

GENERAL NOTE:

IF THE FOLLOWING CITY OF KIRKLAND STANDARD NOTES CONFLICT WITH STANDARD SPECIFICATIONS, THE MORE STRINGENT REQUIREMENT OF THE TWO SHALL APPLY AS DIRECTED BY THE ENGINEER.

CITY OF KIRKLAND EROSION/SEDIMENTATION CONTROL PLAN NOTES (AS OF 01/30/2019)

- THE APPROVED CONSTRUCTION SEQUENCE SHALL BE AS FOLLOWS:
 - CONDUCT PRE-CONSTRUCTION MEETING.
 - FLAG OR FENCE CLEARING LIMITS.
 - POST SIGN WITH NAME AND PHONE NUMBER OF TESC SUPERVISOR.
 - INSTALL CATCH BASIN PROTECTION IF REQUIRED.
 - GRADE AND INSTALL CONSTRUCTION ENTRANCE(S).
 - INSTALL PERIMETER PROTECTION (SILT FENCE, BRUSH BARRIER, ETC.).
 - CONSTRUCT SEDIMENT PONDS AND TRAPS.
 - GRADE AND STABILIZE CONSTRUCTION ROADS.
 - CONSTRUCT SURFACE WATER CONTROLS (INTERCEPTOR DIKES, PIPE SLOPE DRAINS, ETC.) SIMULTANEOUSLY WITH CLEARING AND GRADING FOR PROJECT DEVELOPMENT.
 - MAINTAIN EROSION CONTROL MEASURE IN ACCORDANCE WITH CITY OF KIRKLAND STANDARDS AND MANUFACTURER'S RECOMMENDATIONS.
 - RELOCATE EROSION CONTROL MEASURES OR INSTALL NEW MEASURES SO THAT AS SITE CONDITIONS CHANGE, THE EROSION AND SEDIMENT CONTROL IS ALWAYS IN ACCORDANCE WITH THE CITY TESC MINIMUM REQUIREMENTS.
 - COVER ALL AREAS WITHIN THE SPECIFIED TIME FRAME WITH STRAW, WOOD FIBER MULCH, COMPOST, PLASTIC SHEETING, CRUSHED ROCK OR EQUIVALENT.
 - STABILIZE ALL AREAS THAT REACH FINAL GRADE WITHIN 7 DAYS.
 - SEED OR SOD ANY AREAS TO REMAIN UNWORKED FOR MORE THAN 30 DAYS.
 - UPON COMPLETION OF THE PROJECT, ALL DISTURBED AREAS MUST BE STABILIZED AND BEST MANAGEMENT PRACTICES REMOVED IF APPROPRIATE.
- CONTRACTOR IS RESPONSIBLE FOR KEEPING STREETS CLEAN AND FREE OF CONTAMINANTS AT ALL TIMES AND FOR PREVENTING AN ILLICIT DISCHARGE (KMC 15.52) INTO THE MUNICIPAL STORM DRAIN SYSTEM. IF YOUR CONSTRUCTION PROJECT CAUSES AN ILLICIT DISCHARGE TO THE MUNICIPAL STORM DRAIN SYSTEM, THE CITY OF KIRKLAND STORM MAINTENANCE DIVISION WILL BE CALLED TO CLEAN THE PUBLIC STORM SYSTEM, AND OTHER AFFECTED PUBLIC INFRASTRUCTURE. THE CONTRACTOR(S), PROPERTY OWNER, AND ANY OTHER RESPONSIBLE PARTY MAY BE CHARGED ALL COSTS ASSOCIATED WITH THE CLEAN-UP AND MAY ALSO BE ASSESSED MONETARY PENALTIES (KMC 1.12.200). THE MINIMUM PENALTY IS \$250. A FINAL INSPECTION OF YOUR PROJECT WILL NOT BE GRANTED UNTIL ALL COSTS ASSOCIATED WITH THE CLEAN-UP, AND PENALTIES, ARE PAID TO THE CITY OF KIRKLAND.
- CONSTRUCTION DEWATERING DISCHARGES SHALL ALWAYS MEET WATER QUALITY GUIDELINES LISTED IN COK POLICY E-1. SPECIFICALLY, DISCHARGES TO THE PUBLIC STORMWATER DRAINAGE SYSTEM MUST BE BELOW 25NTU, AND NOT CONSIDERED AN ILLICIT DISCHARGE (PER KMC 15.52.090). TEMPORARY DISCHARGES TO SANITARY SEWER REQUIRE PRIOR AUTHORIZATION AND PERMIT FROM KING COUNTY INDUSTRIAL WASTE PROGRAM (206-263-3000) AND NOTIFICATION TO THE PUBLIC WORKS CONSTRUCTION INSPECTOR.
- ALL WORK AND MATERIALS SHALL BE IN ACCORDANCE WITH CITY OF KIRKLAND STANDARDS AND SPECIFICATIONS.
- THE BOUNDARIES OF THE CLEARING LIMITS SHOWN ON THIS PLAN SHALL BE SET BY SURVEY AND CLEARLY FLAGGED IN THE FIELD BY A CLEARING CONTROL FENCE PRIOR TO CONSTRUCTION. DURING THE CONSTRUCTION PERIOD, NO DISTURBANCE OR REMOVAL OF ANY GROUND COVER BEYOND THE FLAGGED CLEARING LIMITS SHALL BE PERMITTED. THE FLAGGING SHALL BE MAINTAINED BY THE PERMITTEE/CONTRACTOR FOR THE DURATION OF CONSTRUCTION.
- APPROVAL OF THIS EROSION/SEDIMENTATION CONTROL (ESC) PLAN DOES NOT CONSTITUTE AN APPROVAL OF PERMANENT ROAD OR DRAINAGE DESIGN (E.G., SIZE AND LOCATION OF ROADS, PIPES, RESTRICTORS, CHANNELS, RETENTION FACILITIES, UTILITIES, ETC.).
- THE IMPLEMENTATION OF THIS ESC PLAN AND THE CONSTRUCTION, MAINTENANCE, REPLACEMENT, AND UPGRADING OF THESE ESC FACILITIES IS THE RESPONSIBILITY OF THE PERMITTEE/CONTRACTOR UNTIL ALL CONSTRUCTION IS APPROVED.
- A COPY OF THE APPROVED ESC PLANS MUST BE ON THE JOB SITE WHENEVER CONSTRUCTION IS IN PROGRESS.
- THE ESC FACILITIES SHOWN ON THIS PLAN MUST BE CONSTRUCTED PRIOR TO OR IN CONJUNCTION WITH ALL CLEARING AND GRADING ACTIVITIES IN SUCH A MANNER AS TO ENSURE THAT SEDIMENT-LADEN WATER DOES NOT ENTER THE DRAINAGE SYSTEM OR VIOLATE APPLICABLE WATER STANDARDS. WHEREVER POSSIBLE, MAINTAIN NATURAL VEGETATION FOR SILT CONTROL.
- THE ESC FACILITIES SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE DETAILS ON THE APPROVED PLANS. LOCATIONS MAY BE MOVED TO SUIT FIELD CONDITIONS, SUBJECT TO APPROVAL BY THE ENGINEER AND THE CITY OF KIRKLAND INSPECTOR.
- THE ESC FACILITIES SHOWN ON THIS PLAN ARE THE MINIMUM REQUIREMENTS FOR ANTICIPATED SITE CONDITIONS. DURING THE CONSTRUCTION PERIOD, THESE ESC



CITY OF KIRKLAND
 DEPARTMENT OF PUBLIC WORKS
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 DATE: _____

DR	2019/12/06
DESIGNED BY:	DATE
JA	2019/12/06
DRAWN BY:	DATE
KG	2019/12/06
CHECKED BY:	DATE

CITY OF KIRKLAND
TOTEM LAKE PEDESTRIAN BRIDGE
EROSION/SEDIMENT CONTROL AND STORM DRAINAGE NOTES

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FACILITIES SHALL BE UPGRADED (E.G., ADDITIONAL SUMPS, RELOCATION OF DITCHES AND SILT FENCES, ETC.) AS NEEDED FOR UNEXPECTED STORM EVENTS. ADDITIONALLY, MORE ESC FACILITIES MAY BE REQUIRED TO ENSURE COMPLETE SILTATION CONTROL. THEREFORE, DURING THE COURSE OF CONSTRUCTION IT SHALL BE THE OBLIGATION AND RESPONSIBILITY OF THE CONTRACTOR TO ADDRESS ANY NEW CONDITIONS THAT MAY BE CREATED BY HIS ACTIVITIES AND TO PROVIDE ADDITIONAL FACILITIES OVER AND ABOVE THE MINIMUM REQUIREMENTS AS MAY BE NEEDED.

