



MEMORANDUM

To: Kirkland Hearing Examiner
From: Tony Leavitt, Senior Planner
Jon Regala, Planning Supervisor
Date: June 10, 2020
File: DRV18-00312
Subject: **APPEAL OF DESIGN REVIEW BOARD DECISION
CONTINENTAL DIVIDE MIXED USE PROJECT
FILE NO. DRV18-00312**

I. INTRODUCTION

- A. Appellant: Alex Sidles of Bricklin and Newman LLP representing the Rose Hill Community Group (see Enclosure 1)
- B. Applicant: Continental Divide LLC
- C. Action Being Appealed: February 14, 2020 Design Review Board (DRB) decision approving with conditions the Design Response Conference application for the Continental Divide Mixed Use Project (see Enclosure 2). See Section III for additional information regarding the DRB's authority under design review.
- D. Appeal Summary: The appeal identifies the following specific elements being appealed:
 - 1. Failure to Consider Chapter 92 Guidelines and Violation of Chapter 92 Guidelines
 - 2. Failure to Consider and Violation of Pedestrian-Oriented Business District Guidelines.
 - 3. Violation of Rose Hill Business District GuidelinesSee Section V for more information regarding the appeal issues and staff analysis.

II. RULES FOR CONSIDERATION

- A. Rules: Kirkland Zoning Code (KZC) Sections 142.40 and 145.60 set forth the rules for appeals of Design Review Board Decisions. In the event that a project permit does not include an open record public hearing, then the decision of the Design Review Board shall be heard according to the Process I appeal procedures and provisions in KZC 145.60 and judicial review procedures and provisions in KZC 145.110.
- B. Who May Appeal: KZC Section 142.40.2 states the decision of the Design Review Board may be appealed by the applicant or any other individual or entity who submitted written or oral comments to the Design Review Board.

- C. Criteria for Submission of an Appeal: Under KZC Section 142.40.3, the appeal, in the form of a letter of appeal, must be delivered to the Planning Department within 14 calendar days following the date of the distribution of the Design Review Board decision. It must contain a clear reference to the matter being appealed and a statement of the specific elements of the Design Review Board decision disputed by the person filing the appeal. Only those issues under the authority of the Design Review Board as established by KZC 142.35(3) and (4) are subject to appeal.
- D. Participation in the Appeal: Under KZC Section 142.40.6, Only the person(s) who filed the appeal, the applicant, and the chair (or designee) of the Design Review Board may participate in the appeal. These persons may participate in the appeal in either or both of the following ways:
 - 1. By submitting written comments or testimony to the hearing body or officer prior to commencement of the hearing.
 - 2. By appearing in person, or through a representative, at the hearing and submitting oral testimony directly to the hearing body or officer. The hearing body or officer may reasonably limit the extent of oral testimony to facilitate the orderly and timely conduct of the hearing.
- E. Hearing Scope and Considerations: KZC Section 142.40.7 states that the scope of the appeal is limited to the specific elements of the Design Review Board decision disputed in the letter of appeal and the hearing body or officer may only consider comments, testimony, and arguments on these specific elements.
- F. Decision on the Appeal: Pursuant to KZC Section 142.40.11.a, unless substantial relevant information is presented which was not considered by the Design Review Board, the decision of the Design Review Board shall be accorded substantial weight. The decision may be reversed or modified if, after considering all of the evidence in light of the authority of the Design Review Board pursuant to KZC 142.35(3), the hearing body or officer determines that a mistake has been made. Specific allowances established by the applicable use zone charts may not be appealed unless the Design Review Board has approved exceptions to those allowances.

Under KZC Section 142.40.11.b, the hearing body or officer shall consider all information and material within the scope of the appeal submitted by the appellant. The hearing body or officer shall adopt findings and conclusions and either:

 - 1. Affirm the decision being appealed; or
 - 2. Reverse the decision being appealed; or
 - 3. Modify the decision being appealed.

III. DRB AUTHORITY

- A. Pursuant to KZC Sections 142.35.3 and 4, the Design Review Board shall review projects for consistency with the following:
 - 1. Design guidelines for pedestrian-oriented business districts, as adopted in Chapter 3.30 KMC.
 - 2. Design Guidelines for the Rose Hill Business District (RHBD) and the Totem Lake Neighborhood (TLN) as adopted in Chapter 3.30 KMC.
 - 3. Design Principles for Residential Development contained in Appendix C of the Comprehensive Plan for review of attached and stacked dwelling units located within the NE 85th Street Subarea and the Market Street Corridor.

4. The Parkplace Master Plan and Design Guidelines for CBD 5A as adopted in Chapter 3.30 KMC.
- B. For this project, the Design Guidelines for the Rose Hill Business District (RHBD) (see Enclosure 3) and the Design Principles for Residential Development (see Enclosure 4) are the applicable guidelines.
- C. The Design Review Board is also authorized to approve minor variations in development standards within certain Design Districts described in KZC 142.37, provided the variation complies with the criteria of KZC 142.37.

IV. BACKGROUND

- A. Site Location: The subject property, located at 8505 132nd Avenue NE, is 2.26 acres (98,429 square feet) in size and consists of 8 existing parcels (see Enclosure 5).
- B. Zoning and Land Use: The subject property is zoned RH 8 (Rose Hill Business District 8). The site previously contained multiple single-family residences and associated accessory structures. All existing structures have been demolished as part of the proposal.

The majority of the site is relatively flat with the only significant grade change occurring in the southeast corner of the site along NE 85th Street.

The property has street frontage along NE 85th Street, 132nd Avenue NE, and 131st Avenue NE.

The following list summarizes the zoning designation, uses, and allowed heights of properties adjacent to the subject property:

North: RSX 7.2. Single family residence. Maximum height is 30 feet.

East: Residential development (The Pointe) located in Redmond

West and South: RH 8. Single-family and commercial uses to the west. Office use to the south. Maximum height of 35 feet.

- C. Applicant's Proposal: The applicant is proposing to construct a four-story mixed-use project. The main building will have a single-story commercial space along NE 85th Street and transition to 3 stories of residential units above a parking level. A single-story commercial building will be located near NE 85th Street. Parking is proposed to be located in a surface parking lot and structured parking garage beneath the main building.

The proposal includes a request for minor variations to allow encroachments into the required front yard setback along NE 85th Street.

- D. Design Review Board Meetings: The project had four Design Response Conference meetings with the Design Review Board summarized as follows:
 - July 2, 2018: Design Response Conference
 - August 6, 2018: Continuation of Design Response Conference
 - November 18, 2019: Continuation of Design Response Conference
 - January 6, 2020: Continuation of Design Response Conference

The staff memos for the above conferences can be found online by their respective meeting dates and are adopted by reference as if fully set forth herein:

https://www.kirklandwa.gov/depart/planning/Boards_and_Commissions/DRB_Meeting_Information.htm

The DRB issued its approval of the Continental Divide Mixed Use Project with conditions on February 14, 2020 (see Enclosure 2). Section II of the DRB's decision contains a summary of the Design Response Conferences held for the project as well as a summary of public comments received. Section III of the DRB's decision contains an analysis of the project based on applicable design guidelines.

V. **APPEAL ISSUES**

On March 5, 2020, a timely appeal letter was submitted by Alex Sidles of Bricklin and Newman LLP representing the Rose Hill Community Group to the City regarding the DRB's decision on the Continental Divide Mixed Use project (see Enclosure 1).

The appellant's appeal issues are summarized below by topic followed by staff response.

A. **Failure to Consider Chapter 92 Guidelines and Violation of Chapter 92 Guidelines**

1. **Appeal Issues:** The DRB decision purports to apply the Design Guidelines for Rose Hill Business District and Design Guidelines for Residential Development, but no other set of guidelines. The DRB should have also reviewed the project under the Chapter 92 guidelines. Chapter 92 applies "to all new development, with the exception of development in the TL 7 zone." KZC 92.05.2. Yet the DRB did not review the project under Chapter 92.
2. **Staff Response:** The RH8 Use Zone Chart (see Enclosure 6) requires that projects with office, retail, and/or residential uses be permitted through the Design Review Process pursuant to KZC Section 142 (see Enclosure 7). A review of KZC Section 142 shows that it does not provide for review under Chapter 92 in this case. KZC Section 142.15.1.a states that new buildings greater than one (1) story in height or greater than 10,000 square feet of gross floor area shall be reviewed by the Design Review Board pursuant to KZC 142.35.

KZC Section 142.35.3 states the Design Review Board shall review projects for consistency with one or more of the following:

- Design guidelines for pedestrian-oriented business districts, as adopted in Chapter 3.30 KMC.
- Design Guidelines for the Rose Hill Business District (RHBD) and the Totem Lake Business District (TLBD) as adopted in Chapter 3.30 KMC.
- The Design Guidelines for Residential Development, as adopted in KMC 3.30.040, for review of attached and stacked dwelling units located within the Rose Hill Business District (RHBD), the PLA 5C zone, the Houghton/Everest Neighborhood Center, and the Market Street Corridor.
- The Parkplace Master Plan and Design Guidelines for CBD 5A as adopted in Chapter 3.30 KMC.

For this project, there are only two applicable guidelines documents:

- Design Guidelines for the Rose Hill Business District (RHBD)
- Design Principles for Residential Development.

Although Appellants claim the DRB should have conducted review under Ch. 92 in addition to the two guidelines listed above, Ch. 92 does not apply to any design review conducted by the DRB. For instance, the only reference to the design regulations (Chapter 92) in Chapter 142 is with regard to projects that are subject to Administrative Design Review (ADR) under Ch. 142.25. This matter, of course, did not undergo Administrative (i.e., City Staff) review, but was required to undergo review by the Board.

The Rose Hill Business District Design Guidelines do not require the Design Review Board to apply Chapter 92. Although Chapter 92 previously applied to Design Review Board reviews, in 2007 the City Council adopted Ordinance 4097 (see Enclosure 8) that eliminated this requirement. Unfortunately, the Rose Hill Business District Design Guidelines, adopted in January of 2006, were not updated to reflect this code amendment, which may have led to the appellant's confusion.

In summary, KZC Chapter 92 does not apply to this project and this appeal item should be dismissed.

B. Failure to Consider and Violation of Pedestrian-Oriented Business District Guidelines

1. **Appeal Issues:** The DRB is required to review projects under the design guidelines for pedestrian-oriented business districts. See KZC 145.35.3.a. The DRB decision does not apply these guidelines.
2. **Staff Response:** The Design Guidelines for Pedestrian-Oriented Business Districts (see Enclosure 9) is a stand-alone document that only applies to specific business districts in the City as outlined in the introduction section of the guidelines. The Rose Hill Business District is not listed as being subject to the Design Guidelines for Pedestrian-Oriented Business Districts.

C. Violation of Rose Hill Business District Guidelines

The appellant's appeal issues are summarized below by topic followed by staff responses.

1. Introductory Sections

- a. **Appellant:** The introduction to the RHBD Guidelines emphasizes that the East End of the RHBD (the site of the Continental Divide proposal) is to front each street block with two or even three buildings. In addition, the RHBD Guidelines suggest new developments are to be "residential in character," with the example given of "conversion of single-family houses" as an appropriate scale of development. Instead, the Continental Divide proposes a "superblock" with a single, detached building. Rather than convert the existing single-family homes, the developer proposes to demolish them.
- b. **Staff Response:**
 - i. The Design Guidelines Introduction section states the East End, between 128th Avenue NE and the eastern city limits at 132nd Avenue NE, will feature smaller scale businesses and mixed-uses in a setting compatible with surrounding residential uses. Contrary to appellant's contention, there is no discussion of fronting each street block with two or even three buildings.

- ii. The overview section for the East End Design District states that the East End includes the commercial and mixed-use zoned properties in the Rose Hill Business District east of 128th Avenue NE. The area features a mix of smaller scale uses oriented towards both the regional and local population. The style of development should be more residential in character including conversion of single family homes into commercial businesses. Nearly all buildings should feature pitched roofs and porches or smaller covered entries. The overview continues with encouraging smaller sites to be consolidated to maximize development opportunity and to share vehicular access and parking.
- iii. This section discusses the style of the development and does not discuss building scale as the appellant claims. Regardless, the project contains numerous architectural features that reference the surrounding residential neighborhood, including residential fenestration patterns, residential scale windows, balconies, and varying roofline modulation.

2. **Entry Gateway Features**

a. Appellant:

- i. The RHBD Guidelines call for a unique landscaping treatment at the gateway corners of Rose Hill (DG 1A). No discernible “distinctive landscaping with a rose garden or other distinctive soft-scape elements are visible. No detailed design of the gateway landscape feature is presented (even though detailed plans of the plaza are shown).”
- ii. The guidelines also suggest an artwork element (DG-1B). The art piece in the renderings is a placeholder and not designed. The round landscape walls hardly make an impact to fast moving traffic and do not stand a chance visually in front of the massive proposal.
- iii. The application has nothing resembling a city entry feature (DG-1C). No gateway sign with City logo is visible.
- iv. The southeast corner of the project has a masonry element shown, but it does not appear to be a monument sign nor an architectural “gateway element” (DG-1D). It is crowded by the massing of the building directly behind it. It also appears that the element is under-scaled as it is barely visible in the context of the building. Again, no gateway sign with City logo is visible.
- v. In direct opposition to the guidance, no lighting is proposed at the corner to illuminate a gateway element or provide “decorative lighting elements” (DG-1E).

b. Staff Response:

- i. The Entry Gateway Features guidelines states the following:
Incorporate entry gateway features in new development on NE 85th Street at 120th and 132nd Avenues.

Gateway features should incorporate some or all of the following:

- a. Distinctive landscaping including an assortment of varieties of roses.*
- b. Artwork (e.g. vertical sculpture incorporating historical information about Rose Hill).*
- c. A gateway sign with the City logo.*
- d. Multicolored masonry forming a base for an entry sign.*
- e. Decorative lighting elements.*

- ii. An applicant is not required to incorporate all of the above-listed features. Here, the applicant proposed a gateway feature that included the planting of rose bushes with a multicolored masonry raised planter bed with artwork in the form of a large climbing rose sculpture with accent lighting (see Enclosure 2, Attachment 2, Page 21). The Design Review Board properly concluded that the proposed design met the design guideline requirements.

3. **Street Trees**

a. **Appellant:**

- i. DG-2A: The street trees required by Section 2 of the design guidelines are not documented properly, especially on 132nd where the red trees are shown in the middle of a sidewalk and, therefore could not be planted.
- ii. DG-2B: The trees do not represent a “unifying element.” The red trees shown on the plans are not specifically called out and do not relate to the example trees shown elsewhere in the applicant’s submittal to the DRB.

b. **Staff Response:**

- i. The street tree design guideline requires the project to “incorporate street trees, along all street, internal access roads and pathways” and to “encourage development to utilize street trees as a unifying feature of the development”.
- ii. The DRB approved plans call for the planting of maple trees along all three frontages (see Enclosure 2, Attachment 2, Page 60). Staff reviewed the building permit plans and the trees will all be Armstrong Maples. The trees along 132nd Avenue NE are located in tree wells pursuant to Public Works standards. The tree wells are located along the eastern edge of the sidewalk and not in the middle of the sidewalk. Because the frontage trees comprise one species, they represent a unified tree palette within the project.

4. **Street Corners**

a. **Appellant**

- i. DG-3A: The guidelines require design treatments that emphasize street corners. These do not appear in the proposal. The aspects suggested by the applicant are neither recognized treatments by the design guideline nor unique to the corner design which is intended to be “distinctive” and “special.” (DG-3A, 3C, and 3D) Also, no signage

program for the development or gateway element is shown. (DG-3A, 1C). In addition, there are no plazas present at the corner, making the corner feel crowded and not a good space for pedestrian gathering.

- ii. DG-3C: The guidelines call for special landscaping elements on all street corners. The proposal does not include any. There is also no indication how the required seasonal interest will be provided.
- iii. DG-3D: The guidelines also call for visual interest, sense proportion and human scale. The guidelines include suggestions to achieve the required visual interest and scale, including a raised roof line, turret, corner balconies, special awning, and distinctive building materials.
- iv. None of the suggestions have been met here:
Raised Roofline. Instead, the roof line has been lowered relative to most of the rooflines on the project.
Turret. No typical corner type architectural element is present.
Corner Balconies: The previous rooftop deck has been removed and there are no balconies or decks at the corner element.
Special Awning: Awnings have been added at corner entries, but it is not special. The awning treatment is repeated at all the commercial entries along 85th making the corner element totally indistinct from the other storefronts.
Distinctive Building Materials. All of the commercial facades along NE 85th have the same modular brick finish and no "distinctive use of building materials" at the corner is visible.
- v. Finally, as a general violation that applies across the DG-3 guidelines, it is inappropriate to place the public plaza at midblock instead of at the corner. While the plaza may be a good design feature in theory, the design proposal has missed the opportunity to "hit two birds with one stone" and create the entry gateway element carefully described in the design guidelines and provide a successful urban gathering space. A corner building uniquely scaled and clearly differentiated or detached from the adjoining residential bar could potentially better address these issues.

b. **Staff Response:**

- i. The street corners design guidelines state the following:
 - *Encourage design treatments that emphasize street corners through the use of building location and design, plaza spaces, landscaping, distinctive architectural features, and/or signage.*
 - *Incorporate storefronts directly at 124th, 126th, and 128th street corners to reinforce the desired pedestrian-oriented character of the Neighborhood Center.*
 - *Encourage special landscaping elements on all street corners in the Rose Hill Business District. Such landscaping elements should incorporate a variety of plant types and textures that add seasonal interest.*
 - *Encourage all buildings located at or near street corners to incorporate special architectural elements that add visual interest*

and provide a sense of human proportion and scale. This could include a raised roofline, turret, corner balconies, bay windows, special awning or canopy design, and/or distinctive use of building materials.

- ii. At the November 18, 2019 meeting the DRB requested that the applicant remove the second story balcony at the southeast corner of the building to make the corner more commercial in nature and that the lap siding be replaced with a more commercial looking material. Enclosure 2, Attachment 2, Pages 20 thru 22 outline the applicant's response to this issue at the January 6, 2020 meeting.
- iii. The applicant made the changes requested by the DRB on November 18th. The Design Review Board then concluded that the southeast street corner of the main building utilizes a recessed building entry, pedestrian-oriented space with seating, landscaping, and an entry gateway feature to enhance the appearance of the highly visible location. Additionally, they concluded that the building materials helped to differentiate the corner and the commercial portion from the residential facades, meeting the intent of the street corners design guidelines.

5. **Building Location and Orientation**

a. **Appellant:**

- i. The goal of DG-5 is to "minimize negative impacts to adjacent single-family residential areas." By creating a building too bulky and too close to the single-family houses to its north, the Continental Divide project fails to take account of this goal.
- ii. The mass and bulk of the design is made more imposing by the lack of a set back at any of the upper levels overlooking the residential neighbors. The height and width of the unbroken mass of building render it totally out of scale with anything in the East End district if not within the whole of the Rose Hill Design District.
- iii. In addition, the project violates specific policies within DG-5:
DG 5A: The project is not oriented towards the streets, plazas or common open spaces.
DG-5B: The project is not configured to create a focal point. Instead, it is an undistinguished bulk.
DG-5D: The project is not sited and oriented to minimize impacts to adjacent single-family residents. The suggested minimization of windows and stepping back of upper stories has not been provided, nor have landscape trees been provided to screen the single-family houses.

b. **Staff Response:**

- i. The applicable building location and orientation guidelines for this project are the following:
 - *Locate and orient buildings towards streets, plazas or common open spaces, and major internal pathways.*

- *Configure buildings to create focal points especially on larger sites.*
 - *Site and orient multi-story buildings to minimize impacts to adjacent single family residents. For example, if a multi-story building is located near a single family property, provide landscaping elements and/or minimize windows and openings to protect the privacy of adjacent homes. Another consideration is to increase upper level building setbacks.*
 - *Encourage development to locate and orient buildings towards the street with parking to the side or the rear: At a minimum this should include: Non-residential facades located directly adjacent to the sidewalk or buildings featuring a modest landscaped front yard area or plaza area between the sidewalk and the façade. Primary building entries and windows facing the street. Landscaping trimmed to maintain visibility between the sidewalk and the building.*
 - *Office and residential developments are encouraged to locate and orient buildings towards an interior open space or courtyard, where space allows. In this scenario, primary building entries may orient towards the open space provided there is direct visibility into the open space from the sidewalk. Windows should be provided on the street façade.*
 - *Buildings may be located towards the rear of the property provided they meet landscaping, parking, pathway, and façade standards along the front.*
- ii. Throughout their review of the project, the Design Review Board discussed building location and orientation to help minimize impacts on the neighboring residential properties. The Board requested multiple revisions to address these issues and concluded that the final design met these requirements.
- iii. The project complies with the guideline of minimizing negative impacts to adjacent single family residents. The project is located 30 feet from the north property line, and 20 feet from the west property line, and a required 15 foot landscape buffer is located along both property lines. Contrary to appellant's claim, the buffers will include numerous trees that will help screen the building from neighboring residential uses.
- iv. The DRB concluded that the proposed setbacks eliminated the need for any upper story setbacks.
- v. Once again, contrary to appellant's claim, the project is oriented towards streets, plazas, and open spaces on the site. This claim by appellant's seems to lack any factual support.

6. Sidewalks

- a. **Appellant:** Along 132nd, there is no “curb zone” as required by DG-6A and DG-6B. In addition, trees are shown planted in the middle of the sidewalk, which is not consistent with their use as a sidewalk.
- b. **Staff Response:** The proposal includes street trees planted in tree wells along the eastern edge of the sidewalk adjacent to 132nd Avenue NE, which meets the curb zone requirement of this guideline (see Enclosure 2, Attachment 2, Page 9). The proposed design is consistent with adopted Public Works sidewalk design requirements for minor arterials.

7. Interior Pedestrian Connections

- a. **Appellant:** DG-11 Interior Pedestrian Connections. The large, block-wide “superblock” design of the project violates each of the pedestrian connection guidelines. None of the pedestrian connection contemplated in the RHBD Guidelines appear in this project.
- b. **Staff Response:**
 - i. The applicable interior pedestrian connections guidelines for this project are the following:
 - *Provide convenient pedestrian access between the street, bus stops, buildings, parking areas, and open spaces. Internal pedestrian connections are particularly important on large sites where some uses may be placed away from a street.*
 - *Design all buildings abutting a public sidewalk or major internal pathway to provide direct pedestrian access to the sidewalk or pathway.*
 - ii. Guidelines 11.c and 11.d do not apply to the project as the project is not set back from the street, is not adjacent to a similar or complementary use, and does not have a large parking lot with 3 or more parking aisles.
 - iii. The Design Review Board found that the project provided convenient pedestrian access from and to the site including connections between the building and adjacent right-of-ways. All buildings provide direct pedestrian access to public sidewalks and major internal pathways.

8. Architectural Style

- a. **Appellant:**
 - i. Projects in the East End are encouraged to adopt common residential styles, meaning low-slung ranch-style house with swallow gable or hip roofs, and fenestration patterns similar to single-family home. Here, the repetitive shed roofs set on the project’s highly vertical facade modulation bays is not in keeping with residential style.
 - ii. In addition, very few opportunities to relate to human scale are offered on the east or west facade as there are few grade level doors, stoops, or porches; items that typically give large residential developments a more human scale. The location of the bottom level parking garage has the effect of creating long sections of blank facades along the east elevation. The store fronts have no relationship to common residential designs.

b. **Staff Response:**

- i. The applicable architectural style guideline reads:
Encourage buildings in the East End to utilize architectural styles common to neighboring residential areas. This includes gables roofs, front porches or covered entries, and fenestration patterns that to relate to adjacent single family homes.
- ii. The appellant's statement that residential style means a "low slung ranch-style house with swallow gable or hip roofs" is inaccurate. The guideline is very general and provides flexibility on the chosen style as noted in the discussion section (see Enclosure 3, Page 28). The diversity of residential architecture in the East End would also preclude the imposition of a specific architectural style on new mixed-use projects.
- iii. At the July 2, 2018 meeting, the DRB requested that the applicant look at the fenestration patterns on the residential portion of the structure to make the façade and windows more residential in nature. The Board continued to discuss fenestration at the August 6, 2018 meeting and asked the applicant to submit elevations that compared the development of fenestrations over the first two meetings. At the November 18, 2020 meeting, the applicant submitted plans with this comparison (see Enclosure 2, Attachment 2, page 46-49). The DRB reviewed these plans and concluded that the fenestration patterns relate to adjacent single family homes. Other examples of architectural features that promote human scale are described immediately below under "Architectural Scale" and under "Human Scale."

9. **Architectural Scale**

a. **Appellant:**

- i. The Continental Divide project violates each of the DG-17 guidelines. The project lacks differentiated, residential-scale fenestration sizes and patterns; lacks changes in materials at upper levels; and lacks upper-level setbacks. The project lacks basic, middle and top, or a classical type approach to designing the facade that could have achieved better architectural scaling. DG-17A suggests limiting the size of fenestration to 35 square feet, but almost all the fenestration facing the residential zones is larger than 35 square feet.
- ii. In addition, the building's "superblock" style of architecture is incompatible with the adjacent single-family homes and with commercial development along 85th.

b. **Staff Response:**

- i. The architectural scale guidelines outline a combination of techniques that are desirable to reduce the architectural scale of buildings. Residential uses throughout the Rose Hill Business District warrant such techniques at 30-foot intervals.

- ii. Techniques include the following (see Enclosure 3, Page 29 for complete text):
 - Incorporate fenestration techniques that indicate the scale of the building.
 - Encourage vertical modulation on multi-story buildings to add variety and to make large buildings appear to be an aggregation of smaller buildings.
 - Encourage a variety of horizontal building modulation techniques to reduce the architectural scale of the building and add visual interest. Horizontal building modulation is the horizontal articulation or division of an imposing building façade through setbacks, awnings, balconies, roof decks, eaves, and banding of contrasting materials. For residential uses, provide horizontal building modulation based on individual unit size. Horizontal modulation is most effective when combined with roofline modulation and changes in color and/or building materials. The depth and width of the modulation should be sufficient to meet the objectives of the guidelines. Avoid repetitive modulation techniques, since they may not be effective when viewed from a distance. Larger residential buildings will require greater horizontal modulation techniques to provide appropriate architectural scale.
- iii. Encourage a variety of roofline modulation techniques. This can include hipped or gabled rooflines and modulated flat rooflines. Hipped and gabled rooflines are preferred for multi-family buildings and buildings in the East End.
- iv. The DRB and the applicant spent a majority of the time at the meetings discussing architectural scale and minimizing the impact on neighboring single family residential uses. At the July 2, 2018 meeting, the DRB requested revisions to address these guidelines including increasing vertical and horizontal modulation, increasing the variety of rooflines and forms to help create the look of smaller buildings, and increasing the depth and width of horizontal modulation. The DRB and the applicant continued to discuss and address these issues over the next two meetings. Enclosure 2, Pages 46 thru 49 outline the project's response to these guidelines.
- v. In their Decision, the DRB concluded that the additional 1-foot setback from the northern property line (setback total of 30 feet provided) and the reduction in the number of balconies and windows along the north facade helped to reduce impacts on neighboring residential properties. The DRB also concluded that the following design changes throughout the process were successful in addressing concerns regarding architectural scale (through vertical and horizontal modulation):
 - Treatment of the main building facades with small recesses, residential scale windows, and varying roofline modulation.
 - The incorporation of balconies, changes in building color and materials, and vertical building modulation based on individual units.

10. Human Scale

- a. **Appellant:** The overall height, bulk, and scale of the project violates the requirement that architectural building elements must lend the building a human scale. The building's "super block" style of architecture is incompatible with human scale.
- b. **Staff Response:**
 - i. The human scale guidelines encourage a combination of architectural building elements that lend the building a human scale. Examples include arcades, balconies, bay windows, roof decks, trellises, landscaping, awnings, cornices, friezes, art concepts, and courtyards. Window fenestration techniques described in Design Guideline Section 17 (Architectural Scale) can also be effective in giving humans clues as to the size of the building. Consider the distances from which buildings can be viewed (from the sidewalk, street, parking lot, open space, etc.).
 - ii. Enclosure 2, Pages 50 thru 51 show the project's response to these guidelines including the use of balconies, landscaping, window fenestration and courtyards throughout the project.
 - iii. The appellant's general claim that the overall height, bulk and scale of the project automatically violate these guidelines is not responsive to these guidelines. The project has incorporated architectural elements into the project that meet the human scale requirements of this section.

11. Signs

- a. **Appellant:** DG-20 requires signs on all commercial facades adjacent to a sidewalk, but the DRB decision does not include a description or depiction of the project's signage.
- b. **Staff Response:** The Design Review Board briefly discussed signs on the commercial building but determined that the signage for the project would have a minimal effect on the project and applicable zoning code regulations would be sufficient to address future sign design.

12. Violation of Minor Variance Criteria

- a. **Appellant:**
 - i. Violation of Minor Variation Criteria. The DRB decision approves, as "minor variations," a setback encroachment of 2.5 to 7.5 feet for the standalone commercial building, and a 1.5-foot setback encroachment for the main building, for a total setback encroachment of 575 square feet.
 - ii. Contrary to the requirements of KZC 142.37.4, the variations do not result in superior design. Instead, they highlight the out-of-scale height, bulk, and scale of the project. There will be less pedestrian access and worse privacy and view impacts on the adjacent single-family residences.
- b. **Staff Response:**
 - i. KZC Section 142.37.1.a allows an applicant to request minor variations to the minimum required setback in the RH8 zone. The

DRB may grant a minor variation only if it finds that the following are met (KZC Section 142.37.4):

- The request results in superior design and fulfills the policy basis for the applicable design regulations and design guidelines; and
 - The departure will not have any substantial detrimental effect on nearby properties and the City or the neighborhood.
- ii. The applicant requested the following minor setback variations along NE 85th Street:
- 2.5-foot to 7.5-foot encroachment for the standalone commercial building.
 - 1.5-foot encroachment for the main building.
 - Approximately 575 square feet of total setback encroachment.
- iii. The DRB concluded that the proposed minor variations meet the criteria in KZC Section 142.37.4 and that the minor variation results in superior design by helping to create a pedestrian-oriented façade along NE 85th Street. The DRB concluded that superior design elements were the materials used on the NE 85th Street facades (including brick, metal panel siding, architectural concrete, and metal canopies), cornice detailing, the amount of glazing used on the NE 85th Street facades, the revised plaza design, and pedestrian amenities (including seating and potted plants).
- iv. The minor variation is supported by Section 5 of the Design Guidelines (Building Location and Orientation) – East End NE 85th Street Frontage, which encourages locating and orienting buildings towards the street with parking to the side or the rear, primary building entries facing the street, façades with transparent windows, and weather protection along the facades.
- v. Additionally, the DRB found that the reduction will not have a substantial detrimental effect on nearby properties and the City or the neighborhood. The reduction is adjacent to NE 85th Street and over 100 feet from the nearest building (two office building on the south side of NE 85th Street) and the reduction results in a superior experience for pedestrians on NE 85th Street.
- vi. The appellant’s arguments do not include specific reasons for the project not meeting the criteria. Potential impacts to residential properties located on the other side of the property (approximately 280 feet from the proposed encroachments) are not relevant and the encroachment creates a more pedestrian friendly façade along NE 85th Street.

VI. STAFF RECOMMENDATION

Staff recommends that the Hearing Examiner uphold the DRB’s February 14, 2020 decision approving the project with conditions.

VII. JUDICIAL REVIEW (KZC SECTION 145.110)

The action of the City in granting or denying an application under this chapter may be reviewed pursuant to the standards set forth in RCW 36.70C.130 in the King County Superior Court. The land use petition must be filed within 21 calendar days of the issuance of the final land use decision by the City. For more

information on the judicial review process for land use decisions, see Chapter 36.70C RCW.

VIII. ENCLOSURES

1. Appeal Letter
2. Design Review Board Decision and Attachments
3. Design Guidelines for Rose Hill Business District
4. Design Guidelines for Residential Development
5. Vicinity Map
6. RH 8 Zone Use Chart
7. KZC Chapter 142- Design Review
8. Ordinance 4097
9. Design Guidelines for Pedestrian-Oriented Business Districts Introduction Section



BRICKLIN & NEWMAN LLP
lawyers working for the environment

Reply to: Seattle Office

March 5, 2020

Tony Leavitt, Senior Planner
Planning and Building Department
123 Fifth Avenue
Kirkland, WA 98033

Re: Notice of Appeal of Project No. DRV18-00312 (Design Review).

Dear Mr. Leavitt and Planning and Building Department:

On behalf of our client, the Rose Hill Community Group, this is a notice of appeal of the February 14, 2020 decision by the Design Review Board (published February 20) regarding the Continental Divide project, DRV18-00312 (associated with SEP18-00313).

For the reasons stated below, the DRB decision's does not comply with the Kirkland Municipal Code or the relevant design review guidelines. Therefore, the DRB decision should be reversed.

I. Matters Being Appealed

The February 14, 2020 decision of the Design Review Board, approving with conditions project DRV18-00312, the Continental Divide. A copy of the DRB decision is attached.

The appeal of the DRB decision should be consolidated with the Rose Hill Community Group's earlier appeal, dated December 12, 2019, of the SEPA DNS for this project.

II. Specific Elements Being Appealed

- A. Failure to Consider Chapter 92 Guidelines and Violation of Chapter 92 Guidelines.**
The DRB decision purports to apply the Design Guidelines for Rose Hill Business District and Design Guidelines for Residential Development, but no other set of guidelines.

The DRB should have also reviewed the project under the Chapter 92 guidelines. Chapter 92 applies "to all new development, with the exception of development in the TL 7 zone." KZC 92.05.2. Yet the DRB did not review the project under Chapter 92.

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If the DRB had properly reviewed the project under Chapter 92, the DRB would have disapproved the project. The project violates Chapter 92 in multiple ways, including but not limited to exceeding the 120-foot façade length limit.

B. Failure to Consider and Violation of Pedestrian-Oriented Business District Guidelines. The DRB is required to review projects under the design guidelines for pedestrian-oriented business districts. *See* KZC 145.35.3.a. The DRB decision does not apply these guidelines.

If the DRB had considered the guidelines, the project would not have been approved. The project violates the pedestrian-oriented business district guidelines in various ways, including but not limited to:

- Failure to locate pedestrian-oriented plazas along a well-travelled pedestrian route;
- Lack of well defined, safe pedestrian walkways that minimize distances from the public sidewalk to the internal pedestrian system;
- Failure to have a setback of at least ten feet from the sidewalk;
- Failure to document viable street trees;
- Failure to construct entry gateway features that strengthen the character and identity of the neighborhood;
- Failure to use shared accesses and reciprocal vehicular easements in order to reduce the number of curb cuts;
- Failure to provide architectural detailing at all window jambs, sills, and heads;

C. Violation of Rose Hill Business District Guidelines. Unlike the previous two sets of guidelines (Chapter 92 and pedestrian-oriented business districts), the DRB decision purports to apply the Rose Hill Business District (RHBD) Guidelines. However, the decision fails to apply the following guidelines:

- **Introductory Sections.** The introduction to the RHBD Guidelines emphasizes that the East End of the RHBD (the site of the Continental Divide proposal) is to front each street block with two or even three buildings. RHBD Guidelines at 7. In addition, the RHBD Guidelines suggest new developments are to be “residential in character,” with the example given of “conversion of single-family houses” as an appropriate scale of development.

Instead, the Continental Divide proposes a “superblock” with a single, detached building. Rather than convert the existing single-family homes, the developer proposes to demolish them.

- **DG-1 Entry Gateway Features.**

DG-1a The RHBD Guidelines call for a unique landscaping treatment at the gateway corners of Rose Hill (DG 1a). No discernible “distinctive landscaping”

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with a rose garden or other distinctive soft-scape elements are visible. No detailed design of the gateway landscape feature is presented (even though detailed plans of the plaza are shown).

DG 1b - The guidelines also suggest an artwork element. The art piece in the renderings is a placeholder and not designed. The round landscape walls hardly make an impact to fast moving traffic and do not stand a chance visually in front of the massive proposal.

DG-1c - The application has nothing resembling a city entry feature. No gateway sign with City logo is visible.

DG-1d - The southeast corner of the project has a masonry element shown, but it does not appear to be a monument sign nor an architectural “gateway element.” It is crowded by the massing of the building directly behind it. It also appears that the element is under-scaled as it is barely visible in the context of the building. Again, no gateway sign with City logo is visible.

DG-1e – In direct opposition to the guidance, no lighting is proposed at the corner to illuminate a gateway element or provide “decorative lighting elements.”

- **DG-2 Street Trees**

DG-2a – The street trees required by Section 2 of the design guidelines are not documented properly, especially on 132nd where the red trees are shown in the middle of sidewalk and, therefore, could not be planted.

DG-2b – The trees do not represent a “unifying element.” The red trees shown on the plans are not specially called out and do not relate to the example trees shown elsewhere in the applicant’s submittal to the DRB.

- **DG-3 Street Corners**

DG-3a – The guidelines require design treatments that emphasize street corners. These do not appear in the proposal. The aspects suggested by the applicant are neither recognized treatments by the design guideline nor unique to the corner design which is intended to be “distinctive” and “special”. (DG-3a, -3c, and -3d). Also, no signage program either for the development or gateway element is shown. (DG-3a, DG-1c). In addition, there are not plazas present at the corner, making the corner feel crowded and not a good space for pedestrian gathering.

DG-3c – The guidelines call for special landscaping elements on all street corners. The proposal does not include any. There is also no indication how the required seasonal interest will be provided.

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DG 3d - The guidelines also call for visual interest, sense of proportion and human scale. The guidelines include suggestions to achieve the required visual interest and scale, including a raised roof line, turret, corner balconies, special awning, and distinctive building materials. None of the suggestions have been met here:

Raised Roof Line. Instead, the roof line has been lowered relatively to most of the roof lines on the project.

Turret. No typical corner type architectural element is present.

Corner Balconies. The previous rooftop deck has been removed and there are no balconies or decks at the corner element.

Special Awning. Awnings have been added at corner entries, but it is not special. The awning treatment is repeated at all the commercial entries along 85th making the corner element totally indistinct from the other storefronts.

Distinctive Building Materials. All of the commercial facades along 85th have the same modular brick finish and no “distinctive use of building materials” at the corner is visible.

Finally, as a general violation that applies across the DG-3 guidelines, it is inappropriate to place the public plaza at midblock instead of at the corner. While the plaza may be a good design feature in theory, the design proposal has missed the opportunity to “hit two birds with one stone” and create the entry gateway element carefully described in the design guidelines and provide a successful urban gathering space. A corner building uniquely scaled and clearly differentiated or detached from the adjoining residential bar could potentially better address these issues.

- **DG-5 Building Location and Orientation.** The goal of DG-5 is to “minimize negative impacts to adjacent single-family residential areas.” By creating a building too bulky and too close to the single-family houses to its north, the Continental Divide project fails to take account of this goal.

The mass and bulk of the design is made more imposing by the lack of a set back at any of the upper levels overlooking the residential neighbors. The height and width of the unbroken mass of building render it totally out of scale with anything in the East End district, if not within the whole of the Rose Hill Design District.

In addition, the project violates specific policies within DG-5:

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DG 5a - The project is not oriented toward the streets, plazas or common open spaces.

DG-5b – The project is not configured to create a focal point. Instead, it is an undistinguished bulk.

DG-5d – The project is not sited and oriented to minimize impacts to adjacent single-family residents. The suggested minimization of windows and stepping back of upper stories has not been provided, nor have landscape trees been provided to screen the single-family houses.

- **DG-6 Sidewalks.** Along 132nd, there is no “curb zone” as required by DG-6a and -6b. In addition, trees are shown planted in the middle of the sidewalk, which is not consistent with their use as a sidewalk.
- **DG-11 Interior Pedestrian Connections.** The large, block-wide “superblock” design of the project violates each of the pedestrian connection guidelines. None of the pedestrian connections contemplated in the RHBD Guidelines appear in this project.
- **DG-16. Architectural Style.**

DG 16b - Projects in the East End are encouraged to adopt common residential styles, meaning low-slung ranch-style house with shallow gable or hip roofs, and fenestration patterns similar to single-family homes. Here, the repetitive shed roofs set on the project’s highly vertical facade modulation bays is not in keeping with residential style.

In addition, very few opportunities to relate to human scale are offered on the east or west facade as there are few grade level doors, stoops, or porches; items that typically give large residential developments a more human scale. The location of the bottom level parking garage has the effect of creating long sections of blank facades along the east elevation. The store fronts have no relationship to common residential designs.

- **DG-17 Architectural Scale.** The Continental Divide project violates each of the DG-17 guidelines. The project lacks differentiated, residential-scale fenestration sizes and patterns; lacks changes in materials at upper levels; and lacks upper-level setbacks. The project lacks base, middle and top, or a classical type approach to designing the facade that could have achieved better architectural scaling. DG 17a suggests limiting the size of fenestration to 35 square feet, but almost all the fenestration facing the residential zones is larger than 35 square feet.

In addition, the building’s “superblock” style of architecture is incompatible with the adjacent single-family homes and with commercial development along 85th.

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- **DG-18 Human Scale.** The overall height, bulk, and scale of the project violates the requirement that architectural building elements must lend the building a human scale. The building's "superblock" style of architecture is incompatible with human scale.
- **DG-20 Signs.** DG-20 requires signs on all commercial façades adjacent to a sidewalk, but the DRB decision does not include a description or depiction of the project's signage.

D. Violation of Minor Variation Criteria. The DRB decision approves, as "minor variations," a setback encroachment of 2.5 to 7.5 feet for the standalone commercial building, and a 1.5-foot setback encroachment for the main building, for a total setback encroachment of 575 square feet.

Contrary to the requirements of KZC 142.37.4, the variations do not result in superior design. Instead, they highlight the out-of-scale height, bulk, and scale of the project. There will be less pedestrian access and worse privacy and view impacts on the adjacent single-family houses.

III. Demonstration of Standing

The Rose Hill Community Group consists of homeowners and residents who live in the immediate vicinity of the Continental Divide project. The project will impact their views, reduce their privacy, and worsen the aesthetics of their neighborhood. The Rose Hill Community Group provided comments to the City during the design review process.

IV. Conclusion

For the foregoing reasons, the DRB decision should be reversed.

Very truly yours,

BRICKLIN & NEWMAN, LLP



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Attorney for Rose Hill Group

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cc: Client



CITY OF KIRKLAND Planning and Building Department
123 5th Avenue, Kirkland, WA 98033
425.587.3600 ~ www.kirklandwa.gov

DESIGN REVIEW BOARD DECISION

FILE NUMBER: DRV18-00312
PROJECT NAME: CONTINENTAL DIVIDE MIXED-USE PROJECT
CONTINENTAL DIVIDE LLC
APPLICANT:
PROJECT PLANNER: TONY LEAVITT, SENIOR PLANNER

I. SUMMARY OF DECISION

Continental Divide LLC applied for design review of the Continental Divide LLC project at 8505 132ND Avenue NE (see Attachment 1). The applicant is proposing to construct a four-story mixed-use building and a standalone single-story commercial building. The main building will have ground-floor commercial space along NE 85th Street and 3 stories of residential units above a parking level. The standalone building will be located near NE 85th Street. The proposal includes a minor variation request to allow encroachments into the required front yard setback along NE 85th Street.

Kirkland Zoning Code Section 142.35.3 states that the Design Review Board shall review projects for consistency with the following:

- The Design Guidelines for Rose Hill Business District, as adopted in Chapter 3.30 KMC.
- The Design Guidelines for Residential Development, as adopted in KMC 3.30.040, for review of attached and stacked dwelling units located within the Rose Hill Business District (RHBD).

On January 6, 2020, the Design Review Board (DRB) approved the project as shown on the plans dated January 6, 2020 (see Attachment 2) subject to the following conditions:

- A. This application is subject to the applicable requirements contained in the Kirkland Municipal Code, Zoning Code, and Building and Fire Code. It is the responsibility of the applicant to ensure compliance with the various provisions contained in these ordinances. Attachment 3, Development Standards, intended to familiarize the applicant with some of the additional development regulations. This attachment does not include all of the additional regulations.
- B. As part of the application for a building permit the applicant shall submit the following:
 1. Construction plans demonstrating compliance with the project plans approved by the DRB as shown in Attachment 2.
 2. Revised plans that show the second story balcony at the southwest corner of the main building as being removed. The southwest corner of the main building shall

- be redesigned to match the design of the main building's southeast corner (see Conclusion III.B).
3. A lighting plan that shows compliance with the requirements of KZC Section 115.85.2 (Exterior Lighting Requirements for the Rose Hill Business District) (see Conclusion III.C).
- C. Prior to final inspection of a building permit by the Planning Official, the project architect shall submit a letter stating that they have evaluated the project to ensure it is consistent with the plans approved through Design Board Review and no modifications have been made that were not previously approved by the City.

