sidewalk. Windows should be provided on the street façade.

6. Sidewalk and Pathway Widths

Objectives

- To provide wide sidewalks and pathways that promote an increase in pedestrian activity within the Totem Lake Business District Neighborhood.

Discussion

Sidewalks have three overlapping parts with different functions: the curb zone, the movement zone, and the storefront or activity zone. A well-sized and uncluttered movement zone allows pedestrians to move at a comfortable pace.

Guidelines

- Integrate a "curb zone" into the sidewalk or pathway width. This space can include street trees, newspaper stands, street signs, garbage cans, phone booths, mail boxes, etc. Subtle changes in paving patterns between the curb zone and the movement zone can be effective and should be considered.

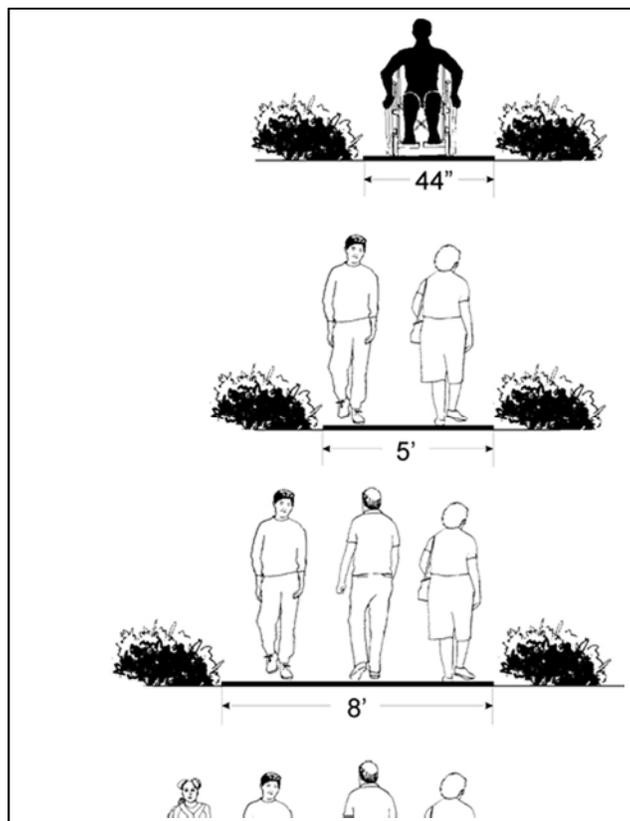


Figure 18

Pathway widths depend on level of activity and location

- b. Sidewalks or pathways adjacent to moving vehicular traffic need generous buffers to make them safer and more inviting. Landscaping elements are particularly important physical and visual buffers between walkways and streets or other vehicle access areas. As a general rule, the higher the travel speed, the greater the buffer should be between moving cars and pedestrians.
- c. Design sidewalks and pathways to support a variety and concentration of activities and provide a separation for the pedestrian from the busy street. Specifically:

Considerations for the “movement zone” widths:

- Curb zones with parallel parking typically need 4'-6'; without parallel parking: 3'-4'.
- 12' accommodates 4 persons walking abreast.
- 8' accommodates 3 persons walking abreast.
- 5' accommodates 2 persons walking abreast.

Considerations for the “store front zone” widths:

- Outdoor dining uses: 6' allows for one table.
- Outdoor displays typically need at least 4' (6' preferable).



7. Pedestrian Coverings

Objectives

- To provide shelter for pedestrians.
- To provide spatial enclosure and add design interest to a retail or office streetscapes.

Discussion

The design and width of pedestrian coverings should be determined by their function, the building's use and the type of street.

As a general rule, the more traffic an entry is expected to accommodate, the larger the covered area should be at the entry. Larger porches and covered entries also invite pedestrian activity. For example, a 5' x 5' covered area allows two adults to converse comfortably out of the rain. A 3' to 4' wide canopy will provide rain cover for window-shopping, a 5' wide or greater canopy will provide cover for a street sale, and a 7' to 8' wide canopy will provide room for a window shopper and a passing couple.

The width of the sidewalk should also be considered when sizing the pedestrian covering (wider sidewalks can accommodate wider pedestrian coverings). Canopies and awnings should be appropriately dimensioned to allow for tree growth, where applicable. The architecture of the building and the spacing of individual storefronts should help determine the appropriate placement and style of the canopy or awning. Continuous, uniform awnings or canopies, particularly for multi-tenant retail buildings, can create a monotonous visual environment and are discouraged.

Guidelines

- Provide weather protection along the primary exterior entrance of all businesses, residential units, and other buildings.
- Design weather protection features to provide adequate width and depth at building entries and along building facades that are oriented toward sidewalks and pathways.
- Pedestrian covering treatments may include: covered porches, overhangs, awnings, canopies, marquees, recessed entries or other similar features. A variety of styles and colors should be considered, where compatible with the architectural style of the building and the ground floor use.
- Back lit, plastic awnings are not appropriate.



8. Blank Walls

Objectives

- To minimize visible blank walls.
- To enhance public safety along sidewalks and pathways.
- To encourage design elements that enhance the character of buildings at all perceived distances.

Discussion

Blank walls on commercial street frontages deaden the pedestrian environment and can break the continuity of uses along a street or pathway. Blank walls can also create a safety problem, – particularly where adjacent to pedestrian areas, as they don't allow for natural surveillance of those areas. However, in some cases fire walls, for example, require the intrusion of a flat, unadorned surface. The adverse impact of a blank wall on the pedestrian streetscape can be mitigated through the methods listed in the Guidelines below.

Guidelines

Avoid blank walls near sidewalks, major internal walkways, parks, and pedestrian areas. The following treatments mitigate the negative effects of blank walls (in order of preference):

- Configure buildings and uses to minimize blank walls exposed to public view.
- Provide a planting bed with plant material to screen most of the wall.
- Install trellises with climbing vines or plant materials to cover the surface of the wall. For long walls, a trellis or trellises should be combined with other design treatments to avoid monotony.
- Provide artwork on the wall surface.
- Provide architectural techniques that add visual interest at a pedestrian scale. This could include a combination of horizontal building modulation, change in building materials and/or color, and use of decorative building materials.
- Other treatments may be proposed that meet the intent of the guidelines.



9. Lighting

Objectives

- To enhance safety.
- To create inviting pedestrian areas.
- To provide adequate lighting without creating excessive glare or light levels.

Discussion

Overpowering and uniform illumination from commercial uses creates glare and destroys the quality of night light, especially for adjacent residential areas. Well placed light fixtures will form individual pools of light and maintain sufficient lighting levels for security and safety purposes.

Guidelines

- Provide adequate lighting levels in all areas used by pedestrians and automobiles, including building entries, walkways, parking areas, circulation areas, and open spaces. Recommended minimum light levels:
 - Building entries: 4 foot candles
 - Primary pedestrian walkway: 2 foot candles
 - Secondary pedestrian walkway: 1-2 foot candles
 - Parking lot: .60 -1 foot candle
 - Enclosed parking garages for common use: 3 foot candles
- Lighting should be provided at consistent levels, with gradual transitions between maximum and minimum levels of lighting and between lit areas and unlit areas.
- Building facades in pedestrian areas should provide lighting to walkways and sidewalks through building mounted lights, canopy- or awning-mounted lights, and display window lights. Encourage variety in the use of building-mounted light fixtures to give visual variety from one facade to the next.
- Minimizing impacts of lighting on adjoining activities and uses should be considered in the design of lighting. This is particularly important adjacent to residential uses.

Parking lot light fixtures should be non-glare and mounted no more than 15' above the ground. Lower level lighting fixtures are preferred to maintain a human scale. Lights up to 20' may be used for safety, when needed. Ideally, all exterior fixtures should be fitted with a full cut-off shield to minimize light spill over onto adjoining properties.



10. Pedestrian Amenities

Objectives

- To provide amenities that enrich the pedestrian environment.
- To increase pedestrian activity in the Totem Lake Business District Neighborhood.

Discussion

Site features and pedestrian amenities, such as lighting, benches, paving, waste receptacles, and other site elements, are an important aspect of a business district's character. These elements reduce apparent walking lengths and unify the district's visual character. In zones where public pedestrian space is required to be provided at along the frontage of a building, additional amenities may be required to provide an attractive gathering space.

Guidelines

Provide pedestrian amenities along all sidewalks, interior pathways and within plazas and other open spaces. Desired amenities include:

- Pedestrian-scaled lighting (placed between 12'-15' above the ground).
- Seating space. This can include benches, steps, railings and planting ledges. Heights between 12" to 20" above the ground are acceptable, with 16" to 18" preferred. An appropriate seat width ranges from 6" to 24".
- Pedestrian furniture such as trash receptacles, consolidated newspaper racks, bicycle racks, and drinking fountains.
- Planting beds and/or potted plants.
- Unit paving such as stones, bricks, or tiles.
- Decorative pavement patterns and tree grates.
- Water features.
- Informational kiosks.
- Transit shelters.
- Decorative clocks.
- Artwork.