- THE ESC FACILITIES SHALL BE INSPECTED BY THE PERMITTEE/CONTRACTOR DAILY DURING NON-RAINFALL PERIODS, EVERY HOUR (DAYLIGHT) DURING A RAINFALL EVENT, AND AT THE END OF EVERY RAINFALL, AND MAINTAINED AS NECESSARY TO ENSURE THEIR CONTINUED FUNCTIONING. IN ADDITION, TEMPORARY SILTATION PONDS AND ALL TEMPORARY SILTATION CONTROLS SHALL BE MAINTAINED IN A SATISFACTORY CONDITION UNTIL SUCH TIME THAT CLEARING AND/OR CONSTRUCTION IS COMPLETED, PERMANENT DRAINAGE FACILITIES ARE OPERATIONAL, AND THE POTENTIAL FOR EROSION HAS PASSED. WRITTEN RECORDS SHALL BE KEPT DOCUMENTING THE REVIEWS OF THE ESC FACILITIES.
- THE ESC FACILITIES ON INACTIVE SITES SHALL BE INSPECTED AND MAINTAINED A MINIMUM OF ONCE A MONTH OR WITHIN 48 HOURS FOLLOWING A STORM EVENT.
- STABILIZED CONSTRUCTION ENTRANCES SHALL BE INSTALLED AT THE BEGINNING OF CONSTRUCTION AND MAINTAINED FOR THE DURATION OF THE PROJECT. ADDITIONAL MEASURES, SUCH AS WASH PADS, MAY BE REQUIRED TO ENSURE THAT ALL PAVED AREAS ARE KEPT CLEAN FOR THE DURATION OF THE PROJECT.
- ALL DENUDED SOILS MUST BE STABILIZED WITH AN APPROVED TESC METHOD (E.G. SEEDING, MULCHING, PLASTIC COVERING, CRUSHED ROCK) WITHIN THE FOLLOWING TIMELINES:
 - MAY 1 TO SEPTEMBER 30 --- SOILS MUST BE STABILIZED WITHIN 7 DAYS OF GRADING.
 - OCTOBER 1 TO APRIL 30 --- SOILS MUST BE STABILIZED WITHIN 2 DAYS OF GRADING.
 - STABILIZE SOILS AT THE END OF THE WORKDAY PRIOR TO A WEEKEND, HOLIDAY, OR PREDICTED RAIN EVENT.
- WHERE SEEDING FOR TEMPORARY EROSION CONTROL IS REQUIRED, FAST GERMINATING GRASSES SHALL BE APPLIED AT AN APPROPRIATE RATE (EXAMPLE: ANNUAL OR PERENNIAL RYE APPLIED AT APPROXIMATELY 80 POUNDS PER ACRE).
- WHERE STRAW MULCH IS REQUIRED FOR TEMPORARY EROSION CONTROL, IT SHALL BE APPLIED AT A MINIMUM THICKNESS OF 2".
- ALL LOTS ADJOINING OR HAVING ANY NATIVE GROWTH PROTECTION EASEMENTS (NGPE) SHALL HAVE A 6' HIGH TEMPORARY CONSTRUCTION FENCE (CHAIN LINK WITH PIER BLOCKS) SEPARATING THE LOT (OR BUILDABLE PORTIONS OF THE LOT) FROM THE AREA RESTRICTED BY THE NGPE AND SHALL BE INSTALLED PRIOR TO ANY GRADING OR CLEARING AND REMAIN IN PLACE UNTIL THE PLANNING DEPARTMENT AUTHORIZES REMOVAL.
- CLEARING LIMITS SHALL BE DELINEATED WITH A CLEARING CONTROL FENCE. THE CLEARING CONTROL FENCE SHALL CONSIST OF A 6-FT. HIGH CHAIN LINK FENCE ADJACENT THE DRIP LINE OF TREES TO BE SAVED, WETLAND OR STREAM BUFFERS, AND SENSITIVE SLOPES. CLEARING CONTROL FENCES ALONG WETLAND OR STREAM BUFFERS OR UPSLOPE OF SENSITIVE SLOPES SHALL BE ACCOMPANIED BY AN EROSION CONTROL FENCE. IF APPROVED BY THE CITY, A FOUR-FOOT HIGH ORANGE MESH CLEARING CONTROL FENCE MAY BE USED TO DELINEATE CLEARING LIMITS IN ALL OTHER AREAS.
- OFF-SITE STREETS MUST BE KEPT CLEAN AT ALL TIMES. IF DIRT IS DEPOSITED ON THE PUBLIC STREET SYSTEM, THE STREET SHALL BE IMMEDIATELY CLEANED WITH POWER SWEEPER OR OTHER EQUIPMENT. ALL VEHICLES SHALL LEAVE THE SITE BY WAY OF THE CONSTRUCTION ENTRANCE AND SHALL BE CLEANED OF ALL DIRT THAT WOULD BE DEPOSITED ON THE PUBLIC STREETS.
- ROCK FOR EROSION PROTECTION OF ROADWAY DITCHES, WHERE REQUIRED, MUST BE OF SOUND QUARRY ROCK, PLACED TO A DEPTH OF 1' AND MUST MEET THE FOLLOWING SPECIFICATIONS: 4"-8" ROCK/40%-70% PASSING; 2"-4" ROCK/30%-40% PASSING; AND 1"-2" ROCK/10%-20% PASSING. RECYCLED CONCRETE SHALL NOT BE USED FOR EROSION PROTECTION, INCLUDING CONSTRUCTION ENTRANCE OR TEMPORARY STABILIZATION ELSEWHERE ON THE SITE.
- IF ANY PART(S) OF THE CLEARING LIMIT BOUNDARY OR TEMPORARY EROSION/SEDIMENTATION CONTROL PLAN IS/ARE DAMAGED, IT SHALL BE REPAIRED IMMEDIATELY.
- ALL PROPERTIES ADJACENT TO THE PROJECT SITE SHALL BE PROTECTED FROM SEDIMENT DEPOSITION AND RUNOFF.
- AT NO TIME SHALL MORE THAN 1' OF SEDIMENT BE ALLOWED TO ACCUMULATE WITHIN A CATCH BASIN. ALL CATCH BASINS AND CONVEYANCE LINES SHALL BE CLEANED IMMEDIATELY FOLLOWING REMOVAL OF EROSION CONTROL BMPS. THE CLEANING OPERATION SHALL NOT FLUSH SEDIMENT-LADEN WATER INTO THE DOWNSTREAM SYSTEM.
- ANY PERMANENT RETENTION/DETENTION FACILITY USED AS A TEMPORARY SETTLING BASIN SHALL BE MODIFIED WITH THE NECESSARY EROSION CONTROL MEASURES AND SHALL PROVIDE ADEQUATE STORAGE CAPACITY. IF THE PERMANENT FACILITY IS TO FUNCTION ULTIMATELY AS AN INFILTRATION OR DISPERSION SYSTEM, THE FACILITY SHALL NOT BE USED AS A TEMPORARY SETTLING BASIN. NO UNDERGROUND DETENTION TANK, DETENTION VAULT, OR SYSTEM WHICH BACKS UNDER OR INTO A POND SHALL BE USED AS A TEMPORARY SETTLING BASIN.
- ALL EROSION/SEDIMENTATION CONTROL PONDS WITH A DEAD STORAGE DEPTH EXCEEDING 6" MUST HAVE A PERIMETER FENCE WITH A MINIMUM HEIGHT OF 3'.
- THE WASHED GRAVEL BACKFILL ADJACENT TO THE FILTER FABRIC FENCE SHALL BE REPLACED AND THE FILTER FABRIC CLEANED IF IT IS NONFUNCTIONAL BY EXCESSIVE SILT ACCUMULATION AS DETERMINED BY THE CITY OF KIRKLAND. ALSO, ALL INTERCEPTOR SWALES SHALL BE CLEANED IF SILT ACCUMULATION EXCEEDS ONE-QUARTER DEPTH.
- PRIOR TO THE OCTOBER 1 OF EACH YEAR (THE BEGINNING OF THE WET SEASON), ALL DISTURBED AREAS SHALL BE REVIEWED TO IDENTIFY WHICH ONES CAN BE SEEDED IN PREPARATION FOR THE WINTER RAINS. THE IDENTIFIED DISTURBED AREA SHALL BE SEEDED WITHIN ONE WEEK AFTER OCTOBER 1. A SITE PLAN DEPICTING THE AREAS TO BE SEEDED AND THE AREAS TO REMAIN UNCOVERED SHALL BE SUBMITTED TO THE PUBLIC WORKS CONSTRUCTION INSPECTOR. THE INSPECTOR CAN REQUIRE SEEDING OF ADDITIONAL AREAS IN ORDER TO PROTECT SURFACE WATERS, ADJACENT PROPERTIES, OR DRAINAGE FACILITIES.

- SEE SPECIAL PROVISION 2-06.3(3) FOR PROTECTION AND PREPARATION OF SUBGRADE AT ALL AREAS USED FOR INFILTRATION OR PERVIOUS PAVEMENT.
- IF THE TEMPORARY CONSTRUCTION ENTRANCE OR ANY OTHER AREA WITH HEAVY VEHICLE LOADING IS LOCATED IN THE SAME AREA TO BE USED FOR INFILTRATION OR PERVIOUS PAVEMENT, 6" OF SEDIMENT BELOW THE GRAVEL SHALL BE REMOVED PRIOR TO INSTALLATION OF THE INFILTRATION FACILITY OR PERVIOUS PAVEMENT (TO REMOVE FINES ACCUMULATED DURING CONSTRUCTION).
- ANY CATCH BASINS COLLECTING RUNOFF FROM THE SITE, WHETHER THEY ARE ON OR OFF THE SITE, SHALL HAVE ADEQUATE PROTECTION FROM SEDIMENT. CATCH BASINS DIRECTLY DOWNSTREAM OF THE CONSTRUCTION ENTRANCE OR ANY OTHER CATCH BASIN AS DETERMINED BY THE CITY INSPECTOR SHALL BE PROTECTED WITH A "STORM DRAIN PROTECTION INSERT" OR EQUIVALENT.
- IF A SEDIMENT POND IS NOT PROPOSED, A BAKER TANK OR OTHER TEMPORARY GROUND AND/OR SURFACE WATER STORAGE TANK MAY BE REQUIRED DURING CONSTRUCTION, DEPENDING ON WEATHER CONDITIONS.
- DO NOT FLUSH CONCRETE BY-PRODUCTS OR TRUCKS NEAR OR INTO THE STORM DRAINAGE SYSTEM. IF EXPOSED AGGREGATE IS FLUSHED INTO THE STORM SYSTEM, IT COULD MEAN RE-CLEANING THE ENTIRE DOWNSTREAM STORM SYSTEM, OR POSSIBLY RE-LAYING THE STORM LINE.
- RECYCLED CONCRETE SHALL NOT BE STOCKPILED ON SITE, UNLESS FULLY COVERED WITH NO POTENTIAL FOR RELEASE OF RUNOFF.

CITY OF KIRKLAND STORM DRAINAGE PLAN NOTES (AS OF 01/30/2019)

- A PRE-CONSTRUCTION CONFERENCE SHALL BE HELD PRIOR TO THE START OF CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR SECURING ALL NECESSARY PERMITS PRIOR TO CONSTRUCTION.
- BEFORE ANY CONSTRUCTION MAY OCCUR, THE CONTRACTOR SHALL HAVE PLANS WHICH HAVE BEEN SIGNED AND APPROVED BY THE CITY OF KIRKLAND PUBLIC WORKS DEPARTMENT, OBTAINED ALL CITY, COUNTY, STATE, FEDERAL AND OTHER REQUIRED PERMITS, AND HAVE POSTED ALL REQUIRED BONDS.
- ALL STORM DRAINAGE IMPROVEMENTS SHALL BE DESIGNED AND CONSTRUCTED IN ACCORDANCE WITH THE LATEST EDITION OF THE CITY OF KIRKLAND PUBLIC WORKS PRE-APPROVED PLANS AND POLICIES AND THE STANDARD SPECIFICATIONS FOR ROAD, BRIDGE AND MUNICIPAL CONSTRUCTION, PREPARED BY WSDOT AND THE AMERICAN PUBLIC WORKS ASSOCIATION (APWA).
- ANY DEVIATION FROM THE APPROVED PLANS WILL REQUIRE WRITTEN APPROVAL, ALL CHANGES SHALL BE SUBMITTED TO THE CITY.
- A COPY OF THE APPROVED STORM WATER PLANS MUST BE ON THE JOB SITE WHENEVER CONSTRUCTION IS IN PROGRESS.
- ALL DISTURBED AREAS SHALL BE SEEDED AND MULCHED OR SIMILARLY STABILIZED TO THE SATISFACTION OF THE CITY OF KIRKLAND DEPARTMENT OF PUBLIC WORKS FOR THE PREVENTION OF ON-SITE EROSION AFTER THE COMPLETION OF CONSTRUCTION.
- MINIMUM COVER OVER STORM DRAINAGE PIPES IN ROW OR VEHICULAR PATH SHAFT BE 18 INCHES, UNLESS OTHER DESIGN IS APPROVED.
- STEEL PIPE SHAFT HAVE ASPHALT TREATMENT #1 OR BETTER INSIDE AND OUTSIDE.
- ALL CATCH BASINS SHALL BE TYPE I UNLESS OTHERWISE NOTED. CATCH BASINS WITH A DEPTH OF OVER FIVE FEET (5') TO THE PIPE INVERT SHALL BE A TYPE II CATCH BASIN. TYPE II CATCH BASINS EXCEEDING FIVE FEET (5') IN DEPTH SHALL HAVE A STANDARD LADDER INSTALLED.
- ALL STORM DRAINAGE MAIN EXTENSIONS WITHIN THE PUBLIC RIGHT-OF-WAY OR IN EASEMENTS MUST BE STAKED FOR LINE AND GRADE PRIOR TO STARTING CONSTRUCTION.
- ROCK FOR EROSION PROTECTION OF ROADWAY DITCHES, WHERE REQUIRED, MUST BE OF SOUND QUARRY ROCK, PLACED TO A DEPTH OF ONE FOOT (1') AND MUST MEET THE FOLLOWING SPECIFICATIONS: 4"-8" ROCK/40%-70% PASSING; 2"-4" ROCK/30%-40% PASSING; 2"-MINUS ROCK/10%-20% PASSING. RECYCLED CONCRETE SHALL NOT BE USED FOR EROSION PROTECTION, INCLUDING FOR CONSTRUCTION ENTRANCE OR TEMPORARY STABILIZATION ELSEWHERE ON SITE.
- ALL PIPE, MANHOLES, CATCH BASINS, AND APPURTENANCES SHALL BE LAID ON A PROPERLY PREPARED FOUNDATION IN ACCORDANCE WITH THE CURRENT STATE OF WASHINGTON STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION (WSDOT). THIS SHALL INCLUDE NECESSARY LEVELING OF THE TRENCH BOTTOM OR THE TOP OF THE FOUNDATION MATERIAL AS WELL AS PLACEMENT AND COMPACTION OF REQUIRED BEDDING MATERIAL TO UNIFORM GRADE SO THAT THE ENTIRE LENGTH OF THE PIPE WILL BE SUPPORTED ON A UNIFORMLY DENSE, UNYIELDING BASE. IF THE NATIVE MATERIAL IN THE BOTTOM OF THE TRENCH MEETS THE REQUIREMENTS FOR "GRAVEL BACKFILL FOR PIPE BEDDING," THE FIRST LIFT OF PIPE BEDDING MAY BE OMITTED PROVIDED THE MATERIAL IN THE BOTTOM OF THE TRENCH IS LOOSENEED, REGRADED, AND COMPACTED TO FORM A DENSE UNYIELDING BASE. ALL PIPE BEDDING SHALL BE APWA CLASS B, TYPE I, OR BETTER. PIPE SHALL NOT BE INSTALLED ON SOD, FROZEN EARTH, LARGE BOULDERS, OR ROCK. PIPE BEDDING FOR FLEXIBLE PIPES SHALL BE PEA GRAVEL TO THE SPRINGLINE OF THE PIPE.
- CONSTRUCTION OF DEWATERING DISCHARGES SHALL ALWAYS MEET WATER QUALITY GUIDELINES LISTED IN COK POLICY E-1. SPECIFICALLY, DISCHARGES TO THE PUBLIC STORMWATER DRAINAGE SYSTEM MUST BE BELOW 25NTU, AND NOT CONSIDERED A PROHIBITED DISCHARGE (PER KMC 15.52.090). TEMPORARY DISCHARGES TO SANITARY SEWER REQUIRE PRIOR AUTHORIZATION AND PERMIT FROM KING COUNTY INDUSTRIAL WASTE PROGRAM (206-263-3000) AND NOTIFICATION TO THE PUBLIC WORKS CONSTRUCTION INSPECTOR.
- ISSUANCE OF A BUILDING OR LAND SURFACE PLODIFICATION PERMIT BY THE CITY OF KIRKLAND DOES NOT RELIEVE THE OWNER OF THE CONTINUING LEGAL OBLIGATION AND/OR LIABILITY CONNECTED WITH STORM SURFACE WATER DISPOSITION. FURTHER, THE CITY OF KIRKLAND DOES NOT ACCEPT ANY OBLIGATION FOR THE PROPER FUNCTIONING AND MAINTENANCE OF THE SYSTEM DURING OR FOLLOWING CONSTRUCTION EXCEPT AS OUTLINED IN THE CITY OF KIRKLAND PUBLIC WORKS STANDARDS.
- ALL TRENCH BACKFILL SHALL BE COMPACTED TO 95 PERCENT DENSITY IN ROADWAYS, ROADWAY SHOULDERS, ROADWAY PRISM AND DRIVEWAYS, AND 85 PERCENT DENSITY IN UNPAVED AREAS. ALL PIPE ZONE COMPACTION SHALL BE 95 PERCENT.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING ADEQUATE SAFEGUARDS, SAFETY DEVICES, PROTECTIVE EQUIPMENT, CONFINED SPACE PROTECTION, FLAGGERS,

AND ANY OTHER NEEDED ACTIONS TO PROTECT THE LIFE, HEALTH, AND SAFETY OF THE PUBLIC, AND TO PROTECT PROPERTY IN CONNECTION WITH THE PERFORMANCE OF WORK COVERED BY THE CONTRACT. ANY WORK WITHIN THE TRAVELED RIGHT-OF-WAY THAT MAY INTERRUPT NORMAL TRAFFIC FLOW SHALL REQUIRE A TRAFFIC CONTROL PLAN APPROVED BY THE CITY OF KIRKLAND, ALL SECTIONS OF THE WSDOT STANDARD SPECIFICATIONS, TRAFFIC CONTROL, AND THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) SHALL APPLY.