II. DESIGN RESPONSE CONFERENCE MEETINGS

A. Background Summary

The DRB held four Design Response Conference meetings for the project. The staff report, plans, and applicant response to the DRB's recommendations from each meeting can be found listed by meeting date at this online web address:

[http://www.kirklandwa.gov/depart/Planning/DRB Meeting Information.htm](http://www.kirklandwa.gov/depart/Planning/DRB_Meeting_Information.htm)

Below is a summary of the Board's discussions at the four Design Response Conferences held for the project.

July 2, 2018 Conference: The Design Review Board reviewed the plans submitted by Encore Architects dated July 2, 2018. Staff provided an overview of the Zoning Code and Comprehensive Plan policies for the Rose Hill Business District (RH) 8 zone and the key design issues for the project. Staff's memo dated June 25, 2018 provides an analysis of project consistency with applicable zoning regulations, Comprehensive Plan policies and Design Guidelines for Pedestrian Oriented Districts.

After receiving public comment on the project and deliberating, the Board requested the applicant to return for a second meeting to respond to the following DRB comments:

- Include two design options for the gateway feature area. The building at the corner of 132nd and 85th needs additional treatment.
- Look at ways to decrease the impacts on neighboring properties - specifically the properties to the north. This could include minimizing the number of windows and balconies on this façade.
- Include additional information regarding the treatment of the 132nd Avenue NE blank wall including full landscape renderings.

- Provide seating areas and other amenities near the bus stop and sidewalks.
- Provide a pedestrian connection between the north building exit and 131st Avenue NE.
- Windows need to be more residential in nature. Create variety and decrease size to match neighboring residential uses.
- Increase the amount of vertical and horizontal modulation, specifically along the longer east and north facades. Varying roof heights and forms would help to create the look of smaller buildings. Increase depth and width of horizontal modulations.
- The masonry material on the commercial façade needs more texture and interest.
- Provide a detailed landscaping plan.

This meeting was continued to August 6, 2018.

August 6, 2018 Conference:

The Design Review Board reviewed the revised plans submitted by Encore Architects dated August 6, 2018. Staff's memo dated July 27, 2018 provides an analysis of project consistency with applicable zoning regulations, Comprehensive Plan policies and Design Guidelines for Pedestrian Oriented Districts.

After receiving public comment on the project and deliberating, the Board requested the applicant to return for a third meeting to respond to the following DRB comments:

- Update plans and elevations to match the renderings. Include dimensional information on plans to help show the depth of modulations and other related items.
- Refine the renderings and models. Bringing the model to the meeting would be beneficial.
- The Board preferred the curved planters for the 85th/ 132nd Corner but would like to see how the curb cuts and utilities impact the design.
- Comparison of the fenestration changes along the north façade from the July 2nd plans to the August 6 plans.
- Provide updated materials boards and sheets.
- Provide more information on the southeast corner building design including upper deck design and material treatment.

- Parapets on the backside of commercial building need additional material treatment.
- Ensure that the project complies with the modulation requirements in the Architectural Scale design guidelines (Section 17).

This meeting was continued to September 17, 2018. Prior to the meeting, the applicant requested that the meeting be cancelled in order to complete the SEPA review.

November 18, 2019 Conference:

The Design Review Board reviewed the revised plans submitted by Encore Architects dated November 18, 2019. Staff's memo dated November 6, 2019 provides an analysis of project consistency with applicable zoning regulations, Comprehensive Plan policies and Design Guidelines for Pedestrian Oriented Districts.

After receiving public comment on the project and deliberating, the Board requested the applicant to return for a fourth meeting to respond to the following DRB comments:

- Submit an updated formal setback modification request. The request should address the criteria in KZC Section 142.37.
- Submit a lighting plan that addresses the Design Guidelines contained in Section 9 - Lighting.
- Ensure that all plans are coordinated throughout the entire packet including landscape plans.
- Provide elevations for all facades for each building including the north facade of the standalone commercial building.
- The design of the southeast street corner needs to ensure compliance with Design Guideline 3.d. The Board requested that the lap siding be replaced with a more commercial looking material. Include the proposed artwork in elevation drawings.
- Look at a reduction in the width of the landscape strip along NE 85th Street, an increase in the sidewalk width, and including more pedestrian amenities and planters along the building facades. See Design Guideline Section 10 for ideas.
- More development of the plaza area. The Board would like to see more hardscape and less landscaping in the area north of the bus stop and between the two buildings. See Design Guidelines Section 12 for ideas.
- Additional development of the standalone commercial façade to create a superior design to offset the modification request. Ideas include materials changes on the parapets and

cornice treatments. The Board recommended looking at the Hectors Building on Lake Street and the Park Lane Public House for some ideas. Also address any blank walls on the backside of the building (see Design Guideline Section 8).

- Incorporate any plans that were submitted at the November 18th meeting into the December 6th packet.

This meeting was continued to January 6, 2020.

January 6, 2020 Conference:

The Design Review Board reviewed the revised plans submitted by Encore Architects dated January 6, 2020. Staff's memo dated December 30, 2019 provides an analysis of project consistency with applicable zoning regulations, Comprehensive Plan policies and Design Guidelines for Pedestrian Oriented Districts.

The applicant presented revised plans, which addressed the requested items from the DRB. The DRB discussed the changes proposed by the applicant and at the conclusion of the meeting voted to approve the project. See Section III below for further information regarding the DRB's discussions and conclusions.

B. Public Comment

All public comment letters and e-mails received during the Design Response Conference meetings were forwarded to the Board for consideration (see Attachment 4). In addition, oral comment from interest parties were provided at the public meetings. All written comments are contained in the City's official file. Below is a summary of the general public comment themes that emerged through the design review process:

- The setback minor modification does not meet the requirements for approval
- The building's east, north and west façades should be mitigated to reduce impacts on neighboring residential properties.
- Neighboring residents were concerned about the project's impacts on their privacy along the northern edge of the site.
- The overall scale of the project is too large for the neighborhood.
- Concerns about traffic impacts on neighboring roads.
- Noise, lighting, and solar access impacts of the project.
- Project does not comply with the Neighborhood Plan.
- Impacts of a future 132nd Avenue right-of-way dedication on the project's gateway feature.

III. DESIGN REVIEW BOARD DISCUSSION AND CONCLUSIONS

Below is a summary of the key issues and conclusions reached by the Design Review Board during the design review process. For more background on these issues and evaluation of how the project meets the Zoning Code see the staff advisory reports from the design response conferences contained in File DRV18-00312 and online on the previously mentioned DRB meeting page.

A. MINOR VARIATION TO REDUCE NE 85TH STREET FRONT YARD SETBACK

The RH 8 Zoning District requires a minimum 10-foot front yard setback along NE 85th Street.

KZC Section 142.37.1.a allows an applicant to request minor variations to the minimum required setback in the RH8 zone. The DRB may grant a minor variation only if it finds that the following are met (KZC Section 142.37.4):

- The request results in superior design and fulfills the policy basis for the applicable design regulations and design guidelines;
- The departure will not have any substantial detrimental effect on nearby properties and the City or the neighborhood.

The applicant requested the following minor setback variations along NE 85th Street:

- 2.5 foot to 7.5 foot encroachment for the standalone commercial building.
- 1.5-foot encroachment for the main building.
- Approximately 575 square feet of total setback encroachment.

The plans show the proposed minor variations and the applicant's response to the criteria (see Attachment 2, Sheets 3 and 4).

DRB Conclusions: The DRB concluded that the proposed minor variations meet the criteria in KZC Section 142.37.4 and that the minor variation results in superior design by helping to create a pedestrian-oriented façade along NE 85th Street. The DRB concluded that superior design elements were the materials used on the NE 85th Street facades (including brick, metal panel siding, architectural concrete, and metal canopies), cornice detailing, the amount of glazing used on the NE 85th Street facades, the revised plaza design, and pedestrian amenities (including seating and potted plants).

The minor variation is supported by Section 5 of the Design Guidelines (Building Location and Orientation) – East End NE 85th Street Frontage, which encourages locating and orienting buildings towards the street with parking to the side or the rear, primary building entries facing the street, façades with transparent windows, and weather protection along the facades.

Additionally, the DRB found that the reduction will not have a substantial detrimental effect on nearby properties and the City or the neighborhood. The reduction is adjacent to NE 85th Street and over 100 feet from the nearest building (two office building on the south side of NE 85th Street) and the reduction results in a superior experience for pedestrians on NE 85th Street.

B. BUILDING MASSING, ARCHITECTURAL AND HUMAN SCALE

DRB Discussion:

The DRB agreed with the applicant's preferred massing model for the site from the Conceptual Design Conference. The applicant's preferred design included an additional 15-foot setback from the north property line (for a total of 30 feet). The zoning code limits any structure within 30 feet of the north property line adjacent to single family residential uses to 15 feet in height if the structure exceeds 50 feet in the width. The applicant chose to keep the entire structure back 30 feet from the north property line.

After review of the plans and listening to public comments, the DRB was concerned about the visual impacts of the north and east façades. They requested that the applicant increase the amount of vertical and horizontal modulation along these facades, decrease the number of windows and balconies along the north façade and treatment of the parking garage along NE 132nd Street. Additionally, the DRB emphasized the importance of the entry gateway feature and the design of the highly visible NE 85th Street and 132nd Avenue NE street corner. Over the next three meetings, the DRB provided feedback on the applicant's responses to the Board's direction.

DRB Conclusions:

The DRB concluded, with conditions, that the proposed buildings are consistent with the applicable Design Guidelines for Rose Hill Business District and the Design Guidelines for Residential Development. The DRB concluded that the additional setback from the northern property line and the reduction in the number of balconies and windows along the north facade helped to reduce impacts on neighboring residential properties.

The DRB agreed that the following design changes throughout the process were successful in addressing the concerns regarding architectural scale (through vertical and horizontal modulation), blank wall treatment, street corner design and entry gateway features:

- Treatment of the main building facades with small recesses, residential scale windows, and varying roofline modulation.
- The incorporation of balconies, changes in building color and materials, and vertical building modulation based on individual units.

- The southeast street corner of the main building utilizes a recessed building entry, pedestrian-oriented space with seating, landscaping, and an entry gateway feature to enhance the appearance of the highly visible location.
- The entry gateway feature utilizes a vertical rose sculpture to enhance the character and identity of the Rose Hill Business District.
- Treatment of the blank parking garage walls along 132nd Avenue NE with landscaping between the building and the sidewalk.

During the DRB's deliberation, the DRB discussed how the second story balcony at the southwest corner of the main building did not fit the commercial design of the building and should be designed to match the southeast corner of the building. The applicant agreed to the design change. As a result, the DRB approval includes a condition that as part of the application for the building permit, the applicant should submit revised plans that show the second story balcony at the southwest corner of the main building as being removed. The southwest corner of the main building should be redesigned to match the design of the main building's southeast corner.

C. VEHICULAR AND PEDESTRIAN ACCESS

DRB Discussion:

Staff provided the DRB background information regarding vehicular access. Vehicular access to and from the property is limited. City guidelines prohibit access from NE 85th Street and the driveway off 132nd Avenue NE will be restricted to right-turn in and out with a median barrier. The Public Works Department approved a driveway modification to allow two driveways off 131st Avenue NE and to allow the driveway accessing the surface parking lot to be less than 75 feet from the intersection of NE 85th Street and 131st Avenue NE. Additional concerns regarding traffic impacts were addressed through the SEPA Process.

Therefore, at the meetings the DRB focused their discussion on pedestrian access to and from the buildings and the adjacent streets, pedestrian amenities located onsite and along NE 85th Street, and the design of the pedestrian plaza located between the standalone commercial and main building along NE 85th Street. Additionally, site lighting was discussed.

Along NE 85th Street, the DRB was concerned that the initial landscaping plan for the large planter strip created too much of a "tunnel effect" for pedestrians and requested a reduction in the width of planter strip and wider sidewalks. They also discussed the need for pedestrian seating areas and planters along the commercial building façade. In regard to the plaza area, the DRB felt that the area had too much landscaping and not enough hardscape to provide for amenities including seating areas.

DRB Conclusions:

The DRB concluded that the proposed plans for the site are consistent with the applicable design guidelines. The DRB agreed that the following design changes throughout the process were successful in addressing the concerns regarding pedestrian access to and around the site, pedestrian amenities, commercial plaza, and lighting throughout the site:

- Widening of the sidewalk along NE 85th Street and a decrease in the width of the landscape strip.
- The addition of planters and seating along NE 85th Street to create a more pedestrian friendly building façade.
- The revised plaza design (with the increase in paved areas) adds additional pedestrian amenities that enrich the pedestrian environment and increases pedestrian activity in the area. The plaza area also provides a small gathering area for commercial customers and tenants, residents and their guests, and transit riders.
- The submitted lighting plan enhances pedestrian safety, creates inviting pedestrian area and provide adequate lighting without creating excessive glare or light levels. As part of the building permit application, the applicant will be required to submit a lighting plan that shows compliance with the requirements of KZC Section 115.85.2 (Exterior Lighting Requirements for the Rose Hill Business District).

C. LANDSCAPING

DRB Discussion:

The DRB discussed the need for landscaping to help soften building massing, screen the parking garage blank wall along 132nd Avenue NE, enhance the pedestrian experience, and provide visual interest. Opportunity areas discussed for landscaping included the residential building courtyard, along the NE 85th façade of the commercial building, plaza area, entry gateway area, and along the west, north and east facades of the main building. The DRB expressed an interest in the landscaping providing year round screening of the building and year around interest. The DRB also discussed the future impacts of a future 132nd Avenue turn lane on the entry gateway area and the landscaping along 132nd Avenue NE. The DRB did not provide direction on this topic since the street improvements and timing were uncertain.

DRB Conclusions:

The DRB concluded that the project was consistent with the guidelines relating to landscaping.

D. BUILDING MATERIALS, COLOR AND DETAIL

DRB Discussion:

Throughout the design review process, the DRB evaluated the proposed materials and colors. The DRB requested that the applicant increase the texture and interest of the masonry material on the commercial façade, incorporate material changes to the parapets and cornices on the commercial façade, and requested that lap siding on the commercial portions of the structure be replaced with material with a more commercial aesthetic. For the residential portion, the DRB was accepting of the materials, color, and details.

The DRB was accepting of the materials that the applicant chose for the final design of the commercial spaces including modular brick, metal panel siding, and architectural concrete. The DRB also ensured that the project utilized materials and color changes on the residential portion to help mitigate building massing.

DRB Conclusions:

The DRB concluded that the project was consistent with the guidelines relating to building materials, colors, and details.

IV. STATE ENVIRONMENTAL POLICY ACT (SEPA) AND CONCURRENCY

The City issued a SEPA Determination of Nonsignificance on December 2, 2019 for the project. Multiple appeals of the determination were filed within the appeal period.

Pursuant to KMC 24.02.230(f), if a land use permit does not include an open record public hearing but provides for an open record appeal (such as Design Review Board and Process I decisions), the SEPA appeal will be consolidated with the open record appeal and decided upon by the hearing examiner. A timely SEPA appeal will be placed on hold until the City's final decision on the underlying permit is issued. Then, if the underlying permit decision is appealed administratively, both appeals will be decided at a consolidated open record appeal hearing. If the underlying permit decision is not appealed, then there will be no administrative SEPA appeal available and judicial appeal procedures may be followed.

V. DEVELOPMENT REVIEW COMMITTEE

Comments and requirements placed on the project by City departments are found on the Development Standards, Attachment 3.

VI. SUBSEQUENT MODIFICATIONS

Modifications to the approval may be requested and reviewed pursuant to the applicable modification procedures and criteria in effect at the time of the requested modification.

VII. APPEALS OF DESIGN REVIEW BOARD DECISIONS AND LAPSE OF APPROVAL

A. Appeals

Section 142.40 of the Zoning Code allows the Design Review Board's decision to be appealed to the Hearing Examiner by the applicant or any person who submitted written or oral comments to the Design Review Board. The appeal must be in the form of a letter of appeal and must be delivered, along with any fees set by ordinance, to the Planning and Building Department by 5:00 p.m., March 5, 2020, fourteen (14) calendar days following the postmarked date of distribution of the Design Review Board's decision.

Only those issues under the authority of the Design Review Board as established by Kirkland Zoning Code 142.35(3) are subject to appeal.

B. Lapse of Approval

The applicant must begin construction or submit to the City a complete building permit application for the development activity, use of land or other actions approved under this chapter within five (5) years after the final approval of the City of Kirkland on the matter, or the decision becomes void.

The applicant must substantially complete construction for the development activity, use of land or other actions approved under this chapter and complete the applicable conditions listed on the notice of decision within seven (7) years after the final approval on the matter or the decision becomes void.

VIII. ATTACHMENTS

1. Vicinity Map
2. Plans dated January 6, 2020
3. Development Standards
4. Public Comments

IX. PARTIES

A list of parties that submitted written or oral comments to the DRB have been attached to file no. DRV18-00312.

Page 11

X. APPROVAL



Chair, Design Review Board

Date: 2/14/2020

Enclosure 2: Design Review Board Decision and Attachments can be found here:

Part 1 (10.3 MB):

https://www.kirklandwa.gov/Assets/Planning/Planning+PDFs/Design+Review+Board/Continental+Divide+Mixed-Use+Project+DRB+Decision+-+DRV18-00312_Part1.pdf

Part 2 (10.2 MB):

https://www.kirklandwa.gov/Assets/Planning/Planning+PDFs/Design+Review+Board/Continental+Divide+Mixed-Use+Project+DRB+Decision+-+DRV18-00312_Part2.pdf

Part 3 (971 kB):

https://www.kirklandwa.gov/Assets/Planning/Planning+PDFs/Design+Review+Board/Continental+Divide+Mixed-Use+Project+DRB+Decision+-+DRV18-00312_Part3.pdf

Attachment 2 (Approved Plans) has been included in the packet for reference ease.



DESIGN RESPONSE CONFERENCE

**8505 132nd Avenue NE
Kirkland, WA**

January 6, 2020



1. SETBACK MODIFICATION..... 3

- Submit an updated formal setback modification request. The request should address the criteria in KZC Section 142.37.
 - a. Also include a site plan that highlights all the encroachments.

2. LIGHTING PLAN..... 5

- Submit a lighting plan that addresses the Design Guidelines contained in Section 9 - Lighting.

3. COORDINATION 8

- Ensure that all plans are coordinated throughout the entire packet including landscape plans.
 - a. We would strongly recommend having the landscape architect at the next meeting.

4. ELEVATIONS..... 12

- Provide elevations for all facades for each building including the north facade of the standalone commercial building.

5. SE STREET CORNER..... 20

- The design of the southeast street corner needs to ensure compliance with Design Guideline 3D.
 - a. The board requested that the lap siding be replaced with a more commercial material
 - b. Include the proposed artwork in elevation drawings.

6. LANDSCAPE, SIDEWALK, AMENITIES..... 23

- Look at reduction in the width of the landscape strip along NE 85th Street, an increase in the sidewalk width, and including more pedestrian amenities and planters along the building facades. See Design Guideline 10 for ideas.

7. PLAZA DEVELOPMENT 28

- More development of the plaza area. They would like to see more hardscape and less landscaping in the area north of the bus stop and between the two buildings. See Design Guideline Section 12 for ideas.

8. COMMERCIAL FACADE DEVELOPMENT 31

- Additional development of the standalone commercial facade to create a superior design to offset the modification request. Ideas include material changes on the parapets and cornice treatments. We recommend looking at the Hectors Building on Lake Street and the Park Lane Public House for some ideas.
 - a. Also address any blank walls on the backside of the building (See Design Guideline Section 8)

9. FINAL PACKET 34

- Incorporate any plans that were submitted at the meeting into the final packet.

1. SETBACK MODIFICATION

- Submit an updated formal setback modification request. The request should address the criteria in KZC Section 142.37.
 - a. Also include a site plan that highlights all the encroachments.

KZC 142.37 DESIGN DEPARTURE AND MINOR VARIATIONS

4. CRITERIA – The Design Review Board may grant a design departure or minor variation only if it finds that all of the following requirements are met:
- a. The request results in superior design and fulfills the policy basis for the applicable design regulations and design guidelines;
 - b. The departure will not have any substantial detrimental effect on nearby properties and the City or the neighborhood.

BACKGROUND:

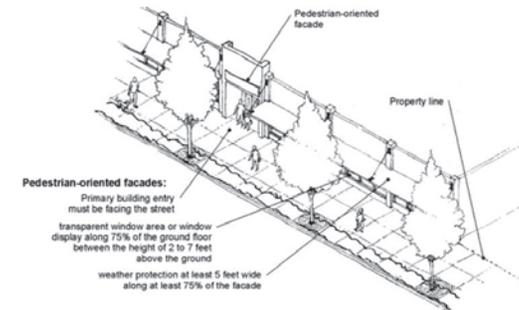
There were originally 4 Minor Variation Setback Requests (balconies facing 131st and 132nd, the NW driveway, and the buildings facing 85th Street). As shown by the red dashed line on the accompanying site plan, the balconies and driveway all comply with the prescribed setbacks and therefore no Design Departures or Minor Variations for these features are requested.

BUILDING PLACEMENT NE 85TH STREET FRONTAGE:

Criteria 4.a.: The most relevant aspect of design with regard to this request is the placement itself of the buildings on the site plan. The question regarding building placement on the NE 85th Street Frontage is,

“GIVEN THE 3 OPTIONS IN FIGURE 19, PAGE 15 OF THE DESIGN GUIDELINES FOR ROSE HILL EAST END NE 85TH STREET FRONTAGE, WHICH IS A SUPERIOR DESIGN TO THE STATED SETBACK REQUIREMENT?”

Considering that the goal of the policy basis is to create pedestrian friendly storefronts on NE 85th Street, the top option (shown below) is the one that conforms with best planning practices to have the storefronts right at the edge of the sidewalk. That is, at the property line where the pedestrian interface occurs; not setback from the property line. Note that in this Design Guideline recommended option, the building is placed such that the storefronts are at the property line.



CONCLUSION CRITERIA 4.A.:

The request does result in a superior design and fulfills the policy basis for the applicable design regulations and design guidelines. It is specifically implementing a recommended and preferred street frontage option directly from the Design Guidelines for Rose Hill East End NE 85th Street Frontage (page 15).

CONCLUSION CRITERIA 4.B.:

The departure request actually produces a better pedestrian experience and a more viable business environment. It will not have any detrimental (let alone substantial) effect on nearby properties, the City, or the neighborhood.

ITEM 1 | SITE PLAN / LEVEL 1 FLOOR PLAN



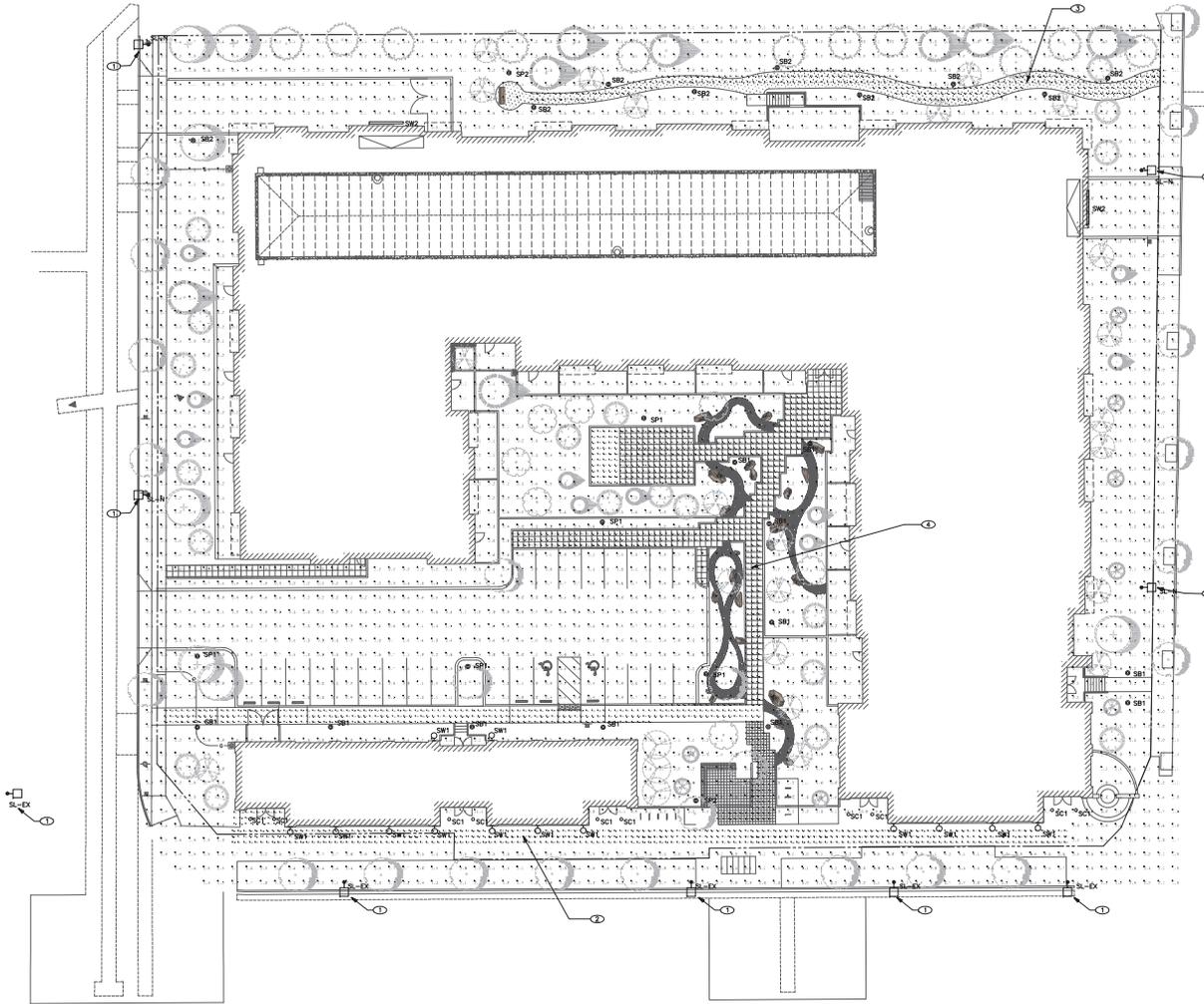
2. LIGHTING PLAN

- Submit a lighting plan that addresses the Design Guidelines contained in Section 9 - Lighting.

DESIGN GUIDELINES SECTION 9 - LIGHTING

- a. Provide adequate lighting levels in all areas used by pedestrians and automobiles, including building entries, walkways, parking areas, circulation areas, and open spaces. Recommended minimum light levels:
 - Building entries: 4 foot candles
 - Primary pedestrian walkway: 2 foot candles
 - Secondary pedestrian walkway: 1-2 foot candles
 - Parking lot: .60 -1 foot candle
 - Enclosed parking garages for common use: 3 foot candles
- b. Lighting should be provided at consistent levels, with gradual transitions between maximum and minimum levels of lighting and between lit areas and unlit areas.
- c. Building facades in pedestrian areas should provide lighting to walkways and sidewalks through building mounted lights, canopy- or awning-mounted lights, and display window lights. Encourage variety in the use of buildingmounted light fixtures to give visual variety from one facade to the next.
- d. Minimizing impacts of lighting on adjoining activities and uses should be considered in the design of lighting. This is particularly important adjacent to residential uses.
- e. Parking lot light fixtures should be non-glare and mounted no more than 15'-20' above the ground. Lower level light fixtures are preferred to maintain a human scale. Ideally, all exterior fixtures should be fitted with a full cut-off shield to minimize light spill over onto adjoining properties

ITEM 2 | SITE PLAN - LIGHTING



Tag	Description	Qty	Lum. Watts	Lum. Lumens
SB1	42-IN BOLLARD	11	13.2	671
SB2	22-IN PATHLIGHT	8	9	350
SC1	SURFACE MOUNTED DOWNLIGHT - MOUNTED @ 10' AFF	10	8,416	759
SL-EX	EXISTING STREET LIGHT - MOUNTED @ 32' AFF	5	176	17716
SL-N	NEW STREET LIGHT - MOUNTED @ 32' AFF	4	111	13000
SP1	POST-TOP POLE MOUNTED LUMINAIRE (TYPE 3) - MOUNTED @ 16' AFF	5	36	3134
SP2	POST-TOP POLE MOUNTED LUMINAIRE (TYPE 5) - MOUNTED @ 16' AFF	3	36	3370
SW1	DECORATIVE WALL SCONCE - MOUNTED @ 10' AFF	14	9.43	919
SW2	WALL MOUNTED LINEAR DOWNLIGHT - MOUNTED @ 10' AFF	2	19.1	2000

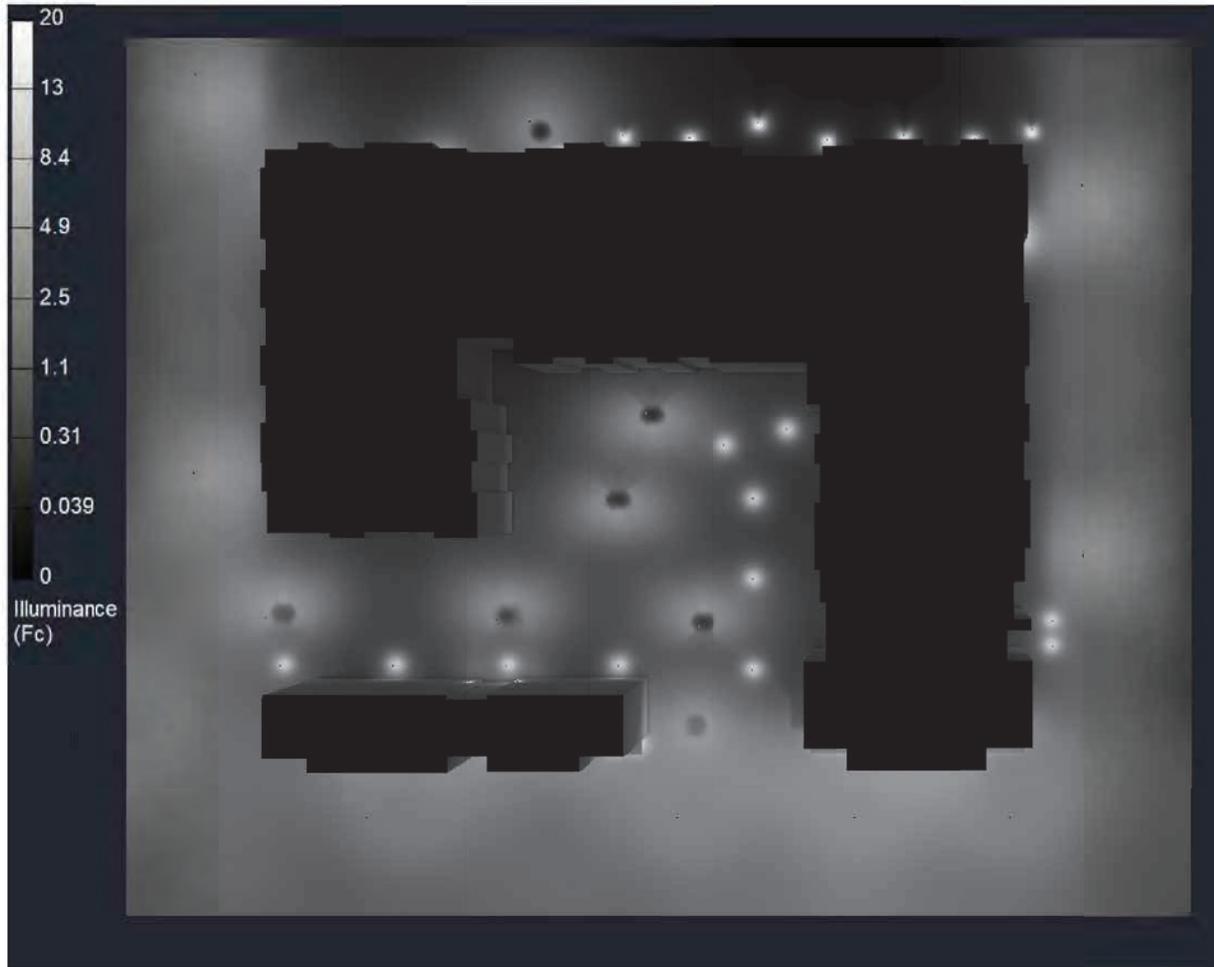
ROSE HILL LIGHTING DESIGN GUIDELINES

- BUILDING ENTRIES: 4FC MINIMUM AVERAGE
- PRIMARY PEDESTRIAN WALKWAYS: 2FC MINIMUM AVERAGE
- SECONDARY PEDESTRIAN WALKWAYS: 1-2FC MINIMUM AVERAGE
- PARKING LOTS: .60-1FC MINIMUM AVERAGE

Label	CalcType	Units	Avg	Max	Min	Avg/Min	Max/Min
OVERALL SITE	Illuminance	Fc	1.16	17.3	0.0	N.A.	N.A.
PRIMARY WALKWAY (SIDEWALK)	Illuminance	Fc	3.33	6.4	1.9	1.75	3.37
SECONDARY WALKWAY (COURTYARD)	Illuminance	Fc	1.39	9.9	0.1	13.90	99.00
SECONDARY WALKWAY (GARDEN PATH)	Illuminance	Fc	1.00	17.9	0.0	N.A.	N.A.
TYPICAL BUILDING ENTRY	Illuminance	Fc	10.52	14.6	4.9	2.15	2.98
PARKING LOT	Illuminance	Fc	0.94	3.6	0.2	4.70	18.00
SECONDARY PEDESTRIAN WALKWAY 2	Illuminance	Fc	1.05	17.9	0.0	N.A.	N.A.

1 SITE PLAN - LIGHTING
SCALE: 1/16"=1'-0"

ITEM 2 | SITE PLAN - PHOTOMETRIC



GLUMAC
engineers for a sustainable future

1501 Fifth Ave, Suite 2210
Seattle, WA 98101
T: 206.262.1010 www.glumac.com
Project Manager: Alex Martin
Engineer/Designer:
Job Number: 05.18.00547

Issue	Date	Description

POLY LINE
CONTINENTAL DIVIDE
KIRKLAND, WA

SHEET TITLE
SITE PLAN - PHOTOMETRIC

SCALE
AS NOTED

DATE
12.16.2019

SHEET NUMBER

1 SITE PLAN - PHOTOMETRIC
SCALE: NTS

3. COORDINATION

- Provide Ensure that all plans are coordinated throughout the entire packet including landscape plans.
 - a. We would strongly recommend having the landscape architect at the next meeting.

PLANS, INCLUDING LANDSCAPE PLANS, HAVE BEEN COORDINATED PER COMMENT 3.

LANDSCAPE ARCHITECT IN ATTENDANCE.

ITEM 3 | LANDSCAPE MASTER PLAN



Rose Hill Business District Design Guideline #22:

Continental Divide's landscape planting enhances the visual quality of the urban environment. The site contains pedestrian/auto, pedestrian, and building-oriented landscapes. Pedestrian/auto landscapes focus on robust plantings to protect pedestrians from traffic with street trees, creating a more hospitable environment. The pedestrian landscape emphasizes plant materials that provide color, texture, shape, and year-round interest. Finally, the building landscape serves to compliment the building while screening any faults and maintaining views. Over the site there is a colorful mix of drought-tolerant and low-maintenance plantings including roses in many highly visible locations.

(Design Guidelines for Rose Hill Business District - <https://www.kirklandwa.gov/Assets/Planning/Planning+PDFs/Rose+Hill+Design+Guidelines.pdf>)

Land Use Buffer Summary:

The applicant shall provide a 15' wide landscape strip to be planted with trees spaced at a rate of 1 tree every 20'. Deciduous trees are to be 2.5" caliper minimum and coniferous trees are to be 8" minimum in height, and at least 70% of trees shall be evergreen. A mix of various shrubs are to be planted to obtain at least 60% coverage within 2 years.

(KZC 95.42.1 Minimum Land Use Buffer Requirements)

Landscape plan data:

Types of plantings: The site is composed of a mix of evergreen and deciduous trees, shrubs, and groundcover to provide year-round structure and interest. While various grasses and perennials add seasonal interest and character. A few prominent plants in the proposed landscape plan are: Vine Maple, Vanderwolf Pine, Western Red Cedar, Western Hemlock, Red & Yellow Twig Dogwood, Rugosa Rose, Flowering Currant, Japanese Pieris, Mountain Laurel, Viburnum, Switch Grass, Lavender, Coreopsis, and Creeping Mahonia, among many others.

Proposed landscape plan by the numbers:

- Trees: 119 - Avg. size: 2" cal. / 8' high min.
- Shrubs/Grasses: 2,633 - Avg. size: Shrub 5-gal / Grass 2-gal
- Groundcover: 2,528 - Avg. size: 4" pot
- Total number of plants on site: 5,280
- Evergreen tree coverage in 15' buffer (Min. 70% Req.): 87.2%

Plant totals represented are approximate and are subject to change

- (A) PERMEABLE PAVER PLAZA
- (B) BUS SHELTER
- (C) ENTRY GATEWAY FEATURE
- (D) PEDESTRIAN PATHWAY
- (E) GARAGE ENTRY
- (F) APARTMENT AMENITY SPACE
- (G) LANDSCAPE PLANTING, TYP.



ITEM 3 | PLAZA DEVELOPMENT



- (A) PERMEABLE PAVER PLAZA
- (B) BICYCLE RACKS
- (C) BENCH SEATING
- (D) BUS SHELTER
- (E) LANDSCAPE PLANTING
- (F) PEDESTRIAN WALK



PEDESTRIAN AMENITIES

The site frontage and plaza along NE 85th creates an active and comfortable pedestrian environment that incorporates many amenities such as various seating options, mixed planting beds of trees, shrubs, and groundcover creating year-round interest, bicycle racks, lighting, varied paving types defining spaces and adding interest, as well as easy access to commercial spaces and public transportation.

DECIDUOUS TREES



ARMSTRONG MAPLE



VINE MAPLE



CALLERY PEAR



IN SPRING



CORAL BARK MAPLE



IN SPRING

EVERGREEN TREES



VANDERWOLF PINE



WEEPING ALASKA CEDAR

GROUND COVER



CREEPING THYME



LILY TURF

SHADE PLANTINGS



WESTERN RED CEDAR



WESTERN HEMLOCK



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BUNCHBERRY DOGWOOD

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MOUNTAIN LAUREL



HEAVENLY BAMBOO



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SHRUBS + GRASSES



BLUE OAT + LAVENDAR



DWARF FOUNTAIN GRASS



MOONLIGHT TICKSEED



RUGOSA ROSE



FLOWERING CURRANT



NINEBARK



4. ELEVATIONS

- Provide elevations for all facades for each building including the north facade of the standalone commercial building.

SEE FOLLOWING ELEVATIONS.

ITEM 4 | SITE PLAN / LEVEL 1 FLOOR PLAN



ITEM 4 | MATERIALS & ELEVATIONS



^ EAST ELEVATION



^ SOUTH ELEVATION

ITEM 4 | MATERIALS & ELEVATIONS



^ WEST ELEVATION



^ NORTH ELEVATION

ITEM 4 | MATERIALS & ELEVATIONS



^ NORTH OFFICE ELEVATION

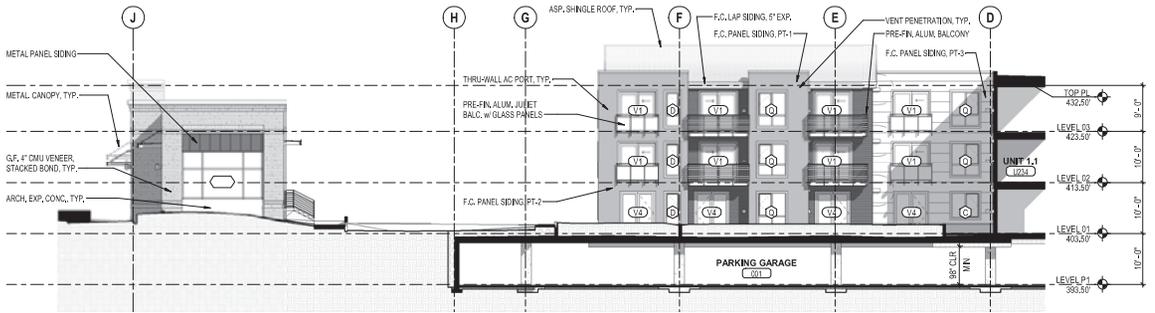


^ EAST OFFICE ELEVATION

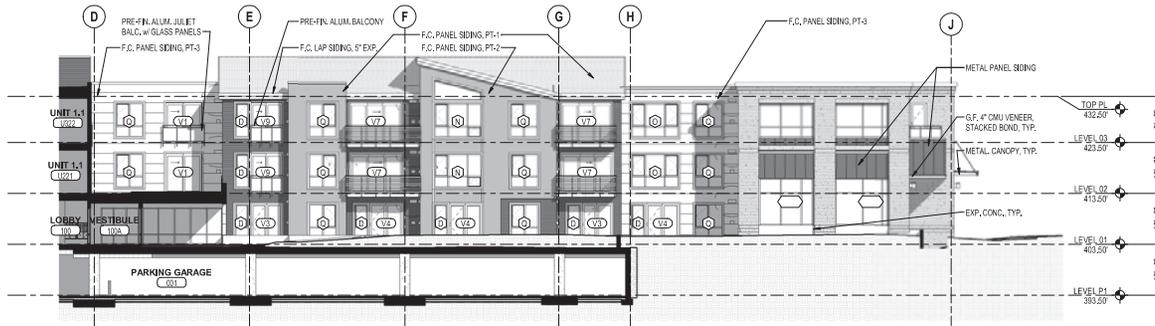


^ WEST OFFICE ELEVATION

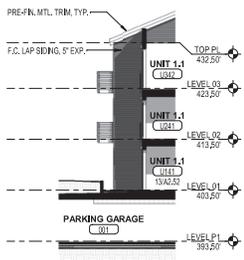
ITEM 4 | ELEVATIONS



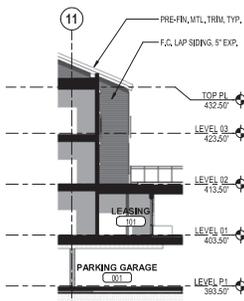
^ 5-EAST COURTYARD ELEVATION



^ 6-WEST COURTYARD ELEVATION



^ 7-EAST COURTYARD INSIDE CORNER ELEVATION

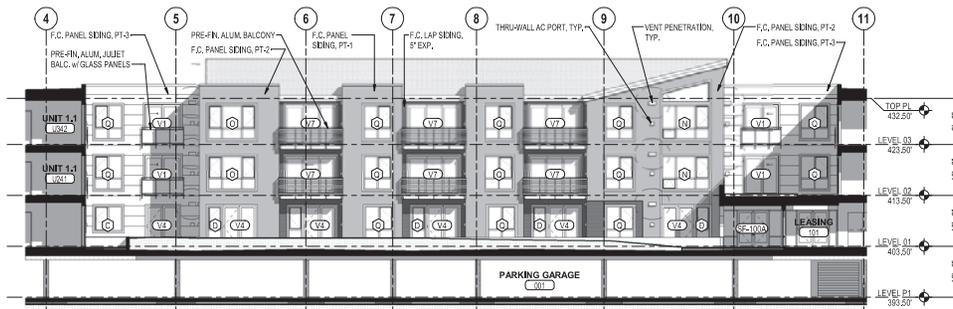


^ 8-WEST COURTYARD INSIDE CORNER ELEVATION

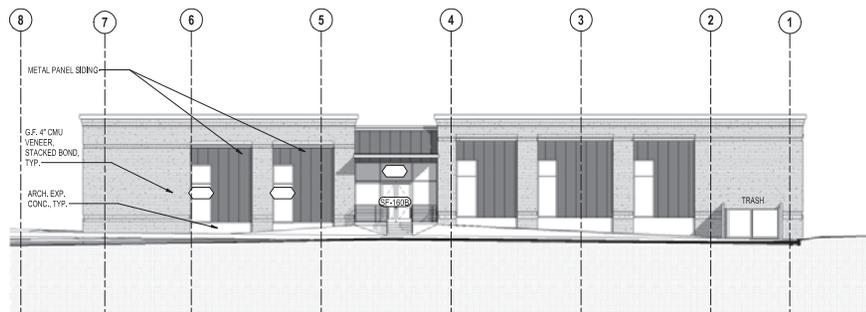
ITEM 4 | ELEVATIONS



^ 9-SOUTH PARTIAL ELEVATION



^ 10-SOUTH COURTYARD ELEVATION



^ 11-NORTH OFFICE ELEVATION

PELLETIER + SCHAAR ARCHITECTS

5. SE STREET CORNER

- The design of the southeast street corner needs to ensure compliance with Design Guideline 3D.
 - a. The board requested that the lap siding be replaced with a more commercial material
 - b. Include the proposed artwork in elevation drawings.