Figure 17. Decorative pavement patterns (top), benches and pedestrian-scale lighting (middle), and informational kiosk (bottom)



Figure 29. This example combines a sculptural water feature with landscaping

11. Interior Pedestrian Connections

Objectives

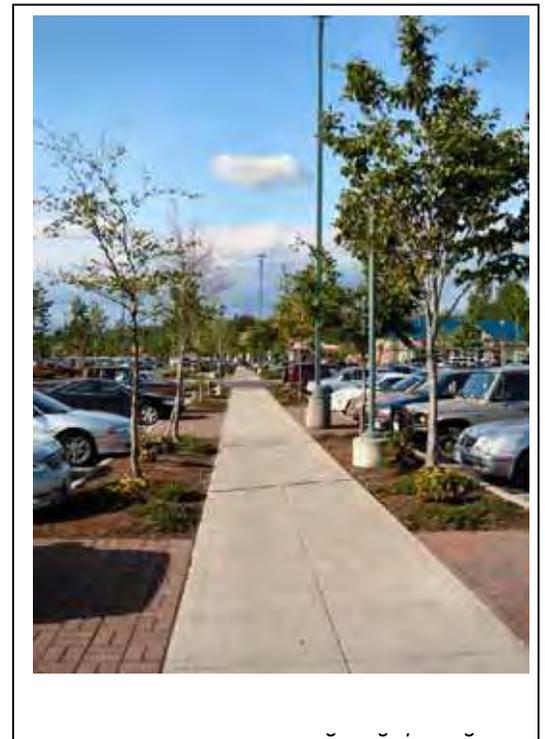
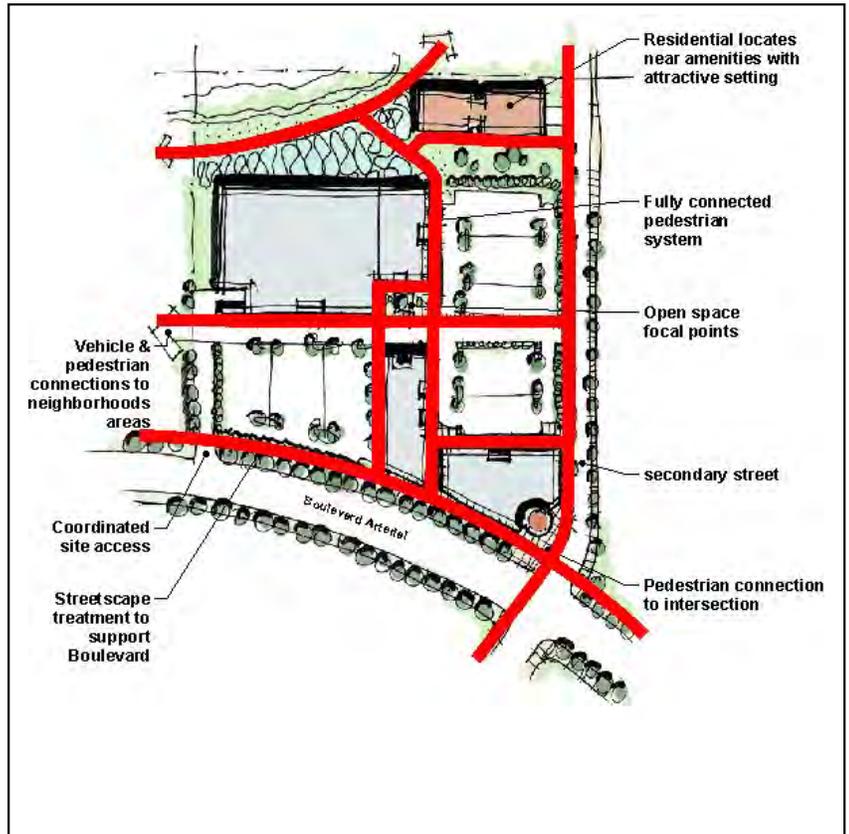
- To enhance pedestrian access to the street, adjacent uses, and adjacent sites, where desirable.
- To make it easier to walk between uses.
- To reduce vehicle trips within the district neighborhood.
- To promote pedestrian activity.
- To enhance pedestrian access through parking lots and between the street and uses.

Guidelines

- Provide convenient pedestrian access between the street, bus stops, buildings, parking areas, and open spaces. Internal pedestrian connections are particularly important on large sites where some uses may be placed away from a street.
- Design all buildings abutting a public sidewalk or major internal pathways to provide direct pedestrian access to the sidewalk or pathway.
- Provide interior pedestrian connections to adjacent properties containing similar uses or complementary uses. This is most applicable to large lots and where storefronts or other uses are set back away from the street. Where an existing connection is not desirable or possible due to the nature of development on the adjacent site, the applicant should provide an opportunity for a future pedestrian connection where such a connection is desirable and future redevelopment of the adjacent site is possible.
- Provide paved walkways through large parking lots. One walkway should be provided for every three parking aisles. Such access routes through parking areas should be separated from vehicular parking and travel lanes by use of contrasting paving material which may be raised above the vehicular pavement and by landscaping.

Special Considerations in TL 4, TL 6B and TL 4 7A

- TL 6B.: Develop a trail along the northern edge of the property to take advantage of the site's greenbelt setting. Provide a landscaped buffer area between the trail and any adjacent residential buildings to enhance the character of the trail and provide privacy to adjacent residents.



- f. TL 6B Enhance connections to TL 10A to the south, to ensure safe and convenient access for employees in TL 10A and the shopping district in TL 6B.
- g. TL 4 Provide for safe and convenient access between development in TL 4 (west of I-405) and the business park directly to the west in TL 10A.
- h. TL 7A Provide for safe and convenient public pedestrian access between NE 124th Street through the subject property to the CKC.

12. Pedestrian Plazas

Objectives

- To provide a variety of pedestrian-oriented areas to attract shoppers to commercial areas and enrich the pedestrian environment.
- To create gathering spaces for the community.
- To configure buildings and uses to encourage pedestrian activity and pedestrian focal points.

Guidelines

- a. Provide pedestrian plazas in conjunction with mixed-use development and non-residential uses.
- a.b. Publicly accessible space at the primary frontage and between buildings will extend the public realm while creating a transition between public and private spaces, and attract public use by being well-designed, interesting spaces that are integrated with the street environment. The spaces should be of sufficient size to allow for a variety of features, including pedestrian/multi-use paths, plazas, seating, public art and water features.
- b.c. Position plazas in visible locations on major streets, major internal circulation routes, close to bus stops, or where there are strong pedestrian flows on neighboring sidewalks. For large sites, development should be configured to create a focal plaza or plazas. Plazas should be no more than 3' above or below the adjacent sidewalk or internal pathway to enhance visibility and accessibility.
- e.d. Incorporate plenty of benches, steps, and ledges for seating. A combination of permanent and moveable seating is encouraged. Seating areas should be provided with views of amenities, landscaping elements, or people watching.
- d.e. Provide storefronts, street vendors, or other pedestrian-oriented uses, to the extent possible, around the perimeter of the plaza
- e.f. Provide landscaping elements that add color and seasonal interest. This can include trees, planting beds, potted plants, trellises, and hanging plants.



Figure 32. Good examples of pedestrian plazas. Notice the



Figure 33. An example of an attractive small plaza space between a sidewalk and a storefront

f.g. Incorporate pedestrian amenities, as described in Section 10.

g.h. Consider the solar orientation and the wind patterns in the design of the open space and choice of landscaping.

h.i. Provide transitional zones along building edges to allow for outdoor eating areas and a planted buffer.

13. Residential Open Space

Objectives

- To create useable space that is suitable for leisure or recreational activities for residents.
- To create open space that contributes to the residential setting.

Guidelines

- Incorporate common open space into multi-family residential uses. In the Totem Lake Business District, - Neighborhood, where very high density residential uses are allowed, the quality of the space in providing respite from the buildings on the site is more critical than the amount of space provided. In some developments, multiple smaller spaces may be more useful than one, larger space. Special recommendations for common open space:
 - Consider open space as a focal point of the residential development.
 - Where possible, open space should be large enough to provide functional leisure or recreational activity. For example, long narrow spaces rarely, if ever, can function as usable common space.
 - Open space should provide for a range of activities and age groups. Children's play areas in particular should be visible from dwelling units and positioned near pedestrian activity.
 - Residential units adjacent to the open space should have individual entrances to the space. Preferably, these units should include a small area of semi-private open space enclosed by low level landscaping or hedges (no taller than 42").
 - Open space should feature paths, seating, lighting, and other pedestrian amenities to make the area more functional and enjoyable. It should be oriented to receive sunlight, (preferably south).
 - Separate common space from ground floor windows, streets, service areas, and parking lots with landscaping and/or low-level fencing. However, care should be used to maintain visibility from dwelling units towards open space for safety.
- Provide private open space for multi-family residential units. For townhouses and other ground-based housing units, provide patios, decks, and/or landscaped front or rear yards adjacent to the units. For all other units, provide balconies large enough to allow for human activity.



Figure 34. Good examples of common open space, including street-level courtyards (left), a children's play area (top right), and a pedestrian corridor (lower right)

14. Parking Lots and Vehicular Circulation

Objectives

- To minimize the impact of parking facilities on the fronting street, pedestrian environment, and neighboring properties.
- To enhance pedestrian and vehicular safety.
- To maintain desired traffic flow on Totem Lake arterials.
- To promote shared parking
- To provide attractive and connected vehicular circulation routes.

Discussion

Parking lots can detract from the pedestrian and visual character of a commercial area. The adverse impacts of parking lots can be mitigated through sensitive design, location, and configuration. Large parking lots can be confusing unless vehicle and pedestrian circulation patterns are well organized and marked. The Totem Lake Business District Neighborhood Plan encourages shared parking between properties to reduce curb cuts, reduce congestion of cars turning in and out of parking lots and consolidating consumer trips between businesses.

Where not specifically prohibited, drive-through facilities for some uses such as fast food restaurants, pharmacies, or auto oriented uses may be appropriate if designed to minimize vehicle queuing along rights of way, blocking driveways or parking aisles, or impeding pedestrian movement. Aesthetically, drive-throughs should be located away from street frontages or screened as viewed from the right of way.

Guidelines

Driveways

- Minimize the number of curb cuts into a development, particularly off of arterials. To the extent possible, adjacent developments should share driveways.