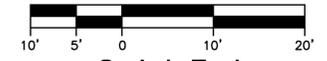
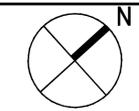
- NO FINAL CUT OR FILL SLOPE SHALL EXCEED SLOPES OF TWO (2) HORIZONTAL TO ONE (1) VERTICAL WITHOUT STABILIZATION BY ROCKERY OR BY A STRUCTURAL RETAINING WALL.
- ALL MANHOLE LADDERS SHALL BE FIRMLY ATTACHED AND EXTEND TO WITHIN 1' OF THE BOTTOM OF THE STRUCTURE.
- APPROXIMATE LOCATIONS OF EXISTING UTILITIES HAVE BEEN OBTAINED FROM AVAILABLE RECORDS AND ARE SHOWN FOR CONVENIENCE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFICATION OF EXISTING UTILITY LOCATIONS WHETHER OR NOT THESE UTILITIES ARE SHOWN ON THE PLANS. THE CONTRACTOR SHALL EXERCISE ALL CARE TO AVOID DAMAGE TO ANY UTILITY. IF CONFLICTS WITH EXISTING UTILITIES ARISE DURING CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE CITY CONSTRUCTION INSPECTOR AND ANY CHANGES REQUIRED SHALL BE APPROVED BY THE DEVELOPMENT ENGINEER PRIOR TO COMMENCEMENT OF RELATED CONSTRUCTION ON THE PROJECT.
- THE UNDERGROUND UTILITY LOCATION SERVICE SHALL BE CONTACTED FOR FIELD LOCATION OF EXISTING UTILITIES PRIOR TO ANY CONSTRUCTION. THE OWNER OR HIS REPRESENTATIVE SHALL BE CONTACTED IF A UTILITY CONFLICT EXISTS. FOR UTILITY LOCATION IN KING COUNTY, CALL 1-800-424-5555. THE CONTRACTOR IS RESPONSIBLE TO ENSURE THAT UTILITY LOCATES ARE MAINTAINED THROUGHOUT THE LIFE OF THE PROJECT.
- THE CONTRACTOR SHALL VERIFY THE LOCATIONS, WIDTHS, THICKNESSES, AND ELEVATIONS OF ALL EXISTING PAVEMENTS AND STRUCTURES THAT ARE TO INTERFACE WITH NEW WORK. PROVIDE ALL TRIMMING, CUTTING, SAW CUTTING, GRADING, LEVELING, SLOPING, COATING, AND OTHER WORK, INCLUDING MATERIALS AS NECESSARY, TO CAUSE THE INTERFACE WITH EXISTING WORKS TO BE PROPER, ACCEPTABLE TO THE ENGINEER AND THE CITY OF KIRKLAND, COMPLETE IN PLACE AND READY TO USE.
- ALL INLET, MANHOLE, AND CATCH BASIN FRAMES AND GRATES SHALL NOT BE ADJUSTED TO GRADE UNTIL IMMEDIATELY PRIOR TO FINAL PAVING. ALL CATCH BASIN GRATES SHALL BE SET 0.10' BELOW PAVEMENT LEVEL.
- OPEN CUT ROAD CROSSINGS FOR UTILITY TRENCHES ON EXISTING TRAVELED ROADWAY SHALL BE BACKFILLED ONLY WITH 5/8" MINUS CRUSHED ROCK AND MECHANICALLY COMPACTED (UNLESS OTHERWISE APPROVED BY THE CITY). FOR STREETS CLASSIFIED AS ARTERIALS OR COLLECTORS, BACKFILL FOR CROSSINGS SHALL BE CDF. CUTS INTO THE EXISTING ASPHALT SHALL BE NEAT LINE CUT WITH SAW OR JACKHAMMER IN A CONTINUOUS LINE. A TEMPORARY COLD MIX PATCH MUST BE PLACED IMMEDIATELY AFTER BACKFILL AND COMPACTION. A PERMANENT HOT MIX PATCH SHALL BE PLACED WITHIN 30 DAYS AND SHALL BE A MINIMUM OF 1" THICKER THAN THE ORIGINAL ASPHALT WITH A MINIMUM THICKNESS OF 2". SEE STANDARD D.02.
- ALL DAMAGES INCURRED TO PUBLIC AND/OR PRIVATE PROPERTY BY THE CONTRACTOR DURING THE COURSE OF CONSTRUCTION SHALL BE PROMPTLY REPAIRED TO THE SATISFACTION OF THE CITY CONSTRUCTION INSPECTOR BEFORE PROJECT APPROVAL AND/OR THE RELEASE OF THE PROJECT'S PERFORMANCE BOND.
- GROUT ALL SEAMS AND OPENINGS IN ALL INLETS, CATCH BASINS, AND MANHOLES. JETSET GROUT IS NOT ALLOWED.
- WHEN WIDENING AN EXISTING ROADWAY WHERE AN EXISTING TYPE I CATCH BASIN WILL REMAIN IN THE TRAVEL LANE, THE EXISTING FRAME AND COVER SHALL BE REPLACED WITH A ROUND, LOCKING FRAME AND COVER.
- FOR OTHER THAN SINGLE-FAMILY DWELLINGS, ALL EXPOSED OR READILY EXPOSED INDOOR STORM DRAINAGE PIPING/PLUMBING SHALL BE LABELED WITH THE WORDS "STORM DRAIN" WITH MINIMUM 2 INCH HIGH LETTERS.
- RECYCLED CONCRETE SHALL NOT BE USED AROUND STORMWATER FACILITIES.

INFILTRATION AND LID FACILITIES PROTECTION NOTES

- REDIRECT SHEET FLOW, BLOCK DRAIN INLETS AND/OR CURB OPENINGS IN PAVEMENT AND INSTALL FLOW DIVERSION MEASURES TO PREVENT CONSTRUCTION SILT LADEN WATER AND DEBRIS FROM ENTERING EXCAVATIONS AND FINISH SURFACES FOR INFILTRATION PONDS AND POROUS PAVEMENTS
- WHERE AMENDED SOILS, INFILTRATION PONDS, AND POROUS PAVEMENTS ARE INSTALLED, THESE AREAS MUST BE PROTECTED AT ALL TIMES FROM BEING OVER COMPACTED. REMEDIATE AND TILL SOIL IN ACCORDANCE WITH OWNER'S GEOTECHNICAL ENGINEER REPRESENTATIVE REQUIREMENTS AT NO ADDITIONAL COST TO OWNER IN ORDER TO RESTORE INFILTRATION.
- INSTALL FLOW DIVERSION MEASURES OUTSIDE OF THE CRITICAL ROOT ZONE OF TREES TO BE PROTECTED. AT NO TIME SHALL CONSTRUCTION STORMWATER BE DIRECTED TO TREES TO BE PROTECTED. CONSTRUCTION STORMWATER SHALL NOT POND WITHIN A TREE'S CRITICAL ROOT ZONE.

EITA NOTES

AN AUTOCAD® CIVIL 3D® 2017 FILE (FOR THE SOLE PURPOSE OF 2D HORIZONTAL LAYOUT) WILL BE PROVIDED TO THE CONTRACTOR AT THE BEGINNING OF THE PROJECT'S CONSTRUCTION. THE AUTOCAD FILE WILL PROVIDE FOR HORIZONTAL LAYOUT OF STORM DRAINAGE INFRASTRUCTURE, PAVING (TRAIL, SIDEWALK, CURBS), GRADING CONTOURS IN LANDSCAPE AREAS, SPOT ELEVATIONS, AND LANDSCAPING SHOWN ON PLAN VIEW OF THE GRADING AND DRAINAGE AND RESTORATION PLANS. PRIOR TO RELEASE OF AUTOCAD FILE, CONTRACTOR WILL BE REQUIRED TO SIGN AN ELECTRONIC INFORMATION TRANSFER AGREEMENT (EITA) FORM FROM MIG/SVR. PLEASE SUBMIT REQUEST FOR MIG/SVR'S EITA FORM THROUGH THE CITY'S PROJECT CONTACT ESTABLISHED FOR THE JOB. THE CITY'S PROJECT CONTACT WILL THEN NOTIFY MIG/SVR'S PROJECT MANAGER FOR A COPY OF THE EITA FORM. AFTER MIG/SVR HAS RECEIVED THE SIGNED EITA FORM, ALLOW UP TO SEVEN (7) BUSINESS DAYS FOR PREPARATION OF THE FILE BY MIG/SVR. IF DISCREPANCIES ARE OBSERVED BETWEEN THE AUTOCAD FILE PROVIDED AND INFORMATION SHOWN ON THE HARD COPY OF THE CONSTRUCTION DOCUMENTS, NOTIFY PROJECT CONTACT BY SUBMITTING A RFI.



LEGEND

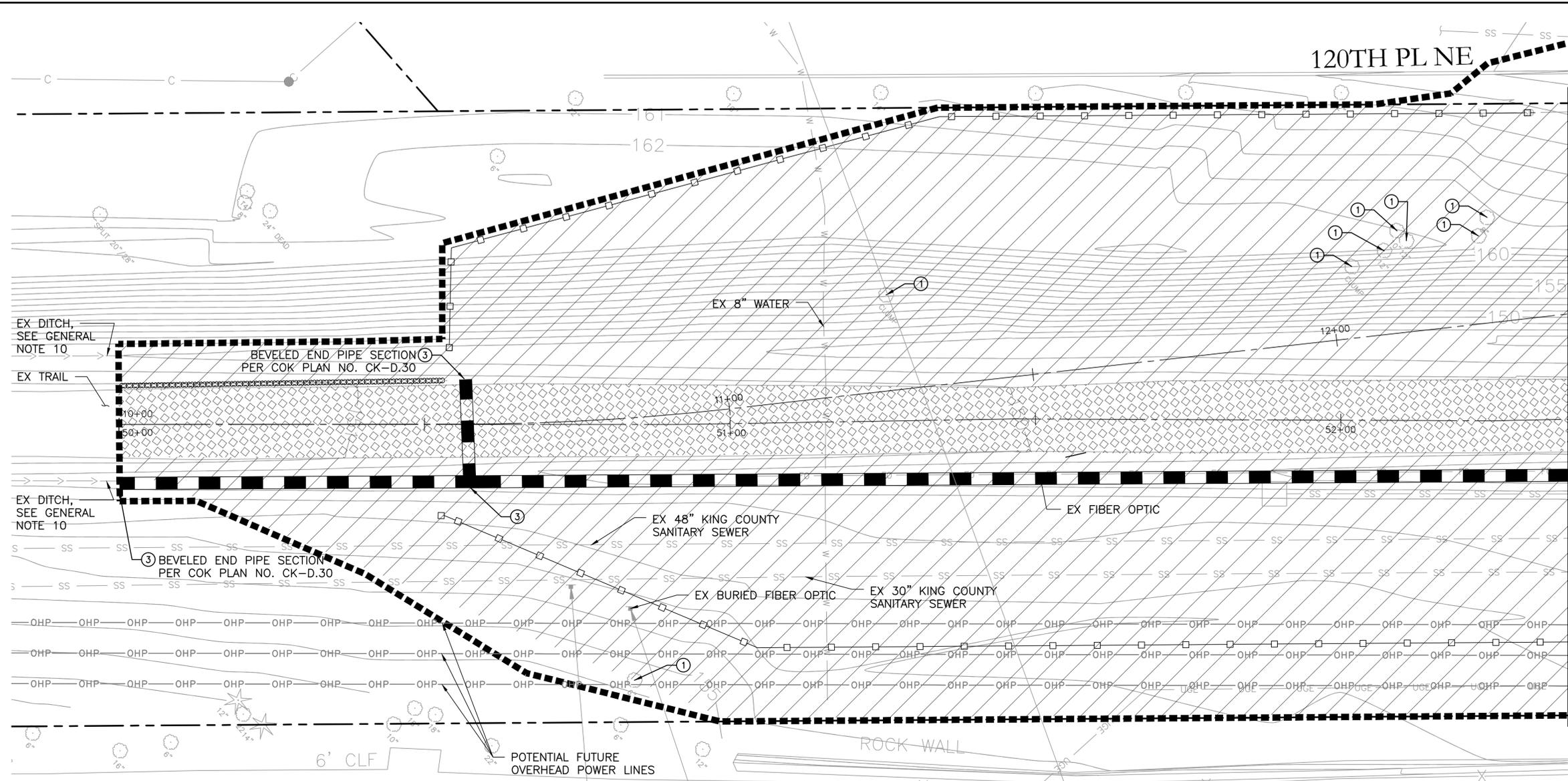
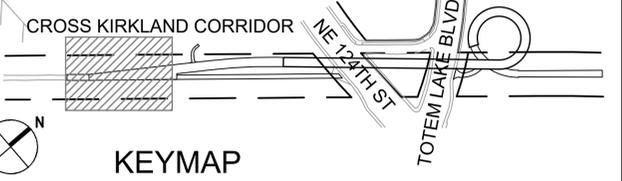
- RIGHT OF WAY LINE
- APPROXIMATE LIMITS OF WORK
- CLEAR & GRUB (SEE GENERAL NOTE 4)
- CLEAR & GRUB OUTSIDE LIMITS OF HIGH VISIBILITY SILT FENCE, SEE SPECIAL PROVISIONS
- REMOVE PAVEMENT
- REMOVE EXISTING GRAVEL SURFACING
- WATTLE (CK-E.10)
- HIGH VISIBILITY FENCE (WSDOT STD PLAN I-10.10-01)
- SILT FENCE (CK-E.03), UNLESS NOTED OTHERWISE
- TEMPORARY STORM DRAIN BYPASS
- FORCE MAIN / PUMPED STORMWATER