DESIGN GUIDELINE #3:

Objective: Encourage all buildings located at or near street corner to incorporate special architectural elements that add visual interest and provide a sense of human proportion and scale. This could include a raised roofline, turret, corner balconies, bay windows, special awning or canopy design, and/or distinctive use of building materials

Incorporate entry gateway features in new development on NE 85th St. at 120th AND 132nd

Avenues. Gateway features should include the following:

- Distinctive landscaping including an assortment of varieties of roses
- Artwork (e.g. vertical sculpture)
- A gateway sign with the city logo
- Multicolored masonry forming a base for an entry sign
- Decorative lighting elements

3d. Encourage all buildings located at or near street corner to incorporate special architectural elements that add visual interest and provide a sense of human proportion and scale. This could include a raised roofline, turret, corner balconies, bay windows, special awning or canopy design, and/or distinctive use of building materials (see the following examples).

ITEM 5 | ROSE HILL NEIGHBORHOOD GATEWAY



View of Corner of NE 85th St. & 132nd Ave. NE

ITEM 5 | MATERIALS & ELEVATIONS



A
METAL PANEL SIDING
DARK GREY



B
MODULAR BRICK
MUTUAL MATERIALS "PEWTER"



C
ARCHITECTURAL CONCRETE
FINE / SACKED FINISH



D
STOREFRONT
BLACK ANODIZED

^ PARTIAL ELEVATION @ COMMERCIAL SPACES

6. LANDSCAPE, SIDEWALK, AMENITIES

- Look at reduction in the width of the landscape strip along NE 85th Street, an increase in the sidewalk width, and including more pedestrian amenities and planters along the building facades. See Design Guideline 10 for ideas.

DESIGN GUIDELINE #10:

Provide pedestrian amenities along all sidewalks, interior pathways, and within plazas and other open spaces. Desired amenities include:

- a. Pedestrian-scaled lighting (placed between 12'-15' above the ground).
- b. Seating space. This can include benches, steps, railings and planting ledges. Heights between 12" to 20" above the ground are acceptable, with 16" to 18" preferred. An appropriate seat width ranges from 6" to 24".
- c. Pedestrian furniture such as trash receptacles, consolidated newspaper racks, bicycle racks, and drinking fountains.
- d. Planting beds and/or potted plants.
- e. Unit paving such as stones, bricks, or tiles.
- f. Decorative pavement patterns and tree grates.
- g. Water features.
- h. Informational kiosks.
- i. Transit shelters.
- j. Decorative clocks.
- k. Artwork.

ITEM 6 | NW DRIVEWAY SECTION



^ NW DRIVEWAY SECTION

- Tertiary Access to the Site:
- Tertiary egress from parking garage, one way traffic.
 - Access only to minor, unclassified street.



ITEM 6 | LANDSCAPE MASTER PLAN



- (A) PERMEABLE PAVER PLAZA (B) BUS SHELTER (C) ENTRY GATEWAY FEATURE (D) PEDESTRIAN PATHWAY
- (E) GARAGE ENTRY (F) APARTMENT AMENITY SPACE (G) LANDSCAPE PLANTING, TYP.

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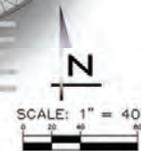
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ITEM 6 | PLAZA DEVELOPMENT



- (A) PERMEABLE PAVER PLAZA
- (B) BICYCLE RACKS
- (C) BENCH SEATING
- (D) BUS SHELTER
- (E) LANDSCAPE PLANTING
- (F) PEDESTRIAN WALK



PEDESTRIAN AMENITIES

The site frontage and plaza along NE 85th creates an active and comfortable pedestrian environment that incorporates many amenities such as various seating options, mixed planting beds of trees, shrubs, and groundcover creating year-round interest, bicycle racks, lighting, varied paving types defining spaces and adding interest, as well as easy access to commercial spaces and public transportation.

DECIDUOUS TREES



ARMSTRONG MAPLE



VINE MAPLE



CALLERY PEAR



IN SPRING



CORAL BARK MAPLE



IN SPRING

EVERGREEN TREES



VANDERWOLF PINE



WEeping ALASKA CEDAR

GROUND COVER



CREeping THYME



LILY TURF

SHADE PLANTINGS



WESTERN RED CEDAR



WESTERN HEMLOCK



PACHYSANDRA



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SPRING BOUQUET VIBURNUM



RHODODENDRON

SHRUBS + GRASSES



BLUE OAT + LAVENDAR



DWARF FOUNTAIN GRASS



MOONLIGHT TICKSEED



RUGOSA ROSE



FLOWERING CURRANT



NINEBARK



7. PLAZA DEVELOPMENT

- More development of the plaza area. They would like to see more hardscape and less landscaping in the area north of the bus stop and between the two buildings. See Design Guideline Section 12 for ideas. Blueline to handle considering max impervious surfaces as well.

DESIGN GUIDELINE #12:

Objectives

- To provide a variety of pedestrian-oriented areas to attract shoppers to commercial areas and enrich the pedestrian environment.
- To create gathering spaces for the community.
- To configure buildings and uses to encourage pedestrian activity and pedestrian focal points.

Guidelines

- a. Provide pedestrian plazas in conjunction with nonresidential uses.
- b. Position plazas in visible locations on major internal circulation routes, close to bus stops, or where there are strong pedestrian flows on neighboring sidewalks. For large sites, development should be configured to create a focal plaza or plazas. Plazas should be no more than 3' above or below the adjacent sidewalk or internal pathway to enhance visibility and accessibility.
- c. Incorporate plenty of benches, steps, and ledges for seating. A combination of permanent and moveable seating is encouraged. Seating areas should be provided with views of amenities, landscaping elements, or people watching.
- d. Provide storefronts, street vendors, or other pedestrian-oriented uses, to the extent possible, around the perimeter of the plaza
- e. Provide landscaping elements that add color and seasonal interest. This can include trees, planting beds, potted plants, trellises, and hanging plants.
- f. Incorporate pedestrian amenities, as described in Section 10.
- g. Consider the solar orientation and wind patterns in the design of the open space and choice of landscaping.
- h. Provide transitional zones along building edges to allow for outdoor eating areas and a planted buffer.

ITEM 7 | PLAZA DEVELOPMENT



- (A) PERMEABLE PAVER PLAZA
- (B) BICYCLE RACKS
- (C) BENCH SEATING
- (D) BUS SHELTER
- (E) LANDSCAPE PLANTING
- (F) PEDESTRIAN WALK



PEDESTRIAN AMENITIES

The site frontage and plaza along NE 85th creates an active and comfortable pedestrian environment that incorporates many amenities such as various seating options, mixed planting beds of trees, shrubs, and groundcover creating year-round interest, bicycle racks, lighting, varied paving types defining spaces and adding interest as well as easy access to commercial spaces and public transportation.

DECIDUOUS TREES



ARMSTRONG MAPLE



VINE MAPLE



CALLERY PEAR



IN SPRING



CORAL BARK MAPLE



IN SPRING

EVERGREEN TREES



VANDERWOLF PINE



WEeping ALASKA CEDAR

GROUND COVER



CREeping THYME



LILY TURF

SHADE PLANTINGS



WESTERN RED CEDAR



WESTERN HEMLOCK



PACHYSANDRA



BUNCHBERRY DOGWOOD

EVERGREEN SHRUBS



MOUNTAIN LAUREL



HEAVENLY BAMBOO



SPRING BOUQUET VIBURNUM



RHODODENDRON

SHRUBS + GRASSES



BLUE OAT + LAVENDAR



DWARF FOUNTAIN GRASS



MOONLIGHT TICKSEED



RUGOSA ROSE



FLOWERING CURRANT



NINEBARK

8. COMMERCIAL FACADE DEVELOPMENT

- Additional development of the standalone commercial facade to create a superior design to offset the modification request. Ideas include material changes on the parapets and cornice treatments. We recommend looking at the Hectors Building on Lake Street and the Park Lane Public House for some ideas.
 - a. Also address any blank walls on the backside of the building (See Design Guideline Section 8)

DESIGN GUIDELINE #8:

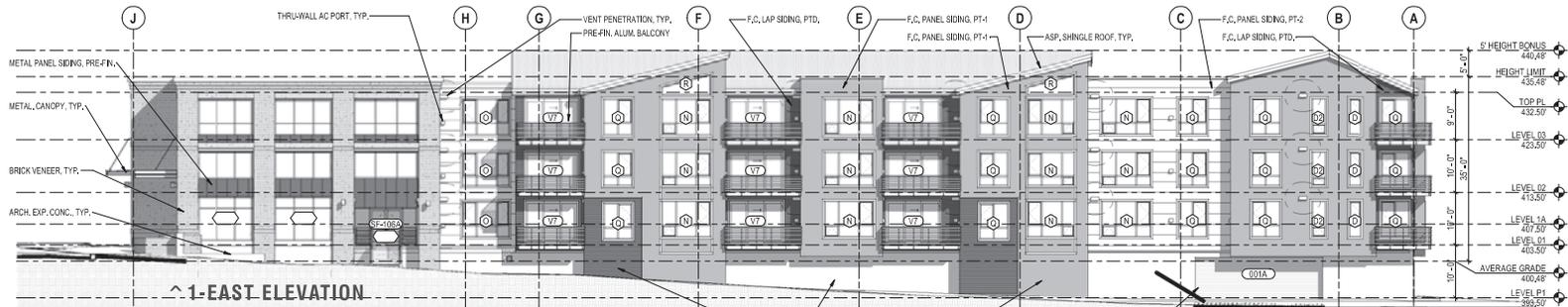
Avoid blank walls near sidewalks, major internal walkways, parks, and pedestrian areas. The following treatments mitigate the negative effects of blank walls (in order of preference):

- a. Configure buildings and uses to minimize blank walls exposed to public view.
- b. Provide a planting bed with plant material to screen most of the wall.
- c. Install trellises with climbing vines or plant materials to cover the surface of the wall. For long walls, a trellis or trellises should be combined with other design treatments to avoid monotony.
- d. Provide artwork on the wall surface.
- e. Provide architectural techniques that add visual interest at a pedestrian scale. This could include a combination of horizontal building modulation, change in building materials and/or color, and use of decorative building materials.
- f. Other treatments may be proposed that meet the intent of the guidelines.

ITEM 8 | COMMERCIAL FACADES



ITEM 8 | BLANK WALLS



9. FINAL PACKET

- Incorporate any plans that were submitted at the meeting into the final packet.

FINAL PACKET HAS BEEN COORDINATED.

PROJECT VISION | SUMMARY

HOUSING FOR A GROWING CITY

Build 134 new Affordable & Market Rate Apartments to meet the growing demand for housing in the City of Kirkland. Kirkland and the Seattle Metro region are growing rapidly, and there is a shortage of affordable housing throughout the region.

ROSE HILL NEIGHBORHOOD GATEWAY

Create a gateway marker with art and landscaping to celebrate the Rose Hill Neighborhood and mark the east entrance to the City of Kirkland at the corner of NE 85th St. and 132nd Ave NE.

NE 85TH ST. PEDESTRIAN EXPERIENCE

Develop a new pedestrian experience for NE 85th St., to make a more walkable neighborhood and set precedence for future projects in the Rose Hill Business District.

NEW PUBLIC & PRIVATE OUTDOOR SPACE

Make new outdoor spaces for the neighborhood along NE 85th St. and within the site for the tenants and residents of the project.

TRANSITION TO RESIDENTIAL SCALE

Design the project with architectural transitions in mass and scale from commercial uses to residential uses along 131st Ave. NE and 132nd Ave. NE.

LANDSCAPING TO ENHANCE THE SITE

Plant extensive landscaping all across the site to protect the privacy of neighbors, enhance the pedestrian experience of public spaces, and provide amenities for the tenants and residents of the project.

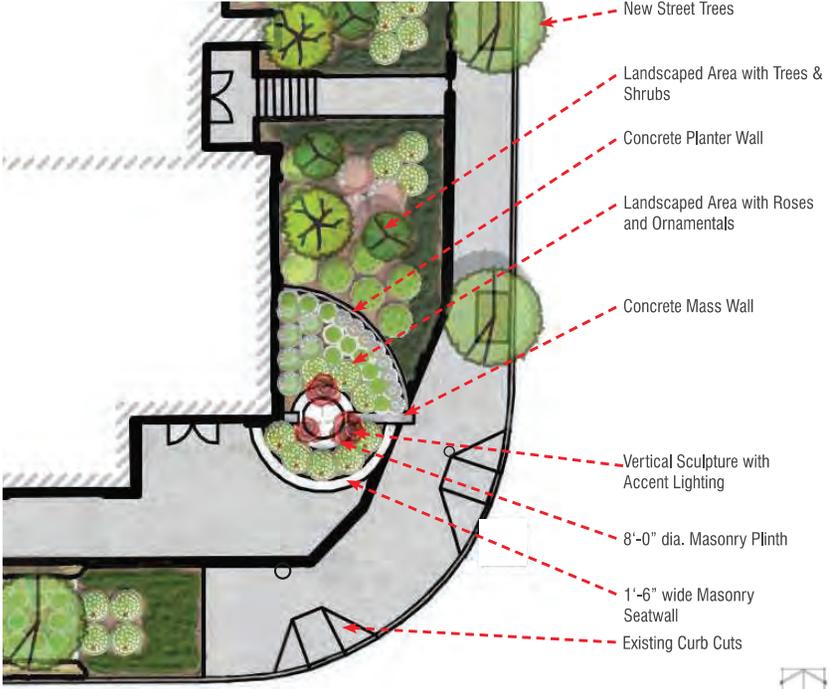


Commercial Space on NE 85th St.

PROJECT DATA

8505 132ND AVE. NE KIRKLAND, WA 98033	LOT AREA:	98,109 SF (2' DEDICATION ON 131ST)	ALLOWABLE LOT COVERAGE:	68,676 SF	CONSTRUCTION TYPE:	I-A / V-A
			ACTUAL LOT COVERAGE:	67,907 SF		(GARAGE / BLDG ABOVE)
PARCEL NO.:	MAX. LOT COVERAGE:	70%	AVERAGE BLDG. ELEVATION:	400.48'	BUILDING AREA:	
LOT 3 - 8635700015					GROUND FLR / P1	54,798
LOT 4 - 8635700020	HEIGHT LIMIT:	35'	REQUIRED PARKING:		LEVEL 1	46,994
LOT 5 - 8635500025		5' BONUS (PEAKED ROOF)	COMMERCIAL	25 STALLS	LEVEL 2	39,520
LOT 6 - 8635500030	MIN. FRONT SETBACK:	10' (0')	RESIDENTIAL	164 (169 - 5)*	LEVEL 3	41,926
LOT 7 - 8635500035		(PED. STORE FRONTS)	GUEST PARKING	16**	TOTAL	183,239 GSF
LOT 8 - 8635700025	MIN. SIDE SETBACK:	20'	TOTAL REQ'D	189		
LOT 9 - 8635700030					COMMERCIAL AREA:	7,378 GSF
LOT 7 BF - 1241900025	MIN. REAR SETBACK:	15' (30')			RESIDENTIAL UNITS:	134
		(35' HT. BLDG @ RSX ZONE)	PARKING PROVIDED:	200 STALLS	AFFORDABLE HOUSING:	13 UNITS
ZONE: RH-8 (ROSE HILL BUSINESS DISTRICT)						

PROJECT VISION | ROSE HILL NEIGHBORHOOD GATEWAY



Plan at Corner of NE 85th St. & 132nd Ave. NE

GATEWAY FEATURES:

- Landscaping, including ornamental shrubs and rose varieties.
- Vertical sculpture (to be designed / selected with community input).
- Feature lighting to highlight artwork.



View of Corner of NE 85th St. & 132nd Ave. NE



View of Corner of NE 85th St. & 132nd Ave. NE (from distance)

DESIGN GUIDELINE #1:

Objective: To enhance the character and identity of the Rose Hill Business District.

Incorporate entry gateway features in new development on NE 85th St. at 120th AND 132nd Avenues. Gateway features should include the following:

- Distinctive landscaping including an assortment of varieties of roses
- Artwork (e.g. vertical sculpture)
- A gateway sign with the city logo
- Multicolored masonry forming a base for an entry sign
- Decorative lighting elements

PROJECT VISION | NE 85TH ST. PEDESTRIAN EXPERIENCE



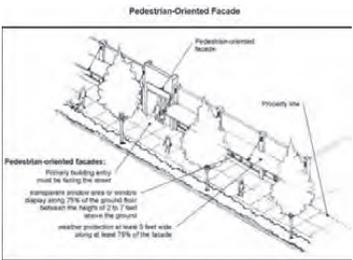
Existing NE 85th St. Streetscape - View Looking West



Existing NE 85th St. Streetscape - View Looking East



NE 85th St. Sidewalk - View Looking East



KZC FIG. 92.10.A

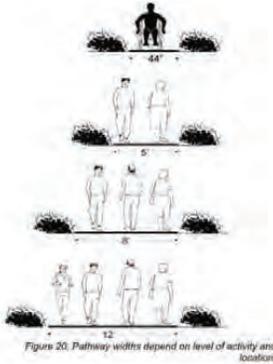
- KZC 92.10.2:**
 Definition of Pedestrian-Oriented Facade:
- Primary entrance on this facade.
 - Transparent windows for 75% of facade between 2' and 7' above sidewalk.
 - Weather protection feature(s) at least 5' wide for 75% of facade.

KZC 92.10.3.a:
 Buildings featuring a Pedestrian-Oriented Facade may be located adjacent to the sidewalk in the RHBD zone

- DESIGN GUIDELINE #4:**
 Objectives:
- To **enhance the pedestrian environment** within the Rose Hill Business District.
 - To **create safe and active sidewalks** and pathways.

Incorporate transparent windows and doors and weather protection features adjacent to a sidewalk or internal pathway. Weather protection features could include awnings, canopies, marquees or other permitted treatments.

PROJECT VISION | NE 85TH ST. PEDESTRIAN EXPERIENCE



Design Guidelines FIG. 20



Figure 21. High-traffic streets without on-street parking warrant wider planting strip buffers

Design Guidelines FIG. 21

DESIGN GUIDELINE #6:

Objectives: To provide wide sidewalks and pathways that promote an increase in pedestrian activity within the Rose Hill Business District.

- Sidewalks or pathways adjacent to moving vehicular traffic need generous buffers to make them safer...
- Design sidewalks and pathways to support a variety and concentration of activities and provide a separation for the pedestrian from the busy street.



Partial Plan - NE 85th St. Sidewalk



Partial Plan - NE 85th St. Sidewalk

PROJECT VISION | NE 85TH ST. PEDESTRIAN EXPERIENCE



Figure 23. Note how these awnings have been integrated into the building's storefront spaces
 Design Guidelines Fig. 23

DESIGN GUIDELINE #7:

- Objectives:
- To provide shelter for pedestrians.
 - To provide spatial enclosure and add design interest to retail or office streetscapes activity.
 - Provide weather protection along the primary exterior entrance of all businesses, residential units, and other buildings.
 - Design weather protection features to provide adequate width and depth at building entries.
 - Pedestrian covering treatments may include: covered porches, overhangs, awnings, canopies, marquees, recessed entries or other similar features. A variety of styles and colors should be considered, where compatible with architectural style of the building and the ground floor use.

PROJECT VISION | NEW PUBLIC & PRIVATE OUTDOOR SPACE

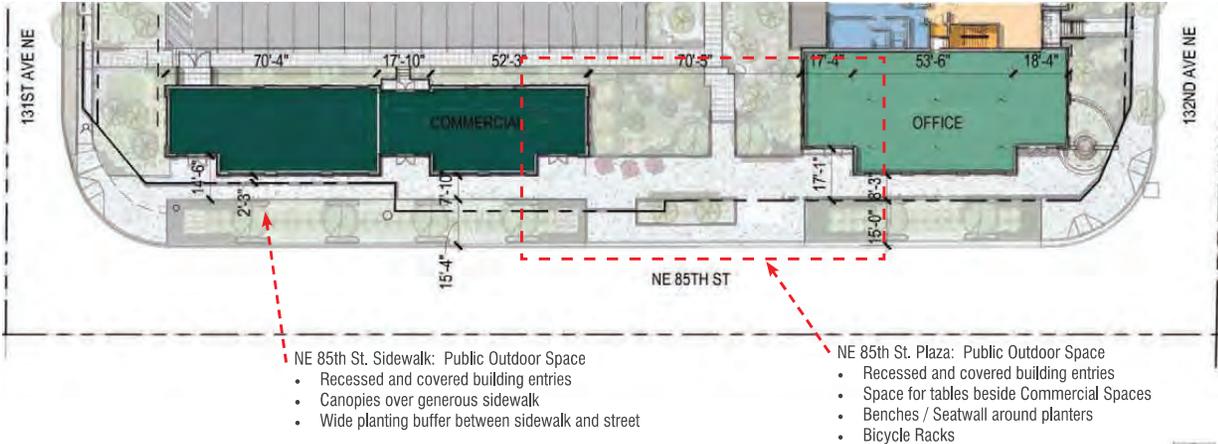
DESIGN GUIDELINE #10:

Objectives:

- To provide amenities that enrich the pedestrian environment.
- To increase pedestrian activity in the Rose Hill Business District.

Provide pedestrian amenities along all sidewalks, interior pathways, and within plazas and other open spaces. Desired amenities include:

- Pedestrian-scaled lighting (12'-15' above ground).
- Seating space. (benches, steps, railings and planting ledges, 16" to 18" above ground, 6" to 24" wide.)
- Pedestrian furniture (trash receptacles, consolidated newspaper racks, bicycle racks, and drinking fountains).
- Planting beds and/or potted plants.
- Unit paving, such as stones, bricks or tiles.
- Decorative pavement patterns and tree grates.
- Water features.
- Informational kiosks.
- Transit shelters.
- Decorative clocks.
- Artwork.



DESIGN GUIDELINE #12:

Objectives:

- To provide a variety of pedestrian-oriented areas to attract shoppers to commercial areas and enrich the pedestrian environment.
- To create gathering spaces for the community.
- To configure buildings and uses to encourage pedestrian activity and pedestrian focal points.
- Provide pedestrian plazas in conjunction with non-residential uses.
- Position plazas in visible locations...close to bus stops, or where there are strong pedestrian flows on neighboring sidewalks.
- Incorporate pedestrian amenities.



Partial Plan - NE 85th St. Plaza

Partial Elevation - NE 85th St. Plaza

PROJECT VISION | NEW PUBLIC & PRIVATE OUTDOOR SPACE



View of Plaza from NE 85th St.



View of Plaza Looking West



Plaza Area Bird's Eye View



View of Plaza Looking East

PROJECT VISION | NEW PUBLIC & PRIVATE OUTDOOR SPACE



Residential Courtyard Style/Sample Images



DESIGN GUIDELINE #13:

Objectives:

- To create usable space that is suitable for leisure or recreational activities for residents.
- To create open space that contributes to the residential setting.

Incorporate common open space into multi-family residential uses.

- Consider open space as a focal point...
- Open space should feature paths, seating, lighting, and other pedestrian amenities....it should be oriented to receive sunlight, (preferable south).

Provide private open space for multi-family residential units....provide patios, decks, and/or landscaped yards....provide balconies large enough to allow for human activity.

PROJECT VISION | NEW PUBLIC & PRIVATE OUTDOOR SPACE



View of Residential Courtyard

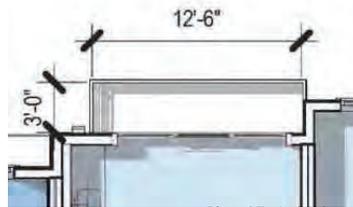


View of Patio @ Club Room

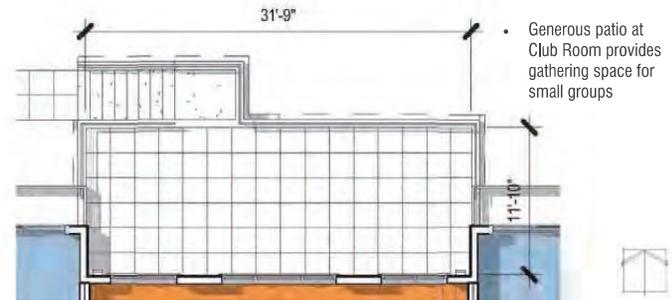


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- Balconies are large enough for activity and furniture
- Sliding Doors provide access without taking away space



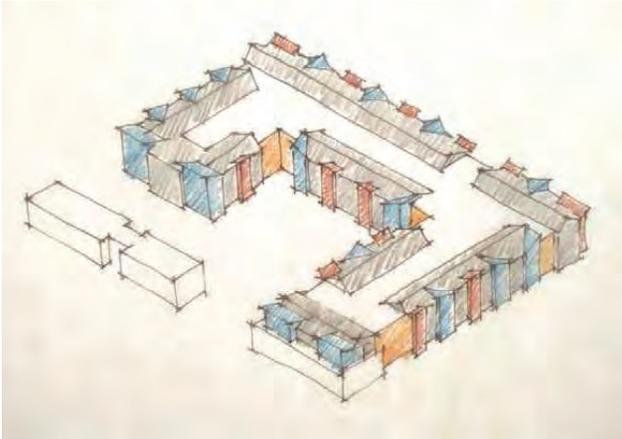
Plan of Typical Balcony



- Generous patio at Club Room provides gathering space for small groups

Plan of Patio @ Club Room

PROJECT VISION | TRANSITION TO RESIDENTIAL SCALE



Previous Massing Diagram - DRC #1

DESIGN GUIDELINE #17:

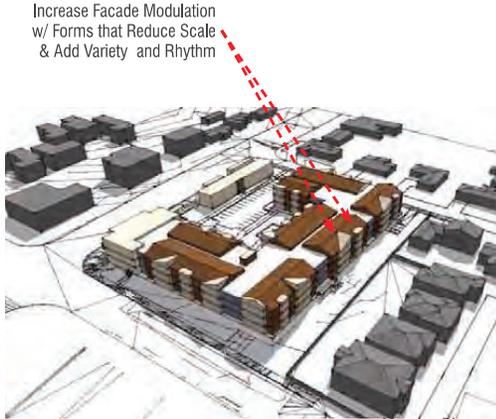
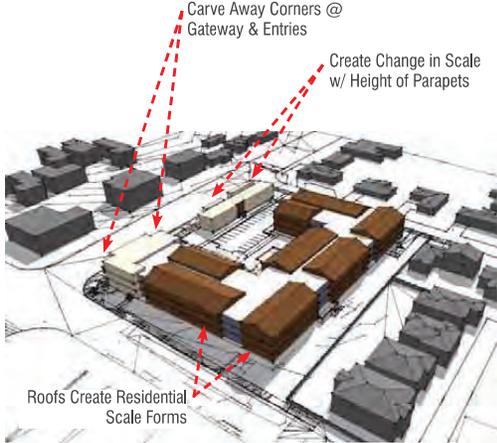
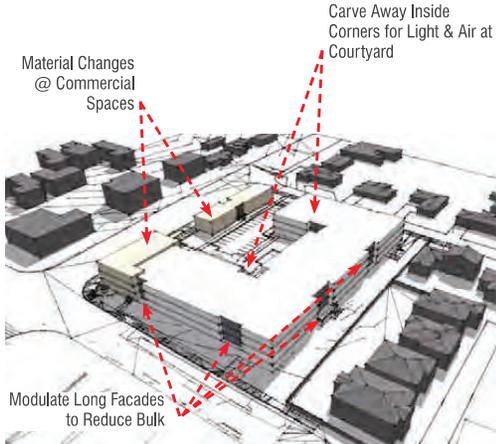
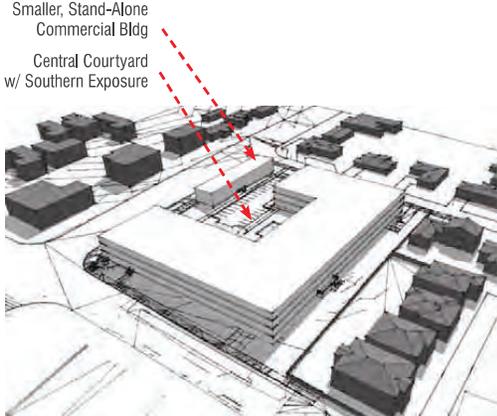
Objectives:

- To encourage an architectural scale of development that is compatible with the vision for the three design districts withing the Rose Hill Business District.
- To add visual interest to buildings.

A combination of techniques is desirable to reduce the architectural scale of buildings. Specifically, these techniques are encouraged at intervals (30 feet in the East end....)...Alternatives will be considered provided they meet the intent of the guidelines.

- Incorporate fenestration techniques that indicate the scale of the building.
- Encourage vertical modulation on multi-story buildings to add variety and to make large buildings appear to be an aggregation of smaller buildings.
- Encourage a variety of horizontal building modulation techniques to reduce the architectural scale of the building and add visual interest.
- Encourage a variety of roofline modulation techniques.

"C" Shape w/ Courtyard

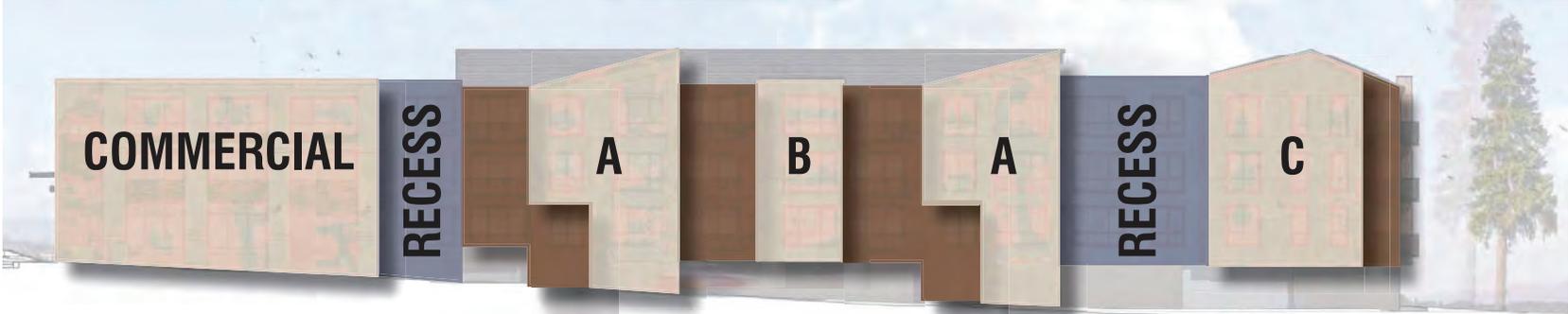


Massing Diagram

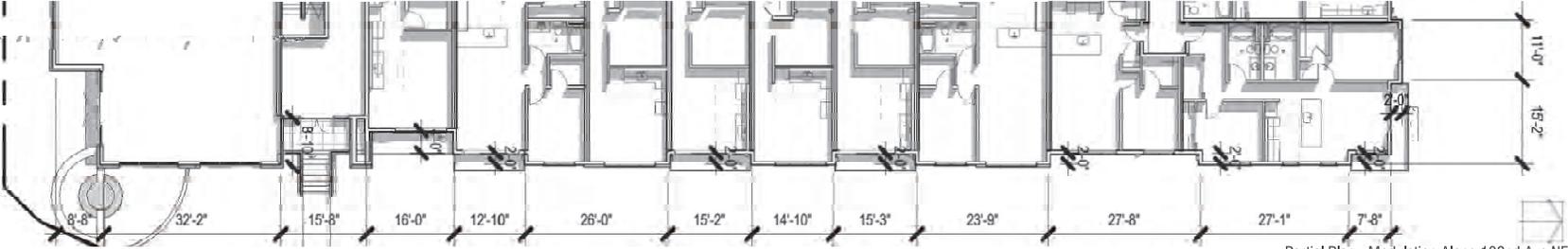
PROJECT VISION | TRANSITION TO RESIDENTIAL SCALE



Partial Elevation - 132nd Ave NE



Partial Elevation Diagram - Modulation Along 132nd Ave NE

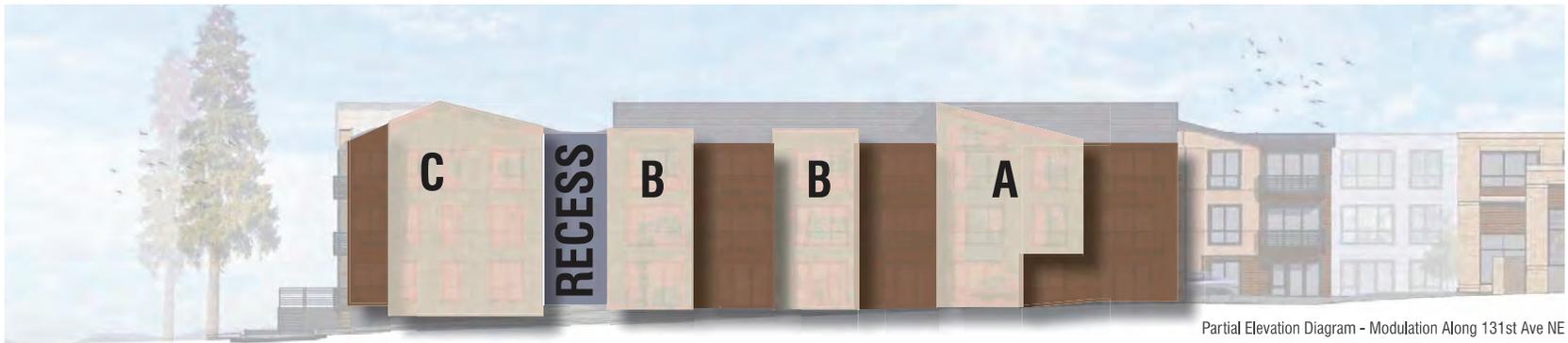


Partial Plan - Modulation Along 132nd Ave NE

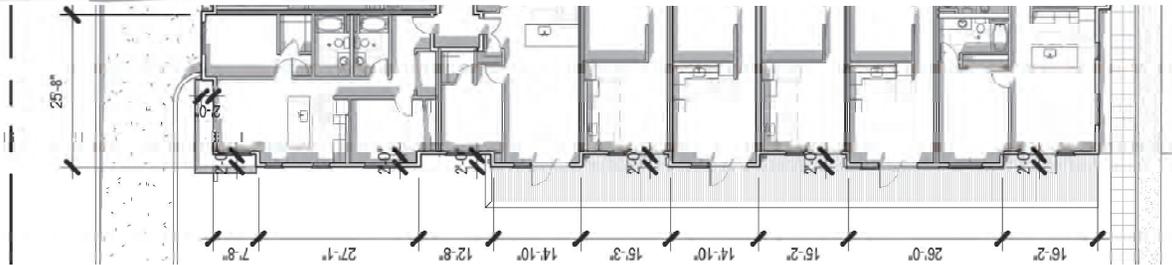
PROJECT VISION | TRANSITION TO RESIDENTIAL SCALE



Partial Elevation - 131st Ave NE



Partial Elevation Diagram - Modulation Along 131st Ave NE

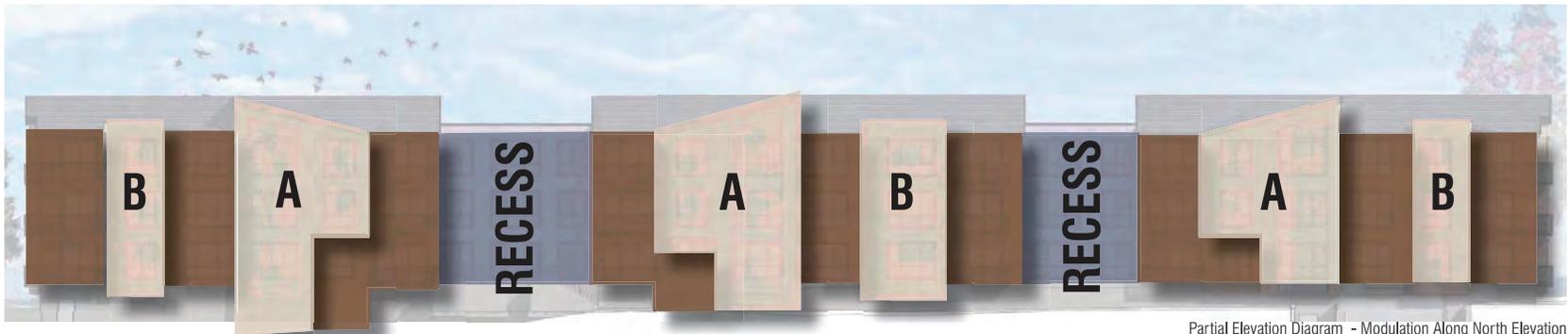


Partial Plan - Modulation Along 131st Ave NE

PROJECT VISION | TRANSITION TO RESIDENTIAL SCALE



Partial Elevation - North Elevation



Partial Elevation Diagram - Modulation Along North Elevation



Partial Plan - Modulation Along North Elevation

PROJECT VISION | TRANSITION TO RESIDENTIAL SCALE

- Detailed Brick Cornice With Soldier Course & Coping
- Partial Glass Deck Railing
- Metal Coping Cap
- Contrasting Material (Lap Siding)
- 1" Recess @ Brick
- Canopies for Weather Protection
- Metal Channel Details
- Decorative Lighting
- Large Storefront Openings & Doors
- 1" Brick Recess & Soldier Course



Partial Elevation Showing Commercial Scale Elements

DESIGN GUIDELINE #18:

Objectives:

- To encourage the use of building components that relate to the size of the human body.
- To add visual interest to buildings.

Encourage a combination of architectural building elements that lend the building a human scale. Examples include arcades, balconies, bay windows, roof decks, trellises, landscaping, awnings, cornices, friezes, art concepts, and courtyards.

20 | MERIT HOMES | CONTINENTAL DIVIDE | DRC PRESENTATION | 11.18.2019



- 2-Piece Fascia Board & Trim
- Balcony with Decorative Railing
- Lap Siding for Smaller Scale
- Smaller Residential Windows and Doors
- Change in Depth @ Material Change
- Decorative Trim & Accent Panel

Partial Elevation Showing Residential Scale Elements

PROJECT VISION | TRANSITION TO RESIDENTIAL SCALE



132nd Ave. NE Facade



View Looking North along 132nd Ave. NE



View Looking North along 131st Ave. NE

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View Looking West along North Facade from Club Room Patio

MERIT HOMES | CONTINENTAL DIVIDE | DRC PRESENTATION | 11.18.2019 21

PROJECT VISION | LANDSCAPING TO ENHANCE THE SITE



LANDSCAPE BUFFER

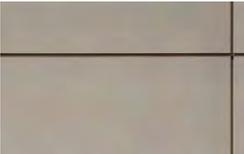
A 15' landscape buffer surrounds the site on 3 sides. This buffer is composed of trees spaced every 20' with an understory of mixed shrubs and groundcovers providing year-round interest. Additionally, the buffer provides a barrier between the neighboring homes and the proposed development to maintain existing privacy.



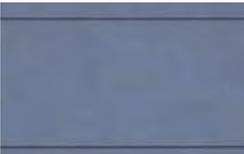
PROJECT VISION | MATERIALS & ELEVATIONS



E F.C. LAP SIDING 7" EXPOSURE
PAINTED "STURDY BROWN"



F FIBER CEMENT PANELS
PAINTED "ANALYTICAL GRAY"



G HORIZONTAL FIBER CEMENT PANELS
PAINTED "DOWNING SLATE"



C ARCHITECTURAL CONCRETE
FINE / SACKED FINISH



H VINYL WINDOWS
BLACK FRAMES

^ PARTIAL ELEVATION @ RESIDENTIAL UNITS

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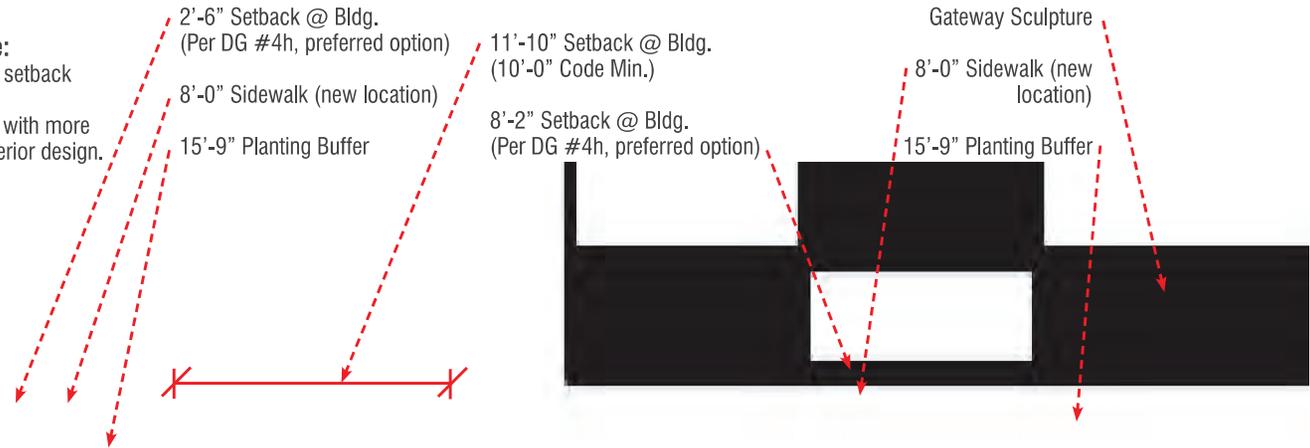
| MERIT HOMES | CONTINENTAL DIVIDE | DRC PRESENTATION | 11.18.2019 29

DRC REVIEW | FROM 8/6/18

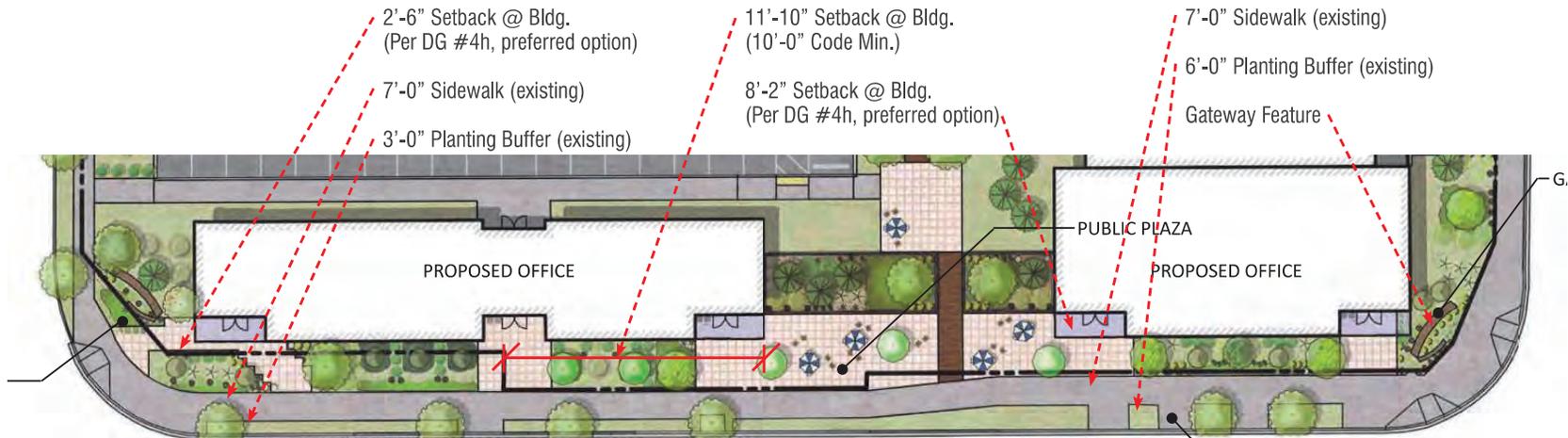
DG #5a, 5b, 5h

NE 85th St. Pedestrian Experience:

- Meets all requirements for requested setback departure.
- Creates more welcoming streetfront, with more separation from vehicle traffic & superior design.