Parking Lot Location and Design

- Locate vehicular parking areas to the side or rear of buildings, to the extent possible.
- Avoid parking layouts that visually dominate a development. Break up large parking lots into smaller ones.
- Take advantage of topography to hide parking underneath buildings.
- Provide a clear and well organized parking lot design. Space should be provided for pedestrians to walk safely in all parking lots.

Parking Lot Landscaping

- Integrate landscaping into parking lots to reduce their visual impact. Provide planting beds with a variety of trees, shrubs, and ground cover to provide visual relief, summer shade, and seasonal interest.

Parking Lot Screening

- Provide low level screening and perimeter landscaping where parking is adjacent to sidewalks in order to improve visual qualities and reduce clutter. While vertical elements such as trees, are encouraged to define the street edge,

all screening methods should maintain visibility at eye level between the street and parking area. For instance, hedges or walls should not be taller than 3 feet and trees should be trimmed to allow visibility between 3 and 8 feet above the ground.

- h. Provide extensive screening and landscaping between parking lots and residential uses and open spaces. A combination of a screen wall with a landscape buffer is preferred.

Vehicular Circulation

- j. Develop an efficient internal vehicular access system that minimizes conflicts with pedestrians and vehicular traffic. For TL 5, TL 6A, and TL 6B, see the “[Redevelopment Concept](#)” illustrations in the [Design Vision for the Totem Lake Business District section Introduction](#).
- k. Configure development to provide interior vehicular connections to adjacent uses, where desirable. Where current connections to adjacent uses are not feasible, but desirable in the future, configure development to provide the opportunity for a future connection, should the adjacent site be redeveloped.
- l. Avoid parking lot configurations with dead-end lanes.
- m. Configure internal access roads to look and function like public streets. This is most applicable to larger sites, such as those in TL 5 and TL 6B, where an internal vehicular circulation system is critical to access interior portions of the sites. The most desirable configuration would include on-street parking, street trees and sidewalks on both sides of the roadway.



Figure X.

Drive-Through Facilities

- n. Design drive-through windows to be oriented away from the street frontage and preferably not located between a building and the street. Where drive-through lanes face a street, avoid large featureless walls and provide sufficient landscaping to soften the visual impact of vehicle stacking areas for drive through windows. Locate driving lanes so as not to interfere with pedestrian or vehicular circulation.

15. Parking Garages

Objectives

- To mitigate the visual impacts of parking garages in the urban environment.

Guidelines

- Mitigate the intrusive qualities of parking garages. Along streets, pedestrian pathways and in pedestrian areas, ground-level commercial uses should be incorporated into parking structures. Extensive landscaping should be used to screen the parking garage near residential areas and in high visibility locations.
- Design and site parking garage entries to complement, not subordinate the pedestrian entry. If possible, locate the parking entry away from the primary street, to either the side or rear of the building.
- Use similar architectural forms, materials, and/or details to integrate the garage with the development.



- d. Locate parking structure service and storage functions away from the street edge and generally not visible from the street, ~~or sidewalks,~~ or the CKC.

16. Architectural Style

Objectives

- To improve the architectural design of commercial buildings in the business district.
- To provide architecture that fits into the context of the adjacent uses surrounding the business district.

Discussion

As there is no single predominate architectural style in the Totem Lake Business District~~Neighborhood~~, the guidelines contained in this document provide flexibility on the chosen styles (provided the architectural style, human scale, building details, and building materials and color standards in KZC Chapter 92 and these guidelines are met).

17. Architectural Scale

Objectives

- To encourage an architectural scale of development that is compatible with the vision for the districts within the Totem Lake Business District~~Neighborhood~~.
- To implement the planning concepts for the distinct design districts within the Totem Lake Business District~~Neighborhood~~.
- To add visual interest to buildings.

Discussion

The guidelines in this section describe a variety of techniques to give a comfortable human scale by providing building elements that help individuals relate to the building. “Architectural scale” means the size of a building relative to the buildings or elements around it. When the buildings in a ~~district~~neighborhood are about the same size and proportion, we say they are “in scale.” As both the vision and development regulations for the Totem Lake Business District~~Neighborhood~~ provide for much larger buildings than currently exist, special care must be taken to design buildings so they do not overpower the others. The exception to this rule is an important civic or cultural building that has a prominent role in the community.



Guidelines

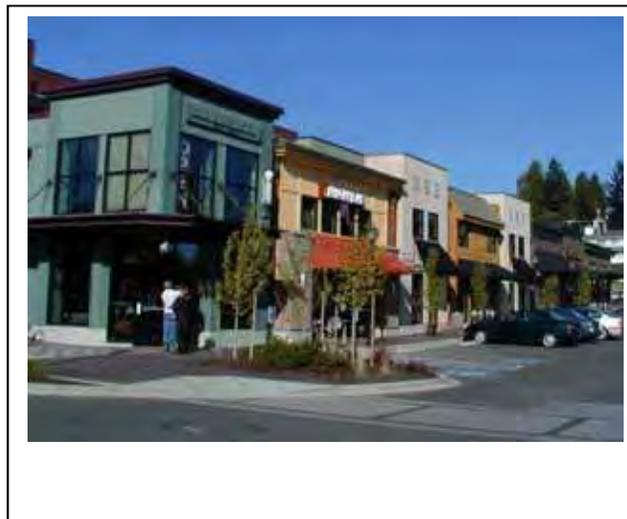
A combination of techniques is desirable to reduce the architectural scale of buildings. Specifically, these techniques are encouraged at intervals of no more than 70 feet for non-residential uses and 30 feet for residential uses. Office buildings are provided with greater flexibility. Alternatives will be considered provided they meet the objectives of the guidelines.

- a. Incorporate fenestration techniques that indicate the scale of the building. For example, the size, location, and number of windows in an urban setting create a sense of interest that relies on a subtle mixture of correct ratios, proportions, and patterns. This is particularly important on upper floors, where windows should be divided into units no larger than 35 square feet, with each window unit separated by a visible mullion or other element at least 6 inches wide. “Ribbon windows” (continuous horizontal bands of glass) or “window walls” (glass over the entire surface) do

little to indicate the scale of the building and are thus discouraged, except in special circumstances where they serve as an accent element.

Patterns of fenestration should also vary depending on whether the street is pedestrian- or automobile-oriented. A window pattern that is interesting from a car may be monotonous to a slow-moving pedestrian; likewise, a window pattern that is interesting to a pedestrian may seem chaotic from a fast-moving car. Thus, pedestrian oriented fenestration should allow for more complex arrangements and irregularity while automobile-oriented fenestration should have more gradual changes in pattern and larger and simpler window types. An optimum design goal would allow for varied treatment of window detailing with unifying features such as 18" to 24" sills, vertical modulation in structure, varied setbacks in elevation, and more highly ornamented upper-story windows.

- b. Encourage vertical modulation on multi-story buildings to add variety and to make large buildings appear to be an aggregation of smaller buildings. Vertical modulation may be particularly effective for tall buildings adjacent to a street, plaza, or residential area to provide compatible architectural scale and to minimize shade and shadow impacts. Vertical modulation is well-suited for residential development and sites with steep topography.
- c. Encourage a variety of horizontal building modulation techniques to reduce the architectural scale of the building and add visual interest. Horizontal building modulation is the horizontal articulation or division of an imposing building façade through setbacks, awnings, balconies, roof decks, eaves, and banding of contrasting materials. Elevations that are modulated with horizontal elements appear less massive than those with sheer, flat surfaces. Specifically:
- For single purpose retail buildings, use horizontal building modulation with roofline modulation and a change in building materials, as necessary to meet objectives of the guidelines from all perceived distances. This is particularly important for large scale retail buildings (over 40,000 square feet) or multi-tenant retail buildings placed adjacent to a parking lot where they can be viewed from relatively great distances.
 - Provide horizontal building modulation for residential uses based on individual unit size. Horizontal modulation is most effective when combined with roofline modulation and changes in color and/or building materials. The depth and width of the modulation should be sufficient to meet the objectives of the guidelines. Avoid repetitive modulation techniques, since they may not be effective when viewed from a distance. Larger residential buildings will require greater horizontal modulation techniques to provide appropriate architectural scale.
- d. Office buildings: Use design techniques to break up long continuous walls. A combination of horizontal building modulation, change in fenestration, and/or change in building materials should be used to accomplish this.
- e. Encourage a variety of roofline modulation techniques. This can include hipped or gabled rooflines and modulated flat rooflines. As a general rule, the larger the building or unbroken roofline, the bigger the modulation should be. In determining the appropriate roof type and amount of modulation, consider at what distance the building can be viewed. For example, a large commercial building adjacent to a parking lot is capable of being viewed from a relatively large distance. Consequently the roofline modulation techniques must be sufficient to provide an appropriate architectural scale that provides visual interest.



18. Human Scale

Objectives

- To encourage the use of building components that relate to the size of the human body
- To add visual interest to buildings.