GENERAL NOTES

1. ALL UTILITIES DESCRIBED AS "FUTURE" ARE PART OF SEPARATE PROJECTS. THESE PLANS ASSUME ALL COMFORT INN BYPASS INFRASTRUCTURE IS INSTALLED PRIOR TO START OF CONSTRUCTION.
2. SEE SHEET C-170 FOR CITY OF KIRKLAND STANDARD EROSION/SEDIMENTATION CONTROL NOTES.
3. PROTECT EXISTING INFRASTRUCTURE, SITE FEATURES AND STRUCTURES NOT IDENTIFIED FOR REMOVAL.
4. INSTALL TEMPORARY SLOPE STABILIZATION (CK-E.06) WHERE SLOPE IS GREATER THAN 3H:1V.
5. NOT USED.
6. CONTRACTOR TO LOCATE AND PROTECT ALL EXISTING UTILITIES TO REMAIN.
7. SEE SPECIFICATIONS FOR HANDLING, CHARACTERIZATION, AND DISPOSAL OF POTENTIALLY IMPACTED SOILS.
8. CONFIRM STOCKPILING, LAYDOWN AREAS, AND ALL WORK WITHIN VICINITY OF KING COUNTY SANITARY SEWERS WITH ENGINEER PRIOR TO CONSTRUCTION.
9. REDIRECT DITCH FLOW AND SIZE TEMPORARY STORM DRAIN BYPASS TO CONVEY 100YR FLOW 13.9 CFS TO DOWNSTREAM STORM DRAIN SYSTEM.
10. SEE SPECIAL PROVISIONS AND HAZARDOUS MATERIALS REPORT IN PROJECT MANUAL APPENDIX FOR REQUIREMENTS FOR REMOVAL, HANDLING, AND DISPOSAL OF CONTAMINATED SOIL.

CONSTRUCTION NOTES

- ① REMOVE TREE
- ② INLET PROTECTION (KC E.11)
- ③ TEMPORARY STORM DRAIN BYPASS, SEE GENERAL NOTE 9
- ④ REMOVE & SALVAGE EXISTING
- ⑤ REMOVE EXISTING



MATCHLINE - SEE SHEET C-172

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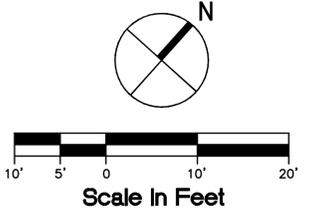
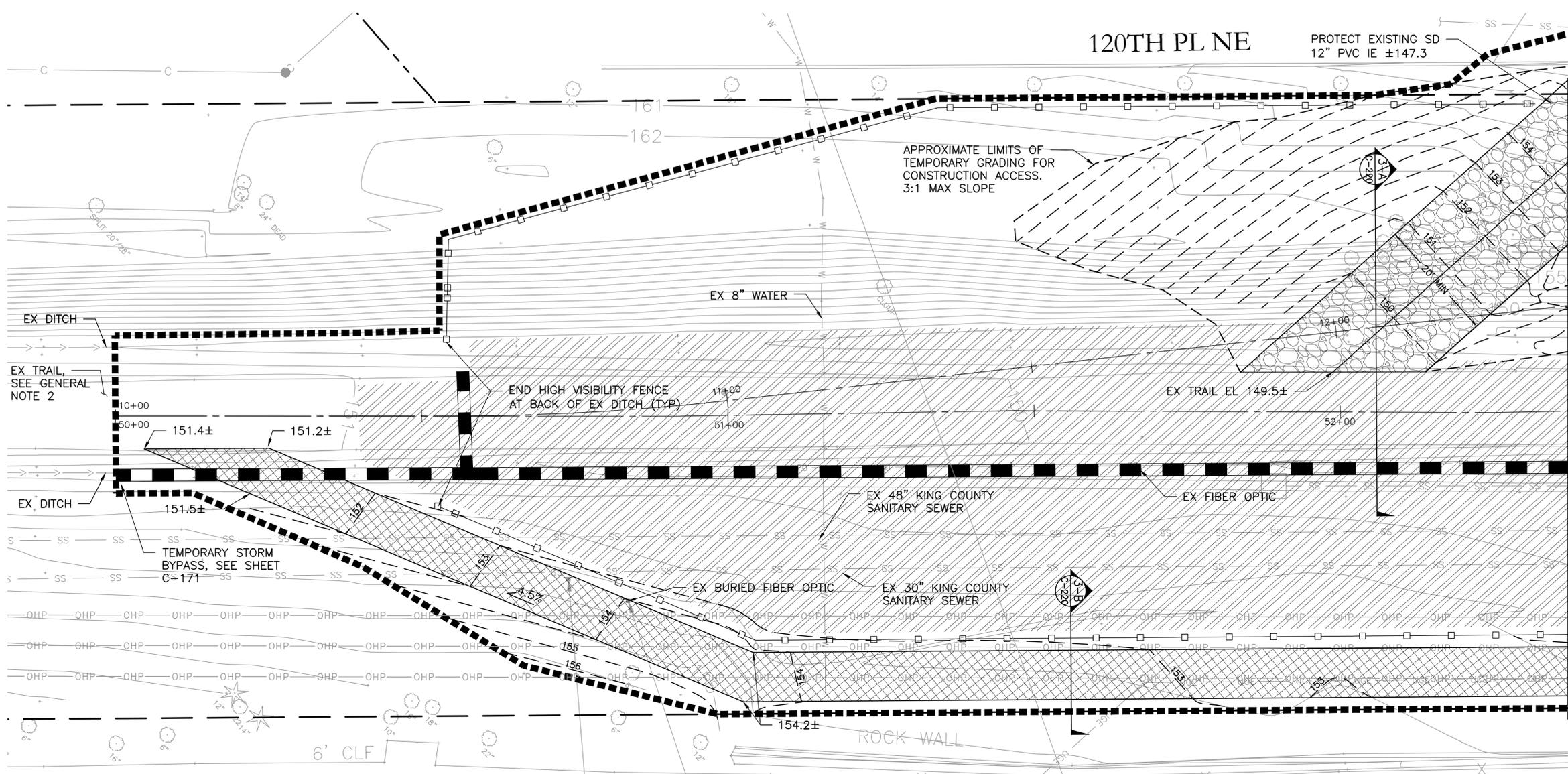


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 DATE: _____

ST 2019/12/06 DATE
 DESIGNED BY: _____
 JA 2019/12/06 DATE
 DRAWN BY: _____
 KG 2019/12/06 DATE
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CITY OF KIRKLAND
 TOTEM LAKE PEDESTRIAN BRIDGE
 DEMOLITION AND EROSION CONTROL PLAN 1

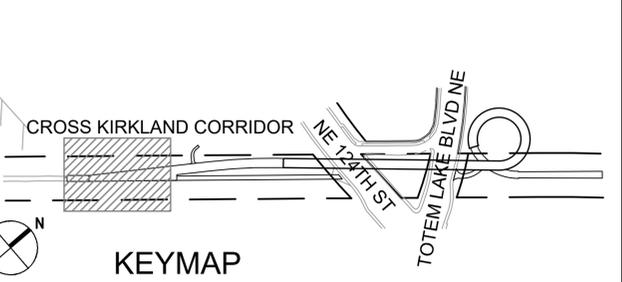
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 SCALE: **AS SHOWN**
 FILENAME: TLPB_170_TESC-Demo.dwg



- LEGEND**
- RIGHT OF WAY LINE
 - APPROXIMATE LIMITS OF WORK
 - STABILIZED CONSTRUCTION ENTRANCE (CK-E.02) (SEE NOTE 2)
 - TEMPORARY GRAVEL TRAIL PER (3-B) C-220
 - QUARRY SPALLS FOR CONSTRUCTION LAYDOWN
 - TEMPORARY STORM DRAIN BYPASS, SEE SHEETS C-171 TO C-173

- GENERAL NOTES**
1. SEE C-171 TO C-177 FOR DEMOLITION AND EROSION CONTROL PLANS.
 2. CONTRACTOR TO COORDINATE CONSTRUCTION ENTRANCES AND EXITS WITH CITY OF KIRKLAND (SEE GENERAL NOTE 4). FOLLOWING CONSTRUCTION CONTRACTOR SHALL RESTORE DISTURBED AREAS OUTSIDE THE LIMITS OF WORK THAT ARE DISTURBED BY CONTRACTOR MEANS AND METHODS AND TEMPORARY CONSTRUCTION ACCESS INCLUDING BUT NOT LIMITED TO THE MEDIAN AND RECTANGULAR RAPID FLASHING BEACON (RRFB) AT 120TH AVE NE AND CRUSHED SURFACING FOR TRAIL ALONG THE CROSS KIRKLAND CORRIDOR.
 3. PROVIDE TEMPORARY CHAIN LINK CONSTRUCTION FENCING TO SECURE THE WORK AREA AND MAINTAIN TEMPORARY TRAIL.
 4. FOR AREAS OUTSIDE LIMITS OF WORK: AS A PART OF PROJECT TEMPORARY TRAFFIC CONTROL PROVIDE HIGH VISIBILITY FENCE, FLAGGERS, AND/OR OTHER TEMPORARY MEASURES TO CONTROL ACCESS TO THE CROSS KIRKLAND CORRIDOR ALONG TEMPORARY CONSTRUCTION ACCESS ROUTES FOR SAFETY OF PEOPLE WALKING AND BIKING ALONG TRAIL.