^ **Current Design - NE 85th St. Sidewalk & Building**



^ **DRC #2 - NE 85th St. Sidewalk & Building Setback**

ENCORE architects

APPENDIX | LEVEL 2 FLOOR PLAN



- OPEN 1 BEDROOM
- 1 BEDROOM
- 2 BEDROOM
- BOH
- RESIDENTIAL AMENITY SPACE



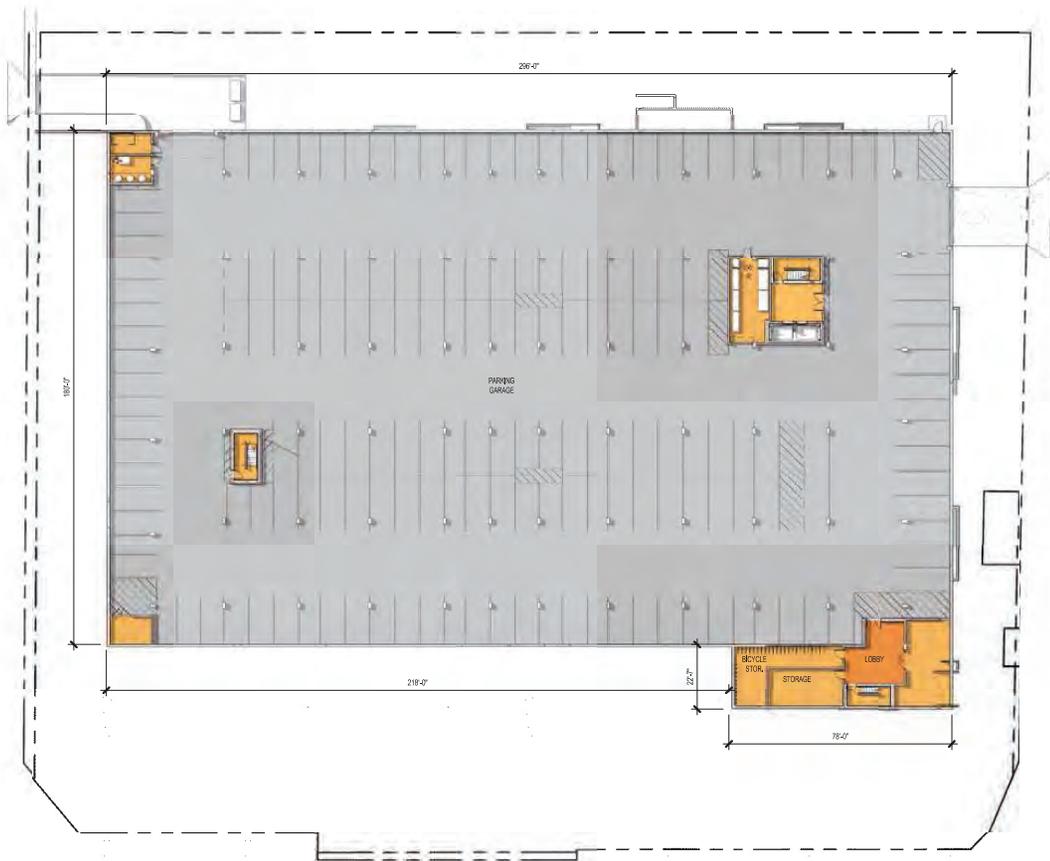
APPENDIX | LEVEL 3 FLOOR PLAN



- OPEN 1 BEDROOM
- 1 BEDROOM
- 2 BEDROOM
- BOH
- RESIDENTIAL AMENITY SPACE



APPENDIX | PARKING / GROUND FLOOR PLAN

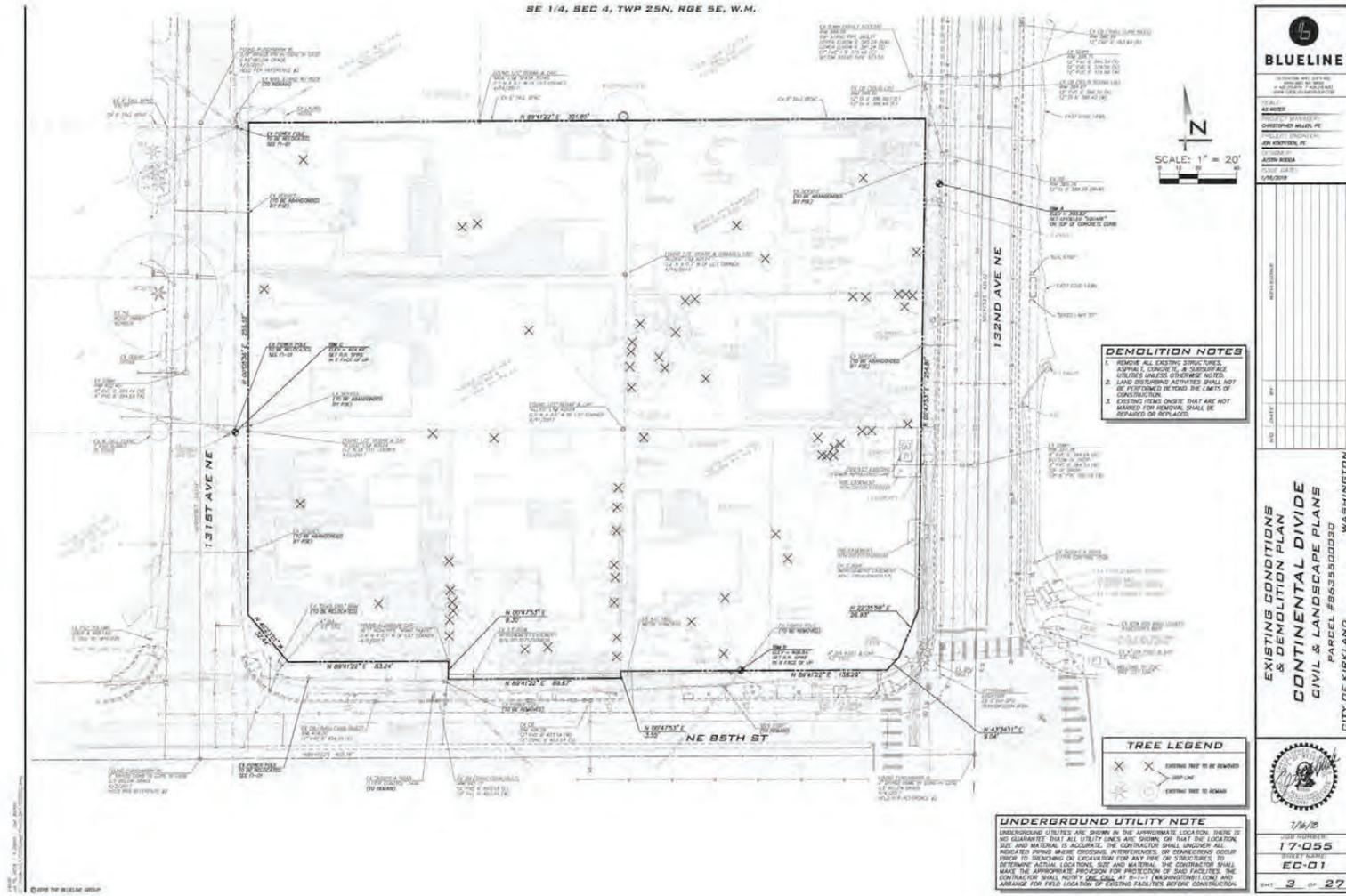


- BOH
- RESIDENTIAL AMENITY SPACE
- PARKING

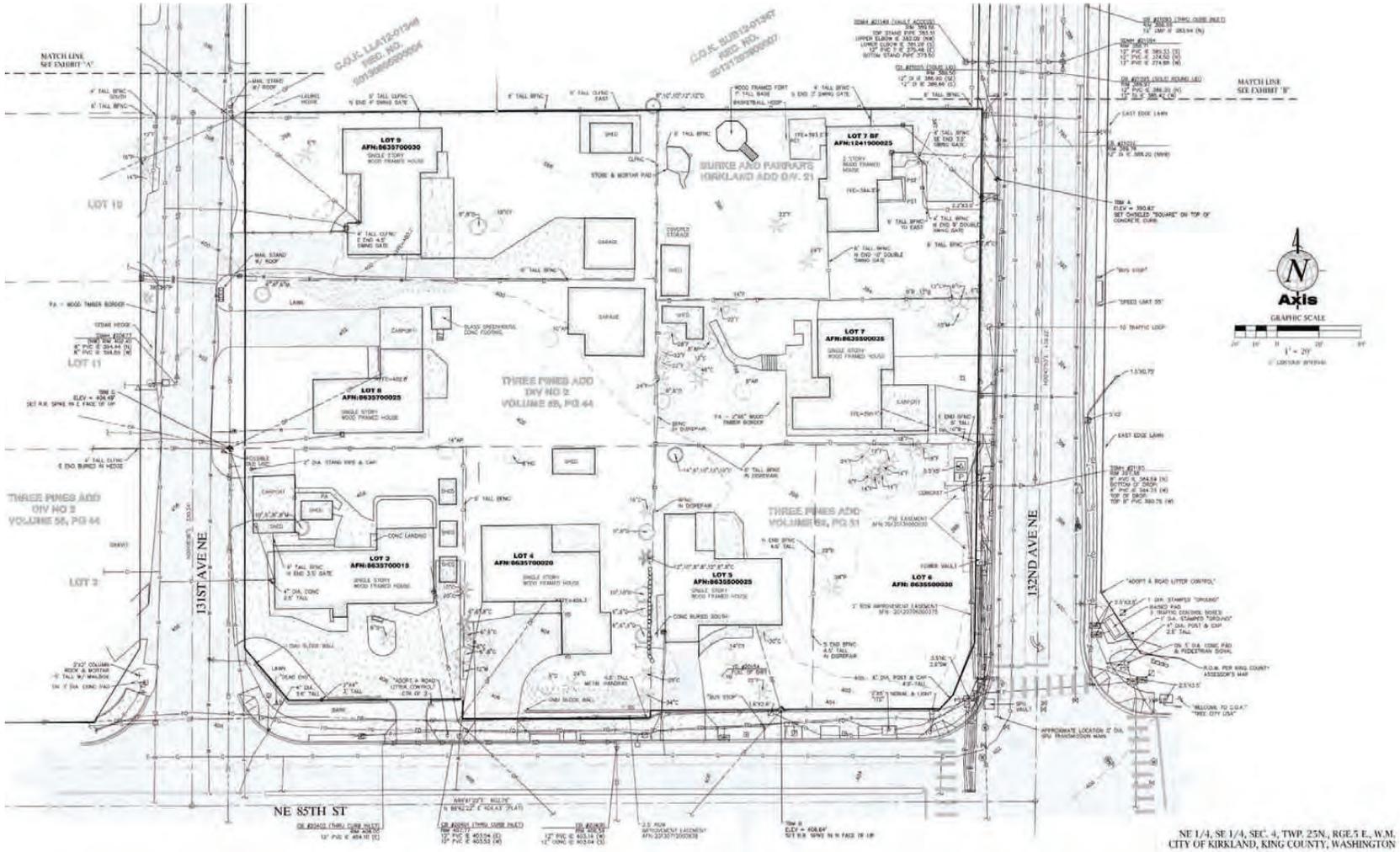
APPENDIX | BUILDING SECTIONS

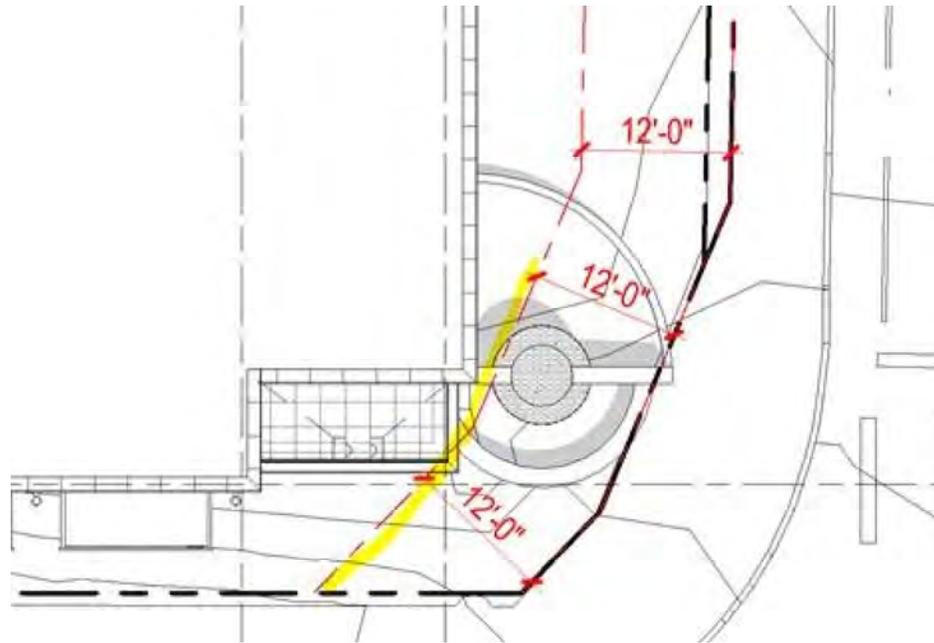


APPENDIX | TREE PLAN



APPENDIX | SURVEY







The City of Kirkland



Design Guidelines

For Rose Hill Business District



MAKERS architecture and urban design

Adopted by the City Council
pursuant to Kirkland Municipal Code
Section 3.30.040, Ordinance 4031 on
January 3, 2006

Jim Lauinger,
Mayor

Eric Shields,
Director,
Planning & Community
Development

Design Guidelines for Rose Hill Business District

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Introduction

This document sets forth Design Guidelines, adopted by Section 3.30.040 of the Kirkland Municipal Code that will be used by the City in the design review process for commercial and multifamily development in the Rose Hill Business District.

Other documents that should be referred to during design review are the NE 85th Street Subarea Plan goals and policies contained in the Comprehensive Plan and the RH Use Zone Charts found in the Kirkland Zoning Code.

Purpose of the Design Guidelines

For projects required to be reviewed by the Design Review Board, the Board will use these guidelines in association with the Design Regulations of the Kirkland Zoning Code. To the extent that the standards of the Design Guidelines or Design Regulations address the same issue but are not generally consistent or contain different levels of specificity, the Design Review Board will determine which standard results in superior design. For Administrative Design Review (ADR), the Planning Official will use these guidelines when necessary to interpret the Design Regulations. They are also intended to assist project applicants and their architects by providing graphic examples of the intent of the City's guidelines and regulations.

The Design Guidelines do not set a particular style of architecture or design theme. They are intended to establish a greater sense of quality, unity, and conformance with Kirkland's physical assets and civic identity. These guidelines are not intended to slow or restrict development, but rather to add consistency and predictability to the permit review process.

Urban Design Goals and Objectives

Urban design goals for the desired future development of the area were adopted in 2001 as part of the NE 85th Street Subarea Plan:

Subarea Plan Design Goal NE 85-17- Provide a coordinated streetscape improvements through the Subarea that enable pedestrians, drivers bicyclists, and other users to have safe and pleasant experience.

Subarea Plan Design Goals NE 85-18 and 18.19- Establish mandatory building and site design standards that apply to all new expanded, or remodeled commercial and multi-family buildings in the Subarea, with the objectives of creating a more attractive commercial area, enhancing pedestrian orientation, and creating effective buffers and transitions between the commercial land uses and the established residential neighborhoods to the north and south.

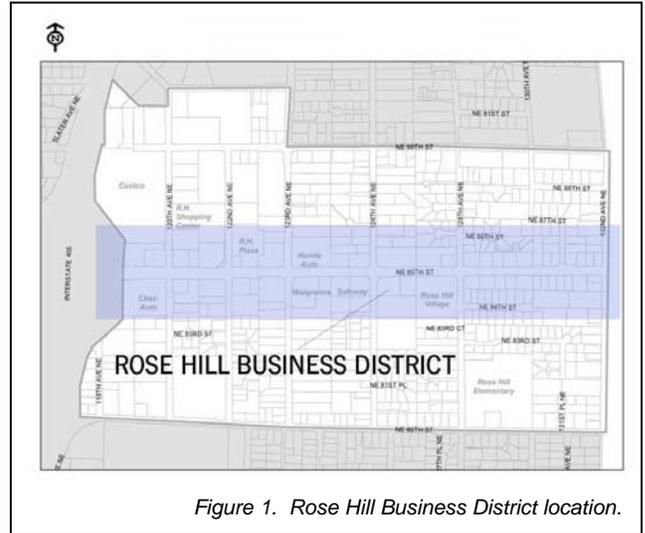


Figure 1. Rose Hill Business District location.

Design objectives promoted in the NE 85th Street Subarea Plan for the Rose Hill Business District are to:

- Create a more attractive and economically viable commercial area by working closely with business and property owners in the subarea to improve and upgrade the appearance of the corridor.
- Ensure that new development meets high standards for building and site design.
- Increase continuity and order by coordinating site orientation, building scale, and streetscape elements of new development to fit into the context of surrounding buildings.
- Improve pedestrian safety and pedestrian friendliness by providing new street improvements along NE 85th Street and connecting streets to enable pedestrians, drivers, bicyclists and other users to have a safe, pleasant experience.
- Create effective buffers and transitions between commercial and multi-family land uses and the established residential neighborhoods to the north and south.

Design Vision for Rose Hill Business District

The future of the Rose Hill Business District will be a vibrant mixed-use corridor combining regional and local uses in a unique setting that accommodates both vehicular and pedestrian traffic. The district will continue to be automobile-oriented, but its existing single-story development pattern will be transformed into a more viable pattern of mixed residential and commercial uses. The NE 85th Street Subarea Plan envisions an attractive, economically healthy commercial area where consumers and nearby residents have access to a mix of regional, community, and local shops and services. A goal of the district is to be pedestrian-friendly and have a sense of community and neighborhood identity.

Whereas Kirkland's downtown and other commercial areas are separate districts, NE 85th Street is a regional transportation corridor running through the residential North and South Rose Hill Neighborhoods. This distinction has encouraged close participation by these neighborhoods in shaping the character of the Rose Hill Business District. Businesses, property owners, and neighborhood residents helped develop the NE 85th Street Subarea Plan, which sets forth goals and policies for future development.

The Rose Hill Business District is comprised of three distinct design districts, the **Regional Center** to the west towards Interstate 405, the centralized, more pedestrian-oriented **Neighborhood Center**, and the smaller scale **East End**.

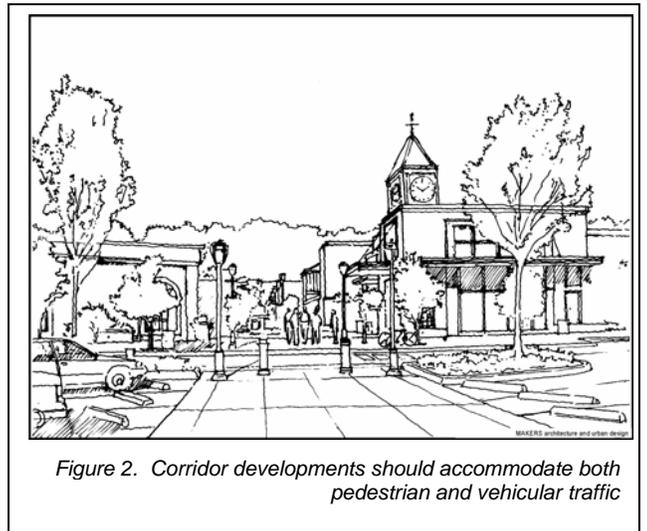


Figure 2. Corridor developments should accommodate both pedestrian and vehicular traffic

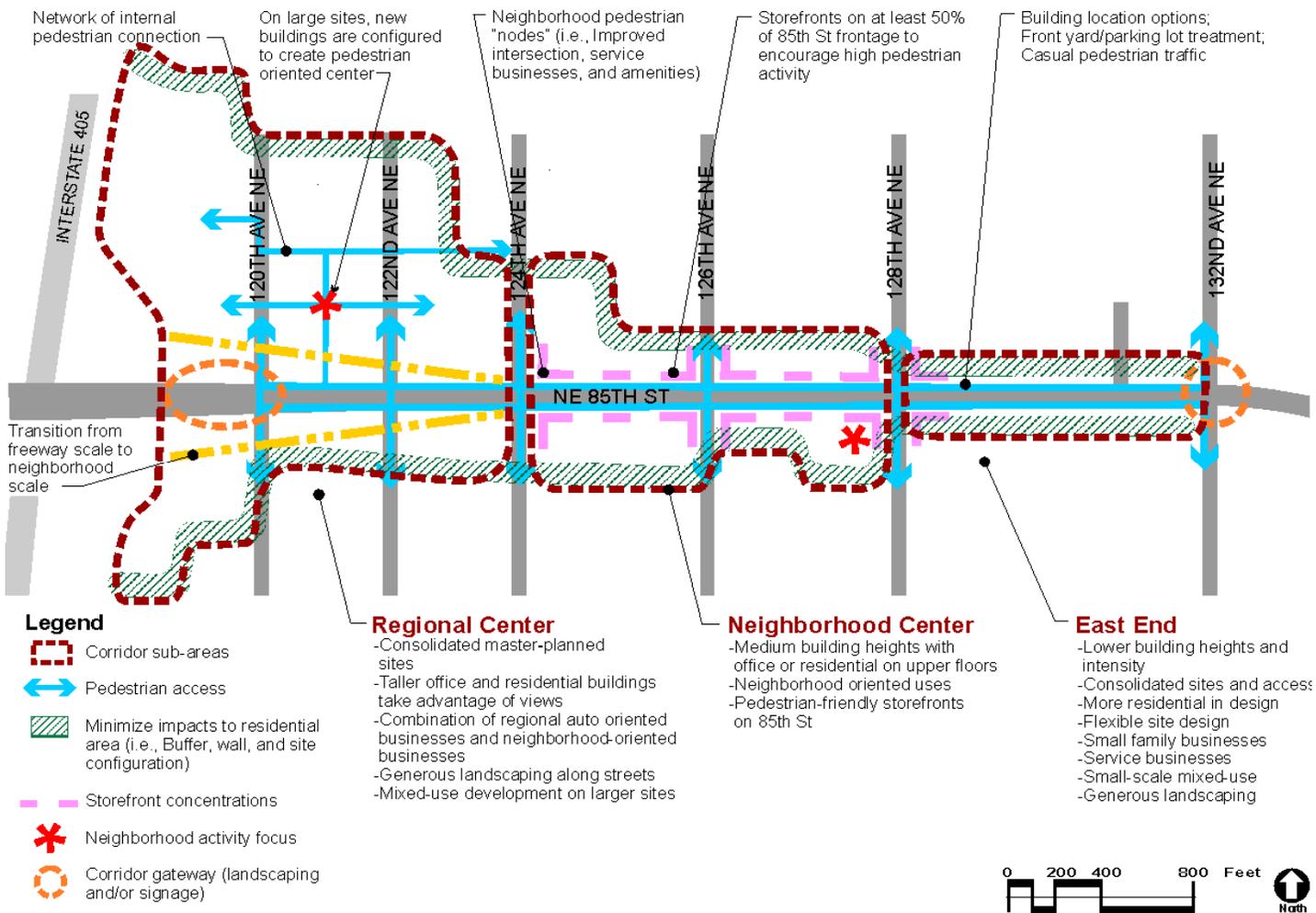


Figure 3. The planning concept for the Rose Hill Business District calls for three distinct design districts.

Visibility from I-405 and easy freeway access allows for larger, regional-oriented uses to dominate the **Regional Center**, the area west of 124th Avenue NE. The **Neighborhood Center**, between 124th and 128th Avenue NE, serves as the focal point for the North and South Rose Hill Neighborhoods and will feature a combination of neighborhood-related uses, regional services, and mixed-uses in a pedestrian-friendly setting. The **East End**, between 128th Avenue NE and the eastern city limits at 132nd Avenue NE, will feature smaller scale businesses and mixed-uses in a setting compatible with surrounding residential uses. Common streetscape elements, including street lights, sidewalks, and landscaping, will tie all three subareas together to create a distinct visual image for the corridor. Where two design districts overlap at 124th Avenue NE and at 128th Avenue intersections, design treatments from both districts will need to be considered during the design review process to produce a superior design at the corners.

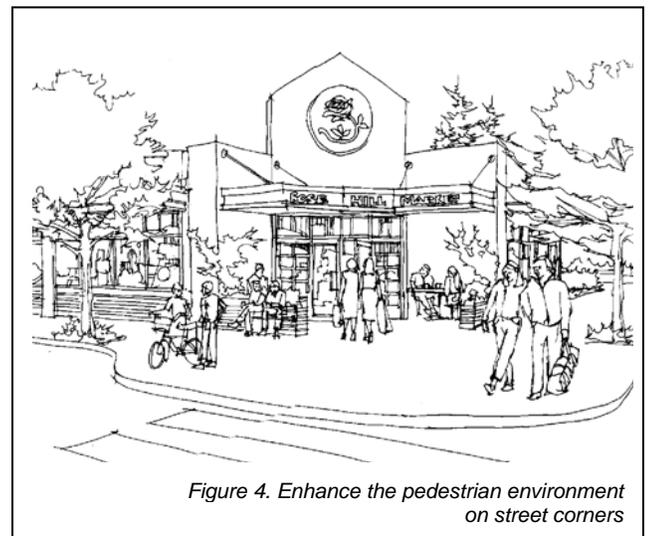


Figure 4. Enhance the pedestrian environment on street corners

NE 85th Street will continue to serve as a primary link between Kirkland and Redmond. While the corridor will maintain heavy vehicular traffic volumes, new transit improvements, wide sidewalks, landscaping, lighting, consolidated driveways, medians, crosswalks, and storefronts should make the corridor more inviting to pedestrians. Steps should be taken to minimize cut-through traffic and other traffic impacts to adjacent neighborhoods. Improvements to sidewalks on streets connecting to NE 85th Street should be made to improve neighborhood access to corridor uses, which will result in increased pedestrian activity for local businesses.

The quality of private development will improve over time, particularly with the adoption of site planning, building, and landscape design standards. Especially within the **Neighborhood Center** district, the corridor should maintain pedestrian-friendly storefronts concentrated at key street corners. Uses that do not front on the street should feature a system of attractive walkways that connect uses to the street and adjacent sites. On larger sites within the corridor, developments should utilize interior roads that look and function more like public streets, with on-street parking, street trees, and wide sidewalks. Storefronts should feature a variety of building details to enhance the pedestrian setting along sidewalks and interior pathways.

Transitional design techniques will be important to the adjacent residential uses. Multifamily development will provide the preferred transitional use between commercial and low-density residential areas. In addition to complying with the adopted standards, commercial and multifamily developments will utilize a variety of techniques to both minimize impacts to adjacent residential developments and maximize property use. Such techniques may include building modulation and articulation, a mix of building materials, and colors coordinated with buildings massing, wide landscape buffers, and low building walls that function as buffers.

Landscaping elements will be a common theme in the corridor and include the neighborhood's namesake roses. Developments will utilize a colorful variety of drought-tolerant and low-maintenance trees, shrubs, and groundcover to soften the appearance of the corridor and add seasonal interest.

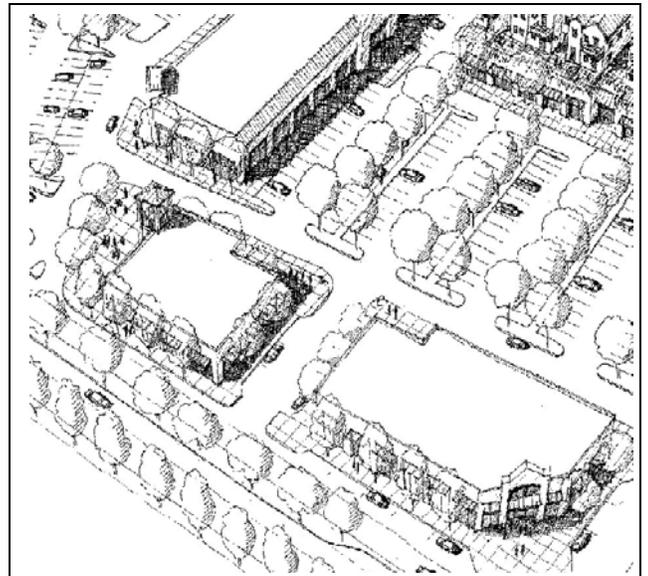


Figure 5. The quality of development will improve over time, enhancing the character of the business

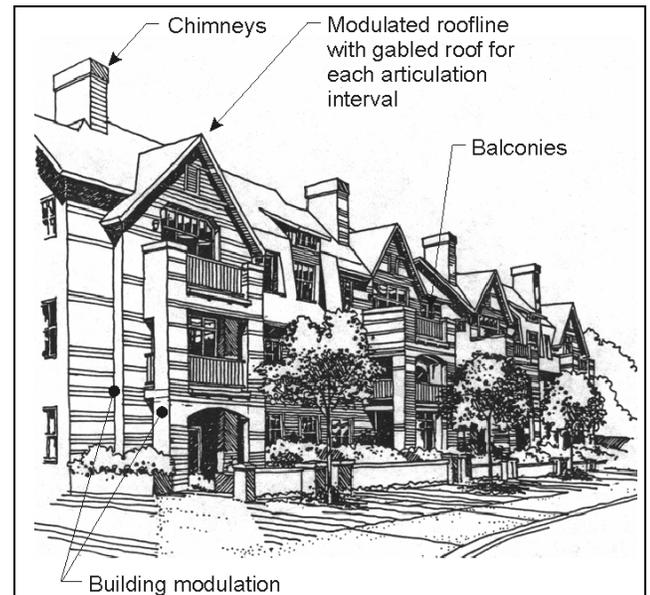


Figure 6. Multi family residential buildings feature a combination of modulation and articulation techniques to reduce their perceived scale and to add visual interest

Overview of Three Design Districts

Regional Center

The Regional Center includes the commercial and mixed-use zoning districts in the Rose Hill Business District between Interstate 405 and 124th Avenue NE. It is an important regional crossroads featuring a concentration of regional-oriented retail, auto dealerships, and office uses that draw customers from the Puget Sound, and local residential areas.

The larger sites within this subarea should coordinate the phasing and organization of major redevelopment around new internal vehicular and pedestrian circulation systems with buildings facing toward the internal circulation networks while also facing toward the adjacent streets. Large sites along NE 85th Street can provide a welcoming face towards NE 85th Street and convenient connections to other side streets, they are often large enough to provide their own pedestrian-oriented focal point. This may include a plaza area surrounded by shops or wide sidewalk areas along an interior access street.

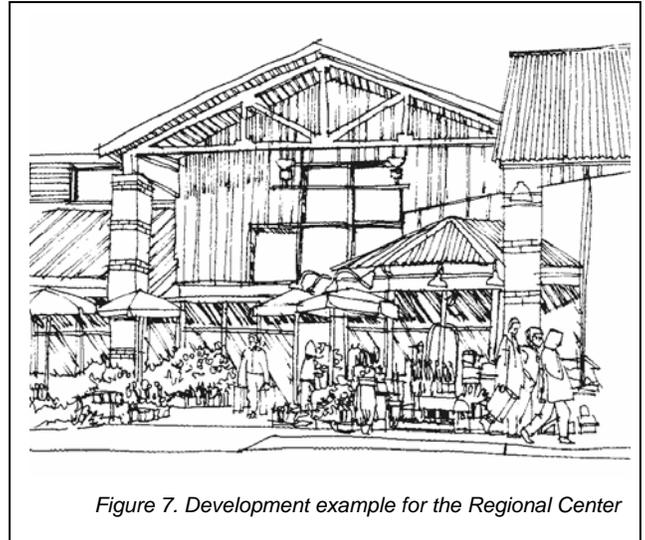


Figure 7. Development example for the Regional Center

Design Considerations for RH 3 Zone

The RH 3 zone should be master-planned to provide coordinated development. For example, Rose Hill Shopping Center could be redeveloped into vibrant mixed-use village, combining local and destination-oriented retail uses with office and residential uses. The Master Plan should be pedestrian oriented and incorporate design standards such as:

- a. Buildings and retail storefronts oriented to internal pedestrian and vehicular routes and to surrounding streets with clearly identifiable building and pedestrian access points and entryways to adjacent streets and internal pedestrian pathways.
- b. Isolated building pads should be minimized.
- c. Design techniques to prevent the dominance of large single occupant structures, such as use of: smaller building footprints, multiple tenant spaces on each floor of a structure abutting a street, stepping back of upper stories along NE 85th Street and corner building treatments at 120th Avenue NE and 122nd Avenue intersections.
- d. Incorporate useable public spaces, plazas or pocket parks, and public amenities, such as art, sculptures, fountains or benches.
- e. Use landscaping to emphasize entries into buildings, pedestrian areas, and pedestrian routes to enhance public spaces, parking areas, and to screen blank walls and service areas.
- f. Placement of loading and service areas shall be located away from NE 85th Street and pedestrian areas.

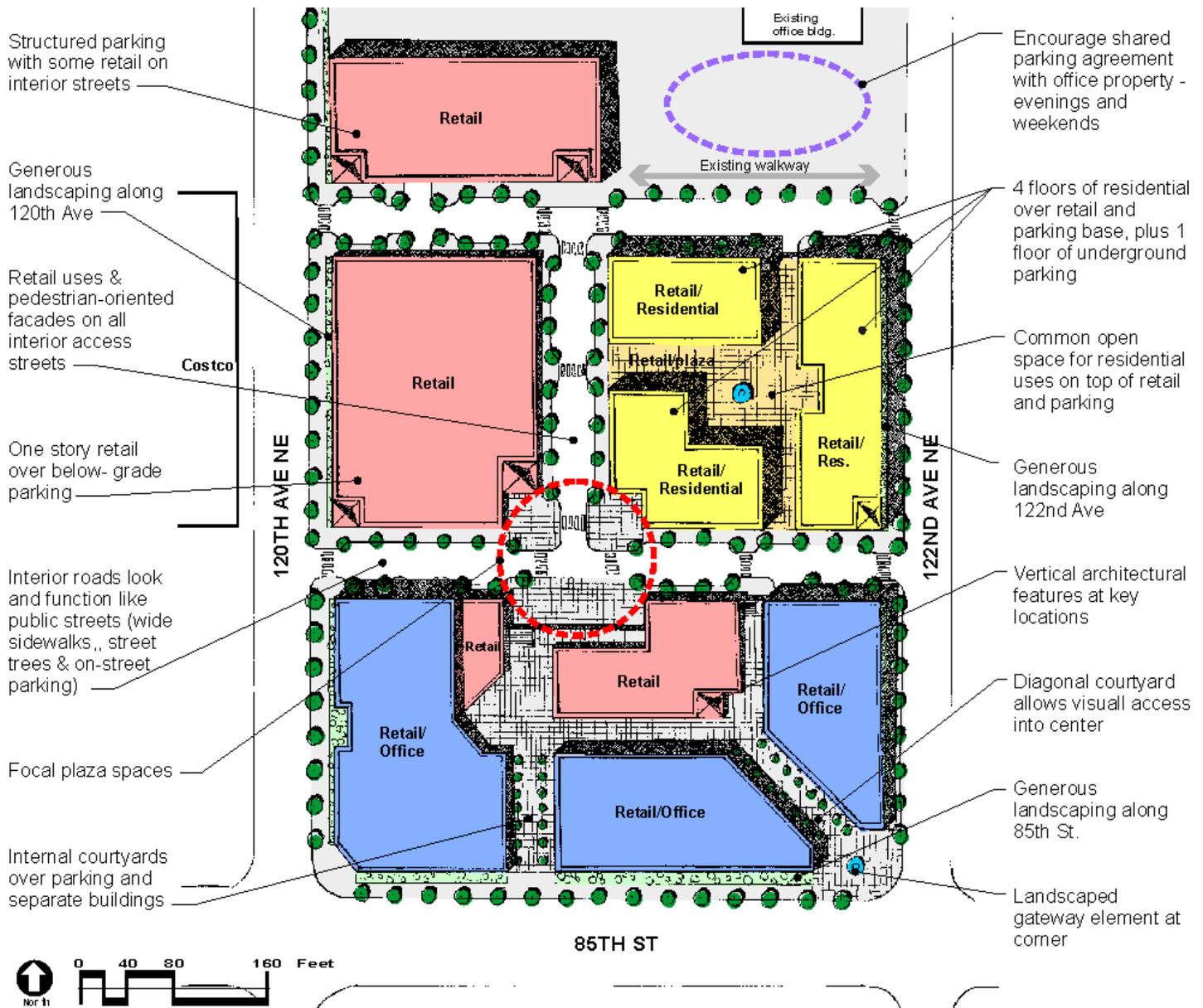


Figure 8. An example of redevelopment of the RH 3 zone consistent with the design guidelines.

Design Considerations for RH 2A-2C Zones

The following design techniques should be incorporated into new development in the RH 2A-2C zones to minimize the dominance of large single occupant structures:

- smaller building footprints
- multiple tenant spaces on the ground floor of a structure abutting pedestrian or vehicular routes,
- stepping back of upper stories adjacent to residential uses,
- providing openness by limiting the floor area on upper stories and modulating upper stories in height as development transitions from NE 85th Street to the south portion of the zone,
- separating the buildings and providing ample building modulation,

- f. providing corner building treatments at NE 85th Street and 120th Avenue NE intersections,
- g. use of underground or structured parking is preferred over large ground level parking lots

As in the RH 3 zone, use landscaping to emphasize entries into buildings, pedestrian areas, and pedestrian routes, to enhance public spaces, parking areas, and to screen blank walls and service areas. Landscaping should also be provided in plazas, along pedestrian circulation routes, in parking areas and to buffer adjoining residential uses. Landscape buffers should not apply along property lines adjacent to I-405.

Neighborhood Center

The Neighborhood Center includes the commercial and mixed-use zoning districts between 124th and 128th Avenue NE. Land uses should include a mix of regional and local-oriented retail. The area also should include in some mixed use buildings office and multifamily uses on upper floors where not adjacent to NE 85th Street. A concentration of storefronts directly on NE 85th, distinguishes this area from the Regional Center and the East End. As a result, this is the most pedestrian-friendly stretch of the corridor.

East End

The East End includes the commercial and mixed-use zoned properties in the Rose Hill Business District east of 128th Avenue NE. The area features a mix of smaller scale uses oriented towards both the regional and local population. The style of development should be more residential in character including conversion of single family homes into commercial businesses. Nearly all buildings should feature pitched roofs and porches or smaller covered entries.

Over time, many smaller sites should be consolidated to maximize development opportunity and share vehicular access and parking. The design guidelines provide a number of street frontage options for businesses. In the future, the resulting development will be a mix of storefronts directly on the street, storefronts with small landscaped setbacks, businesses maintaining parking in front, and multi-story buildings with parking underneath.

Design Guidelines

The following design guidelines for the Rose Hill Business District (RHBD) are intended to help guide the future development of the business district toward the future vision statement found in the NE 85th Street Subarea Plan. These guidelines describe the urban design concept that will give the district its distinct design character. Specific design guidelines unique to each of the three design districts are included to address the characteristics of each location such as site planning, vehicular access, landscaping, parking lot location, building scale, building materials and color, building entries, service areas, roof treatments, degree of pedestrian oriented frontage and transition areas between commercial and residential uses. Improvements to streets, parks, and the development of new public facilities will create a dynamic setting for civic activity and private development.

1. Entry Gateway Features

The Comprehensive Plan calls for gateway features at the key entry points into neighborhoods and business districts.

Objectives

- To enhance the character and identity of the Rose Hill Business District.

Guideline

Incorporate entry gateway features in new development on NE 85th Street at 120th and 132nd Avenues. Gateway features should incorporate some or all of the following:

- a. Distinctive landscaping including an assortment of varieties of roses.
- b. Artwork (e.g. vertical sculpture incorporating historical information about Rose Hill).
- c. A gateway sign with the City logo.
- d. Multicolored masonry forming a base for an entry sign.
- e. Decorative lighting elements.

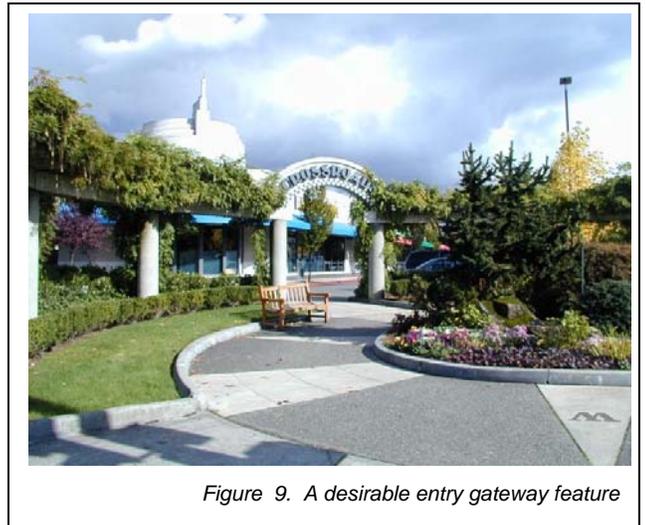


Figure 9. A desirable entry gateway feature

2. Street Trees

Objectives

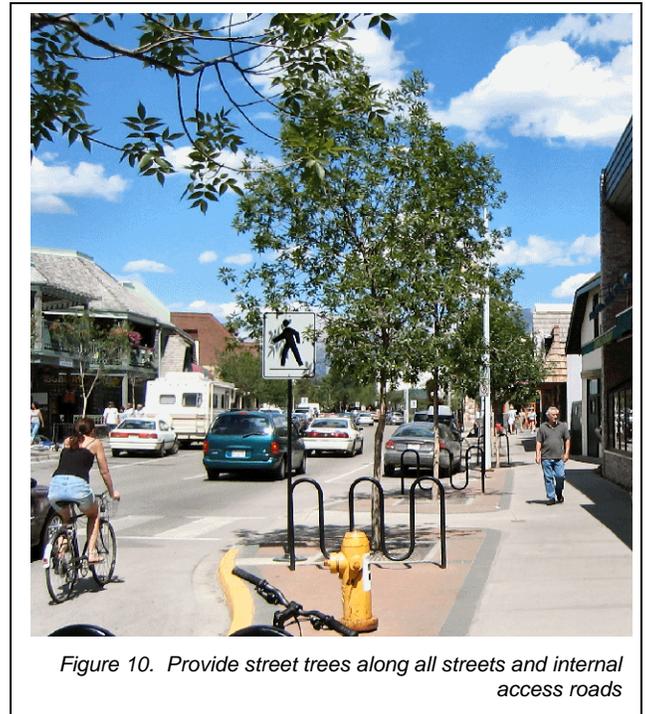
- To enhance the pedestrian environment of the Rose Hill Business District.
- To utilize street trees to upgrade the character and identity of the Rose Hill Business District.
- To utilize trees that provide seasonal interest.

Discussion

The repetition of trees bordering streets, internal roadways, and pathways can unify a community's landscape. Trees can add color, texture, and form to the urban environment. A strong street tree planting scheme can establish community identity and provide a respite from the weather and the built environment.

Guidelines

- a. Incorporate street trees, along all streets, internal access roads, and pathways.
- b. Encourage developments to utilize street trees as a unifying feature of the development.
- c. Select and maintain tree species that will accommodate pedestrian and vehicular traffic, and maintain visibility into and through sites for safety purposes.



3. Street Corners

Objectives

- To enhance the appearance of highly visible locations.
- To upgrade the character and identity of the Rose Hill Business District and its individual subareas.
- To enhance pedestrian access and safety.

Discussion

Street corners, especially along arterial corridors, provide special opportunities for visual punctuation and an enhanced pedestrian environment. Buildings on corner sites that incorporate architectural design elements create visual interest for the pedestrian and provide a sense of human proportion and scale.

Street corners can be an excellent location for plazas, particularly where adjacent storefronts and building entries are provided.

Guidelines

- a. Encourage design treatments that emphasize street corners through the use of building location and design, plaza spaces, landscaping, distinctive architectural features, and/or signage.
- b. Incorporate storefronts directly at 124th, 126th, and 128th street corners to reinforce the desired pedestrian-oriented character of the Neighborhood Center.
- c. Encourage special landscaping elements on all street corners in the Rose Hill Business District. Such landscaping elements should incorporate a variety of plant types and textures that add seasonal interest.
- d. Encourage all buildings located at or near street corner to incorporate special architectural elements that add visual interest and provide a sense of human proportion and scale. This could include a raised roofline, turret, corner balconies, bay windows, special awning or canopy design, and/or distinctive use of building materials (see the following examples).



