

NE 116TH ST CROSSWALK UPGRADES

KIRKLAND, WASHINGTON

PROJECT# 37-20-PW

CIP# NMC0120100

NE 116TH ST FROM 104TH AVE NE TO 113TH PL NE

LENGTH 3,050 FEET = 0.58 MILES



VICINITY MAP
N.T.S.

CITY OFFICIALS

PENNY SWEET	MAYOR
JAY ARNOLD	DEPUTY MAYOR
KELLI CURTIS	COUNCIL MEMBER
TOBY NIXON	COUNCIL MEMBER
NEAL BLACK	COUNCIL MEMBER
DAVE ASHER	COUNCIL MEMBER
JON PASCAL	COUNCIL MEMBER
KURT TRIPLETT	CITY MANAGER
JULIE UNDERWOOD	INTERIM PUBLIC WORKS DIRECTOR
ROD STEITZER, P.E.	CAPITAL PROJECTS MANAGER

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JEFF MILES	LAKE WASH. SCHOOL DIST.	425-882-5120
GREG MATOTE	NORTHSHORE UTILITY DIST.	425-398-4400
	EMERGENCY	911
	ONE CALL	800-424-5555



REUSE OF DOCUMENTS

ALL DRAWINGS ARE INSTRUMENTS OF PROFESSIONAL SERVICE FOR THIS PROJECT.
REUSE OR ALTERATION IS AT THE USER'S SOLE RISK.

DSGN					
DR					
CHK					
APVD	NO.	DATE	REVISION	BY	APVD



720 3RD AVENUE, SUITE 2020,
SEATTLE, WA 98104
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NE 116TH STREET
CROSSWALK UPGRADES
KIRKLAND, WA

COVER

PROJECT NO.	NMC0120100
DATE	09/01/2020
DRAWING NO.	CV1
SHEET NO.	1 / 15

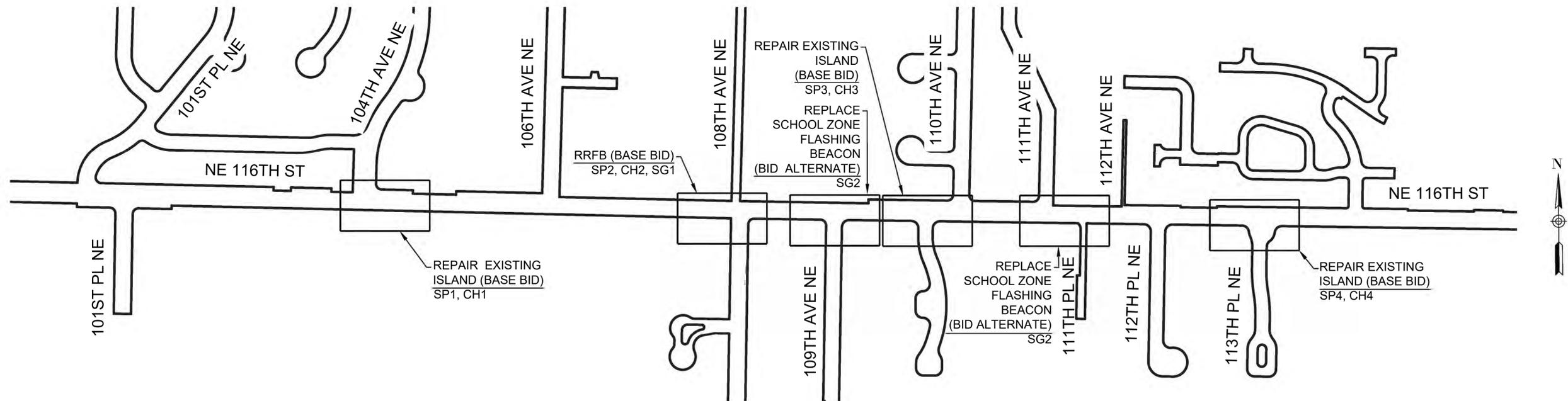


CITY OF KIRKLAND
123 FIFTH AVENUE
KIRKLAND, WA 98033
PHONE: (425) 587-3000



SHEET INDEX

SHEET NO.	DRAWING NUMBER	SHEET NAME
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LOCATION MAP AND SHEET KEY
1"=200'



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DSGN					
DR					
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APVD					
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TOOLE DESIGN

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NE 116TH STREET
CROSSWALK UPGRADES
KIRKLAND, WA

SHEET INDEX AND
LOCATION MAP

PROJECT NO.	NMC0120100
DATE	09/01/2020
DRAWING NO.	NT1
SHEET NO.	2 / 15

GENERAL NOTES

- A PRE-CONSTRUCTION CONFERENCE SHALL BE HELD PRIOR TO THE START OF CONSTRUCTION.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR SECURING ALL NECESSARY PERMITS PRIOR TO CONSTRUCTION.
- ALL ROADWAY WORK AND MATERIAL SHALL BE IN ACCORDANCE WITH THE CURRENT APWA AND CITY OF KIRKLAND STANDARDS AND SPECIFICATIONS.
- ALL PUBLIC ROADWAYS SHALL BE CONSTRUCTED OF 2" CLASS 1/2" HMA AC PAVING ON 4" ASPHALT-TREATED BASE (ATB), UNLESS OTHERWISE APPROVED BY THE PUBLIC WORKS DEPARTMENT.
- A COPY OF THE APPROVED ROADWAY PLANS MUST BE ON THE JOB SITE WHENEVER CONSTRUCTION IS IN PROGRESS.
- DENSITY TEST REPORTS WILL BE REQUIRED FOR ALL PUBLIC ROADWAYS AND ALL PRIVATE ROADWAYS WITHIN PLATS. ALL TRENCH BACKFILL SHALL BE COMPACTED TO 95 PERCENT DENSITY IN ROADWAYS, ROADWAY SHOULDERS, ROADWAY PRISM AND DRIVEWAYS, AND 85 PERCENT DENSITY IN UNPAVED AREAS. ALL PIPE ZONE COMPACTION SHALL BE 95 PERCENT.
- ALL COMMERCIAL AND RESIDENTIAL DRIVEWAYS MUST CONFORM TO THE CITY OF KIRKLAND DEPARTMENT OF PUBLIC WORKS DRIVEWAY POLICY.
- ALL CONCRETE FOR SIDEWALKS AND CURB AND GUTTER MUST BE 4,000 PSI MINIMUM. (5-3/4 SACK MIX.)
- IN THE CASE OF NEW ROAD CONSTRUCTION OR RECONSTRUCTION REQUIRING MAILBOXES TO BE MOVED OR REARRANGED, THE CONTRACTOR SHALL COORDINATE WITH THE U.S. POSTAL SERVICE FOR THE NEW LOCATION OF THE MAILBOX STRUCTURE.
- ANY ROADWAY SIGNAGE OR STRIPING REMOVED OR TEMPORARILY MOVED BY THE CONTRACTOR SHALL BE RESTORED SO AS TO MEET THE CURRENT CITY OF KIRKLAND STANDARDS.**
- IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO PROVIDE ADEQUATE TEMPORARY TRAFFIC CONTROL TO ENSURE TRAFFIC SAFETY DURING CONSTRUCTION ACTIVITIES. THEREFORE, THE CONTRACTOR SHALL SUBMIT A TRAFFIC CONTROL PLAN TO THE PUBLIC WORKS DEPARTMENT AT LEAST 48 HOURS PRIOR TO STARTING ANY WORK IN THE RIGHT-OF-WAY. ALL TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (MUTCD) OR AS MODIFIED BY THE TRAFFIC ENGINEER.
- WHERE A SIDEWALK IS TO BE CONSTRUCTED ABOVE A SLOPE OR ADJACENT TO A ROCKERY OR RETAINING WALL WHERE THE LOWEST FINISHED ELEVATION OF THE SLOPE, ROCKERY, OR RETAINING WALL IS TO BE THIRTY INCHES (30") OR MORE BELOW THE FINISHED ELEVATION OF THE SIDEWALK, A SAFETY RAILING SHALL BE REQUIRED WHEN: (A) THE PLANE OF THE WALL FACE IS LESS THAN 4' IN HORIZONTAL DISTANCE FROM THE OUTSIDE EDGE OF THE SIDEWALK; (B) THE SLOPES ADJACENT TO THE SIDEWALK AVERAGE GREATER THAN TWO TO ONE.
- THE MAXIMUM GRADE FOR PRIVATE ROADWAYS SHALL BE TWENTY PERCENT (20%), OR FIFTEEN PERCENT (15%) IF USED FOR FIRE ACCESS. FOR PUBLIC ROADWAYS, THE MAXIMUM GRADE SHALL BE FIFTEEN PERCENT (15%).
- DEAD-END STREETS SHALL BE APPROPRIATELY SIGNED AND BARRICADED. SEE MOST CURRENT EDITION OF THE MUTCD.
- SIDEWALK AND CURB AND GUTTER CANNOT BE POURED MONOLITHICALLY. THERE MUST BE A COLD JOINT OR FULL-DEPTH EXPANSION JOINT BETWEEN THEM.
- MEASURES SHALL BE TAKEN BY THE CONTRACTOR TO PROVIDE GROUND COVER IN AREAS WITHIN THE RIGHT-OF-WAY WHICH HAVE BEEN STRIPPED OF NATURAL VEGETATION OR HAVE A POTENTIAL FOR EROSION.
- THE CONTRACTOR SHALL COORDINATE WITH PUGET POWER FOR THE DESIGN AND INSTALLATION OF STREET LIGHTS ON ALL NEWLY-CREATED PUBLIC ROADWAYS AND EXISTING ROADWAYS.**
- WHEN AN EXISTING ROADWAY IS TO RECEIVE A HALF-STREET OVERLAY, THE EXISTING ROADWAY MUST BE COLD PLANNED AT THE EDGE OF THE GUTTER AND CENTERLINE. WHEN THE EXISTING ROADWAY IS TO RECEIVE A FULL-STREET OVERLAY, IT MUST BE COLD PLANNED AT THE EDGE OF BOTH GUTTERS. SEE CITY OF KIRKLAND STANDARD DETAIL NO. R.13.
- THE TEMPORARY EROSION AND SEDIMENTATION CONTROL (TESC) MEASURES SHOWN IN THESE PLANS SHALL BE CONSIDERED A MINIMUM. THE CONTRACTOR SHALL PROVIDE ANY REASONABLE ADDITIONAL MEASURES AS MAY BE REQUIRED TO FACILITATE ACTUAL SITE RUNOFF CONDITIONS AT THE TIME OF CONSTRUCTION. ALL NECESSARY TEMPORARY EROSION AND SEDIMENTATION CONTROL MEASURES SHALL BE IN PLACE PRIOR TO ANY DEMOLITION OR CONSTRUCTION ACTIVITIES.
- WHEN INSTALLING NEW SIDEWALK, THE AREA BEHIND THE SIDEWALK MUST BE GRADED SO THAT THE YARD DRAINAGE DOES NOT DRAIN OVER THE SIDEWALK.
- ANY EXISTING PUBLIC IMPROVEMENTS DAMAGED DURING CONSTRUCTION SHALL BE REPLACED PRIOR TO FINAL INSPECTION.
- THE CONTRACTOR IS RESPONSIBLE FOR KEEPING ALL PUBLIC STREETS FREE FROM MUD AND DEBRIS AT ALL TIMES. THE CONTRACTOR SHALL BE PREPARED TO USE POWER SWEEPERS OR OTHER PIECES OF EQUIPMENT NECESSARY TO KEEP THE ROADWAYS CLEAN.
- BACKFILL IN ALL STREET CUTS ON ARTERIALS WILL BE CONTROL DENSITY FILL (CDF). CONTRACTOR MUST PROVIDE STEEL PLATING NECESSARY TO ALLOW THE CDF TO CURE.
- WHEN CONSTRUCTING NEW CURB AND GUTTER WHICH DOES NOT ALIGN WITH THE EXISTING EDGE OF PAVEMENT, THE ROADWAY MUST BE TAPERED FROM THE ENDS OF THE NEW CURB AND GUTTER TO MATCH THE EXISTING PAVEMENT. THE ENTRY TAPER INTO THE NEW IMPROVEMENTS SHALL BE 5:1, AND LEAVING THE NEW IMPROVEMENTS SHALL BE 10:1.
- WHEN AN EXISTING ROADWAY IS TO BE WIDENED, THE EXISTING PAVEMENT MUST BE SAW CUT AT LEAST ONE FOOT FROM THE EDGE TO PROVIDE A PROPER MATCH BETWEEN NEW AND EXISTING ASPHALT. HOWEVER, WHEN THE EXISTING PAVEMENT CONTAINS ALLIGATORING AREAS, THOSE AREAS MUST BE REMOVED PRIOR TO WIDENING. ALL SAW CUTS SHALL BE PARALLEL OR PERPENDICULAR TO THE RIGHT-OF-WAY CENTERLINE.
- ALL ROCKERIES MUST BE CONSTRUCTED IN ACCORDANCE WITH THE MOST CURRENT GUIDELINES OF THE ASSOCIATION.

