



MEMORANDUM

To: Design Review Board
From: Aoife Blake, Associate Planner
Date: September 10, 2020
File No.: DRV20-00473
Subject: **MIRRA TOWNHOMES PROJECT
DESIGN RESPONSE CONFERENCE**

I. MEETING GOALS

At the September 21 Design Review Board (DRB) meeting, the DRB should conduct a Design Response Conference and determine if the project is consistent with the design guidelines contained in *Design Guidelines for Pedestrian Oriented Business Districts*, as adopted in Kirkland Municipal Code (KMC) Section 3.30.040.

During the Design Response Conference, the DRB should provide feedback on the following topics:

- Building massing
- Pedestrian and vehicle access
- Landscaping
- Materials, colors, and details

The DRB should deliberate on the applicant's proposed design and continue the meeting to a future date, if the DRB determines that additional information is needed to decide on the project.

II. PROPOSAL

The site is a collection of five properties located at 203 and 207 1st Avenue S, 200 2nd Avenue S and 106, 110 and 114 2nd Street S (see Attachment 1). Schuyler Tutt of Medici Architects has applied for a Design Response Conference for a new residential development with below grade parking on the subject property (see Attachment 2). The project consists of 22 attached dwelling units and 45 parking stalls. The parking will be provided in a shared underground parking garage. Vehicular access is proposed from 2nd Street S.

III. SITE

The subject property currently contains six residential buildings. All buildings will be removed with development. The site slopes downward from an elevation of about 75 feet at its southwest corner to an elevation of 68 feet at its northeast corner, over a

distance of about 224 feet. The site has three street frontages: 1st Avenue S to the north, 2nd Street S to the west, and 2nd Avenue S to the south. These streets are designated as local neighborhood access streets, and 2nd Avenue S is also a major pedestrian sidewalk.

The following list summarizes the zoning designation, uses, and allowed heights of properties adjacent to the subject property:

- North:* **CBD 1B (Commercial/Multifamily):** Maximum height is 55' above the midpoint of the frontage of the property on the abutting right-of-way, excluding 1st Avenue South.
- East:* **CBD 3 & 4 (Church):** Maximum height is 41' above Average Building Elevation (ABE) in CBD 3 except no portion of a structure within 100 feet of the southerly boundary of 2nd Avenue S abutting the RM 3.6 zone may exceed 30'. Maximum height is 54' above ABE or existing grade in CBD 4 except no portion of a structure within 100 feet of the southerly boundary of 2nd Avenue S abutting the RM 3.6 zone shall exceed 35', and no portion of a structure within 40' of 1st Avenue S shall exceed 41'.
- South:* **RM 3.6 (Single-Family):** Maximum height is 30' above ABE.
- West:* **CBD 4 (Multifamily):** Maximum height of structure is 55.4 feet above ABE west of 2nd Street S, including any adjacent structure in CBD 1 west of 2nd Street S developed with a structure in this zone.

Additional photographs prepared by the applicant that show the surrounding properties are contained in Attachment 2.

IV. CONCEPTUAL DESIGN CONFERENCE

A Conceptual Design Conference was held on March 2, 2020. The DRB provided direction to the applicant in preparation for the Design Response Conference. At the meeting, the DRB discussed:

- A. How the design guidelines affect or pertain to the proposed development.
- B. Which guidelines applied to the proposed development; and
- C. The application materials that are needed for the Design Response Conference.

The DRB's feedback from the conference is summarized in Section V.B below under the DRB's discussion on the various design topics.

V. DESIGN RESPONSE CONFERENCE

The Design Review Board reviews projects for consistency with design guidelines for pedestrian-oriented business districts, as adopted in Kirkland Municipal Code Chapter 3.30. In addition to the standard guidelines contained in the *Design Guidelines for Pedestrian-Oriented Business Districts*, the following information summarizes key guidelines which apply specifically to the project or project area. See also Section VI for information regarding zoning regulations and how they affect the proposed development.

A. Pedestrian-Oriented Design Guidelines

1. General

The following is a list of key design issues and/or design techniques that should be addressed with this project as identified in the design guidelines.

- Building Scale
 - Vertical and horizontal modulation
 - Architectural scale
- Pedestrian-Orientation
 - Pedestrian friendly building fronts
 - Blank wall treatment
- Landscaping
- Building material, color, and detail

See the adopted Design Guidelines for Pedestrian-Oriented Business Districts for complete text and explanations.

2. Special Considerations for Downtown Kirkland

In addition to the standard guidelines contained in the *Design Guidelines for Pedestrian-Oriented Business Districts*, the following list summarizes some of the key guidelines and special considerations that apply specifically to the project or project area.

- Sidewalk Width: New development should provide sidewalks at the recommended width. Providing wider sidewalks throughout downtown is a long-term endeavor. NOTE: 2nd Avenue South is a major pedestrian sidewalk and subject to the zoning code provisions of KZC 110.52.
- Parking Location: The Downtown Plan calls for limiting the number of vehicle curb cuts.
- Parking Garage: Garages built on Downtown Kirkland's perimeter slopes, near residential areas, or near the waterfront can fit less obtrusively into the landscape when terraced. Treatment of the facade of the parking structure can be just as effective in mitigating the visual impacts of parking garages as pedestrian-oriented businesses, plazas, or landscaped setbacks at the ground level.
- Fenestration Patterns: Breaking larger window areas into smaller units to achieve a more intimate scale is most important in Design Districts 1, 2, 4, 8, and the southwest portion of 3 where new buildings should fit with older structures that have traditional styled windows.
- Street Corners: Special attention should be paid to both the design and detailing of new buildings on corner sites in the pedestrian oriented design districts. Existing buildings could incorporate some of these elements (human-scale and visual punctuation) through the use of such elements as awnings and well-designed signs at the corner.

B. Compliance with Design Guidelines

1. Scale

a. DRB Discussion

The DRB expressed a preference for massing Option 3 at the Conceptual Design Conference. Other design direction provided by the DRB included:

- Provide blank wall treatment on building elevations along on 2nd Avenue S.
- The project should look for opportunities to create a strong street presence along 2nd Street S.

b. Supporting Design Guidelines

The *Design Guidelines for Pedestrian Oriented Business Districts* contain the following policy statements that address the use of these techniques:

- Architectural Elements: Decks, Bay Windows, Arcades, Porches. Architectural building elements such as arcades, balconies, bay windows, roof decks, trellises, landscaping, awnings, cornices, friezes, art concepts, and courtyards should be encouraged.
- Building Modulation - Vertical Vertical building modulation should be used to add variety and to make large buildings appear to be an aggregation of smaller buildings.
- Building Modulation - Horizontal Horizontal building modulation may be used to reduce the perceived mass of a building and to provide continuity at the ground level of large building complexes. Building design should incorporate strong pedestrian-oriented elements at the ground level and distinctive roof treatments.
- Blank Walls Blank walls should be avoided near sidewalks, parks, the Cross Kirkland Corridor and Eastside Rail Corridor, and pedestrian areas. Where unavoidable, blank walls should be treated with landscaping, art, or other architectural treatments.

c. Staff Analysis

At the direction of the DRB, the applicant has pursued massing Option 3 and has provided detailed plans for review (see Attachment 2). Staff has reviewed the plans and has the following comments regarding the scale of the building based on the applicable design guidelines.

- *The height of the buildings (4 stories) requires the use of vertical and horizontal modulation, and architectural elements to help break up the façades of the buildings and to avoid a flat (straight up and down) facade.*
- *The applicant has provided projects views from key vantage points identified in Attachment 2. The plans indicate how the project's modulation, materials, and fenestration respond to these vantage points.*
- *The proposal successfully incorporates fenestration patterns, vertical modulation and material changes on all facades to help break up the building mass, create a human scale, and add visual interest.*
- *In addition to the above techniques, the roof decks add some horizontal modulation to the west facade of Buildings 1 & 2 and east façade of Buildings 3 & 4. The roof decks on Buildings 1 & 2 establish a relationship with 2nd Street S in creating an active streetscape.*

- *The proposal incorporates elements to enhance the human scale along the central pedestrian corridor with fenestrations on the ground level, landscaping, small canopies (only for east units), trellis, lighting, and seating (see Attachment 2, sheets 17-19, 37 & 38).*
- *The proposal effectively utilizes treatments, such as fenestrations, material changes and landscaping, to minimize a blank wall along 2nd Avenue S façade (see Attachment 2, sheet 41).*
- *The applicant has been instructed to have a digital model available at the September 21 meeting.*

The DRB should provide input on the proposed design and identify any needed changes to the facades, including the following items:

- *Does the west façade facing 2nd Street S contain enough vertical and horizontal modulation, and architectural elements to produce a strong street presence?*
- *Have all the facades utilized adequate techniques in breaking the buildings mass and are they adequately designed to deal with blank walls?*
- *Does the building design respond appropriately to human scale concerns along the central pedestrian corridor?*

2. Pedestrian and Vehicular Access

a. DRB Discussion

The DRB provided the following direction regarding pedestrian and vehicular access:

- Garage treatment, hardscape, and building design should contribute to a good pedestrian friendly pathway along the surrounding streets.
- Look for opportunities for street activation along 2nd Street S.

b. Supporting Design Guidelines

The *Design Guidelines for Pedestrian Oriented Business Districts* contain the following statements that pertain to vehicular and pedestrian access:

- **"Pedestrian-Friendly" Building Fronts**
To mitigate the negative effects of blank walls:
 - Recess the wall with niches that invite people to stop, sit, and lean.
 - Install trellises with climbing vines or plant materials.
 - Provide a planting bed with plant material that screens at least 50 percent of the surface.
 - Provide artwork on the surface.
- **Upper-Story Activities Overlooking the Street**
All buildings on pedestrian-oriented streets should be encouraged to have upper-story activities overlooking the street, as well as balconies and roof decks with direct access from living spaces. Planting trellises and architectural elements are encouraged in conjunction with decks and bay windows. Upper-story commercial activities are also encouraged.
- **Parking Location and Entrances**

Minimize the number of driveways by restricting curb cuts and by encouraging property and business owners to combine parking lot entrances and coordinate parking areas. Encourage side and rear yard parking areas by restricting parking in front yards. Require extensive screening where there is front yard parking.

- Lighting from Buildings
 - All building entries should be well lit. Building facades in pedestrian areas should provide lighting to walkways and sidewalks through building-mounted lights, canopy or awning-mounted lights, and display window lights.
 - Encourage variety in the use of light fixtures to give visual variety from one building facade to the next. Back-lit or internally-lit translucent awnings should be prohibited.
- Pedestrian-Oriented Plazas

Successful pedestrian-oriented plazas are generally located in sunny areas along a well-traveled pedestrian route. Plazas must provide plenty of sitting areas and amenities and give people a sense of enclosure and safety.

c. Staff Analysis

The applicant has submitted plans based on Public Works Department direction regarding right-of-way improvement requirements (see Attachment 2, Sheet 9).

Vehicular Access

The proposed plans show the Public Works approved vehicular access from 2nd Street S (see Attachment 2). The appearance of the parking garage lid at the entrance is proposed to be softened by using an open railing and vertical plantings to create a roof-deck feel. Lighting is proposed along the driveway for security and wayfinding.

Pedestrian Access

Public pedestrian sidewalks will be located along the north, west, and south facades. 2nd Avenue S is a designated major pedestrian sidewalk. See Section VI.F for additional information regarding required sidewalk improvements.

A 6-foot wide central pedestrian corridor is proposed to bisect the development to provide pedestrian access to units in Buildings 3 and 4 and to the parking garage. Overall, the project incorporates many different techniques to create a "Pedestrian-Friendly" environment as previously discussed in the section V.B.1 'Scale'.

Back-of-House Functions

A garbage and recycling enclosure is located on the north façade of Building 1, accessed from 1st Avenue S. A trash pick-up layout has been provided by the applicant and approved by the City (see Attachment 2, sheet 12). The enclosure is screened with cedar clad steel doors.

The DRB should provide input on the following items:

- *Is the parking garage entrance and surrounding area adequately treated and screened?*
- *Are the features provided throughout the project cohesive and enrich the pedestrian experience?*
- *Is the design of the garbage and recycling enclosure aligned with the overall design of the development?*

3. **Landscaping**

a. DRB Discussion

The DRB provided the following direction regarding open space and landscaping:

- Provide a detailed landscape plan showing public spaces, courtyard concept and 2nd Avenue S walkway treatments.
- Provide a solar access study for the central pedestrian corridor and any other onsite open spaces

b. Supporting Design Guidelines

The *Design Guidelines* and Zoning Regulations contain the following guideline addressing the visual quality of landscapes:

- KZC Chapter 95 requires that a landscape plan be approved as part of the Design Review Process.
- The placement and amount of landscaping for new and existing development should be mandated through design standards. Special consideration should be given to the purpose and context of the proposed landscaping. The pedestrian/auto landscape requires strong plantings of a structural nature to act as buffers or screens.
- The pedestrian landscape should emphasize the subtle characteristics of the plant materials. The building landscape should use landscaping that complements the building's favorable qualities and screens its faults.

c. Staff Analysis

Landscaping should be placed in areas to help mitigate building massing and enhance the pedestrian experience along the project frontages.

The applicant has provided a solar study (see Attachment 2, sheet 16) and a landscaping plan (see Attachment 2, sheets 17-18). The landscape plan shows the location of the proposed plantings and a plant schedule.

The DRB should provide input on the following items:

- *What changes, if any, are needed to the landscape plan?*
- *Are there other opportunities for landscaping?*

4. **Building Materials, Color, and Details**

a. DRB Discussion

This topic was not discussed in detail at the Conceptual Design Conference.

b. Staff Analysis

Attachment 2, pages 24 - 36 contain color elevation drawings and callouts for the proposed building materials. The DRB should provide feedback to the applicant regarding the proposed materials and colors. The DRB should discuss whether additional or larger samples of materials and colors should be provided at the next Design Response Conference.

VI. KEY ZONING REGULATIONS

The applicant's proposal is also subject to the applicable requirements contained in the Kirkland Municipal Code, Zoning Code, Fire and Building Code, and Public Works Standards. It is the responsibility of the applicant to ensure compliance with the various provisions contained in these ordinances. Attachment 3, Development Standards, is provided to familiarize the applicant with some of these additional development regulations. These regulations and standards are not under the review authority of the DRB and will be reviewed for compliance as part of the building permit review for the project.

Development of the subject property is subject to the regulations for the CBD 4 zone (see Attachment 4). The following regulations are important to point out as they form the basis of any new development on the site. Below are some of the key zoning standards that apply to the development followed by staff comment in italics.

- A. Permitted Uses: Permitted uses in this zone include, but are not limited to retail, restaurants, office, and stacked or attached dwelling units.

Staff Comment: The applicant is proposing 22 units attached dwellings. The proposal is consistent with the permitted uses for the CBD 4 zone.

- B. Required Yards: Attached Dwelling Units must provide a 10' front required yard. The side and rear required yards are 0'.

Staff Comment: The site contains three required front yards and one required side yard. The proposed plans meet the required yards for the CBD 4 zone.

- C. Height: The CBD 4 zone allows a maximum height of 54' above ABE except no portion of a structure within 100 feet of the southerly boundary of 2nd Avenue S abutting the RM 3.6 zone shall exceed 35', and no portion of a structure within 40' of 1st Avenue S shall exceed 41'.

KZC section(s) 50.62 allow(s) the following exceptions to this height:

- Decorative parapets may exceed the height limit by a maximum of four (4) feet; provided, that the average height of the parapet around the perimeter of the structure shall not exceed two (2) feet.
- For structures with a peaked roof, the peak may extend five (5) feet above the height limit if the slope of the roof is greater than three (3) feet vertical to 12 feet horizontal and eight (8) feet above the height limit if the slope of the roof is equal or greater than four (4) feet vertical to 12 feet horizontal.

Staff Comment: The applicant has provided a preliminary height calculation. The applicant must demonstrate compliance with the City's height requirements as part of any building permit.

- D. Parking: The project is required to comply with the following parking standards:
- Residential

- 1.2 stalls per studio unit
- 1.3 stalls per 1-bedroom unit
- 1.6 stalls per 2-bedroom unit
- 1.8 stalls per 3 or more-bedroom unit
- Guest Parking: A minimum 10% of the total number of required parking spaces shall be provided for guest parking and located in a common area accessible by guests. If the required number of guest parking spaces results in a fraction, the applicant shall provide the number of spaces equal to the next higher whole number.

Staff Comment: The City has approved a parking modification (TRAN20-00266) to allow the use of tandem parking stalls subject to conditions. The applicant will be required to demonstrate compliance with the conditions of approval for the parking modification and the applicable City parking requirements as part of any building permit.

- E. Affordable Housing Requirements: The project is required to provide at least 10 percent of the units as affordable housing units as defined in KZC Chapter 5.

Staff Comment: A review for compliance with the City's Affordable Housing Regulations will occur as part of the building permit.

- F. Sidewalks. 1st Avenue S, 2nd Street S, and 2nd Avenue S are designated neighborhood access streets, and 2nd Avenue S is also a major pedestrian sidewalk. Therefore, KZC 110.30 - 110.50 establishes that:

- 1st Avenue S and 2nd Street S require a minimum 5' wide sidewalk behind a 4.5' landscape strip along the entire frontage of the subject property abutting 1st Avenue S and 2nd Street S.
- 2nd Avenue S requires a minimum 8' wide sidewalk with 4x6 tree wells with street trees 30 ft on-center per KZC 110.52.3.
- A dedication is required on the northwest and southwest corner of the property to encompass all improvements.
- The final sidewalk configuration should be approved through the design review process.

Staff Comment: The preliminary plans submitted by the applicant illustrates compliance with the sidewalk width and location standards but not with the land dedication requirement. The applicant is required to demonstrate compliance with the City's right-of-way requirements with any development permit.

VII. STATE ENVIRONMENTAL POLICY ACT

SEPA is the state law that requires an evaluation of a development proposal for environmental impacts. The applicant has submitted an Environmental Checklist and the City is currently reviewing their application. The DRB decision on the project will not be issued until after the SEPA determination has been issued.

VIII. PUBLIC COMMENT

No public comment has been received as of the date of this staff report.

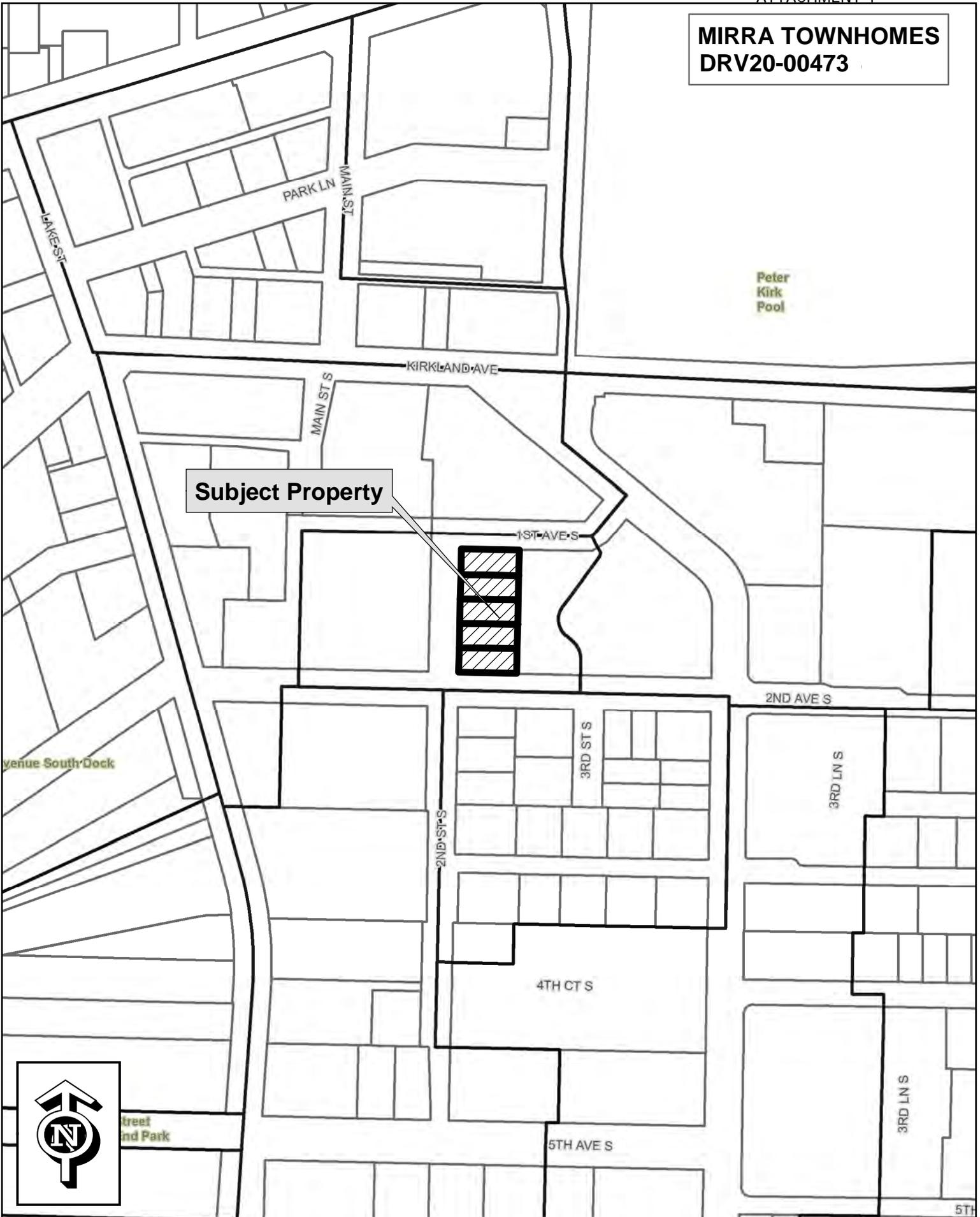
IX. SUBSEQUENT MODIFICATIONS

Modifications to the approval may be requested and reviewed pursuant to the applicable modification procedures and criteria in effect at the time of the requested modification.