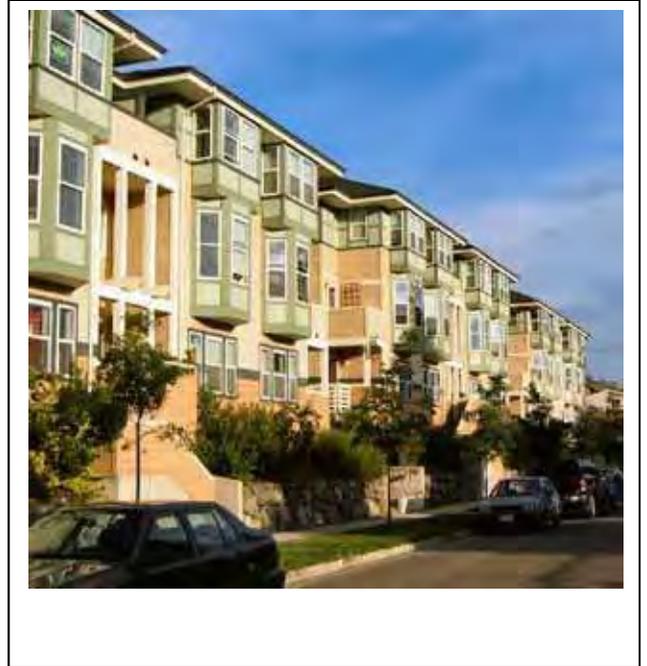
Discussion

The term “human scale” is generally used to indicate a building’s size relative to a person, but the actual size of a building or room is often not as important as its perceived size. A variety of design techniques may be used to give a space or structure the desired effect; for example, to make a room either more intimate or spacious, or a building either more or less imposing.

Special elements in a building facade create a distinct character in an urban context. A bay window suggests housing, while an arcade suggests a public walkway with retail frontage. Each element must be designed for an appropriate urban setting and for public or private use. A building should incorporate special features that enhance its character and surroundings. Such features give a building a better defined “human scale.”

Guidelines

- Encourage a combination of architectural building elements that lend the building a human scale. Examples include arcades, balconies, bay windows, roof decks, trellises, landscaping, awnings, cornices, friezes, art concepts, and courtyards. Window fenestration techniques described in Section 17 can also be effective in giving humans clues as the size of the building. Consider the distances from which buildings can be viewed (from the sidewalk, street, parking lot, open space, etc.).



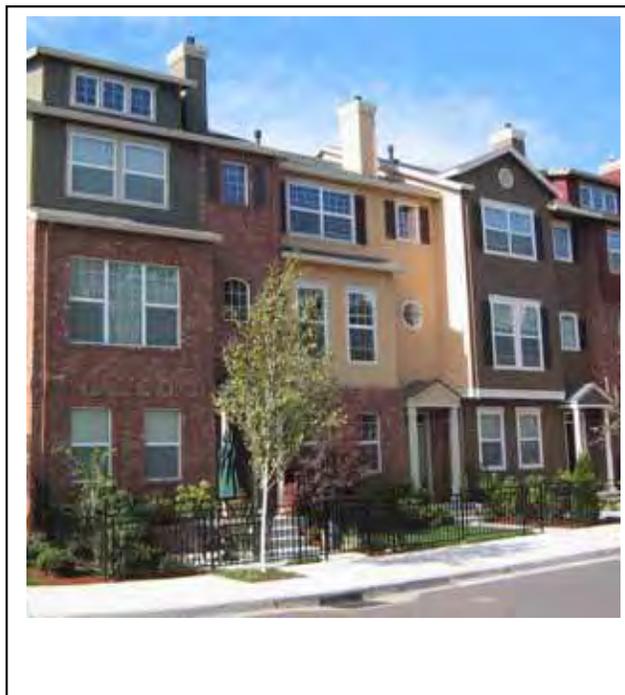
19. Building Details and Materials

Objectives

- To use building and site design details that add visual interest to buildings/sites at a pedestrian scale.
- To use a variety of quality building materials such as brick, stone, glass, timber, and metal, which are appropriate to the Pacific Northwest climate, and complementary to the desired visual character of the district.

Guidelines

- Encourage the integration of ornament and applied art with the structures and the site environment. For example, significant architectural features should not be hidden, nor should the urban context be overshadowed. Emphasis should be placed on highlighting building features such as doors, windows, eaves, and on materials such as wood siding and ornamental masonry. Ornament may take the form of traditional or contemporary elements. Original artwork or hand-crafted details should be considered in special areas. Ornament and applied art can be used to emphasize the edges and transition between public and private space, and between walls to ground, roof to sky, and architectural features to adjacent elements. Ornament may consist of raised surfaces, painted surfaces, ornamental or textured banding, changing of materials, or lighting.
- Use a variety of quality building materials such as brick, stone, timber, and metal, to add visual interest to the buildings and reduce their perceived scale. Masonry or other durable materials should be used near the ground level (first 2 feet above sidewalk or ground level).



20. Signs

Objectives

- To encourage the use of creative, well-crafted signs that contribute to the character of the district.

Discussion

Kirkland's Zoning Code regulates signs throughout the city in order to create a high-quality urban environment. Automobile-oriented signs typically found on commercial strips can be overpowering and obtrusive. Pedestrian signs are smaller and closer to viewers; thus, creative, well-crafted signs are more cost effective than large signs mounted high on poles. A balance between the needs of a high traffic corridor and pedestrians should be considered in the design of signs. Signs should be an integral part of a building's façade or act as a center identification for the passing motorist to a commercial center. The location, architectural style, and mounting of signs should conform to a building's architecture and not cover up or conflict with its prominent architectural features. A sign's design and mounting should be appropriate for the setting.

Guidelines

- a. Provide pedestrian oriented signs on all commercial facades where adjacent to a sidewalk or walkway. This includes signs located within 15' of the ground plane, such as "blade" signs which hang below canopies. Small signs located on canopies or awnings are also effective along building facades at the street. Signs with quality graphics and a high level of craftsmanship are important in attracting customers. Sculpted signs and signs that incorporate artwork add interest.
- b. External lighting is preferred. If internal lit cabinet signs are used, darker background with lighter lettering is more aesthetically pleasing. Neon signs are appropriate when integrated with the building's architecture.
- c. Ground-mounted signs should feature a substantial base and be integrated with the landscaping and other site features.
- d. Mounting supports should reflect the materials and design character of the building or site elements or both. Too much variety, too much uniformity though unified by common design elements, signs can still express the individual character of businesses.
- e. Master-planned, larger commercial centers are encouraged to combine signage for the whole complex that complements the architectural design of the center and oriented to automobile traffic.

21. Service Areas

Objectives

- To provide essential service areas without adversely impacting the quality of development.
- To locate and design site service and storage areas to promote ease of use, safety, and visual cohesion.

Guidelines

- a. Locate and design service and storage areas to minimize impacts on the pedestrian environment and adjacent uses. Service elements should generally be concentrated and located where they are accessible to service vehicles and convenient for tenant use.
- b. The design of service enclosures should be compatible with the design of adjacent buildings. This may be accomplished by the use of similar building materials, details, and architectural styles. Such enclosures should be made of masonry, ornamental metal, heavy wood timber, or other durable materials.
- c. Roof-mounted mechanical equipment should be located so as not to be visible from the street, public open space, parking areas, or from the ground level of adjacent properties. Screening features should blend with the architectural character of the building. Equipment screening and preferred location should be included in the early design of a building.

22. Visual Quality of Landscapes

Objectives

- To enhance the visual quality of the urban environment.

Discussion

The relationship between landscaping and architecture is symbiotic; plant materials add to a building's richness, while the building points to the architectural qualities of the landscaping. Foliage can soften the hard edges and improve the visual quality of the urban environment. Landscaping treatment in the urban environment can be categorized as a pedestrian/auto, pedestrian, or building landscape.

The pedestrian/auto oriented landscape applies to where the pedestrian and auto are in close proximity. Raised planting strips can be used to protect the pedestrian from high-speed and high-volume traffic. Street trees help create a hospitable environment for both the pedestrian and the driver by reducing scale, providing shade and seasonal variety, and mitigating noise impacts.

The pedestrian landscape offers variety at the ground level through the use of shrubs, ground cover, and trees. Pedestrian circulation, complete with entry and resting points, should be emphasized. If used effectively, plant materials can give the pedestrian visual cues for moving through the urban environment. Plant materials that provide variety in texture, color, fragrance, and shape are especially desirable.

The Building Landscape. Landscaping around urban buildings, particularly buildings with blank walls, can reduce scale and add diversity through pattern, color, and form.

Examples of how landscaping is used to soften and enhance the visual quality of the urban environment include:

- Dense screening of parking lots;
- Tall cylindrical trees to mark an entry;
- Continuous street tree plantings to protect pedestrians;
- Several clusters of dense trees along long building facades;
- Cluster plantings at focal points;
- Parking with trees and shrubs planted internally as well as on the perimeter.

Guidelines

- Consider the purpose and context of the proposed landscaping. The pedestrian/auto oriented landscape requires strong plantings of a structural nature to act as buffers or screens. The pedestrian landscape should emphasize the subtle characteristics of the plant materials. The building landscape should use landscaping that complements the building's favorable qualities and screens its faults while not blocking views of the business or signage.

Other considerations:

- Encourage a colorful mix of drought tolerant and low maintenance trees, shrubs and perennials. Except in special circumstances, ivy and grass lawn should be avoided.
- Take advantage of on-site topography to hide parking and enhance views.
- Use wooded slopes as a natural site amenity and to screen unwanted views, where applicable.

23. Territorial Views

Objectives

- To encourage development to take advantage of views, while minimizing impacts to public views.
- To configure buildings and site features to enhance views from surrounding properties.