**CITY OF KIRKLAND STANDARD
EROSION AND SEDIMENT CONTROL (ESC) NOTES**

- THE APPROVED CONSTRUCTION SEQUENCE SHALL BE AS FOLLOWS:
 - CONDUCT PRE-CONSTRUCTION MEETING.
 - FLAG OR FENCE CLEARING LIMITS AS DIRECTED BY ENGINEER.
 - POST SIGN WITH NAME AND PHONE NUMBER OF TESC SUPERVISOR.
 - INSTALL CATCH BASIN PROTECTION IF REQUIRED.
 - GRADE AND INSTALL CONSTRUCTION ENTRANCE(S).
 - INSTALL PERIMETER PROTECTION (SILT FENCE, BRUSH BARRIER, ETC.).
 - CONSTRUCT SEDIMENT PONDS AND TRAPS.
 - GRADE AND STABILIZE CONSTRUCTION ROADS.
 - CONSTRUCT SURFACE WATER CONTROLS (INTERCEPTOR DIKES, PIPE SLOPE DRAINS, ETC.) SIMULTANEOUSLY WITH CLEARING AND GRADING FOR PROJECT DEVELOPMENT.
 - MAINTAIN EROSION CONTROL MEASURE IN ACCORDANCE WITH CITY OF KIRKLAND STANDARDS AND MANUFACTURER'S RECOMMENDATIONS.
 - RELOCATE EROSION CONTROL MEASURES OR INSTALL NEW MEASURES SO THAT AS SITE CONDITIONS CHANGE, THE EROSION AND SEDIMENT CONTROL IS ALWAYS IN ACCORDANCE WITH THE CITY TESC MINIMUM REQUIREMENTS.
 - COVER ALL AREAS WITHIN THE SPECIFIED TIME FRAME WITH STRAW, WOOD FIBER MULCH, COMPOST, PLASTIC SHEETING, CRUSHED ROCK OR EQUIVALENT.
 - STABILIZE ALL AREAS THAT REACH FINAL GRADE WITHIN 7 DAYS.
 - SEED OR SOD ANY AREAS TO REMAIN UNWORKED FOR MORE THAN 30 DAYS.
 - UPON COMPLETION OF THE PROJECT, ALL DISTURBED AREAS MUST BE STABILIZED AND BEST MANAGEMENT PRACTICES REMOVED IF APPROPRIATE.
- CONTRACTOR IS RESPONSIBLE FOR KEEPING STREETS CLEAN AND FREE OF CONTAMINANTS AT ALL TIMES AND FOR PREVENTING AN ILLICIT DISCHARGE (KMC 11.2.00) INTO THE MUNICIPAL STORM DRAIN. IF YOUR CONSTRUCTION PROJECT CAUSES AN ILLICIT DISCHARGE TO THE MUNICIPAL STORM DRAIN SYSTEM, THE CITY OF KIRKLAND STORM MAINTENANCE DIVISION WILL BE CALLED TO CLEAN THE PUBLIC STORM SYSTEM, AND OTHER AFFECTED PUBLIC INFRASTRUCTURE. THE CONTRACTOR(S), PROPERTY OWNER, AND ANY OTHER RESPONSIBLE PARTY MAY BE CHARGED ALL COSTS ASSOCIATED WITH THE CLEAN-UP AND MAY ALSO BE ASSESSED MONETARY PENALTIES (KMC 11.2.00). THE MINIMUM PENALTY IS \$250. A FINAL INSPECTION OF YOUR PROJECT WILL NOT BE GRANTED UNTIL ALL COSTS ASSOCIATED WITH THE CLEAN-UP, AND PENALTIES, ARE PAID TO THE CITY OF KIRKLAND.
- CONSTRUCTION DEWATERING DISCHARGES SHALL ALWAYS MEET WATER QUALITY GUIDELINES LISTED IN COK POLICY E-1. SPECIFICALLY, DISCHARGES TO THE PUBLIC STORMWATER DRAINAGE SYSTEM MUST BE BELOW 25NTU, AND NOT CONSIDERED AN ILLICIT DISCHARGE (PER KMC 15.52.090). TEMPORARY DISCHARGES TO SANITARY SEWER REQUIRE PRIOR AUTHORIZATION AND PERMIT FROM KING COUNTY INDUSTRIAL WASTE PROGRAM (206-263-3000) AND NOTIFICATION TO THE PUBLIC WORKS CONSTRUCTION INSPECTOR.
- ALL WORK AND MATERIALS SHALL BE IN ACCORDANCE WITH CITY OF KIRKLAND STANDARDS AND SPECIFICATIONS.
- THE BOUNDARIES OF THE CLEARING LIMITS AS DIRECTED BY THE ENGINEER SHALL BE CLEARLY FLAGGED IN THE FIELD BY A CLEARING CONTROL FENCE PRIOR TO CONSTRUCTION. DURING THE CONSTRUCTION PERIOD, NO DISTURBANCE OR REMOVAL OF ANY GROUND COVER BEYOND THE FLAGGED CLEARING LIMITS SHALL BE PERMITTED. THE FLAGGING SHALL BE MAINTAINED BY THE PERMITTEE/CONTRACTOR FOR THE DURATION OF CONSTRUCTION.
- APPROVAL OF THIS EROSION/SEDIMENTATION CONTROL (ESC) PLAN DOES NOT CONSTITUTE AN APPROVAL OF PERMANENT ROAD OR DRAINAGE DESIGN (E.G., SIZE AND LOCATION OF ROADS, PIPES, RESTRICTIONS, CHANNELS, RETENTION FACILITIES, UTILITIES, ETC.).
- THE IMPLEMENTATION OF THIS ESC PLAN AND THE CONSTRUCTION, MAINTENANCE, REPLACEMENT, AND UPGRADING OF THESE ESC FACILITIES IS THE RESPONSIBILITY OF THE PERMITTEE/CONTRACTOR UNTIL ALL CONSTRUCTION IS APPROVED.
- A COPY OF THE APPROVED ESC PLANS MUST BE ON THE JOB SITE WHENEVER CONSTRUCTION IS IN PROGRESS.
- THE ESC FACILITIES SHOWN ON THIS PLAN MUST BE CONSTRUCTED PRIOR TO OR IN CONJUNCTION WITH ALL CLEARING AND GRADING ACTIVITIES IN SUCH A MANNER AS TO ENSURE THAT SEDIMENT-LADEN WATER DOES NOT ENTER THE DRAINAGE SYSTEM OR VIOLATE APPLICABLE WATER STANDARDS, WHEREVER POSSIBLE, MAINTAIN NATURAL VEGETATION FOR SILT CONTROL.
- THE ESC FACILITIES SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE DETAILS ON THE APPROVED SYSTEM. LOCATIONS MAY BE MOVED TO SUIT FIELD CONDITIONS, SUBJECT TO APPROVAL BY THE ENGINEER AND THE CITY OF KIRKLAND INSPECTOR.
- THE ESC FACILITIES SHOWN ON THIS PLAN ARE THE MINIMUM REQUIREMENTS FOR ANTICIPATED SITE CONDITIONS. DURING THE CONSTRUCTION PERIOD, THESE ESC FACILITIES SHALL BE UPGRADED (E.G., ADDITIONAL SUMPS, RELOCATION OF DITCHES AND SILT FENCES, ETC.) AS NEEDED FOR UNEXPECTED STORM EVENTS. ADDITIONALLY, MORE ESC FACILITIES MAY BE REQUIRED TO ENSURE COMPLETE SILTATION CONTROL. THEREFORE, DURING THE COURSE OF CONSTRUCTION IT SHALL BE THE OBLIGATION AND RESPONSIBILITY OF THE CONTRACTOR TO ADDRESS ANY NEW CONDITIONS THAT MAY BE CREATED BY HIS ACTIVITIES AND TO PROVIDE ADDITIONAL FACILITIES OVER AND ABOVE THE MINIMUM REQUIREMENTS AS MAY BE NEEDED.
- THE ESC FACILITIES SHALL BE INSPECTED BY THE PERMITTEE/CONTRACTOR DAILY DURING NON-RAINFALL PERIODS. EVERY HOUR (DAYLIGHT) DURING A RAINFALL EVENT, AND AT THE END OF EVERY RAINFALL, AND MAINTAINED AS NECESSARY TO ENSURE THEIR CONTINUED FUNCTIONING. IN ADDITION, TEMPORARY SILTATION PONDS AND ALL TEMPORARY SILTATION CONTROLS SHALL BE MAINTAINED IN A SATISFACTORY CONDITION UNTIL SUCH TIME THAT CLEARING AND/OR CONSTRUCTION IS COMPLETED. PERMANENT DRAINAGE FACILITIES ARE OPERATIONAL, AND THE POTENTIAL FOR EROSION HAS PASSED. WRITTEN RECORDS SHALL BE KEPT DURING THE REVIEWS OF THE ESC FACILITIES.
- THE ESC FACILITIES ON INACTIVE SITES SHALL BE INSPECTED AND MAINTAINED A MINIMUM OF ONCE A MONTH OR WITHIN 48 HOURS FOLLOWING A STORM EVENT.
- STABILIZED CONSTRUCTION ENTRANCES SHALL BE INSTALLED AT THE BEGINNING OF CONSTRUCTION AND MAINTAINED FOR THE DURATION OF THE PROJECT. ADDITIONAL MEASURES, SUCH AS WASH PADS, MAY BE REQUIRED TO ENSURE THAT ALL PAVED AREAS ARE KEPT CLEAN FOR THE DURATION OF THE PROJECT.
- ALL DENUDED SOILS MUST BE STABILIZED WITH AN APPROVED TEMPORARY ESC METHOD (E.G. SEEDING, MULCHING, PLASTIC COVERING, CRUSHED ROCK) WITHIN THE FOLLOWING TIMELINES:
 - MAY 1 TO SEPTEMBER 30 - SOILS MUST BE STABILIZED WITHIN 7 DAYS OF GRADING.
 - OCTOBER 1 TO APRIL 30 - SOILS MUST BE STABILIZED WITHIN 2 DAYS OF GRADING.
 - STABILIZE SOILS AT THE END OF THE WORKDAY PRIOR TO A WEEKEND, HOLIDAY, OR PREDICTED RAIN EVENT.

**CITY OF KIRKLAND STANDARD
EROSION AND SEDIMENT CONTROL NOTES (CONT)**

- WHERE SEEDING FOR TEMPORARY EROSION CONTROL IS REQUIRED, FAST GERMINATING GRASSES SHALL BE APPLIED AT AN APPROPRIATE RATE (EXAMPLE: ANNUAL OR PERENNIAL RYE APPLIED AT APPROXIMATELY 80 POUNDS PER ACRE).
- WHERE STRAW MULCH IS REQUIRED FOR TEMPORARY EROSION CONTROL, IT SHALL BE APPLIED AT A MINIMUM THICKNESS OF 2".
- ALL LOTS ADJOINING OR HAVING ANY NATIVE GROWTH PROTECTION EASEMENTS (NGPE) SHALL HAVE A 6' HIGH TEMPORARY CONSTRUCTION FENCE (CHAIN LINK WITH PIER BLOCKS) SEPARATING THE LOT OR BUILDABLE PORTIONS OF THE LOT) FROM THE AREAS RESTRICTED BY THE NGPE AND SHALL BE INSTALLED PRIOR TO ANY GRADING OR CLEARING AND REMAIN IN PLACE UNTIL THE PLANNING DEPARTMENT AUTHORIZES REMOVAL.
- CLEARING LIMITS SHALL BE DELINEATED WITH A CLEARING CONTROL FENCE. THE CLEARING CONTROL FENCE SHALL CONSIST OF A 6-FT. HIGH CHAIN LINK FENCE ADJACENT THE DRIP LINE OF TREES TO BE SAVED, WETLAND OR STREAM BUFFERS, AND SENSITIVE SLOPES. CLEARING CONTROL FENCES ALONG WETLAND OR STREAM BUFFERS OR UPSLOPE OF SENSITIVE SLOPES SHALL BE ACCOMPANIED BY AN EROSION CONTROL FENCE, IF APPROVED BY THE CITY. A FOUR-FOOT HIGH ORANGE MESH CLEARING CONTROL FENCE MAY BE USED TO DELINEATE CLEARING LIMITS IN ALL OTHER AREAS.
- OFF-SITE STREETS MUST BE KEPT CLEAN AT ALL TIMES. IF DIRT IS DEPOSITED ON THE PUBLIC STREET SYSTEM, THE STREET SHALL BE IMMEDIATELY CLEANED WITH POWER SWEEPERS OR OTHER EQUIPMENT. ALL VEHICLES SHALL LEAVE THE SITE BY WAY OF THE CONSTRUCTION ENTRANCE AND SHALL BE CLEANED OF ALL DIRT THAT WOULD BE DEPOSITED ON THE PUBLIC STREETS.
- ROCK FOR EROSION PROTECTION OF ROADWAY DITCHES, WHERE REQUIRED, MUST BE OF SOUND QUARRY ROCK, PLACED TO A DEPTH OF 1' AND MUST MEET THE FOLLOWING SPECIFICATIONS: 4"-8" ROCK/40%-70% PASSING; 2"-4" ROCK/30%-40% PASSING; AND 1"-2" ROCK/10%-20% PASSING. RECYCLED CONCRETE SHALL NOT BE USED FOR EROSION PROTECTION, INCLUDING CONSTRUCTION ENTRANCE OR TEMPORARY STABILIZATION ELSEWHERE ON THE SITE.
- IF ANY PART(S) OF THE CLEARING LIMIT BOUNDARY OR TEMPORARY EROSION/SEDIMENTATION CONTROL PLAN IS/ARE DAMAGED, IT SHALL BE REPAIRED IMMEDIATELY.
- ALL PROPERTIES ADJACENT TO THE PROJECT SITE SHALL BE PROTECTED FROM SEDIMENT DEPOSITION AND RUNOFF.
- AT NO TIME SHALL MORE THAN 1' OF SEDIMENT BE ALLOWED TO ACCUMULATE WITHIN A CATCH BASIN. ALL CATCH BASINS AND CONVEYANCE LINES SHALL BE CLEANED IMMEDIATELY FOLLOWING REMOVAL OF EROSION CONTROL BMP'S. THE CLEANING OPERATION SHALL NOT FLUSH SEDIMENT-LADEN WATER INTO THE DOWNSTREAM SYSTEM.
- ANY PERMANENT RETENTION/DETENTION FACILITY USED AS A TEMPORARY SETTLING BASIN SHALL BE MODIFIED WITH THE NECESSARY EROSION CONTROL MEASURES AND SHALL PROVIDE ADEQUATE STORAGE CAPACITY. IF THE PERMANENT FACILITY IS TO FUNCTION ULTIMATELY AS AN INFILTRATION OR DISPERSION SYSTEM, THE FACILITY SHALL NOT BE USED AS A TEMPORARY SETTLING BASIN. NO UNDERGROUND DETENTION TANK, DETENTION VAULT, OR SYSTEM WHICH BACKS UNDER OR INTO A POND SHALL BE USED AS A TEMPORARY SETTLING BASIN.
- ALL EROSION/SEDIMENTATION CONTROL PONDS WITH A DEAD STORAGE DEPTH EXCEEDING 6" MUST HAVE A PERIMETER FENCE WITH A MINIMUM HEIGHT OF 3'.
- THE WASHED GRAVEL BACKFILL ADJACENT TO THE FILTER FABRIC FENCE SHALL BE REPLACED AND THE FILTER FABRIC CLEANED IF IT IS NONFUNCTIONAL BY EXCESSIVE SILT ACCUMULATION AS DETERMINED BY THE CITY OF KIRKLAND. ALSO, ALL INTERCEPTOR SWALES SHALL BE CLEANED IF SILT ACCUMULATION EXCEEDS ONE-QUARTER DEPTH.
- PRIOR TO THE OCTOBER 1 OF EACH YEAR (THE BEGINNING OF THE WET SEASON), ALL DISTURBED AREAS SHALL BE REVIEWED TO IDENTIFY WHICH ONES CAN BE SEEDED IN PREPARATION FOR THE WINTER RAINS. THE IDENTIFIED DISTURBED AREA SHALL BE SEEDED WITHIN ONE WEEK AFTER OCTOBER 1. A SITE PLAN DEPICTING THE AREAS TO BE SEEDED AND THE AREAS TO REMAIN UNCOVERED SHALL BE SUBMITTED TO THE PUBLIC WORKS CONSTRUCTION INSPECTOR. THE INSPECTOR CAN REQUIRE SEEDING OF ADDITIONAL AREAS IN ORDER TO PROTECT SURFACE WATERS, ADJACENT PROPERTIES, OR DRAINAGE FACILITIES.
- ANY AREA TO BE USED FOR INFILTRATION OR PERVIOUS PAVEMENT (INCLUDING A 5-FOOT BUFFER) MUST BE SURROUNDED BY SILT FENCE PRIOR TO CONSTRUCTION AND UNTIL FINAL STABILIZATION OF THE SITE TO PREVENT SOIL COMPACTION AND SILTATION BY CONSTRUCTION ACTIVITIES.
- IF THE TEMPORARY CONSTRUCTION ENTRANCE OR ANY OTHER AREA WITH HEAVY VEHICLE LOADING IS LOCATED IN THE SAME AREA TO BE USED FOR INFILTRATION OR PERVIOUS PAVEMENT, 6" OF SEDIMENT BELOW THE GRAVEL SHALL BE REMOVED PRIOR TO INSTALLATION OF THE INFILTRATION FACILITY OR PERVIOUS PAVEMENT (TO REMOVE FINES ACCUMULATED DURING CONSTRUCTION).
- ANY CATCH BASINS COLLECTING RUNOFF FROM THE SITE, WHETHER THEY ARE ON OR OFF THE SITE, SHALL HAVE ADEQUATE PROTECTION FROM SEDIMENT. CATCH BASINS DIRECTLY DOWNSTREAM OF THE CONSTRUCTION ENTRANCE OR ANY OTHER CATCH BASIN AS DETERMINED BY THE CITY INSPECTOR SHALL BE PROTECTED WITH A "STORM DRAIN PROTECTION INSERT" OR EQUIVALENT.
- IF A SEDIMENT POND IS NOT PROPOSED, A BAKER TANK OR OTHER TEMPORARY GROUND AND/OR SURFACE WATER STORAGE TANK MAY BE REQUIRED DURING CONSTRUCTION, DEPENDING ON WEATHER CONDITIONS.
- DO NOT FLUSH CONCRETE BY PRODUCTS OR TRUCKS NEAR OR INTO THE STORM DRAINAGE SYSTEM. IF EXPOSED AGGREGATE IS FLUSHED INTO THE STORM SYSTEM, IT COULD MEAN RE-CLEANING THE ENTIRE DOWNSTREAM STORM SYSTEM, OR POSSIBLY RE-LAYING THE STORM LINE.
- RECYCLED CONCRETE SHALL NOT BE STOCKPILED ON SITE, UNLESS FULLY COVERED WITH NO POTENTIAL FOR RELEASE OF RUNOFF.