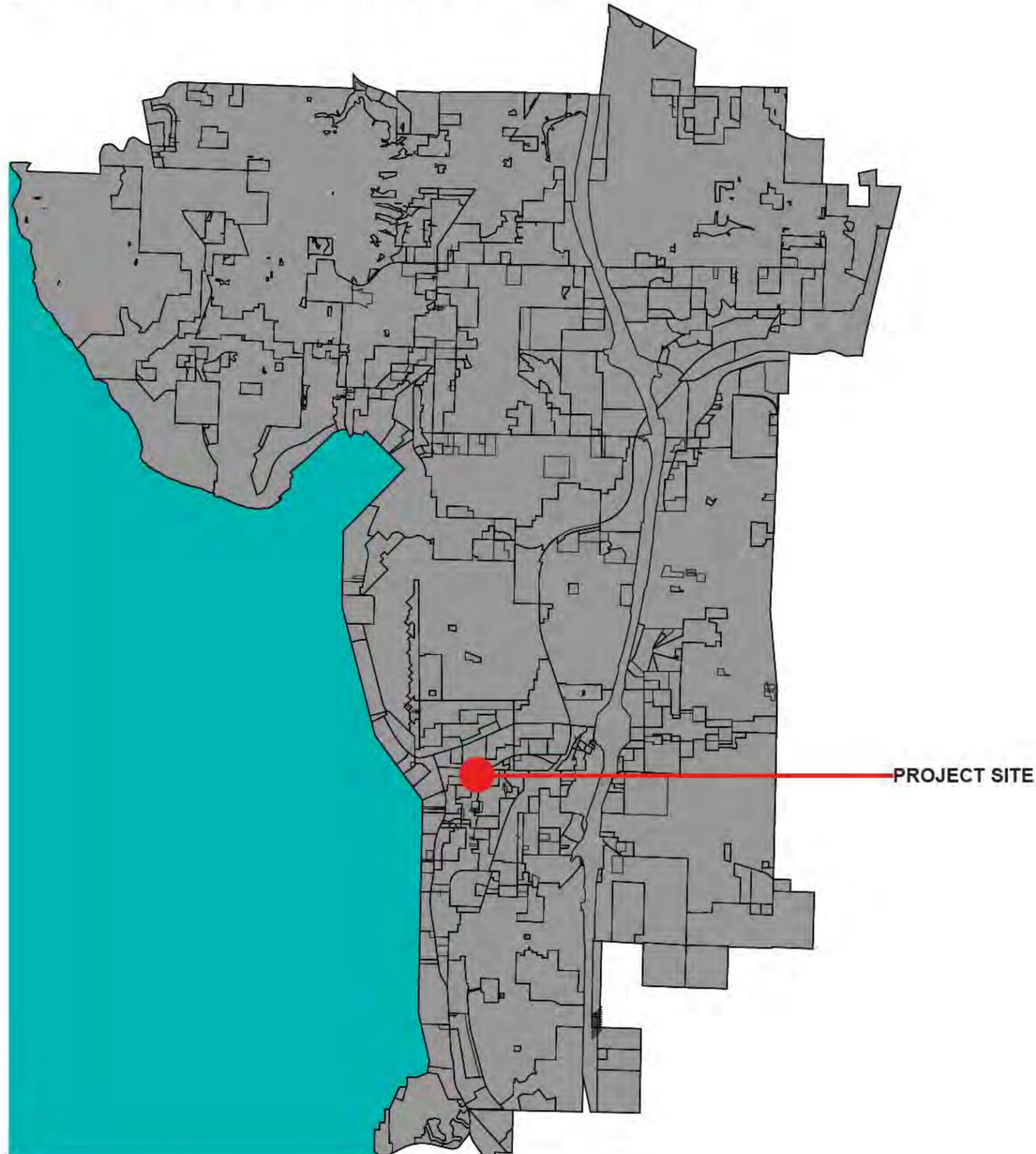
X. ATTACHMENTS

1. Vicinity Map
2. Plans
3. Development Standards
4. CBD 4 Use Zone Chart

**MIRRA TOWNHOMES
DRV20-00473**



203 1st Ave S, Kirkland WA 98033



PROJECT ADDRESS

203 1st Ave S
Kirkland, WA 98033

KIRKLAND PERMIT NUMBERS

DRV20-00033, TRAN20-00320, TRAN20-00266

PROJECT TEAM

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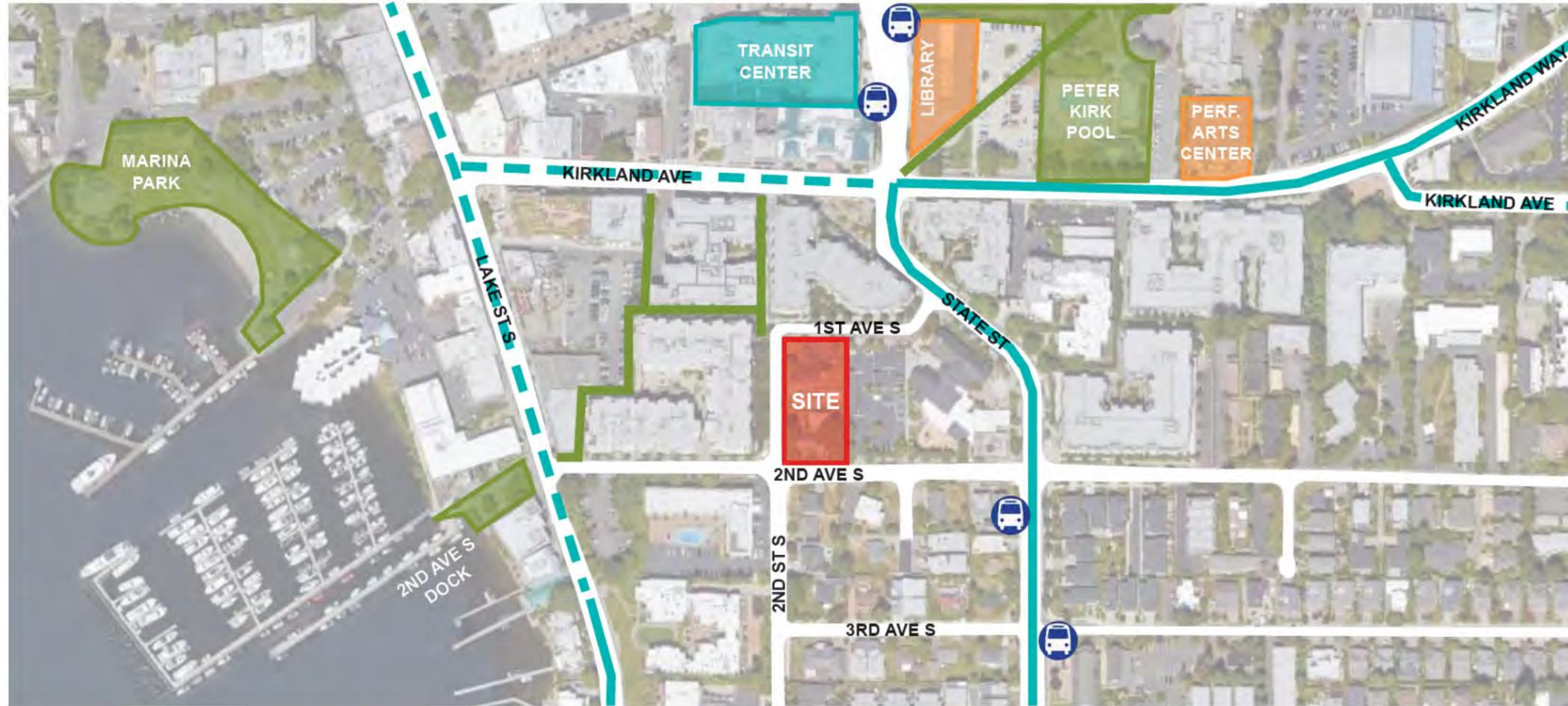
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VICINITY MAP AND DESIGN OBJECTIVES



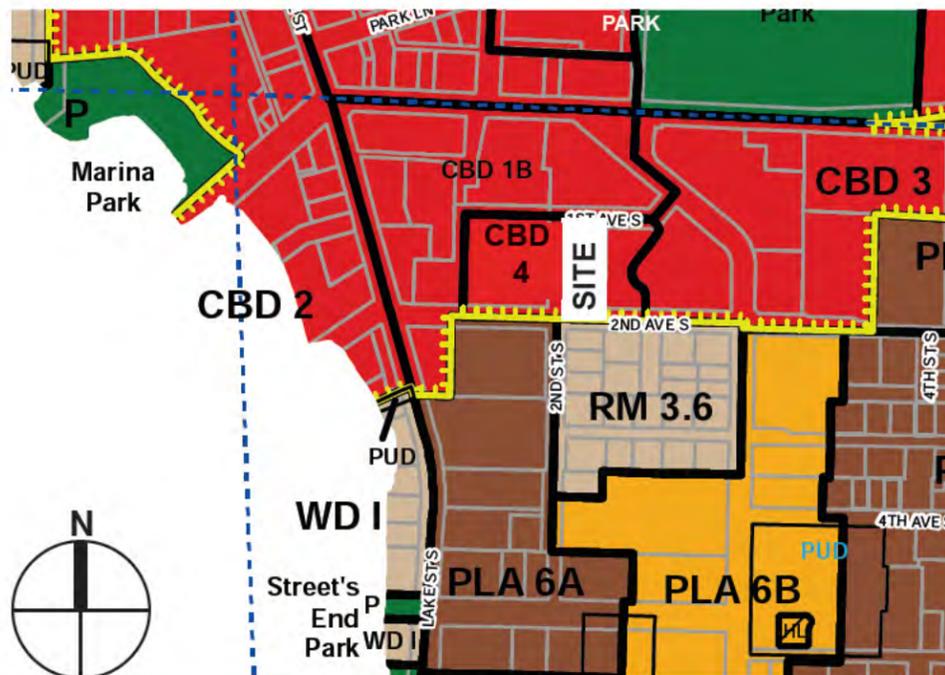
PROJECT DESCRIPTION

Residential development with on-site parking.

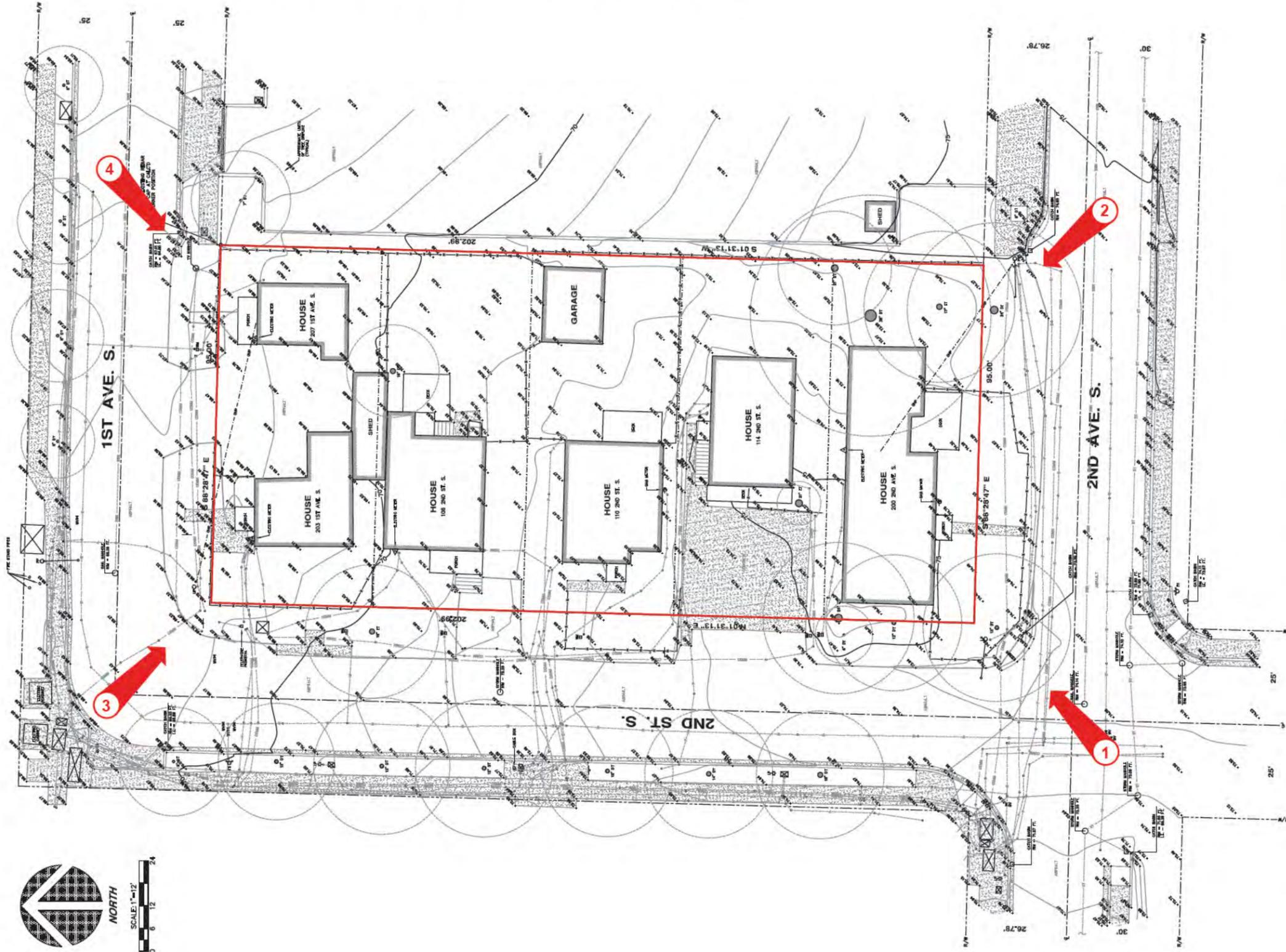
DEVELOPMENT OBJECTIVES

To create a pedestrian friendly townhome development which provides a transition from multi-family structures to single family uses. The project seeks to maximize density potential while integrating landscape, daylighting, and fostering an active street level approach.

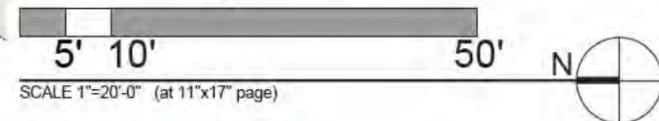
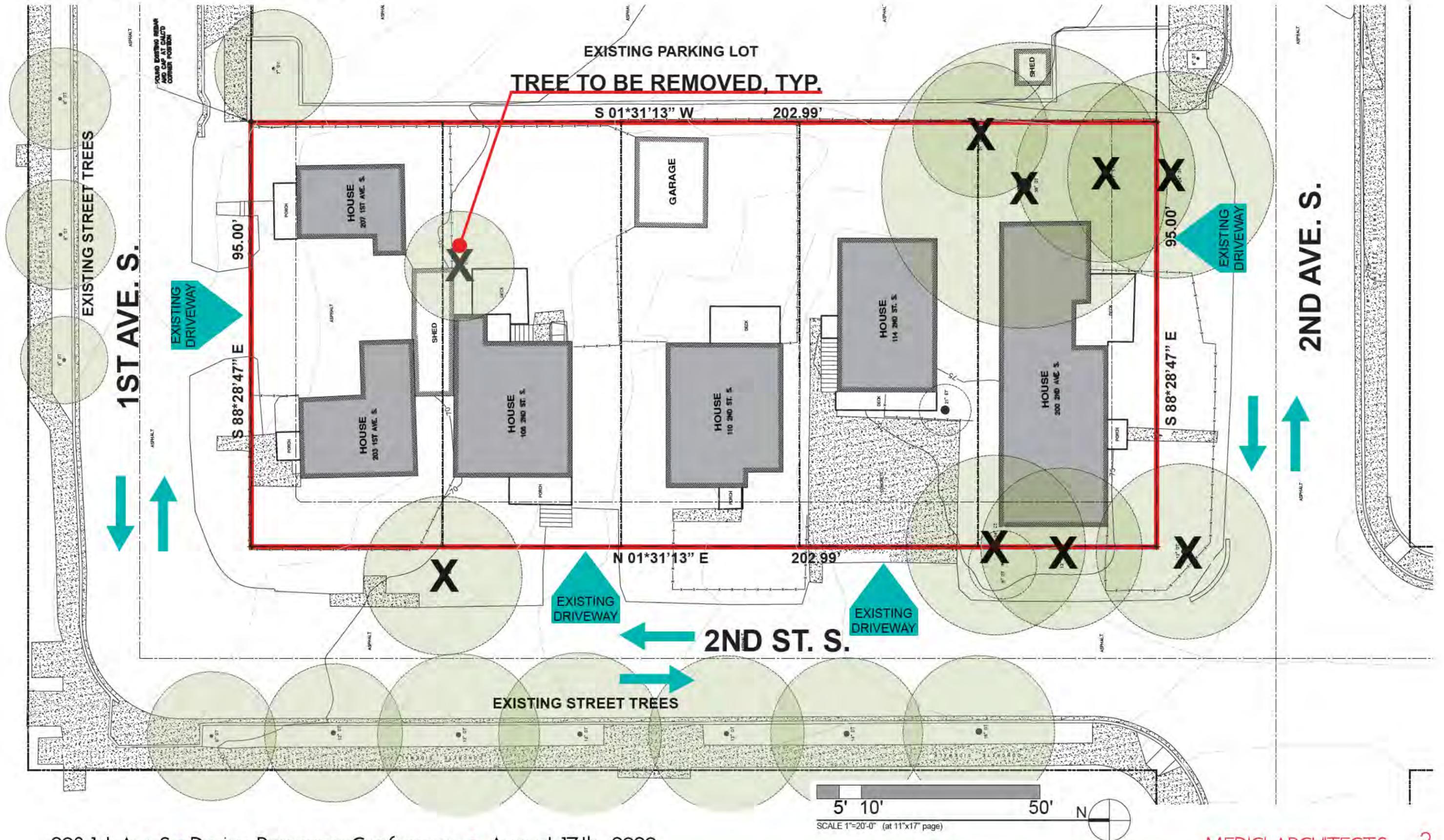
-  PUBLIC TRAIL
-  BIKE FRIENDLY
-  DEDICATED BIKE LANES
-  BUS STOP



SITE SURVEY / CONTEXT PHOTOS

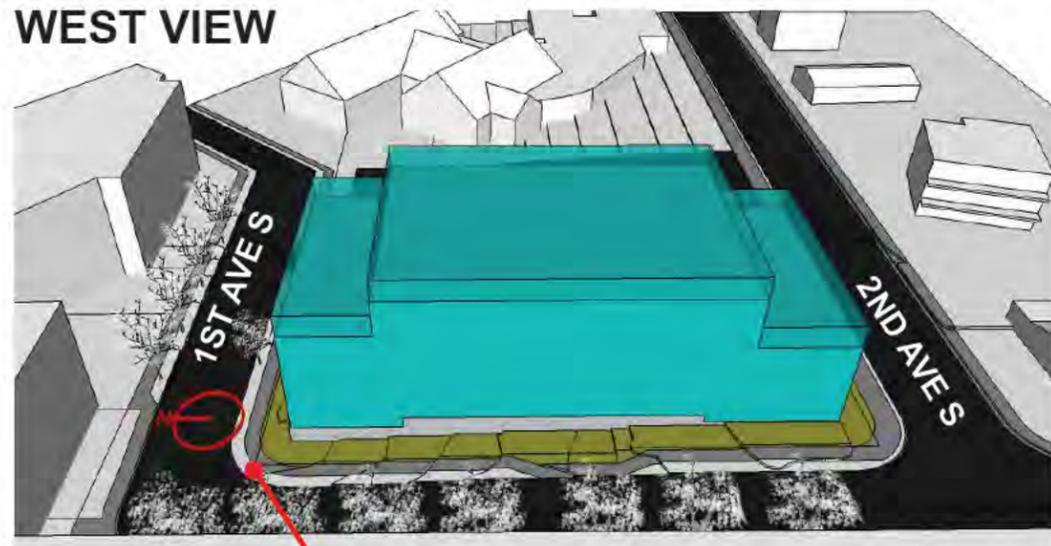


EXISTING SITE PLAN



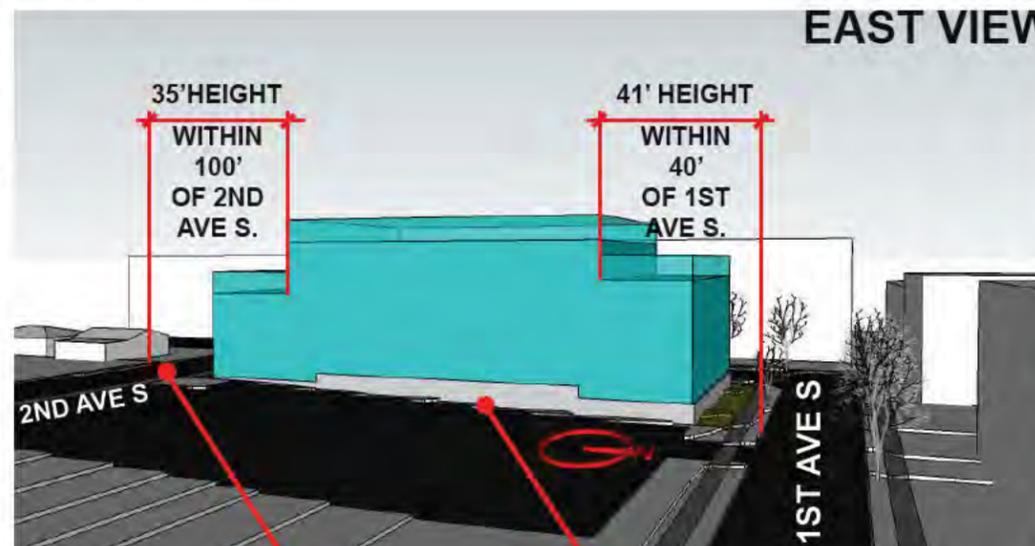
ZONING DATA AND CODE ANALYSIS

WEST VIEW



10'0" Front Yards

EAST VIEW



Adjacent medium residential zone
 0'-0" Setback @ Rear

PROJECT ADDRESS

203 and 207 1st Ave S/ 200 2nd Ave S/ 106,110 and 114 2nd St S, Kirkland WA 98033