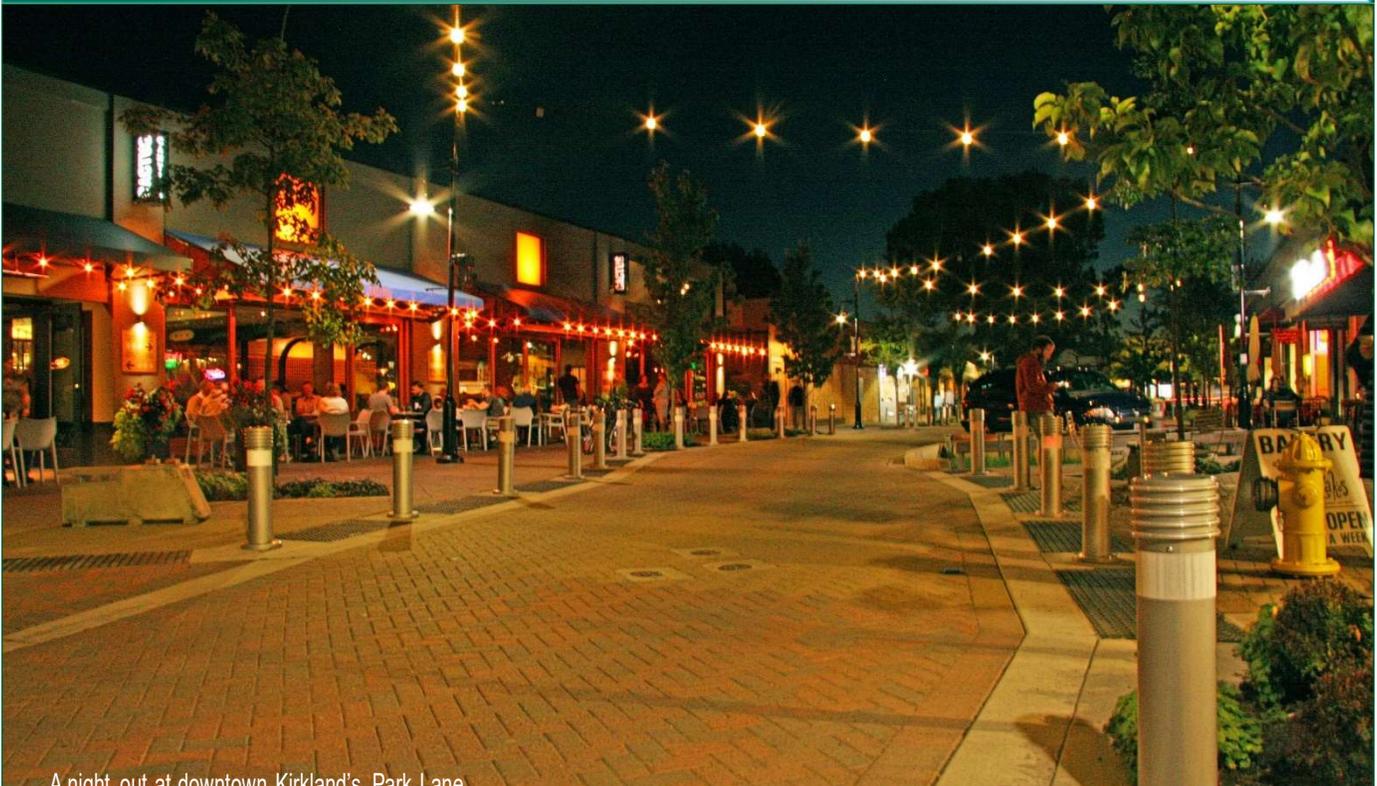
Guideline

- a. Encourage rooflines to roughly follow the slope of the existing terrain. Parking garages should be terraced into slopes to minimize building bulk, wherever possible. Buildings are encouraged to step down hillsides.

The City of Kirkland

Design Guidelines

For Pedestrian-Oriented Business Districts



A night out at downtown Kirkland's Park Lane

Adopted by the City Council pursuant to
 Kirkland Municipal Code Section 3.30.040.
 Dated August 3, 2004.
 Updated: January 16, 2018, O-4636 & R-5292.
 Attest:



Amy Walen,
 Mayor

Eric Shields
 Director,
 Planning & Community
 Development

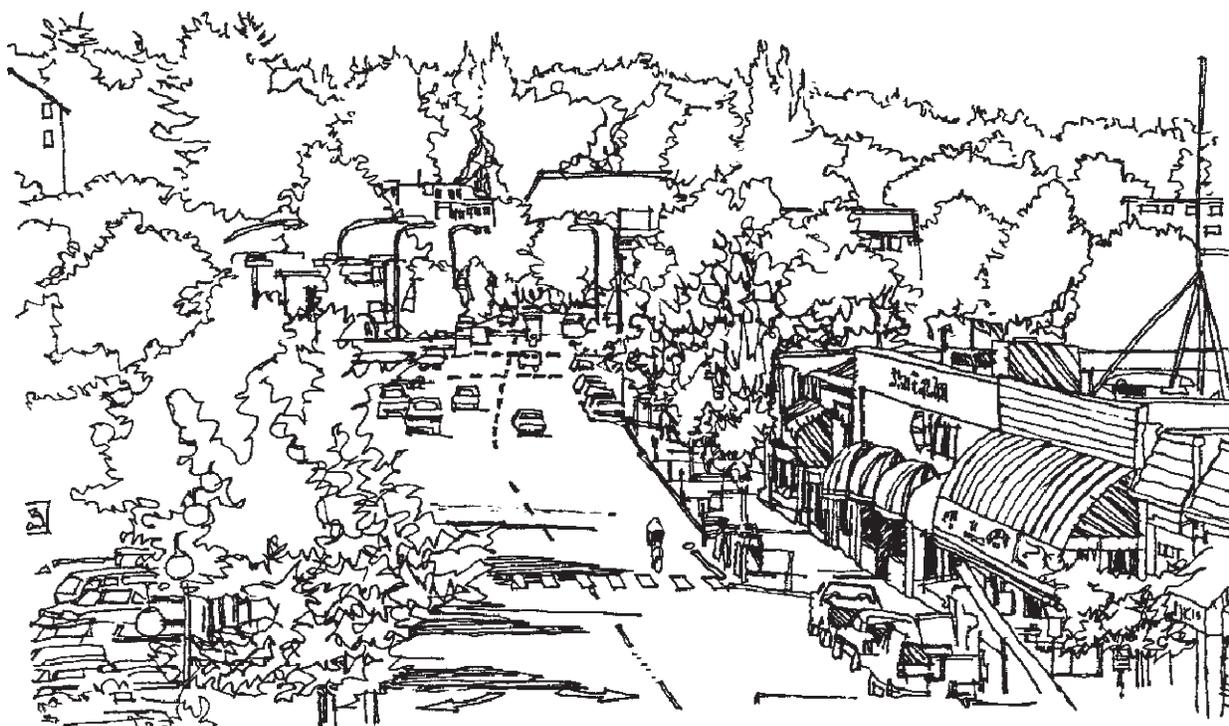
Introduction

This document sets forth a series of Design Guidelines, adopted by Section 3.30 of the Kirkland Municipal Code, that will be used by the City in the ~~in the design review process.~~ For ~~Board-Design~~ Board Review (BDBR) process, ~~the Design Review Board will use these guidelines in association with the Design Regulations of the Kirkland Zoning Code. To the extent that the standards of the Design Guidelines or Design Regulations address the same issue but are not entirely consistent or contain different levels of specificity, the Design Review Board will determine which standard results in superior design.~~ For Administrative Design Review (ADR), the Planning Official will use these guidelines when necessary to interpret the Design Regulations. They are also intended to assist project developers and their architects by providing graphic examples of the intent of the City's guidelines and regulations.

Most of the concepts presented in the Design Guidelines are applicable to any pedestrian-oriented business district.* “Special Considerations” have been added, such as for Downtown Kirkland, to illustrate how unique characteristics of that pedestrian-oriented business district relate to the Guideline.

The Design Guidelines do not set a particular style of architecture or design theme. Rather, they will establish a greater sense of -quality, unity, and conformance with Kirkland's physical assets and civic role.

The Design Guidelines will work with improvements to streets and parks and the development of new public facilities to create a dynamic setting for civic activities and private development. It is important to note that these Guidelines are not intended to slow or restrict development, but rather to add consistency and predictability to the permit review process.



** The guidelines also apply to residential development in the Central Business District (CBD), the Juanita Business District (JBD), the North Rose Hill Business District, the Market Street Corridor (MSC), the Finn Hill Neighborhood Center (FHNC), the Houghton/Everest Neighborhood Center*

- Views of Water
- Culverted Creeks

Purpose of the Design Guidelines for the Totem Lake Business District Core

The Kirkland City Council adopted an updated plan for the Totem Lake Business district in 2015~~a new neighborhood plan for Totem Lake in early 2002~~. The vision set forth in the Plan for the Totem Lake Business District Core is of a dense, compact community, with a mix of business, commercial and residential uses and a high level of transit and pedestrian activity.

The Plan establishes key overall design principles for the Business District Core, as well as specific design objectives for the site containing the Village at Totem Lake~~Totem Lake Mall~~ (TL 2), Evergreen Health~~Hospital~~ campus (TL 3), and the mixed-use area west of the campus (TL 1). Design objectives promoted in the plan for the Business District Core include:

- Accommodate high density, transit-oriented development, consistent with the district's role as ~~position in~~ an Urban Center.
- Ensure that public and private development contribute to a lively and inviting character in the Business District Core.
- Reinforce the character of the Business District Core through public investments.
- Produce buildings that exhibit high quality design, incorporate pedestrian features and amenities and display elements of both continuity and individuality.
- Provide public spaces that are gathering ~~places~~focal points for the community
- Provide visual and functional connections between adjacent developments through landscaping, public spaces and pedestrian connections.

Design considerations specific to the three subareas within the district include:

Mixed-Use Area (TL 1)

- Break up the mass of larger buildings through techniques such as towers over podiums, to create a varied building footprint and the perception of a smaller overall building mass.
- Incorporate features that create distinctive roof forms, to contribute to a skyline that is visually interesting throughout the district.