**CITY OF KIRKLAND STANDARD
STORM DRAINAGE NOTES**

- A PRE-CONSTRUCTION CONFERENCE SHALL BE HELD PRIOR TO THE START OF CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR SECURING ALL NECESSARY PERMITS PRIOR TO CONSTRUCTION.
- BEFORE ANY CONSTRUCTION MAY OCCUR, THE CONTRACTOR SHALL HAVE PLANS WHICH HAVE BEEN SIGNED AND APPROVED BY THE CITY OF KIRKLAND PUBLIC WORKS DEPARTMENT, OBTAINED ALL CITY, COUNTY, STATE, FEDERAL AND OTHER REQUIRED PERMITS, AND HAVE POSTED ALL REQUIRED BONDS.
- ALL STORM DRAINAGE IMPROVEMENTS SHALL BE DESIGNED AND CONSTRUCTED IN ACCORDANCE WITH THE LATEST EDITION OF THE CITY OF KIRKLAND PUBLIC WORKS PRE-APPROVED PLANS AND POLICIES AND THE STANDARD SPECIFICATIONS FOR ROAD, BRIDGE AND MUNICIPAL CONSTRUCTION, PREPARED BY WSDOT AND THE AMERICAN PUBLIC WORKS ASSOCIATION (APWA).
- ANY DEVIATION FROM THE APPROVED PLANS WILL REQUIRE WRITTEN APPROVAL. ALL CHANGES SHALL BE SUBMITTED TO THE CITY.
- A COPY OF THE APPROVED STORM WATER PLANS MUST BE ON THE JOB SITE WHENEVER CONSTRUCTION IS IN PROGRESS.
- ALL DISTURBED AREAS SHALL BE SEEDED AND MULCHED OR SIMILARLY STABILIZED TO THE SATISFACTION OF THE CITY OF KIRKLAND DEPARTMENT OF PUBLIC WORKS FOR THE PREVENTION OF ON-SITE EROSION AFTER THE COMPLETION OF CONSTRUCTION.
- MINIMUM COVER OVER STORM DRAINAGE PIPES IN ROW OR VEHICULAR PATH SHALL BE 18 INCHES, UNLESS OTHER DESIGN IS TREATED.
- STEEL PIPE SHALL HAVE ASPHALT TREATMENT #1 OR BETTER INSIDE AND OUTSIDE.
- ALL CATCH BASINS SHALL BE TYPE I UNLESS OTHERWISE NOTED. CATCH BASINS WITH A DEPTH OF OVER FIVE FEET (5') TO THE PIPE INVERT SHALL BE A TYPE II CATCH BASIN. TYPE II CATCH BASINS EXCEEDING FIVE FEET (5') IN DEPTH SHALL HAVE A STANDARD LADDER INSTALLED.
- ALL STORM DRAINAGE MAIN EXTENSIONS WITHIN THE PUBLIC RIGHT-OF-WAY OR IN EASEMENTS MUST BE STAKED FOR LINE AND GRADE PRIOR TO STARTING CONSTRUCTION.
- ROCK FOR EROSION PROTECTION OF ROADWAY DITCHES, WHERE REQUIRED, MUST BE OF SOUND QUARRY ROCK, PLACED TO A DEPTH OF ONE FOOT (1') AND MUST MEET THE FOLLOWING SPECIFICATIONS: 4"-8" ROCK/40%-70% PASSING; 2"-4" ROCK/30%-40% PASSING; 2"-MINUS ROCK/10%-20% PASSING. RECYCLED CONCRETE SHALL NOT BE USED FOR EROSION PROTECTION, INCLUDING FOR CONSTRUCTION ENTRANCE OR TEMPORARY STABILIZATION ELSEWHERE ON SITE.
- ALL PIPE, MANHOLES, CATCH BASINS, AND APPURTENANCES SHALL BE LAID ON A PROPERLY PREPARED FOUNDATION IN ACCORDANCE WITH THE CURRENT STATE OF WASHINGTON STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION (WSDOT). THIS SHALL INCLUDE NECESSARY LEVELING OF THE TRENCH BOTTOM OR THE TOP OF THE FOUNDATION MATERIAL AS WELL AS PLACEMENT AND COMPACTION OF REQUIRED BEDDING MATERIAL TO UNIFORM GRADE SO THAT THE ENTIRE LENGTH OF THE PIPE WILL BE SUPPORTED ON A UNIFORMLY DENSE, UNYIELDING BASE. IF THE NATIVE MATERIAL IN THE BOTTOM OF THE TRENCH MEETS THE REQUIREMENTS FOR "GRAVEL BACKFILL FOR PIPE BEDDING," THE FIRST LIFT OF PIPE BEDDING MAY BE OMITTED PROVIDED THE MATERIAL IN THE BOTTOM OF THE TRENCH IS LOOSENEED, REGRADED, AND COMPACTED TO FORM A DENSE UNYIELDING BASE. ALL PIPE BEDDING SHALL BE APWA CLASS B, TYPE I, OR BETTER. PIPE SHALL NOT BE INSTALLED ON SOD, FROZEN EARTH, LARGE BOULDERS, OR ROCK. PIPE BEDDING FOR FLEXIBLE PIPES SHALL BE PEA GRAVEL TO THE SPRINGLINE OF THE PIPE.
- CONSTRUCTION OF DEWATERING DISCHARGES SHALL ALWAYS MEET WATER QUALITY GUIDELINES LISTED IN COK POLICY E-1. SPECIFICALLY, DISCHARGES TO THE PUBLIC STORMWATER DRAINAGE SYSTEM MUST BE BELOW 25NTU, AND NOT CONSIDERED A PROHIBITED DISCHARGE (PER KMC 15.52.090). TEMPORARY DISCHARGES TO SANITARY SEWER REQUIRE PRIOR AUTHORIZATION AND PERMIT FROM KING COUNTY INDUSTRIAL WASTE PROGRAM (206-263-3000) AND NOTIFICATION TO THE PUBLIC WORKS CONSTRUCTION INSPECTOR.
- ISSUANCE OF A BUILDING OR LAND SURFACE MODIFICATION PERMIT BY THE CITY OF KIRKLAND DOES NOT RELIEVE THE OWNER OF THE CONTINUING LEGAL OBLIGATION AND/OR LIABILITY CONNECTED WITH STORM SURFACE WATER DISPOSITION. FURTHER, THE CITY OF KIRKLAND DOES NOT ACCEPT ANY OBLIGATION FOR THE PROPER FUNCTIONING AND MAINTENANCE OF THE SYSTEM DURING OR FOLLOWING CONSTRUCTION EXCEPT AS OUTLINED IN THE CITY OF KIRKLAND PUBLIC WORKS STANDARDS.
- ALL TRENCH BACKFILL SHALL BE COMPACTED TO 95 PERCENT DENSITY IN ROADWAYS, ROADWAY SHOULDERS, ROADWAY PRISM AND DRIVEWAYS, AND 85 PERCENT DENSITY IN UNPAVED AREAS. ALL PIPE ZONE COMPACTION SHALL BE 95 PERCENT.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING ADEQUATE SAFEGUARDS, SAFETY DEVICES, PROTECTIVE EQUIPMENT, CONFINED SPACE PROTECTION, FLAGGERS, AND ANY OTHER NEEDED ACTIONS TO PROTECT THE LIFE, HEALTH, AND SAFETY OF THE PUBLIC, AND TO PROTECT PROPERTY IN CONNECTION WITH THE PERFORMANCE OF WORK COVERED BY THE CONTRACT. ANY WORK WITHIN THE TRAVELED RIGHT-OF-WAY THAT MAY INTERRUPT NORMAL TRAFFIC FLOW SHALL REQUIRE A TRAFFIC CONTROL PLAN APPROVED BY THE CITY OF KIRKLAND. ALL SECTIONS OF THE WSDOT STANDARD SPECIFICATIONS, TRAFFIC CONTROL, AND THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) SHALL APPLY.
- NO FINAL CUT OR FILL SLOPE SHALL EXCEED SLOPES OF TWO (2) HORIZONTAL TO ONE (1) VERTICAL WITHOUT STABILIZATION BY ROCKERY OR BY A STRUCTURAL RETAINING WALL.
- ALL MANHOLE LADDERS SHALL BE FIRMLY ATTACHED AND EXTEND TO WITHIN 1' OF THE BOTTOM OF THE STRUCTURE.
- APPROXIMATE LOCATIONS OF EXISTING UTILITIES HAVE BEEN OBTAINED FROM AVAILABLE RECORDS AND ARE SHOWN FOR CONVENIENCE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFICATION OF EXISTING UTILITY LOCATIONS WHETHER OR NOT THESE UTILITIES ARE SHOWN ON THE PLANS. THE CONTRACTOR SHALL EXERCISE ALL CARE TO AVOID DAMAGE TO ANY UTILITY IF CONFLICTS WITH EXISTING UTILITIES ARISE DURING CONSTRUCTION. THE CONTRACTOR SHALL NOTIFY THE CITY CONSTRUCTION INSPECTOR AND ANY CHANGES REQUIRED SHALL BE APPROVED BY THE DEVELOPMENT ENGINEER PRIOR TO COMMENCEMENT OF RELATED CONSTRUCTION ON THE PROJECT.
- THE UNDERGROUND UTILITY LOCATION SERVICE SHALL BE CONTACTED FOR FIELD LOCATION OF EXISTING UTILITIES PRIOR TO ANY CONSTRUCTION. THE OWNER OR HIS REPRESENTATIVE SHALL BE CONTACTED IF A UTILITY CONFLICT EXISTS. FOR UTILITY LOCATION IN KING COUNTY, CALL 1-800-424-5555. THE CONTRACTOR IS RESPONSIBLE TO ENSURE THAT UTILITY LOCATES ARE MAINTAINED THROUGHOUT THE LIFE OF THE PROJECT.

**CITY OF KIRKLAND STANDARD
STORM DRAINAGE NOTES (CONT)**

- THE CONTRACTOR SHALL VERIFY THE LOCATIONS, WIDTHS, THICKNESSES, AND ELEVATIONS OF ALL EXISTING PAVEMENTS AND STRUCTURES THAT ARE TO INTERFACE WITH NEW WORK. PROVIDE ALL TRIMMING, CUTTING, SAW CUTTING, GRADING, LEVELING, SLOPING, COATING, AND OTHER WORK, INCLUDING MATERIALS AS NECESSARY, TO CAUSE THE INTERFACE WITH EXISTING WORKS TO BE PROPER, ACCEPTABLE TO THE ENGINEER AND THE CITY OF KIRKLAND, COMPLETE IN PLACE AND READY TO USE.
- ALL INLET, MANHOLE, AND CATCH BASIN FRAMES AND GRATES SHALL NOT BE ADJUSTED TO GRADE UNTIL IMMEDIATELY PRIOR TO FINAL PAVING. ALL CATCH BASIN GRATES SHALL BE SET 0.10' BELOW PAVEMENT LEVEL.
- OPEN CUT ROAD CROSSINGS FOR UTILITY TRENCHES ON EXISTING TRAVELED ROADWAY SHALL BE BACKFILLED ONLY WITH 8" MINUS CRUSHED ROCK AND MECHANICALLY COMPACTED (UNLESS OTHERWISE APPROVED BY THE CITY) FOR STREETS CLASSIFIED AS ARTERIALS OR COLLECTORS. BACKFILL FOR CROSSINGS SHALL BE CONTROL DENSITY FILL (CDF). CUTS INTO THE EXISTING ASPHALT SHALL BE NEAT LINE CUT WITH SAW OR JACKHAMMER IN A CONTINUOUS LINE. A TEMPORARY COLD MIX PATCH MUST BE PLACED IMMEDIATELY AFTER BACKFILL AND COMPACTION. A PERMANENT HOT MIX PATCH SHALL BE PLACED WITHIN 30 DAYS AND SHALL BE A MINIMUM OF 1" THICKER THAN THE ORIGINAL ASPHALT WITH A MINIMUM THICKNESS OF 2". SEE STANDARD D.02.
- ALL DAMAGES INCURRED TO PUBLIC AND/OR PRIVATE PROPERTY BY THE CONTRACTOR DURING THE COURSE OF CONSTRUCTION SHALL BE PROMPTLY REPAIRED TO THE SATISFACTION OF THE CITY CONSTRUCTION INSPECTOR BEFORE PROJECT APPROVAL AND/OR THE RELEASE OF THE PROJECT'S PERFORMANCE BOND.
- GROUT ALL SEAMS AND OPENINGS IN ALL INLETS, CATCH BASINS, AND MANHOLES. JETSET GROUT IS NOT ALLOWED.
- WHEN WIDENING AN EXISTING ROADWAY WHERE AN EXISTING TYPE I CATCH BASIN WILL REMAIN IN THE TRAVEL LANE, THE EXISTING FRAME AND COVER SHALL BE REPLACED WITH A ROUND, LOCKING FRAME AND COVER.
- FOR OTHER THAN SINGLE-FAMILY DWELLINGS, ALL EXPOSED OR READILY EXPOSED INDOOR STORM DRAINAGE PIPING/PLUMBING SHALL BE LABELED WITH THE WORDS "STORM DRAIN" WITH MINIMUM 2 INCH HIGH LETTERS.
- RECYCLED CONCRETE SHALL NOT BE USED AROUND STORMWATER FACILITIES.

CHANNELIZATION AND SIGNING NOTES

- THE CONTRACTOR SHALL FURNISH AND INSTALL ALL SIGNAGE UNLESS NOTED OTHERWISE ON THE PLANS.
- NOT USED.
- ALL STRIPING LINES SHALL BE PAINT. ALL OTHER PAVEMENT MARKINGS SHALL BE PLASTIC.
- LOCATION SIGNALS SHALL BE VERIFIED BY THE ENGINEER PRIOR TO INSTALLATION. SIGNAL LOCATIONS SHOWN ARE APPROXIMATE ONLY. ACTUAL LOCATIONS SHALL BE DETERMINED IN THE FIELD BASED ON CONDITIONS. SIGNALS SHALL NOT BE PLACED TO CONFLICT WITH DRIVEWAYS, UTILITIES, OR VEGETATION. SIGNALS SHALL NOT BE PLACED WHERE SIGHT LINES TO THE SIGN IS BLOCKED BY TREES.
- CONTRACTOR SHALL REMOVE EXISTING PAVEMENT STRIPING, MARKING AND SIGNAGE IN CONFLICT WITH NEW CHANNELIZATION.
- ALL CHANNELIZATION DIMENSIONS ADJACENT TO CURB ARE MEASURED TO THE FACE OF CURB.
- SOLID BICYCLE LANE LINE STRIPES ARE WHITE, 6 INCHES WIDE UNLESS OTHERWISE NOTED.