Figure 11. This building uses a cropped corner with entry and decorative roofline, building materials, and details to provide visual interest

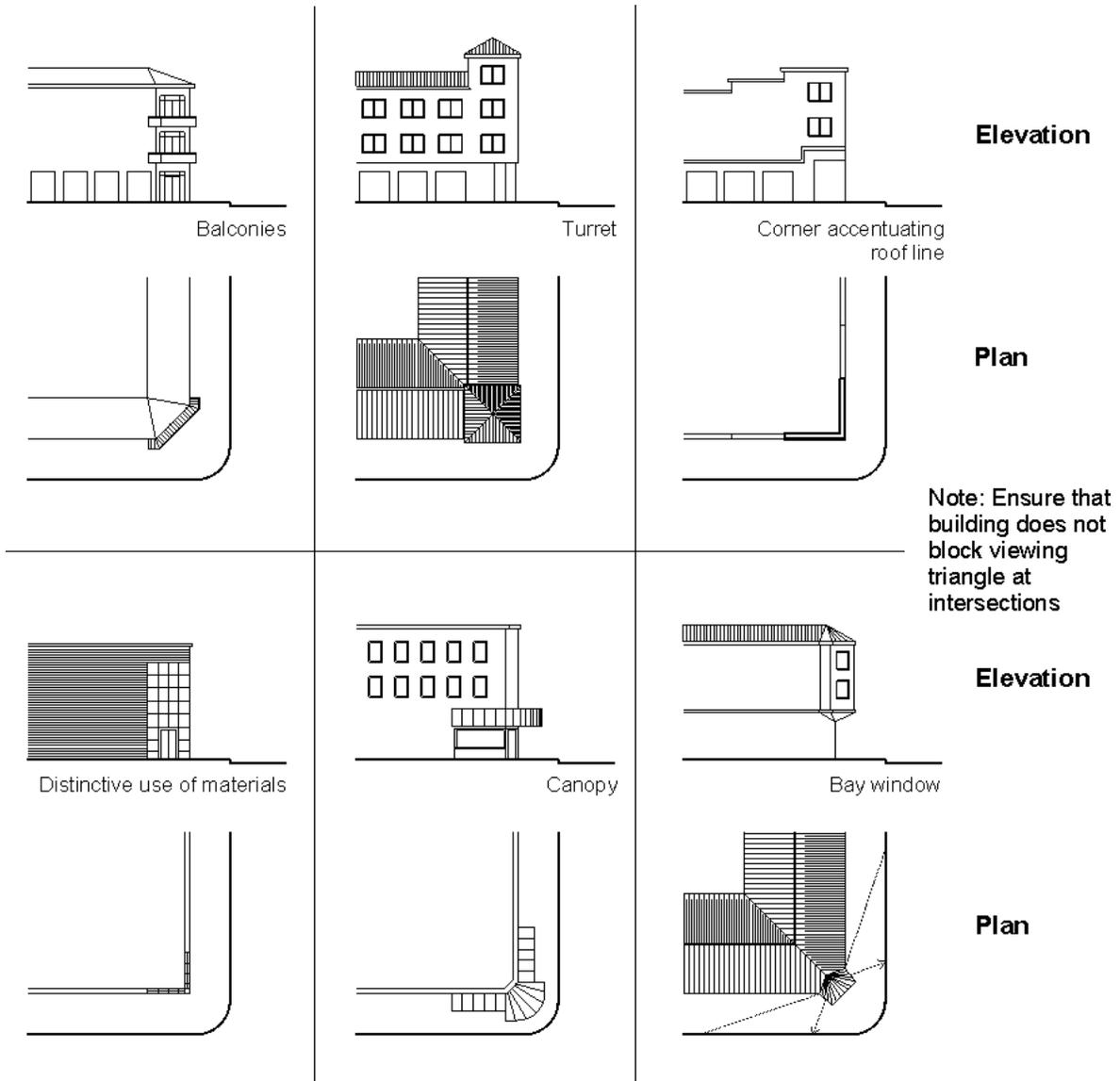


Figure 12. Desirable building elements for street corners.

4. Pedestrian-Friendly Building Fronts

Objectives

- To enhance the pedestrian environment within the Rose Hill Business District.
- To create safe and active sidewalks and pathways.

Guidelines

Incorporate transparent windows and doors and weather protection features along all non-residential facades adjacent to a sidewalk or internal pathway. Weather protection features could include awnings, canopies, marquees, or other permitted treatments.

Alternative treatments may be considered if they meet the objectives. For example, reduced transparency and weather protection levels may be considered if an alternative configuration provides other amenities above and beyond what is required by KZC Chapter 92 and the Design Guidelines and, if building details or architectural treatments provide interest at close range and won't "deaden" the pedestrian environment or create a potential safety problem.

5. Building Location and Orientation

Objectives

- To enhance the character and identity of the Rose Hill Business District.
- To upgrade the appearance of streets within the Rose Hill Business District.
- To increase pedestrian circulation.
- To encourage landscaping elements between the sidewalk and buildings or parking areas in the Regional Center.
- To create focal points, particularly on large sites.
- To coordinate development with adjacent sites, where desirable.
- To encourage development configurations that minimize negative impacts to adjacent single family residential areas.

Guidelines

- a. Locate and orient buildings towards streets, plazas or common open spaces, and major internal pathways.
- b. Configure buildings to create focal points especially on larger sites.
- c. Configure development to provide opportunities for coordinated pedestrian and vehicular access. Where there are no current opportunities for coordinated access, developments should provide the opportunity for future coordination, where desirable, should the adjacent site be redeveloped in the future.



Figure 13. An example of a pedestrian-friendly building façade

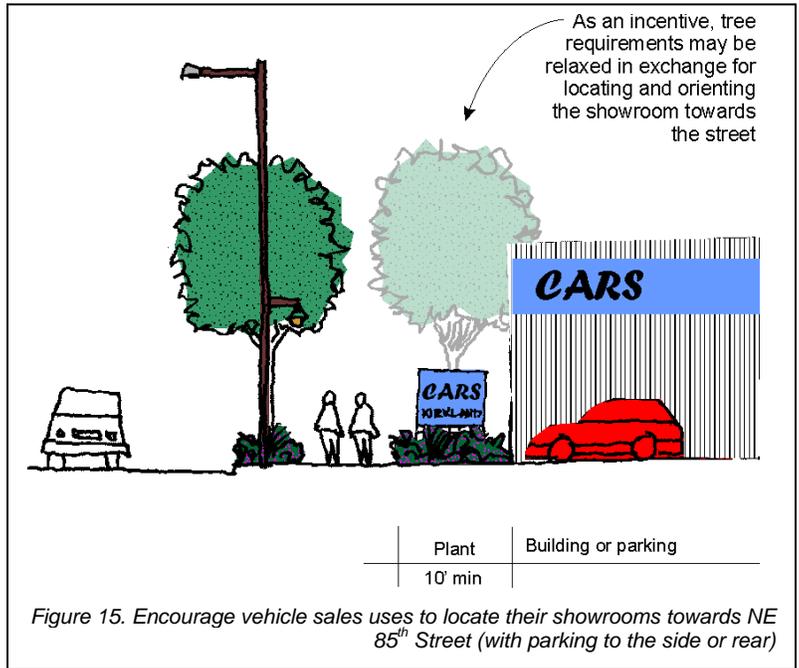


Figure 14. Encourage developments to place parking lots to the side or rear, as accomplished here

d. Site and orient multi-story buildings to minimize impacts to adjacent single family residents. For example, if a multi-story building is located near a single family property, provide landscaping elements and/or minimize windows and openings to protect the privacy of adjacent homes. Another consideration is to increase upper level building setbacks.

e. Encourage vehicle or equipment sales uses to locate their showrooms towards NE 85th Street (parking to the side or rear) by offering the following incentives:

- Allow vehicle display areas between a portion of the property street frontage if the display is integrated creatively with landscaping. This could include cars on a rock outcropping or on a discreet structure that allows a display vehicle to “float” over the landscaping.
- Allow increased signage through coordinated master sign plans.
- Allow modifications in perimeter landscaping adjacent to a street. For example, on portions of the site where parking lots are adjacent to NE 85th Street or a clustering of the required trees may be permitted provided the perimeter landscaping treatment meets the objectives of the Building Location and Orientation Design Guidelines contained in this section.



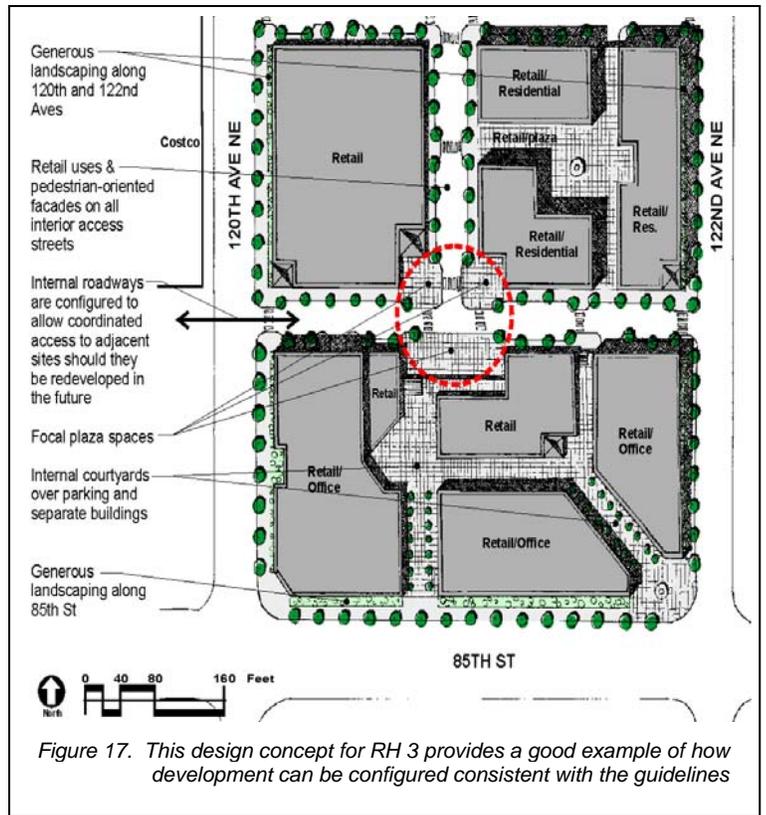
Regional Center NE 85th Street Frontage

f. Provide landscaping between the sidewalk and building or parking lot to provide visual relief and enhance the pedestrian experience along the corridor. Such landscaping should include a combination of ground cover, shrubs, and trees and be trimmed to maintain visibility into the site and buildings. Specifically:

- Where surface parking lots are adjacent to the street, provide a row of trees and shrubs. Trim trees and shrubs to maintain visibility at eye level from street into parking area. Vehicles sales uses may cluster trees and use low level shrubs to improve visibility for vehicle display areas.
- Where buildings face the street, trees may not be appropriate. Otherwise, provide trees as necessary to add visual interest and screen blank walls.



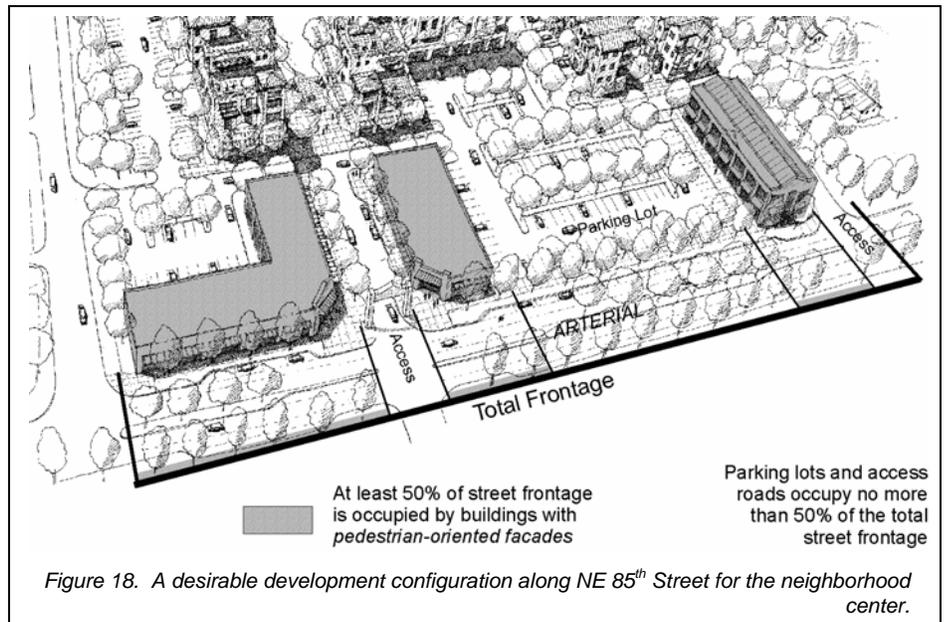
- A variety of landscaping materials is encouraged in addition to the use of grass lawn.
- As an alternative, developments may use some or all of this frontage area as a plaza space, provided landscaping elements and pedestrian amenities are included and it meets the intent of the Design Guidelines.



Neighborhood Center NE 85th Street Frontage

- g. Encourage developments to locate storefronts directly on the sidewalk along NE 85th Street to reinforce this area as the pedestrian-oriented center of the Rose Hill Business District.

A minimum percentage (50%) of pedestrian-oriented facades fronting NE 85th Street strives to achieve this objective. Relaxation of this standard may be considered where alternative development configurations can better meet the intent of the Design Guidelines. For example, an alternative scenario with reduced storefront percentage may allow a larger and more inviting plaza space than would be possible under the 50% frontage requirement.



Alternatives that reduce the percentage of storefront buildings should include design features that define the street edge and enhance the pedestrian environment. Vertical landscaping and/or architectural elements are important in framing the street edge.

East End NE 85th Street Frontage

h. Encourage development to locate and orient buildings towards the street with parking to the side or the rear: At a minimum this should include:

- Non-residential facades located directly adjacent to the sidewalk or buildings featuring a modest landscaped front yard area or plaza area between the sidewalk and the façade.
- Primary building entries and windows facing the street.
- Landscaping trimmed to maintain visibility between the sidewalk and the building.

Office and residential developments are encouraged to locate and orient buildings towards an interior open space or courtyard, where space allows. In this scenario, primary building entries may orient towards the open space provided there is direct visibility into the open space from the sidewalk. Windows should be provided on the street façade.

Buildings may be located towards the rear of the property provided they meet landscaping, parking, pathway, and façade standards along the front (see Figure 19).

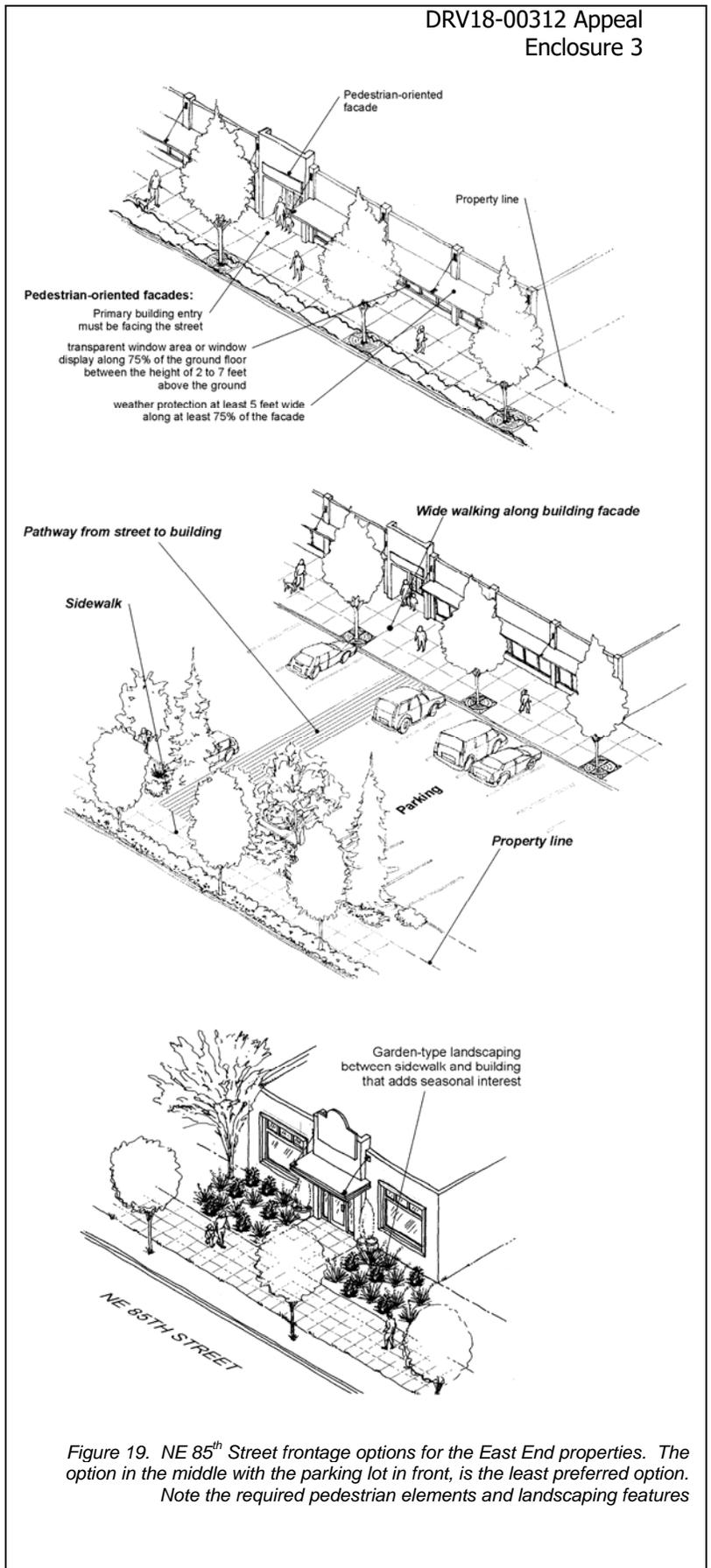


Figure 19. NE 85th Street frontage options for the East End properties. The option in the middle with the parking lot in front, is the least preferred option. Note the required pedestrian elements and landscaping features

6. Sidewalk and Pathway Widths

Objectives

- To provide wide sidewalks and pathways that promote an increase in pedestrian activity within the Rose Hill Business District.

Discussion

Sidewalks have three overlapping parts with different functions: the curb zone, the movement zone, and the storefront or activity zone. A well-sized and uncluttered movement zone allows pedestrians to move at a comfortable pace.

Guidelines

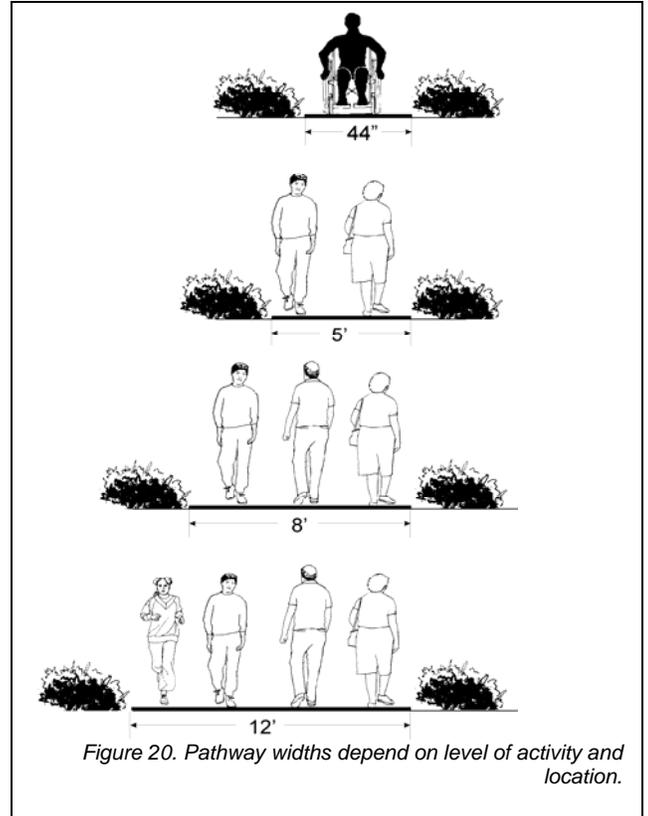
- Integrate a “curb zone” into the sidewalk or pathway width. This space can include street trees, newspaper stands, street signs, garbage cans, phone booths, mail boxes, etc. Subtle changes in paving patterns between the curb zone and the movement zone can be effective and should be considered.
- Sidewalks or pathways adjacent to moving vehicular traffic need generous buffers to make them safer and more inviting. Landscaping elements are particularly important physical and visual buffers between walkways and streets or other vehicle access areas. As a general rule, the higher the travel speed, the greater the buffer should be between moving cars and pedestrians.
- Design sidewalks and pathways to support a variety and concentration of activities and provide a separation for the pedestrian from the busy street. Specifically:

Considerations for the “movement zone” widths:

- Curb zones with parallel parking typically need 4'-6'; without parallel parking: 3'-4'.
- 12' accommodates 4 persons walking abreast.
- 8' accommodates 3 persons walking abreast.
- 5' accommodates 2 persons walking abreast.

Considerations for the “store front zone” width:

- Outdoor dining uses: 6' allows for one table.
- Outdoor displays typically need at least 4' (6' preferable).



7. Pedestrian Coverings

Objectives

- To provide shelter for pedestrians.
- To provide spatial enclosure and add design interest to retail or office streetscapes.

Discussion

The design and width of pedestrian coverings should be determined by their function, the building's use and the type of street.

As a general rule, the more traffic an entry is expected to accommodate, the larger the covered area should be at the entry. Larger porches and covered entries also invite pedestrian activity. For example, a 5' x 5' covered area allows two adults to converse comfortably out of the rain. A 3' to 4' canopy will provide rain cover for window-shopping, a 5' or greater canopy will provide cover for a street sale, and a 7' to 8' canopy will provide room for a window shopper and a passing couple.

The width of the sidewalk also should be considered when sizing the pedestrian covering (wider sidewalks can accommodate wider pedestrian coverings). Canopies and awnings should be appropriately dimensioned to allow for tree growth, where applicable. The architecture of the building and the spacing of individual storefronts should help determine the appropriate placement and style of the canopy or awning. Continuous, uniform awnings or canopies, particularly for multi-tenant retail buildings, can create a monotonous visual environment and are discouraged.

Guidelines

- a. Provide weather protection along the primary exterior entrance of all businesses, residential units, and other buildings.
- b. Design weather protection features to provide adequate width and depth at building entries.
- c. Pedestrian covering treatments may include: covered porches, overhangs, awnings, canopies, marquees, recessed entries or other similar features. A variety of styles and colors should be considered, where compatible with the architectural style of the building and the ground floor use.
- d. Back lit, plastic awnings are not appropriate.



Figure 22. Wider pedestrian coverings allow for outdoor dining



Figure 23. Note how these awnings have been integrated into the building's storefront spaces

8. Blank Walls

Objectives

- To minimize visible blank walls.
- To enhance public safety along sidewalks and pathways.
- To encourage design elements that enhance the character of buildings at all perceived distances.

Discussion

Blank walls on commercial street frontages deaden the pedestrian environment and can break the continuity of uses along a street or pathway. Blank walls can also create a safety problem, particularly where adjacent to pedestrian areas, as they don't allow for natural surveillance of those areas. (However, in some cases fire walls require the intrusion of a flat, unadorned surface). The adverse impact of a blank wall on the pedestrian streetscape can be mitigated through the methods listed in the Guidelines below.

Guidelines

Avoid blank walls near sidewalks, major internal walkways, parks, and pedestrian areas. The following treatments mitigate the negative effects of blank walls (in order of preference):

- a. Configure buildings and uses to minimize blank walls exposed to public view.
- b. Provide a planting bed with plant material to screen most of the wall.
- c. Install trellises with climbing vines or plant materials to cover the surface of the wall. For long walls, a trellis or trellises should be combined with other design treatments to avoid monotony.
- d. Provide artwork on the wall surface.
- e. Provide architectural techniques that add visual interest at a pedestrian scale. This could include a combination of horizontal building modulation, change in building materials and/or color, and use of decorative building materials.
- f. Other treatments may be proposed that meet the intent of the guidelines.



Figure 24. For large walls, landscaping beds with trees and shrubs are encouraged

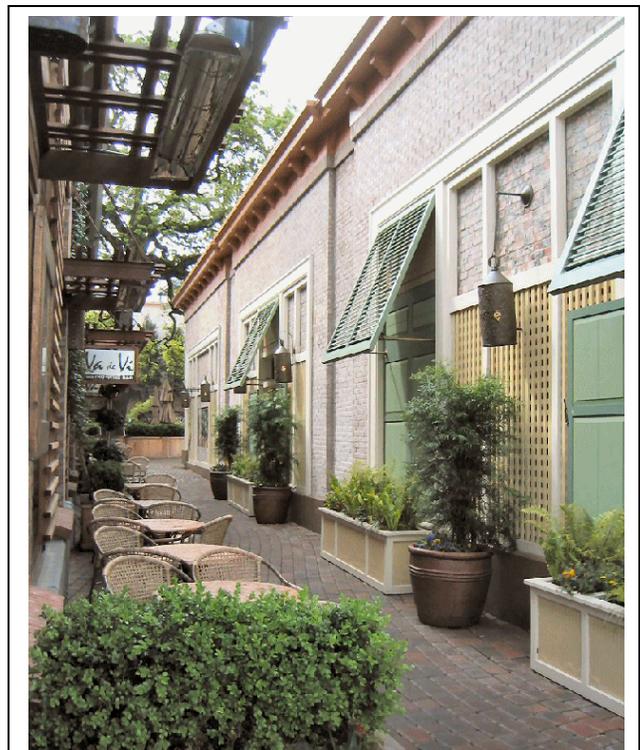


Figure 25 This building was a combination of alternating building materials, details, and landscaping elements to add visual interest at a close range

9. Lighting

Objectives

- To enhance safety.
- To create inviting pedestrian areas.
- To provide adequate lighting without creating excessive glare or light levels.

Discussion

Overpowering and uniform illumination from commercial uses creates glare and destroys the quality of night light, especially for adjacent residential areas. Well placed light fixtures will form individual pools of light and maintain sufficient lighting levels for security and safety purposes.

Guidelines

- Provide adequate lighting levels in all areas used by pedestrians and automobiles, including building entries, walkways, parking areas, circulation areas, and open spaces. Recommended minimum light levels:
 - Building entries: 4 foot candles
 - Primary pedestrian walkway: 2 foot candles
 - Secondary pedestrian walkway: 1-2 foot candles
 - Parking lot: .60 -1 foot candle
 - Enclosed parking garages for common use: 3 foot candles
- Lighting should be provided at consistent levels, with gradual transitions between maximum and minimum levels of lighting and between lit areas and unlit areas.
- Building facades in pedestrian areas should provide lighting to walkways and sidewalks through building mounted lights, canopy- or awning-mounted lights, and display window lights. Encourage variety in the use of building-mounted light fixtures to give visual variety from one facade to the next.
- Minimizing impacts of lighting on adjoining activities and uses should be considered in the design of lighting. This is particularly important adjacent to residential uses.
- Parking lot light fixtures should be non-glare and mounted no more than 15'-20' above the ground. Lower level light fixtures are preferred to maintain a human scale. Ideally, all exterior fixtures should be fitted with a full cut-off shield to minimize light spill over onto adjoining properties.



10. Pedestrian Amenities

Objectives

- To provide amenities that enrich the pedestrian environment.
- To increase pedestrian activity in the Rose Hill Business District.

Discussion

Site features and pedestrian amenities, such as lighting, benches, paving, waste receptacles, and other site elements, are an important aspect of a business district's character. These elements reduce apparent walking lengths and unify the district's visual character.

Guidelines

Provide pedestrian amenities along all sidewalks, interior pathways, and within plazas and other open spaces. Desired amenities include:

- a. Pedestrian-scaled lighting (placed between 12'-15' above the ground).
- b. Seating space. This can include benches, steps, railings and planting ledges. Heights between 12" to 20" above the ground are acceptable, with 16" to 18" preferred. An appropriate seat width ranges from 6" to 24".
- c. Pedestrian furniture such as trash receptacles, consolidated newspaper racks, bicycle racks, and drinking fountains.
- d. Planting beds and/or potted plants.
- e. Unit paving such as stones, bricks, or tiles.
- f. Decorative pavement patterns and tree grates.
- g. Water features.
- h. Informational kiosks.
- i. Transit shelters.
- j. Decorative clocks.
- k. Artwork.



Figure 27. Consolidated newspaper racks



Figure 28. Bicycle racks



Figure 29. Potted plants



Figure 31. Decorative pavement patterns (top), benches and pedestrian-scale lighting (middle), and informational kiosk (bottom)



Figure 30. This example combines a sculptural water feature with landscaping

11. Interior Pedestrian Connections

Objectives

- To enhance pedestrian access to the street, adjacent uses, and adjacent sites, where desirable.
- To make it easier to walk between uses on the NE 85th Street corridor.
- To reduce vehicle trips within the corridor.
- To promote pedestrian activity.
- To enhance pedestrian access through parking lots and between the street and uses.

Guidelines

- a. Provide convenient pedestrian access between the street, bus stops, buildings, parking areas, and open spaces. Internal pedestrian connections are particularly important on large sites where some uses may be placed away from a street.
- b. Design all buildings abutting a public sidewalk or major internal pathways to provide direct pedestrian access to the sidewalk or pathway.
- c. Provide interior pedestrian connections to adjacent properties containing similar uses or complementary uses. This is most applicable to large lots and where storefronts or other uses are set back away from the street. Where an existing connection is not desirable or possible due to the nature of development on the adjacent site, the applicant should provide an opportunity for a future pedestrian connection where such a connection is desirable and future redevelopment of the adjacent site is possible.
- d. Provide paved walkways through large parking lots. One walkway should be provided for every three parking aisles. Such access routes through parking areas should be separated from vehicular parking and travel lanes by use of contrasting paving material which may be raised above the vehicular pavement and by landscaping.

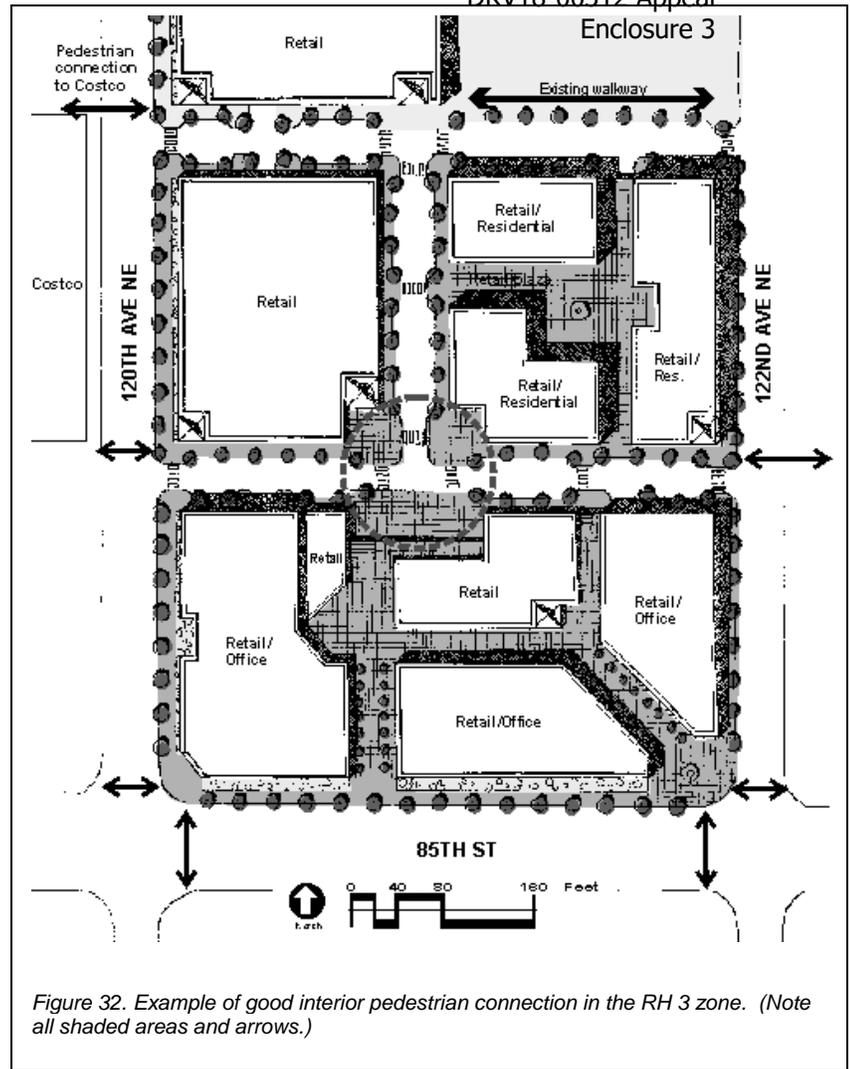


Figure 32. Example of good interior pedestrian connection in the RH 3 zone. (Note all shaded areas and arrows.)

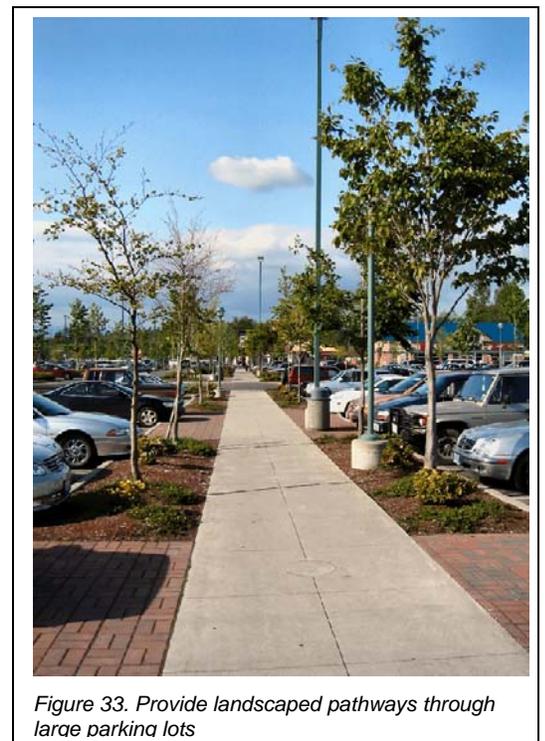


Figure 33. Provide landscaped pathways through large parking lots

12. Pedestrian Plazas

Objectives

- To provide a variety of pedestrian-oriented areas to attract shoppers to commercial areas and enrich the pedestrian environment.
- To create gathering spaces for the community.
- To configure buildings and uses to encourage pedestrian activity and pedestrian focal points.

Guidelines

- a. Provide pedestrian plazas in conjunction with non-residential uses.
- b. Position plazas in visible locations on major internal circulation routes, close to bus stops, or where there are strong pedestrian flows on neighboring sidewalks. For large sites, development should be configured to create a focal plaza or plazas. Plazas should be no more than 3' above or below the adjacent sidewalk or internal pathway to enhance visibility and accessibility.
- c. Incorporate plenty of benches, steps, and ledges for seating. A combination of permanent and moveable seating is encouraged. Seating areas should be provided with views of amenities, landscaping elements, or people watching.
- d. Provide storefronts, street vendors, or other pedestrian-oriented uses, to the extent possible, around the perimeter of the plaza
- e. Provide landscaping elements that add color and seasonal interest. This can include trees, planting beds, potted plants, trellises, and hanging plants.
- f. Incorporate pedestrian amenities, as described in Section 10.
- g. Consider the solar orientation and wind patterns in the design of the open space and choice of landscaping.
- h. Provide transitional zones along building edges to allow for outdoor eating areas and a planted buffer.



Figure 34. Good examples of pedestrian plazas. Notice the decorative pavements, landscaping components, adjacent building facades, and other amenities and design details

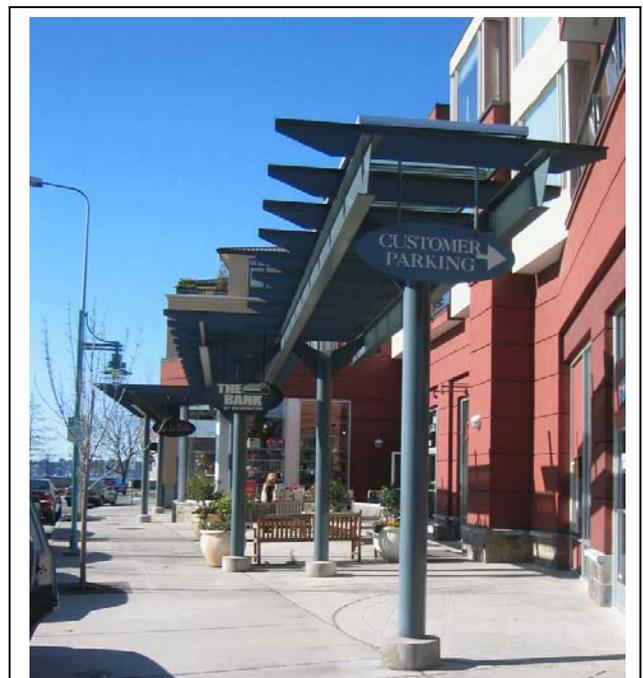


Figure 35. An example of an attractive small plaza space between a sidewalk and a storefront

13. Residential Open Space

Objectives

- To create useable space that is suitable for leisure or recreational activities for residents.
- To create open space that contributes to the residential setting.

Guidelines

- a. Incorporate common open space into multi-family residential uses. Special recommendations for common open space:
 - Consider open space as a focal point of the residential development.
 - Open space should be large enough to provide functional leisure or recreational activity. For example, long narrow spaces rarely, if ever, can function as usable common space.
 - Open space should provide for a range of activities and age groups. Children's play areas in particular should be visible from dwelling units and positioned near pedestrian activity.
 - Residential units adjacent to the open space should have individual entrances to the space. Preferably, these units should include a small area of semi-private open space enclosed by low level landscaping or hedges (no taller than 42").
 - Open space should feature paths, seating, lighting, and other pedestrian amenities to make the area more functional and enjoyable. It should be oriented to receive sunlight, (preferably south).
 - Separate common space from ground floor windows, streets, service areas, and parking lots with landscaping and/or low-level fencing. However, care should be used to maintain visibility from dwelling units towards open space for safety.
- b. Provide private open space for multi-family residential units. For townhouses and other ground-based housing units, provide patios, decks, and/or landscaped front or rear yards adjacent to the units. For all other units, provide balconies large enough to allow for human activity.



Figure 36. Good examples of common open space, including street-level courtyards (left), a children's play area (top right), and a pedestrian corridor (lower right)

14. Parking Lots and Vehicular Circulation

Objectives

- To minimize the impact of parking facilities on the fronting street, pedestrian environment, and neighboring properties.
- To enhance pedestrian and vehicular safety.
- To maintain desired traffic flow on NE 85th Street.
- To promote shared parking
- To provide attractive and connected vehicular circulation routes.

Discussion

Parking lots can detract from the pedestrian and visual character of a commercial area. The adverse impacts of parking lots can be mitigated through sensitive design, location, and configuration. Large parking lots can be confusing unless vehicle and pedestrian circulation patterns are well organized and marked. The NE 85th Street Subarea Plan encourages shared parking between properties to reduce curb cuts reduce congestion of cars turning in and out of parking lots and consolidating consumer trips between businesses.

Where not specifically prohibited, drive-through facilities for some uses such as fast food restaurants, pharmacies, or auto oriented uses may be appropriate if designed to minimize vehicle queuing along rights of way, blocking driveways or parking aisles, or impeding pedestrian movement. Aesthetically, drive-throughs should be located away from street frontages or screened as viewed from the right of way.

Guidelines

Driveways And Vehicular Circulation

- a. Minimize the number of curb cuts into a development, particularly off of NE 85th Street. To the extent possible, adjacent developments should share driveways.
- b. Develop an efficient internal vehicular access system that minimizes conflicts with pedestrians and NE 85th Street traffic flow.
- c. Configure internal access roads to look and function like public streets. This is most applicable larger sites, such as those in the Regional Center, where an internal vehicular circulation system is critical to access interior portions of the sites. The most desirable configuration would include on-street parking, street trees, and sidewalks on both sides of the roadway. Figure 7, in the Overview of Three Design Districts section, provides a good example of how a redevelopment scenario for the RH 3 zone could accommodate an internal roadway network that looks and functions like public streets.
- d. Configure development to provide interior vehicular connections to adjacent uses, where desirable. Where current connections to adjacent uses are not feasible, but desirable in the future, configure development to provide the opportunity for a future connection, should the adjacent site be redeveloped.
- e. Avoid parking lot configurations with dead-end lanes

Parking Lot Location and Design

- f. Locate vehicular parking areas to the side or rear of buildings, to the extent possible. This is most important on street corners and in the Neighborhood Center, where a concentration of storefronts along the street is desired.

- g. Avoid parking layouts that visually dominate a development. Break up large parking lots into smaller ones.
- h. Take advantage of topography to hide parking underneath buildings.
- i. Provide a clear and well organized parking lot design. Space should be provided for pedestrians to walk safely in all parking lots.

Parking Lot Landscaping

- j. Integrate landscaping into parking lots to reduce their visual impact. Provide planting beds with a variety of trees, shrubs, and ground cover to provide visual relief, summer shade, and seasonal interest.

Parking Lot Screening

- k. Provide low level screening and perimeter landscaping where parking is adjacent to sidewalks in order to improve visual qualities and reduce clutter. While vertical elements such as trees, are encouraged to define the street edge, all screening methods should maintain visibility at eye level between the street and parking area. For instance, hedges or walls should not be taller than 3 feet and trees should be trimmed to allow visibility between 3 and 8 feet above the ground.
- l. Provide extensive screening and landscaping between parking lots and residential uses and open spaces. A combination of a screen wall with a landscape buffer is preferred.

Drive-Through Facilities

- m. Design drive-through windows should be oriented away from the street frontage and preferably not located between a building and the street. Where drive-through lanes face a street, avoid large featureless walls and provide sufficient landscaping to soften the visual impact of vehicle stacking areas for drive through windows. Locate driving curb cuts and lanes so as not to interfere with pedestrian or vehicular circulation.

15. Parking Garages

Objectives

- To mitigate the visual impacts of parking garages in the urban environment.

Guidelines

- a. Mitigate the intrusive qualities of parking garages. Along streets, pedestrian pathways, and in pedestrian areas, ground-level commercial uses should be incorporated into parking structures. Extensive landscaping should be used to screen the parking garage near residential areas and in high visibility locations.
- b. Design and site parking garage entries to complement, not subordinate the pedestrian entry. If possible, locate the parking entry away from the primary street, to either the side or rear of the building.
- c. Utilize similar architectural forms, materials, and/or details to integrate the garage with the development.
- d. Locate parking structure service and storage functions away from the street edge and generally not visible from the street or sidewalks.



16. Architectural Style

Objectives

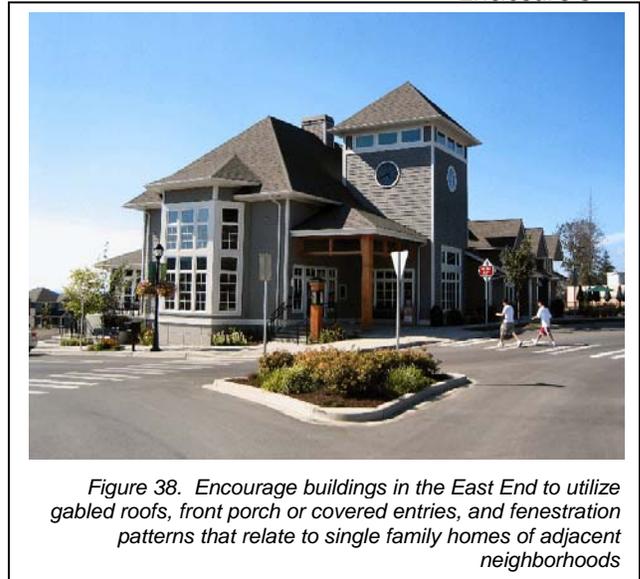
- To improve the architectural design of commercial buildings in the business district.
- To provide architecture that fits into the context of the adjacent uses surrounding the business district.

Discussion

As there is no single predominate architectural style in the Rose Hill Business District, the guidelines provide flexibility on the chosen styles (provided the architectural scale, human scale, building details, and building materials and color standards in KZC Chapter 92 and these guidelines are met).

Guidelines

- a. Discourage architecture that is defined predominately by corporate identity features and may be difficult to adapt to future uses. For example, some fast food franchises have very specific architectural features that reinforce their identity as a generic national chain and are not adaptable to other uses when a franchise relocates.
- b. Encourage buildings in the East End to utilize architectural styles common to neighboring residential areas. This includes gabled roofs, front porches or covered entries, and fenestration patterns that relate to adjacent single family homes.



17. Architectural Scale

Objectives

- To encourage an architectural scale of development that is compatible with the vision for the three design districts within the Rose Hill Business District.
- To add visual interest to buildings.

Discussion

“Architectural scale” means the size of a building relative to the buildings or elements around it. When the buildings in a neighborhood are about the same size and proportion, we say they are “in scale.” As both the vision and development regulations for the Rose Hill Business District provide for much larger buildings than currently exist, special care must be taken to design buildings so they do not overpower the others. The exception to this rule is an important civic or cultural building that has a prominent role in the community.



Guidelines

A combination of techniques is desirable to reduce the architectural scale of buildings. Specifically, these techniques are encouraged at intervals (30 feet in the East end, 50 feet in the Neighborhood Center, and 70 feet in the Regional Center). Residential uses throughout the Rose Hill Business District warrant such techniques at 30-foot intervals. Office buildings are provided greater flexibility (see office design guidelines section). Alternatives will be considered provided they meet the intent of the guidelines.