- Ensure appropriate transitions from lower density uses north of the Business District Core through providing residentially scaled façades and centered building masses in development along NE 132nd Street.

Retail Center (TL 2)

The Totem Lake Business District Plan direction for the TL2 area is to support its continued growth as a vibrant, intensive retail and mixed-use center for the Kirkland community and surrounding region. These guidelines are intended to support~~promote~~ the vision of this area as a "village-like" community gathering place, with high-quality urban and architectural design in redevelopment. To provide for flexibility and increased development potential, while ensuring coordinated development and design integrity over time, redevelopment should occur within the context of an overall site development or Master Plan for the entire property.

Evergreen Health~~Hospital~~ Medical Center Campus (TL 3)

The Totem Lake Business District Plan acknowledges the important role the hospital plays in the Kirkland community, and supports growth on the campus to strengthen this role. Design objectives stated in the Plan for the Evergreen Hospital campus are consistent with those expressed in the Master Plan approved for the site:

- Taller buildings should be located toward the center of the site and designed to minimize shadowing and transition impacts on residential areas.
- Public access to usable green spaces on the campus can help to offset the impacts of taller buildings on the site.
- Ensure campus edges are compatible with neighboring uses.
- Enhance and improve pedestrian access with the campus and to surrounding uses, particularly the transit center and to TL 2.

The approved Master Plan for the hospital campus includes additional, unique design guidelines that apply to institutional development in a campus environment:

- *Respond to Physical Environment:* New buildings should be attractive as well as functional additions to the campus.
- *Enhance the Skyline:* The upper portion of buildings should be designed to promote visual interest and variety on the skyline, except where building function dictates uninterrupted vertical mass.





Guideline

A sidewalk should support a variety and concentration of activity yet avoid overcrowding and congestion. The average sidewalk width should be between 10' and 18'. New buildings on pedestrian-oriented streets should be set back a sufficient distance to provide at least 10' of sidewalk. If outdoor dining, seating, vending, or displays are desired, an additional setback is necessary.

Special Consideration for Downtown Kirkland

Most of the business core of Kirkland is already developed with fairly narrow sidewalks. New development should provide sidewalks at the recommended width. Providing wider sidewalks throughout downtown is a long-term endeavor.

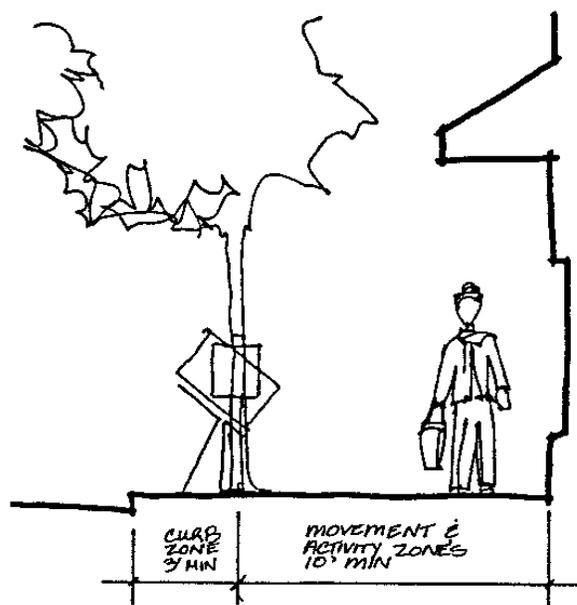
Special Consideration for Juanita Business District

A concentrated, organized, retail-oriented core with a unified pedestrian circulation network is a goal of the Juanita Business District. The pedestrian system will also serve to connect the perimeter of the district to the core.

Special Consideration for the Totem Lake Business District Core

New development in [the Business District Core #2](#) should provide sidewalks at the recommended width, to contribute to the pedestrian-orientation of new development. Public gathering places, such as pedestrian-oriented plazas linked to the sidewalk, should be encouraged.

Sidewalk Width – Curb Zone



Issue

The curb zone contains parking meters, garbage cans, newspaper stands, street signs, light poles, mail boxes, phone booths, bus stops, and trees. The curb zone is also a buffer between vehicular traffic and pedestrians.

Discussion

The curb zone may be integrated into the sidewalk design in a number of ways.

- *A curb zone with parallel parking.* Getting in and out of parked cars requires 2'-6"; so the curb zone width should be between 4'-6" and 5'-6".
- *A curb zone without parallel parking.* Space is not needed to park cars; the curb zone width should be between 3' and 4'.
- *A curb zone with street furniture clustered in sidewalk bulbs along the street; parking is allotted in the pockets between the bulbs.* Clusters of street elements – benches, newspaper stands, covered bus stops – require a sidewalk width of about 8' to 12'.

The curb zone may be visually separated from the movement zone by changes in color or surface material. Street furniture and other elements may be grouped and unified by color and shape to give the street a less cluttered appearance.

The design of the curb zone and street elements provides an opportunity for Kirkland to develop a visual identity that differs from street to street yet is still characteristic of Kirkland.

Special Considerations for the Totem Lake Business District Core

Public spaces, such as landscaped and/or furnished plazas and courtyards should be incorporated into the development, and be visible and accessible from either a public sidewalk or pedestrian connection. Primary pedestrian access points to retail development in TL 2 along 120th Avenue NE, as well as along NE 128th Street in TL 1A and the planned street grid in TL 1B where pedestrians and cyclists travel between transit and the district's core, may be especially effective locations for public plazas.

Open spaces are especially important in these areas TL 1, where the built environment may be dense. Well-designed publicly accessible space at the primary frontage and between buildings will extend the public realm while creating a transition between public and private spaces, and attract public use by being well-designed, interesting spaces that are integrated with the street environment. The spaces should be of sufficient size to allow for a variety of features, including pedestrian/multi-use paths, plazas, seating, public art and water features, and appropriate landscaping. open spaces in front of and between buildings, visually linked with the open spaces of adjacent developments, will help to provide relief for the pedestrian.

A key opportunity to create a public gathering space and to reduce circulation barriers between the lower commercial area and the office, residential and transit areas uphill, exists within the 120th Avenue NE right of way and the southwestern corner of the Evergreen Health campus (TL 3). A linear, terraced landscaped “park” designed to follow the topography in this location would help visitors, residents and employees to negotiate the grade, gather and rest. The park would ease and encourage movement between uses in the district, and provide an attractive landmark within its core.

Pedestrian Connections

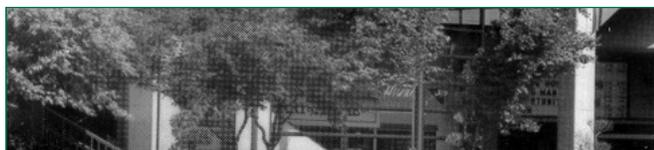
Issue

The ability to walk directly into a commercial center from the public sidewalk, the Cross Kirkland Corridor and Eastside Rail Corridor, or a bus stop is essential to both pedestrian and vehicular safety.

Discussion

Well defined, direct pedestrian connections from the building to the public sidewalk are not always available in commercial centers. The connection between the internal pedestrian system on the site and the public sidewalk is often interrupted by landscaping or an automobile driveway.

Properly located landscaping can be used along with special paving to help define pedestrian links through the site



Guideline

Commercial developments should have well defined, safe pedestrian walkways that minimize distances from the public sidewalk, the Cross Kirkland Corridor and Eastside Rail Corridor, and transit facilities to the internal pedestrian system and building entrances.



Public Improvements and Site Features

Introduction

Site features and pedestrian amenities such as lighting, benches, paving, waste receptacles, and other site elements are an important aspect of a pedestrian-oriented business district's character. If these features are design-coordinated and high quality, they can help to unify and upgrade the district's visual character. Development of a master plan for public spaces can provide a coordinated approach to their installation throughout the district.

The guidelines in this section apply primarily to elements associated with street right-of-ways, public parks, and required *major pedestrian pathways*. Although the standards do not apply to private property, except where a *major pedestrian pathway* is required, property owners are encouraged to utilize the standards in private development where they are appropriate. However, there may be cases where different site features, such as light fixtures and benches, should be selected to complement the architectural design of the individual site



Pathway Width

Issue

Pathways must be sufficiently wide to handle projected pedestrian traffic. A pathway that is too narrow will have maintenance problems at its edges. A pathway that is too wide is unnecessarily costly and a poor use of space.

Discussion

A pedestrian path of 10' to 12' can accommodate groups of persons walking four abreast or two couples passing each other.

A path near a major park feature or special facility like a transit center should be at least 12' wide. An 8' path will accommodate pedestrian traffic of less than 1,000 persons per hour.

Empirical Comparison:

- Green Lake path = 8'
- Burke-Gilman Path = 8'
- Typical sidewalk = 8' to 14'

Guideline

Design all major pedestrian pathways to be at least 8' wide.

Other pathways with less activity can be 6' wide.

Special Considerations for Juanita Business District

Through-site connections from street to street are a desirable pedestrian amenity in Land Use Area JBD-1.

The goal of these pedestrian connections will be to knit the individual developments into a more cohesive whole, providing convenient pedestrian mobility throughout even if the parcels are developed individually.

Special Consideration for North Rose Hill Business District

Buildings in the NRHBD will be setback at least ten feet from the sidewalk. Landscaping and entry features will be located within this setback yard. Therefore, the sidewalk can be somewhat narrower than on a pedestrian oriented street.