LANDSCAPE AND RESTORATION NOTES

- PROTECT EXISTING TREES PER SITE PREPARATION NOTES AND PLANS. PROTECT ALL VEGETATION ON PRIVATE PROPERTY.
- TRIM AND REMOVE VEGETATION AND TREES PER SITE PREPARATION NOTES AND PLANS.
- RESTORE EXISTING LANDSCAPE IMPACTED BY CONSTRUCTION TO EXISTING CONDITIONS OR BETTER PER SPECIFICATIONS. RESTORED LANDSCAPE TO MEET AND MATCH EXISTING LANDSCAPE GRADES AND PAVEMENT EDGES.



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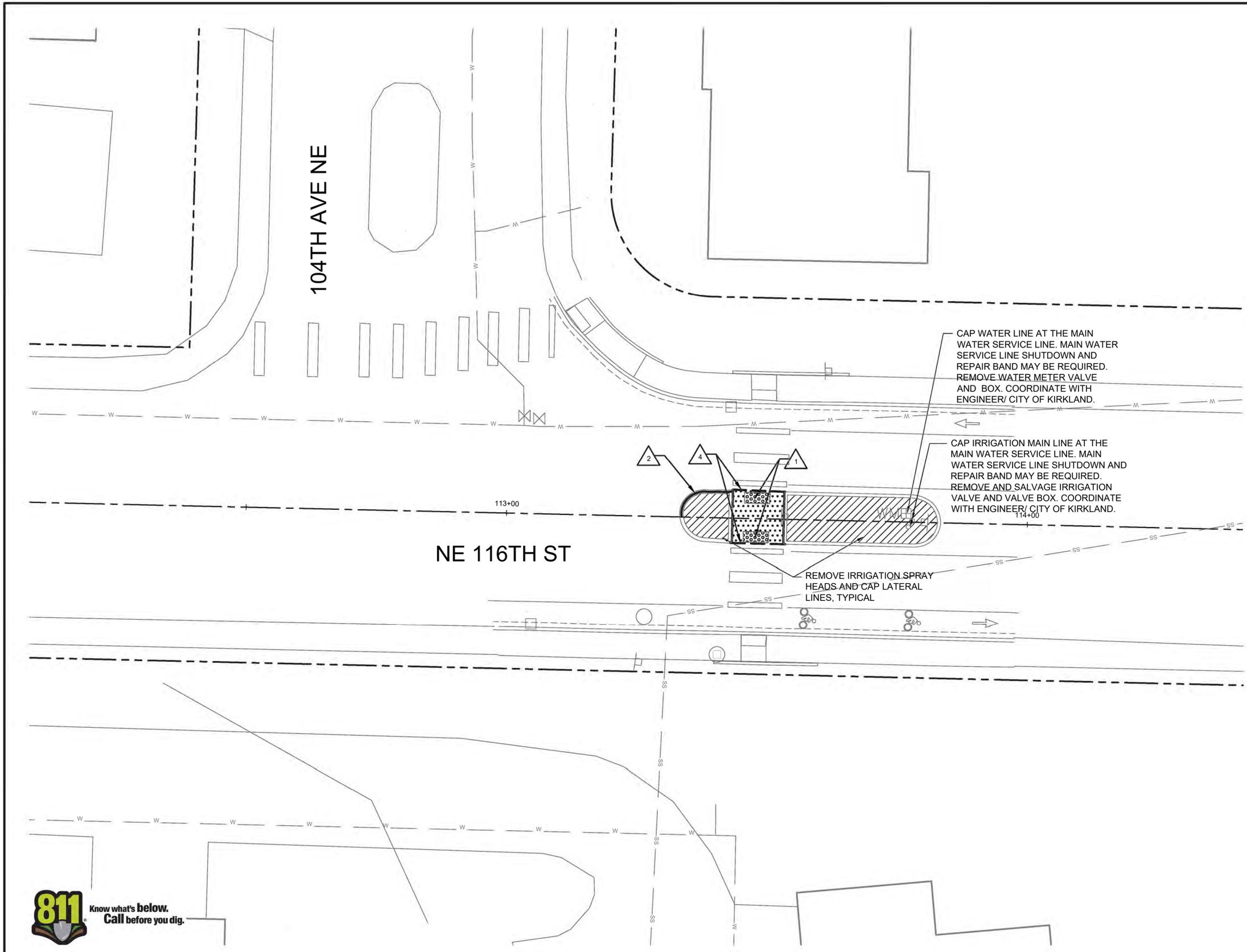


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NE 116TH STREET
CROSSWALK UPGRADES
KIRKLAND, WA

GENERAL NOTES

PROJECT NO.	NMC0120100
DATE	09/01/2020
DRAWING NO.	NT2
SHEET NO.	3 / 15



- SITE PREPARATION NOTES:**
- SEE NT SHEET SERIES FOR GENERAL NOTES AND LEGEND.
 - SEE SG SHEETS FOR TRENCHING FOR RRFB INSTALLATION.
 - ALL DIMENSIONS ARE IN FEET, UNLESS OTHERWISE NOTED.
 - UNDERGROUND UTILITY LOCATIONS SHOWN ARE APPROXIMATE BASED ON CITY OF KIRKLAND GIS DATA. LOCATIONS OF POTENTIAL CONFLICT MUST BE POTHOLED PRIOR TO TRENCHING.
 - ADDITIONAL UNDERGROUND UTILITIES MAY EXIST THAT ARE NOT DEPICTED ON THESE PLANS. CONTRACTOR SHALL COORDINATE WITH THE ENGINEER TO DETERMINE IF ADDITIONAL UNDERGROUND UTILITIES MAY EXIST WITHIN THE LIMITS OF WORK.
 - CALL UTILITIES UNDERGROUND LOCATION CENTER AT 1-800-424-5555 48 HOURS PRIOR TO CONSTRUCTION.
 - DRIVEWAY ACCESS MUST BE MAINTAINED AT ALL TIMES UNLESS OTHERWISE AGREED TO BY THE CITY OF KIRKLAND.
 - PAVEMENT TO BE SAWCUT WHEN REMOVAL LIMIT ABUTS PAVEMENT TO REMAIN IN PLACE.
 - VEGETATION PROTECTION AND RESTORATION PER SPECIFICATIONS. SAVE AND PROTECT ALL EXISTING VEGETATION OUTSIDE THE CLEARING AND GRUBBING LIMITS.
 - TREE CRITICAL ROOT ZONE (CRZ) IS THE AREA EQUAL TO A 1-FOOT RADIUS FROM THE BASE OF THE TREE'S TRUNK FOR EACH 1-INCH OF THE TREE'S TRUNK DIAMETER TAKEN 4.5-FEET ABOVE GRADE. ALL WORK IN CRZ SHALL BE DONE BY HAND. SEE STANDARD PLAN NO. CK-R-49.
 - SEE SPECIFICATIONS FOR TREE AND VEGETATION TRIMMING AND PRUNING REQUIREMENTS. ALL PRUNING SHALL BE PERFORMED BY A CERTIFIED ARBORIST AT THE DIRECTION OF THE ENGINEER. PRUNING AND TRIMMING MAY BE REQUIRED BY CITY OF KIRKLAND FOLLOWING SIGN INSTALLATION TO PROVIDE VISIBILITY OF SIGNS.
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 - THE CONSTRUCTION INSPECTOR SHALL PERFORM A PROJECT WALK-THROUGH WITH WATER DEPARTMENT PERSONNEL PRIOR TO FINAL CURB AND ASPHALT REPLACEMENT/PLACEMENT.
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- SITE PREPARATION LEGEND:**
- RIGHT-OF-WAY
 - CLEARING AND GRUBBING
 - REMOVE DETECTABLE WARNING SURFACE
 - REMOVE ASPHALT PAVEMENT
 - REMOVE CONCRETE PAVEMENT
 - REMOVE DAMAGED CURB AS SHOWN OR AS DIRECTED BY ENGINEER
 - SAWCUT PAVEMENT

- SITE PREPARATION CONSTRUCTION NOTES:**
- REMOVE DETECTABLE WARNING SURFACE
 - REMOVE DAMAGED CURB AS SHOWN OR AS DIRECTED BY ENGINEER
 - TRIM / PRUNE TREE OR VEGETATION, SEE NOTE 11
 - SAW CUT PAVEMENT FULL DEPTH



CITY OF KIRKLAND
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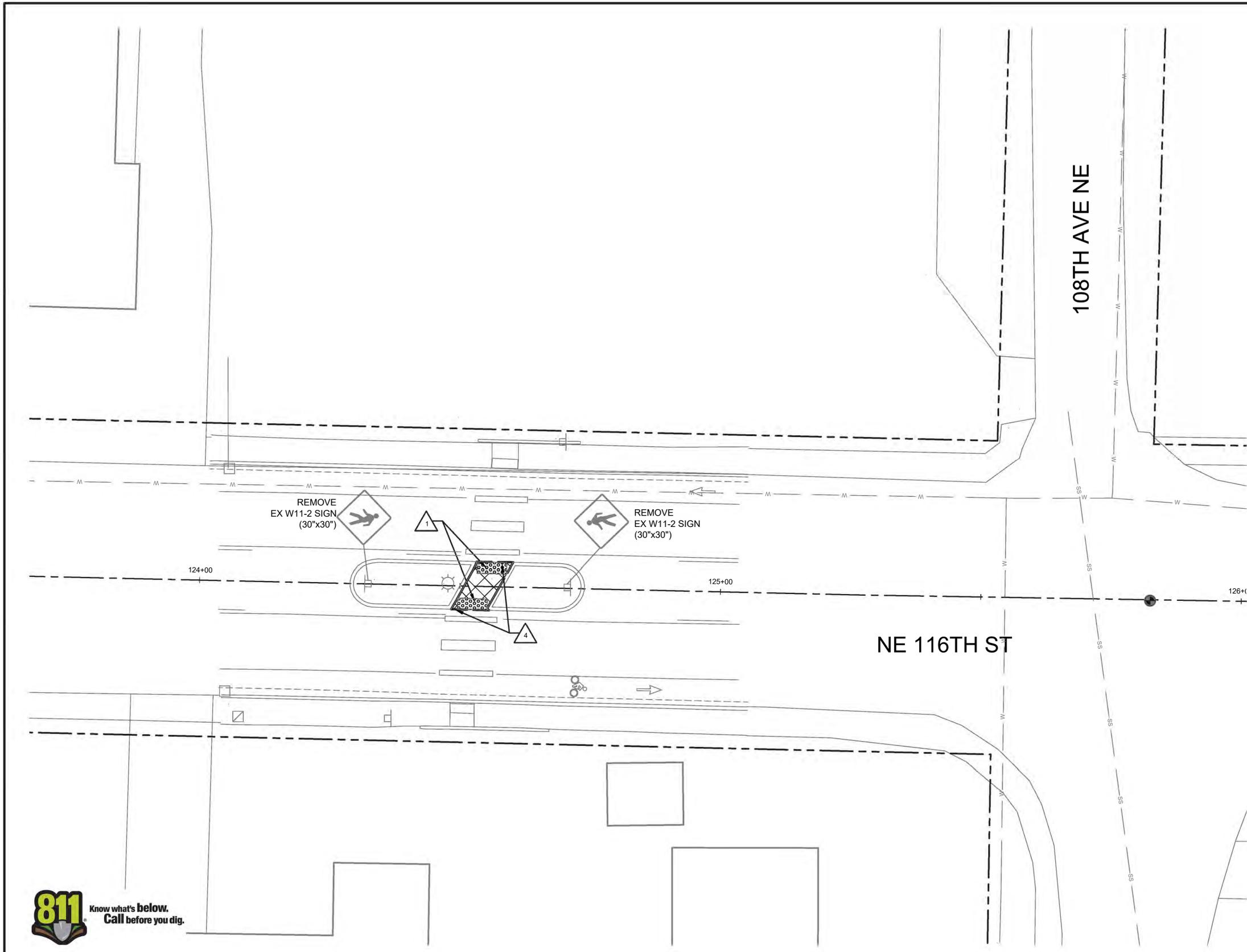
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**NE 116TH STREET
 CROSSWALK UPGRADES
 KIRKLAND, WA**

SCALE IN FEET

**SITE PREPARATION PLAN
 (SCHEDULE A - BASE BID)**

PROJECT NO.	NMC0120100
DATE	09/01/2020
DRAWING NO.	SP1
SHEET NO.	4 / 15



- SITE PREPARATION NOTES:**
- SEE NT SHEET SERIES FOR GENERAL NOTES AND LEGEND.
 - SEE SG SHEETS FOR TRENCHING FOR RRFB INSTALLATION.
 - ALL DIMENSIONS ARE IN FEET, UNLESS OTHERWISE NOTED.
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 - CALL UTILITIES UNDERGROUND LOCATION CENTER AT 1-800-424-5555 48 HOURS PRIOR TO CONSTRUCTION.
 - DRIVEWAY ACCESS MUST BE MAINTAINED AT ALL TIMES UNLESS OTHERWISE AGREED TO BY THE CITY OF KIRKLAND.
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 - VEGETATION PROTECTION AND RESTORATION PER SPECIFICATIONS. SAVE AND PROTECT ALL EXISTING VEGETATION OUTSIDE THE CLEARING AND GRUBBING LIMITS.
 - TREE CRITICAL ROOT ZONE (CRZ) IS THE AREA EQUAL TO A 1-FOOT RADIUS FROM THE BASE OF THE TREE'S TRUNK FOR EACH 1-INCH OF THE TREE'S TRUNK DIAMETER TAKEN 4.5- FEET ABOVE GRADE. ALL WORK IN CRZ SHALL BE DONE BY HAND. SEE STANDARD PLAN NO. CK-R-49.
 - SEE SPECIFICATIONS FOR TREE AND VEGETATION TRIMMING AND PRUNING REQUIREMENTS. ALL PRUNING SHALL BE PERFORMED BY A CERTIFIED ARBORIST AT THE DIRECTION OF THE ENGINEER. PRUNING AND TRIMMING MAY BE REQUIRED BY CITY OF KIRKLAND FOLLOWING SIGN INSTALLATION TO PROVIDE VISIBILITY OF SIGNS.
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- SITE PREPARATION LEGEND:**
- RIGHT-OF-WAY
 - [Hatched Box] CLEARING AND GRUBBING
 - [Dotted Box] REMOVE DETECTABLE WARNING SURFACE
 - [Cross-hatched Box] REMOVE ASPHALT PAVEMENT
 - [Diagonal-hatched Box] REMOVE CONCRETE PAVEMENT
 - [Solid Line] REMOVE DAMAGED CURB AS SHOWN OR AS DIRECTED BY ENGINEER
 - SAWCUT PAVEMENT

- SITE PREPARATION CONSTRUCTION NOTES:**
- 1 REMOVE DETECTABLE WARNING SURFACE
 - 2 REMOVE DAMAGED CURB AS SHOWN OR AS DIRECTED BY ENGINEER
 - 3 TRIM / PRUNE TREE OR VEGETATION, SEE NOTE 11
 - 4 SAW CUT PAVEMENT FULL DEPTH



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 CITY OF KIRKLAND
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**NE 116TH STREET
 CROSSWALK UPGRADES
 KIRKLAND, WA**

SCALE IN FEET

**SITE PREPARATION PLAN
 (SCHEDULE A - BASE BID)**

PROJECT NO.	NMC0120100
DATE	09/01/2020
DRAWING NO.	SP2
SHEET NO.	5 / 15



SITE PREPARATION NOTES:

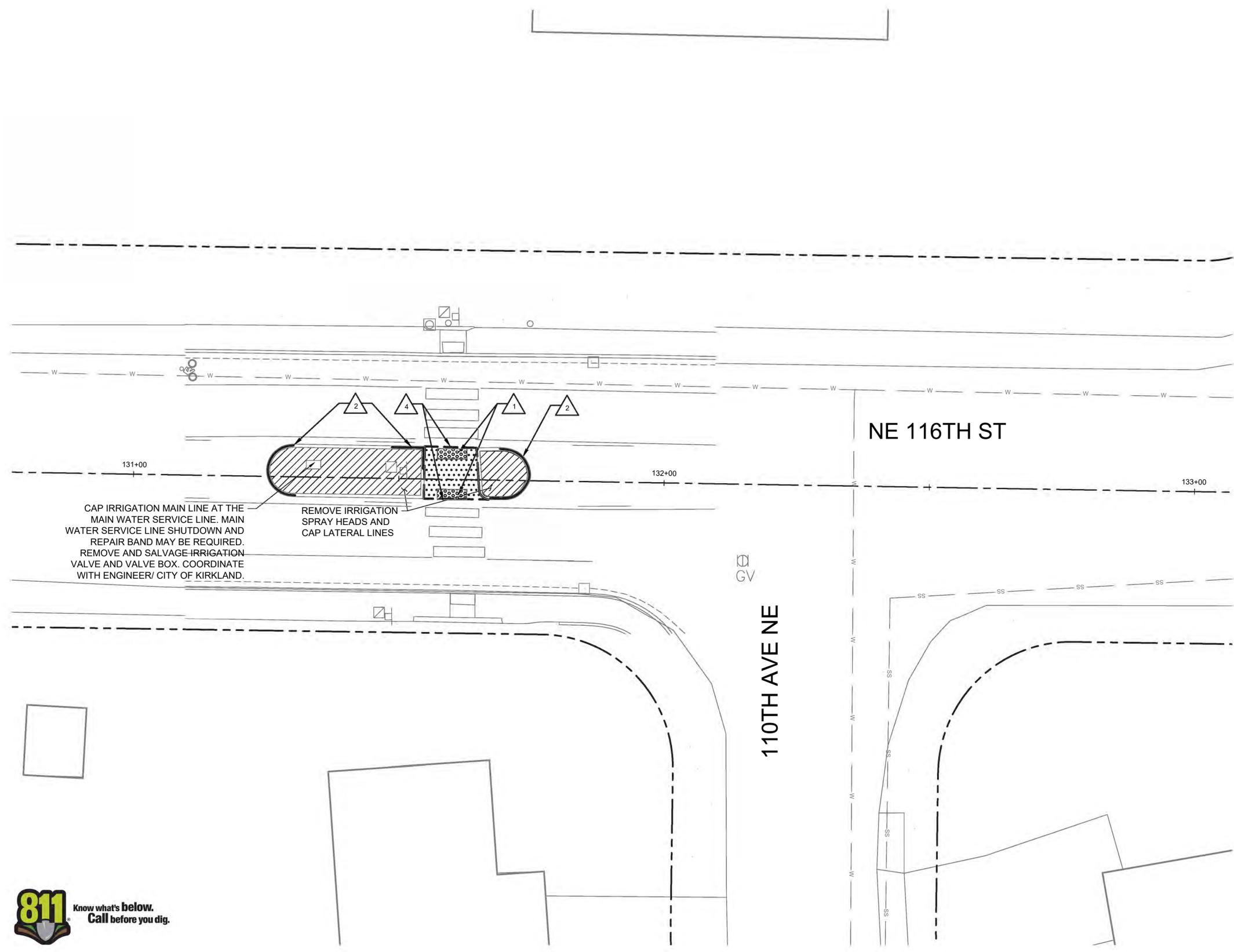
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SITE PREPARATION CONSTRUCTION NOTES:

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SCALE IN FEET

**SITE PREPARATION PLAN
(SCHEDULE A - BASE BID)**

PROJECT NO.	NMC0120100
DATE	09/01/2020
DRAWING NO.	SP3
SHEET NO.	6 / 15



SITE PREPARATION NOTES:

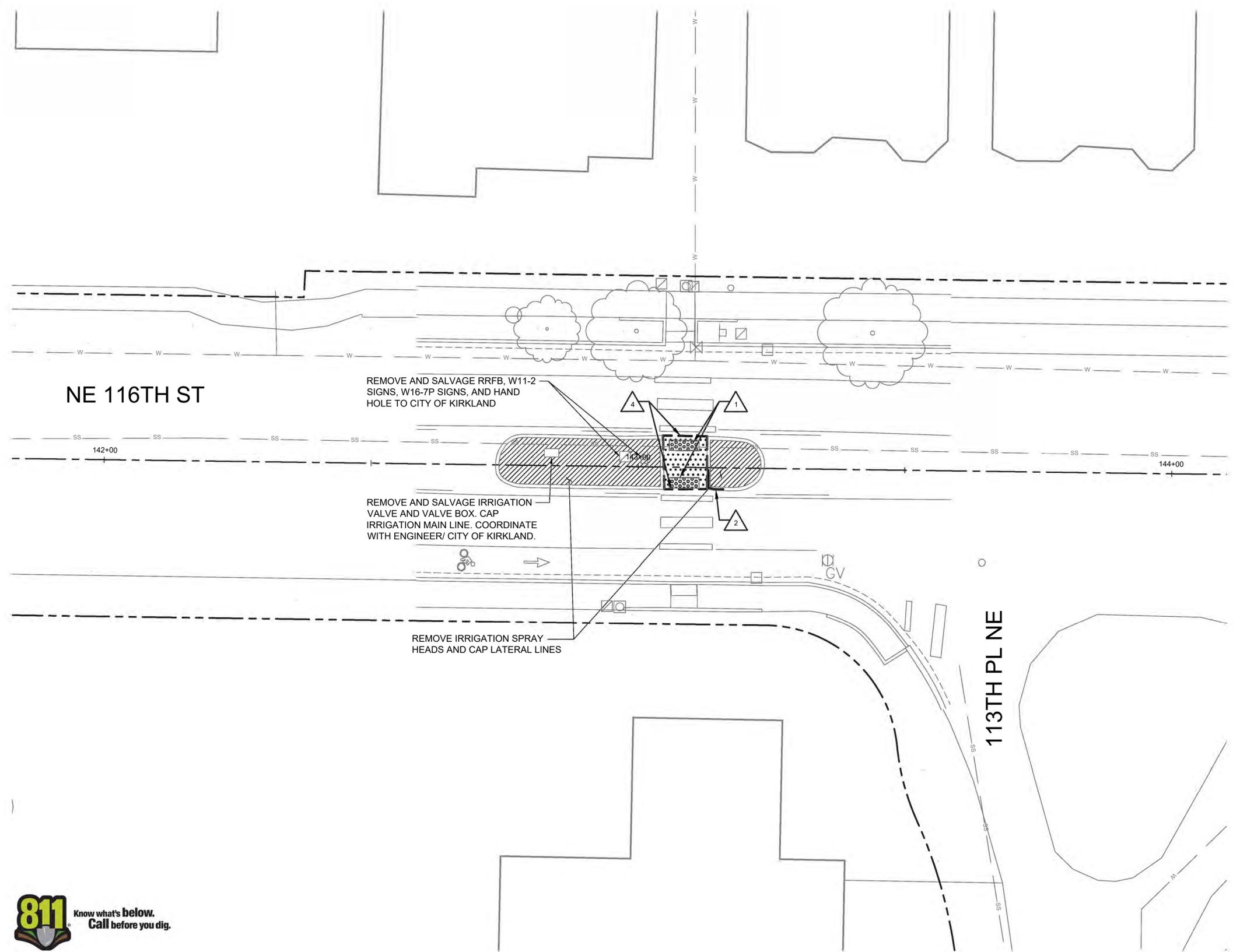
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16. PROJECT CLOSE-OUT GENERAL CONSTRUCTION:
 - THE CONSTRUCTION INSPECTOR SHALL PERFORM A PROJECT WALK-THROUGH WITH WATER DEPARTMENT PERSONNEL PRIOR TO FINAL CURB AND ASPHALT REPLACEMENT/PLACEMENT.
 - ALL ITEMS IDENTIFIED DURING THE INSPECTOR/WATER DEPARTMENT PERSONNEL WALK-THROUGH WILL BE INCORPORATED INTO THE ORIGINAL (FIRST) PUNCHLIST GIVEN TO THE CONTRACTOR.

SITE PREPARATION LEGEND:

- RIGHT-OF-WAY
- CLEARING AND GRUBBING
- REMOVE DETECTABLE WARNING SURFACE
- REMOVE ASPHALT PAVEMENT
- REMOVE CONCRETE PAVEMENT
- REMOVE DAMAGED CURB AS SHOWN OR AS DIRECTED BY ENGINEER
- SAWCUT PAVEMENT

SITE PREPARATION CONSTRUCTION NOTES:

- 1 REMOVE DETECTABLE WARNING SURFACE
- 2 REMOVE DAMAGED CURB AS SHOWN OR AS DIRECTED BY ENGINEER
- 3 TRIM / PRUNE TREE OR VEGETATION, SEE NOTE 11
- 4 SAW CUT PAVEMENT FULL DEPTH



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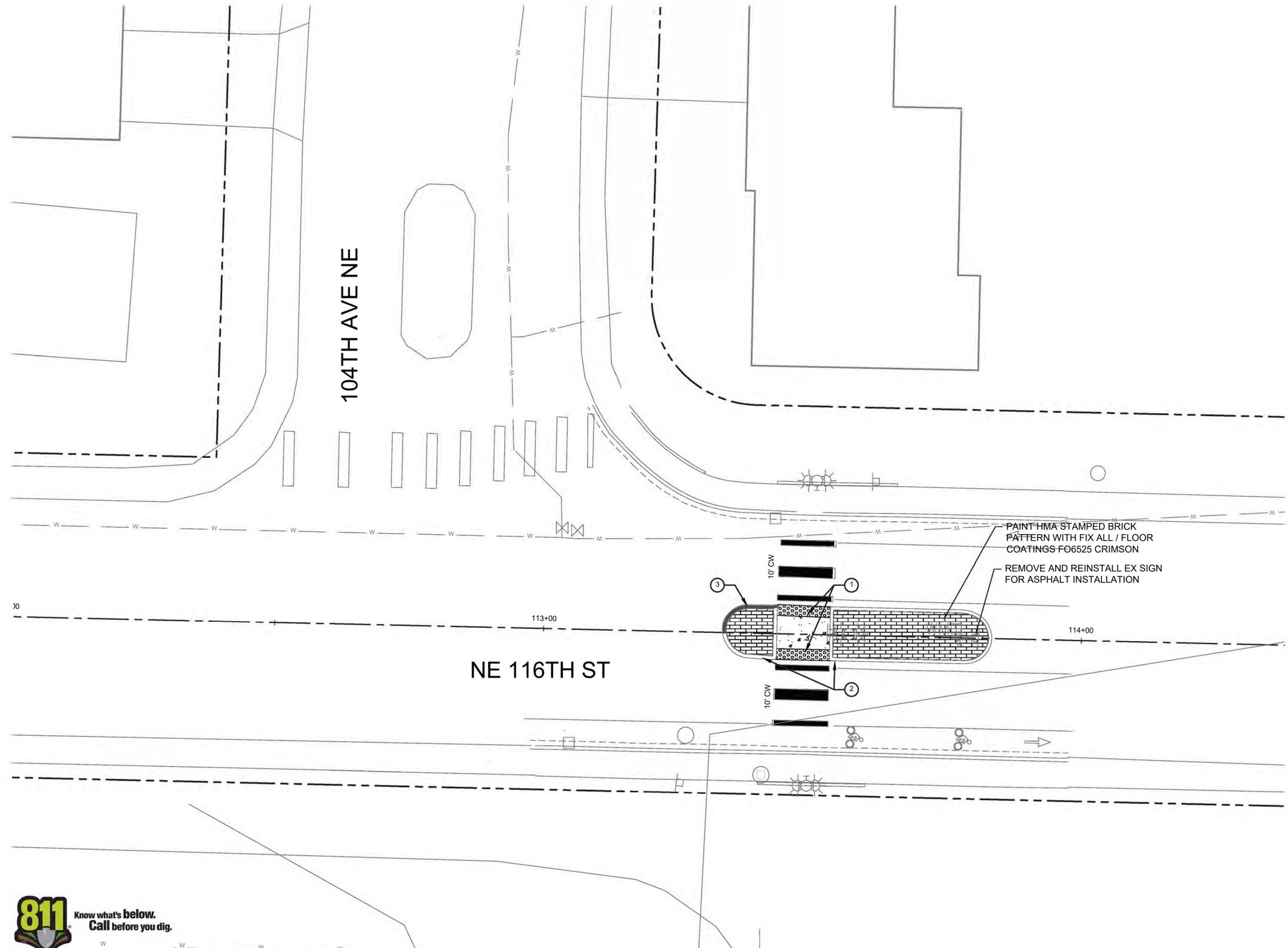
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**NE 116TH STREET
CROSSWALK UPGRADES
KIRKLAND, WA**

SCALE IN FEET

**SITE PREPARATION PLAN
(SCHEDULE A - BASE BID)**

PROJECT NO.	NMC0120100
DATE	09/01/2020
DRAWING NO.	SP4
SHEET NO.	7 / 15



- NOTES:**
- SEE NT SHEET SERIES FOR GENERAL NOTES AND LEGEND.
 - SEE SP SHEET SERIES FOR TREE PROTECTION, TREE AND VEGETATION TRIMMING, AND SIGNAGE AND CHANNELIZATION REMOVALS.
 - ALL DIMENSIONS ARE IN FEET, UNLESS OTHERWISE NOTED.
 - SIGN AND SYMBOL MARKING LOCATIONS SHALL BE VERIFIED BY THE ENGINEER PRIOR TO INSTALLATION. LOCATIONS SHOWN ARE APPROXIMATE ONLY. ACTUAL LOCATIONS SHALL BE DETERMINED IN THE FIELD BASED ON CONDITIONS. SIGNS SHALL NOT BE PLACED TO CONFLICT WITH DRIVEWAYS, UTILITIES, OR VEGETATION. SIGNS SHALL NOT BE PLACED WHERE SIGHT LINES TO THE SIGN ARE BLOCKED BY TREES.
 - UNDERGROUND UTILITY LOCATIONS SHOWN ARE APPROXIMATE BASED ON CITY OF KIRKLAND GIS DATA. LOCATIONS OF POTENTIAL CONFLICT MUST BE POTHOLED PRIOR TO TRENCHING.
 - ADDITIONAL UNDERGROUND UTILITIES MAY EXIST THAT ARE NOT DEPICTED ON THESE PLANS. CONTRACTOR SHALL COORDINATE WITH THE ENGINEER TO DETERMINE IF ADDITIONAL UNDERGROUND UTILITIES MAY EXIST WITHIN THE LIMITS OF WORK.
 - CALL UTILITIES UNDERGROUND LOCATION CENTER AT 1-800-424-5555 48 HOURS PRIOR TO CONSTRUCTION.