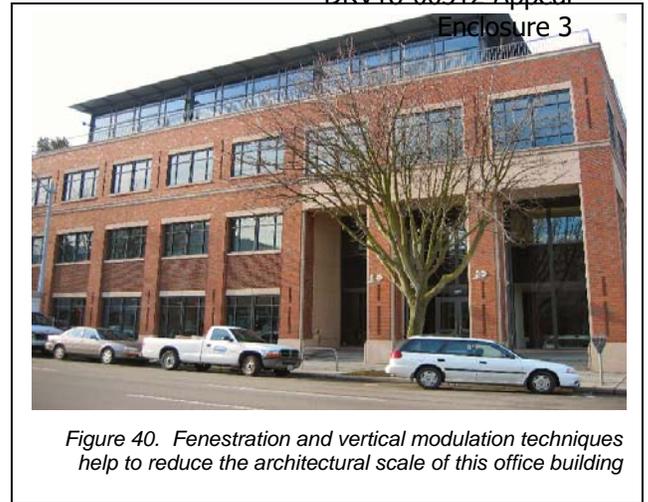


Figure 40. Fenestration and vertical modulation techniques help to reduce the architectural scale of this office building

- a. Incorporate fenestration techniques that indicate the scale of the building. For example, the size, location, and number of windows in an urban setting create a sense of interest that relies on a subtle mixture of correct ratios, proportions, and patterns. This is particularly important on upper floors, where windows should be divided into units no larger than 35 square feet, with each window unit separated by a visible mullion or other element. “Ribbon windows” (continuous horizontal bands of glass) or “window walls” (glass over the entire surface) do little to indicate the scale of the building and are thus discouraged, except in special circumstances where they serve as an accent element.
- b. Encourage vertical modulation on multi-story buildings to add variety and to make large buildings appear to be an aggregation of smaller buildings. Vertical modulation may be particularly effective for tall buildings adjacent to a street, plaza, or residential area to provide compatible architectural scale and to minimize shade and shadow impacts. Vertical modulation is well-suited for residential development and sites with steep topography.
- c. Encourage a variety of horizontal building modulation techniques to reduce the architectural scale of the building and add visual interest. Horizontal building modulation is the horizontal articulation or division of an imposing building façade through setbacks, awnings, balconies, roof decks, eaves, and banding of contrasting materials. Elevations that are modulated with horizontal elements appear less massive than those with sheer, flat surfaces. Specifically:
 - For single purpose retail buildings, utilize horizontal building modulation with roofline modulation and a change in building materials, as necessary to meet the objectives of the guidelines from all perceived distances. This is particularly important for large scale retail buildings (over 40,000 square feet) or multi-tenant retail buildings placed adjacent to a parking lot where they can be viewed from relatively great distances.
 - For residential uses, provide horizontal building modulation based on individual unit size. Horizontal modulation is most effective when combined with roofline modulation and changes in color and/or building materials. The depth and width of the modulation should be sufficient to meet the objectives of the guidelines. Avoid repetitive modulation techniques, since they may not be effective when viewed from a distance. Larger residential buildings will require greater horizontal modulation techniques to provide appropriate architectural scale.

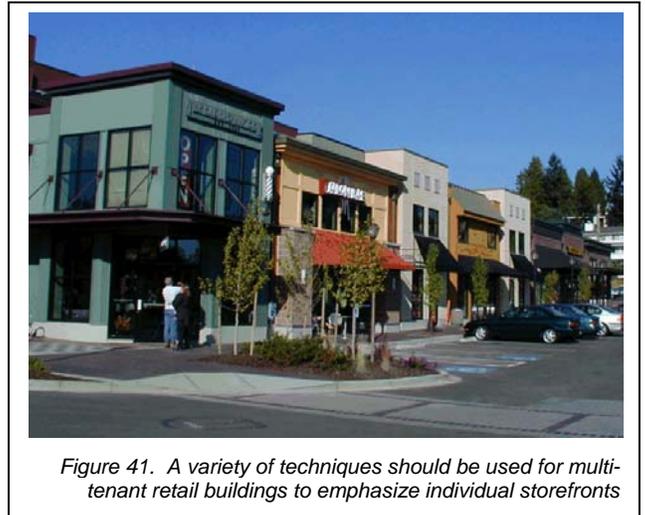


Figure 41. A variety of techniques should be used for multi-tenant retail buildings to emphasize individual storefronts

- d. Office buildings. Utilize design techniques to break up long continuous walls. A combination of horizontal building modulation, change in fenestration, and/or change in building materials should be used to accomplish this.
- e. Encourage a variety of roofline modulation techniques. This can include hipped or gabled rooflines and modulated flat rooflines. Hipped and gabled rooflines are preferred for multi-family buildings and buildings in the East End. As a general rule, the larger the building or unbroken roofline, the bigger the modulation should be. In determining the appropriate roof type and amount of modulation, consider at what distance the building can be viewed. For example, a large commercial building adjacent to a parking lot is capable of being viewed from a relatively large distance. Consequently the roofline modulation techniques must be sufficient to provide an appropriate architectural scale that provides visual interest.

18. Human Scale

Objectives

- To encourage the use of building components that relate to the size of the human body
- To add visual interest to buildings.

Guidelines

- a. Encourage a combination of architectural building elements that lend the building a human scale. Examples include arcades, balconies, bay windows, roof decks, trellises, landscaping, awnings, cornices, friezes, art concepts, and courtyards. Window fenestration techniques described in Section 17 can also be effective in giving humans clues as the size of the building. Consider the distances from which buildings can be viewed (from the sidewalk, street, parking lot, open space, etc.).

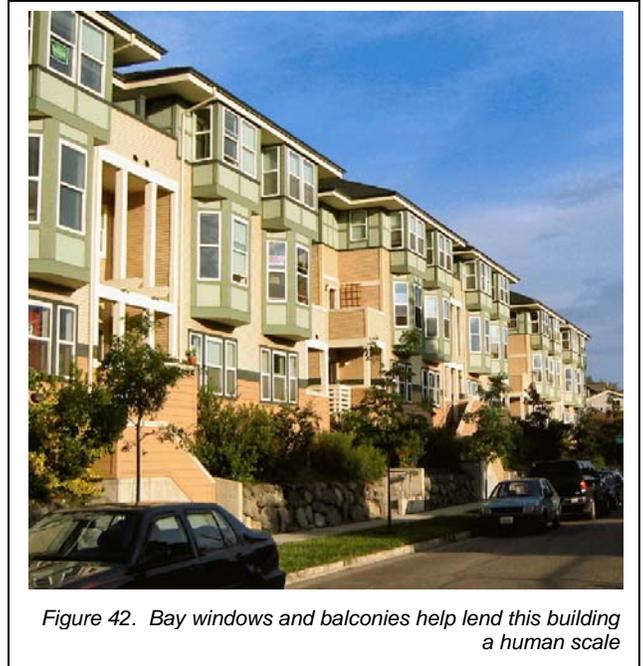


Figure 42. Bay windows and balconies help lend this building a human scale

19. Building Details and Materials

Objectives

- To utilize details that add visual interest to buildings and sites at a pedestrian scale.
- To utilize a variety of quality building materials such as brick, stone, glass, timber, and metal, which are appropriate to the Pacific Northwest climate, and complementary to the desired visual character of the district.

Guidelines

- a. Encourage the integration of ornament and applied art with the structures and the site environment. For example, significant architectural features should not be hidden, nor should the urban context be overshadowed. Emphasis should be placed on highlighting building features such as doors, windows, eaves, and on materials such as wood siding and ornamental masonry. Ornament may take the form of traditional or contemporary elements. Original artwork or hand-crafted details should be considered in special areas. Ornament and applied art can be used to emphasize the edges and transition between public and private space, and between walls to ground, roof to sky, and architectural features to adjacent elements. Ornament may consist of raised surfaces, painted surfaces, ornamental or textured banding, changing of materials, or lighting. The use of overly ornate details, however, can degrade the integrity of the district, and thus is discouraged.
- b. Utilize a variety of quality building materials such as brick, stone, timber, and metal, to add visual interest to the buildings and reduce their perceived scale. Masonry or other durable materials should be used near the ground level (first 2 feet above sidewalk or ground level).
- c. Limit the use of concrete block, metal siding, and stucco or similar materials including Exterior Insulation and Finish System (EIFS) on all visible building facades from the street and pedestrian routes and near primary entrances. Such materials should be trimmed properly and used in conjunction with other preferred materials. EIFS should be sheltered from extreme weather by roof overhangs or other methods.



Figure 43. Consider changes in building materials with modulation techniques



Figure 44. A combination of materials is preferred

20. Signs

Objectives

- To encourage the use of creative, well-crafted signs that contribute to the character of the district.

Discussion

Kirkland's Zoning Code regulates signs throughout the city in order to create a high-quality urban environment. Automobile-oriented signs typically found on commercial strips can be overpowering and obtrusive. Pedestrian signs are smaller and closer to viewers; thus, creative, well-crafted signs are more cost effective than large signs mounted high on poles. A balance between the needs of a high traffic corridor and pedestrians should be considered in the design of signs. Signs should be an integral part of a building's façade or act as a center identification for the passing motorist to a commercial center. The location, architectural style, and mounting of signs should conform to a building's architecture and not cover up or conflict with its prominent architectural features.

Guidelines

- Provide pedestrian oriented signs on all commercial facades where adjacent to a sidewalk or walkway. This includes signs located within 15' of the ground plane, such as "blade" signs which hang below canopies. Small signs located on canopies or awnings are also effective along building facades at the street. Signs with quality graphics and a high level of craftsmanship are important in attracting customers. Sculpted signs and signs that incorporate artwork add interest.
- External lighting is preferred. If internal lit cabinet signs are used, darker background with lighter lettering is more aesthetically pleasing. Neon signs are appropriate when integrated with the building's architecture.
- Ground-mounted signs should feature a substantial base and be integrated with the landscaping and other site features.
- Mounting supports should reflect the materials and design character of the building or site elements or both.
- Master-planned, larger commercial centers are encouraged to combine signage for the whole complex that complements the architectural design of the center and is oriented to automobile traffic.

21. Service Areas

Objectives

- To provide essential service areas without adversely impacting the quality of development.
- To locate and design service and storage areas to promote ease of use, safety, and visual cohesion.

Guidelines

- Locate and design service and storage areas to minimize impacts on the pedestrian environment and adjacent uses. Service elements should generally be concentrated and located where they are accessible to service vehicles and convenient for tenant use.
- The design of service enclosures should be compatible with the design of adjacent buildings. This may be accomplished by the use of similar building materials, details, and architectural styles. Such enclosures should be made of masonry, ornamental metal, heavy wood timber, or other durable materials.

- c. Roof-mounted mechanical equipment should be located so as not to be visible from the street, public open space, parking areas, or from the ground level of adjacent properties. Screening features should blend with the architectural character of the building. Screening of equipment and their location should be included in the early design of the building.

22. Visual Quality of Landscapes

Objectives

- To enhance the visual quality of the urban environment.

Discussion

The relationship between landscaping and architecture is symbiotic; plant materials add to a building's richness, while the building points to the architectural qualities of the landscaping. Foliage can soften the hard edges and improve the visual quality of the urban environment. Landscaping treatment in the urban environment can be categorized as a pedestrian/auto, pedestrian, or building landscape.

The pedestrian/auto oriented landscape applies to where the pedestrian and auto are in close proximity. Raised planting strips can be used to protect the pedestrian from high-speed and high-volume traffic. Street trees help create a hospitable environment for both the pedestrian and the driver by reducing scale, providing shade and seasonal variety, and mitigating noise impacts.

Pedestrian landscape offers variety at the ground level through the use of shrubs, ground cover, and trees. Pedestrian circulation, complete with entry and resting points, should be emphasized. If used effectively, plant materials can give the pedestrian visual cues for moving through the urban environment. Plant materials that provide variety in texture, color, fragrance, and shape are especially desirable.

The building landscape refers to landscaping around urban buildings, particularly buildings with blank walls. Landscaping around buildings can reduce scale and add diversity through pattern, color, and form.

Guidelines

- a. Consider the purpose and context of the proposed landscaping. The pedestrian/auto oriented landscape requires strong plantings of a structural nature to act as buffers or screens. The pedestrian landscape should emphasize the subtle characteristics of the plant materials. The building landscape should use landscaping that complements the building's favorable qualities and screens its faults while not blocking views of the business or signage.

Other considerations:

- Encourage a colorful mix of drought tolerant and low maintenance trees, shrubs, perennials. Except in special circumstances ivy and grass lawn should be avoided.
- Encourage the use of rose bushes in highly visible locations together with other plants to reinforce the identity of "Rose Hill" (low maintenance and drought tolerant varieties).
- Take advantage of on-site topography to hide parking and enhance views.
- Utilize wooded slopes as a natural site amenity and to screen unwanted views, where applicable.

23. Territorial Views to the West and North

Objectives

- To maintain and enhance mountain views from NE 85th Street.
- To encourage development to take advantage of views, while minimizing public view impacts.

Discussion

Views of the Olympic Mountains give the Rose Hill Business District its sense of place within the regional context. Maintaining public views and enhancing natural land forms is an important value to the design character of Kirkland. The scale relationships of built forms to their terrain should minimize visual barriers to views and lessen the impact on surrounding neighborhoods. This is especially relevant to zones in the Regional Center, terracing, the stepping down of horizontal elements, are effective ways to develop hillsides and maintain views.

The visual character of a landscape should be reflected in the buildings. Buildings that do not conform to steep inclines detract from the natural features of the site and should be avoided. In contrast, buildings that use the terrain as an opportunity for variation in the built form easily fit into their setting without disruption. Terracing a building to roughly parallel the slope of a site will create a building envelope that follows the contour of its property. Terraced roof decks, modulated roofs, and sloped roofs can carry out this objective.

Guideline

- a. Encourage rooflines to roughly follow the slope of the existing terrain. Parking garages should be terraced into slopes to minimize building bulk. Existing public views should be maintained. This can be accomplished by widening setbacks as development approaches toward I-405. Buildings should step down hillsides. Buildings and rooftop appurtenances should be sited to maximize public views.

THE CITY OF KIRKLAND

Design Guidelines for Residential Development

Adopted by the City Council
Pursuant to Kirkland Municipal Code
Section 3.30.040, Ordinance 3606 on
December 18, 1997
Revised by Ordinance 4496 on
December 8, 2015

Attest:



Amy Walen,
Mayor



Eric Shields,
Director,
Planning and Building

(December 2015 Revision)



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INTRODUCTION

This document sets forth a series of general design guidelines for **both single-family and multifamily** residential development adopted by Section 3.30 of the Kirkland Municipal Code that will be used by the City in the design review process for attached or stacked dwelling units within the NE 85th Street Subarea, the PLA 5C Zone, and the Market Street Corridor. For projects required to be reviewed by the Design Review Board, the Board will use these guidelines in association with the Design Regulations of the Kirkland Zoning Code. To the extent that the standards of the Design Guidelines or Design Regulations address the same issue but are not entirely consistent or contain different levels of specificity, the Design Review Board will determine which standard results in superior design. For Administrative Design Review (ADR), the Planning Official will use these guidelines when necessary to interpret the Design Regulations.

The design guidelines are also intended to assist project applicants and their architects by providing graphic examples of the intent of the City's guidelines and regulations for attached or stacked dwelling units. Not all of these guidelines will result in design regulations. Zoning Code regulations relating to single-family residential development will be limited in order to provide for freedom of design.

The purpose of these design guidelines is to encourage residential development that creates livable residential communities and reinforces the positive qualities of the City's existing neighborhoods.

SITE PLANNING AND RELATIONSHIP TO THE STREET

Introduction

Good site design creates developments that respond in a positive way to both the conditions of the site and the context of the surrounding neighborhood. The location of structures and their relationship to the street, incorporation of open space within the development, landscaping, preservation of existing vegetation, and the layout of the parking areas are all part of what makes a development successful. These elements also determine if the development will be a positive addition to the neighborhood.

Building Setbacks

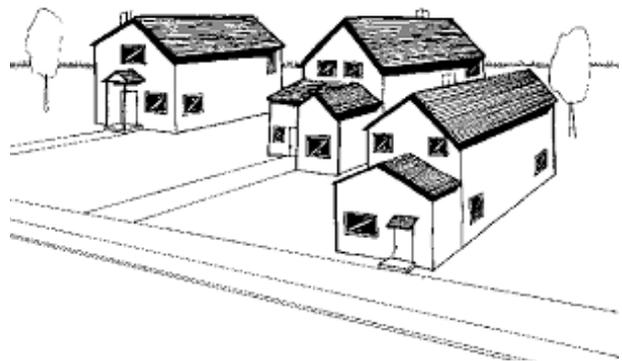
Issue

Building setbacks establish a pattern along the street and provide a semi-private space for residents.



Discussion

The setbacks of residences along the street create a rhythm, which adds to the atmosphere of the streetscape. If the setback area between the right-of-way and the residence is designed properly, it will provide a buffer zone for the residents while still allowing social interaction with passersby. If a building is set too close to the right-of-way, it can disrupt this buffer zone.



▲ Buffer zone disrupted by house too close to the street.

Guideline

New buildings should be set back from the right-of-way to provide semi-private areas for residents and open space along the street.

Entries

Issue

Distinct entryways provide a transition between the street and the inside of the residence.



Discussion

The front yard and entryway act as a visual and physical transition leading to the private area of the residence. This semi-private space provides a welcoming spot for guests, a secure area for those who live there, a visible connection between the neighborhood and the residence, and fosters community interaction.

The entrance to a residence, or some indication of it, should be visible from the street and should not have to compete with the driveway or garage to be noticed. Since the entry area is as much a part of the semi-private space of the yard as of the private area of the house, it should be allowed to intrude into a portion of the front setback yard



Guidelines

Entrances should be located on the front facades of residences and should be clearly visible from the street.

Covered entries and porches should be allowed to project into a portion of front setback yards.

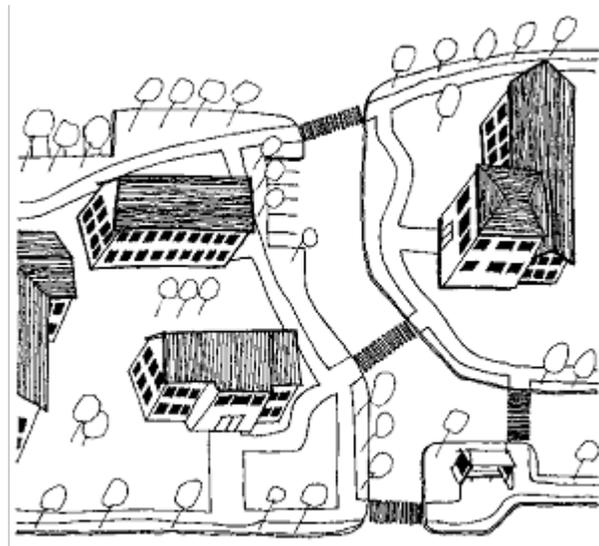
Pedestrian Connections

Issue

Well-defined, direct pedestrian connections from the building to the street are necessary for multifamily residential developments.

Discussion

The ability to walk into a multifamily residential development from the public sidewalk or a bus stop is essential to both pedestrian and vehicular safety. Direct pedestrian connections that are defined by the use of paving and landscaping provide an important link between the building and the street.



Guideline

Multifamily developments should have well defined, safe pedestrian walkways that minimize distances from the public sidewalk and transit facilities to the internal pedestrian system and building entrances.

Blank Walls

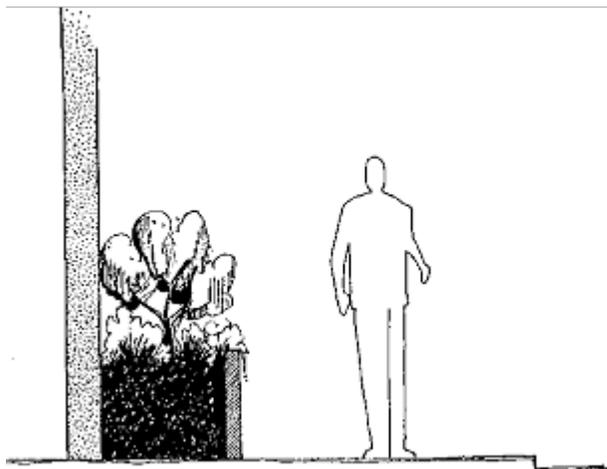
Issue

Blank walls detract from the visual character of buildings.

Discussion

Blank walls detract from their surroundings when they occur on the street front elevations of buildings and pedestrian areas. In situations where a blank wall is a development necessity, the adverse impact on streets, parks, and pedestrian areas can be mitigated through landscaping, seating, or architectural treatment.

Examples of such treatment include installing trellises for plants, providing landscaped planting beds to screen the wall, and incorporating decorative tile or masonry into the wall design.



▲ *Blank wall treatment*

Guideline

Blank walls should be avoided near sidewalks, parks, and pedestrian areas. Where unavoidable, blank walls should be enhanced with landscaping or architectural treatments.

Infill

Issue

Infill development can be designed to protect neighbors' privacy.

Discussion

Infill development can have adverse effects upon neighboring properties if the location and nature of existing development on adjacent lots is not taken into account. Window location, driveway screening, and siting of new buildings are important design issues when trying to protect the privacy of the users of both outdoor and indoor space on adjacent lots.



Guidelines

Infill development should be designed to minimize the disruption of privacy for indoor and outdoor activities on adjacent properties.

Rear lot driveways should be screened with a fence or landscaping unless the driveway is shared by the affected development.

Accessory Structures

Issue

The design and location of accessory structures can impact the character of the site and the neighborhood.



Discussion

Accessory structures can be designed in a way that will be in character with the primary residential structure on the site. The size and location of an accessory structure such as an accessory dwelling unit, detached garage or storage shed, and the location of the entrance to an accessory dwelling unit determine the extent the structure will impact the neighborhood. An accessory dwelling unit in a single-family zone should be designed to maintain the single-family look of the primary house on the lot.

Guideline

The size and design of accessory structures should make them unobtrusive and consistent with the character of the primary structure and the neighborhood.

PARKING LOCATION AND DESIGN

Introduction

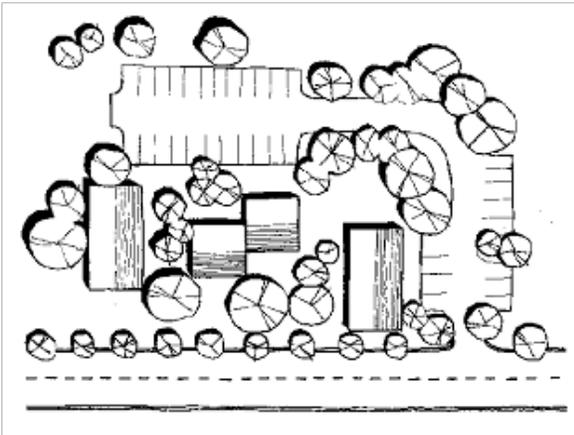
Parking is an important part of a residential development. Parking lot location, entrances and circulation, pedestrian safety, landscaping, and parking garage design are all considerations when developing a residential project. Improperly located and poorly designed parking areas can overwhelm the positive aspects of a residential project and make it a detriment to the neighborhood where it is located.

Parking Locations, Entrances, and Landscaping

Issue

Parking lots can have negative impacts on the visual character and pedestrian orientation of residential developments.

Discussion



Parking lots are typically unsightly and require vast quantities of space, but the adverse impacts of parking lots can be mitigated through sensitive design. It is best to locate lots to the back or side of buildings. Large parking lots can be broken up into smaller lots to serve residents more conveniently and allow for natural surveillance. When this is not possible, landscaping can be used to break up and screen the parking areas as long as clear lines of sight are maintained to increase safety.

Parking lot entrances disrupt pedestrian movement and through-traffic on the adjoining street. Potential conflict is reduced and land is used more efficiently if parking lots are accessed by a limited number of entrances.

Perimeter landscaping that forms a screen can separate parking lots from adjacent uses or the public rights-of-way. Trees along the edges of and within parking lots can effectively soften an otherwise barren space.

Interior plantings can be consolidated to provide islands of greenery or be planted at regular intervals. Use of drought-tolerant plants can improve the likelihood that the landscaping will survive and remain attractive.

Guidelines

Locate parking areas to the side, to the rear, or within structures whenever possible. Multiple, scattered, small parking areas that are away from the street are also desirable. When large paved areas are necessary, existing vegetation, topography, or new landscaping should be used to break them up internally and screen them from adjacent properties.

Locate parking areas to allow natural surveillance by maintaining clear lines of sight for those who park there and for occupants of nearby buildings within the development.

Minimize the number of driveways and encourage combined parking lot entrances.

Integrate parking lots into the surrounding community and the site by creatively using landscaping to reduce their visual impact. Require less landscaping if existing vegetation is preserved or if the lot is hidden from view.

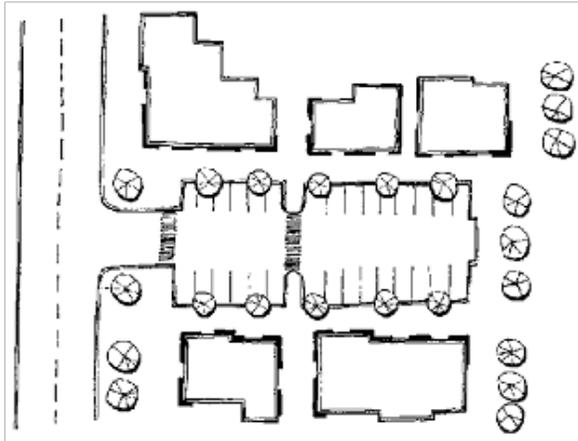
Pedestrian Circulation Within Parking Areas

Issue

Safe circulation patterns within parking areas are necessary for pedestrians.

Discussion

Good pedestrian circulation is a critical element of parking lot design. All parking lots need a clear path from the sidewalk to the building entrance. Large lots also require circulation routes from stalls to building entrances. A separate pedestrian area in front of the main building entrance provides a safe stopping point before entering the building. Where appropriate, pedestrian access to adjacent properties can also be made available.



Guideline

Parking lot design should provide clear and well organized routes for pedestrians.

Garages and Carports

Issue

Garages and carports are often unsightly and do not blend with residential development.

Discussion

Single-family garages and carports often dominate the streetscape and detract from the pedestrian orientation of the neighborhood. This can also be true of poorly designed parking garages and carports for multifamily developments.



If alleys are used for access, street character is improved by eliminating driveways and street facing garages. The neighborhood becomes more comfortable for pedestrians when sidewalks are uninterrupted by driveways and front yards are free of driveways, garages, and parked cars.

Architectural elements and landscaping can help screen carports and the bulk of multifamily parking garages. They can also help provide the appearance of a solid base if an open air garage is on the first floor of the building. If garage entrances are minimized, they will not dominate the street frontage of a building.



Guidelines

Attached garages should not dominate the building front.

The roof forms and materials used for carports should match the residential structures that they are associated with.

Garages should derive access from alleys, where possible.

Architectural elements and landscaping should be used to break up the bulk of parking garages; to visually connect multifamily parking garages to the ground; and to screen multifamily carports.

SCALE

Introduction

The scale of a building is the perceived size of that building relative to a person or the building's surroundings. The term "human scale" is used to indicate a building's perceived size relative to a person, and the term "architectural scale" refers to the size of the building relative to the buildings or elements around it.

Although the actual size of a building makes a difference, the building's perceived size is also important. There are a variety of design techniques that can be used to give a building a human scale, meaning that the size of the building will be perceived as being of a proportion to which individuals can relate.

When the buildings in a neighborhood are all about the same size and proportion, they are said to be in scale with the neighborhood (i.e., architectural scale). Larger buildings can more effectively fit with smaller ones if their form is composed of smaller elements which relate to the surrounding buildings.

The following principles illustrate design techniques that help new development blend into existing neighborhoods. For a more detailed description of building scale, see Design Guidelines for Pedestrian-Oriented Business Districts, adopted by reference in the Kirkland Municipal Code.

Size Relationship of House to Lot

Issue

Large houses on small lots look out of proportion.

Discussion

Kirkland has an established pattern of house size to lot size. When large residences cover more lot area than is normally seen in this established pattern, they appear incompatible with their neighbors and disrupt the streetscape. In some situations, this can be mitigated by preserving adjacent open space.

Guideline

The size of new residences should maintain a reasonable proportion of building to lot size that fits the established pattern of development in Kirkland.

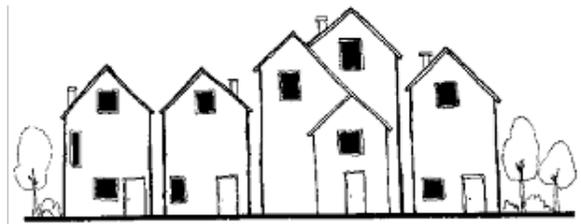
Building Modulation

Issue

Building modulation can be used to improve human and architectural scale.

Discussion

Vertical building modulation is the vertical division of a building facade through architectural features, terracing, or differing rooflines. By altering an elevation vertically, a larger building will appear to be more of an aggregation of smaller buildings.



Horizontal building modulation is the horizontal division of a building facade through the use of methods such as setbacks, balconies, eaves, and banding of contrasting materials. Elevations that are modulated appear less massive than those with sheer flat surfaces.

Guideline

Building modulation should be used to reduce the perceived mass and height of buildings.

Roof Forms

Issue

Sloped roofs and flat roofs with parapets or cornice treatments are on many of Kirkland's historic homes and are representative of the City's residential character.



Discussion

Rooflines are a critical element in the image of a structure since they create the visual edge or top of the building. The type of roof style used can affect the building’s individuality, interest, and human scale. Sloped roofs can be a desirable element since they convey a residential image and represent historic Kirkland residences to many people. Flat roofs, with detailing such as cornice or parapet treatments, can also add interest and vertical articulation.

These roof forms can help newer buildings to fit into existing Kirkland neighborhoods.



Principle

Moderate to steeply pitched roofs should be encouraged. When flat roofs are used, they should include parapets or cornice treatments.

Architectural Elements



Issue

Architectural elements such as balconies and bay windows can help an individual relate to a building by giving it a human scale.

Discussion

Elements in a building facade can create a distinct character, for example, bay windows suggest housing. These special elements can be used to give a building a human

scale and enhance its surroundings. Requirements for specific architectural features may be overly regulatory, but some features that can be reasonably incorporated into residential buildings include balconies, bay windows, roof decks, trellises, cornices, and prominent chimneys.

Upper-story architectural elements such as balconies, roof decks, and bay windows also improve the relationship between the upper-story living areas and the street or open space below. This relationship provides a people-oriented quality and adds additional security at night.

Guideline

The use of architectural building elements such as balconies, roof decks, bay windows, trellises, cornices, and prominent chimneys should be encouraged.

Window Patterns

Issue

Large windows detract from the human scale of a building.

Discussion

The size, location, and number of windows creates interest and can help provide a human scale to large buildings. We look to windows for visual clues as to the size and function of the building. If window areas are divided into units that we can associate with small-scale residences, then we will be better able to judge the building’s size relative to our own bodies. Breaking window areas into units of about 35 square feet or less with each window unit separated by a visible mullion or other element at least six inches wide would accomplish this goal. Another successful approach is multiple-paned windows with visible mullions separating several smaller panes of glass.

Guideline

Large walls of windows should be discouraged and architectural detailing at window jambs, sills, and heads should be emphasized.

BUILDING MATERIAL, COLOR, AND DETAIL

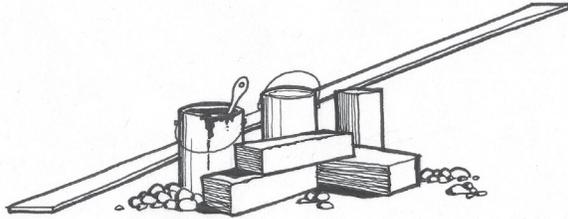
Introduction

From a distance, the most noticeable building qualities are the overall form and color of a building. Details, such as texture of materials, quality of finishes, and small decorative elements, become more apparent close-up. Kirkland features a variety of materials and colors, which provide a stimulating streetscape. The following design principles are intended to support this variety.

Building Materials and Color

Issue

Materials and color can add to or detract from a building's exterior appearance, the streetscape, and the community's identity.



Discussion

There are a variety of materials and colors used in Kirkland, which help to bolster a sense of place and community identity. The selection and use of these exterior colors and materials are key ingredients in determining how a building will look. Some materials such as stone, brick, stained or painted wood, and tile can give a sense of permanence or provide texture and scale that will help a new building fit better in its surroundings. Other materials such as mirrored glass and cinder blocks can have negative impacts.

Guidelines

Construct building exteriors from high quality and durable materials that are attractive when viewed from a distance or up close. Materials that suggest permanence, or have texture and pattern, are encouraged.

Natural colors of brick, stone, and tile, and stained or painted wood are desirable.

The materials and colors chosen for new buildings should be compatible with those of existing neighboring buildings.

Lighting



Issue

Attractive lighting can be designed to provide security without producing glare on neighboring properties.

Discussion

All building entries and parking areas require lighting for security and to provide an inviting space. However, security lights on building

facades or in outdoor areas can be overpowering to neighboring properties unless they are properly located and designed. Well-placed lights with light sources that are hidden by fixtures maintain sufficient lighting levels for security and safety purposes, but do not produce glare.

Guidelines

Lighting should be adequate to provide security for building entries, parking lots, pedestrian areas and walkways. Light sources should be hidden by fixtures and not produce glare on neighboring properties.

Screening of Dumpsters, Utilities, and Mechanical Equipment

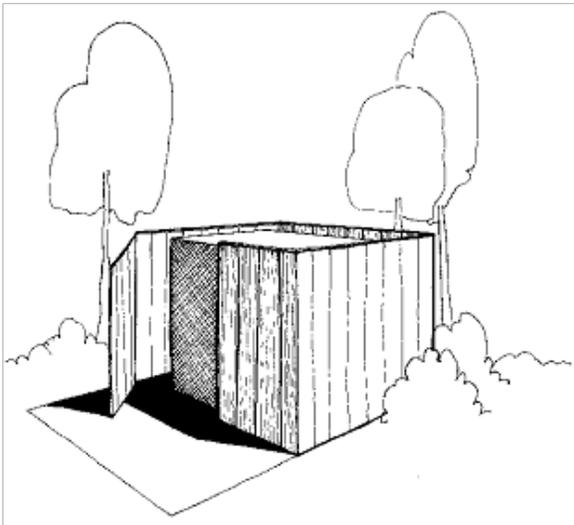
Issue

Service elements can be screened or located so that they are not visible from the street and adjacent properties.

Discussion

Unightly service elements, such as dumpsters, utility meters, and rooftop mechanical equipment can detract from the appearance of residential projects and create hazards for pedestrians, bicyclists, and automobiles.

These service elements are best located away from the street front and adjacent properties when possible. When such elements cannot be located away from the street front, they can be situated away from pedestrian paths and screened from view.



Guideline

Locate service elements for multifamily residential development so that they are not visible from the street, pedestrian paths, or adjacent properties when possible, or screen them from view.

LANDSCAPE DESIGN AND SITE ELEMENTS

Introduction

An important aspect of any building is its physical setting. The natural features of a place are key to residents' and visitors' perception. This section lays out principles that serve to merge the design of structures and places with the natural environment. It discusses the concepts behind new landscaping as well as the maintenance and protection of existing natural features.

Visual Quality of Landscapes

Issue

There is an important relationship between landscaping, site design, and architecture.

Discussion

A well-designed site has a strong relationship between natural vegetation, new landscaping, and architecture. The plant materials add to a building's richness, while the building points to the architectural qualities of the landscaping. Foliage can soften the hard edges and improve the visual quality of the built environment. It can also be used to screen elements on- or off-site that are not visual assets. Drought-tolerant plants can help to ensure a natural, long lasting and low maintenance landscape design.



Guidelines

The placement and amount of landscaping for new and existing developments should complement the architecture on the site. Large, mature plantings should be used to mitigate the scale of large structures.

When possible, significant natural vegetation should be preserved and incorporated into the site design, and drought-tolerant plants should be used when new landscaping is required.

Open Space

Issue

Residential projects can be designed to maximize open space.



Discussion

Well organized outdoor spaces are created by the grouping and orientation of buildings and building elements. These outdoor spaces can provide buffering, preservation of natural areas, and active and passive recreation space. They can also provide for important hydrologic functions, and preserve or enhance views.

Guidelines

Site residential projects to maximize opportunities for creating usable, attractive, well-integrated open space.

Site recreational areas to allow for natural observation by the residents of the development.

Retaining Walls



Issue

Retaining walls can have a negative impact on adjacent properties.

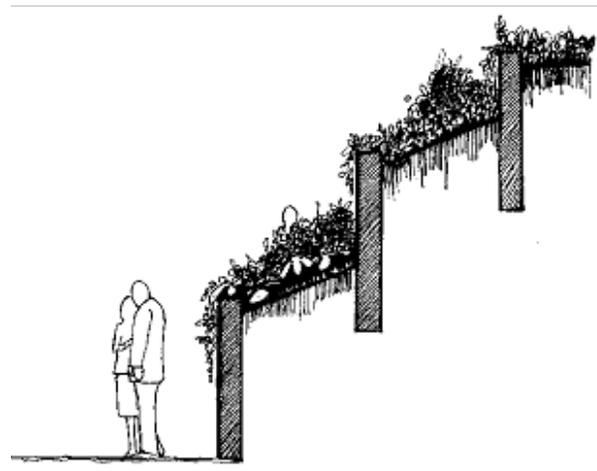
Discussion

Retaining walls are often necessary when developing a residential site.

The following are examples of techniques that can help reduce the impact of retaining walls

on adjacent properties:

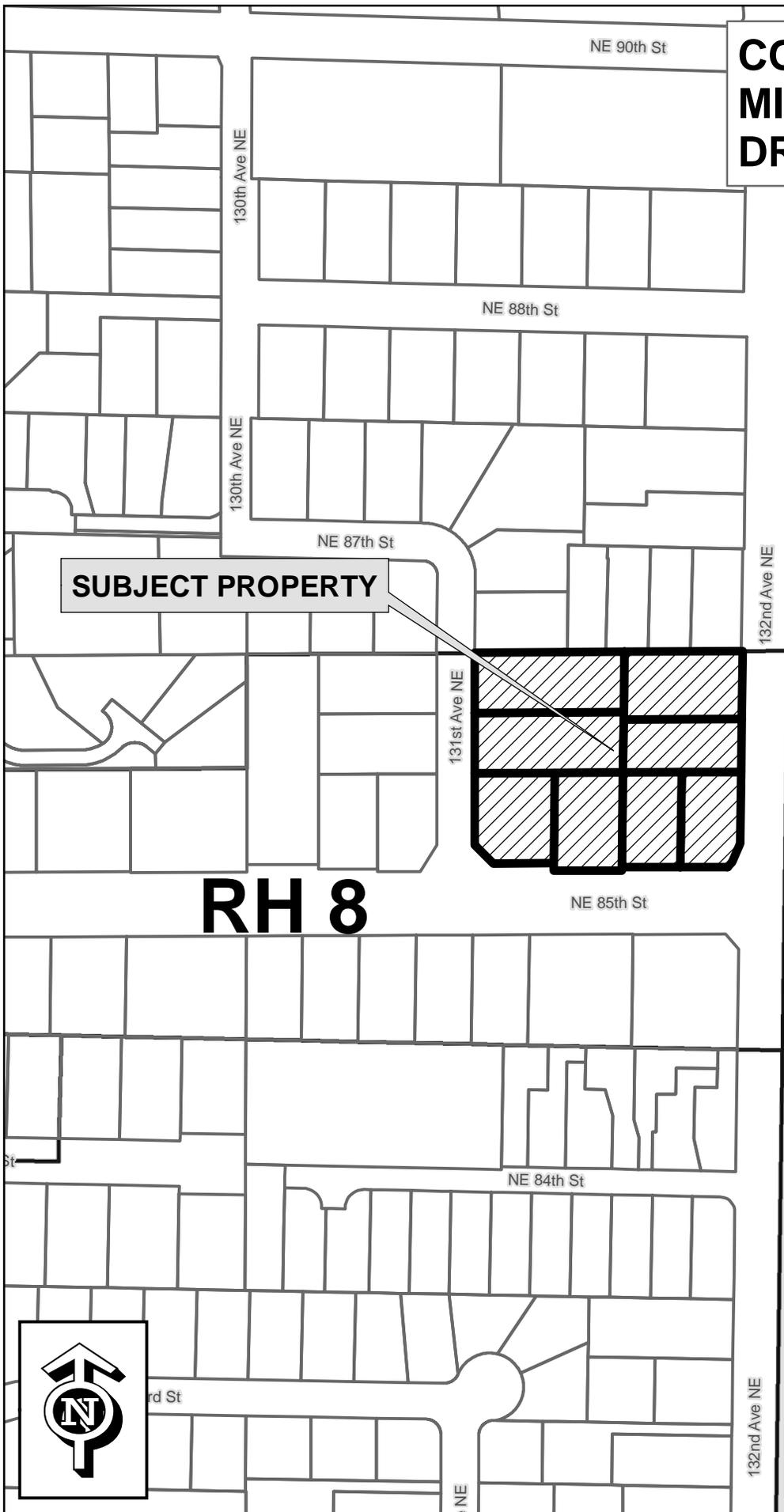
- Terracing and landscaping the retaining wall;
- Substituting a stone wall, rockery, modular masonry, or other special material in place of a concrete retaining wall;
- Locating hanging plant materials above and climbing plant material below the retaining wall;
- Installing trellises for vines;
- Putting in a landscaped planting bed that screens at least half of the wall.



Guidelines

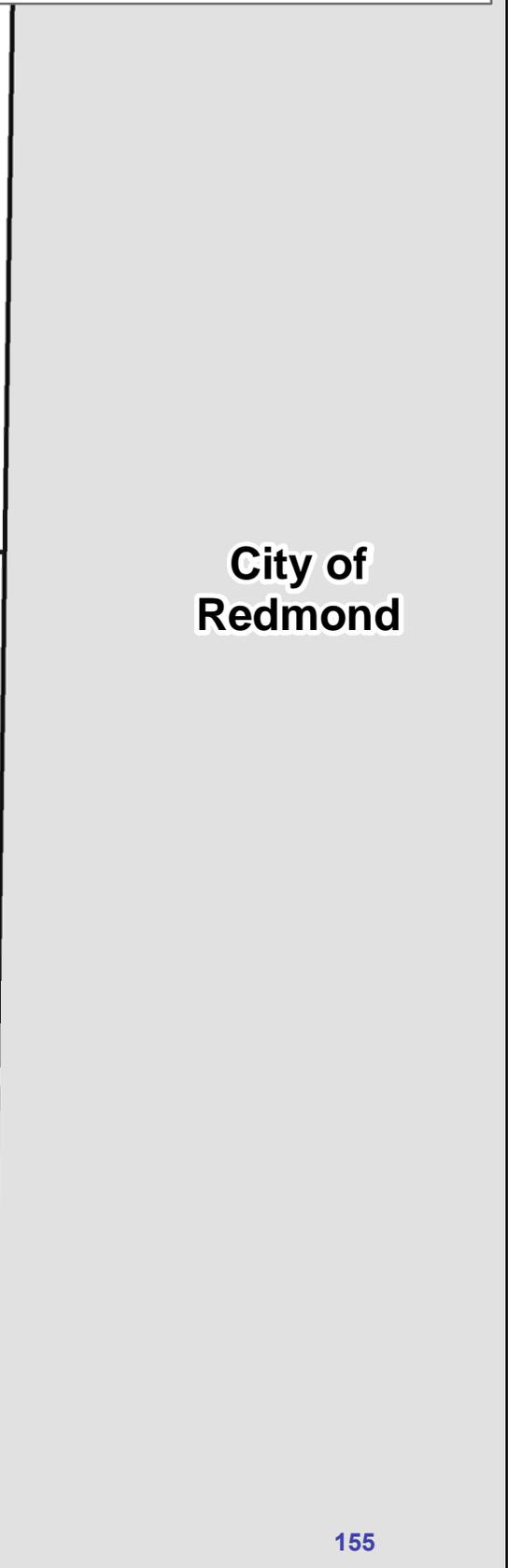
Avoid retaining walls that extend higher than eye level (about five feet) when possible. Where high retaining walls are unavoidable, terrace the wall so that no single run is higher than eye level, and design them to reduce the impact on pedestrians and neighboring properties.

**CONTINENTAL DIVIDE
MIXED USE
DRV18-00312**



SUBJECT PROPERTY

RH 8



**City of
Redmond**



53.80 User Guide – RH 8 zone.

The charts in KZC [53.84](#) contain the basic zoning regulations that apply in the RH 8 zone of the City. Use these charts by reading down the left hand column entitled Use. Once you locate the use in which you are interested, read across to find the regulations that apply to that use.