MATCHLINE - SEE SHEET C-172A

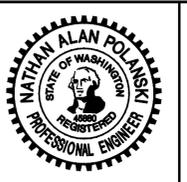


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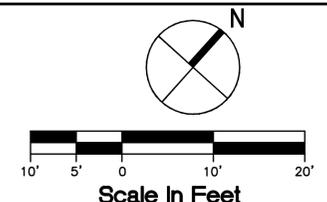
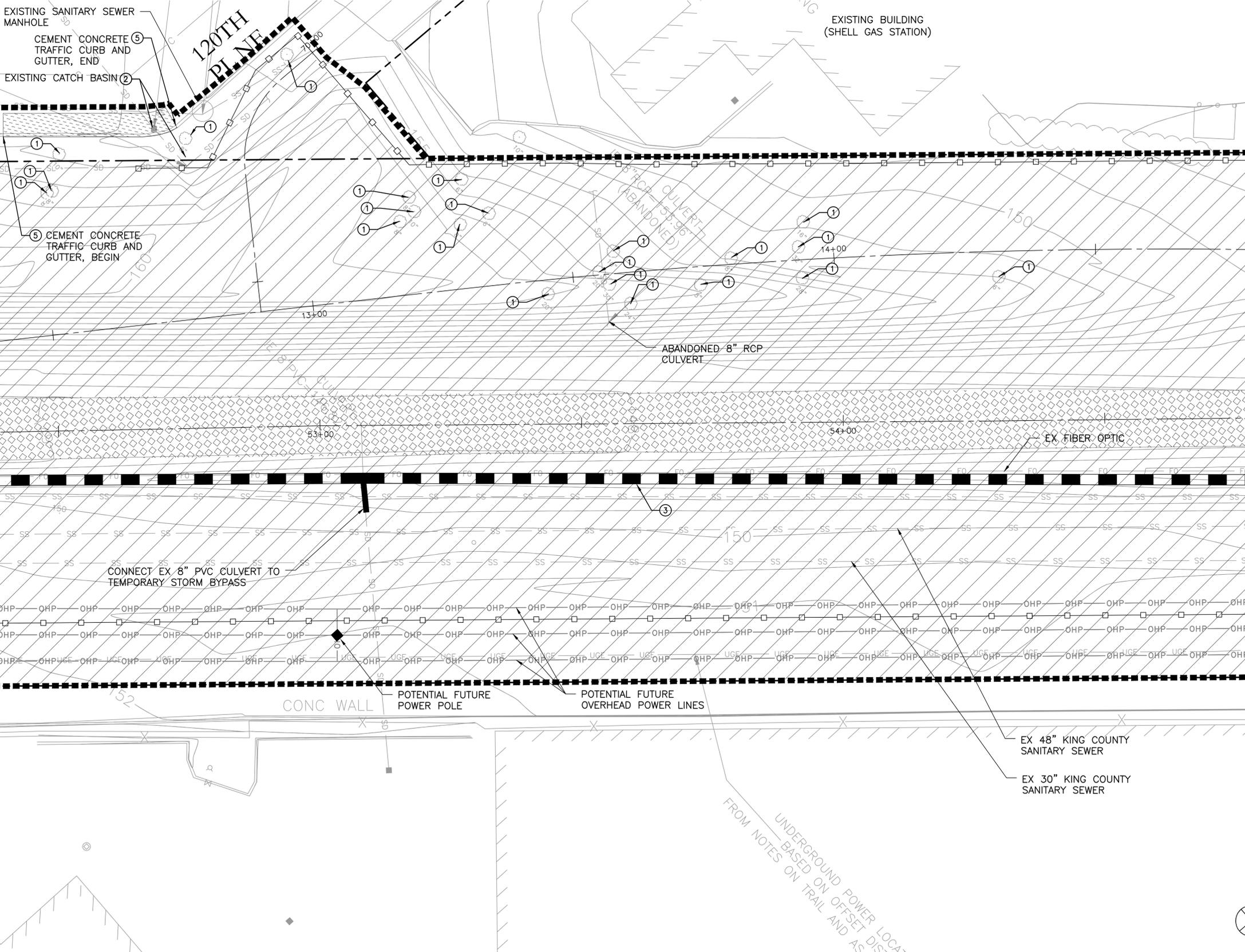


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 JA DRAWN BY: 2019/12/06 DATE
 KG CHECKED BY: 2019/12/06 DATE

CITY OF KIRKLAND
 TOTEM LAKE PEDESTRIAN BRIDGE
 CONSTRUCTION ACCESS AND
 TEMPORARY TRAIL PLAN 1

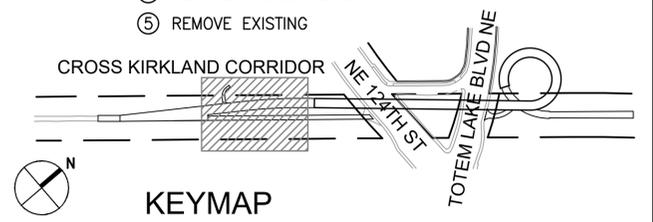
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TLC-C-171A
 SCALE:
AS SHOWN
 FILENAME:
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- LEGEND**
- RIGHT OF WAY LINE
 - APPROXIMATE LIMITS OF WORK
 - CLEAR & GRUB (SEE GENERAL NOTE 4)
 - CLEAR & GRUB OUTSIDE LIMITS OF HIGH VISIBILITY SILT FENCE, SEE SPECIAL PROVISIONS
 - REMOVE PAVEMENT
 - REMOVE EXISTING GRAVEL SURFACING
 - WATTLE (CK-E.10)
 - HIGH VISIBILITY FENCE (WSDOT STD PLAN I-10.10-01)
 - SILT FENCE (CK-E.03), UNLESS NOTED OTHERWISE
 - TEMPORARY STORM DRAIN BYPASS
 - FORCE MAIN / PUMPED STORMWATER

- GENERAL NOTES**
1. ALL UTILITIES DESCRIBED AS "FUTURE" ARE PART OF SEPARATE PROJECTS. THESE PLANS ASSUME ALL COMFORT INN BYPASS INFRASTRUCTURE IS INSTALLED PRIOR TO START OF CONSTRUCTION.
 2. SEE SHEET C-170 FOR CITY OF KIRKLAND STANDARD EROSION/SEDIMENTATION CONTROL NOTES.
 3. PROTECT EXISTING INFRASTRUCTURE, SITE FEATURES AND STRUCTURES NOT IDENTIFIED FOR REMOVAL.
 4. INSTALL TEMPORARY SLOPE STABILIZATION (CK-E.06) WHERE SLOPE IS GREATER THAN 3H:1V.
 5. NOT USED.
 6. CONTRACTOR TO LOCATE AND PROTECT ALL EXISTING UTILITIES TO REMAIN.
 7. SEE SPECIFICATIONS FOR HANDLING, CHARACTERIZATION, AND DISPOSAL OF POTENTIALLY IMPACTED SOILS.
 8. CONFIRM STOCKPILING, LAYDOWN AREAS, AND ALL WORK WITHIN VICINITY OF KING COUNTY SANITARY SEWERS WITH ENGINEER PRIOR TO CONSTRUCTION.
 9. REDIRECT DITCH FLOW AND SIZE TEMPORARY STORM DRAIN BYPASS TO CONVEY 100YR FLOW 13.9 CFS TO DOWNSTREAM STORM DRAIN SYSTEM.
 10. SEE SPECIAL PROVISIONS AND HAZARDOUS MATERIALS REPORT IN PROJECT MANUAL APPENDIX FOR REQUIREMENTS FOR REMOVAL, HANDLING, AND DISPOSAL OF CONTAMINATED SOIL.

- CONSTRUCTION NOTES**
- ① REMOVE TREE
 - ② INLET PROTECTION (KC E.11)
 - ③ TEMPORARY STORM DRAIN BYPASS, SEE GENERAL NOTE 9
 - ④ REMOVE & SALVAGE EXISTING
 - ⑤ REMOVE EXISTING



MATCHLINE - SEE SHEET C-171

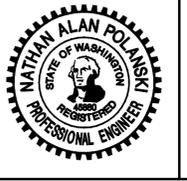
MATCHLINE - SEE SHEET C-173

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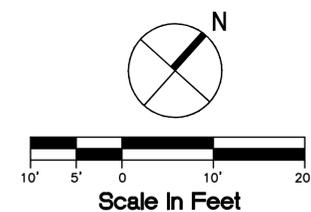


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CITY OF KIRKLAND
 TOTEM LAKE PEDESTRIAN BRIDGE
 DEMOLITION AND EROSION CONTROL PLAN 2

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LEGEND

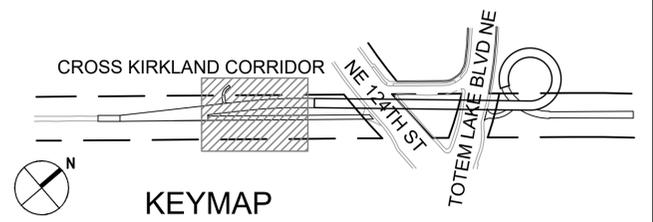
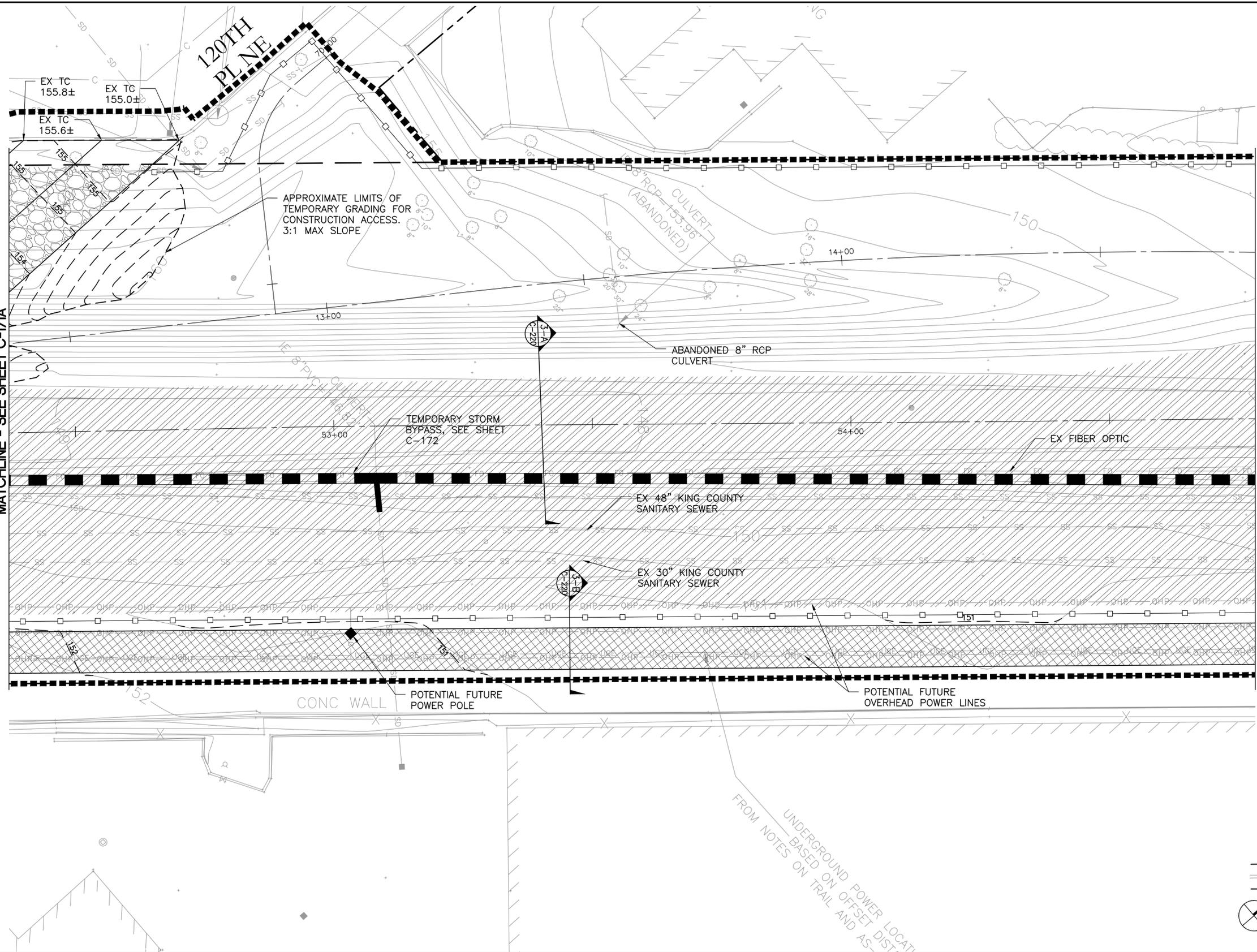
- RIGHT OF WAY LINE
- APPROXIMATE LIMITS OF WORK
- STABILIZED CONSTRUCTION ENTRANCE (CK-E.02) (SEE NOTE 2)
- TEMPORARY GRAVEL TRAIL PER (3-B) C-220
- QUARRY SPALLS FOR CONSTRUCTION LAYDOWN
- TEMPORARY STORM DRAIN BYPASS, SEE SHEETS C-171 TO C-173

GENERAL NOTES

1. SEE C-171 TO C-177 FOR DEMOLITION AND EROSION CONTROL PLANS.
2. CONTRACTOR TO COORDINATE CONSTRUCTION ENTRANCES AND EXITS WITH CITY OF KIRKLAND (SEE GENERAL NOTE 4). FOLLOWING CONSTRUCTION CONTRACTOR SHALL RESTORE DISTURBED AREAS OUTSIDE THE LIMITS OF WORK THAT ARE DISTURBED BY CONTRACTOR MEANS AND METHODS AND TEMPORARY CONSTRUCTION ACCESS INCLUDING BUT NOT LIMITED TO THE MEDIAN AND RECTANGULAR RAPID FLASHING BEACON (RRFB) AT 120TH AVE NE AND CRUSHED SURFACING FOR TRAIL ALONG THE CROSS KIRKLAND CORRIDOR.
3. PROVIDE TEMPORARY CHAIN LINK CONSTRUCTION FENCING TO SECURE THE WORK AREA AND MAINTAIN TEMPORARY TRAIL.
4. FOR AREAS OUTSIDE LIMITS OF WORK: AS A PART OF PROJECT TEMPORARY TRAFFIC CONTROL PROVIDE HIGH VISIBILITY FENCE, FLAGGERS, AND/OR OTHER TEMPORARY MEASURES TO CONTROL ACCESS TO THE CROSS KIRKLAND CORRIDOR ALONG TEMPORARY CONSTRUCTION ACCESS ROUTES FOR SAFETY OF PEOPLE WALKING AND BIKING ALONG TRAIL.