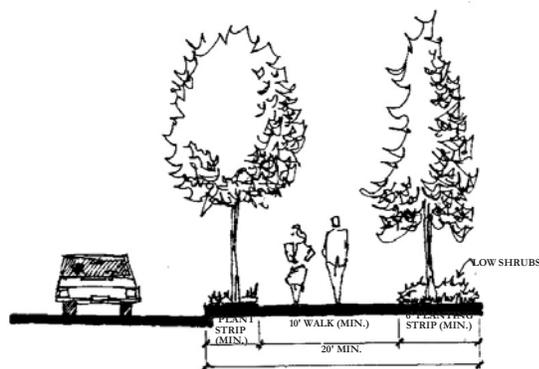
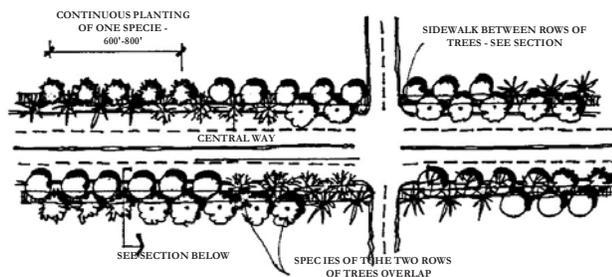
Special Consideration for Houghton/Everest Neighborhood Center

Through block pedestrian connections and connections to the Cross Kirkland Corridor are important features that will help to provide pedestrian access throughout the center.

Special Considerations for the Totem Lake Business District Core

Through-site connections from street to street, between the upper and lower portions of TL 2, and within TL 2 are needed to provide convenient pedestrian mobility, and to contribute to the village-like character desired for TL 2. Pedestrian connections to surrounding related uses, such as the hospital campus and transit center should also be provided.

Within TL 1, buildings should be set back at least ten feet from the sidewalk. Landscaping, ~~and~~ entry features and publicly accessible spaces described in the Pedestrian-Oriented Plaza section should be located within this setback yard, ~~allowing the sidewalk to be somewhat narrower than on a pedestrian oriented street.~~



Proposal for a distinctive, double-row tree planting of street trees on Central Way.

Street trees in the business district should be upgraded with varieties that will not block views of businesses or the lake.

Some preliminary ideas for a street tree planting plan are:

98th Avenue NE: Limb up existing maples and add flowering pear trees (flowers and good fall color) along the curb.

Juanita Drive: Choose street trees that will screen large buildings but still allow views to the lake (flowering pears for example).

97th Avenue NE/120th Place NE: Plant trees to screen parking lots and service entrances. Possibilities are zelkova (elm-like with good fall color) or flowering pears.

Special Considerations for the Market Street Corridor

A consistent street tree plan should be used to add character to the Corridor. The landscape strip on the east side of Market Street adds interest and provides a more secure pedestrian environment. Additional street trees should be considered on the west side of Market Street in order to provide a similar environment.

Special considerations for North Rose Hill Business District

Feature a diverse planting of street trees that take into account width of landscape strip, location of overhead utility lines, and maintenance requirements.

Some preliminary ideas for a street tree planting plan are:

NE 116th Street: Add street trees that will buffer the pedestrian corridor from traffic while providing some visual access to adjacent businesses. (Quercus rubra (red oak), Tilia cordata 'Greenspire' (littleleaf linden), Zelkova serrata 'Village Green' for example).

124th Avenue NE: Choose street trees that will buffer the pedestrian but still allow some visual access to adjoining businesses (Carpinus japonicus (Japanese hornbeam), Cercidiphyllum japonicum (Katsura), Fraxinus pennsylvanica 'Summit' (Summit ash) for example).

Slater Avenue NE: Add trees with flowers and good fall colors as a transition to the residential portion of the neighborhood (Malus sp. (flowering crab), Styax japonicus

(Japanese snowbell), Crataegus phaenopyrum (Washington hawthorn), Prunus padus 'Summer Glow' (bird cherry- red leaves) for example).

Special Considerations for the Totem Lake Business District Core

Street trees within this area should be selected to achieve the varying objectives of the district. Existing high-quality trees should be retained, and opportunities for increasing diversity in species should be sought to ensure long-term resiliency throughout the district.

Some preliminary ideas for a street tree planting plan are:

Totem Lake Boulevard: South of NE 128th Street, trees should be planted that balance the goals of creating a "green corridor" along the boulevard, providing a safe and inviting pedestrian experience and enabling visibility of the site's businesses to the freeway traveler. Smaller trees planted at frequent intervals anchored by larger, "boulevard" trees at primary site entrances or gateways would achieve these objectives. As an alternative or additional component, groupings of trees planted behind a meandering sidewalk may also be effective.

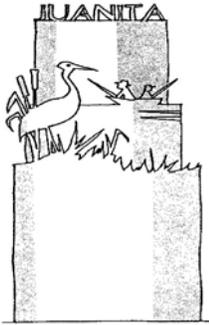
North of NE 128th Street to NE 132nd Street, plantings should be complementary-unified with those used along Totem Lake Boulevard to the south. Alternatively, a different tree species may be used in transition areas of the Totem Lake Circulator.

120th Avenue NE: South of NE 128th Street, choose

street trees that will emphasize the pedestrian connection between the upper and lower mall, such as the use of larger trees at crossings and major points of entry.

Choose spacing and varieties to create a plaza-like character to encourage pedestrian activity. Trees in planters and colorful flower beds will soften the area for pedestrians but allow visual access to adjoining businesses.

- A trellis incorporating landscaping. A trellis or arbor is adaptable to space constraints.
- Similar artwork such as a different animal or bird sculpture at each entry.



Guideline

Construct entry gateway features at locations noted in the Comprehensive Plan. Gateways may be constructed in conjunction with commercial development. Emphasis should be placed on framing the view into the district.

Special Consideration for Downtown Kirkland

The transit center is another “gateway” experience. The center should be a focal feature that provides comfort and amenities for transit users. Some form of shelter with a strong architectural identity should be pursued.

Special Consideration for Juanita Business District

The entry features should be “identity-giving elements” that reflect the business district and Juanita Bay. If successful they can become an identifying symbol or logo for the district and an attraction in themselves.

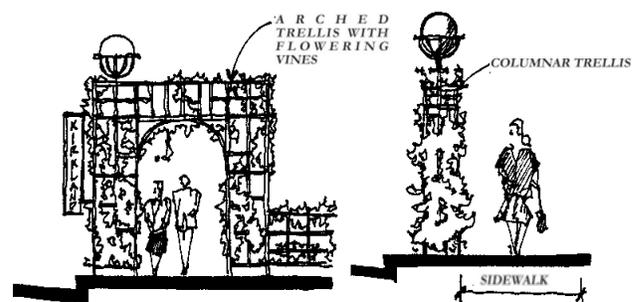
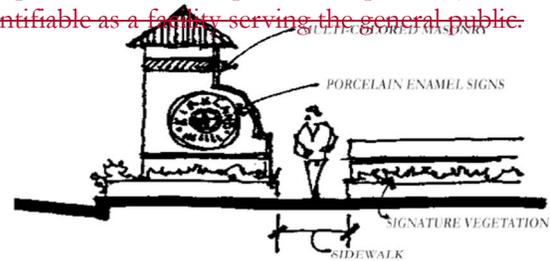
Special Consideration for North Rose Hill Business District

Use public art and private efforts to establish gateway features that strengthen the character and identity of the neighborhood. Use landscaping, signs, structures or other features that identify the neighborhood.

At the southwest corner of NE 116th Street and 124th Avenue NE a neighborhood gateway feature such as open space or plaza with signage should be integrated with a pedestrian connection linking Slater and NE 116th Street. In the alternative, a corner land mark consisting of a combination of open space and architectural building design features should be provided to identify the business district.

Special Considerations for the Totem Lake Business District Core

The Transit Center on the hospital campus ~~is should be~~ a “landmark” feature for both the Totem Lake Business District Core and the hospital campus, providing a focal point for residents, employees and visitors. Wayfinding measures undertaken within the district should use a coordinated system of A combination of signs and symbols linking the transit center to the pedestrian connection along NE 128th Street, the Bus Rapid Transit station flyer stop and the Park and Ride should be provided. Design of the transit center should be compatible with campus development yet be clearly identifiable as a facility serving the general public.



A prominent entry to the district exists at the intersection of NE 128th Street and Totem Lake Boulevard, where vehicles and pedestrians arrive from the crossing over I-405. Entry features provided in this area should contribute to the identity associated with the Business District Core.

Public art and private efforts can be used to establish gateway features to strengthen the character and identity of the Business District Core and the remainder of the district neighborhood. At the northern entry to the Business District Core at 120th Avenue NE and NE 132nd Street, a neighborhood entry sign or other identifying neighborhood feature should be provided. ~~Another important entry point identified in the neighborhood plan is along Totem Lake Boulevard, just east of 120th Avenue NE. A feature providing a sense of entry into the Business District Core at this location would be appropriate.~~

Public Art

Issue

Art begins with the perceptions and expressive talents of individual artists. “Public art” applies that expression to the public realm either by its location in a public setting or by its emphasis on subjects relevant to the larger community. Public art contributes to the unique character, history, and sense of place of a community.

Discussion

Public art is more than merely urban decoration; it can play an integral role in civic revitalization. Public art can make us more aware of our surroundings; reinforce the design character of our streets, parks, and buildings; commemorate special events; and serve as a catalyst for public activity and civic pride. At its best, art opens our eyes to new perceptions and helps us understand who we are and what is special about our community.

Public art is generally most effective when it is integrated with larger civic improvement efforts. Opportunities for art can be identified earlier and funding can be used more effectively. For example, emblems, lighting, pavement decorations, and decorative pedestrian furniture can be incorporated as part of a street improvement project at little cost to the total project such as in Seattle’s Third Avenue transit corridor, Port Angeles’s Maritime Flags, and Portland’s Transit Mall.