Section 53.82

Section 53.82 – GENERAL REGULATIONS

The following regulations apply to all uses in this zone unless otherwise noted:

1. Refer to Chapter [1](#) KZC to determine what other provisions of this code may apply to the subject property.
 2. Development creating four or more new dwelling units that includes lots or portions of lots adjoining 131st Avenue NE or 132nd Avenue NE that are located more than 120 feet north of NE 85th Street shall provide at least 10 percent of the units as affordable housing units as defined in Chapter [5](#) KZC. See Chapter [112](#) KZC for additional affordable housing incentives and requirements.
 3. For structures located within 30 feet of a parcel in a low density zone (or a low density use in PLA 17), KZC [115.136](#) establishes additional limitations on structure size.
 4. On lots that are not abutting NE 85th Street or are not consolidated with at least one lot abutting NE 85th Street, development shall be subject to the permitted uses and regulations in the RSX zone, except that isolated parcels may be developed independently with office use.
 5. If the lot area of the subject property is equal to or greater than 18,000 square feet, maximum building height is 35 feet above average building elevation, except maximum building height is 30 feet within 30 feet of an RSX zone, on lots located more than 120 feet north of NE 85th Street, between 132nd Avenue NE and parcels abutting 131st Avenue NE.
 6. The street level floor of all structures on the subject property shall be a minimum of 15 feet in height. This requirement does not apply to:
 - a. The following uses: vehicle service stations, automotive service centers, private lodges or clubs, stacked dwelling units, churches, schools, day-care centers, mini-schools or mini-day-care centers, assisted living facilities, convalescent centers or nursing homes, public utilities, government facilities or community facilities.
 - b. Parking garages.
 - c. Additions to existing nonconforming development where the Planning Official determines it is not feasible.
 7. Within required front yards, canopies and similar entry features may encroach; provided, that the total horizontal dimension of such elements may not exceed 25 percent of the length of the structure.
 8. Some development standards or design regulations may be modified as part of the design review process. See Chapters [92](#) and [142](#) KZC for requirements.
 9. The Public Works Official shall approve the number, location and characteristics of driveways on NE 85th Street in accordance with the driveway and sight distance policies contained in the Public Works Pre-Approved Plans manual. Taking into consideration the characteristics of this corridor, the Public Works Official may:
 - a. Require access from side streets; and/or
 - b. Encourage properties to share driveways, circulation and parking areas; and/or
 - c. Restrict access to right turn in and out; or
 - d. Prohibit access altogether along NE 85th Street.
- (GENERAL REGULATIONS CONTINUED ON NEXT PAGE)
- (GENERAL REGULATIONS CONTINUED FROM PREVIOUS PAGE)
10. Drive-through and drive-in facilities are not permitted in this zone.
 11. See Chapters [100](#) and [162](#) KZC for information about nonconforming signs. KZC [162.35](#) describes when nonconforming signs must be brought into conformance or removed.
 12. For lighting requirements associated with development see KZC [115.85\(2\)](#).
 13. Prior to any of the following uses occupying a structure on a property adjoining a residential zone, the applicant shall submit a noise study prepared by a qualified acoustical consultant for approval by the Planning Official:
 - Establishments expected to operate past 9:00 p.m.
 - Retail establishment providing entertainment, recreational or cultural activities.
 - Veterinary offices.
 - Any establishment where animals are kept on site.
 - Establishments involving a large truck loading dock for deliveries.

The study shall verify that the noise expected to emanate from the site adjoining any residential-zoned property complies with the standards specified in KZC [115.95\(1\)](#) and (2) and WAC [173-60-040\(1\)](#) for a Class B source property and a Class A receiving property.
 14. A City entryway feature shall be provided on the parcel located at the northwest corner of the intersection of NE 85th Street and 132nd Avenue, or adjacent parcel under common ownership with such parcel. Entryway features shall include such elements as: a sign, art, landscaping and lighting. See Chapter [92](#) KZC, Design Regulations.

Section 53.84



USE ZONE CHART

DIRECTIONS: FIRST, read down to find use...THEN, across for REGULATIONS

Section 53.84	USE ↓ REGULATIONS ↑	Required Review Process	MINIMUMS			MAXIMUMS		Landscape Category (See Ch. 95)	Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)	Special Regulations (See also General Regulations)	
			Lot Size	REQUIRED YARDS (See Ch. 115)			Lot Coverage					Height of Structure
				Front	Side	Rear						
.010	Office Use	D.R., Chapter 142 KZC	None	10' adjacent to NE 85th St., otherwise 20'.	0'	15'	70%	30' above average building elevation. See Gen. Regs. 3 and 5.	A	D	If a medical, dental or veterinary office, then 1 per each 200 sq. ft. of gross floor area. Otherwise, 1 per each 300 sq. ft. of gross floor area.	1. The following regulations apply to veterinary offices only: a. May only treat small animals on the subject property. b. Outside runs and other outside facilities for the animals are not permitted. 2. Ancillary assembly and manufacture of goods on the premises of this use are permitted only if: a. The ancillary assembled or manufactured goods are subordinate to and dependent on this use. b. The outward appearance and impacts of this use with ancillary assembly or manufacturing activities must be no different from other office uses.
.020	Restaurant								E	1 per each 100 sq. ft. of gross floor area.	1. May not be located above the street level floor of a structure. 2. Must be oriented toward NE 85th Street. 3. Gross floor area for each individual use may not exceed 4,000 sq. ft.	
.030	Entertainment, Cultural and/or Recreational Facility									See KZC 105.25.	1. Gross floor area for each individual use may not exceed 4,000 sq. ft.	
.040	Any Retail Establishment other than those specifically listed, limited or prohibited in this zone, selling goods or providing services, including banking and related financial services.								D	1 per each 300 sq. ft. of gross floor area.	1. The following uses are not permitted in this zone: a. Vehicle service stations. b. Automotive service centers. c. Uses with drive-in facilities or drive-through facilities. d. Retail establishments providing storage services unless accessory to another permitted use. e. A retail establishment involving the sale, service or rental of motor vehicles, sailboats, motor boats, recreation trailers, heavy equipment and similar vehicles; provided, that motorcycle sales, service or rental is permitted if conducted indoors. f. Storage and operation of heavy equipment, except delivery vehicles associated with retail uses. g. Storage of parts unless conducted entirely within an enclosed structure.	

REGULATIONS CONTINUED ON NEXT PAGE

DIRECTIONS: FIRST, read down to find use...THEN, across for REGULATIONS												
Section 53.84	USE  REGULATIONS 	Required Review Process	MINIMUMS			MAXIMUMS		Landscape Category (See Ch. 95)	Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)	Special Regulations (See also General Regulations)	
			Lot Size	REQUIRED YARDS (See Ch. 115)			Lot Coverage					Height of Structure
				Front	Side	Rear						
.040	Any Retail Establishment other than those specifically listed, limited or prohibited in this zone, selling goods or providing services, including banking and related financial services. (continued)									REGULATIONS CONTINUED FROM PREVIOUS PAGE 2. This use must be oriented toward NE 85th Street and may not be located above the street level floor of a structure except for personal service establishments that provide services involving the care of a person, or of a person's apparel, such as laundry and dry cleaning services, beauty shops, barber shops, shoe repair shops and tailors may be located above the street level floor; provided, that the use of exterior areas adjoining residential uses is prohibited. 3. Gross floor area for each individual use may not exceed 4,000 sq. ft. 4. A delicatessen, bakery, or other similar use may include, as part of the use, accessory seating if: a. The seating and associated circulation area does not exceed more than 10 percent of the gross floor area of the use; and b. It can be demonstrated to the City that the floor plan is designed to preclude the seating area from being expanded. 5. Retail establishments selling marijuana or products containing marijuana are not permitted on properties abutting the school walk routes shown on Plate 46.		

Section 53.84



USE ZONE CHART

DIRECTIONS: FIRST, read down to find use...THEN, across for REGULATIONS

Section 53.84	USE REGULATIONS 	Required Review Process	MINIMUMS			MAXIMUMS		Landscape Category (See Ch. 95)	Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)	Special Regulations (See also General Regulations)	
			Lot Size	REQUIRED YARDS (See Ch. 115)			Lot Coverage					Height of Structure
				Front	Side	Rear						
.050	Stacked Dwelling Units See Spec. Reg. 1.	D.R., Chapter 142 KZC	None	10' adjacent to NE 85th St., otherwise 20'.	0'	15'	70%	30' above average building elevation. See Gen. Regs. 3 and 5.	A	A	1.2 per studio unit. 1.3 per 1 bedroom unit. 1.6 per 2 bedroom unit. 1.8 per 3 or more bedroom unit. See KZC 105.20 for visitor parking requirements.	<ol style="list-style-type: none"> At least 60% of the linear frontage of the property along NE 85th Street shall only include commercial use. The commercial use shall be at the street level floor and oriented toward NE 85th Street. Commercial uses shall have a minimum depth of 20 feet and an average depth of at least 30 feet (as measured from the face of the building along the street). Stacked Dwelling Units are not permitted on the street level floor within 30 feet of the property line along NE 85th Street. The Planning Director or Design Review Board may approve a minor reduction in the depth requirements if the applicant demonstrates that the requirement is not feasible given the configuration of existing or proposed improvements and the design of the retail frontage will maximize visual interest. Chapter 115 KZC contains regulations regarding home occupations and other accessory uses, facilities and activities associated with this use.
.060	Assisted Living Facility, Convalescent Center or Nursing Home See Spec. Reg. 1.										Independent unit: 1.7 per unit. Assisted living facility: 1 per unit. Convalescent Center or Nursing Home: 1 per each bed.	<ol style="list-style-type: none"> At least 60% of the linear frontage of the property along NE 85th Street shall only include commercial use. The commercial use shall be at the street level floor and oriented toward NE 85th Street. Commercial uses shall have a minimum depth of 20 feet and an average depth of at least 30 feet (as measured from the face of the building along the street). Assisted Living, Convalescent Center or Nursing Home is not permitted on the street level floor within 30 feet of the property line along NE 85th Street. The Planning Director or Design Review Board may approve a minor reduction in the depth requirements if the applicant demonstrates that the requirement is not feasible given the configuration of existing or proposed improvements and the design of the retail frontage will maximize visual interest. Chapter 115 KZC contains regulations regarding home occupations and other accessory uses, facilities and activities associated with this use.
.070	Church										1 per every 4 people based on maximum occupancy load of any area of worship. See Spec. Reg. 1.	<ol style="list-style-type: none"> No parking is required for day-care or school ancillary to the use.

Section 53.84



USE ZONE CHART

DIRECTIONS: FIRST, read down to find use...THEN, across for REGULATIONS												
Section 53.84	USE ↓ REGULATIONS ↑	Required Review Process	MINIMUMS				MAXIMUMS		Landscape Category (See Ch. 95)	Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)	Special Regulations (See also General Regulations)
			Lot Size	REQUIRED YARDS (See Ch. 115)			Lot Coverage	Height of Structure				
				Front	Side	Rear						
.080	School, Day-Care Center, Mini-School or Mini-Day-Care Center	D.R., Chapter 142 KZC.	None	10' adjacent to NE 85th St., otherwise 20'.	0'	15'	70%	30' above average building elevation. See Gen. Regs. 3 and 5.	A	B	See KZC 105.25.	<ol style="list-style-type: none"> 1. A six-foot-high fence is required only along the property lines adjacent to the outside play areas. 2. An on-site passenger loading area must be provided. The City shall determine the appropriate size of the loading areas on a case-by-case basis, depending on the number of attendees and the extent of the abutting right-of-way improvements. Carpooling, staggered loading/unloading time, right-of-way improvements or other means may be required to reduce traffic impacts on nearby residential uses. 3. May include accessory living facilities for staff persons. 4. To reduce impacts on nearby residential uses, hours of operation of the use may be limited and parking and passenger loading areas relocated. 5. For school use, structure height may be increased, up to 35 feet, if: <ol style="list-style-type: none"> a. The school can accommodate 200 or more students; and b. The required side and rear yards for the portions of the structure exceeding the basic maximum structure height are increased by one foot for each additional one foot of structure height; and c. The increased height is not specifically inconsistent with the applicable neighborhood plan provisions of the Comprehensive Plan. d. The increased height will not result in a structure that is incompatible with surrounding uses or improvements.
.090	Public Utility											<ol style="list-style-type: none"> 1. Landscape Category A or B may be required depending on the type of use on the subject property and the impacts associated with the use on the nearby uses.
.100	Government Facility Community Facility								C See Spec. Reg. 1.			
.110	Public Park	Development standards will be determined on a case-by-case basis. See KZC 45.50 for required review process.										

Chapter 142 – DESIGN REVIEW**Sections:**

- [142.05](#) User Guide
- [142.15](#) Development Activities Requiring D.R. Approval
- [142.25](#) Administrative Design Review (A.D.R.) Process
- [142.35](#) Design Board Review (D.B.R.) Process
- [142.37](#) Design Departure and Minor Variations
- [142.40](#) Appeals of Design Review Board Decisions
- [142.50](#) Modifications
- [142.55](#) Lapse of Approval for Design Review Board Decisions
- [142.60](#) Bonds

142.05 User Guide

Various places in this code indicate that certain developments, activities, or uses are required to be reviewed through design review or D.R. Design review may either be administrative design review (A.D.R.) or design board review (D.B.R.). This chapter describes these design review processes.

(Ord. 4177 § 2, 2009; Ord. 4107 § 1, 2007; Ord. 4097 § 1, 2007; Ord. 4037 § 1, 2006; Ord. 4030 § 1, 2006)

142.15 Development Activities Requiring D.R. Approval

1. Design Board Review (D.B.R.)

a. The following development activities shall be reviewed by the Design Review Board pursuant to KZC [142.35](#):

- 1) New buildings greater than one (1) story in height or greater than 10,000 square feet of gross floor area, or in the Market Street Corridor Historic District (MSC 3 Zone).
- 2) Additions to existing buildings where:
 - a) The new gross floor area is greater than 10 percent of the existing building's gross floor area; and
 - b) The addition is greater than 2,000 square feet of gross floor area; and
 - c) Either:
 - 1) The existing building and addition total more than 10,000 square feet of gross floor area; or
 - 2) The addition adds another story; or
 - 3) Is in the Market Street Corridor Historic District (MSC 3 zone).
- 3) Renovations to existing facades, where the building is identified by the City as an historic structure or is in the Market Street Corridor Historic District (MSC 3 zone).

b. Exemptions from D.B.R. – The following development activities shall be reviewed through the administrative design review process in KZC [142.25](#):

- 1) Any development where administrative design review is indicated in the applicable Use Zone Chart. **163**

- 2) Any development in the following zones within the Rose Hill Business District (RHBD): RH 5 except development that includes lots or portions of lots located more than 120 feet north of NE 85th Street, between 132nd Avenue NE and properties abutting 131st Avenue NE, PR 3.6, RM, PLA 17A.
- 3) Any development in the MSC 1 and MSC 4 zones located within the Market Street Corridor.
2. Administrative Design Review (A.D.R.) – All other development activities not requiring D.B.R. review under subsection (1) of this section shall be reviewed through the A.D.R. process pursuant to KZC [142.25](#).
3. Exemptions from Design Review – The following development activities shall be exempt from either A.D.R. or D.B.R. and compliance with the design regulations of Chapter [92](#) KZC:
- Any activity which does not require a building permit; or
 - Interior work that does not alter the exterior of the structure; or
 - Normal building maintenance including the repair or maintenance of structural members; or
 - Any development listed as exempt in the applicable Use Zone Chart.

(Ord. 4683 § 2, 2019; Ord. 4498 § 3, 2015; Ord. 4392 § 1, 2012; Ord. 4390 § 1, 2012; Ord. 4177 § 2, 2009; Ord. 4107 § 1, 2007; Ord. 4097 § 1, 2007; Ord. 4037 § 1, 2006; Ord. 4030 § 1, 2006; Ord. 3833 § 1, 2002)

142.25 Administrative Design Review (A.D.R.) Process

1. Authority – The Planning Official shall conduct A.D.R. in conjunction with a related development permit pursuant to this section.

The Planning Official shall review the A.D.R. application for compliance with the design regulations contained in Chapter [92](#) KZC, or in zones where so specified, with the applicable design guidelines adopted by KMC [3.30.040](#). In addition, the following guidelines and policies shall be used to interpret how the regulations apply to the subject property:

- Design guidelines for pedestrian-oriented business districts, as adopted in KMC [3.30.040](#).
 - Design guidelines for the Rose Hill Business District (RHBD), the Totem Lake Business District (TLBD) and Yarrow Bay Business District (YBD) as adopted in KMC [3.30.040](#).
 - For review of attached or stacked dwelling units within the Rose Hill Business District (RHBD), the PLA 5C zone, the Houghton/Everest Neighborhood Center, and the Market Street Corridor, Design Guidelines for Residential Development as adopted in KMC [3.30.040](#).
2. Application – As part of any application for a development permit requiring A.D.R., the applicant shall show compliance with the design regulations in Chapter [92](#) KZC, or where applicable, the design guidelines adopted by KMC [3.30.040](#), by submitting an A.D.R. application on a form provided by the Planning and Building Department. The application shall include all documents and exhibits listed on the application form, as well as application materials required as a result of a pre-design conference.
3. Pre-Design Conference – Before applying for A.D.R. approval, the applicant may schedule a pre-design meeting with the Planning Official. The meeting will be scheduled by the Planning Official upon written request by the applicant. The purpose of this meeting is to provide an opportunity for an applicant to discuss the project concept with the Planning Official and for the Planning Official to designate which design regulations, or design guidelines, apply to the proposed development based primarily on the location and nature of the proposed development.

4. A.D.R. Approval

a. The Planning Official may grant, deny, or conditionally approve the A.D.R. application. The A.D.R. approval or conditional approval will become conditions of approval for any related development permit, and no development permit will be issued unless it is consistent with the A.D.R. approval or conditional approval.

b. Additions or Modifications to Existing Buildings

1) Applications involving additions or modifications to existing buildings shall comply with the design regulations of Chapter 92 KZC, or where applicable, the design guidelines adopted by KMC 3.30.040 to the extent feasible depending on the scope of the project. The Planning Official may waive compliance with a particular design regulation if the applicant demonstrates that it is not feasible given the existing development and scope of the project.

2) The Planning Official may waive the A.D.R. process for applications involving additions or modifications to existing buildings if the design regulations are not applicable to the proposed development activity.

5. Lapse of Approval – The lapse of approval for the A.D.R. decision shall be tied to the development permit and all conditions of the A.D.R. approval shall be included in the conditions of approval granted for that development permit.

6. Design departure and minor variations may be requested pursuant to KZC 142.37.

(Ord. 4683 § 2, 2019; Ord. 4637 § 3, 2018; Ord. 4496 § 2, 2015; Ord. 4495 § 2, 2015; Ord. 4491 § 3, 2015; Ord. 4392 § 1, 2012; Ord. 4357 § 1, 2012; Ord. 4333 § 1, 2011; Ord. 4177 § 2, 2009; Ord. 4107 § 1, 2007; Ord. 4097 § 1, 2007; Ord. 4037 § 1, 2006; Ord. 4030 § 1, 2006; Ord. 3956 § 1, 2004)

142.35 Design Board Review (D.B.R.) Process

1. Timing of D.B.R. – For any development activity that requires D.B.R. approval, the applicant must comply with the provisions of this chapter before a building permit can be approved; provided, that an applicant may submit a building permit application at any time during the design review process. An applicant may request early design review, but such review shall not be considered a development permit or to in any way authorize a use or development activity. An application for D.R. approval may be considered withdrawn for all purposes if the applicant has not submitted information requested by the City within 60 calendar days after the request and the applicant does not demonstrate reasonable progress toward submitting the requested information.

2. Public Meetings – All meetings of the Design Review Board shall be public meetings and open to the public.

3. Authority – The Design Review Board shall review projects for consistency with the following:

a. Design guidelines for pedestrian-oriented business districts, as adopted in Chapter 3.30 KMC.

b. Design Guidelines for the Rose Hill Business District (RHBD) and the Totem Lake Business District (TLBD) as adopted in Chapter 3.30 KMC.

c. The Design Guidelines for Residential Development, as adopted in KMC 3.30.040, for review of attached and stacked dwelling units located within the Rose Hill Business District (RHBD), the PLA 5C zone, the Houghton/Everest Neighborhood Center, and the Market Street Corridor.

d. The Parkplace Master Plan and Design Guidelines for CBD 5A as adopted in Chapter 3.30 KMC.

4. The Design Review Board is authorized to approve minor variations in development standards within certain Design Districts described in KZC [142.37](#), provided the variation complies with the criteria of KZC [142.37](#).
5. Pre-Design Conference – Before applying for D.B.R. approval, the applicant shall attend a pre-design conference with the Planning Official. The conference will be scheduled by the Planning Official upon written request by the applicant. The purpose of this conference is for the Planning Official to discuss how the design regulations, design guidelines, and other applicable provisions of this code and the Comprehensive Plan relate to the proposed development and to assist the applicant in preparing for the conceptual design conference. A pre-design conference may be combined with a pre-submittal meeting.
6. Conceptual Design Conference – Before applying for design review approval, the applicant shall attend a conceptual design conference (CDC) with the Design Review Board. The conference will be scheduled by the Planning Official to occur within 30 days of written request by the applicant. The applicant shall submit a complete application for Design Review within six (6) months following the CDC, or the results of the CDC will be null and void and a new CDC will be required prior to application for design review approval. The purpose of this conference is to provide an opportunity for the applicant to discuss the project concept with the Design Review Board and:
- a. To discuss how the design regulations, design guidelines and other applicable provisions of the Comprehensive Plan affect or pertain to the proposed development;
 - b. For the Design Review Board to designate which design regulations, design guidelines and other applicable provisions of the Comprehensive Plan apply to the proposed development based primarily on the location and nature of the proposed development; and
 - c. For the Design Review Board to determine what models, drawings, perspectives, 3-D CAD models, or other application materials the applicant will need to submit with the design review application.
7. Application – Following the conceptual design conference, the applicant shall submit the design review application on a form provided by the Planning and Building Department. The application shall include all documents and exhibits listed on the application, as well as all application materials required as a result of the conceptual design conference.
8. Public Notice
- a. Contents – On receipt of a complete design review application, the Planning Official shall schedule a design response conference with the Design Review Board to occur within 60 calendar days of receiving the complete application. The Planning Official shall provide public notice of the design response conference. Public notice shall contain the name of the applicant and project, the location of the subject property, a description of the proposed project, time and place of the first design response conference, and a statement of the availability of the application file.
 - b. Distribution – The Planning Official shall distribute this notice at least 14 calendar days before the first design response conference as follows:
 - 1) By distributing the notice or a summary thereof to owners of all property within 300 feet of any boundary of the subject property.
 - 2) By publishing once in the official newspaper of the City.
 - 3) By requiring the applicant to erect a public notice sign(s) conspicuously on or near the subject property as follows:

- a) The sign(s) shall be designed and constructed to City standards. A copy of the notice described in subsection (8)(a) of this section and a site plan and/or vicinity map shall be attached to each sign.
- b) The Planning and Building Department is authorized to develop the standards for the public notice signs necessary for implementation of this section.
- c) One (1) sign shall be erected on or near the subject property facing each public right-of-way adjacent to the subject property and private easement or tract road providing primary vehicular access to the subject property and to any property that abuts the subject property. The Planning and Building Department shall approve the location of each sign.
- d) The signs may not be removed until 21 calendar days after the final decision of the City on the application, and the applicant shall remove the signs within seven (7) calendar days thereafter.
- 4) By posting the notice on the City's website.
9. Design Response Conference – The design response stage allows the Design Review Board to review the design plans and provide direction to the applicant on issues to be resolved for final approval. The applicant shall present a summary of the project to the Design Review Board. The Planning Official shall present a review of the project for consistency with the requirements specified in subsection (3) of this section. Public comment relevant to the application may be taken. Persons commenting must provide their full name and mailing address. The Design Review Board may reasonably limit the extent of comments to facilitate the orderly and timely conduct of the conference.

The Design Review Board shall decide whether the application complies with the requirements specified in subsection (3) of this section. The Design Review Board shall make its decision by motion that adopts approved project drawings in addition to changes or conditions required by the Design Review Board. If the Design Review Board finds that the application does not meet those requirements, it shall specify what requirements have not been met and options for meeting those requirements. The Design Review Board may continue the conference if necessary to gather additional information necessary for its decision on the design review application. If the conference is continued to a specific date, no further public notice is required; otherwise notice shall be mailed to all parties participating in the design response conference.

Conceptual Master Plan Conference for TL 2 – The Design Review Board shall consider a Conceptual Master Plan (CMP) for properties over one and one-half (1-1/2) acres in size in TL 2. The CMP shall incorporate the design principles set forth in the special regulations for the use in the TL 2 zoning chart.

Conceptual Master Plan Conference for TL 5 – The Design Review Board shall consider a Conceptual Master Plan (CMP) for properties over four (4) acres in size in TL 5. The CMP shall incorporate the design principles set forth in the special regulations for the use in the TL 5 zoning chart.

Conceptual Master Plan Conference for RHBD – The Design Review Board shall consider a Conceptual Master Plan (CMP) in the RH 3 zone within the Rose Hill Business District (RHBD). The CMP shall incorporate the design considerations for the RH 3 zone set forth in the Design Guidelines for the Rose Hill Business District.

10. Approval – After reviewing the D.B.R. application and other application materials, the Design Review Board may grant, deny or conditionally approve subject to modifications the D.B.R. application for the proposed development. No development permit for the subject property requiring D.B.R. approval will be issued until the proposed development is granted D.B.R. approval or conditional approval. The terms of D.B.R. approval or conditional approval will become a condition of approval on each subsequent development permit and no

subsequent development permit will be issued unless it is consistent with the D.B.R. approval or conditional approval. The Planning Official shall send written notice of the D.B.R. decision to the applicant and all other parties who participated in the conference(s) within 14 calendar days of the approval. If the D.B.R. is denied, the decision shall specify the reasons for denial. The final D.B.R. decision of the City on the D.B.R. application shall be the date of distribution of the written D.B.R. decision or, if the D.B.R. decision is appealed, the date of the City's final decision on the appeal. Notwithstanding any other provision of this code, if an applicant submits a complete application for a building permit for the approved D.B.R. development within 180 days of the final D.B.R. decision, the date of vesting for the building permit application shall be the date of the final D.B.R. decision.

Additional Approval Provision for TL 2 and TL 5 – The Notice of Approval for a Conceptual Master Plan (CMP) shall set thresholds for subsequent D.B.R. or A.D.R. review of projects following approval of a CMP in TL 2 or TL 5. The Notice of Approval shall also include a phasing plan for all improvements shown or described in the CMP.

Additional Approval Provision for RHBD – The Design Review Board shall determine the thresholds for subsequent D.B.R. or A.D.R. review of projects following approval of a Conceptual Master Plan (CMP) in the RHBD. The Notice of Approval for the CMP will state the thresholds for future review of projects and also include a phasing plan for all improvements shown or described in the CMP.

(Ord. 4683 § 2, 2019; Ord. 4637 § 3, 2018; Ord. 4496 § 3, 2015; Ord. 4495 § 2, 2015; Ord. 4491 § 3, 2015; Ord. 4392 § 1, 2012; Ord. 4193 § 1, 2009; Ord. 4177 § 2, 2009; Ord. 4171 § 1, 2009; Ord. 4121 § 1, 2008; Ord. 4107 § 1, 2007; Ord. 4097 § 1, 2007; Ord. 4037 § 1, 2006; Ord. 4030 § 1, 2006; Ord. 3956 § 1, 2004; Ord. 3954 § 1, 2004; Ord. 3889 § 2, 2003; Ord. 3833 § 1, 2002; Ord. 3814 § 1, 2001)

142.37 Design Departure and Minor Variations

1. General – This section provides a mechanism for obtaining approval to depart from strict adherence to the design regulations or for requesting minor variations from requirements in the following zones:

- a. In the CBD and YBD: minimum required yards; and
- b. In the Business District Core: minimum required yards, floor plate maximums and building separation requirements; and
- c. In the RHBD, FHNC, the PLA 5C zone, the HENC, and the TLBD: minimum required yards, and landscape buffer; and
- d. In the MSC 1 and MSC 4 zones of the Market Street Corridor: minimum required front yards; and
- e. In the MSC 2 zone of the Market Street Corridor: height (up to an additional five (5) feet), and minimum required front yards.

This section does not apply when a design regulation permits the applicant to propose an alternate method for complying with it or the Use Zone Chart allows the applicant to request a reduced setback administratively.

2. Process – If a design departure or minor variation is requested, the D.R. decision, including the design departure or minor variation, will be reviewed and decided upon using the D.B.R. process.

3. Application Information – The applicant shall submit a complete application on the form provided by the Planning and Building Department, along with all information listed on that form, including a written response to the criteria in subsection (4) of this section.

4. Criteria – The Design Review Board may grant a design departure or minor variation only if it finds that all of the following requirements are met:

- a. The request results in superior design and fulfills the policy basis for the applicable design regulations and design guidelines;
- b. The departure will not have any substantial detrimental effect on nearby properties and the City or the neighborhood.

(Ord. 4683 § 2, 2019; Ord. 4637 § 3, 2018; Ord. 4636 § 3, 2018; Ord. 4495 § 2, 2015; Ord. 4491 § 3, 2015; Ord. 4437 § 1, 2014; Ord. 4392 § 1, 2012; Ord. 4390 § 1, 2012; Ord. 4333 § 1, 2011; Ord. 4177 § 2, 2009)

142.40 Appeals of Design Review Board Decisions

1. Jurisdiction – Appeals of the decision of the Design Review Board will be heard as follows:

- a. If a related development permit requires an open record public hearing, then the appeal shall be heard at that hearing and decided upon by the hearing body or officer or officer hearing the related development permit.
- b. If there are no other open record hearings required for related development permits, then the decision of the Design Review Board shall be heard according to the Process I appeal procedures and provisions in KZC [145.60](#) and judicial review procedures and provisions in KZC [145.110](#).

Only those issues under the authority of the Design Review Board as established by KZC [142.35](#)(3) and (4) are subject to appeal.

2. Who May Appeal – The decision of the Design Review Board may be appealed by the applicant or any other individual or entity who submitted written or oral comments to the Design Review Board.

3. Time To Appeal/How To Appeal – The appeal, in the form of a letter of appeal, must be delivered to the Planning and Building Department within 14 calendar days following the date of the distribution of the Design Review Board decision. It must contain a clear reference to the matter being appealed and a statement of the specific elements of the Design Review Board decision disputed by the person filing the appeal.

4. Fees – The person filing the appeal shall include with the letter of appeal the fee as established by ordinance.

5. Notice

a. Content – The Planning Official shall prepare a notice of the appeal containing the following:

- 1) The file number and a brief written description of the matter being appealed.
- 2) A statement of the scope of the appeal including a summary of the specific matters disputed in the letter of appeal.
- 3) The time and place of the public hearing on the appeal.
- 4) A statement of who may participate in the appeal.
- 5) A statement of how to participate in the appeal.

b. Distribution – At least 14 calendar days before the hearing on the appeal, the Planning Official shall distribute this notice, or a summary thereof, to the applicant, appellant(s), and Design Review Board. The notice of appeal may be combined with the hearing notice for the related development permit, if applicable.

6. Participation in the Appeal – Only the person(s) who filed the appeal, the applicant, and the chair (or designee) of the Design Review Board may participate in the appeal. These persons may participate in the appeal in either or both of the following ways:

- a. By submitting written comments or testimony to the hearing body or officer prior to commencement of the hearing.
- b. By appearing in person, or through a representative, at the hearing and submitting oral testimony directly to the hearing body or officer. The hearing body or officer may reasonably limit the extent of oral testimony to facilitate the orderly and timely conduct of the hearing.

7. Scope of the Appeal – The scope of the appeal is limited to the specific elements of the Design Review Board decision disputed in the letter of appeal and the hearing body or officer may only consider comments, testimony, and arguments on these specific elements.

8. Staff Report on the Appeal

a. Content – The Planning Official shall prepare a staff report containing the following:

- 1) The written decision of the Design Review Board.
- 2) All written comments received by the Design Review Board.
- 3) The letter of appeal.
- 4) All written comments on the appeal received by the Planning and Building Department from the appellant or applicant and within the scope of the appeal.
- 5) An analysis of the specific element(s) of the Design Review Board's decision disputed in the letter of appeal.

The Planning Official may present the staff report orally to the hearing body or officer.

b. Distribution – At least seven (7) calendar days before the hearing, the Planning Official shall distribute the staff report to the hearing body or officer, the appellant, and the applicant.

9. Electronic Sound Recordings – The hearing body or officer shall make a complete electronic sound recording of each hearing.

10. Continuation of the Hearing – The hearing body or officer may continue the hearing if, for any reason, it is unable to hear all of the testimony on the appeal or if it determines that it needs more information within the scope of the appeal. If, during the hearing, the hearing body or officer announces the time and place of the continued hearing on the matter, no further notice of that hearing need be given.

11. Decision on the Appeal

a. Criteria – Unless substantial relevant information is presented which was not considered by the Design Review Board, the decision of the Design Review Board shall be accorded substantial weight. The decision may be reversed or modified if, after considering all of the evidence in light of the authority of the Design Review Board pursuant to KZC [142.35\(3\)](#), the hearing body or officer determines that a mistake has been made. Specific allowances established by the applicable use zone charts may not be appealed unless the Design Review Board has approved exceptions to those allowances.

b. General – The hearing body or officer shall consider all information and material within the scope of the appeal submitted by the appellant. The hearing body or officer shall adopt findings and conclusions and

either:

- 1) Affirm the decision being appealed; or
- 2) Reverse the decision being appealed; or
- 3) Modify the decision being appealed.

c. Issuance of Written Decision – Within eight (8) calendar days after the public hearing, the hearing body or officer shall issue a written decision on the appeal. Within four (4) business days after it is issued, the hearing body or officer shall distribute the decision by mail to the appellant and the applicant.

d. Effect – If the appeal hearing is combined with an open record hearing for a related development permit, the decision on the appeal shall become part of the decision on the related development permit. The final decision of the City on the appeal of the Design Review Board decision shall occur at the same stage as the final decision of the City on the related development permit. Any appeal or challenge of the action of the hearing body or officer on the appeal of the Design Review Board decision shall be limited to the scope of the initial appeal.

(Ord. 4491 § 3, 2015; Ord. 4413 § 1, 2013; Ord. 4320 § 1, 2011; Ord. 4250 § 1, 2010; Ord. 4193 § 1, 2009; Ord. 4177 § 2, 2009; Ord. 4107 § 1, 2007; Ord. 4097 § 1, 2007; Ord. 4037 § 1, 2006; Ord. 4030 § 1, 2006; Ord. 3954 § 1, 2004)

142.50 Modifications

1. The Planning Official may approve a modification to the D.R. approval for the proposed development if:

- a. The need for the modification was not known and could not reasonably have been known before the D.R. approval was granted;
- b. The modification is minor and will not, in any substantial way, change the proposed development or violate any requirement imposed by the Design Review Board. The Planning Official may consult with the Design Review Board in his/her decision; and
- c. The development that will result from the modification will be consistent with the design regulations, design guidelines, and Comprehensive Plan.

2. Any modification, other than as specified in subsection (1) of this section, must be reviewed and decided upon as a new D.R. approval under this chapter.

(Ord. 4107 § 1, 2007; Ord. 4097 § 1, 2007; Ord. 4037 § 1, 2006; Ord. 4030 § 1, 2006)

142.55 Lapse of Approval for Design Review Board Decisions

For final approvals under this chapter issued on or before December 31, 2014, the applicant must begin construction or submit to the City a complete building permit application for the development activity, use of land or other actions approved under this chapter within seven (7) years after the final approval of the City of Kirkland on the matter, or the decision becomes void. For final approvals under this chapter issued on or after January 1, 2015, the applicant must begin construction or submit to the City a complete building permit application for the development activity, use of land or other actions approved under this chapter within five (5) years after the final approval of the City of Kirkland on the matter, or the decision becomes void.

For final approvals under this chapter issued on or before December 31, 2014, the applicant must substantially complete construction for the development activity, use of land or other actions approved under this chapter and

complete the applicable conditions listed on the notice of decision within nine (9) years after the final approval on the matter or the decision becomes void. For final approvals under this chapter issued on or after January 1, 2015, the applicant must substantially complete construction for the development activity, use of land, or other actions approved under this chapter and complete the applicable conditions listed on the notice of decision within seven (7) years after the final approval on the matter, or the decision becomes void.

For development activity, use of land, or other actions with phased construction, lapse of approval may be extended when approved under this chapter and made a condition of the notice of decision.

(Ord. 4372 § 1, 2012; Ord. 4107 § 1, 2007; Ord. 4097 § 1, 2007; Ord. 4037 § 1, 2006; Ord. 4030 § 1, 2006)

142.60 Bonds

The Planning Official may require a bond under Chapter [175](#) KZC to ensure compliance with any aspect of a D.R. approval.

(Ord. 4107 § 1, 2007; Ord. 4097 § 1, 2007; Ord. 4037 § 1, 2006; Ord. 4030 § 1, 2006)

The Kirkland Zoning Code is current through Ordinance 4713, passed December 10, 2019.

Disclaimer: The City Clerk's Office has the official version of the Kirkland Zoning Code. Users should contact the City Clerk's Office for ordinances passed subsequent to the ordinance cited above.

Chapter 142 – DESIGN REVIEW

Sections:

- 142.05 User Guide
- 142.15 Development Activities Requiring D.R. Approval
- ~~142.17 Design-Review-Process~~
- ~~142.20 Timing~~
- 142.25 Administrative Design Review (A.D.R.)
- 142.35 Design Board Review (D.B.R.)
- 142.40 Appeals of Design Review Board Decisions
- 142.50 Modifications
- 142.55 Lapse of Approval
- 142.60 Bonds

142.05

User Guide

Various places in this code indicate that certain developments, activities, or uses are required to be reviewed through design review or D.R. Design review may either be administrative design review (A.D.R.) or design board review (D.B.R.). This chapter describes these design review processes.

142.15

Development Activities Requiring D.R. Approval

- ~~1. All development activities subject to KZC 92.35(2) and (5) need not be reviewed through D.R., but shall comply with this chapter.~~
- ~~2. The following development activities are subject to D.R. unless subsection (3) of this section applies:

 - ~~a. The development of an undeveloped site.~~
 - ~~b. The addition of new floor area to an existing building.~~
 - ~~c. A change to the exterior appearance of over 25 percent of a facade visible from a street or park.~~~~
- ~~3. The following activities are not subject to D.R.:

 - ~~a. Any activity which does not require a building permit; or~~
 - ~~b. Any activity on the exterior of a building of which the total cost or fair market value, whichever is higher, does not exceed \$10,000; or~~
 - ~~c. Interior work which does not alter the exterior of the structure; or~~
 - ~~d. Normal building maintenance including the repair or maintenance of structural members; or~~
 - ~~e. Any development listed as exempt in the applicable Use Zone Chart.~~~~
- ~~4. See also KZC 162.35, Certain Nonconformances Specifically Regulated, for additional information regarding the application of design regulations to existing development and~~

~~remodels. The City encourages voluntary compliance with the design regulations, the Comprehensive Plan, and design guidelines even for projects which do not require D.R. approval according to the terms of this subsection.~~

Development Activities Requiring D.R. Approval

1. Design Board Review (D.B.R.)

a. The following development activities shall be reviewed by the Design Review Board pursuant to KZC 142.35:

1). New buildings greater than one story in height or greater than 10,000 square feet of gross floor area.

2). Additions to existing buildings where:

a) The new gross floor area is greater than 10% of the existing building's gross floor area; and

b) The addition is greater than 2,000 square feet of gross floor area; and

c) Either:

1) The existing building and addition total more than 10,000 square feet of gross floor area; or

2) The addition adds another story.

3). Renovations to existing facades, where the building is identified by the City as an historic structure.

b. Exemptions from D.B.R.: The following development activities shall be reviewed through the Administrative Design Process in KZC 142.25:

1) Any development where administrative design review is indicated in the applicable Use Zone Chart.

2) Any development in the following zones within the NE 85th Street Subarea: RH8, PR 3.6, RM, PLA 17A.

2. Administrative Design Review (A.D.R.) All other development activities not requiring D.B.R. review under Section 1 above shall be reviewed through the A.D.R. process pursuant to KZC 142.25.

3. Exemptions from Design Review The following development activities shall be exempt from either A.D.R. or D.B.R. and compliance with the design regulations of Chapter 92, KZC:

a. Any activity which does not require a building permit; or

b. Interior work that does not alter the exterior of the structure; or

c. Normal building maintenance including the repair or maintenance of structural members; or

d. Any development listed as exempt in the applicable Use Zone Chart.

Sections 142.17 and 142.20 deleted.

~~See also KZC-162.35, Certain Nonconformance's Specifically Regulated, for additional information regarding the application of design regulations to existing development and remodels. Design Review Process~~

~~1. Unless otherwise specified in the Use Zone Chart, the following development activities shall be reviewed administratively pursuant to KZC-142.25:~~

~~a. All new one-story buildings containing less than 10,000 square feet of gross floor area.~~

~~b. The following additions to existing buildings:~~

~~1) To one-story buildings where the existing and new floor area total less than 10,000 square feet of gross floor area; or~~

~~2) To buildings greater than one story or 10,000 square feet of gross floor area where the gross floor area is expanded by less than 10 percent.~~

~~c. Renovations to existing facades, unless the building is identified by the City as a historic structure.~~

~~d. Development activities in RH-8, PR, RM, and PLA-17A Zones located within the NE 85th Street Subarea.~~

~~2. Unless otherwise specified in the Use Zone Chart, all other development activities shall be reviewed by the Design Review Board pursuant to KZC-142.35.~~

~~142.20~~

Timing

~~For any development activity that requires D.R. approval, the applicant must comply with the provisions of this chapter before a building permit can be approved; provided, that an applicant may submit a building permit application at any time during the design review process. An applicant may request early design review, but such review shall not be considered a development permit or to in any way authorize a use or development activity.~~

~~An application for D.R. approval may be considered withdrawn for all purposes if the applicant has not submitted information requested by the City within 60 calendar days after the request and the applicant does not demonstrate reasonable progress toward submitting the requested information.~~

142.25

Administrative Design Review (A.D.R.) Process

1. Authority - The Planning Official shall conduct review the A.D.R. in conjunction with a related development permit pursuant to KZC 142.25.

The Planning Official shall review the A.D.R. application for compliance with the design regulations contained in Chapter 92 KZC. In addition, the following guidelines and policies shall be used to interpret how the regulations apply to the subject property:

- a. Design guidelines for pedestrian-oriented business districts, as adopted in KMC 3.30.040.
- b. Design guidelines for the Rose Hill Business District (RHBD) and the Totem Lake Neighborhood (TLN) as adopted in KMC 3.30.040.
- c. The neighborhood plans contained in the Comprehensive Plan for areas where Design Review is required, such as the Downtown Plan, Juanita Business District Plan, the Totem Lake Neighborhood Plan, the North Rose Hill Neighborhood Plan for the North Rose Hill Business District, and the NE 85th Street Subarea Plan for the Rose Hill Business District (RHBD).
- d. For review of attached or stacked dwelling units within the NE 85th Street Subarea, Appendix C, Design Principles for Residential Development contained in the Comprehensive Plan.

2. Application - As part of any application for a development permit requiring A.D.R., the applicant shall show compliance with the design regulations in Chapter 92, KZC by submitting an A.D.R. application on a form provided by the Planning Department. The application shall include all documents and exhibits listed on the application form, as well as application materials required as a result of a pre-design conference.

3. Pre-Design Conference - Before applying for A.D.R. approval, the applicant may ~~shall~~ schedule and attend an A.D.R. ~~a~~ pre-design meeting with the Planning Official. The meeting will be scheduled by the Planning Official upon written request by the applicant. The purpose of this meeting is to provide an opportunity for an applicant to discuss the project concept with the Planning Official and:

- a. ~~For the Planning Official to designate which design regulations apply to the proposed development based primarily on the location and nature of the proposed development.~~
- b. ~~To discuss how the design guidelines and other applicable provisions of this code and Comprehensive Plan affect or pertain to the proposed development;~~
- c. ~~For the Planning Official to determine what models, drawings, perspectives, 3D-CAD model, or other application materials the applicant will need to submit with the A.D.R. application.~~

4. A.D.R. Approval - After reviewing the A.D.R. application for compliance with the design regulations contained in Chapter 92, KZC, and prior to issuance of any development permit requiring A.D.R.,

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~~a. The Planning Official may grant, deny, or conditionally approve, subject to modifications, the A.D.R. application approval for the proposed development. No development permit for the subject property requiring A.D.R. approval will be issued until the proposed development is granted A.D.R. approval or conditional approval. The terms of A.D.R. approval or conditional approval will become a conditions of approval for on each subsequent any related development permit and no subsequent development permit will be issued unless it is consistent with the A.D.R. approval or conditional approval.~~

b. Additions Or Modifications To Existing Buildings -

1) Applications involving additions or modifications to existing buildings shall comply with the design regulations of Chapter 92 to the extent feasible depending on the scope of the project. The Planning Official may waive compliance with a particular design regulation if the applicant demonstrates that it is not feasible given the existing development and scope of the project.

2) The Planning Official may waive the A.D.R. process for applications involving additions or modifications to existing buildings if the design regulations are not applicable to the proposed development activity.