PAVING AND CHANNELIZATION LEGEND:

- RIGHT-OF-WAY
- CAST-IN-PLACE TRUNCATED DOME TACTILE WARNING SURFACE PER PRE-APPROVED PLAN NO. CK-R.25B
- FULL DEPTH HMA WITH STAMPED BRICK PATTERN (1 DT1)
- 4" DEPTH CLASS 4000 CEMENT CONCRETE PAVEMENT OVER 2" DEPTH 1-1/4" MINUS SUBBASE MATERIAL
- CLASS 3000 CEMENT CONCRETE EXTRUDED CURB PER PRE-APPROVED PLAN NO. CK-R.19.
- 10' CW PER PRE-APPROVED PLAN NO. CK-R.28 & CK-R.28B

PAVING AND CHANNELIZATION CONSTRUCTION NOTES:

- INSTALL DETECTABLE WARNING STRIP FULL WIDTH OF ISLAND OPENING. DETECTABLE WARNING STRIP SHALL CONFORM TO CITY OF KIRKLAND PRE-APPROVED PLAN CK-R.25. EMBED WARNING STRIP INTO FRESHLY-PLACED CONCRETE TO RUN FLUSH WITH ADJOINING WALKING SURFACES.
- PAINT CURB YELLOW AND APPLY GLASS TRAFFIC PAINT BEADS PER SPECIFICATIONS
- REPAIR CEMENT CONCRETE EXTRUDED CURB PER PRE-APPROVED PLAN NO. CK-R.19.
- RESTORE ASPHALT WHERE SIGN POST REMOVED
- POTHOLE LOCATIONS OF POTENTIAL UTILITY CONFLICT
- RESTORE LANDSCAPE, SIDEWALK PAVEMENT, ASPHALT PAVEMENT, AND EXTRUDED CURB TO EXISTING CONDITION OR PER PLANS AT RRFB SIGNAL POST AND TRENCHING LOCATIONS. SEE CITY OF KIRKLAND POLICY R-5, AND PRE-APPROVED PLANS CK-R.12, CK-R.13A, AND CK-R.23.



CITY OF KIRKLAND
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 PHONE: (425) 587-3000



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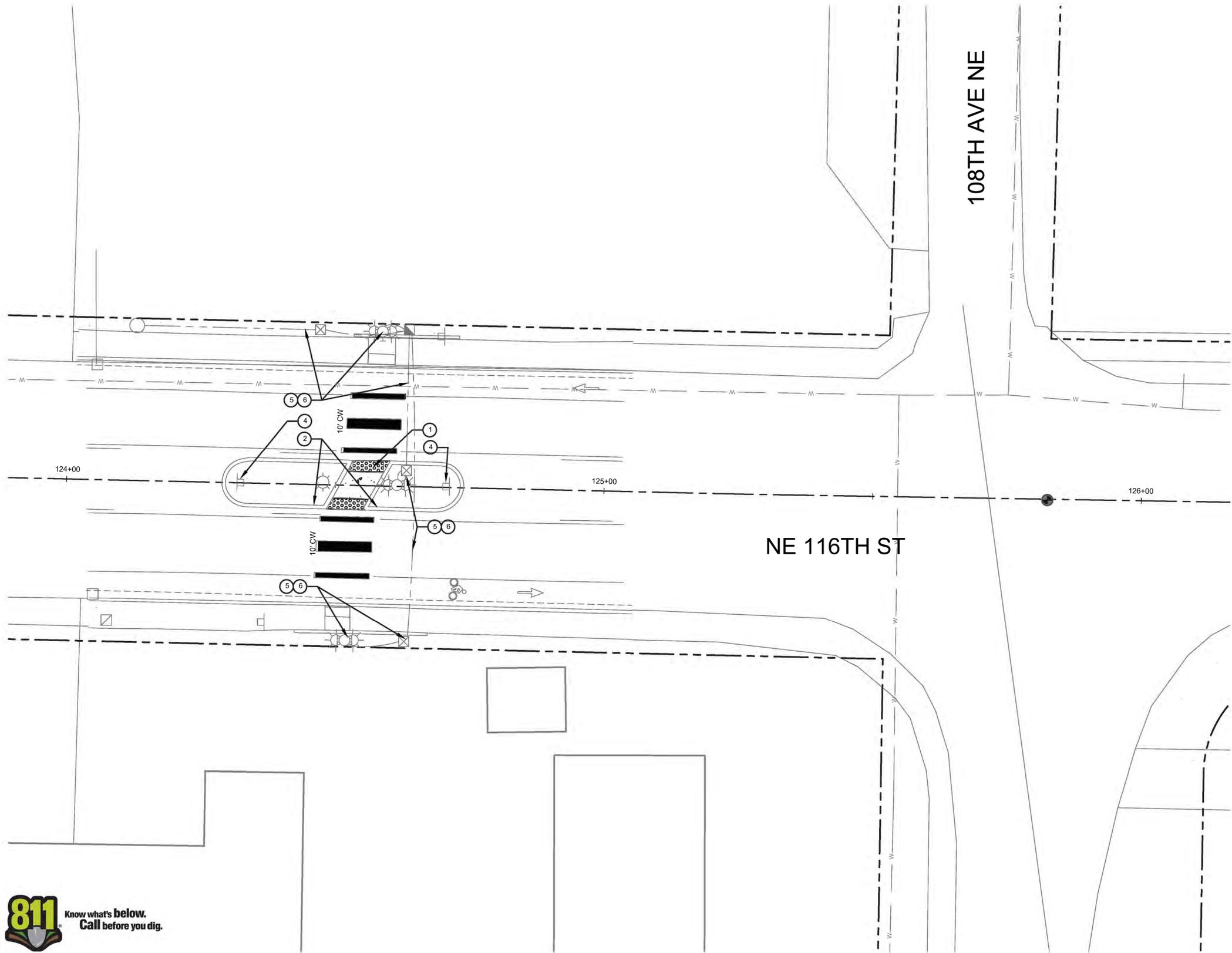
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**NE 116TH STREET
 CROSSWALK UPGRADES
 KIRKLAND, WA**

SCALE IN FEET

**PAVING AND CHANNELIZATION PLAN
 (SCHEDULE A - BASE BID)**

PROJECT NO.	NMC0120100
DATE	09/01/2020
DRAWING NO.	CH1
SHEET NO.	8 / 15



- NOTES:**
1. SEE NT SHEET SERIES FOR GENERAL NOTES AND LEGEND.
 2. SEE SP SHEET SERIES FOR TREE PROTECTION, TREE AND VEGETATION TRIMMING, AND SIGNAGE AND CHANNELIZATION REMOVALS.
 3. ALL DIMENSIONS ARE IN FEET, UNLESS OTHERWISE NOTED.
 4. SIGN AND SYMBOL MARKING LOCATIONS SHALL BE VERIFIED BY THE ENGINEER PRIOR TO INSTALLATION. LOCATIONS SHOWN ARE APPROXIMATE ONLY. ACTUAL LOCATIONS SHALL BE DETERMINED IN THE FIELD BASED ON CONDITIONS. SIGNS SHALL NOT BE PLACED TO CONFLICT WITH DRIVEWAYS, UTILITIES, OR VEGETATION. SIGNS SHALL NOT BE PLACED WHERE SIGHT LINES TO THE SIGN ARE BLOCKED BY TREES.
 5. UNDERGROUND UTILITY LOCATIONS SHOWN ARE APPROXIMATE BASED ON CITY OF KIRKLAND GIS DATA. LOCATIONS OF POTENTIAL CONFLICT MUST BE POTHOLED PRIOR TO TRENCHING.
 6. ADDITIONAL UNDERGROUND UTILITIES MAY EXIST THAT ARE NOT DEPICTED ON THESE PLANS. CONTRACTOR SHALL COORDINATE WITH THE ENGINEER TO DETERMINE IF ADDITIONAL UNDERGROUND UTILITIES MAY EXIST WITHIN THE LIMITS OF WORK.
 7. CALL UTILITIES UNDERGROUND LOCATION CENTER AT 1-800-424-5555 48 HOURS PRIOR TO CONSTRUCTION.

PAVING AND CHANNELIZATION LEGEND:

- RIGHT-OF-WAY
- CAST-IN-PLACE TRUNCATED DOME TACTILE WARNING SURFACE PER PRE-APPROVED PLAN NO. CK-R.25B
- FULL DEPTH HMA WITH STAMPED BRICK PATTERN 1 DT1
- 4" DEPTH CLASS 4000 CEMENT CONCRETE PAVEMENT OVER 2" DEPTH 1-1/4" MINUS SUBBASE MATERIAL
- CLASS 3000 CEMENT CONCRETE EXTRUDED CURB PER PRE-APPROVED PLAN NO. CK-R.19.
- 10' CW PER PRE-APPROVED PLAN NO. CK-R.28 & CK-R.28B

PAVING AND CHANNELIZATION CONSTRUCTION NOTES:

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3. REPAIR CEMENT CONCRETE EXTRUDED CURB PER PRE-APPROVED PLAN NO. CK-R.19.
4. RESTORE ASPHALT WHERE SIGN POST REMOVED
5. POT HOLE LOCATIONS OF POTENTIAL UTILITY CONFLICT
6. RESTORE LANDSCAPE, SIDEWALK PAVEMENT, ASPHALT PAVEMENT, AND EXTRUDED CURB TO EXISTING CONDITION OR PER PLANS AT RRFB SIGNAL POST AND TRENCHING LOCATIONS. SEE CITY OF KIRKLAND POLICY R-5, AND PRE-APPROVED PLANS CK-R.12, CK-R.13A, AND CK-R.23.



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**NE 116TH STREET
CROSSWALK UPGRADES
KIRKLAND, WA**

**PAVING AND CHANNELIZATION PLAN
(SCHEDULE A - BASE BID)**



PROJECT NO.	NMC0120100
DATE	09/01/2020
DRAWING NO.	CH2
SHEET NO.	9 / 15



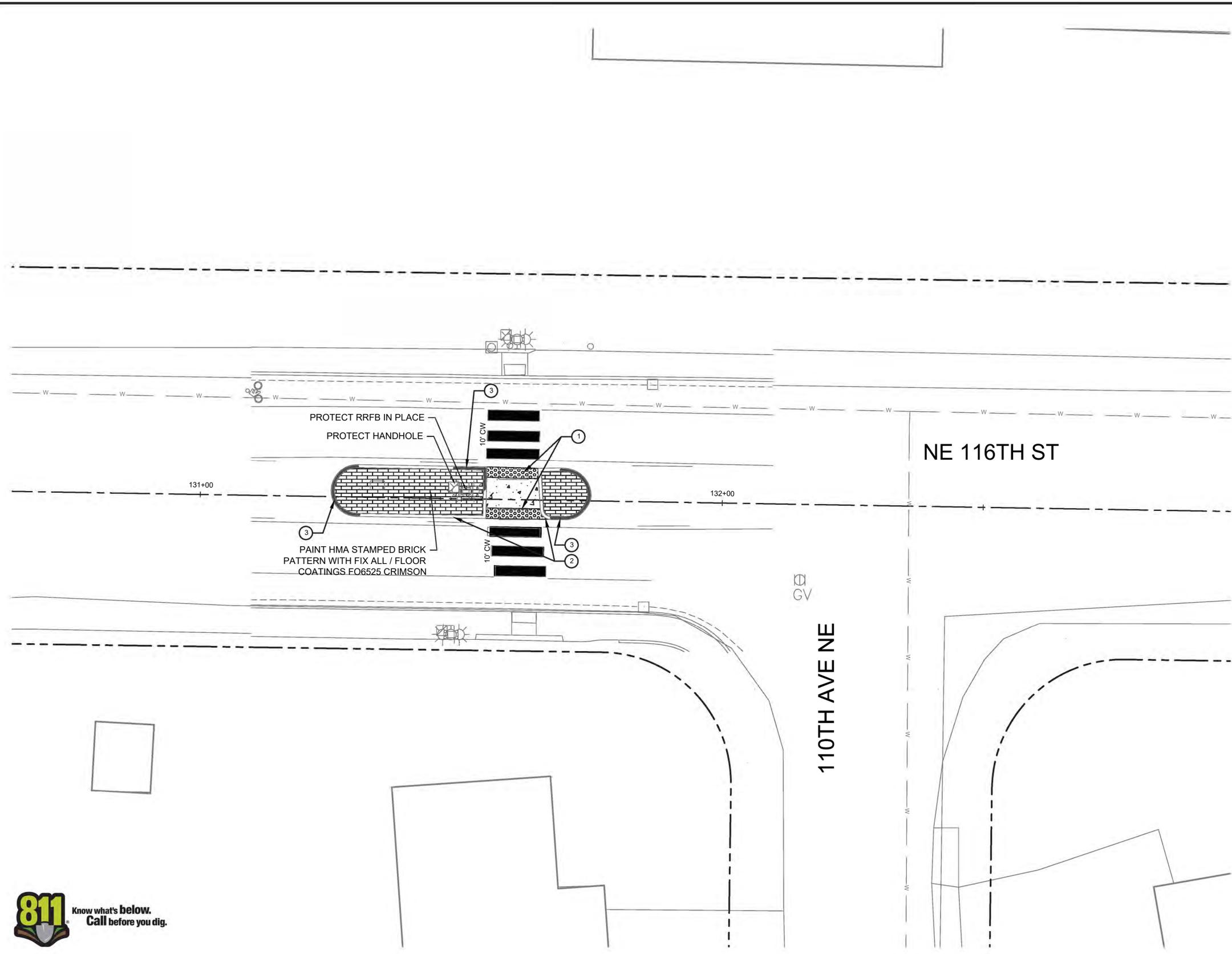
- NOTES:**
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PAVING AND CHANNELIZATION LEGEND:

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- CLASS 3000 CEMENT CONCRETE EXTRUDED CURB PER PRE-APPROVED PLAN NO. CK-R.19.
- 10' CW 10' CROSSWALK PER PRE-APPROVED PLAN NO. CK-R.28 & CK-R.28B

PAVING AND CHANNELIZATION CONSTRUCTION NOTES:

- INSTALL DETECTABLE WARNING STRIP FULL WIDTH OF ISLAND OPENING. DETECTABLE WARNING STRIP SHALL CONFORM TO CITY OF KIRKLAND PRE-APPROVED PLAN CK-R.25. EMBED WARNING STRIP INTO FRESHLY-PLACED CONCRETE TO RUN FLUSH WITH ADJOINING WALKING SURFACES.
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- RESTORE LANDSCAPE, SIDEWALK PAVEMENT, ASPHALT PAVEMENT, AND EXTRUDED CURB TO EXISTING CONDITION OR PER PLANS AT RRFB SIGNAL POST AND TRENCHING LOCATIONS. SEE CITY OF KIRKLAND POLICY R-5, AND PRE-APPROVED PLANS CK-R.12, CK-R.13A, AND CK-R.23.



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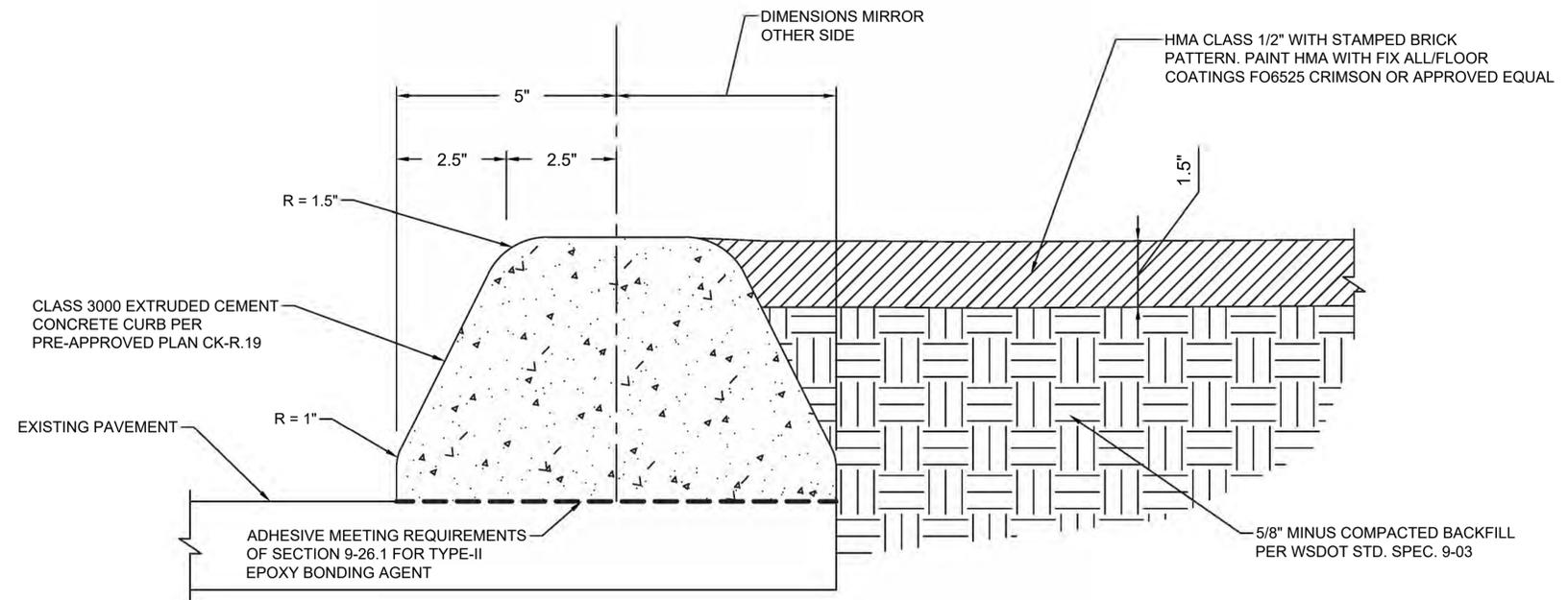
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**NE 116TH STREET
 CROSSWALK UPGRADES
 KIRKLAND, WA**

SCALE IN FEET

**PAVING AND CHANNELIZATION PLAN
 (SCHEDULE A - BASE BID)**

PROJECT NO.	NMC0120100
DATE	09/01/2020
DRAWING NO.	CH3
SHEET NO.	10 / 21



BRICK PATTERNED ASPHALT TRAFFIC ISLAND

N.T.S.