The involvement of an artist in the design of a park, fountain, street lighting, or signs can add a special quality that has more impact than if the artwork and the functional element were decorated separately. The famous art nouveau detailing on Paris’s metro stations is a good example.

Guideline

Kirkland should continue its tradition of encouraging public art pieces.



Parking Lot Location and Design

Introduction

In pedestrian-oriented business districts, improperly located and poorly designed parking lots can destroy the ambiance and qualities that attract people to the district in the first place. This section contains guidelines to direct development of parking facilities. The number of required stalls is specified in the Kirkland Zoning Code. The guidelines in this section deal with:

- Parking lot location – Parking in front of buildings is discouraged, and combined lots that serve more than one business or use are encouraged.

Natural Features

Introduction

General

An important aspect of a pedestrian-oriented business district is its physical setting. Natural features of a place are key to residents' and visitors' perception. This section lays out guidelines which serve to merge the design of structures and places with the natural environment. It discusses concepts behind new landscaping as well as the maintenance and protection of existing natural features.

Special Considerations for Downtown Kirkland

A primary goal stated in the Downtown Plan's Vision Statement is to "clarify Downtown's natural physical setting." Besides its excellent waterfront, Downtown Kirkland's most important natural feature is its bowl-shaped topography which provides views down from the heights and views from the downtown of the wooded hillsides surrounding the district. The valley topography also helps to define the downtown's edges and facilitates the transition from largely commercial activities in the valley floor to the mostly residential areas in the uplands. Although Peter Kirk Park is a man-made open space, it too provides a naturalizing function.

Special Considerations for Juanita Business District

The underlying goal of redevelopment in the business district is to create a neighborhood-scale, pedestrian district which takes advantage of the amenities offered by Juanita Bay.

Special Considerations for the Totem Lake Business District Core

An important goal in the Totem Lake Business District Plan is to establish a green corridor extending in an east/west direction across the ~~district~~ neighborhood. Portions of the green corridor follow Totem Lake Boulevard, along the western boundary of TL 2. Properties abutting the designated greenbelt should be landscaped with materials that complement the natural areas of the green corridor where possible.

Visual Quality of Landscapes

Issue

The relationship between landscaping and architecture is symbiotic; plant materials add to a building's richness, while the building points to the architectural qualities of the landscaping.

Discussion

Foliage can soften the hard edges and improve the visual quality of the urban environment. Landscaping treatment in the urban environment can be categorized as a *pedestrian/ auto, pedestrian, or building landscape*.



The Pedestrian/Auto Landscape applies to where the pedestrian and auto are in close proximity. Raised planting strips can be used to protect the pedestrian from high-speed and high-volume traffic. Street trees help create a hospitable environment for both the pedestrian and the driver by reducing scale, providing shade and seasonal variety, and mitigating noise impacts.

The Pedestrian Landscape offers variety at the ground level through the use of shrubs, ground cover, and trees. Pedestrian circulation, complete with entry and resting points, should be emphasized. If used effectively, plant materials can give the pedestrian visual cues for moving through the urban environment. Plant materials that provide variety in texture, color, fragrance, and shape are especially desirable.

The Building Landscape. Landscaping around urban buildings – particularly buildings with blank walls – can reduce scale and add diversity through pattern, color, and form.

Examples of how landscaping is used to soften and enhance the visual quality of the urban environment include:

- Dense screening of parking lots;
- Tall cylindrical trees to mark an entry;

- Continuous street tree plantings to protect pedestrians;
- Several clusters of dense trees along long building facades;
- Cluster plantings at focal points;
- Parking with trees and shrubs planted internally as well as on the perimeter.

Guidelines

The placement and amount of landscaping for new and existing development should be mandated through design standards. Special consideration should be given to the purpose and context of the proposed landscaping. The pedestrian/auto landscape requires strong plantings of a

structural nature to act as buffers or screens.

The pedestrian landscape should emphasize the subtle characteristics of the plant materials. The building landscape should use landscaping that complements the building's favorable qualities and screens its faults.

Along the Cross Kirkland Corridor and Eastside Rail Corridor, landscape design should screen where necessary, but generally soften the edge between the public and private space to integrate and complement corridor functions.

Special Consideration for North Rose Hill Business District

A dense landscape buffer should be utilized to provide a transition separating commercial uses from adjoining single family or multi-family residential uses.

Special Consideration for the Totem Lake Business District Core

Within TL 1, special landscaping elements such as gateways,

arches, fountains and sculptures should be incorporated, in order to create a lively streetscape and provide visual interest along the street edge. Where possible, existing mature landscaping should be retained and incorporated into new development to soften the impact of increased site coverage and preserve the green character of the area.

Protection and Enhancement of Wooded Slopes

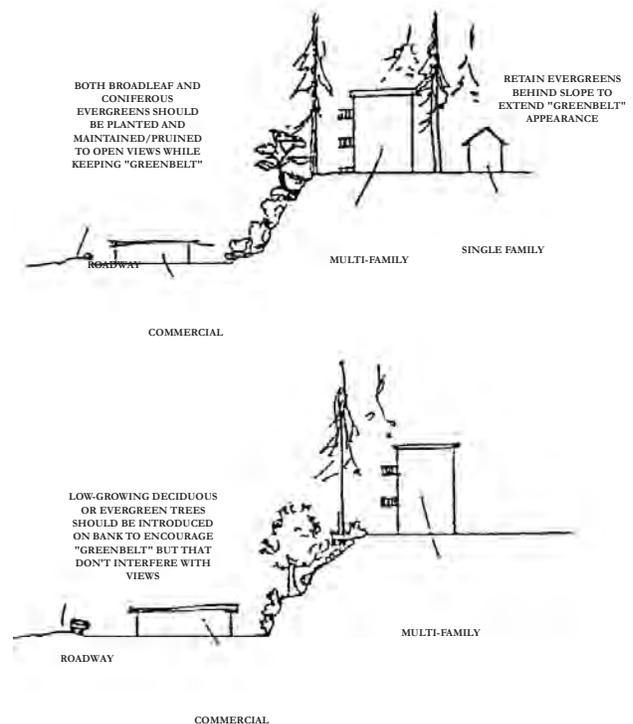
Issue

Topography provides opportunities for natural screening that maintains views.

Discussion

New plantings on wooded slopes should be selected for their slender, open growth pattern. Limbing-up and thinning-out branches should also be allowed to maintain views while keeping the character of the wooded hillsides. Weed species should be removed and replaced with appropriate native species. Wooded slopes can:

- Reduce visual impacts of the urban environment.
- Separate uses by providing a transition zone.
- Mitigate urban noise and air pollution for upland uses.
- Provide wildlife habitat.



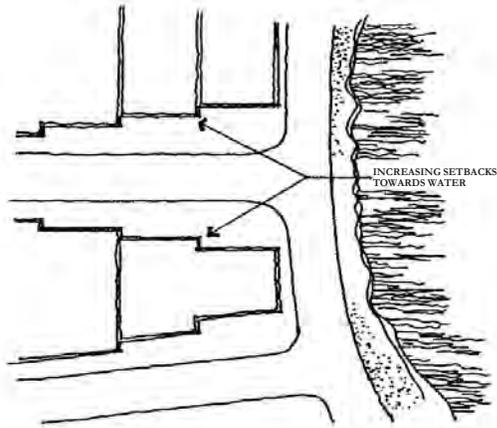
Guidelines

Vegetation on slopes should be preserved and maintained as a buffer using native vegetation wherever possible.

New multifamily and single-family residential developments on slopes should be required to retain about 30 percent of the site in wooded open space and inventoried significant trees. Tree removal or enhancement can be determined by the use and site design.

Property owners of lowlands should be sensitive to upland uses and enhance hillsides to maintain existing views. Deciduous trees should be restricted to small varieties; coniferous evergreens should be thinned-out or limbed-up to allow for views from adjoining properties.

Proximity views require much larger fields of vision, therefore, development should remain a comfortable distance from the shore and be set back along view corridors. This will allow views of the water to widen from increasingly closer distances and will eliminate an abrupt change between development and shoreline.



Guideline

Existing views should be maintained. This can be accomplished by widening setbacks as development approaches the water. Buildings should step down hillsides. Buildings and rooftop appurtenances should be placed perpendicular to the water in order to safeguard views.

Special Consideration for Juanita Business District

View corridors to the Lake should be explored through new development in the business district. Existing residential views and view opportunities through Juanita Beach Park and down public streets should be preserved.

Special Consideration for Houghton/Everest Neighborhood Center

Buildings, landscaping and streetscape features along the NE 68th Street corridor should be designed to preserve existing views from the public right-of-way. Public spaces should be oriented to take advantage of views when possible.

Culverted Creeks

Issue

Often stream beds fall victim to progress and their stream banks are reduced to a drain pipe. One way to further the objective of clarifying the natural physical setting is to reopen stream beds wherever possible.

Guideline

Opportunities should be sought to restore portions of culverted creeks to their natural state.

Special Consideration for Downtown Kirkland

A former stream bed, now enclosed in culverts, flows through the center of downtown from 6th Street, through Peter Kirk Park, just south of Central Way and into Marina Park. A restored stream bed could be incorporated in the parks and other public sites, and possibly on private property.

Special Considerations for the Totem Lake Business District Core

One channel of the Totem Lake tributary extends along I-405, west of Totem Lake Boulevard in a culvert to Totem Lake. If it is feasible, restoration of this stream bed could be incorporated into the green corridor design developed for this segment of Totem Lake Boulevard. Another tributary of Juanita Creek runs across the northwest section of the Business District Core, with portions in a culvert and other portions remaining in an open stream bed. Redevelopment of these properties could include restoration of the culverted portions of the stream as an amenity provided on site.