~~Application — Following the pre-design meeting, the applicant shall submit an A.D.R. application on a form provided by the Planning Department. The application shall include all documents and exhibits listed on the application form, as well as all application materials required as a result of the pre-design meeting.~~

~~3. Decision — The Planning Official shall review the A.D.R. application for compliance with the design regulations contained in Chapter 92-KZC. In addition, the following guidelines and policies shall be used to interpret how the regulations apply to the subject property:~~

~~a. Design guidelines for pedestrian-oriented business districts, as adopted in KMC 3.30.040.~~

~~b. Design guidelines for the Rose Hill Business District (RHBD) and the Totem Lake Neighborhood (TLN) as adopted in KMC 3.30.040.~~

~~c. The neighborhood plans contained in the Comprehensive Plan for areas where Design Review is required, such as the Downtown Plan, Juanita Business District Plan, the Totem Lake Neighborhood Plan, the North Rose Hill Neighborhood Plan for the North Rose Hill Business District, and the NE 85th Street Subarea Plan for the Rose Hill Business District (RHBD).~~

~~d. For review of attached or stacked dwelling units within the NE 85th Street Subarea; Appendix C, Design Principles for Residential Development contained in the Comprehensive Plan.~~

~~After reviewing the A.D.R. application and other application materials, the Planning Official may grant, deny or conditionally approve subject to modifications the A.D.R. approval for the proposed development. No development permit for the subject property requiring A.D.R. approval will be issued until the proposed development is granted A.D.R. approval or conditional approval. The terms of A D.R. approval or conditional approval will become a~~

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~~condition of approval on each subsequent development permit and no subsequent development permit will be issued unless it is consistent with the A.D.R. approval or conditional approval. The Planning Official shall send written notice of the A.D.R. decision to the applicant. If the A.D.R. is denied, the decision shall specify the reasons for denial.~~

5. Lapse of Approval- The lapse of approval for the A.D.R. decision shall be tied to the development permit and all conditions of the A.D.R. approval shall be included in the conditions of approval granted for that development permit.

4. Requests for Reconsideration

- a. ~~Only the applicant may request reconsideration of the A.D.R. decision.~~
- b. ~~Only the City and the applicant may participate in the request for reconsideration.~~
- c. ~~The applicant may request the Planning Director to reconsider any aspect of the Planning Official's A.D.R. decision by delivering a written request for reconsideration to the Planning Department within seven calendar days following the postmarked date of distribution of the Planning Official's written decision. The applicant shall specify in the request what aspect of the decision he/she wishes to have reconsidered and the reason for the request.~~
- d. ~~Within seven calendar days after receiving a request for reconsideration, the Planning Director shall notify the applicant whether or not the Planning Director will reconsider the decision. The Planning Director may reconsider the decision only if he/she concludes that there is substantial merit in the request.~~
- e. ~~If the Planning Director reconsiders the decision, the Planning Director shall send written notice of the final A.D.R. decision to the applicant. The decision shall specify the reasons for modifications, if applicable.~~

65. Design Departure and Minor Variations

- a. General – This section provides a mechanism for obtaining approval to depart from strict adherence to the design regulations or for requesting minor variations from requirements in the following zones:
- 1) In the CBD: minimum required yards setback requirements; and
 - 2) In the Totem Center: minimum required yards setback, floor plate maximums and building separation requirements; and
 - 3) In the RHBD and the TLN: minimum required yards setback, landscape buffer and horizontal facade requirements.

This section does not apply when a design regulation permits the applicant to propose an alternate method for complying with it or the use zone chart allows the applicant to request a reduced setback administratively.

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- b. Process – If a design departure or minor variation is requested, the D.R. decision, including the design departure or minor variation, will be reviewed and decided upon using the D.B.R. process.
- c. Application Information – The applicant shall submit a complete application on the form provided by the Planning Department, along with all information listed on that form, including a written response to the criteria in subsection (65)(d) of this section.
- d. Criteria – The Design Review Board may grant a design departure or minor variation only if it finds that all of the following requirements are met:
 - 1) The request results in superior design and fulfills the policy basis for the applicable design regulations and design guidelines;
 - 2) The departure will not have any substantial detrimental effect on nearby properties and the City or the neighborhood.

142.35

Design Board Review (D.B.R.) Process

1. Timing of D.B.R. - For any development activity that requires D.B.R. approval, the applicant must comply with the provisions of this chapter before a building permit can be approved; provided, that an applicant may submit a building permit application at any time during the design review process. An applicant may request early design review, but such review shall not be considered a development permit or to in any way authorize a use or development activity.

An application for D.R. approval may be considered withdrawn for all purposes if the applicant has not submitted information requested by the City within 60 calendar days after the request and the applicant does not demonstrate reasonable progress toward submitting the requested information.

2. Public Meetings – All meetings of the Design Review Board shall be public meetings and open to the public.
32. Authority – The Design Review Board shall review projects for consistency with the following:
 - a. Design guidelines for pedestrian-oriented business districts, as adopted in Chapter 3.30 KMC.
 - b. Design Guidelines for the Rose Hill Business District (RHBD) and the Totem Lake Neighborhood (TLN) as adopted in Chapter 3.30 KMC.
 - c. ~~The design regulations contained in Chapter 92-KZC, except as provided for in subsection (2)(d) of this section. To the extent that the standards of the design guidelines or design regulations address the same issue but are not entirely consistent or contain different levels of specificity, the Design Review Board shall determine which standard results in superior design.~~
 - d. ~~The design regulations contained in Chapter 92-KZC do not apply to development in TL 2, where development is proposed within a Conceptual Master Plan.~~
 - ce. The applicable neighborhood plans contained in the Comprehensive Plan for areas where Design Review is required.

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- df. The Design Principles for Residential Development contained in Appendix C of the Comprehensive Plan for review of attached and stacked dwelling units located within the NE 85th Street Subarea.
43. The Design Review Board is authorized to approve minor variations in development standards within the following areas certain Design Districts described in Section 142.25.(6)(a) provided the variation complies with the criteria of KZC 142.25(65)(bd):
- a. ~~In the CBD: minimum required yards.~~
 - b. ~~In Totem Center: minimum required yards, from setback, floor plate maximums and building separation requirements.~~
 - c. ~~In the RHBD and TLN: minimum required yards, from setback, landscape buffer and horizontal facade requirements.~~
54. Pre-Design Conference – Before applying for D.B.R. approval, the applicant shall attend a pre-design conference with the Planning Official. The conference will be scheduled by the Planning Official upon written request by the applicant. The purpose of this conference is for the Planning Official to discuss how the design regulations, design guidelines, and other applicable provisions of this code and the Comprehensive Plan relate to the proposed development and to assist the applicant in preparing for the conceptual design conference. A pre-design conference may be combined with a pre-submittal meeting.
65. Conceptual Design Conference – Before applying for design review approval, the applicant shall attend a conceptual design conference with the Design Review Board. The conference will be scheduled by the Planning Official to occur within 30 days of written request by the applicant. The purpose of this conference is to provide an opportunity for the applicant to discuss the project concept with the Design Review Board and:
- a. To discuss how the design regulations, design guidelines and other applicable provisions of the Comprehensive Plan affect or pertain to the proposed development;
 - b. For the Design Review Board to designate which design regulations, design guidelines and other applicable provisions of the Comprehensive Plan apply to the proposed development based primarily on the location and nature of the proposed development; and
 - c. For the Design Review Board to determine what models, drawings, perspectives, 3-D CAD model, or other application materials the applicant will need to submit with the design review application.
76. Application – Following the conceptual design conference, the applicant shall submit the design review application on a form provided by the Planning Department. The application shall include all documents and exhibits listed on the application, as well as all application materials required as a result of the conceptual design conference.
87. Public Notice
- a. Contents – On receipt of a complete design review application, the Planning Official shall schedule a design response conference with the Design Review Board to occur within

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60 calendar days of receiving the complete application. The Planning Official shall provide public notice of the design response conference. Public notice shall contain the name of the applicant and project, the location of the subject property, a description of the proposed project, time and place of the first design response conference, and a statement of the availability of the application file.

- b. Distribution – The Planning Official shall distribute this notice at least 14 calendar days before the first design response conference as follows:
- 1) By mailing the notice or a summary thereof to owners of all property within 300 feet of any boundary of the subject property.
 - 2) Publish once in the official newspaper of the City.
 - 3) Post conspicuously on the subject property on a public notice sign. The Department of Planning and Community Development is authorized to develop standards and procedures for public notice signs.

98. Design Response Conference – The design response stage allows the Design Review Board to review the design plans and provide direction to the applicant on issues to be resolved for final approval. The applicant shall present a summary of the project to the Design Review Board. The Planning Official shall present a review of the project for consistency with the requirements specified in subsection (32) of this section. Public comment relevant to the application may be taken. Persons commenting must provide their full name and mailing address. The Design Review Board may reasonably limit the extent of comments to facilitate the orderly and timely conduct of the conference.

The Design Review Board shall decide whether the application complies with the requirements specified in subsection (32) of this section. The Design Review Board shall make its decision by motion that adopts approved project drawings in addition to changes or conditions required by the Design Review Board. If the Design Review Board finds that the application does not meet those requirements, it shall specify what requirements have not been met and options for meeting those requirements. The Design Review Board may continue the conference if necessary to gather additional information necessary for its decision on the design review application. If the conference is continued to a specific date, no further public notice is required; otherwise notice shall be mailed to all parties participating in the design response conference.

Conceptual Master Plan Conference for TL 2 – The Design Review Board shall consider a Conceptual Master Plan (CMP) for properties over one and one-half acres in size in TL 2. The CMP shall incorporate the design principles set forth in the special regulations for the use in the TL 2 zoning chart.

Conceptual Master Plan Conference for TL 5 – The Design Review Board shall consider a Conceptual Master Plan (CMP) for properties over four acres in size in TL 5. The CMP shall incorporate the design principles set forth in the special regulations for the use in the TL 5 zoning chart.

Conceptual Master Plan Conference for RHBD – The Design Review Board shall consider a Conceptual Master Plan (CMP) in the RH 3 zone within the NE 85th Street Subarea. The CMP shall incorporate the design considerations for the RH 3 zone set forth in the Design Guidelines for the Rose Hill Business District.

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109. Approval – After reviewing the D.B.R. application and other application materials, the Design Review Board may grant, deny or conditionally approve subject to modifications the D.B.R. application for the proposed development. No development permit for the subject property requiring D.B.R. approval will be issued until the proposed development is granted D.B.R. approval or conditional approval. The terms of D.B.R. approval or conditional approval will become a condition of approval on each subsequent development permit and no subsequent development permit will be issued unless it is consistent with the D.B.R. approval or conditional approval. The Planning Official shall send written notice of the D.B.R. decision to the applicant and all other parties who participated in the conference(s) within 14 calendar days of the approval. If the D.B.R. is denied, the decision shall specify the reasons for denial. The final D.B.R. decision of the City on the D.B.R. application shall be the postmarked date of the written D.B.R. decision or, if the D.B.R. decision is appealed, the date of the City's final decision on the appeal. Notwithstanding any other provision of this code, if an applicant submits a complete application for a building permit for the approved D.B.R. development within 180 days of the final D.B.R. decision, the date of vesting for the building permit application shall be the date of the final D.B.R. decision.

Additional Approval Provision for TL 2 and TL 5 – The Notice of Approval for a Conceptual Master Plan (CMP) shall set thresholds for subsequent D.B.R. or A.D.R. review of projects following approval of a CMP in TL 2 or TL 5. The Notice of Approval shall also include a phasing plan for all improvements shown or described in the CMP.

Additional Approval Provision for RHBD – The Design Review Board shall determine the thresholds for subsequent D.B.R. or A.D.R. review of projects following approval of a Conceptual Master Plan (CMP) in the RHBD. The Notice of Approval for the CMP will state the thresholds for future review of projects and also include a phasing plan for all improvements shown or described in the CMP.

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Appeals of Design Review Board Decisions

1. Jurisdiction – Appeals of the decision of the Design Review Board will be heard as follows:
 - a. If a related development permit requires an open record public hearing, then the appeal shall be heard at that hearing and decided upon by the hearing body or officer or officer hearing the related development permit.
 - b. If there are no other open record hearings required for related development permits, then the decision of the Design Review Board shall be heard at an open record hearing by the City Council.

Only those issues under the authority of the Design Review Board as established by KZC 142.35(2) are subject to appeal.
2. Who May Appeal – The decision of the Design Review Board may be appealed by the applicant or any other individual or entity who submitted written or oral comments to the Design Review Board.
3. Time To Appeal/How To Appeal – The appeal, in the form of a letter of appeal, must be delivered to the Planning Department within 14 calendar days following the postmarked date of the distribution of the Design Review Board decision. It must contain a clear reference to the matter being appealed and a statement of the specific elements of the Design Review Board decision disputed by the person filing the appeal.

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4. Fees – The person filing the appeal shall include with the letter of appeal the fee as established by ordinance.
5. Notice
 - a. Content – The Planning Official shall prepare a notice of the appeal containing the following:
 - 1) The file number and a brief written description of the matter being appealed.
 - 2) A statement of the scope of the appeal including a summary of the specific matters disputed in the letter of appeal.
 - 3) The time and place of the public hearing on the appeal.
 - 4) A statement of who may participate in the appeal.
 - 5) A statement of how to participate in the appeal.
 - b. Distribution – At least 14 calendar days before the hearing on the appeal, the Planning Official shall send a copy or a summary of this notice to the applicant, appellant(s), and Design Review Board. The notice of appeal may be combined with the hearing notice for the related development permit, if applicable.
6. Participation in the Appeal – Only the person(s) who filed the appeal, the applicant, and the chair (or designee) of the Design Review Board may participate in the appeal. These persons may participate in the appeal in either or both of the following ways:
 - a. By submitting written comments or testimony to the hearing body or officer prior to commencement of the hearing.
 - b. By appearing in person, or through a representative, at the hearing and submitting oral testimony directly to the hearing body or officer. The hearing body or officer may reasonably limit the extent of oral testimony to facilitate the orderly and timely conduct of the hearing.
7. Scope of the Appeal – The scope of the appeal is limited to the specific elements of the Design Review Board decision disputed in the letter of appeal and the hearing body or officer may only consider comments, testimony, and arguments on these specific elements.
8. Staff Report on the Appeal
 - a. Content – The Planning Official shall prepare a staff report containing the following:
 - 1) The written decision of the Design Review Board.
 - 2) All written comments received by the Design Review Board.
 - 3) The letter of appeal.

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- 4) All written comments on the appeal received by the Planning Department from the appellant or applicant and within the scope of the appeal.
- 5) An analysis of the specific element(s) of the Design Review Board's decision disputed in the letter of appeal.

The Planning Official may present the staff report orally to the hearing body or officer.

- b. Distribution – At least seven calendar days before the hearing, the Planning Official shall distribute copies of the staff report to the hearing body or officer, the appellant, and the applicant.
9. Electronic Sound Recordings – The hearing body or officer shall make a complete electronic sound recording of each hearing.
10. Continuation of the Hearing – The hearing body or officer may continue the hearing if, for any reason, it is unable to hear all of the testimony on the appeal or if it determines that it needs more information within the scope of the appeal. If, during the hearing, the hearing body or officer announces the time and place of the continued hearing on the matter, no further notice of that hearing need be given.

142.55

Lapse of Approval For Design Review Board Decisions

1. General – Unless otherwise specified in the decision granting D.B.R. approval, the applicant must begin construction or submit to the City a complete building permit application for development of the subject property consistent with the D.B.R. approval within one year after the final decision granting the D.B.R. approval or that decision becomes void. The applicant must substantially complete construction consistent with the D.R. approval and complete all conditions listed in the D.B.R. approval decision within three years after the final decision on the D.B.R. approval or the decision becomes void. "Final decision" means the final decision of the Planning Official or Design Review Board.
2. Extensions
 - a. Application – The applicant may apply for a one-time extension, of up to one year, of the time limits under subsection (1) of this section. The application for the extension must be submitted by letter prior to the expiration of the applicable time limit under subsection (1) of this section. The letter of application must be submitted to the Planning Department and, along with any other supplemental documentation, must demonstrate that the applicant is making substantial progress toward developing the subject property consistent with the D.B.R. approval and that circumstances beyond his/her control prevent compliance with the applicable time limit under subsection (1) of this section.
 - b. Fee – The applicant shall include with the letter of request the fee as established by ordinance.
 - c. Review Process – An application for a time extension will be reviewed by the Planning Official.
3. Appeals
 - a. Who Can Appeal – Any person who is aggrieved by a time extension or denial of a time extension under this section may appeal that determination.
 - b. How To Appeal – The applicant must file a letter of appeal within 14 days of the approval or denial of the time extension indicating how the determination affects his/her property and presenting any relevant arguments or information on the correctness of the determination. The applicant shall include the appeal fee as established by ordinance.
 - c. Applicable Procedures – All appeals of decisions under this section will be reviewed and decided upon using Process IIA, described in Chapter 150 KZC.

142.60

Bonds

The Planning Official may require a bond under Chapter 175 KZC to ensure compliance with any aspect of a D.R. approval.

11. Decision on the Appeal

- a. Criteria – Unless substantial relevant information is presented which was not considered by the Design Review Board, the decision of the Design Review Board shall be accorded substantial weight. The decision may be reversed or modified if, after considering all of the evidence in light of the design regulations, design guidelines, and Comprehensive Plan, the hearing body or officer determines that a mistake has been made. Specific allowances established by the applicable use zone charts may not be appealed unless the Design Review Board has approved exceptions to those allowances.
- b. General – The hearing body or officer shall consider all information and material within the scope of the appeal submitted by the appellant. The hearing body or officer shall adopt findings and conclusions and either:
 - 1) Affirm the decision being appealed; or
 - 2) Reverse the decision being appealed; or
 - 3) Modify the decision being appealed.
- c. Issuance of Written Decision – Within eight calendar days after the public hearing, the hearing body or officer shall issue a written decision on the appeal. Within four business days after it is issued, the hearing body or officer shall distribute the decision by mail to the appellant and the applicant.
- d. Effect – If the appeal hearing is combined with an open record hearing for a related development permit, the decision on the appeal shall become part of the decision on the related development permit. The final decision of the City on the appeal of the Design Review Board decision shall occur at the same stage as the final decision of the City on the related development permit. Any appeal or challenge of the action of the hearing body or officer on the appeal of the Design Review Board decision shall be limited to the scope of the initial appeal.

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Modifications

1. The Planning Official may approve a modification to the D.R. approval for the proposed development if:
 - a. The need for the modification was not known and could not reasonably have been known before the D.R. approval was granted;
 - b. The modification is minor and will not, in any substantial way, change the proposed development or violate any requirement imposed by the Design Review Board. The Planning Official may consult with the Design Review Board in his/her decision; and
 - c. The development that will result from the modification will be consistent with the design regulations, design guidelines, and Comprehensive Plan.
2. Any modification, other than as specified in subsection (1) of this section, must be reviewed and decided upon as a new D.R. approval under this chapter.

142.55

Lapse of Approval For Design Review Board Decisions

1. General – Unless otherwise specified in the decision granting D.B.R. approval, the applicant must begin construction or submit to the City a complete building permit application for development of the subject property consistent with the D.B.R. approval within one year after the final decision granting the D.B.R. approval or that decision becomes void. The applicant must substantially complete construction consistent with the D.R. approval and complete all conditions listed in the D.B.R. approval decision within three years after the final decision on the D.B.R. approval or the decision becomes void. "Final decision" means the final decision of the Planning Official or Design Review Board.

2. Extensions

- a. Application – The applicant may apply for a one-time extension, of up to one year, of the time limits under subsection (1) of this section. The application for the extension must be submitted by letter prior to the expiration of the applicable time limit under subsection (1) of this section. The letter of application must be submitted to the Planning Department and, along with any other supplemental documentation, must demonstrate that the applicant is making substantial progress toward developing the subject property consistent with the D.B.R. approval and that circumstances beyond his/her control prevent compliance with the applicable time limit under subsection (1) of this section.
- b. Fee – The applicant shall include with the letter of request the fee as established by ordinance.
- c. Review Process – An application for a time extension will be reviewed by the Planning Official.

3. Appeals

- a. Who Can Appeal – Any person who is aggrieved by a time extension or denial of a time extension under this section may appeal that determination.
- b. How To Appeal – The applicant must file a letter of appeal within 14 days of the approval or denial of the time extension indicating how the determination affects his/her property and presenting any relevant arguments or information on the correctness of the determination. The applicant shall include the appeal fee as established by ordinance.
- c. Applicable Procedures – All appeals of decisions under this section will be reviewed and decided upon using Process IIA, described in Chapter 150 KZC.

142.60

Bonds

The Planning Official may require a bond under Chapter 175 KZC to ensure compliance with any aspect of a D.R. approval.

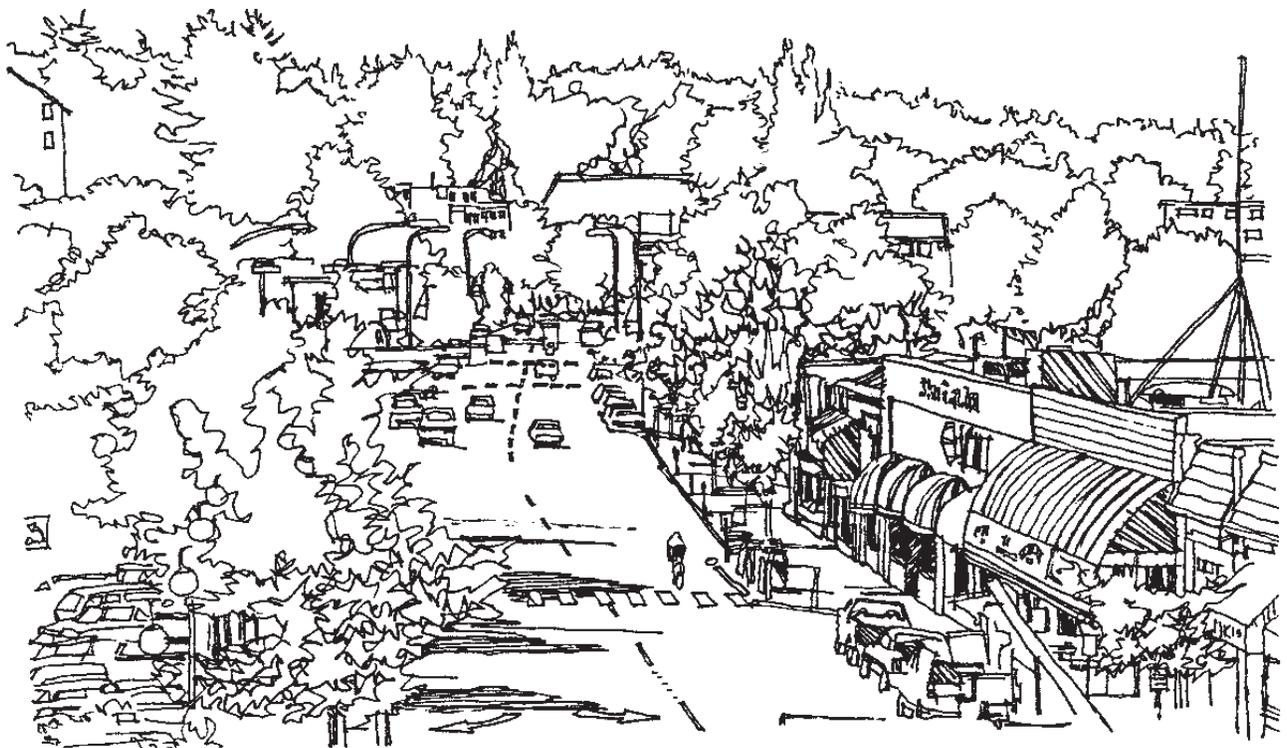
Introduction

This document sets forth a series of Design Guidelines, adopted by Section 3.30 of the Kirkland Municipal Code, that will be used by the City in the design review process. For Board Design Review (BDR), the Design Review Board will use these guidelines in association with the Design Regulations of the Kirkland Zoning Code. To the extent that the standards of the Design Guidelines or Design Regulations address the same issue but are not entirely consistent or contain different levels of specificity, the Design Review Board will determine which standard results in superior design. For Administrative Design Review (ADR), the Planning Official will use these guidelines when necessary to interpret the Design Regulations. They are also intended to assist project developers and their architects by providing graphic examples of the intent of the City's guidelines and regulations.

Most of the concepts presented in the Design Guidelines are applicable to any pedestrian-oriented business district.* “Special Considerations” have been added, such as for Downtown Kirkland, to illustrate how unique characteristics of that pedestrian-oriented business district relate to the Guideline.

The Design Guidelines do not set a particular style of architecture or design theme. Rather, they will establish a greater sense of quality, unity, and conformance with Kirkland's physical assets and civic role.

The Design Guidelines will work with improvements to streets and parks and the development of new public facilities to create a dynamic setting for civic activities and private development. It is important to note that these Guidelines are not intended to slow or restrict development, but rather to add consistency and predictability to the permit review process.



** The guidelines also apply to residential development in the Central Business District (CBD), the Juanita Business District (JBD), the North Rose Hill Business District, the Market Street Corridor (MSC), the Finn Hill Neighborhood Center (FHNC), the Houghton/Everest Neighborhood Center (HENC), the Totem Lake Business District Core, and Planned Area 5C (PLA5C); and to mixed use development throughout the City.*



Kirkland Design Guidelines

The drawing below illustrates many of the design Guidelines described in this appendix

- 1 Pedestrian plazas and places for vendors encouraged through several regulations.
- 2 Buildings on corner lots may be required to incorporate an architectural or pedestrian-oriented feature at the corner. Many options are possible including plazas, artwork, turrets, curved corners, step backs, setbacks, etc.

Special architectural requirements placed on use of concrete block and metal siding.

- 3 “Architectural scale” requirements direct large buildings to fit more comfortably with neighboring development. This example employs building setbacks, decks, curved surfaces, and recessed entries to reduce appearance of building mass.
- 4 Parking garages on pedestrian-oriented streets or through-block sidewalks may incorporate pedestrian-oriented uses or pedestrian-oriented space into front facades.

Street trees required along certain streets.

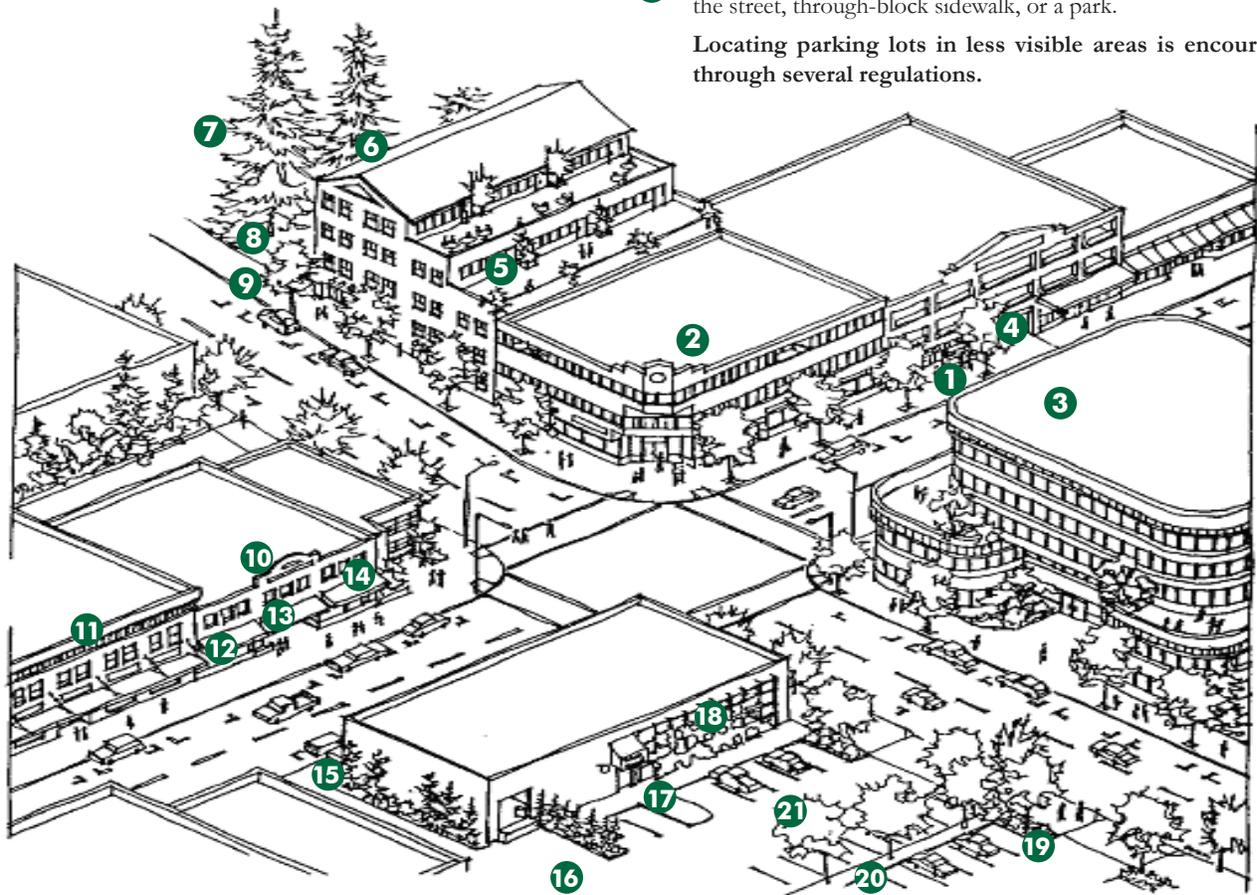
- 5 Human scale features such as balconies or decks, bay windows, covered entries, gable or hipped rooflines, multiple paned windows, or pedestrian-oriented space may be required.
- 6 More flexible method of measuring building height on slopes.
- 7 New policies regarding tree protection and enhancement of wooded slopes. Standards for size, quantity, quality, and maintenance of landscape plant materials are set by the Zoning Code.

- 8 Standards for size, quantity, quality, and maintenance of landscape plant materials are set by the Zoning Code.
- 9 Standards are set for pathway width, pavement, lighting, and site features on required major pathways and public properties.
- 10 A building cornerstone or plaque may be required.
- 11 Covering up existing masonry or details with synthetic materials is restricted.
- 12 Ground story facades of buildings on pedestrian-oriented streets or adjacent to parks may be required to feature display windows, artwork, or pedestrian-oriented space.
- 13 Pedestrian weather protection required on pedestrian-oriented streets.
- 14 Architectural detail elements such as decorative or special windows, doors, railings, grillwork, lighting, trellises, pavements, materials, or artwork to add visual interest may be required.

Size of parking lots abutting pedestrian-oriented streets may be restricted.

- 15 Quantity and locations of driveways are regulated.
- 16 Visible service areas and loading docks must be screened.
- 17 Provision for pedestrian circulation is required in large parking lots.
- 18 Blank walls near streets or adjacent to through-block sidewalks must be treated with landscaping, artwork, or other treatment.
- 19 Screening of parking lots near streets is required.
- 20 Standards for curbs, signing, lighting, and equipment are set for parking lots.
- 21 Internal landscaping is required on large parking lots visible from the street, through-block sidewalk, or a park.

Locating parking lots in less visible areas is encouraged through several regulations.



Purpose of the Design Guidelines for Downtown Kirkland

In 1989 the Kirkland City Council adopted Kirkland's Downtown Plan which set a vision for the downtown's future and outlined policies and public actions to make that vision a reality. One of the recommended actions is the adoption of a set of Downtown Design Guidelines to be used in reviewing all new development and major renovations in the downtown area. The goal of the Design Guidelines as stated in the plan is to

... balance the desired diversity of project architecture with the equally desired overall coherence of the downtown's visual and historic character. This is to be achieved by injecting into each projects' creative design process a recognition and respect of design guidelines and methods which incorporate new development into downtown's overall pattern.

In addition, the guidelines are intended to further the following urban design goals stated in the plan:

- ◆ Promote a sense of community identity by emphasizing Kirkland's natural assets, maintaining its human scale, and encouraging activities that make downtown the cultural, civic, and commercial heart of the community.
- ◆ Maintain a high-quality environment by ensuring that new construction and site development meet high standards.
- ◆ Orient to the pedestrian by providing weather protection, amenities, human scale elements, and activities that attract people to downtown.
- ◆ Increase a sense of continuity and order by coordinating site orientation, building scale, and streetscape elements of new development to better fit with neighboring buildings.
- ◆ Incorporate parks and natural features by establishing an integrated network of trails, parks, and open spaces and maintaining existing trees and incorporating landscaping into new development.
- ◆ Allow for diversity and growth through flexible guidelines that are adaptable to a variety of conditions and do not restrict new development.

Purpose of the Design Guidelines for PLA5C

Planned Area 5C is part of the Moss Bay Neighborhood and is designated for high density residential and office uses. It is located just east of the Central Business District (CBD) and shares many of the CBD's

characteristics, although retail uses are not allowed.

The adjacent steep hillside to the north of PLA5C is part of the 85th Street right-of-way and it limits potential view obstruction from the five to six story buildings which can be developed in PLA5C.

The following guidelines, which encourage wide sidewalks, do not apply to PLA5C since there are no "pedestrian oriented streets" or "major pedestrian sidewalks" designated in the Zoning Code for this area.

- ◆ Sidewalk Width: Movement Zone
- ◆ Sidewalk Width: Storefront Activity Zone

An additional guideline that does not apply is "Height Measurement on Hillsides."

Purpose of the Design Guidelines for Juanita Business District

The Juanita Business District Plan was adopted in 1990 by the City Council. It states that "the underlying goal of redevelopment in the business district is to create a neighborhood-scale, pedestrian district which takes advantage of the amenities offered by Juanita Bay."

As part of the Juanita Business District Plan, Design Regulations and Design Guidelines were established for new development and major renovations in the Business District (JBD). These guidelines and regulations are intended to further the following urban design features stated in the plan:

- ◆ Pedestrian pathways from the surrounding residential areas to and through the business district and on to Juanita Beach Park should be acquired and improved.
- ◆ View corridors to the lake should be explored through new development in the business district.
- ◆ Entry features, such as signs or sculpture, should be established in the locations shown in the Juanita Business District Plan.
- ◆ Coordinated streetscape improvements should be used throughout the business district, including street trees, street furniture, and other amenities, like flowers, banners, and signs.

Purpose of the Design Guidelines for the Market Street Corridor, including the Market Street Historic District

The City Council adopted the Market Street Corridor Plan in December of 2006 as part of the Market and Norkirk Neighborhood planning process. The new plan



was created for commercial and multifamily properties adjoining Market Street extending from the Central Business District at the south end to 19th Avenue at the north end. The plan includes a vision for the corridor of an attractive, economically healthy area that accommodates neighborhood oriented businesses, office uses and multifamily housing in a way that complements and protects the adjacent residential neighborhoods.

The historic 1890's buildings at the intersection of Market Street and 7th Avenue create a unique sense of place that represents the original town center of Kirkland. The plan establishes an historic district in this area that will reflect the City's past through both its old and new buildings and its streetscape. New development and renovation within this historic district should reflect the scale and design features of the existing historic resources in the district.

As part of the Market Street Corridor Plan, Design Regulations and Guidelines are established for new development and major renovations in the Market Street Corridor (MSC). These guidelines and regulations are intended to further the following design objectives that are stated in the plan:

- ◆ Encourage preservation of structures and locations that reflect Kirkland's heritage.
- ◆ Support a mix of higher intensity uses along the Market Street Corridor while minimizing impacts on adjacent residential neighborhoods.
- ◆ Maintain and enhance the character of the historic intersection at 7th Avenue and Market Street.
- ◆ Provide streetscape, gateway and public art improvements that contribute to a sense of identity and enhanced visual quality.
- ◆ Provide transitions between low density residential uses within the neighborhoods and the commercial and multifamily residential uses along Market Street.

Except for the MSC2 zone, the following guidelines, which suggest wider sidewalks, do not apply since there are no "pedestrian oriented streets" or "major pedestrian sidewalks" designated in the Zoning Code for the Market Street Corridor.

- ◆ Sidewalk Width: Movement Zone
- ◆ Sidewalk Width: Storefront Activity Zone

Additional guidelines that do not apply to the Market Street Corridor include:

- ◆ Protection and Enhancement of Wooded Slopes

- ◆ Height Measurement on Hillsides
- ◆ Culverted Creeks

Purpose of the Design Guidelines for North Rose Hill Business District

The North Rose Hill Business District goals and policies were adopted in 2003 as part of the North Rose Hill Neighborhood Plan. Development in the North Rose Hill Business District (NRHBD) is to complement the Totem Lake Business District and encourage increased residential capacity to help meet housing needs. Commercial uses are to be limited to those that are compatible with the residential focus of the NRHBD.

As part of the NRH plan, design regulations and guidelines were established for new development and major renovations in the Business District (NRHBD). These guidelines and regulations are intended to further the following urban design goals and policies stated in the plan:

- ◆ Ensure that public improvements and private development contribute to neighborhood quality and identity in the Business District through:
 - *Establishment of building and site design standards.*
 - *Utilization of the design review process.*
 - *Location and sharing of parking lots .*
 - *Utilization of high quality materials, public art, bicycle and pedestrian amenities, directional signs on all arterials, and other measures for public buildings and public infrastructure, such as streets and parks.*
- ◆ Provide transitions between commercial and residential uses in the neighborhood.
- ◆ Provide streetscape improvements that contribute to a sense of neighborhood identity and enhanced visual quality.

Since the focus of the NRHBD is on increasing residential capacity while accommodating supportive commercial uses, rather than developing into a destination retail business district, the following guidelines do not apply to this business district.

- ◆ Sidewalk Width – Movement Zone
- ◆ Sidewalk Width – Curb Zone
- ◆ Sidewalk Width – The Storefront Activity Zone
- ◆ Pedestrian Coverings
- ◆ Pedestrian-Friendly Building Fronts
- ◆ Upper-Story Activities Overlooking the Street

In addition, the following do not apply:

- ◆ Protection and Enhancement of Wooded Slopes
- ◆ Height Measurement on Hillsides



- ◆ Views of Water
- ◆ Culverted Creeks

Purpose of the Design Guidelines for the Totem Lake Business District Core

The Kirkland City Council adopted a new neighborhood plan for Totem Lake in early 2002. The vision set forth in the Plan for the Totem Lake Business District Core is of a dense, compact community, with a mix of business, commercial and residential uses and a high level of transit and pedestrian activity.

The Plan establishes key overall design principles for the Business District Core, as well as specific design objectives for the Totem Lake Mall (TL 2), Evergreen Hospital campus (TL 3), and the mixed-use area west of the campus (TL 1). Design objectives promoted in the plan for the Business District Core include:

- ◆ Accommodate high density, transit-oriented development, consistent with the district's position in an Urban Center.
- ◆ Ensure that public and private development contribute to a lively and inviting character in the Business District Core.
- ◆ Reinforce the character of the Business District Core through public investments
- ◆ Produce buildings that exhibit high quality design, incorporate pedestrian features and amenities and display elements of both continuity and individuality
- ◆ Provide public spaces that are focal points for the community
- ◆ Provide visual and functional connections between adjacent developments through landscaping, public spaces and pedestrian connections.

Design considerations specific to the three subareas within the district include:

Mixed-Use Area (TL 1)

- ◆ Break up the mass of larger buildings through techniques such as towers over podiums, to create a varied building footprint and the perception of a smaller overall building mass.
- ◆ Incorporate features that create distinctive roof forms, to contribute to a skyline that is visually interesting throughout the district.

- ◆ Ensure appropriate transitions from lower density uses north of the Business District Core through providing residentially scaled façades and centered building masses in development along NE 132nd Street.

Retail Center (TL 2)

The Totem Lake Business District Plan direction for the TL2 area is to support its growth as a vibrant, intensive retail center for the Kirkland community and surrounding region. These guidelines are intended to promote the vision of this area as a "village-like" community gathering place, with high-quality urban and architectural design in redevelopment. To provide for flexibility and increased development potential, while ensuring coordinated development and design integrity over time, redevelopment should occur within the context of an overall site development or Master Plan for the entire property.

Evergreen Hospital Medical Center Campus (TL 3)

The Totem Lake Business District Plan acknowledges the important role the hospital plays in the Kirkland community, and supports growth on the campus to strengthen this role. Design objectives stated in the Plan for the Evergreen Hospital campus are consistent with those expressed in the Master Plan approved for the site:

- ◆ Taller buildings should be located toward the center of the site and designed to minimize shadowing and transition impacts on residential areas.
- ◆ Public access to usable green spaces on the campus can help to offset the impacts of taller buildings on the site.
- ◆ Ensure campus edges are compatible with neighboring uses.
- ◆ Enhance and improve pedestrian access with the campus and to surrounding uses, particularly the transit center and to TL 2.

The approved Master Plan for the hospital campus includes additional, unique design guidelines that apply to institutional development in a campus environment:

- ◆ *Respond to Physical Environment:* New buildings should be attractive as well as functional additions to the campus.
- ◆ *Enhance the Skyline:* The upper portion of buildings should be designed to promote visual interest and variety on the skyline, except where building function dictates uninterrupted vertical mass.



- ◆ Avoid blank facades in buildings located on the perimeter of the campus.
- ◆ Use materials and forms that reinforce the visual coherence of the campus.
- ◆ Provide inviting and useable open space.
- ◆ Enhance the campus with landscaping.
- ◆ Guidelines for the transit center to be located on the hospital campus should be developed and incorporated with guidelines for the rest of the campus.

The following guidelines do not apply to the Totem Lake Business District Core:

- ◆ Height Measurement on Hillsides
- ◆ Views of Water

Purpose of the Design Guidelines for Finn Hill Neighborhood Center (FHNC)

The Finn Hill Neighborhood Plan was adopted in early 2018 by the City Council. The Neighborhood Plan sets the vision for the Finn Hill Neighborhood Center north of NE 141st ST along Juanita Drive as a mixed use, neighborhood scale commercial and residential village to strengthen the neighborhood identity.

The design guidelines are intended to further the following design objectives described in the Plan for the FHNC and summarized below:

- ◆ Building and site design is attractive, pedestrian oriented and compatible in scale and character with the surrounding neighborhood.
- ◆ Pedestrian paths connect between uses on a site and adjacent properties.
- ◆ Parking lots or parking garages are oriented to the back or side of buildings or treated with landscaping or design features.
- ◆ Streetscape improvements are attractive to identify Finn Hill as unique to other commercial districts and multi-modal in design.
- ◆ Public gathering spaces contain seating and landscaping.
- ◆ Bicycle and pedestrian amenities are provided including directional signage.
- ◆ Green building and sustainable site techniques are utilized.
- ◆ Art, signs and landscaping are used to add character to the commercial area.

The following guidelines do not apply to this district:

- ◆ Protection and enhancement of wooded slopes
- ◆ Height measurement on Hillsides
- ◆ Culverted Creeks
- ◆ Open Space at Street Level

Purpose of the Design Guidelines for the Houghton/Everest Neighborhood Center (HENC)

The plan for the HENC was adopted in 2017. The primary goal of the plan is to promote a strong and vibrant pedestrian oriented neighborhood center with a mix of commercial and residential land uses that primarily serve the adjacent neighborhoods.

In addition, the HENC contains an important interface with the Cross Kirkland Corridor (CKC). Successfully integrating site and building design, as well as public access, with this important transportation and open space amenity will mutually benefit the neighborhood center and the CKC. Thoughtful design of the interface will attract nonmotorized customers and residents to the neighborhood center and create an attractive and safe space for pedestrians and bicyclists using the CKC.

The Guidelines are intended to further the following design objectives that are stated in the Comprehensive Plan.

- ◆ Coordinate development on both sides of the NE 68th Street Corridor in the Everest and Central Houghton neighborhoods.
- ◆ Promote a pedestrian-oriented development concept through standards for a coordinated master plan for the center.
- ◆ Reduce ingress and egress conflicts within and around the center through creation of a circulation system for all users including vehicles, bicycles and pedestrians.
- ◆ Design buildings with careful attention given to modulation, upper story step backs, and use of materials to reduce the appearance of bulk and mass.
- ◆ Coordinate street improvements.
- ◆ Provide transitions between commercial and low density residential areas.
- ◆ Discourage southbound through traffic on 106th Avenue NE.



- ◆ Enhance the gateway at the corner of NE 68th Street and 108th Avenue NE.
- ◆ Provide gathering spaces and relaxation areas within the center.

The following guidelines do not apply to the Neighborhood Center:

- ◆ Protection and Enhancement of Wooded Slopes
- ◆ Height Measurement on Hillsides
- ◆ Culverted Creeks

Purpose of the Design Guidelines for Neighborhood Business Districts

The Comprehensive Plan establishes a hierarchy of commercial districts, with regional goods and services at the upper end and neighborhoods goods and services at the lower end.

Kirkland's Neighborhood Business Districts (BN, BNA, and MSC2) are important in providing neighborhood goods and services. Given the more localized draw for residents to meet their everyday needs, an emphasis on convenient and attractive pedestrian connections and vehicular access is important.

In addition, because these districts are surrounded by the residential land uses they serve, the design character and context of new development is critical to ensure that it integrates into the neighborhood.

The design guidelines are intended to further the following design objectives that are stated in the Plan:

- ◆ Establish development standards that promote attractive commercial areas and reflect the distinctive role of each area.
- ◆ Encourage and develop places and events throughout the community where people can gather and interact.
- ◆ Moss Bay neighborhood: Ensure that building design is compatible with the neighborhood in size, scale, and character.
- ◆ South Rose Hill neighborhood: Residential scale and design are critical to integrate these uses into the residential area.

The following guidelines do not apply to these districts:

- ◆ Protection and Enhancement of Wooded Slopes
- ◆ Height Measurement on Hillsides
- ◆ Culverted Creeks