PARCEL #(s)

1244000155; 4397700140; 4397700135; 4397700130; 4397700125

ZONE

CBD 4

DESIGN DISTRICT

Central Business District, Pedestrian-Oriented Business District

LOT AREA

19,284 SF .44 acre

DENSITY

Unlimited

LOT COVERAGE

100% Allowed

MAXIMUM HEIGHT

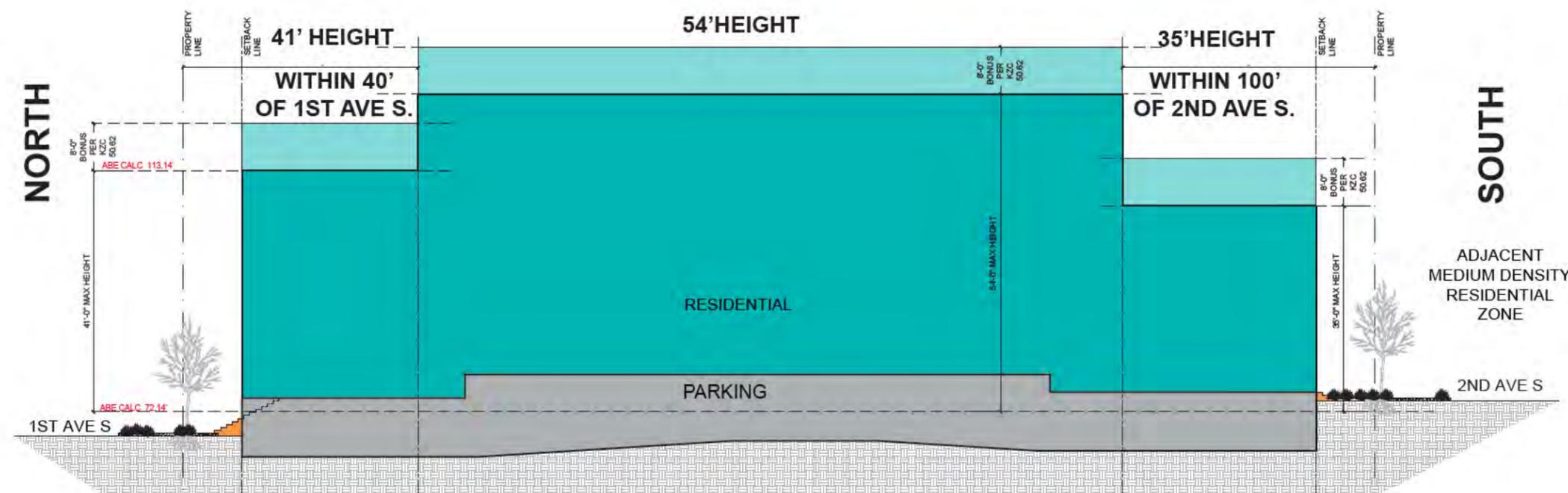
54' (41' within 40' of 1st Ave S, 35' within 100' of the southerly boundary of 2nd Ave S. Structures with peaked roof forms are allowed increased heights.

SETBACKS

0' Setback at rear, 10'-0" setback at all front yards under KZC 50.32 Table.

LANDSCAPE

No buffer requirements.



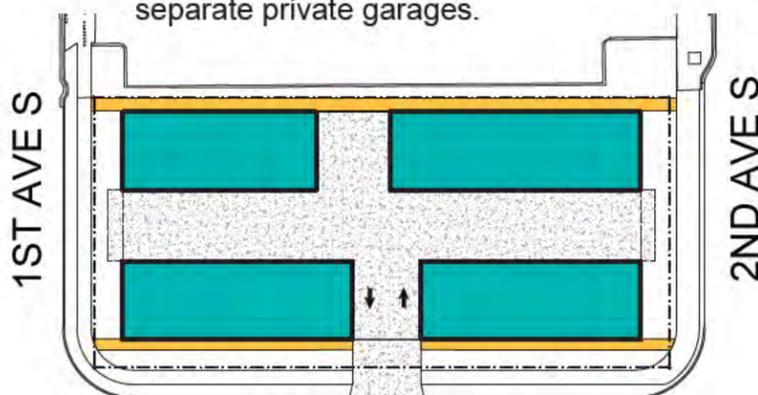
USE KEY

 RESIDENTIAL AREA	 PARKING & ACCESS	 PEDESTRIAN ACCESS
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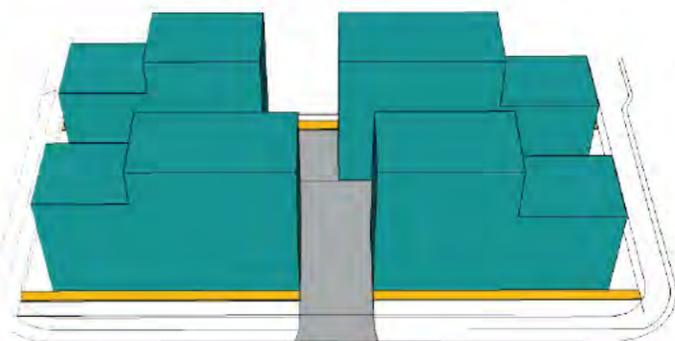
ARCHITECTURAL MASSING: Design Schemes

SCHEME ONE PRIVATE GARAGE

Single vehicular access point serving separate private garages.

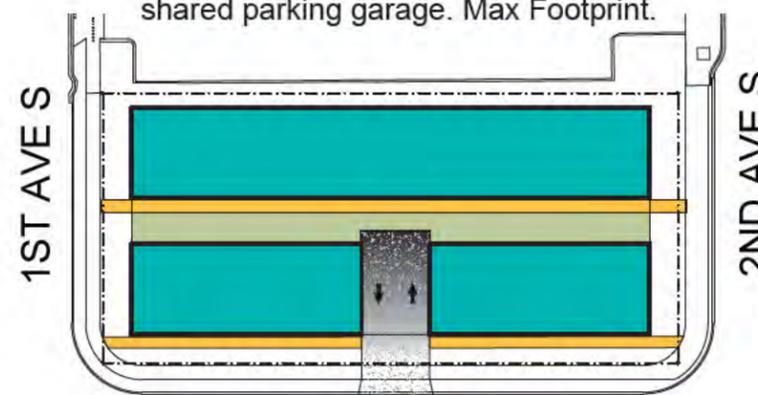


2ND ST S

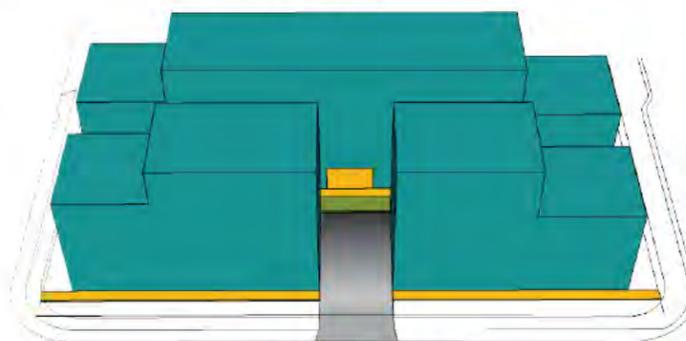


SCHEME TWO MAX ENVELOPE

Single vehicular access point serving shared parking garage. Max Footprint.

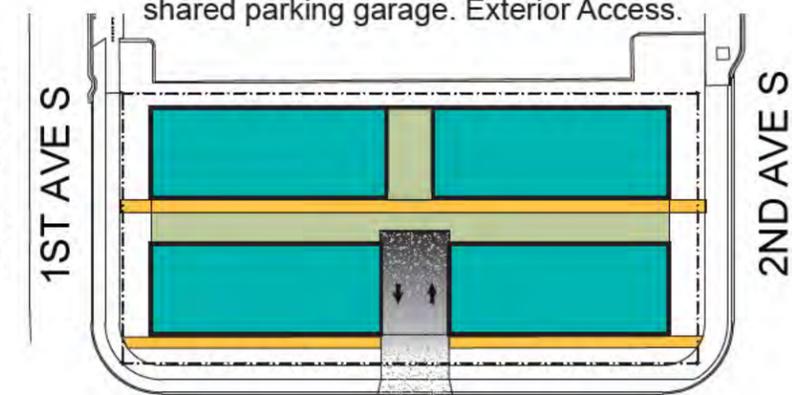


2ND ST S

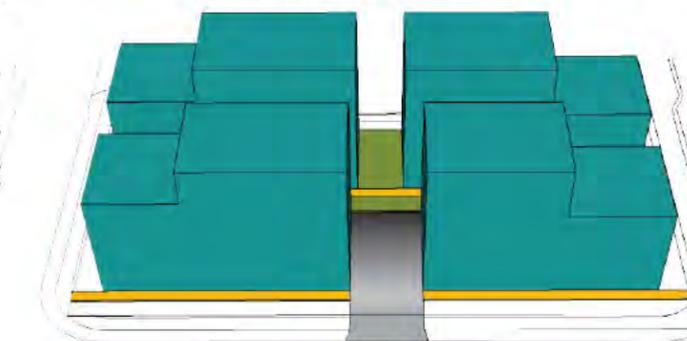


SCHEME THREE SELECTED

Single vehicular access point serving shared parking garage. Exterior Access.



2ND ST S

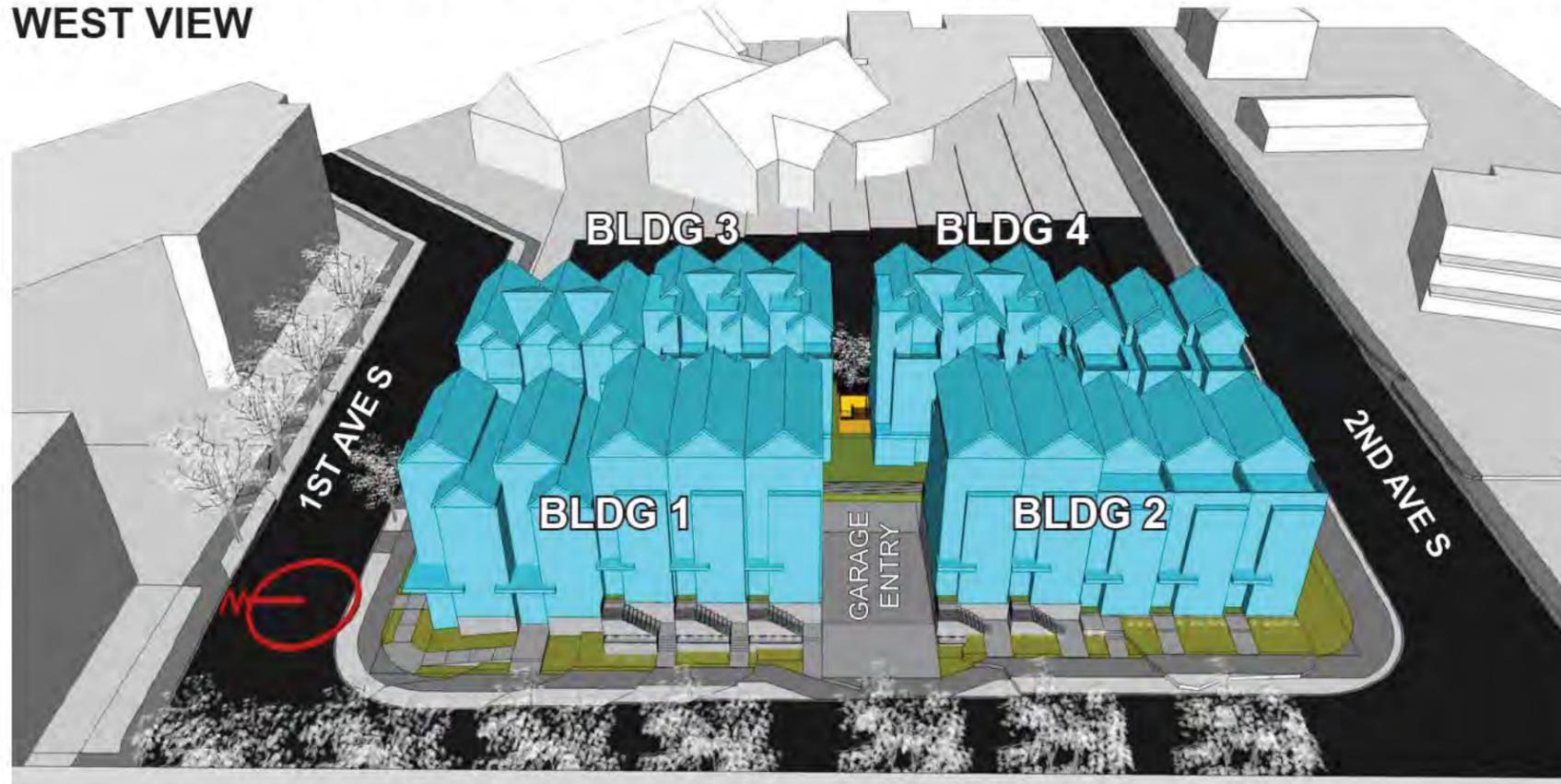


USE KEY

- RESIDENTIAL AREA
- PEDESTRIAN ACCESS
- PARKING & ACCESS
- LIGHT & ACCESS

ARCHITECTURAL MASSING: Scheme Three Preferred

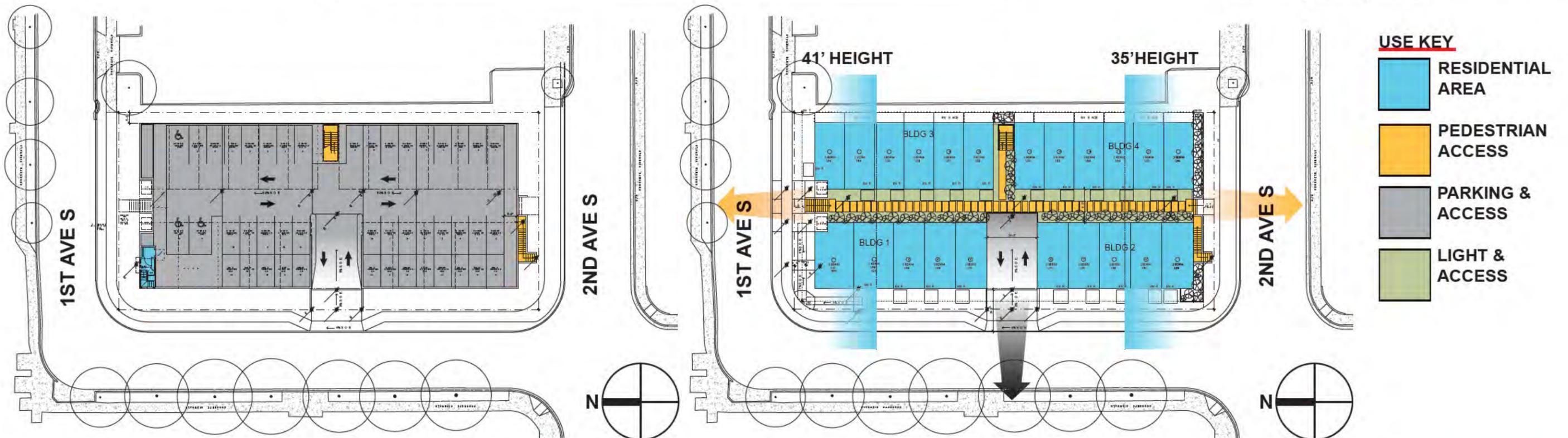
WEST VIEW



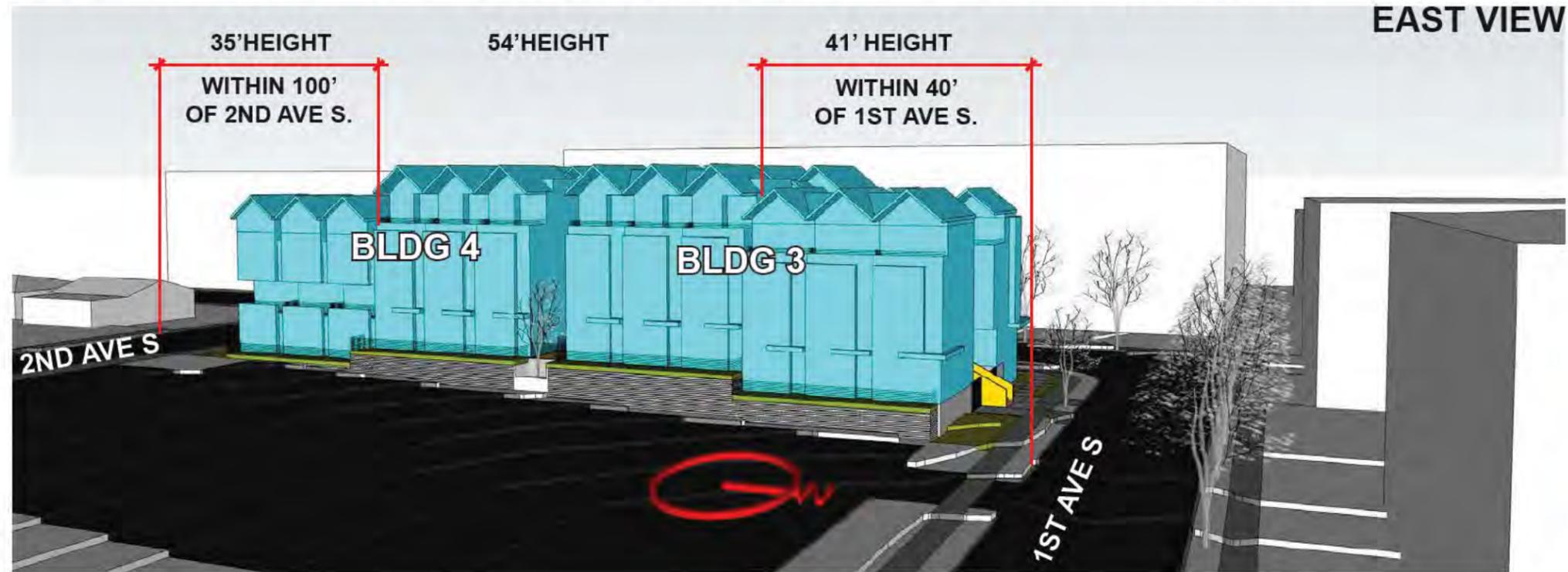
SCHEME THREE TRADITIONAL FORM

Opportunities:

- Zoning Code-compliant massing with no departures required
- Provide single curb cut and consolidated underground parking
- Through-site pedestrian access
- Maximizes development potential
- Modulated envelope provides interest at pedestrian level
- Opportunity to provide pedestrian friendly features
- Provide view and daylight opportunities at the central access corridors
- Integrated landscape and tree elements
- All facades are activated and engage the public right of way or interior pedestrian corridor
- Provides visually distinct units
- Pitched roofs take advantage of allowed height bonus
- Traditional gable roof forms provide visual transition between adjacent single family uses and larger multi-family developments.



ARCHITECTURAL MASSING: Scheme Three Preferred



EAST VIEW

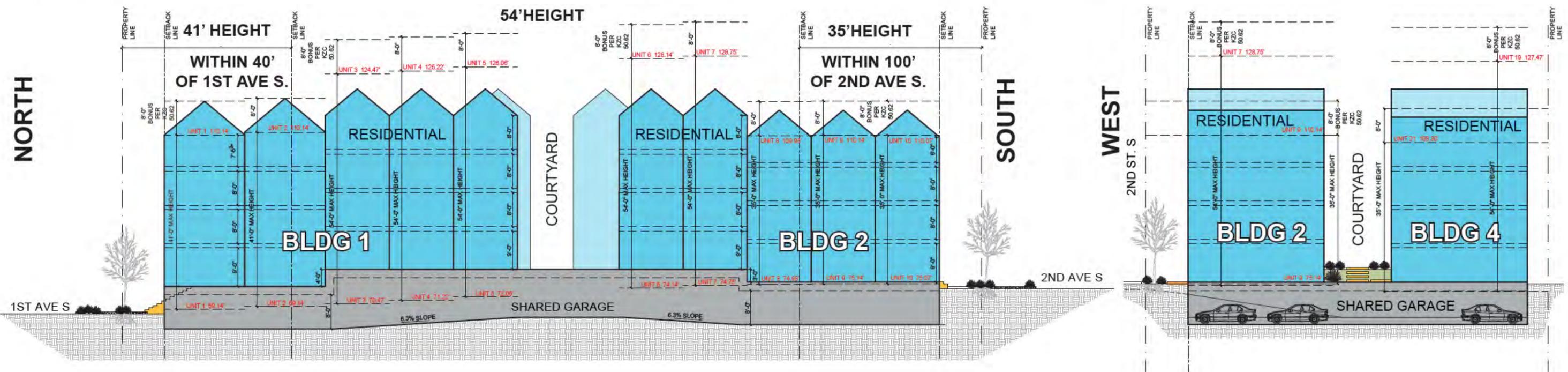
DESIGN NARRATIVE

This design provides residential character by reducing scale through traditional pitched roof forms, facade articulation, and pedestrian level details.

The traditional form allows for maximum development potential while creating opportunities for balconies, roof decks, and facade modulation.