MATCHLINE - SEE SHEET C-171A

MATCHLINE - SEE SHEET C-173A



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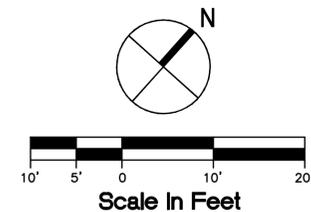


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CITY OF KIRKLAND
 TOTEM LAKE PEDESTRIAN BRIDGE
 CONSTRUCTION ACCESS AND
 TEMPORARY TRAIL PLAN 2

SHEET:
TLC-C-172A
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AS SHOWN
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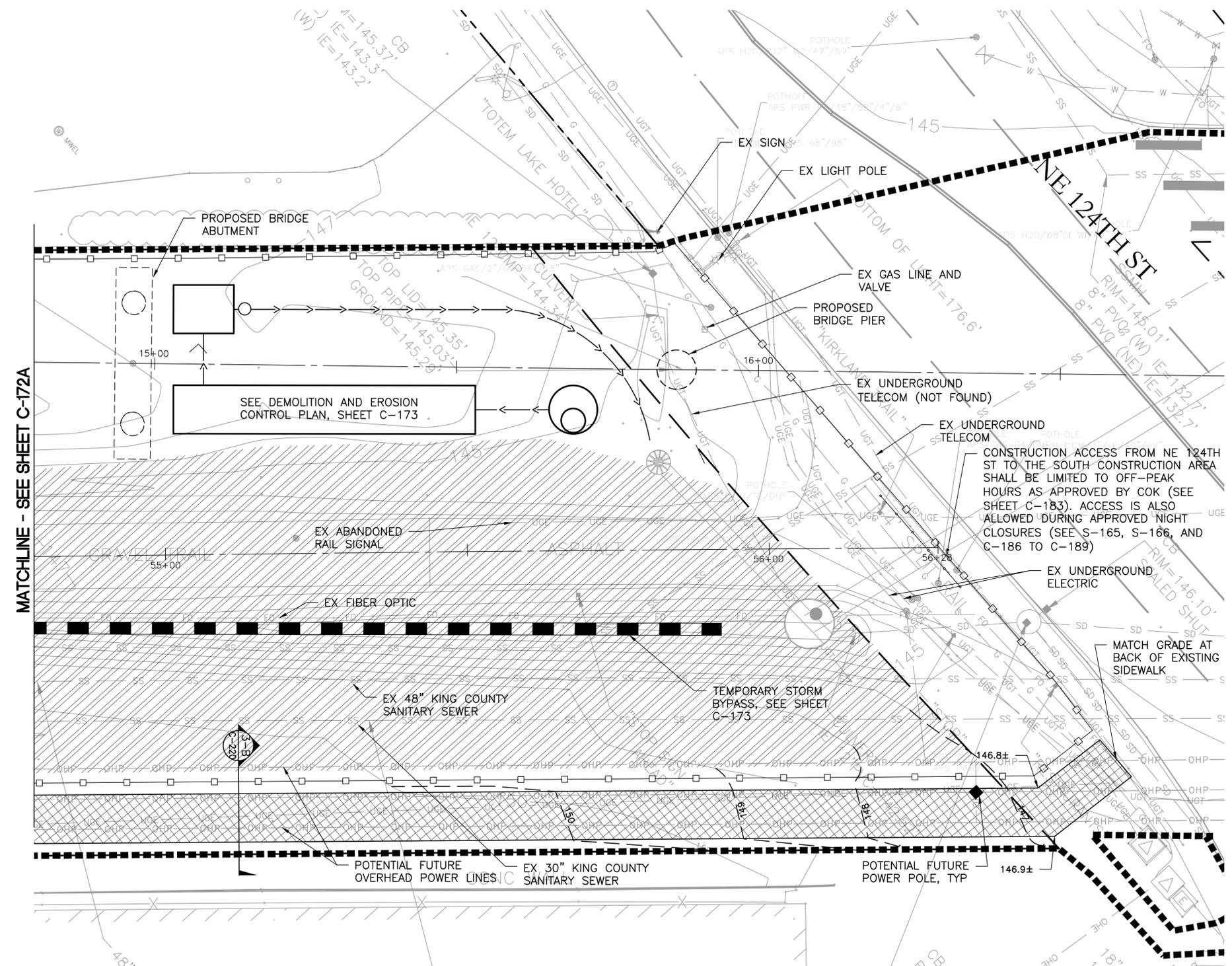


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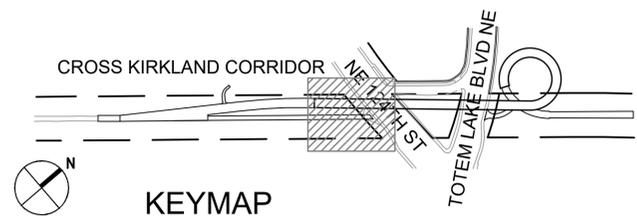
- RIGHT OF WAY LINE
- APPROXIMATE LIMITS OF WORK
- STABILIZED CONSTRUCTION ENTRANCE (CK-E.02) (SEE NOTE 2)
- TEMPORARY GRAVEL TRAIL PER (3-B C-220)
- QUARRY SPALLS FOR CONSTRUCTION LAYDOWN
- TEMPORARY STORM DRAIN BYPASS, SEE SHEETS C-171 TO C-173

GENERAL NOTES

1. SEE C-171 TO C-177 FOR DEMOLITION AND EROSION CONTROL PLANS.
2. CONTRACTOR TO COORDINATE CONSTRUCTION ENTRANCES AND EXITS WITH CITY OF KIRKLAND (SEE GENERAL NOTE 4). FOLLOWING CONSTRUCTION CONTRACTOR SHALL RESTORE DISTURBED AREAS OUTSIDE THE LIMITS OF WORK THAT ARE DISTURBED BY CONTRACTOR MEANS AND METHODS AND TEMPORARY CONSTRUCTION ACCESS INCLUDING BUT NOT LIMITED TO THE MEDIAN AND RECTANGULAR RAPID FLASHING BEACON (RRFB) AT 120TH AVE NE AND CRUSHED SURFACING FOR TRAIL ALONG THE CROSS KIRKLAND CORRIDOR.
3. PROVIDE TEMPORARY CHAIN LINK CONSTRUCTION FENCING TO SECURE THE WORK AREA AND MAINTAIN TEMPORARY TRAIL.
4. FOR AREAS OUTSIDE LIMITS OF WORK: AS A PART OF PROJECT TEMPORARY TRAFFIC CONTROL PROVIDE HIGH VISIBILITY FENCE, FLAGGERS, AND/OR OTHER TEMPORARY MEASURES TO CONTROL ACCESS TO THE CROSS KIRKLAND CORRIDOR ALONG TEMPORARY CONSTRUCTION ACCESS ROUTES FOR SAFETY OF PEOPLE WALKING AND BIKING ALONG TRAIL.



MATCHLINE - SEE SHEET C-172A



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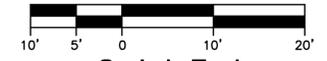
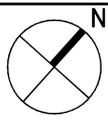


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 JA DRAWN BY: 2019/12/06 DATE
 KG CHECKED BY: 2019/12/06 DATE

CITY OF KIRKLAND
 TOTEM LAKE PEDESTRIAN BRIDGE
 CONSTRUCTION ACCESS AND
 TEMPORARY TRAIL PLAN 3

SHEET:
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AS SHOWN
 FILENAME:
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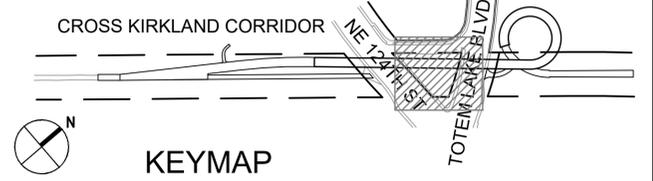
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- APPROXIMATE LIMITS OF WORK
- ▨ CLEAR & GRUB (SEE GENERAL NOTE 4)
- ▧ CLEAR & GRUB OUTSIDE LIMITS OF HIGH VISIBILITY SILT FENCE, SEE SPECIAL PROVISIONS
- ▩ REMOVE PAVEMENT
- REMOVE EXISTING GRAVEL SURFACING
- ⋯ WATTLE (CK-E.10)
- HIGH VISIBILITY FENCE (WSDOT STD PLAN I-10.10-01)
- SILT FENCE (CK-E.03), UNLESS NOTED OTHERWISE
- ▬ TEMPORARY STORM DRAIN BYPASS
- FORCE MAIN / PUMPED STORMWATER

GENERAL NOTES

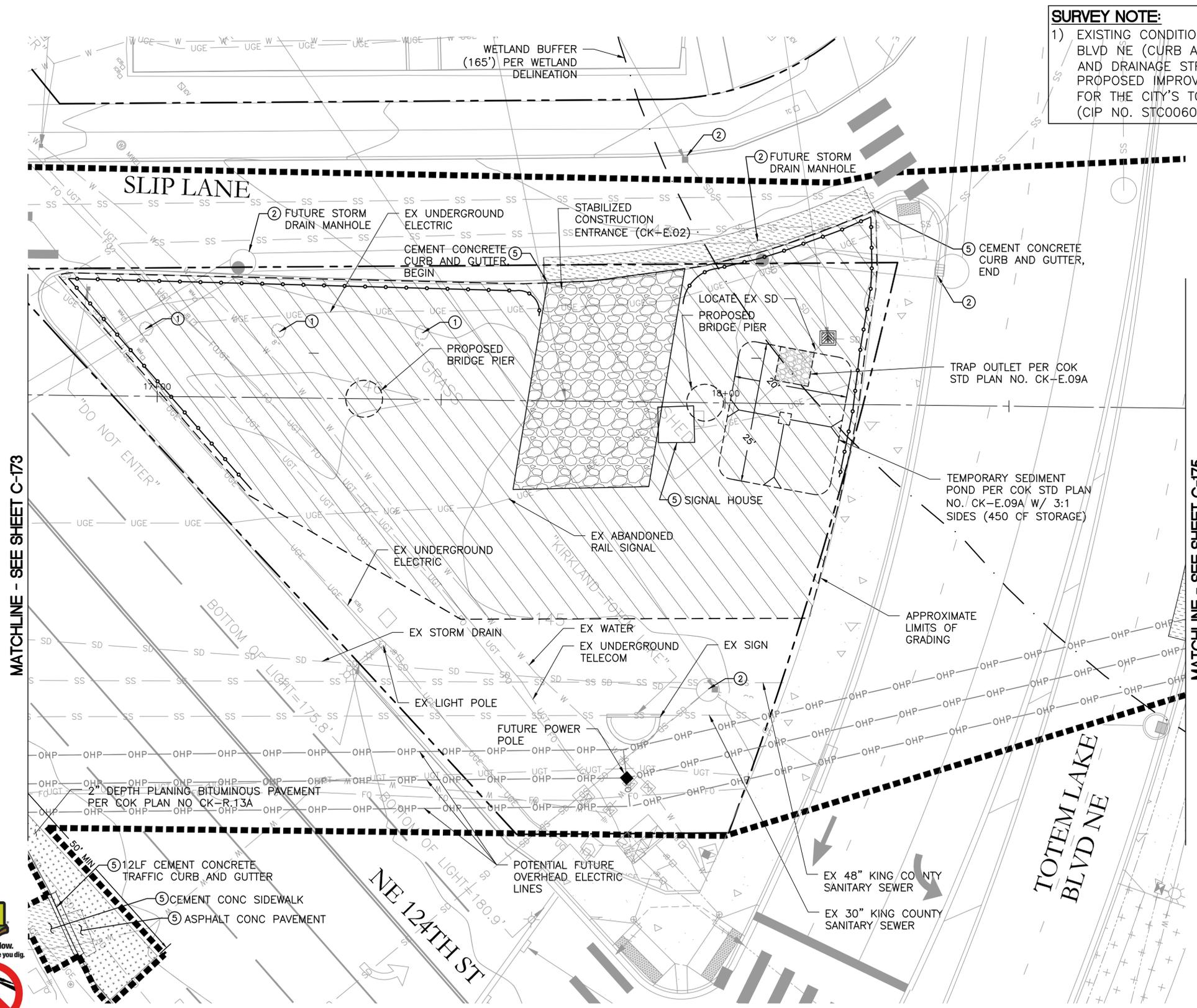
1. ALL UTILITIES DESCRIBED AS "FUTURE" ARE PART OF SEPARATE PROJECTS. THESE PLANS ASSUME ALL COMFORT INN BYPASS INFRASTRUCTURE IS INSTALLED PRIOR TO START OF CONSTRUCTION.
2. SEE SHEET C-170 FOR CITY OF KIRKLAND STANDARD EROSION/SEDIMENTATION CONTROL NOTES.
3. PROTECT EXISTING INFRASTRUCTURE, SITE FEATURES AND STRUCTURES NOT IDENTIFIED FOR REMOVAL.
4. INSTALL TEMPORARY SLOPE STABILIZATION (CK-E.06) WHERE SLOPE IS GREATER THAN 3H:1V.
5. NOT USED.
6. CONTRACTOR TO LOCATE AND PROTECT ALL EXISTING UTILITIES TO REMAIN.
7. SEE SPECIFICATIONS FOR HANDLING, CHARACTERIZATION, AND DISPOSAL OF POTENTIALLY IMPACTED SOILS.
8. CONFIRM STOCKPILING, LAYDOWN AREAS, AND ALL WORK WITHIN VICINITY OF KING COUNTY SANITARY SEWERS WITH ENGINEER PRIOR TO CONSTRUCTION.
9. REDIRECT DITCH FLOW AND SIZE TEMPORARY STORM DRAIN BYPASS TO CONVEY 100YR FLOW 13.9 CFS TO DOWNSTREAM STORM DRAIN SYSTEM.
10. SEE SPECIAL PROVISIONS AND HAZARDOUS MATERIALS REPORT IN PROJECT MANUAL APPENDIX FOR REQUIREMENTS FOR REMOVAL, HANDLING, AND DISPOSAL OF CONTAMINATED SOIL.