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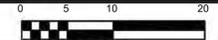
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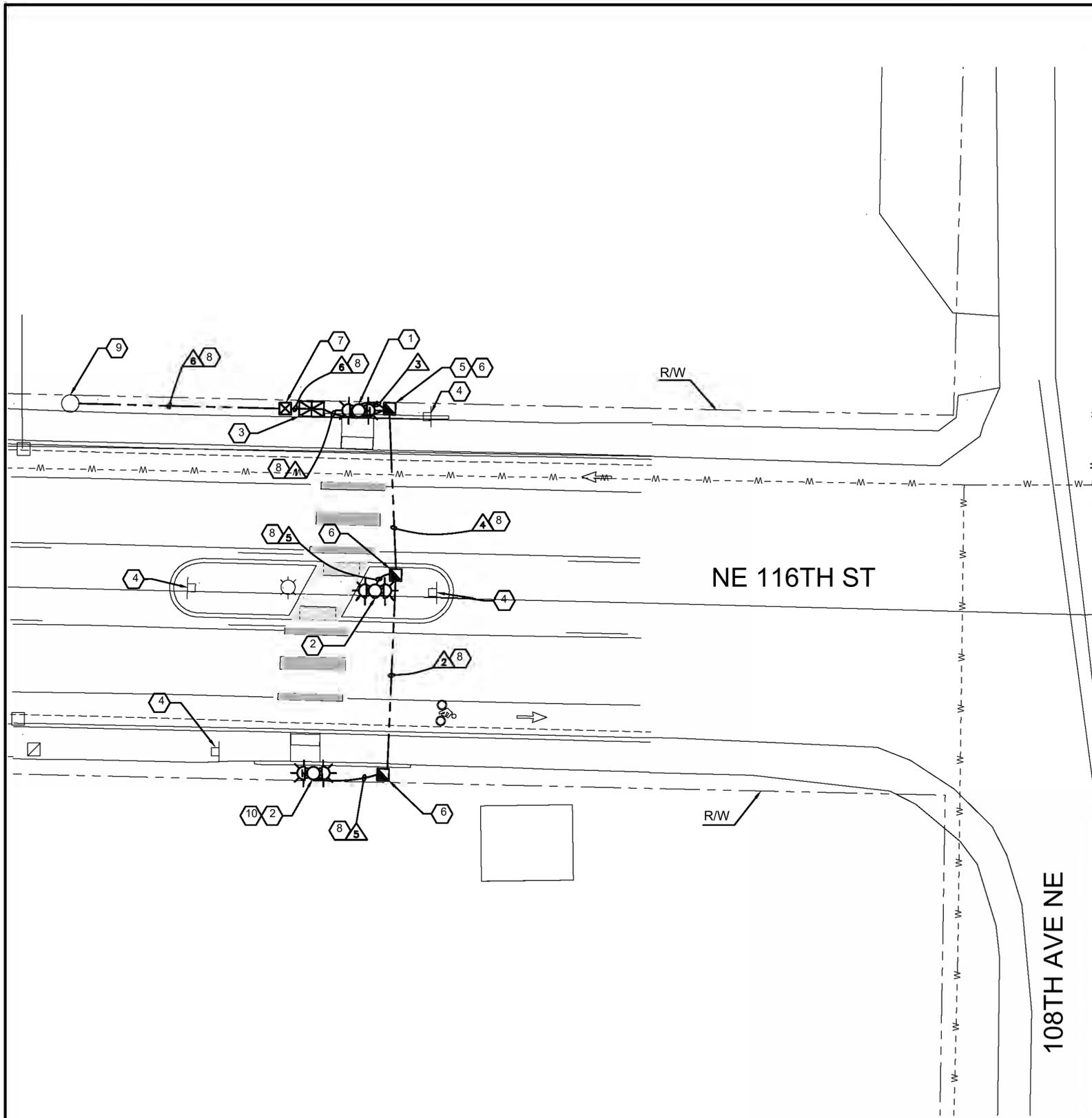
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DETAILS
(SCHEDULE A - BASE BID)

PROJECT NO.	NMC0120100
DATE	09/01/2020
DRAWING NO.	DT1
SHEET NO.	12 / 15



CONSTRUCTION NOTES:

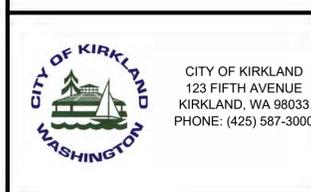
- 1 INSTALL AC-POWERED RECTANGULAR RAPID FLASH BEACON (RRFB) SYSTEM BACK-TO-BACK INCLUDING POLE, FOUNDATION, CONTROLLER CABINET ENCLOSURE AND CONTROLLER UNIT, SIGNS, LIGHT BARS, AND PUSH BUTTON PER DRAWING TSDT1 DETAIL A. CONTROLLER CABINET ENCLOSURE SHALL BE INSTALLED ON NORTH SIDE OF THE POLE.
- 2 INSTALL AC-POWERED RECTANGULAR RAPID FLASH BEACON (RRFB) SYSTEM BACK-TO-BACK INCLUDING POLE, FOUNDATION, SIGNS, LIGHT BARS, AND PUSH BUTTON PER DRAWING TSDT1 DETAIL A.
- 3 SUPPLY AND INSTALL SERVICE CABINET. SERVICE SHALL BE 120VAC AND SERVICE CABINET SHALL BE MILBANK CP3A1111XASL3 OR APPROVED EQUAL. INSTALL SERVICE CABINET FOUNDATION PER SERVICE CABINET MANUFACTURER'S REQUIREMENTS AND SPECIAL PROVISION 8-20. COORDINATE WITH CITY OF KIRKLAND AND PSE FOR SERVICE CONNECTION.
- 4 REMOVE EXISTING POST AND SIGN AND RETURN TO CITY OF KIRKLAND TO RECYCLE. COORDINATE WITH THE CITY ENGINEER FOR DELIVERY.
- 5 RELOCATE BUS STOP SIGN.
- 6 INSTALL SLIP-RESISTANT TYPE 2 JUNCTION BOX PER WSDOT STANDARD PLAN J-40.10-04. MARK LID WITH "TS".
- 7 INSTALL PSE SERVICE JUNCTION BOX. COORDINATE WITH CITY OF KIRKLAND AND PSE FOR JUNCTION BOX STANDARD, LOCATION, AND SERVICE CONNECTION.
- 8 TRENCH FOR CONDUIT INSTALLATION AND RESTORE TO MATCH EXISTING CONDITION. OPEN TRENCHING SHALL BE PER SPECIAL PROVISION 8-20.
- 9 INSTALL ONE 2" CONDUIT RISER ON POLE FOR RRFB SERVICE. PULL SERVICE CONDUCTORS TO TOP OF RISER AND COIL 15' OF WIRE FOR CONNECTION BY PSE. COORDINATE WITH THE CITY ENGINEER AND PSE FOR CONDUIT RISER INSTALLATION, NUMBER OF WIRES, WIRE SIZE AND SERVICE CONNECTION.
- 10 SEE TSDT1 DETAIL B FOR INSTALLING SIGNAL STANDARD FOUNDATION ON CEMENT CONCRETE CURB.

GENERAL NOTES:

- 1. RRFB, PUSH BUTTON POST, AND CONDUIT LOCATIONS SHOWN ARE APPROXIMATE. FINAL INSTALLATION LOCATION SHALL AVOID CURB RAMP AND LANDING AREA, AND SHALL BE APPROVED BY THE CITY ENGINEER PRIOR TO CONSTRUCTION.
- 2. ALL WORK SHALL BE PERFORMED PER CITY OF KIRKLAND STANDARD PLANS AND SPECIFICATIONS, UNLESS OTHERWISE NOTED IN THE DRAWING.
- 3. INVESTIGATE EXISTING UNDERGROUND UTILITIES PRIOR TO ANY POLE FOUNDATION EXCAVATION OR CONDUIT TRENCHING TO AVOID DAMAGE TO ANY EXISTING UNDERGROUND UTILITIES.
- 4. CONDUITS SHALL BE SCHEDULE 80 PVC UNLESS NOTED OTHERWISE.
- 5. ALL MATERIALS SHALL GO THROUGH THE SUBMITTAL REVIEW PROCESS AND BE APPROVED BY THE CITY ENGINEER.
- 6. REFERENCE TO CITY'S PRE-APPROVED POLICY R-24: RECTANGULAR RAPID FLASH BEACON (RRFB) INSTALLATION POLICY FOR RRFB COMPONENT LIST.
- 7. REFERENCE SHEETS SP1 AND CH1 FOR SITE PREPARATION, PAVING, AND CHANNELIZATION PLANS.
- 8. NO UNDERGROUND UTILITIES HAVE BEEN LOCATED DURING DESIGN. THE CONTRACTOR SHALL COORDINATE WITH THE CITY ENGINEER DURING CONSTRUCTION TO AVOID UTILITY CONFLICTS DURING TRENCHING ACTIVITIES.
- 9. ANY EXCAVATION IN PROXIMITY TO AN EX. POLE OR DOWN GUY MUST BE DONE WITHOUT UNDERMINING THEIR STABILITY. INSPECTION IS REQUIRED PRIOR TO ANY EXCAVATION. CONTRACTOR IS RESPONSIBLE FOR TEMPORARY SUPPORT WHICH MAY BE REQUIRED TO STABILIZE THE POLE. IF EXCAVATION IS DONE WITHIN FIVE FEET (5') OF ANY EX POLE, THE CONTRACTOR MUST PROVIDE TEMPORARY SUPPORT TO BE APPROVED BY THE CITY ENGINEER.

WIRING SCHEDULE

RUN NO.	CONDUIT SIZE	SERVICE POWER 2-#6	CONTROLLER 2-#10	RRFB 1-4C	PUSH BUTTON 1-8C	GROUND #8	PULL TAPE	REMARK
1	2" PVC	1				1		
2	2" PVC			1	1	1		
	2" PVC SPARE						1	
3	2" PVC	1		2	2	1		
4	2" PVC			2	2	1		
	2" PVC SPARE						1	
5	2" PVC			1	1	1		
6	2" PVC							COORDINATE WITH CITY OF KIRKLAND AND PSE FOR NUMBER OF SERVICE CONDUCTORS AND CONDUCTOR SIZE.



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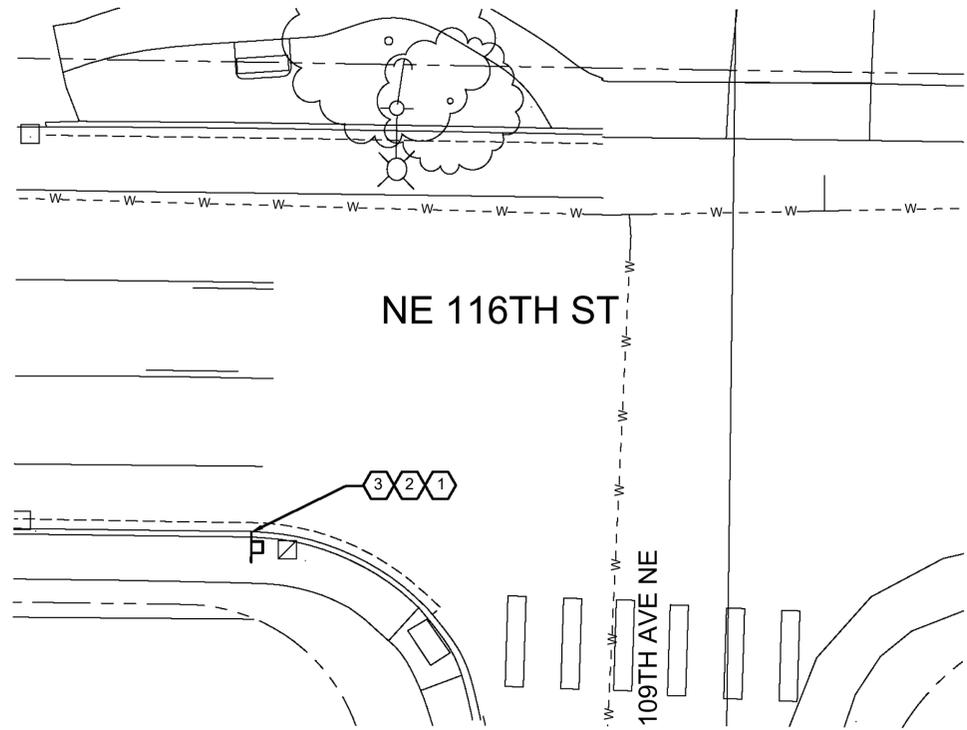
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CHK	SYZ				
APVD	XPZ	NO.	DATE	REVISION	BY