USE KEY

- RESIDENTIAL AREA
- PARKING & ACCESS
- PEDESTRIAN ACCESS
- LIGHT & ACCESS



DESIGN CONCEPTS OVERVIEW: CDC Discussion Issues



PEDESTRIAN-ORIENTED ELEMENTS

Upper Story Activities: Each private entry faces pedestrian paths, while still providing privacy and separation through entry stoops and landscaping. Large bay windows in the living spaces and an upper story deck connect occupants with the sidewalk.

Lighting: Each pedestrian entry and path is lit with appropriate light fixtures, as well as the vehicular entrance to the parking garage. Fixtures are selected to provide interest and security without causing light pollution to adjacent homes or uses.

Blank Walls: each facade is treated with a variety of texture through material and massing changes. Fenestration is provided where feasible, and landscaping spills over to areas where programmatically required to be solid.

SCALE

Proportion of Buildings: The topography of the site and varying maximum height restriction results in three story buildings, with a stepped back fourth story composed of private deck space and pitched roofs to reduce the mass.

Inter-Building Relationship: The project is composed of four residential buildings separated for light and access.

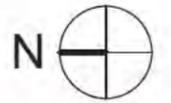
Garage Entry Location: The location of the garage entrance is central to the site to achieve safe sight-distance triangle for pedestrians and vehicles, while consuming a proportionally small length of the overall project, allowing greater area for landscape and unit entries.



FINAL CONCEPT

PROPOSED SITE PLAN

Scale 1" = 20'-0"



GROSS FLOOR AREA:

PARKING GARAGE -	13,902 SF
RESIDENTIAL -	
Garage:	275.5 SF
1st Floor:	10,017 SF
2nd Floor:	10,000 SF
3rd Floor:	10,000 SF
4th Floor:	6,450 SF
Total:	36,742.5 SF
Covered Decks:	1,661 SF
TOTAL:	38,403.5 SF

USE KEY

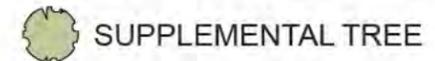
- RESIDENTIAL AREA
- PEDESTRIAN ACCESS
- PARKING & ACCESS
- LANDSCAPE ELEMENTS

LOT COVERAGE CALCULATION

Lot Size:	19,284 SF (.44 acre)	Allowed Lot Coverage:	100 %
Building Footprint:	13,772.5 SF		
Site Improvements:	1,343.5 SF		
Rear Decks:	915 SF		
Driveway:	1,108 SF		
Planters:	780 SF		
TOTAL:	17,919 SF	92.92%	

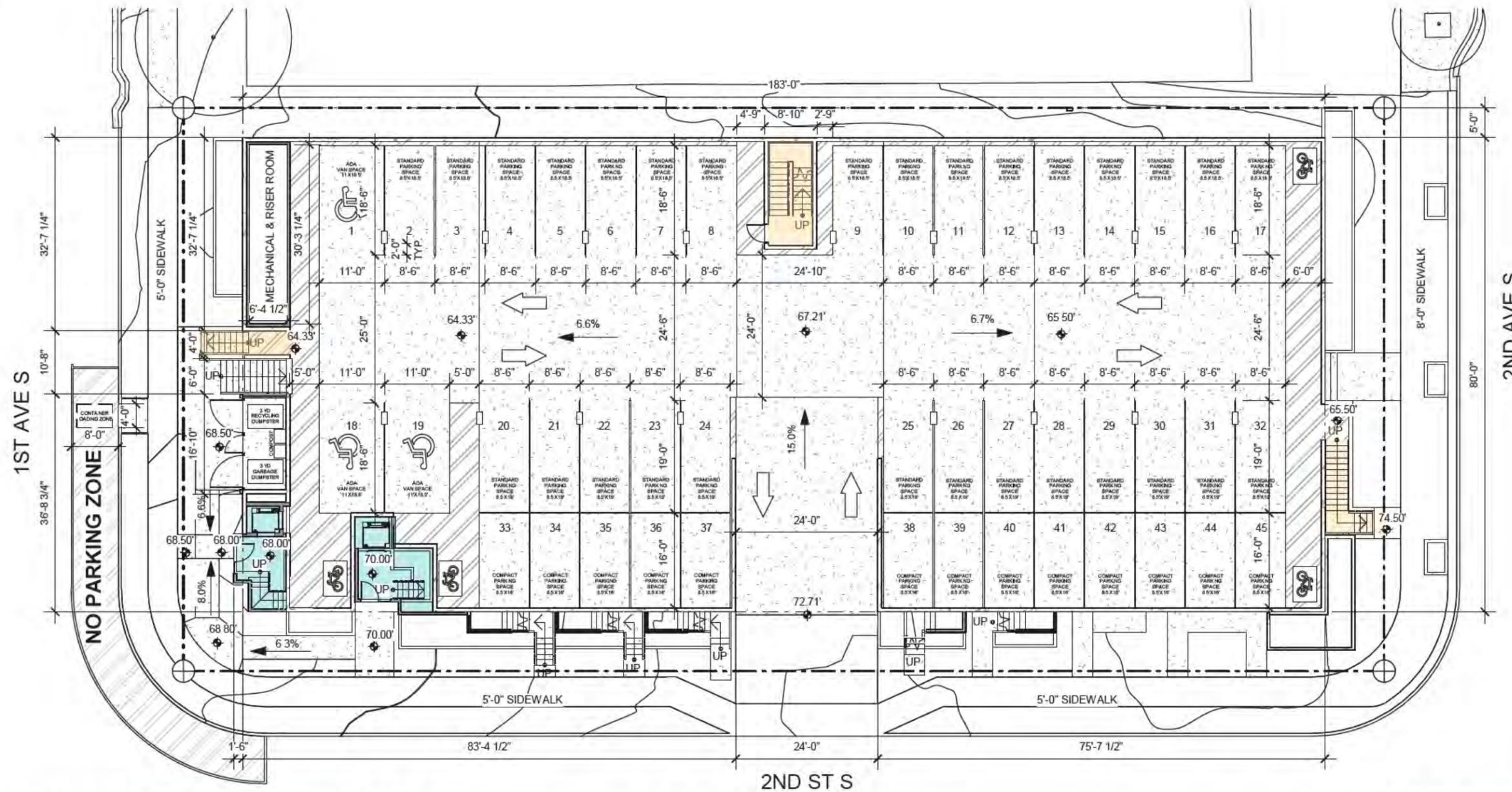
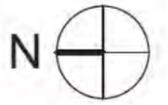
TREE DENSITY CALCULATION

Lot Size:	19,284 SF (.44 acre)	30 Tree Credits per acre
Required Tree Credits:	13.2	
Provided Tree Credits:	15.0	



GARAGE PLAN

Scale 1" = 20'-0"



PARKING MODIFICATION

Approved Parking Modification
 TRAN20-00266

USE KEY

- RESIDENTIAL AREA
- PEDESTRIAN ACCESS

PARKING CALCULATION per KZC50.32.080

Required 1.8 parking spaces per three (3) or more bedroom unit.
 Required: 40 parking spaces
 Provided: 41 parking spaces
 Guest Parking per KZC105.20.3 - 10% of total required parking
 Required: 4 guest parking spaces
 Provided: 4 guest parking spaces
 Total: 45 parking spaces

PARKING ASSIGNMENT

# of Units	Assigned Parking/ Unit	Total Spaces	Type
13	2	26	Tandem
6	2	12	Side-by-Side Singles
3	1	3	Single
22		41	Total Assigned Stalls
		4	Guest Stalls
		45	Total Parking Spaces

BICYCLE PARKING CALCULATION per KZC105.32

Required 1 bicycle space per 12 parking spaces.
 Required: 4
 Provided: 4

GARAGE SECTION



PARKING GARAGE ENTRANCE

The centrally located garage entrance is open to above primarily for vehicle clearance.

Four sconces have been added to the garage entrance to shed light, provide security and promote wayfinding.

The protective railing from the central pedestrian corridor has been reduced to a half wall of solid parapet with an open railing above. This adds levity to the entrance and a thinner profile for the deck floating above the entrance. These features are to make the driveway lid feel more like a roof deck above rather than an overbearing solid concrete plinth.

Steel planters are set adjacent to the railing which will add material interest to the entrance. Vertical plantings will add a softness and movement to the space. This also acts as a buffer for resident entries from vehicle headlights.



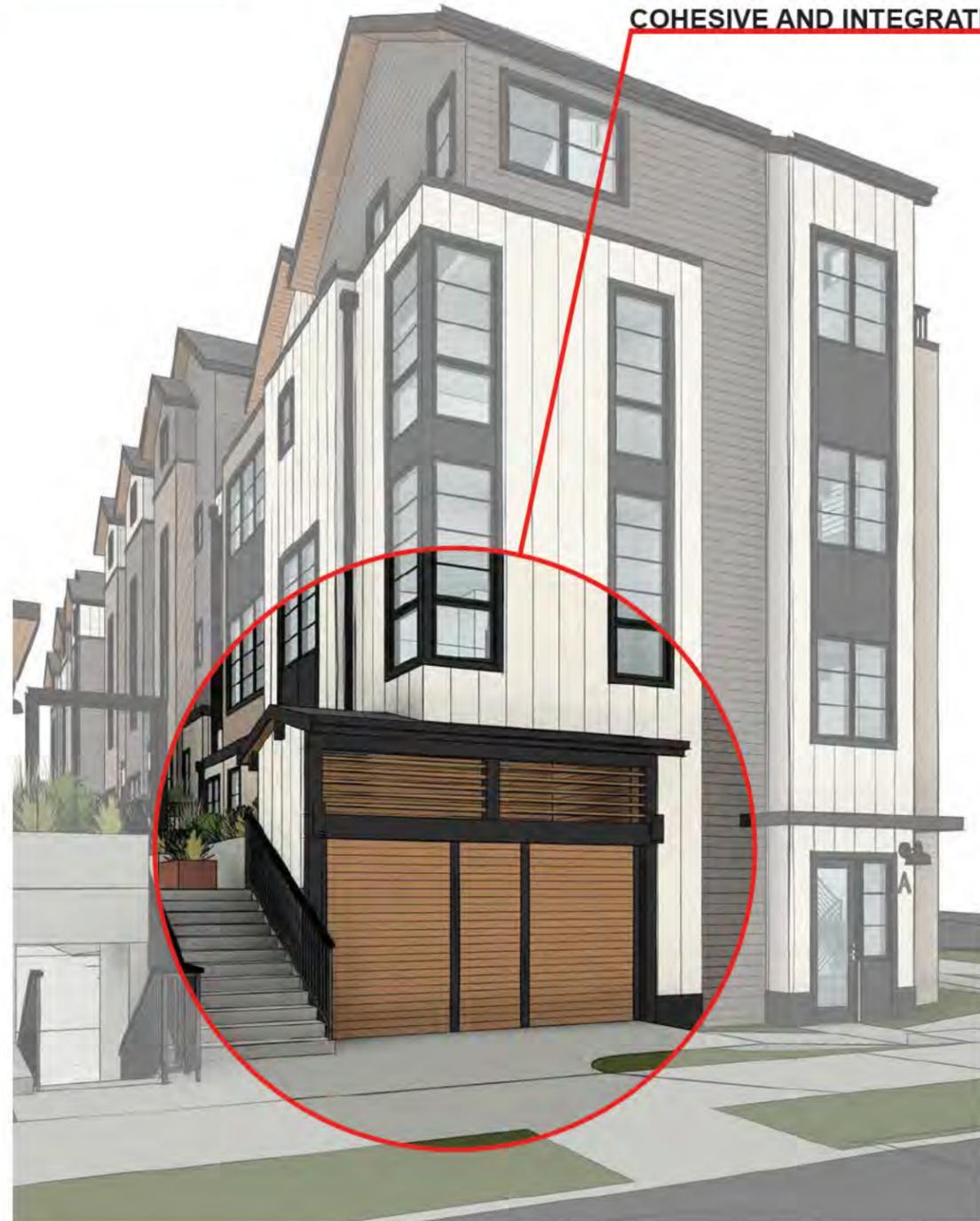
LIGHTING AND MATERIAL INTEREST

PROJECT DETAILS: Garbage Enclosure

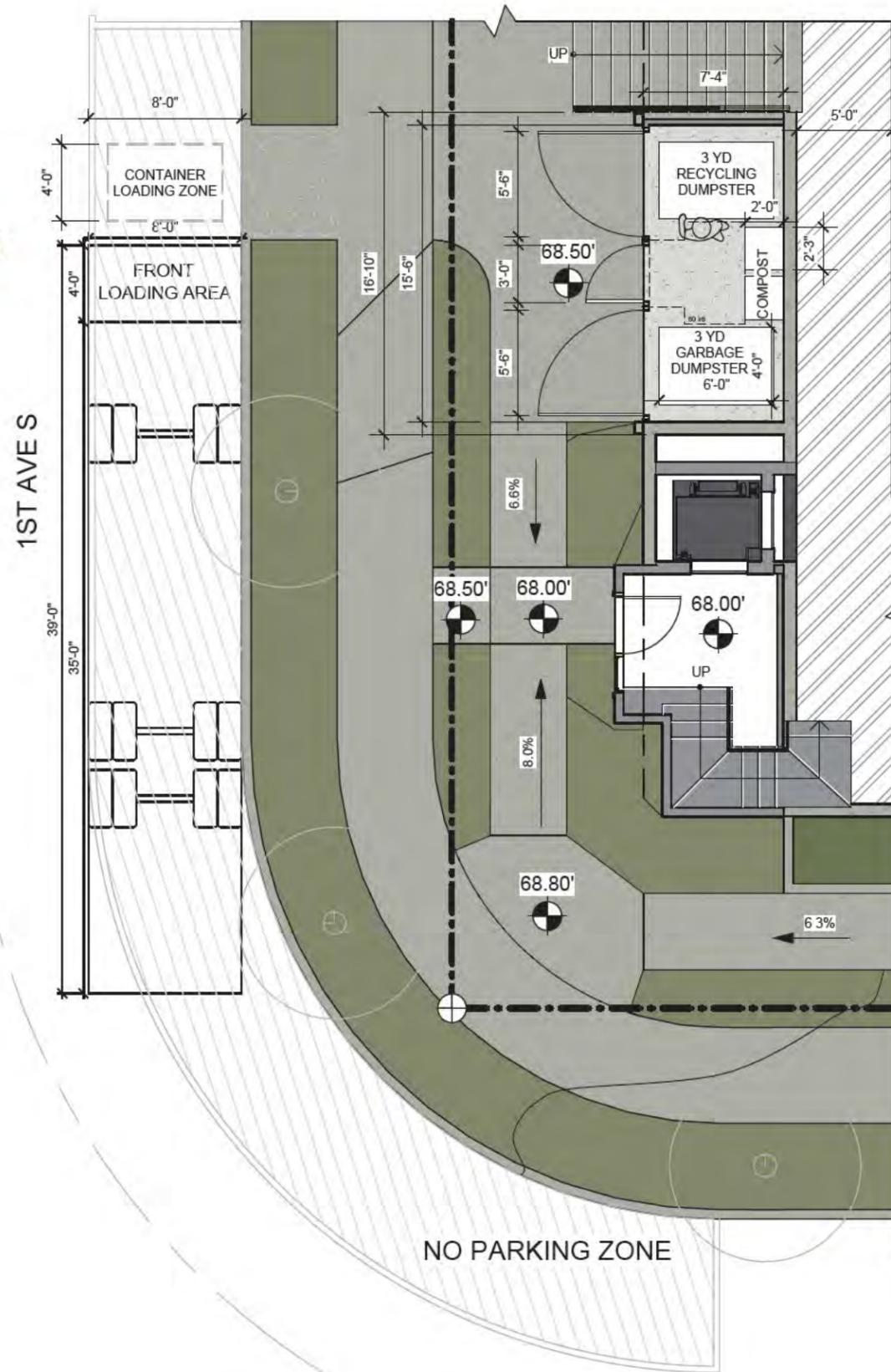


FORM AND MATERIALS

Cedar clad steel doors add interest and detail to the enclosure space. The doors are mounted on casters for ease of service and a single accessible man door is provided for residents to access the enclosure. Open wood slatting above is provided for ventilation and two (2) outdoor light fixtures are present for light and security.



COHESIVE AND INTEGRATED STRUCTURE



Scale 1/8" = 1'-0"



ENCLOSURE REQUIREMENTS

Garbage and Recycling enclosure sizing was calculated using CoK Policy G-9.

Provided: 3yd Garbage
 3yd Recycle
 2 Compost

To be serviced once (1x) per week.

The garbage enclosure was designed to be encompassed within the buildable setback in order to minimize intrusion into yards and pedestrian areas.

Waste Management has approved the location and service method for the truck to approach from 1st Ave S traveling east and the driver to manually roll dumpsters from the enclosure to the container loading zone.

There must be a no parking zone located at the NW corner to provide a safe loading space for the driver to stop.

This reduces the need for a permanent staging area on or off site. When the dumpsters are not serviced they are not visible from the street.

APPROVED UNDER

TRAN20-00320

ABE SITE PLAN AND CALCULATION

Scale 1" = 20'-0"



ABE CALCULATION

KZC 115.59 Height Regulations Method for calculation used is Option 1: Midpoint elevation of rectangle segment length.

KZC 115.59.2 Attached but Independent Building Units - When a building or structure contains townhouses or other attached but otherwise independent building units, the ABE is calculated separately for each unit.

BUILDING 1 HEIGHTS

Unit A - ABE: 69.07' Max: 110.07'
 Unit B - ABE: 69.67' Max: 110.67'
 Unit C - ABE: 70.47' Max: 124.47'
 Unit D - ABE: 71.25' Max: 125.25'
 Unit E - ABE: 72.09' Max: 126.09'

BUILDING 2 HEIGHTS

Unit F - ABE: 74.22' Max: 128.22'
 Unit G - ABE: 74.83' Max: 128.83'
 Unit H - ABE: 75.12' Max: 110.12'
 Unit I - ABE: 75.23' Max: 110.23'
 Unit J - ABE: 75.08' Max: 110.08'

BUILDING 3 HEIGHTS

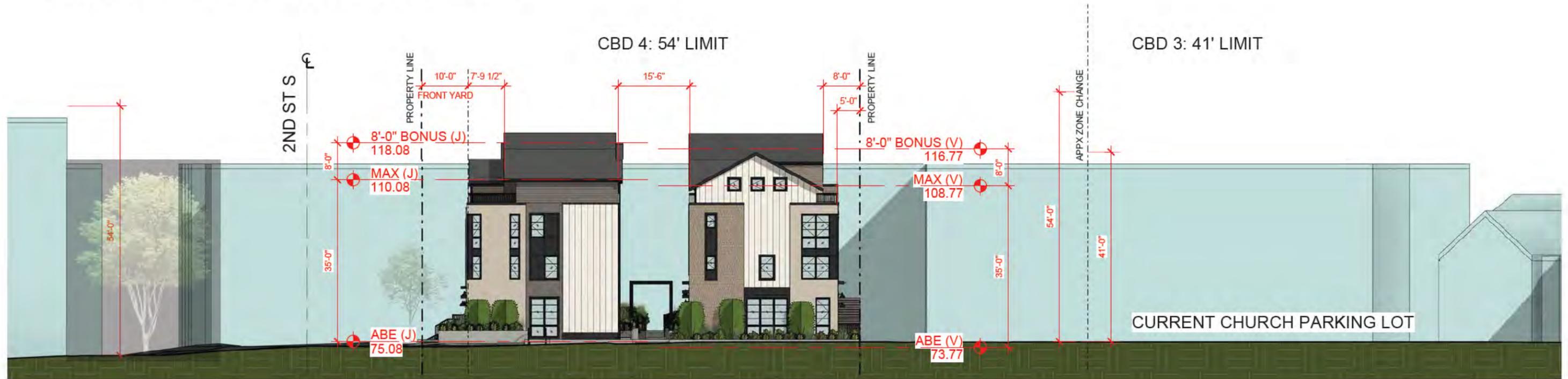
Unit K - ABE: 69.31' Max: 110.31'
 Unit L - ABE: 69.67' Max: 110.67'
 Unit M - ABE: 70.05' Max: 124.05'
 Unit N - ABE: 70.49' Max: 124.49'
 Unit O - ABE: 70.87' Max: 124.87'
 Unit P - ABE: 71.35' Max: 125.35'

BUILDING 4 HEIGHTS

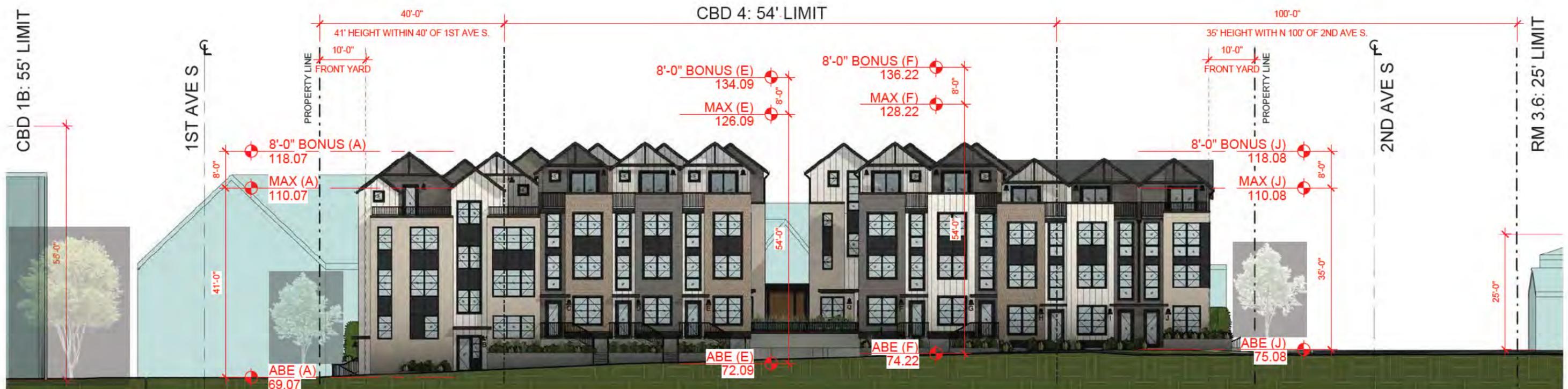
Unit Q - ABE: 72.19' Max: 126.19'
 Unit R - ABE: 73.04' Max: 127.04'
 Unit S - ABE: 73.60' Max: 127.60'
 Unit T - ABE: 73.33' Max: 108.33'
 Unit U - ABE: 73.32' Max: 108.32'
 Unit V - ABE: 73.77' Max: 108.77'