CONSTRUCTION NOTES

- ① REMOVE TREE
- ② INLET PROTECTION (KC E.11)
- ③ TEMPORARY STORM DRAIN BYPASS, SEE GENERAL NOTE 9
- ④ REMOVE & SALVAGE EXISTING
- ⑤ REMOVE EXISTING



SURVEY NOTE:
 1) EXISTING CONDITIONS SHOWN ALONG TOTEM LAKE BLVD NE (CURB ALIGNMENT, SIDEWALK, STRIPING AND DRAINAGE STRUCTURES) ARE BASED ON PROPOSED IMPROVEMENTS SHOWN IN THE BID SET FOR THE CITY'S TOTEM LAKE GATEWAY PROJECT (CIP NO. STC006050).



MATCHLINE - SEE SHEET C-173

MATCHLINE - SEE SHEET C-175

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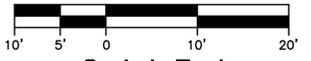
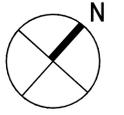
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JA	DRAWN BY:	2019/12/06	DATE
KG	CHECKED BY:	2019/12/06	DATE

CITY OF KIRKLAND
 TOTEM LAKE PEDESTRIAN BRIDGE
 DEMOLITION AND EROSION CONTROL PLAN 4

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MATCHLINE - SEE SHEET C-176



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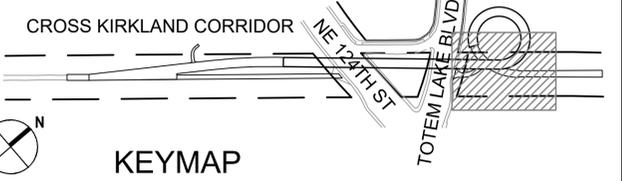
- RIGHT OF WAY LINE
- APPROXIMATE LIMITS OF WORK
- CLEAR & GRUB (SEE GENERAL NOTE 4)
- CLEAR & GRUB OUTSIDE LIMITS OF HIGH VISIBILITY SILT FENCE, SEE SPECIAL PROVISIONS
- REMOVE PAVEMENT
- REMOVE EXISTING GRAVEL SURFACING
- WATTLE (CK-E.10)
- HIGH VISIBILITY FENCE (WSDOT STD PLAN I-10.10-01)
- SILT FENCE (CK-E.03), UNLESS NOTED OTHERWISE
- TEMPORARY STORM DRAIN BYPASS
- FORCE MAIN / PUMPED STORMWATER

GENERAL NOTES

1. ALL UTILITIES DESCRIBED AS "FUTURE" ARE PART OF SEPARATE PROJECTS. THESE PLANS ASSUME ALL COMFORT INN BYPASS INFRASTRUCTURE IS INSTALLED PRIOR TO START OF CONSTRUCTION.
2. SEE SHEET C-170 FOR CITY OF KIRKLAND STANDARD EROSION/SEDIMENTATION CONTROL NOTES.
3. PROTECT EXISTING INFRASTRUCTURE, SITE FEATURES AND STRUCTURES NOT IDENTIFIED FOR REMOVAL.
4. INSTALL TEMPORARY SLOPE STABILIZATION (CK-E.06) WHERE SLOPE IS GREATER THAN 3H:1V.
5. NOT USED.
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10. SEE SPECIAL PROVISIONS AND HAZARDOUS MATERIALS REPORT IN PROJECT MANUAL APPENDIX FOR REQUIREMENTS FOR REMOVAL, HANDLING, AND DISPOSAL OF CONTAMINATED SOIL.

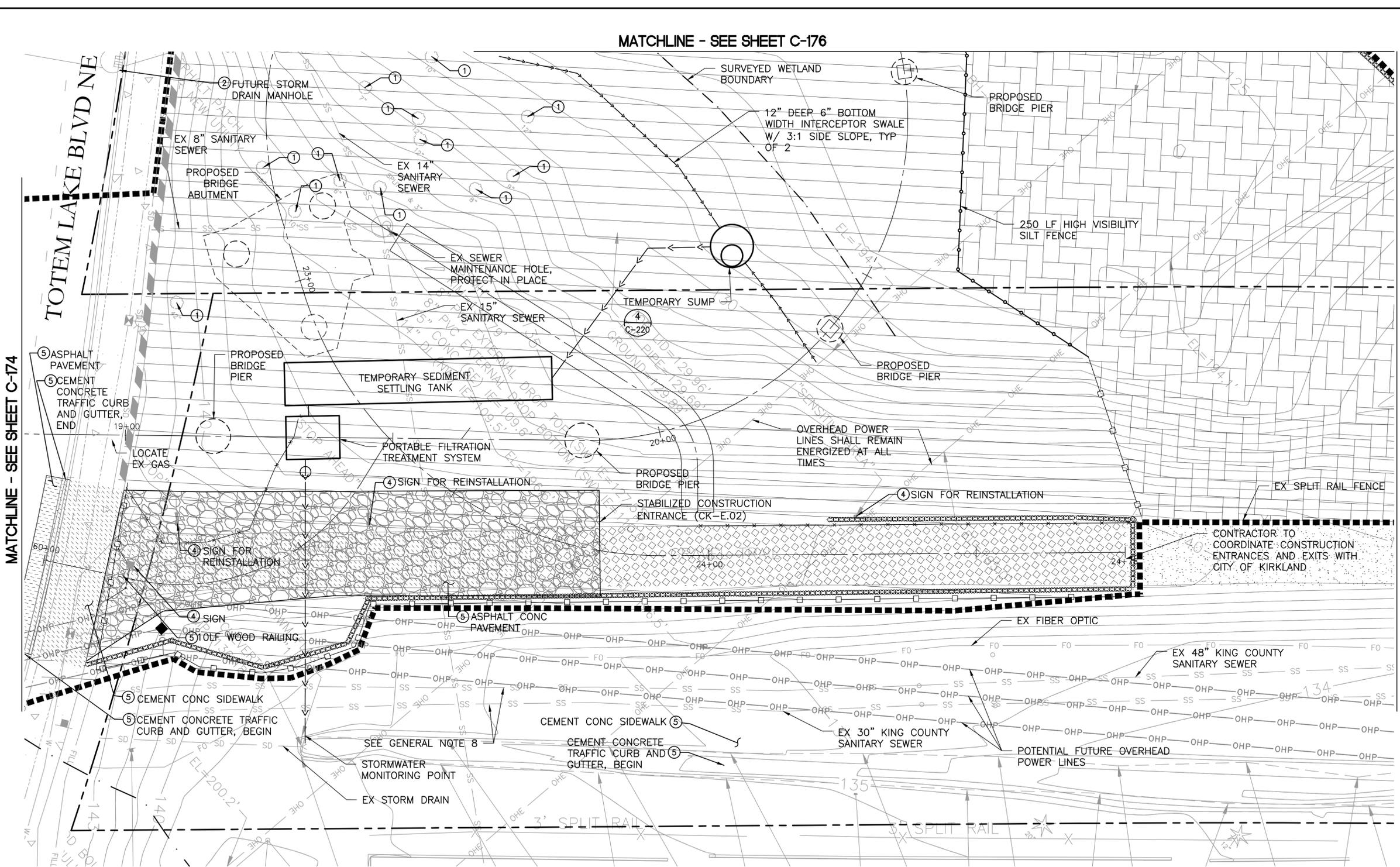
CONSTRUCTION NOTES

- ① REMOVE TREE
- ② INLET PROTECTION (KC E.11)
- ③ TEMPORARY STORM DRAIN BYPASS, SEE GENERAL NOTE 9
- ④ REMOVE & SALVAGE EXISTING
- ⑤ REMOVE EXISTING



MATCHLINE - SEE SHEET C-174

MATCHLINE - SEE SHEET C-177



SURVEY NOTE:
 1) EXISTING CONDITIONS SHOWN ALONG TOTEM LAKE BLVD NE (CURB ALIGNMENT, SIDEWALK, STRIPING AND DRAINAGE STRUCTURES) ARE BASED ON PROPOSED IMPROVEMENTS SHOWN IN THE BID SET FOR THE CITY'S TOTEM LAKE GATEWAY PROJECT (CIP NO. STC006050).

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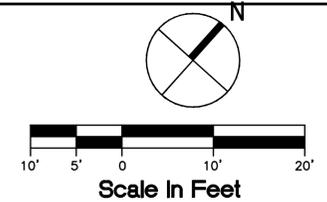
ST DESIGNED BY: 2019/12/06 DATE
 JA DRAWN BY: 2019/12/06 DATE
 KG CHECKED BY: 2019/12/06 DATE

CITY OF KIRKLAND
 TOTEM LAKE PEDESTRIAN BRIDGE
 DEMOLITION AND EROSION CONTROL PLAN 5

SHEET: TLC - C - 175
 SCALE: AS SHOWN
 FILENAME: TLPB_170_TESC-Demo.dwg

SURVEY NOTE:

1) EXISTING CONDITIONS SHOWN ALONG TOTEM LAKE BLVD NE (CURB ALIGNMENT, SIDEWALK, STRIPING AND DRAINAGE STRUCTURES) ARE BASED ON PROPOSED IMPROVEMENTS SHOWN IN THE BID SET FOR THE CITY'S TOTEM LAKE GATEWAY PROJECT (CIP NO. STC006050).