NE 116TH STREET
CROSSWALK UPGRADES
KIRKLAND, WA

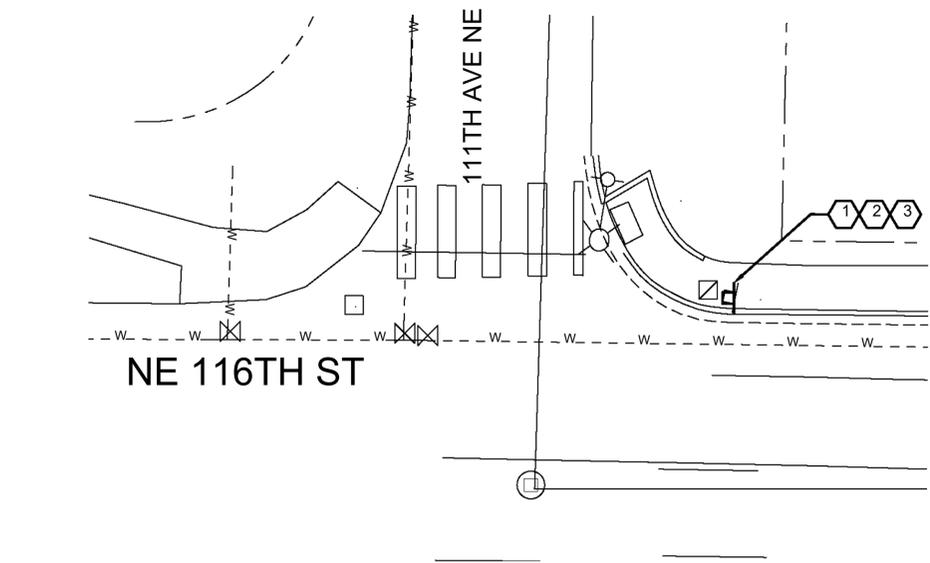
SCALE IN FEET
NE 116TH ST & 108TH AVE NE
SIGNAL PLAN
SCHEDULE A - BASE BID

PROJECT NO. NMC0120100
DATE 09/01/2020
DRAWING NO. SG1
SHEET NO. 13/15



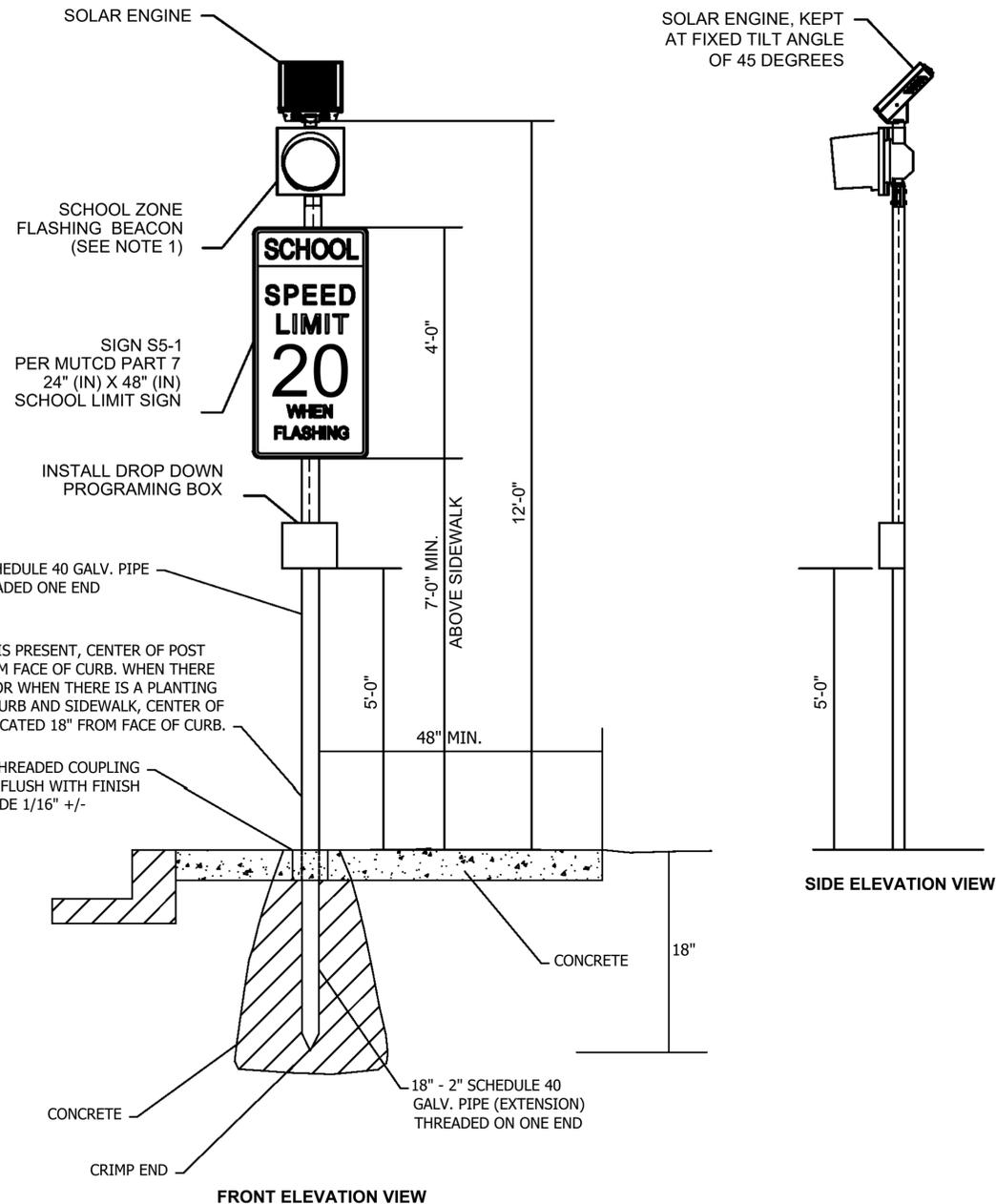
CONSTRUCTION NOTES:

- 1 REMOVE EXISTING SCHOOL ZONE FLASHING BEACON SYSTEM AND POST.
- 2 INSTALL SOLAR-POWERED SCHOOL ZONE FLASHING BEACON SYSTEM INCLUDING SINGLE INTEGRATED SOLAR ENGINE AND FLASHING BEACON, SIGN, FOUNDATION, AND ASSOCIATED WIRING AND MOUNTING HARDWARE PER DETAIL A. PROVIDE 18" FROM CENTER OF POST TO FACE OF CURB AND PROVIDE MINIMUM 4' SIDEWALK WIDTH BEHIND THE POST.
- 3 COORDINATE WITH PSE TO REMOVE EXISTING SERVICE CONDUCTORS TO NEAREST PSE DISCONNECT.



LEGEND:

- SCHOOL ZONE FLASHING BEACON SYSTEM AND POST
- TYPE 1 JUNCTION BOX



SCHOOL ZONE FLASHING BEACON INSTALLATION DETAIL A (NTS)

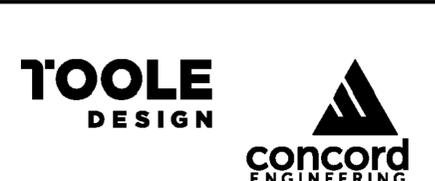
NOTES:

- 1. INSTALL CARMANAH R829-E SOLAR SCHOOL ZONE FLASHING BEACON SYSTEM, OR APPROVED EQUAL, INCLUDING SOLAR ENGINE, FLASHING BEACON, AND SCHOOL LIMIT SIGN PER MANUFACTURER'S INSTALLATION INSTRUCTIONS.
- 2. SEE CITY OF KIRKLAND STANDARD PLAN CK-R.43 FOR SIGN INSTALLATION AND FOUNDATION DETAILS.



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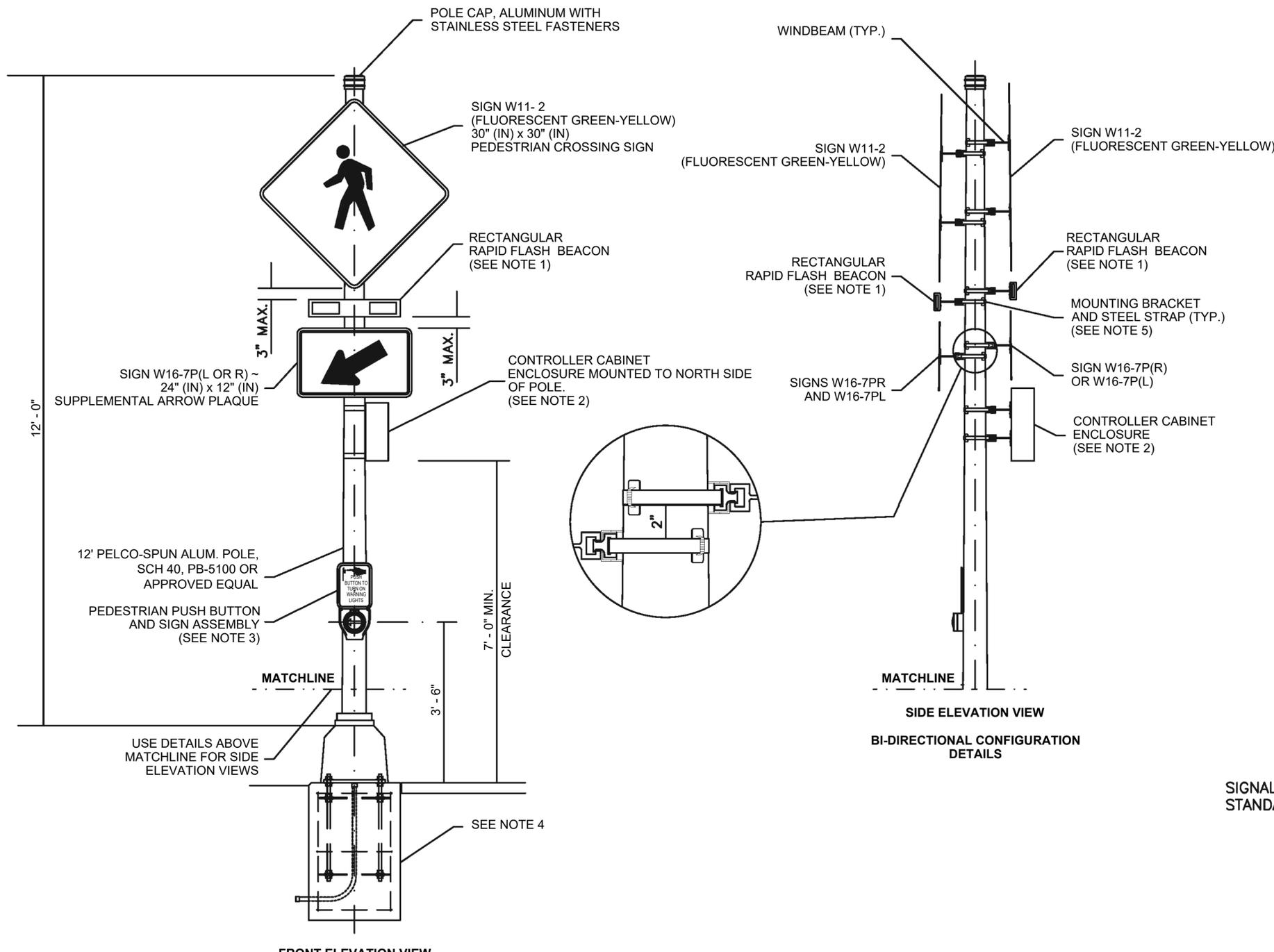
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APVD	XPZ	NO.	DATE	REVISION	BY



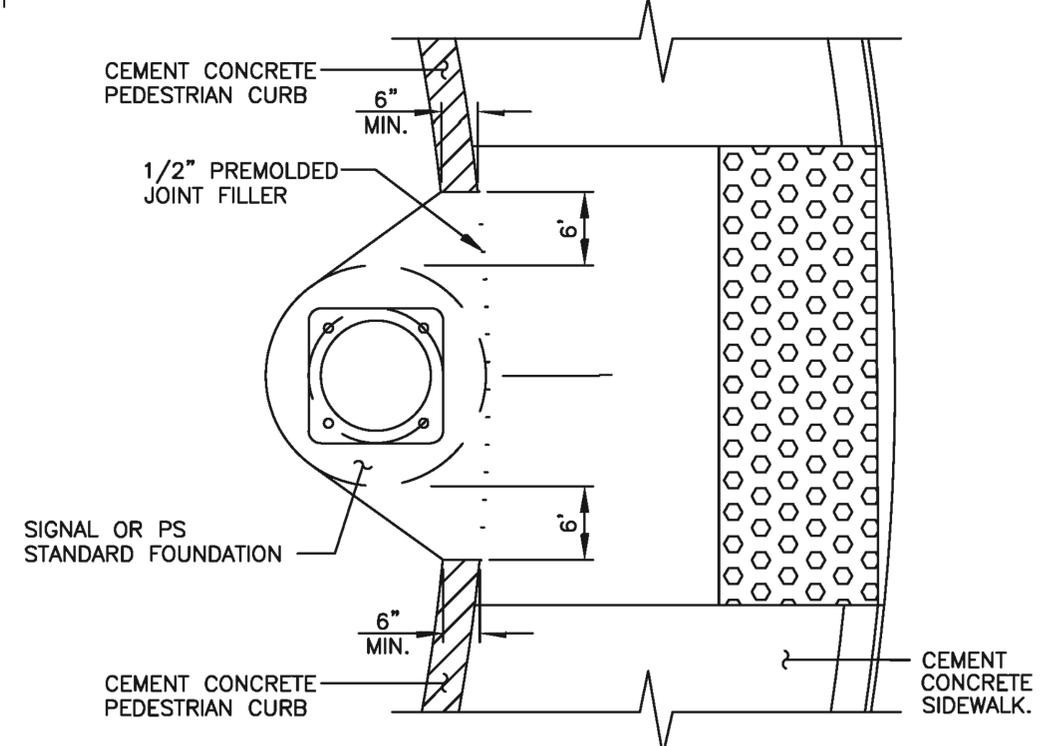
NE 116TH STREET
CROSSWALK UPGRADES
KIRKLAND, WA

SCALE IN FEET
NE 116TH ST & 109TH AVE NE
NE 116TH ST & 111TH AVE NE
SIGNAL PLAN
SCHEDULE B - BID ALTERNATE

PROJECT NO. NMC0120100
DATE 09/01/2020
DRAWING NO. SG2
SHEET NO. 14 / 15



- NOTES**
1. RECTANGULAR RAPID FLASH BEACON SHALL BE ELTEC RRFB LIGHT HEADS AND ELTEC RRFB POLE MOUNT LIGHT ENCLOSURE OR APPROVED EQUAL. SEE MANUFACTURER'S INSTALLATION GUIDE FOR INSTALLATION DETAILS.
 2. CONTROLLER CABINET ENCLOSURE SHALL BE SLIMLINE 100 CABINET OR APPROVED EQUAL. THE CABINET ENCLOSURE SHALL BE SIZED BY THE RRFB MANUFACTURER.
 3. THE PEDESTRIAN PUSH BUTTON AND SIGN ASSEMBLY SHALL BE POLARA MODEL XAV2E-LED PUSH BUTTON STATION OR APPROVED EQUAL. PEDESTRIAN PUSH BUTTON CUSTOM MESSAGE SHALL BE "YELLOW LIGHTS ARE FLASHING". SEE MANUFACTURER'S INSTALLATION GUIDE FOR INSTALLATION DETAILS.
 4. SEE WSDOT STANDARD PLAN J-21.10-04 FOR SIGNAL STANDARD FOUNDATION. POLE BASE SHALL BE PELCO PB-5334 OR APPROVED EQUAL. COLLAR SHALL BE PELCO PB-5325 OR APPROVED EQUAL. SEE MANUFACTURER'S INSTALLATION GUIDE FOR BASE AND COLLAR INSTALLATION DETAILS.
 5. SEE WSDOT STANDARD PLAN G-30.10-04 FOR SIGN INSTALLATION ON SIGNAL STANDARD DETAILS.



**AC POWERED RECTANGULAR RAPID FLASH BEACON
DETAIL A (NTS)**

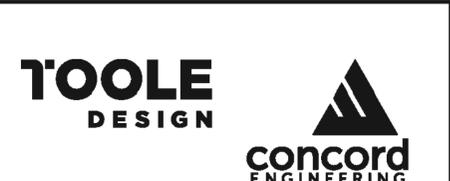
**SIGNAL STANDARD FOUNDATION ON CEMENT CONCRETE CURB
DETAIL B (NTS)**



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CHK	SYZ				
APVD	XPZ	NO.	DATE	REVISION	BY



NE 116TH STREET
CROSSWALK UPGRADES
KIRKLAND, WA

SCALE IN FEET 0 5 10 20

NE 116TH ST
SIGNAL DETAILS

PROJECT NO.	NMC0120100
DATE	09/01/2020
DRAWING NO.	TDST1
SHEET NO.	15 / 15