SITE ELEVATIONS

PROPOSED ELEVATIONS WITH CURRENT EAST PROPERTY



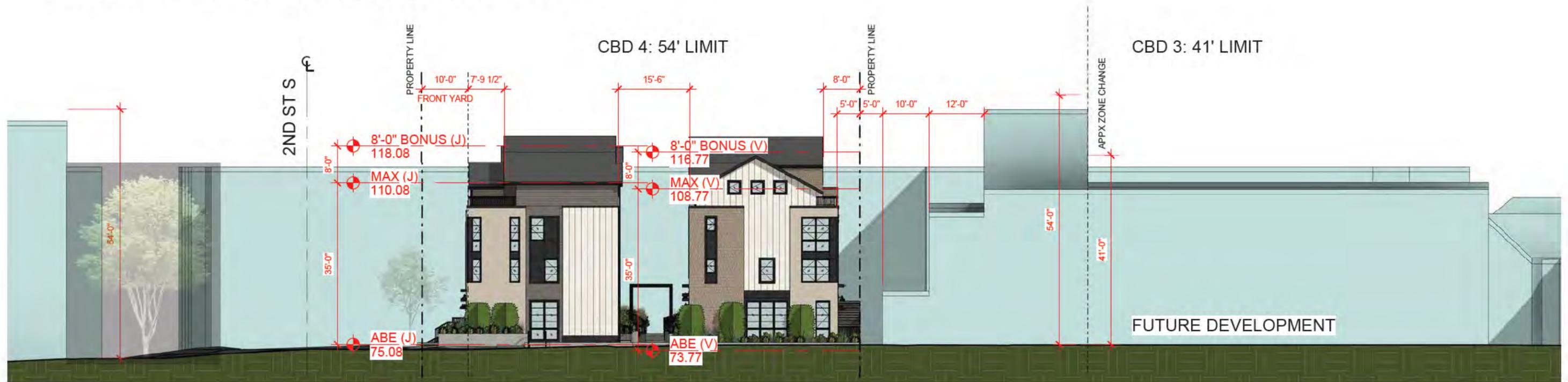
SOUTH ELEVATION



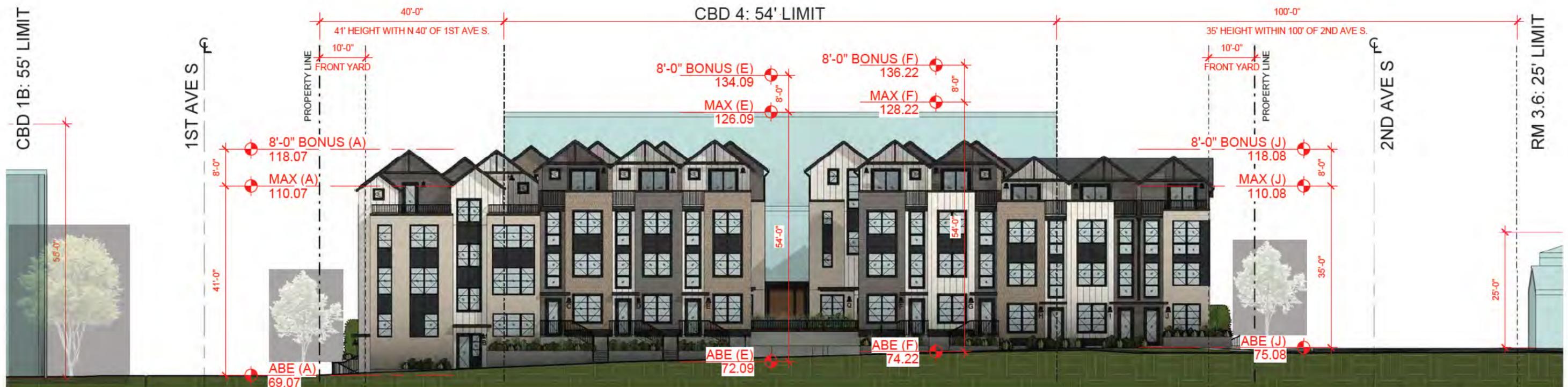
WEST ELEVATION

SITE ELEVATIONS

PROPOSED ELEVATIONS WITH FUTURE DEVELOPED EAST PROPERTY



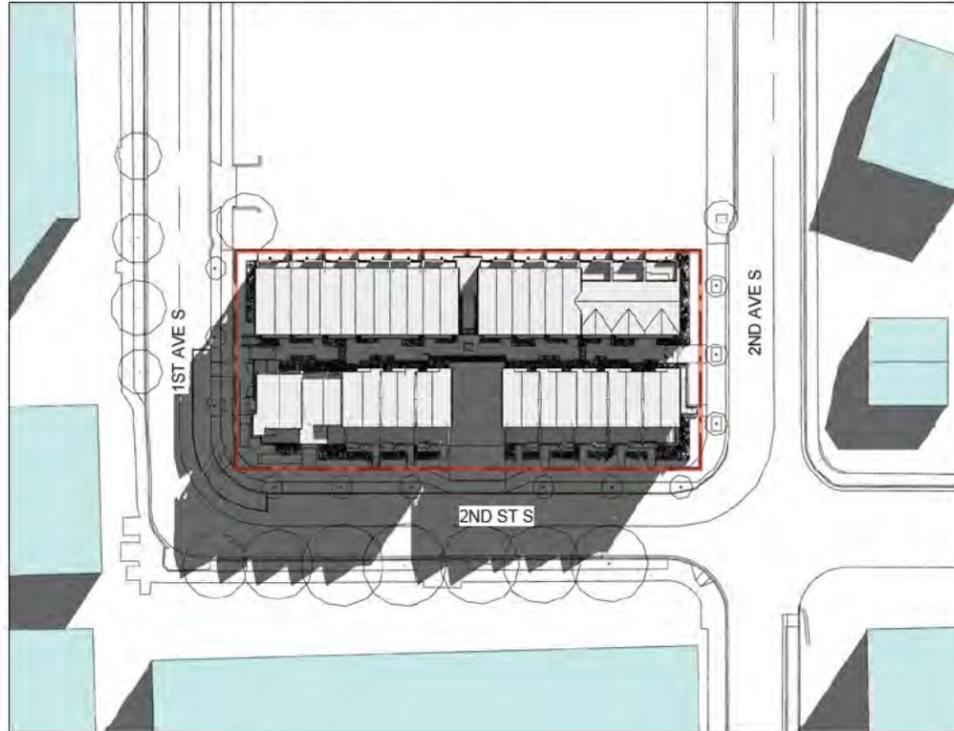
SOUTH ELEVATION



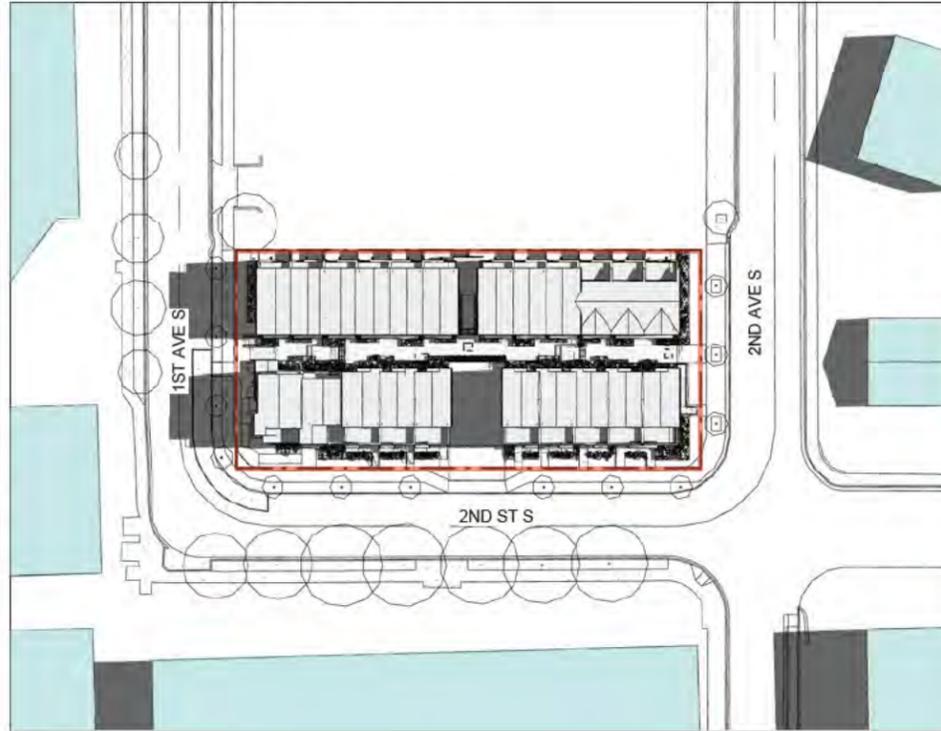
WEST ELEVATION

SOLAR STUDY - Spring/Fall Equinox

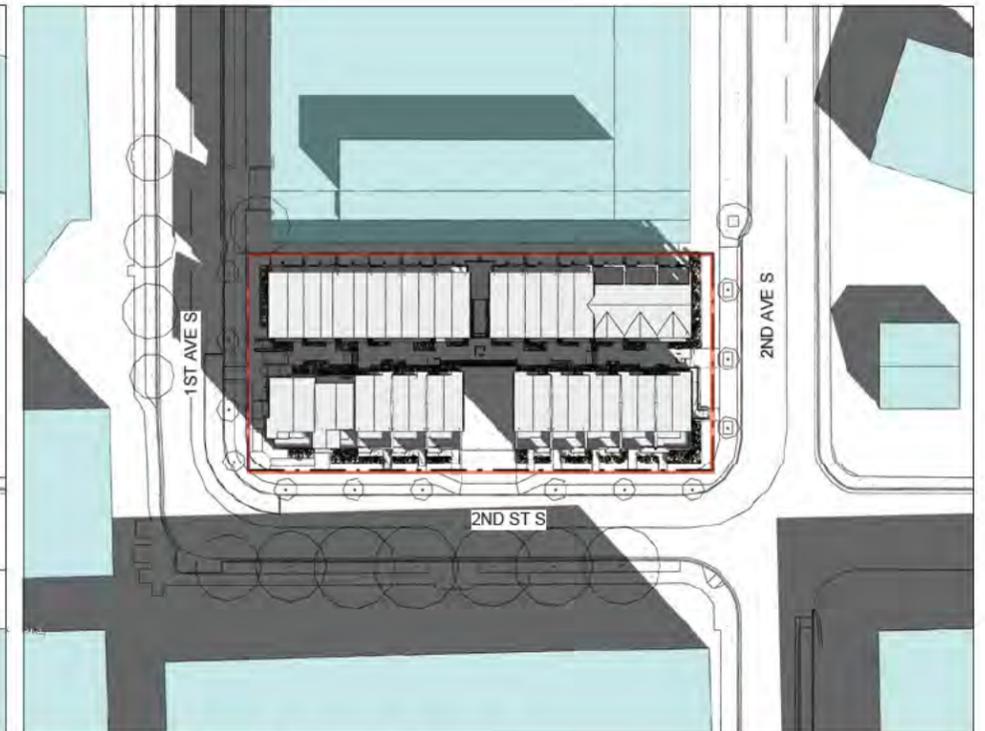
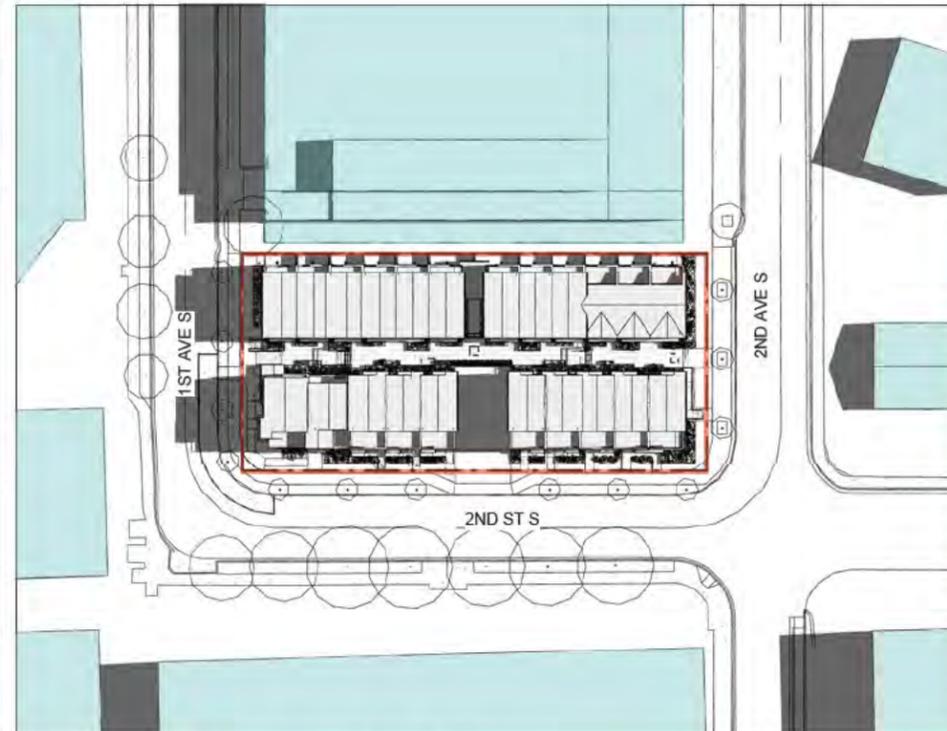
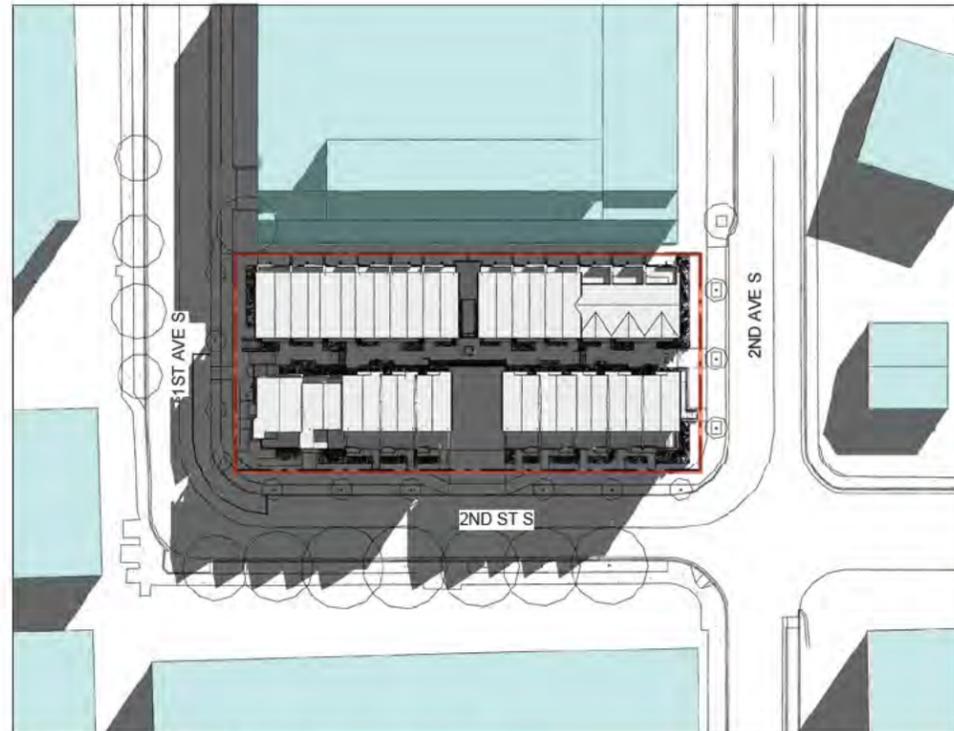
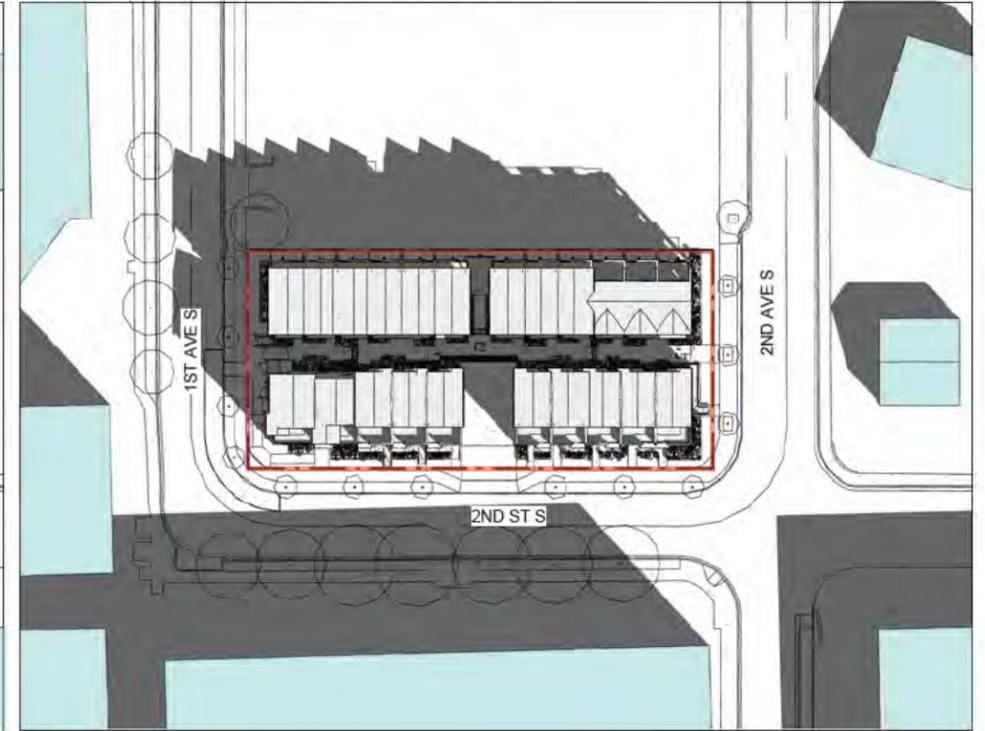
EXISTING: MARCH / SEPT. 21 - 9AM



EXISTING: MARCH / SEPT. 21 - 12PM



EXISTING: MARCH / SEPT. 21 - 3PM



FUTURE: MARCH / SEPT. 21 - 9AM

FUTURE: MARCH / SEPT. 21 - 12PM

FUTURE: MARCH / SEPT. 21 - 3PM

LANDSCAPE PLAN



N  RENDERED LANDSCAPE PLAN
SCALE: NTS

LANDSCAPE DETAILS

PLANT SCHEDULE

TREES	BOTANICAL / COMMON NAME	SIZE	DROUGHT TOLERANT	NATIVE	QTY
	<i>Pyrus calleryana</i> 'Redspire' / Redspire Pear Street Tree	2" Cal	No	No	6
	<i>Quercus Ilex</i> / Holly Oak Street Tree	2" Cal	No	No	3
	<i>Tilia cordata</i> 'DeGroot' / DeGroot Littleleaf Linden Street Tree	2" Cal	No	No	3

SUPPLEMENTAL TREES	BOTANICAL / COMMON NAME	SIZE	DROUGHT TOLERANT	NATIVE	QTY
	<i>Carpinus betulus</i> 'Fastigiata' / Pyramidal European Hornbeam	2" Cal	No	No	15

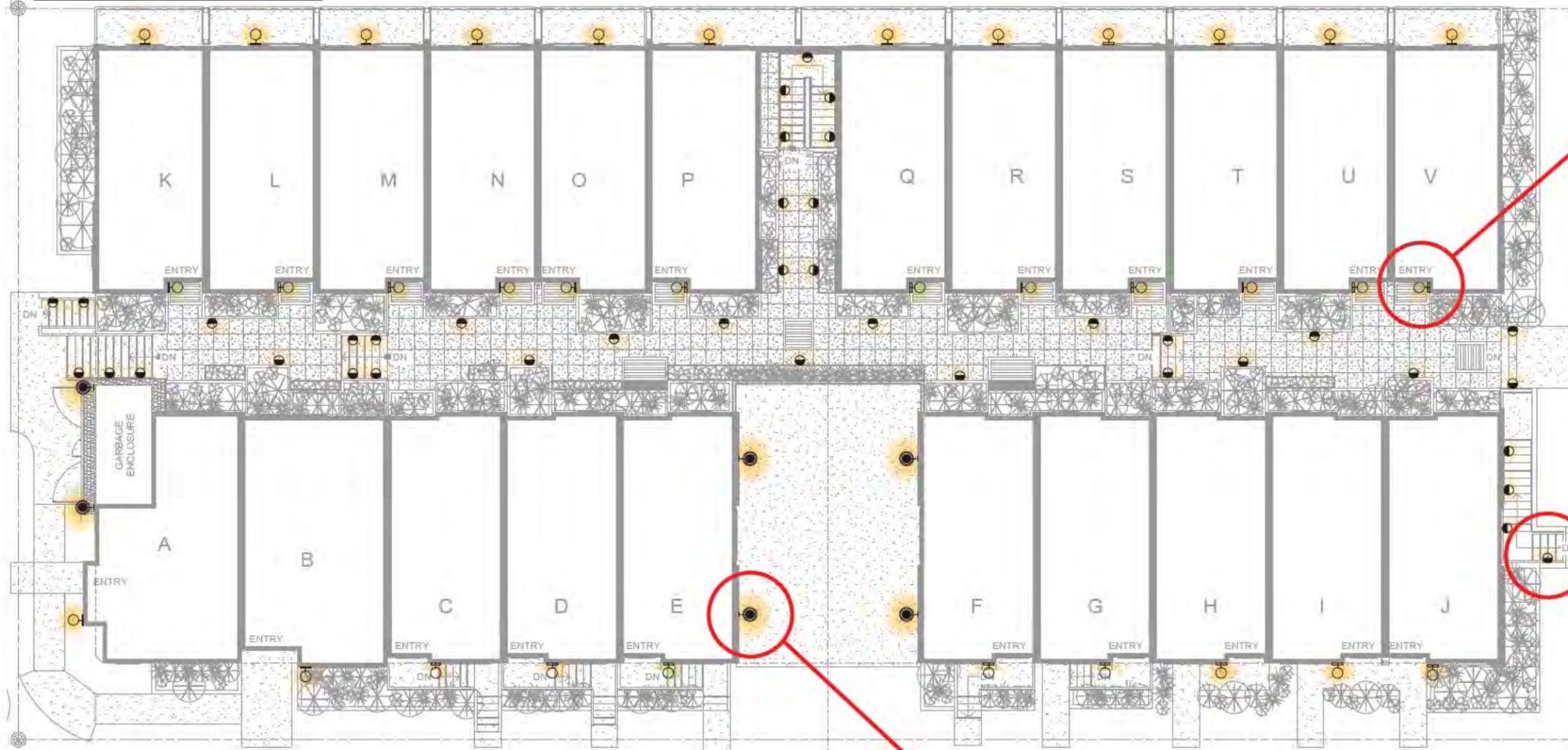
GROUND COVERS	BOTANICAL / COMMON NAME	SIZE	DROUGHT TOLERANT	NATIVE	SPACING	QTY
	<i>Lysimachia nummularia</i> 'Aurea' / Golden Creeping Jenny	4" pot	Yes	No	18" o.c.	178
	<i>Rubus calycinoides</i> 'Emerald Carpet' / Creeping Raspberry	4" pot	Yes	No	18" o.c.	734
	<i>Thymus praecox</i> 'Elfin' / Elfin Creeping Thyme	4" pot	Yes	No	18" o.c.	153
	<i>Vinca minor</i> 'Bowles Blue' / Dwarf Periwinkle	4" pot	Yes	No	18" o.c.	100

PLANT SCHEDULE

SHRUBS	BOTANICAL / COMMON NAME	SIZE	DROUGHT TOLERANT	NATIVE	QTY
	<i>Acorus gramineus</i> 'Ogon' / Golden Variegated Sweetflag	1 gal	Yes	No	129
	<i>Berberis thunbergii</i> 'Helmond Pillar' / Columnar Barberry	2 gal	Yes	No	11
	<i>Bergenia cordifolia</i> 'Winterglut' / Winterglow Bergenia	1 gal	Yes	No	75
	<i>Calamagrostis x acutiflora</i> 'Karl Foerster' / Feather Reed Grass	2 gal	Yes	No	47
	<i>Calluna vulgaris</i> 'Wickwar Flame' / Wickwar Flame Heather	1 gal	Yes	No	36
	<i>Carex testacea</i> / Orange Sedge	1 gal	Yes	No	71
	<i>Cyrtomium fortunei</i> / Japanese Holly Fern	1 gal	Yes	No	43
	<i>Equisetum hyemale</i> / Horsetail Reed Grass	1 gal	Yes	No	50
	<i>Evonymus japonicus</i> 'Greenspire' / Greenspire Upright Euonymus	20" Ht min	Yes	No	14
	<i>Festuca glauca</i> / Blue Fescue	1 gal	Yes	No	48
	<i>Hakonechloa macra</i> 'Aureola' / Golden Variegated Hakonechloa	1 gal	Yes	No	62
	<i>Hydrangea quercifolia</i> / Oakleaf Hydrangea	3 gal	Yes	No	6
	<i>Leucothoe fontanesiana</i> 'Rainbow' / Rainbow Leucothoe	2 gal	Yes	No	30
	<i>Lonicera pileata</i> 'Moss Green' / Moss Green Honeysuckle	2 gal	Yes	No	89
	<i>Mahonia x media</i> 'Charity' / Mahonia	2 gal	Yes	No	23
	<i>Nandina domestica</i> 'Gulf Stream' TM / Heavenly Bamboo	2 gal	Yes	No	41
	<i>Ophiopogon planiscapus</i> 'Nigrescens' / Black Mondo Grass	1 gal	Yes	No	76
	<i>Osmanthus heterophyllus</i> 'Goshiki' / Goshiki Holly	2 gal	Yes	No	41
	<i>Sarcococca ruscifolia</i> / Fragrant Sarcococca	2 gal	Yes	No	95

LIGHTING PLAN

CODE	SYMBOL	DESCRIPTION
LL6		OUTDOOR STEP + PATH LIGHT
SCE1		8.5" OUTDOOR SCONCE
SCE3		OUTDOOR SCONCE



DL DestinationLighting.
 Customer Service: 1-800-653-6556 or cs@destinationlighting.com
 M-F: 7am-5pm & Sunday: 11am-4pm (PST)

Barn Light Gooseneck Arm Black 8.5-Inch Wide



DL DestinationLighting.
 Customer Service: 1-800-653-6556 or cs@destinationlighting.com
 M-F: 7am-5pm & Sunday: 11am-4pm (PST)

Hinkley Low Voltage Black 1-Light Nuvi deck: 15222BK



DL DestinationLighting.
 Customer Service: 1-800-653-6556 or cs@destinationlighting.com
 M-F: 7am-5pm & Sunday: 11am-4pm (PST)

Quorum Lighting Cylinder Noir Outdoor Wall Light



FLOOR PLANS: 1st Floor Plans

Scale 1/16" = 1'-0"



GROSS FLOOR AREA:

PARKING GARAGE -
 1,3902 SF

RESIDENTIAL -

Garage:	275.5 SF
1st Floor:	10,017 SF
2nd Floor:	10,000 SF
3rd Floor:	10,000 SF
4th Floor:	6,450 SF
Total:	36,742.5 SF
Covered Decks:	1,661 SF
TOTAL:	38,403.5 SF

- UNIT A:
2,192 SF
- UNIT B:
2,362 SF + 60 SF Deck
- UNIT C-J:
1,671 SF + 69 SF Deck
- UNIT K-V:
1,607 SF + 81 SF Deck

USE KEY

- RESIDENTIAL AREA
- PEDESTRIAN ACCESS
- PRIVATE AMENITY

FLOOR PLANS: 2nd Floor Plans

Scale 1/16" = 1'-0"



GROSS FLOOR AREA:

PARKING GARAGE -
 1,3902 SF

RESIDENTIAL -

Garage:	275.5 SF
1st Floor:	10,017 SF
2nd Floor:	10,000 SF
3rd Floor:	10,000 SF
4th Floor:	6,450 SF
Total:	36,742.5 SF
Covered Decks:	1,661 SF
TOTAL:	38,403.5 SF

- UNIT A:
2,192 SF
- UNIT B:
2,362 SF + 60 SF Deck
- UNIT C-J:
1,671 SF + 69 SF Deck
- UNIT K-V:
1,607 SF + 81 SF Deck

USE KEY

	RESIDENTIAL AREA
	PEDESTRIAN ACCESS

FLOOR PLANS: 3rd Floor Plans

Scale 1/16" = 1'-0"



GROSS FLOOR AREA:

PARKING GARAGE -
 1,3902 SF

RESIDENTIAL -

Garage:	275.5 SF
1st Floor:	10,017 SF
2nd Floor:	10,000 SF
3rd Floor:	10,000 SF
4th Floor:	6,450 SF
Total:	36,742.5 SF
Covered Decks:	1,661 SF
TOTAL:	38,403.5 SF

UNIT A:
 2,192 SF

UNIT B:
 2,362 SF + 60 SF Deck

UNIT C-J:
 1,671 SF + 69 SF Deck

UNIT K-V:
 1,607 SF + 81 SF Deck

USE KEY

RESIDENTIAL AREA

FLOOR PLANS: 4th Floor Plans

Scale 1/16" = 1'-0"



GROSS FLOOR AREA:

PARKING GARAGE -
 1,3902 SF

RESIDENTIAL -

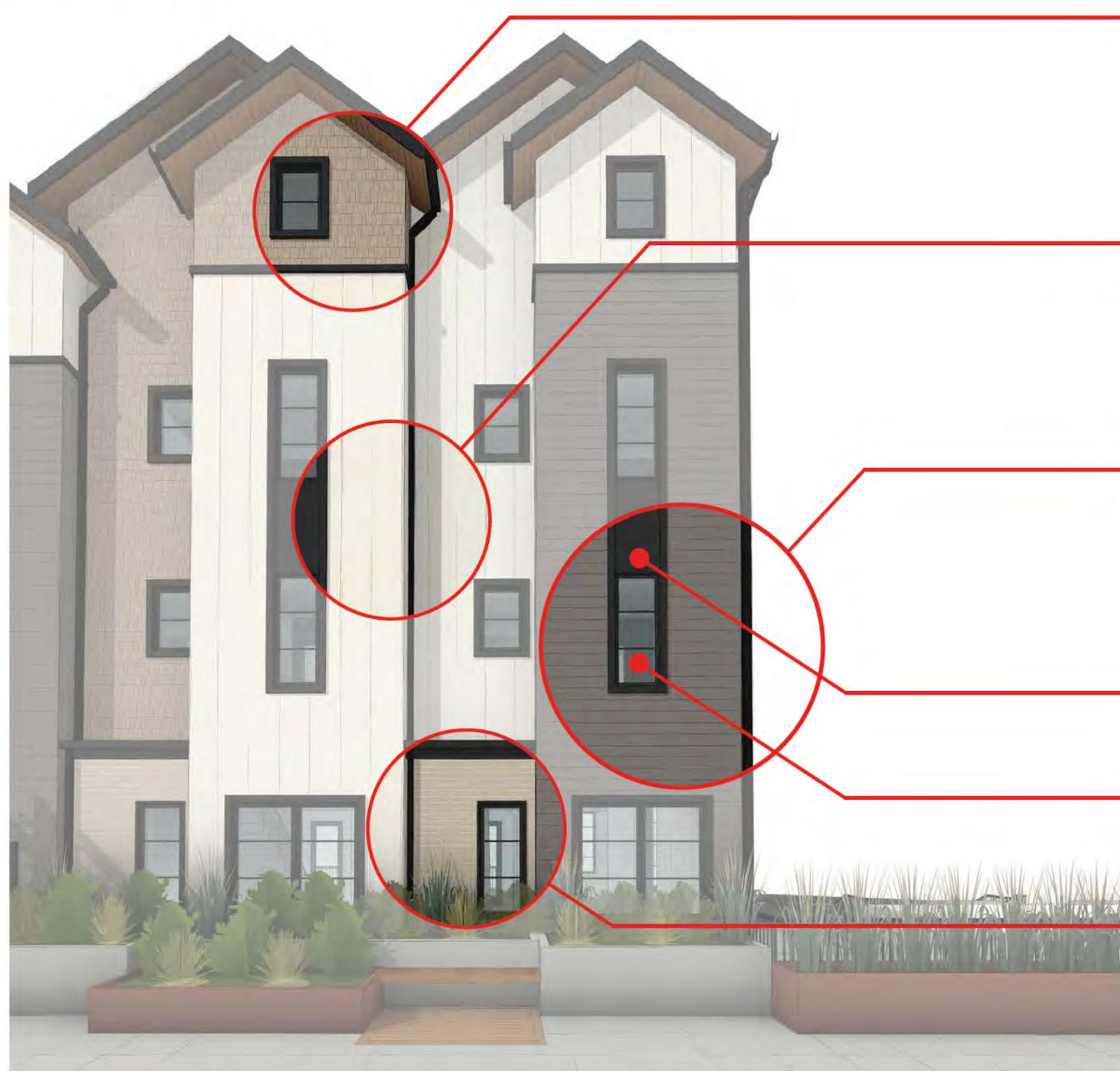
Garage:	275.5 SF
1st Floor:	10,017 SF
2nd Floor:	10,000 SF
3rd Floor:	10,000 SF
4th Floor:	6,450 SF
Total:	36,742.5 SF
Covered Decks:	1,661 SF
TOTAL:	38,403.5 SF

- UNIT A:
2,192 SF
- UNIT B:
2,362 SF + 60 SF Deck
- UNIT C-J:
1,671 SF + 69 SF Deck
- UNIT K-V:
1,607 SF + 81 SF Deck

USE KEY

	RESIDENTIAL AREA
	PRIVATE AMENITY

MATERIALS



FIBER CEMENT SHINGLE
- STONE LION



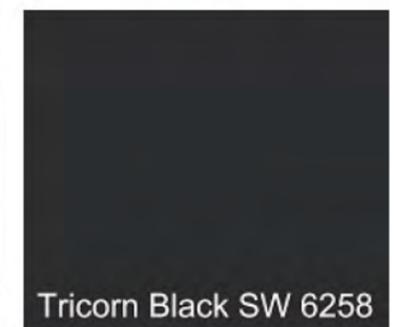
FIBER CEMENT PANEL
- BOARD AND BATTEN
- GREEK VILLA



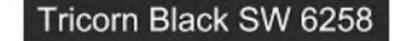
FIBER CEMENT LAP
- GAUNTLET GRAY



FIBER CEMENT PANEL
- TRICORN BLACK



PLYGEM WINDOW
- BLACK FRAME
- HORIZONTAL GRILLE



FACE BRICK
- MISSION STYLE
- ASPEN COLOR
- WHITE GROUT



ELEVATIONS: Building 1 WEST

Scale 1/8" = 1'-0"

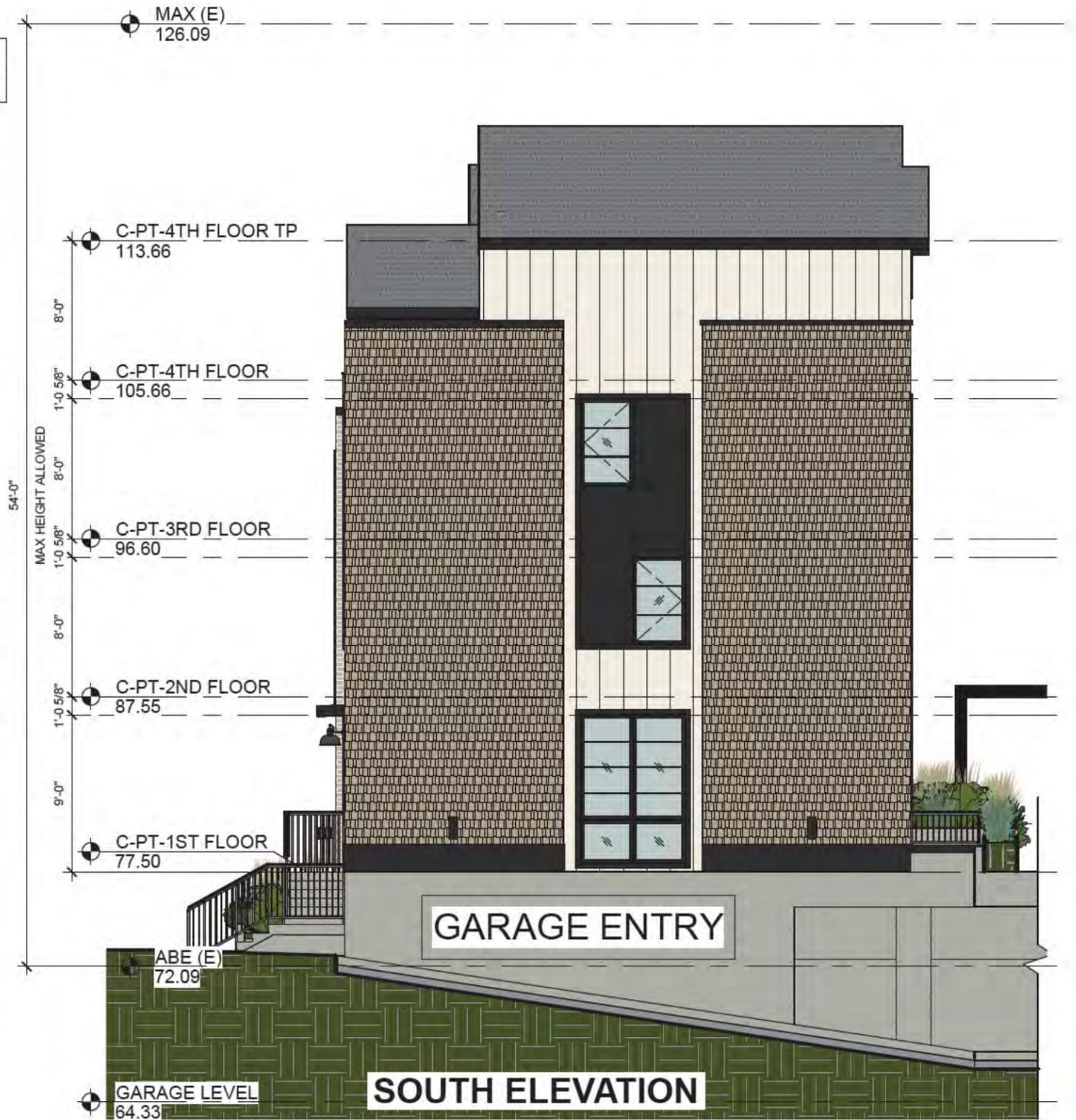


ELEVATIONS: Building 1 NORTH & SOUTH

A

E

Scale 1/8" = 1'-0"



ELEVATIONS: Building 1 EAST

Scale 1/8" = 1'-0"



ELEVATIONS: Building 2 WEST

Scale 1/8" = 1'-0"



ELEVATIONS: Building 2 NORTH & SOUTH

Scale 1/8" = 1'-0"



ELEVATIONS: Building 3 WEST



ELEVATIONS: Building 3 NORTH & SOUTH

Scale 1/8" = 1'-0"



ELEVATIONS: Building 3 EAST

P O N M L K

Scale 1/8" = 1'-0"



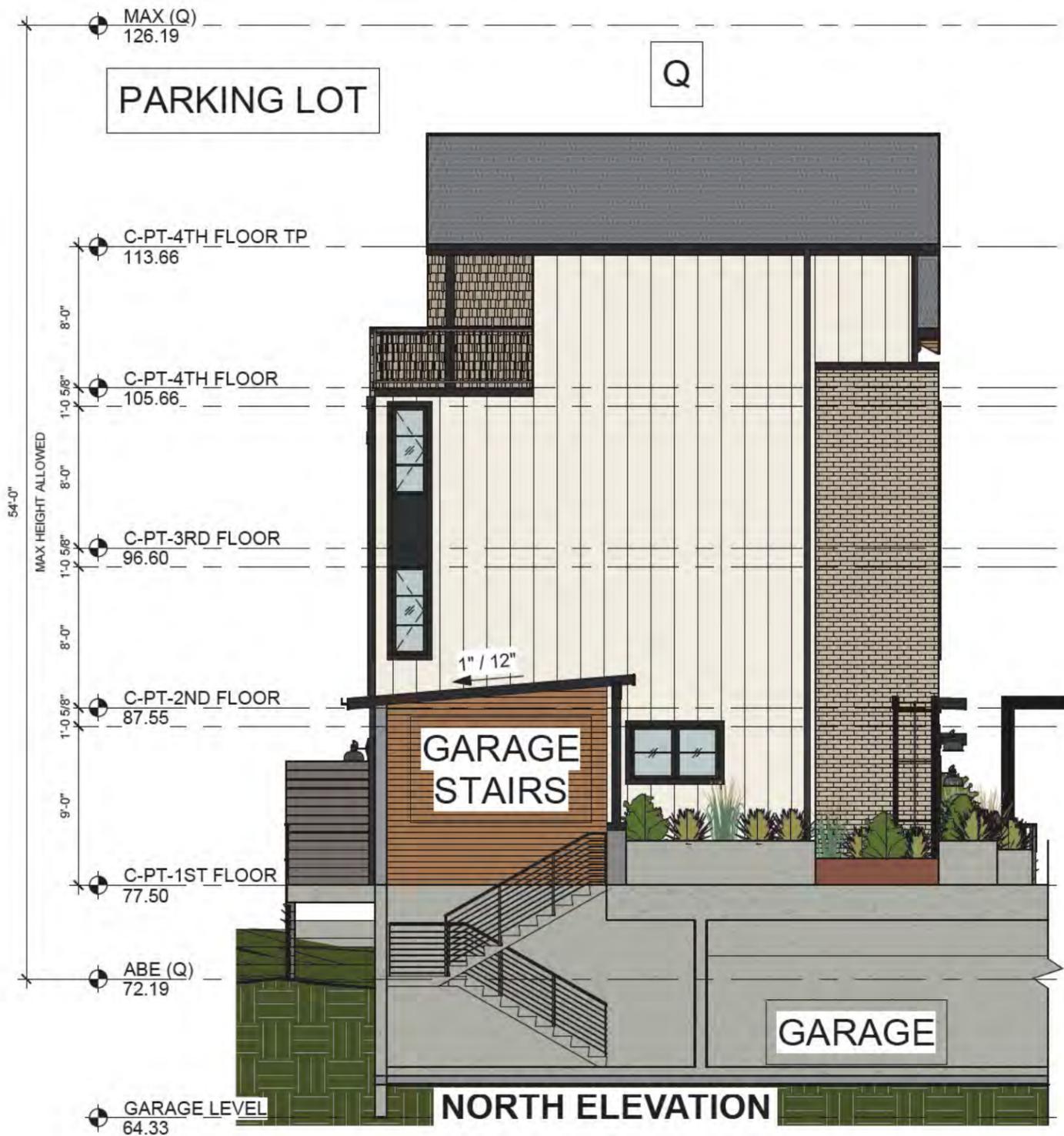
ELEVATIONS: Building 4 WEST

Scale 1/8" = 1'-0"



ELEVATIONS: Building 4 NORTH & SOUTH

Scale 1/8" = 1'-0"



ELEVATIONS: Building 4 EAST

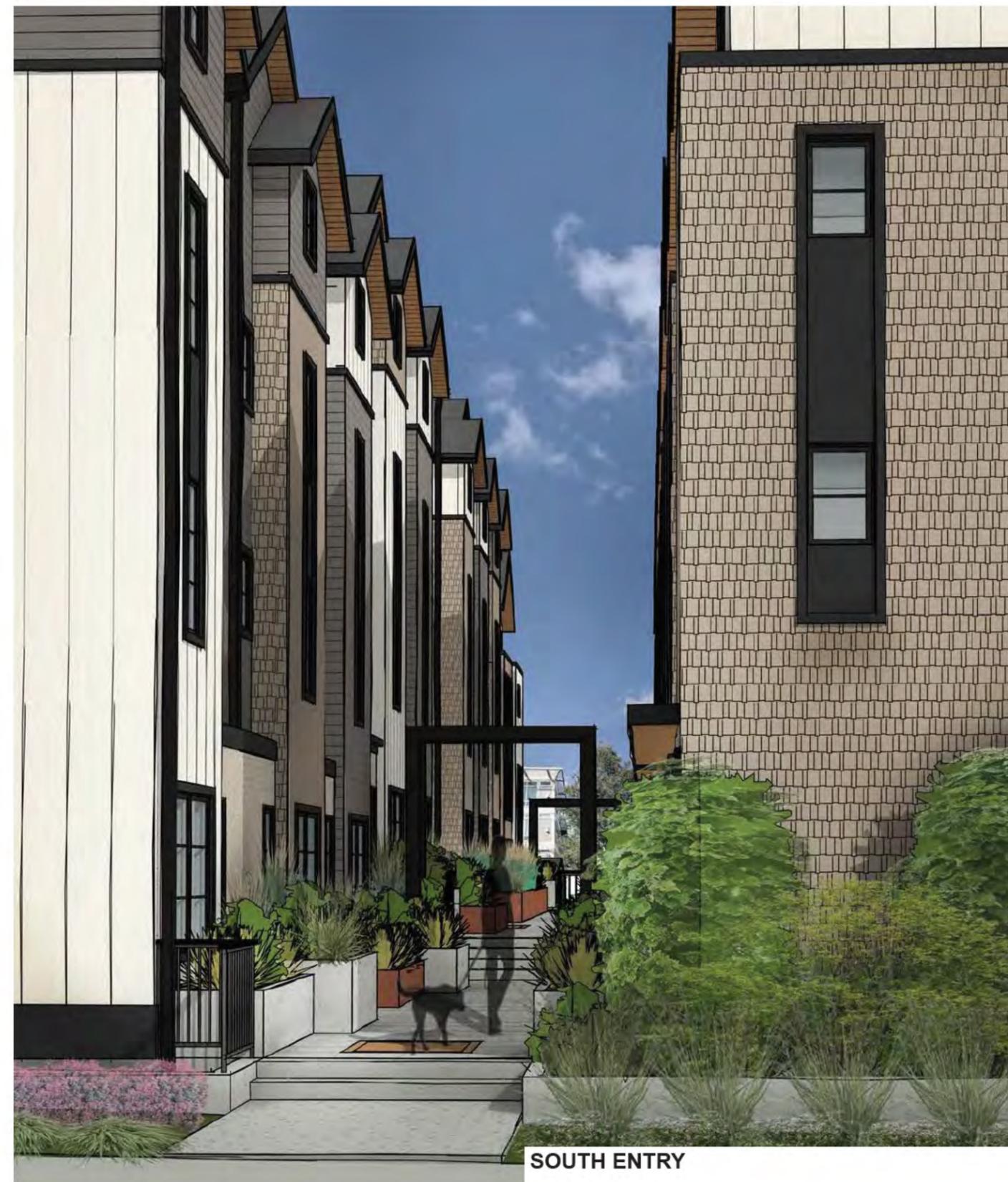
Scale 1/8" = 1'-0"



RENDERING: Corridor



RENDERING: Corridor North & South Entry



RENDERING: West and East Elevation



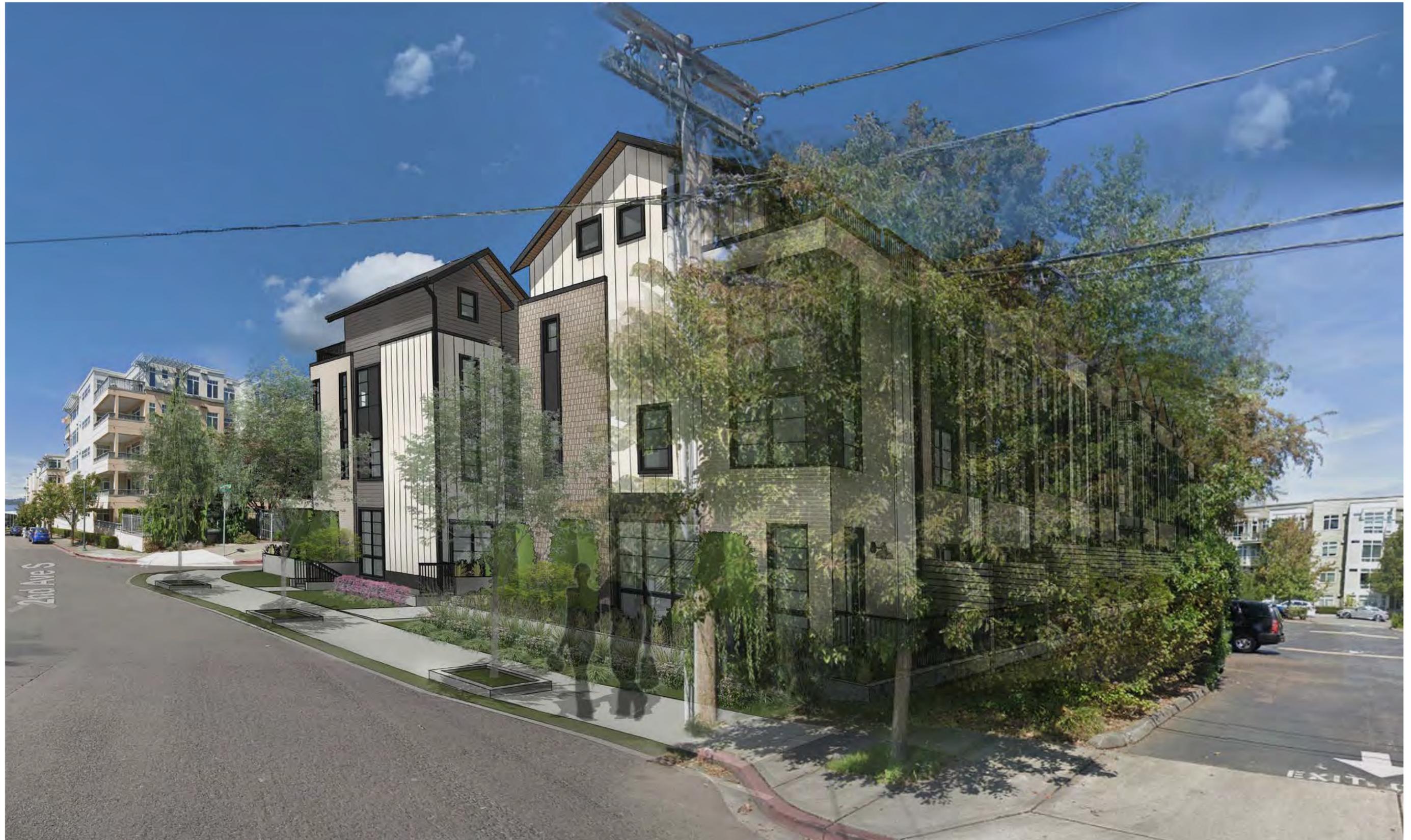
RENDERING: 1st Ave S, NW Corner



RENDERING: 1st Ave S, NE Corner



RENDERING: 2nd Ave S, SE Corner



RENDERING: 2nd Ave S, SW Corner





CITY OF KIRKLAND
Planning and Building Department
123 5th Avenue, Kirkland, WA 98033
425.587.3600 ~ www.kirklandwa.gov

DEVELOPMENT STANDARDS LIST

File: DRV20-00473

ZONING CODE STANDARDS

20.10-60.187 Required Yards for Multi-family Development: The side yard may be reduced to zero feet if the side of the dwelling unit is attached to a dwelling unit on an adjoining lot. If one side of a dwelling unit is so attached and the opposite side is not, the side that is not attached must provide a minimum side yard of five feet. The rear yard may be reduced to zero feet if the rear of the dwelling unit is attached to a dwelling unit on an adjoining lot.

92.35 Prohibited Materials In Design Districts. If in a design district the following building materials are prohibited or limited in use: mirrored glass or reflective materials, corrugated fiberglass, chain link fencing, metal siding, concrete block, backlit awnings. Water spigots are required along building facades along sidewalks for cleaning and plant watering. Commercial buildings with more than one tenant shall install a cornerstone or plaque.

95.51.2.a Required Landscaping. All required landscaping shall be maintained throughout the life of the development. The applicant shall submit an agreement to the city to be recorded with King County which will perpetually maintain required landscaping. Prior to issuance of a certificate of occupancy, the proponent shall provide a final as-built landscape plan and an agreement to maintain and replace all landscaping that is required by the City.

95.50 Tree Installation Standards. All supplemental trees to be planted shall conform to the Kirkland Plant List. All installation standards shall conform to Kirkland Zoning Code Section 95.45.

95.52 Prohibited Vegetation. Plants listed as prohibited in the Kirkland Plant List shall not be planted in the City.

100.25 Sign Permits. Separate sign permit(s) are required. In JBD and CBD cabinet signs are prohibited.

105.18 Pedestrian Walkways. All uses, except single family dwelling units and duplex structures, must provide pedestrian walkways designed to minimize walking distances from the building entrance to the right of way and adjacent transit facilities, pedestrian connections to adjacent properties, between primary entrances of all uses on the subject property, through parking lots and parking garages to building entrances. Easements may be required. In design districts through block pathways or other pedestrian improvements may be required. See also Plates 34 in Chapter 180.

105.32 Bicycle Parking. All uses, except single family dwelling units and duplex structures with 6 or more vehicle parking spaces must provide covered bicycle parking within 50 feet of an entrance to the building at a ratio of one bicycle space for each twelve motor vehicle parking spaces. Check with Planner to determine the number of bike racks required and location.

105.18 Entrance Walkways. All uses, except single family dwellings and duplex structures, must provide pedestrian walkways between the principal entrances to all businesses, uses, and/or buildings on the subject property.

105.18.2 Walkway Standards. Pedestrian walkways must be at least 5' wide; must be distinguishable from traffic lanes by pavement texture or elevation; must have adequate lighting for security and safety. Lights must be non-glare and mounted no more than 20' above the

ground.

105.19 Public Pedestrian Walkways. The height of solid (blocking visibility) fences along pedestrian pathways that are not directly adjacent a public or private street right-of-way shall be limited to 42 inches unless otherwise approved by the Planning or Public Works Directors. All new building structures shall be setback a minimum of five feet from any pedestrian access right-of-way, tract, or easement that is not directly adjacent a public or private street right-of-way. If in a design district, see section and Plate 34 for through block pathways standards.

105.20 Required Parking. @ parking spaces are required for this use.

105.58 Parking Lot Locations in Design Districts. See section for standards unique to each district.

105.65 Compact Parking Stalls. Up to 50% of the number of parking spaces may be designated for compact cars.

105.60.2 Parking Area Driveways. Driveways which are not driving aisles within a parking area shall be a minimum width of 24 feet.

105.60.3 Wheelstops. Parking areas must be constructed so that car wheels are kept at least 2' from pedestrian and landscape areas.

105.60.4 Parking Lot Walkways. All parking lots which contain more than 25 stalls must include pedestrian walkways through the parking lot to the main building entrance or a central location. Lots with more than 25,000 sq. ft. of paved area must provide pedestrian routes for every 3 aisles to the main entrance.

105.77 Parking Area Curbing. All parking areas and driveways, for uses other than detached dwelling units must be surrounded by a 6" high vertical concrete curb.

105.96 Drive Through Facilities. See section for design criteria for approving drive through facilities.

110.52 Sidewalks and Public Improvements in Design Districts. See section, Plate 34 and public works approved plans manual for sidewalk standards and decorative lighting design applicable to design districts.

110.60.5 Street Trees. All trees planted in the right-of-way must be approved as to species by the City. All trees must be two inches in diameter at the time of planting as measured using the standards of the American Association of Nurserymen with a canopy that starts at least six feet above finished grade and does not obstruct any adjoining sidewalks or driving lanes.

115.25 Work Hours. It is a violation of this Code to engage in any development activity or to operate any heavy equipment before 7:00 am. or after 8:00 pm Monday through Friday, or before 9:00 am or after 6:00 pm Saturday. No development activity or use of heavy equipment may occur on Sundays or on the following holidays: New Year's Day, Memorial Day, Independence Day, Labor Day, Thanksgiving, and Christmas Day. The applicant will be required to comply with these regulations and any violation of this section will result in enforcement action, unless written permission is obtained from the Planning official.

115.40 Fence Location. Fences over 6 feet in height may not be located in a required setback yard. A detached dwelling unit abutting a neighborhood access or collector street may not have a fence over 3.5 feet in height within the required front yard. No fence may be placed within a high waterline setback yard or within any portion of a north or south property line yard, which is coincident with the high waterline setback yard.

A detached dwelling unit may not have a fence over 3.5 feet in height within 3 feet of the property line abutting a principal or minor arterial except where the abutting arterial contains an improved landscape strip between the street and sidewalk. The area between the fence and property line shall be planted with vegetation and maintained by the property owner.

115.45 Garbage and Recycling Placement and Screening. For uses other than detached dwelling units, duplexes, moorage facilities, parks, and construction sites, all garbage receptacles and dumpsters must be setback from property lines, located outside landscape buffers, and screened from view from the street, adjacent properties and pedestrian walkways or parks by a

solid sight-obscuring enclosure.

115.75.2 Fill Material. All materials used as fill must be non-dissolving and non-decomposing. Fill material must not contain organic or inorganic material that would be detrimental to the water quality, or existing habitat, or create any other significant adverse impacts to the environment.

115.90 Calculating Lot Coverage. The total area of all structures and pavement and any other impervious surface on the subject property is limited to a maximum percentage of total lot area. See the Use Zone charts for maximum lot coverage percentages allowed. Section 115.90 lists exceptions to total lot coverage calculations. See Section 115.90 for a more detailed explanation of these exceptions.

115.95 Noise Standards. The City of Kirkland adopts by reference the Maximum Environmental Noise Levels established pursuant to the Noise Control Act of 1974, RCW 70.107. See Chapter 173-60 WAC. Any noise, which injures, endangers the comfort, repose, health or safety of persons, or in any way renders persons insecure in life, or in the use of property is a violation of this Code.

115.115 Required Setback Yards. This section establishes what structures, improvements and activities may be within required setback yards as established for each use in each zone.

115.115.3.g Rockeries and Retaining Walls. Rockeries and retaining walls are limited to a maximum height of four feet in a required yard unless certain modification criteria in this section are met. The combined height of fences and retaining walls within five feet of each other in a required yard is limited to a maximum height of 6 feet, unless certain modification criteria in this section are met.

115.115.3.n Covered Entry Porches. In residential zones, covered entry porches on dwelling units may be located within 13 feet of the front property line if certain criteria in this section are met. This incentive is not effective within the disapproval jurisdiction of the Houghton Community Council.

115.115.3.p HVAC and Similar Equipment: These may be placed no closer than five feet of a side or rear property line, and shall not be located within a required front yard; provided, that HVAC equipment may be located in a storage shed approved pursuant to subsection (3)(m) of this section or a garage approved pursuant to subsection (3)(o)(2) of this section. All HVAC equipment shall be baffled, shielded, enclosed, or placed on the property in a manner that will ensure compliance with the noise provisions of KZC 115.95.

115.115.5.b Driveway Setbacks. For attached and stacked dwelling units in residential zones, driveways shall have a minimum 5' setback from all property lines except for the portion of any driveway, which connects with an adjacent street. Vehicle parking areas shall have a minimum 20-foot setback from all front property lines and meet the minimum required setbacks from all other property lines for the use.

115.120 Rooftop Appurtenance Screening. New or replacement appurtenances on existing buildings shall be surrounded by a solid screening enclosure equal in height to the appurtenance. New construction shall screen rooftop appurtenances by incorporating them in to the roof form.

115.135 Sight Distance at Intersection. Areas around all intersections, including the entrance of driveways onto streets, must be kept clear of sight obstruction as described in this section.

Prior to issuance of a grading or building permit:

95.30(4) Tree Protection Techniques. A description and location of tree protection measures during construction for trees to be retained must be shown on demolition and grading plans.

95.34 Tree Protection. Prior to development activity or initiating tree removal on the site, vegetated areas and individual trees to be preserved shall be protected from potentially damaging activities. Protection measures for trees to be retained shall include (1) placing no construction material or equipment within the protected area of any tree to be retained; (2) providing a visible

temporary protective chain link fence at least 6 feet in height around the protected area of retained trees or groups of trees until the Planning Official authorizes their removal; (3) installing visible signs spaced no further apart than 15 feet along the protective fence stating "Tree Protection Area, Entrance Prohibited" with the City code enforcement phone number; (4) prohibiting excavation or compaction of earth or other damaging activities within the barriers unless approved by the Planning Official and supervised by a qualified professional; and (5) ensuring that approved landscaping in a protected zone shall be done with light machinery or by hand.

27.06.030 Park Impact Fees. New residential units are required to pay park impact fees prior to issuance of a building permit. Please see KMC 27.06 for the current rate. Exemptions and/or credits may apply pursuant to KMC 27.06.050 and KMC 27.06.060. If a property contains an existing unit to be removed, a "credit" for that unit shall apply to the first building permit of the subdivision.

Prior to occupancy:

95.51.2.a Required Landscaping. All required landscaping shall be maintained throughout the life of the development. The applicant shall submit an agreement to the city to be recorded with King County which will perpetually maintain required landscaping. Prior to issuance of a certificate of occupancy, the proponent shall provide a final as-built landscape plan and an agreement to maintain and replace all landscaping that is required by the City

110.60.5 Landscape Maintenance Agreement. The owner of the subject property shall sign a landscape maintenance agreement, in a form acceptable to the City Attorney, to run with the subject property to maintain landscaping within the landscape strip and landscape island portions of the right-of-way (see Attachment @). It is a violation to pave or cover the landscape strip with impervious material or to park motor vehicles on this strip.

110.60.6 Mailboxes. Mailboxes shall be installed in the development in a location approved by the Postal Service and the Planning Official. The applicant shall, to the maximum extent possible, group mailboxes for units or uses in the development.

110.75 Bonds. The City may require or permit a bond to ensure compliance with any of the requirements of the Required Public Improvements chapter.



DEVELOPMENT STANDARDS

DRV20-00473

FIRE DEPARTMENT

FIRE DEPARTMENT COMMENTS

Contact: Todd Anderson at 425-587-3639 or tanderson@kirklandwa.gov.

ACCESS

All three schemes as shown provide adequate FD Access. (No portion of the exterior can be greater than 150' from a FD Access road).

FIRE FLOW

Fire flow requirement for this project is 2000 gpm; the closest node to the property has a fire flow of 1500 gpm.

HYDRANTS

Existing Hydrants are adequate for the proposed project.

FIRE SPRINKLERS

If the parking garage is shared, the system in the garage shall be a full NFPA 13, and the units shall be protected with a 13R system. ***If the units are constructed as attached but independent units (townhouse) then each individual unit will require its own 13R system***

A dedicated sprinkler riser room is required, and it shall be placed on an exterior wall. The underground line shall run from the outside directly up into the riser room (meaning, it shall not run under the slab for any distance). If the riser room has direct access from the outside, a PIV is not required. The sprinkler riser room may be used for other mechanical equipment, but not for the main electrical room nor shall it be used for storage; it may be used to house the fire alarm panel.

Submit plans, specifications and calculations electronically for approval at www.MyBuildingPermit.com. All plans shall be designed and stamped by a person holding a State of Washington Certificate of Competency Level III certification. The system shall be installed by a state licensed sprinkler contractor. REF RCW 18.60 State of Washington.

NOTE: TWO PERMITS are required from the Fire Department for installation of the fire sprinkler system, one for the underground and one for the sprinkler system itself. No work shall be performed on the sprinkler system without a Fire Department permit.

The civil drawings may be used as reference but do not constitute permission to install the fire sprinkler underground. The underground permit is NOT over-the-counter, so should be applied for well in advance of the anticipated date of start of construction.

FIRE ALARM

A fire alarm system is required to be installed throughout the building. A separate permit is required from the Fire Department prior to installation. Submit plans and specifications for approval electronically at MyBuildingPermit.com. The system shall comply with Washington State Barrier Free requirements regarding installation of visual devices and pull stations. The low-frequency requirement is also required for this project. The specific requirements for the system can be found in Kirkland Operating Policy 10.

FIRE EXTINGUISHERS

Portable fire extinguishers are required per Section 906 of the IFC. Minimum rating is 2A10BC. Extinguishers shall be mounted or in cabinets so that the top of the extinguisher is no more than 5 feet above the finished floor. Travel distance to a fire extinguisher shall not exceed 75 feet as measured along the route of travel.

KEY BOX

A Key box is required (Knox Box). It shall be installed in an approved accessible location no higher than six feet above grade. The keys shall provide the Fire Department with access to the FACP and any sprinkler riser rooms. In most cases it will be located at the front entrance to the building. The box shall be 4400 style, flush-mounted and monitored for tamper at the alarm panel. The box may be purchased on-line at www.knoxbox.com; or by filling out an order form which is available from the Fire Department office. Contact the Fire Prevention Bureau at 425-587-3661 for more information.

EMERGENCY RADIO COVERAGE (Effective 7-1-16)

This is not a requirement for a radio system per se, only providing you with information regarding the City's radio requirement for new buildings. The building "may" need a radio system because it is not exempted outright from the requirement (via any of the below thresholds)

510.1 Emergency Responder Radio Coverage. All new buildings shall have approved radio coverage for emergency responders within any building meeting any of the following conditions.

1. There are more than five stories above grade plane (as defined by the International Building Code, Section 202);
 2. The total building area is 50,000 square feet or more;
 3. The total basement area is 10,000 square feet or more;
- Or
4. There are floors used for human occupancy more than 30 feet below the finish floor of the lowest level of exit discharge.

Exception:

1. Buildings and area of buildings that have minimum radio coverage signal strength levels of the King County Regional 800 MHz Radio System within the building in accordance with Section 510.4.1. (This may be determined through analysis during the construction phase.)

If it is determined that a radio system is required, a fire department construction permit is required for installation.

GATES INSTALLED ON ACCESS ROADS

In most cases, primary access roads may not be obstructed by gates. However, the installation of security gates may be approved when, in the opinion of the Fire Marshal, firefighting or medical and/or rescue operations would not be impaired.

1. General Requirements:

- a. A building permit is required from Kirkland Building Department prior to installing automated gates. The permit will be routed to Fire for approval and conditions. A final inspection by a Fire Department representative is required.
- b. The use of directional-limiting devices (tire spikes) is prohibited.
- c. Gates shall have an approved means of emergency operation. Examples include the following:
 - 1) The gate fails to the "open" position when the power is off. It shall remain open until power is restored.
 - 2) Battery or generator power backup providing normal use of the gate.
 - 3) Approved manual operation of the gate.

2. Acceptable means of gate access:

- a. Automatic switch controllers such as Click2Enter or another approved access system. Gates equipped with Click2Enter shall be identified by an approved sign
- b. When there are only one or two homes behind a security gate, a Knox switch or keypad may be used.
- c. A Knox padlock may be approved on a case by case basis, such as when the gate is for secondary access or service vehicle/fire department access only.
- d. Where a fence is provided on each side of a gate for a commercial property, a man door shall be provided at an approved location with a Knox key for access to the man door.
- e. The unobstructed width of gates shall be as follows:
 - 1) For commercial or multifamily applications, the gate shall open a full 20 feet.

Exception: For split gates on commercial or multifamily projects where there is a post in between the exit and entrance to a facility, the minimum unobstructed width of each lane shall be 12 feet.

- 2) For short plats or subdivisions, the gate shall open the width as dictated by the required width of the access road (i.e. 16 or 20 feet, see D.1.b)
- 3) For gates accessing one single family home via a driveway, the gate width shall be a full 10 feet.

FIRE SAFETY DURING CONSTRUCTION

The building is approximately 40,000 s.f. (not including garage). In addition to the general fire safety requirements in IFC 3308, the Kirkland Fire Department has several requirements for high rise and/or wood-frame buildings more than 50,000 square feet in area. Don't worry about that.

3308.8.1 Job Site Security. After above grade combustibile construction has begun, the job site shall be secured with controlled access. In addition, off hours guard service and/or motion controlled surveillance may be required at the discretion of the fire code official.

3308.8.2 Job shacks and other temporary structures. Job shacks and other temporary structures located within or less than 20' from the permanent building shall be:

- Constructed of non-combustible materials or 1 hour fire-resistive construction.
- Shall not be equipped with fuel fired heaters
- Shall be equipped with monitored fire alarm system when located below grade
- Shall not function as offices unless protected with automatic sprinkler systems

PUBLIC WORKS DEPARTMENT

Public Works Staff Contacts

Ryan Schauble, Development Engineer
Phone: 425-587-3842 / E-mail: rschauble@kirklandwa.gov

General Conditions:

1. All public improvements associated with this project including street and utility improvements, must meet the City of Kirkland Public Works Pre-Approved Plans and Policies Manual. A Public Works Pre-Approved Plans and Policies manual can be purchased from the Public Works Department, or it may be retrieved from the Public Works Department's page at the City of Kirkland's web site.
2. This project will be subject to Public Works Permit and Connection Fees. It is the applicant's responsibility to contact the Public Works Department by phone or in person to determine the fees. The applicant should anticipate the following fees:
 - o Water, Sewer, and Surface Water Connection Fees *
 - o Side Sewer Inspection Fee *
 - o Septic Tank Abandonment Inspection Fee
 - o Water Meter Fee *
 - o Right-of-way Fee
 - o Review and Inspection Fee
 - o Building Permits associated with this proposed project will be subject to the traffic, park, and school impact fees per Chapter 27 of the Kirkland Municipal Code. The impact fees shall be paid prior to issuance of the Building Permit(s). Any existing buildings within this project which are demolished will receive a Traffic Impact Fee credit, Park Impact Fee Credit and School Impact Fee Credit. This credit will be applied to the first Building Permits that are applied for within the project. The credit amount for each demolished building will be equal to the most currently adopted Fee schedule.

* Fee to be paid with the issuance of a Building Permit.

3. All street and utility improvements shall be permitted by obtaining a Land Surface Modification (LSM) Permit, including the required LSM Checklist.

4. Subdivision Performance and Maintenance Securities:

- The subdivision can be recorded in advance of installing all the required street and utility improvements by posting a performance security equal to 130% of the value of work. This security amount will be determined by using the City of Kirkland's Improvement Evaluation Packet (available in either Excel or PDF). Contact the Development Engineer assigned to this project to assist with this process.
 - If a recording Performance Security has not yet been posted, then prior to issuance of the LSM Permit a standard right of way restoration security ranging from \$10,000.00 to 30,000.00 (value determined based on amount of ROW disruption) shall be posted with Public Works Department. This security will be held until the project has been completed.
 - Prior to Final Inspection of the Land Surface Modification improvements, there will be a condition of the permit to establish a two year Maintenance security.
5. Prior to submittal of a Building or Zoning Permit, the applicant must apply for a Concurrency Test Notice. Contact Thang Nguyen, Transportation Engineer, at 425-587-3869 for more information. A separate Concurrency Permit will be created.
6. After concurrency has passed, the project will receive a concurrency test notice that allows the applicant to proceed with all development permits. A "Certificate of Concurrency" is established with a development or building permit. It will read as follows: CERTIFICATE OF CONCURRENCY: This project has been reviewed and approved for water, sewer, and traffic concurrency. Any water and sewer mitigating conditions are listed within the conditions below. Any traffic mitigating conditions will be found in an attached memorandum from the Public Works Traffic Engineering Analyst to the Planning Department Project Planner. Upon issuance of this permit, this project shall have a valid Certificate of Concurrency and concurrency vesting until the permit expires. This condition shall constitute issuance of a Certificate of Concurrency pursuant to chapter 25.12 of the Kirkland Municipal Code.
7. All civil engineering plans which are submitted in conjunction with a building, grading, or right-of-way permit must conform to the Public Works Policy G-7, Engineering Plan Requirements. This policy is contained in the Public Works Pre-Approved Plans and Policies manual.
8. All street improvements and underground utility improvements (storm, sewer, and water) must be designed by a Washington State Licensed Engineer; all drawings shall bear the engineers stamp.
9. All plans submitted in conjunction with a building, grading or right-of-way permit must have elevations which are based on the King County datum only (NAVD 88).
10. A completeness check meeting is required prior to submittal of any Building Permit applications.
11. Prior to issuance of any commercial or multifamily Building Permit, the applicant shall provide a plan for garbage, recycling and composting storage and pickup. The plan shall conform to Policy G-9 in the Public Works Pre-approved Plans and be approved by Waste Management and the City. Important feature is to provide enough storage area for recycling and composting; and being able to pick up containers without storing in the ROW overnight. Submit the plan with a cover letter to explain how Policy G-9 requirements will be met. Please contact John MacGillivray, 425.587.3804, if you have questions. The allowance for pick-up by WM shall meet ALL the requirements of the Mirra Townhomes Curb Cuts/Garbage Location Variance, TRAN20-00320, prior to approval of any Building Permits.
12. The required tree plan shall include any significant tree in the public right-of-way along the property frontage.

Sanitary Sewer Conditions:

1. A 6" sanitary sewer main is provided along the 1st Avenue South frontage. This line will only be served by the proposed property and will not be extended in the future. Provide a capacity/conveyance analysis to determine if this 6" line is adequate to serve the project. An 8" main exists along most of the 2nd Street South frontage and is adequate to serve this project.

2. Side sewers serving the property shall be PVC gravity sewer pipe per Public Works Pre-Approved Criteria. Remove and replace any substandard pipes. Verify existing pipe condition by video inspection if the pipe is to remain.
3. Any existing septic system shall be abandoned per City standards with a Demo Permit.

Water System Conditions:

1. The existing water main in the right-of-way is adequate to serve the project.
2. It is preferred and in the best interest for both the City and developer to provide one larger water meter to serve the development. The applicant can provide separate sub-meters on the back side of the main meter. The sub-meters will be privately maintained. City of Kirkland will set the water meter. The water size is determined when the Building Permit is submitted and is sized per the Uniform Plumbing Code.
3. The existing water services shall be abandoned at the main, unless approved otherwise by Public Works. Credit will be given for all existing water meters that are in use.
4. See Fire Department conditions for fire flow requirements.

Surface Water Conditions:

1. Provide temporary and permanent storm water control in accordance with the 2016 King County Surface Water Design Manual (KCSWDM) and the City of Kirkland Addendum (Policy D-10).
2. To determine the drainage review level required, the target impervious surface area is the maximum allowable lot coverage area for the project, plus any offsite improved impervious areas. See Policies D-2 and D-3 in the Public Works Pre-Approved Plans for drainage review information, or contact Kirkland Surface Water staff at (425) 587-3800 for assistance. The Kirkland Drainage Review Flow Chart is a helpful tool to determine a project's drainage review level. Drainage review levels are summarized below:
 - Full Drainage Review
 - o Any non-single-family residential project that creates more than 2,000 sf of new and/or replaced impervious surface, or greater than 7,000 sf of land disturbing activity will trigger a Full Drainage Review.
 - o Single family residential projects that propose improvements greater than the Simplified thresholds explained above will be subject to a Full Drainage Review.
3. This project is in a Level 1 Flow Control Area (Potential Direct Discharge), and is required to comply with core drainage requirements in the KCSWDM.
 - a) To qualify for direct discharge, the applicant must demonstrate (at a minimum):
 - The conveyance system between the project site and Lake Washington will be comprised of manmade conveyance elements and will be within public right-of-way or a public or private drainage easement, AND
 - The conveyance system will have adequate capacity per Core Requirement #4, Conveyance System, for the entire contributing drainage area, assuming build-out conditions to current zoning for the equivalent area portion and existing conditions for the remaining area;
 - b) If a stormwater detention system is required, this project may be designed to Level 1 flow control standards. Existing conditions may be used as the pre-developed condition. Calculations of the existing impervious surface area for modeling shall be in accordance with the formula described in the KCSWDM.
4. The project may qualify for an exception to detention if the target surfaces will generate no more than a 0.15 cfs increase in the existing conditions 100-year peak flow. The 15-minute time step must be used to perform the flow control analysis. Do not use the 1-hour time step. Approved hydrologic modeling programs are MGS Flood and WWHM 2012.

5. Evaluate the feasibility and applicability of dispersion, infiltration, and other stormwater Low Impact Development (LID) Best Management Practices (BMPs) per the KCSWDM. If feasible, stormwater LID BMPs are required to the maximum extent feasible. If LID BMPs are infeasible, pervious pavement cannot be used to reduce overall impervious lot coverage. The Private Maintenance Agreement will be recorded on all projects that construct a stormwater LID BMP or facility, per Policy D-7.
6. Soil information may be necessary for designing LID BMPs per the KCSWDM, and there are other reasons a soil report is necessary for a project (e.g., steep slopes, sensitive areas, etc.). Refer to Policy D-8 for details.
7. Special inspections may be required for LID BMPs on this project. Provide documentation of inspections by a licensed geotechnical professional that the BMP will function as designed.
8. If the project will create or replace more than 5,000 square feet of pollution generating impervious surface (PGIS), provide water quality treatment in accordance with the KCSWDM. The enhanced treatment level is required for multi-family residential and single family residential projects with eight or more dwelling units per acre density.
9. Soil Amendment per Pre-Approved Plan E.12 is required for all landscaped areas.
10. All roof and driveway drainage must be tight-lined to the storm drain system or utilize low impact development techniques on-site.
11. Provide collection and conveyance of right-of-way storm drainage. Extend the storm main south along 2nd Street South to the north side of the proposed driveway entrance. New structures may have to be added to ensure catch basins are located within curb flow lines. Provide a plan and profile design for the storm sewer system. Size and material of construction shall be in accordance with the City Kirkland Pre-Approved Plans and Notes. Refer to Policy D-5 for details.
12. Construction Stormwater Pollution Prevention Plan (CSWPPP):
 - All proposed projects that will conduct construction activities onsite, or offsite must provide stormwater pollution prevention and spill controls to prevent, reduce, or eliminate the discharge of pollutants (including sediment) to onsite or adjacent stormwater systems or watercourses.
 - Refer to Core Requirement No. 5 in the KCSWDM and Policy D-12.
 - Provide an erosion control report and plan with the Building or Land Surface Modification Permit application. The plan shall be in accordance with the KCSWDM.
 - Construction drainage control shall be maintained by the developer and will be subject to periodic inspections. During the period from May 1 and September 30, all denuded soils must be covered within 7 days; between October 1 and April 30, all denuded soils must be covered within 12 hours. Additional erosion control measures may be required based on site and weather conditions. Exposed soils shall be stabilized at the end of the workday prior to a weekend, holiday, or predicted rain event.

Street and Pedestrian Improvement Conditions:

1. The subject property abuts 2nd Street South, 1st Avenue South, and 2nd Avenue South. These streets are classified as Neighborhood Access streets. Zoning Code sections 110.10 and 110.25 require the applicant to make half-street improvements in rights-of-way abutting the subject property. Section 110.30-110.50 establishes that this street must be improved with the following:

2nd Street South and 1st Avenue South

- A. Install Type-A concrete curb and gutter. The face of curb shall be 14 feet from the ROW CL (28' curb to curb). Widen the street pavement to meet the curb and gutter.
- B. Install a 4.5-ft landscape strip behind the curb, with street trees 30 feet on-center.
- C. Install a 5-ft wide concrete sidewalk behind the landscape strip.
- D. Remove and replace existing half-street improvements in substandard condition.
- E. Dedicate sufficient area on the northwest corner of the property to encompass all improvements. Curb radius at corner to be 25' per COK Std. Plan No. CK-R.24.

- F. Underground the existing power service pole at the corner of 1st Avenue South and 2nd Street South.
- G. 1st Avenue South and 2nd Street South were overlaid in 2018. Per COK Policy R-7, when the asphalt that is to be trenched is less than five years old, a full-street overlay is required.

2nd Avenue South

- A. Install Type-A concrete curb and gutter. The face of curb shall be 15.5 feet from the ROW CL (match curb to the east). Widen the street pavement to meet the curb and gutter.
- B. 2nd Avenue South is a major pedestrian sidewalk. As such, an 8ft wide sidewalk with 4x6 tree wells with street trees 30 ft on-center is required.
- C. Provide pedestrian lighting 60 ft on-center (3 lights total).
- D. Remove and replace existing half-street improvements in substandard condition.
- E. Dedicate sufficient area on the northwest corner of the property to encompass all improvements. Curb radius at corner to be 25' per COK Std. Plan No. CK-R.24.

Transportation Comments (Per Thang Nguyen):

- The driveway location as shown is acceptable. The driveway must meet sight distance per PW pre-approved plans policy R-13.
 - Transportation concurrency must be completed and passed prior to any other land use or public works permit review. I believe you can start the building design review concurrently but to be sure, check with the planning department. The concurrency review turn around is two weeks. I will be out of the office April 15 to 23rd so you need to submit for concurrency by March 29th or you won't get your concurrency approval until end of April.
 - Yes, public works will review and regulate the parking garage security gate. The visitor stalls should be available to the public at all times. In special conditions we may allow it to be secure if there is a way to easily accommodate the visitors and it doesn't create a queue that blocks the sidewalk or disrupt traffic flow on the street.
 - For parking for the residents can be secure. Security gate must be located at least 25 feet from the back of sidewalk.
 - The parking aisle must be 24 feet and cleared of any fixed object. The parking stall length must meet the City of Kirkland parking plate. For a standard size stall and two-way traffic, the total clear width is 61 feet.
 - The end stalls must have a minimum 5-foot turnaround space for a 24 foot drive aisle.
2. Meet the requirements of the Kirkland Driveway Policy R-4. Spacing Table from R-4.
 3. Meet the requirements of the Kirkland Intersection Sight Distance Policy R.13. All street and driveway intersections shall not have any visual obstructions within the sight distance triangle.
 4. When three or more utility trench crossings occur within 150 lineal ft. of street length or where utility trenches parallel the street centerline, the street shall be overlaid with new asphalt or the existing asphalt shall be removed and replaced per the City of Kirkland Street Asphalt Overlay Policy R-7.
 - Existing streets with 4-inches or more of existing asphalt shall receive a 2-inch (minimum thickness) asphalt overlay. Grinding of the existing asphalt to blend in the overlay will be required along all match lines.
 - Existing streets with 3-inches or less of existing asphalt shall have the existing asphalt removed and replaced with an asphalt thickness equal or greater than the existing asphalt provided however that no asphalt shall be less than 2-inches thick and the subgrade shall be compacted to 95% density.
 5. Prior to the final of the building or grading permit, pay for the installation of stop and street signs at the new intersections.
 6. It shall be the responsibility of the applicant to relocate any above-ground or below-ground utilities which conflict with the project, associated street, or utility improvements.
 7. Underground all new and existing on-site utility lines and overhead transmission lines. Underground any new

off-site transmission lines.

8. Zoning Code Section 110.60.7.b establishes the requirement that existing utility and transmission (power, telephone, etc.) lines on-site and in rights-of-way adjacent to the site must be underground. The Public Works Director may determine if undergrounding transmission lines in the adjacent right-of-way is not feasible and defer the undergrounding by signing an agreement to participate in an undergrounding project, if one is ever proposed. In this case, the Public Works Director has determined that undergrounding of existing overhead utility on _____ is not feasible at this time and the undergrounding of off-site/frontage transmission lines should be deferred with a Local Improvement District (LID) No Protest Agreement. The final recorded subdivision document shall include the following note:

Local Improvement District (LID) Waiver Agreement. Chapter 110.60.7.b of the Kirkland Zoning Code requires all overhead utility lines along the frontage of the subject property to be converted to underground unless the Public Works Director determines that it is infeasible to do so at the time of the subdivision recording. If it is determined to be infeasible, then the property owner shall consent to the formation of a Local Improvement District, hereafter formed by the City or other property owners. During review of this subdivision it was determined that it was infeasible to convert the overhead utility lines to underground along the frontage of this subdivision on (((street name))). Therefore, in consideration of deferring the requirement to underground the overhead utility lines at the time of the subdivision recording, the property owner and all future property owners of lots within this subdivision hereby consent to the formation of a Local Improvement District hereafter formed by the City or other property owners

9. New LED street lights may be required per Puget Sound Energy design and Public Works approval. Contact the INTO Light Division at PSE for a lighting analysis. If lighting is necessary, design must be submitted prior to issuance of a grading or building permit.

Brynja Myren - Account Sales Manager, Intolight, PUGET SOUND ENERGY
Tel 425-462-3833 | Cell 206-604-3348 | Fax 425-462-3149
Email brynja.myren@pse.com | Website: www.intolight.com

Section 50.32

Zone
CBD-4

USE ZONE CHART

DIRECTIONS: FIRST, read down to find use...THEN, across for REGULATIONS

Section 50.32	USE ↓ REGULATIONS ↓	Required Review Process	MINIMUMS			MAXIMUMS		Landscape Category (See Ch. 95)	Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)	Special Regulations (See also General Regulations)	
			Lot Size	REQUIRED YARDS (See Ch. 115)			Lot Coverage					Height of Structure
				Front	Side	Rear						
.060	Office Use	D.R., Chapter 142 KZC.	None	10'	0'	0'	100%	54' above average building elevation or existing grade.	D	D	One per each 350 sq. ft. of gross floor area.	<ol style="list-style-type: none"> The following regulations apply to veterinary offices only: <ol style="list-style-type: none"> May only treat small animals on the subject property. Outside runs and other outside facilities for the animals are not permitted. Site must be designed so that noise from this use will not be audible off the subject property. A certification to this effect, signed by an Acoustical Engineer, must be submitted with the D.R. and building permit applications. A veterinary office is not permitted if the subject property contains dwelling units. Ancillary assembly and manufacture of goods on premises may be permitted as part of an office use if: <ol style="list-style-type: none"> The ancillary assembled or manufactured goods are subordinate to and dependent on this office use; and The outward appearance and impacts of this office use with ancillary assembly and manufacturing activities must be no different from other office uses. Primary vehicle access to the subject property may not be directly from Second Avenue South between Second Street South and State Street unless no other alternative exists.
.070	Church								B	One per every 4 people based on maximum occupancy load of any area of worship. See Spec. Reg. 2.	<ol style="list-style-type: none"> Primary vehicular access to the subject property may not be directly from Second Avenue South between Second Street South and State Street unless no other alternative exists. No parking is required for daycare or school ancillary to the use. 	
.080	Stacked or Attached Dwelling Units								A	1.2 per studio unit. 1.3 per 1 bedroom unit. 1.6 per 2 bedroom unit. 1.8 per 3 or more bedroom unit. See KZC 105.20 for visitor parking requirements.		