LEGEND

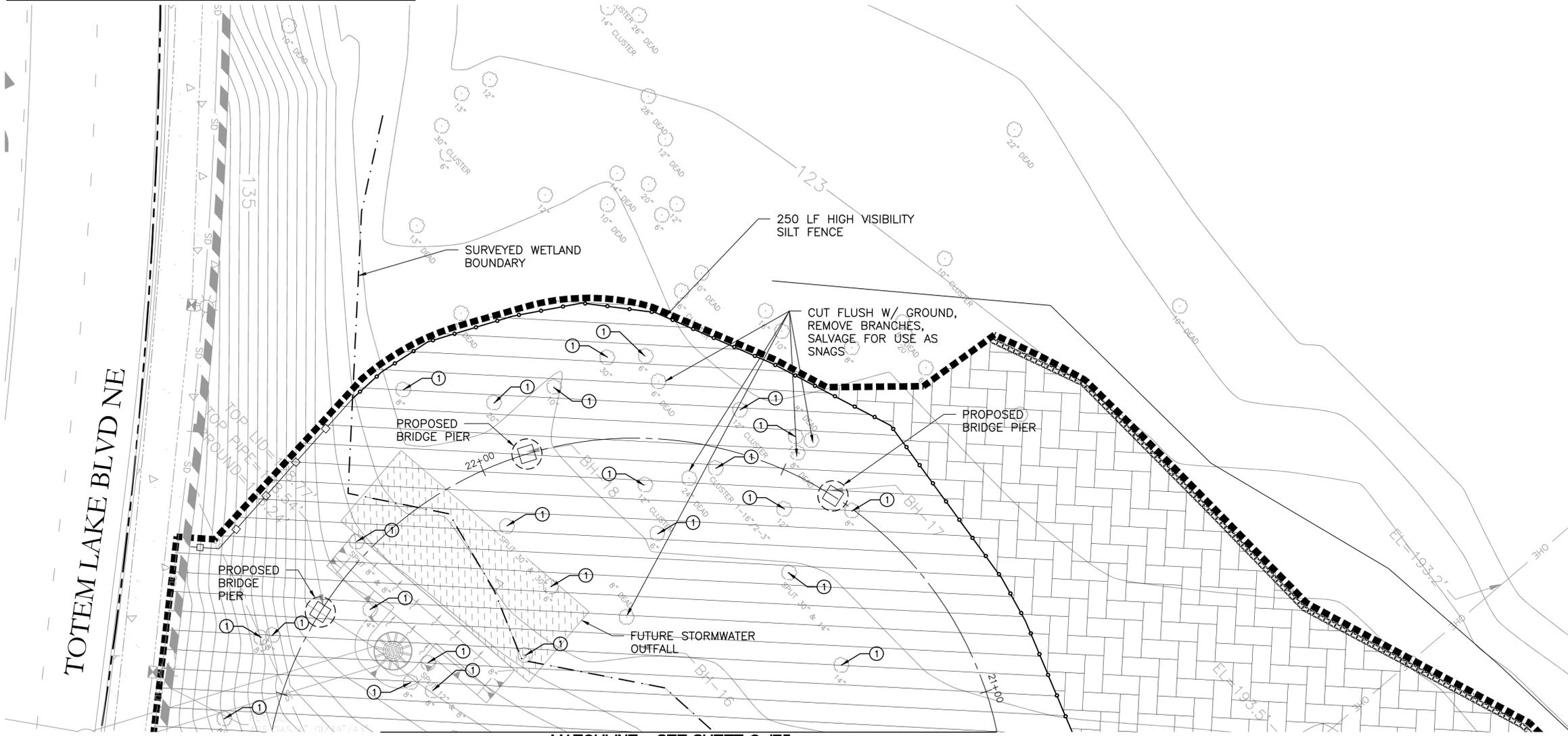
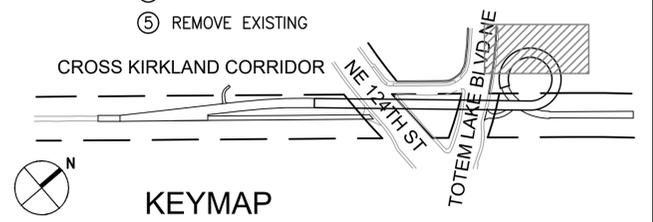
- RIGHT OF WAY LINE
- APPROXIMATE LIMITS OF WORK
- ▨ CLEAR & GRUB (SEE GENERAL NOTE 4)
- ▧ CLEAR & GRUB OUTSIDE LIMITS OF HIGH VISIBILITY SILT FENCE, SEE SPECIAL PROVISIONS
- ▩ REMOVE PAVEMENT
- REMOVE EXISTING GRAVEL SURFACING
- ⋯ WATTLE (CK-E.10)
- HIGH VISIBILITY FENCE (WSDOT STD PLAN I-10.10-01)
- SILT FENCE (CK-E.03), UNLESS NOTED OTHERWISE
- ▬ TEMPORARY STORM DRAIN BYPASS
- FORCE MAIN / PUMPED STORMWATER

GENERAL NOTES

1. ALL UTILITIES DESCRIBED AS "FUTURE" ARE PART OF SEPARATE PROJECTS. THESE PLANS ASSUME ALL COMFORT INN BYPASS INFRASTRUCTURE IS INSTALLED PRIOR TO START OF CONSTRUCTION.
2. SEE SHEET C-170 FOR CITY OF KIRKLAND STANDARD EROSION/SEDIMENTATION CONTROL NOTES.
3. PROTECT EXISTING INFRASTRUCTURE, SITE FEATURES AND STRUCTURES NOT IDENTIFIED FOR REMOVAL.
4. INSTALL TEMPORARY SLOPE STABILIZATION (CK-E.06) WHERE SLOPE IS GREATER THAN 3H:1V.
5. NOT USED.
6. CONTRACTOR TO LOCATE AND PROTECT ALL EXISTING UTILITIES TO REMAIN.
7. SEE SPECIFICATIONS FOR HANDLING, CHARACTERIZATION, AND DISPOSAL OF POTENTIALLY IMPACTED SOILS.
8. CONFIRM STOCKPILING, LAYDOWN AREAS, AND ALL WORK WITHIN VICINITY OF KING COUNTY SANITARY SEWERS WITH ENGINEER PRIOR TO CONSTRUCTION.
9. REDIRECT DITCH FLOW AND SIZE TEMPORARY STORM DRAIN BYPASS TO CONVEY 100YR FLOW 13.9 CFS TO DOWNSTREAM STORM DRAIN SYSTEM.
10. SEE SPECIAL PROVISIONS AND HAZARDOUS MATERIALS REPORT IN PROJECT MANUAL APPENDIX FOR REQUIREMENTS FOR REMOVAL, HANDLING, AND DISPOSAL OF CONTAMINATED SOIL.

CONSTRUCTION NOTES

- ① REMOVE TREE
- ② INLET PROTECTION (KC E.11)
- ③ TEMPORARY STORM DRAIN BYPASS, SEE GENERAL NOTE 9
- ④ REMOVE & SALVAGE EXISTING
- ⑤ REMOVE EXISTING



File: G:\15c\15094c_TLPB\Current\TLPB_170_TESC-Demo.dwg Save Date: Nov 22, 2019 3:29 PM Saved By: JENNIFERA



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KG	CHECKED BY:	2019/12/06	DATE

CITY OF KIRKLAND
TOTEM LAKE PEDESTRIAN BRIDGE
 DEMOLITION AND EROSION CONTROL PLAN 6

SHEET:	TLC - C - 176
SCALE:	AS SHOWN
FILENAME:	TLPB_170_TESC-Demo.dwg

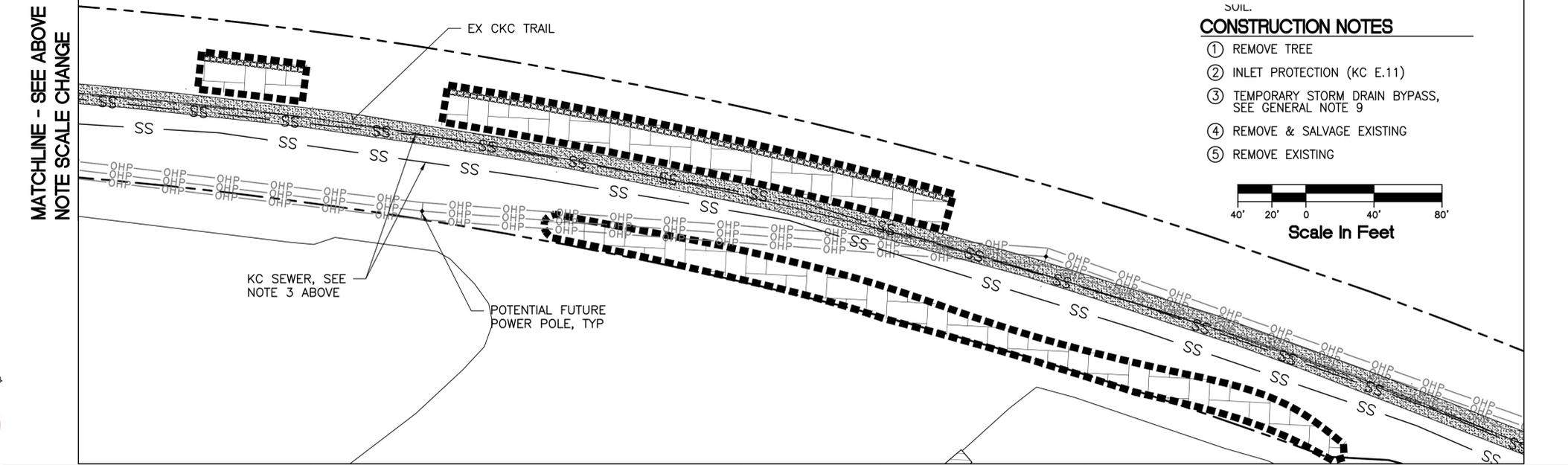
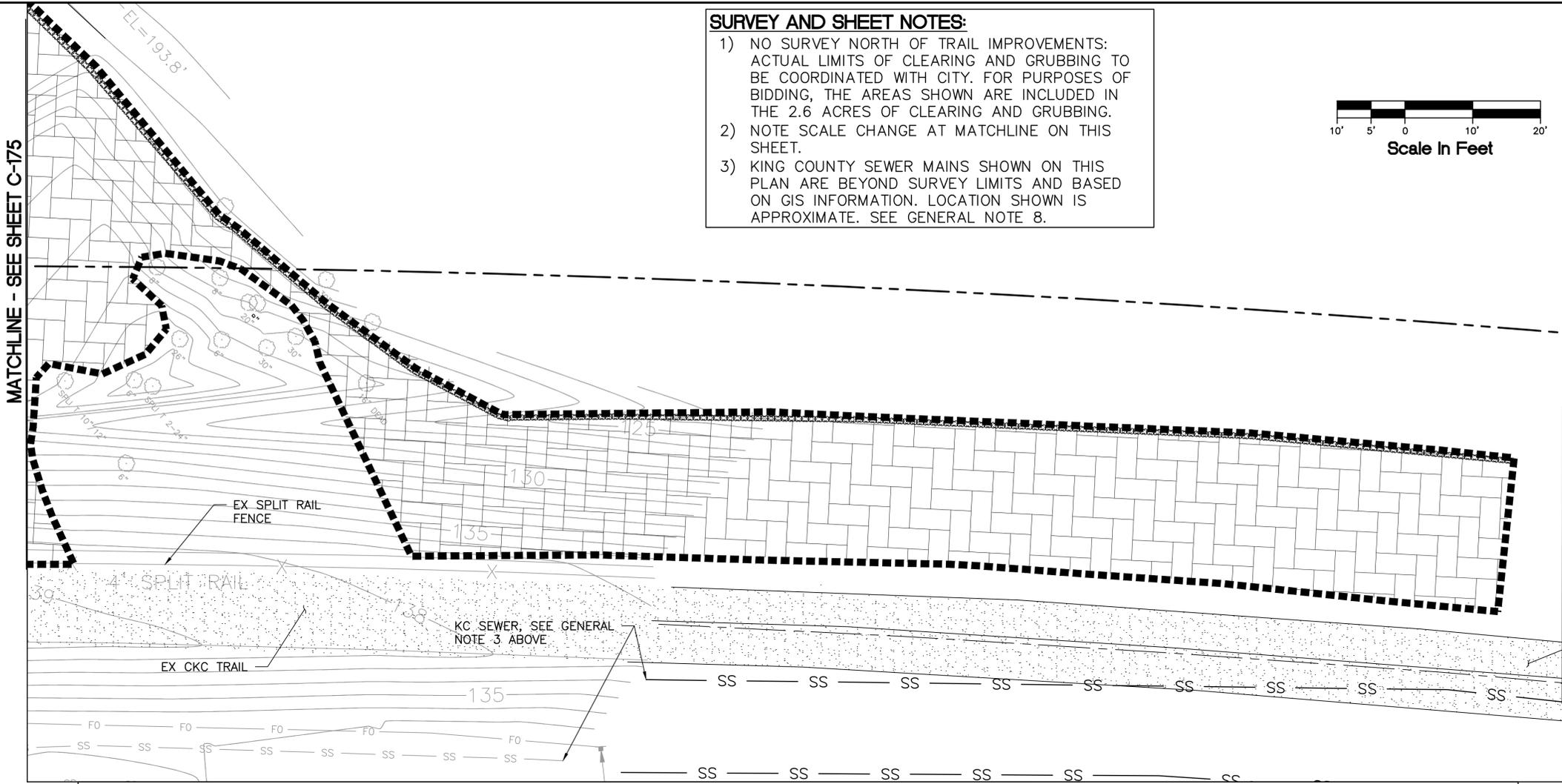
SURVEY AND SHEET NOTES:

- 1) NO SURVEY NORTH OF TRAIL IMPROVEMENTS: ACTUAL LIMITS OF CLEARING AND GRUBBING TO BE COORDINATED WITH CITY. FOR PURPOSES OF BIDDING, THE AREAS SHOWN ARE INCLUDED IN THE 2.6 ACRES OF CLEARING AND GRUBBING.
- 2) NOTE SCALE CHANGE AT MATCHLINE ON THIS SHEET.
- 3) KING COUNTY SEWER MAINS SHOWN ON THIS PLAN ARE BEYOND SURVEY LIMITS AND BASED ON GIS INFORMATION. LOCATION SHOWN IS APPROXIMATE. SEE GENERAL NOTE 8.

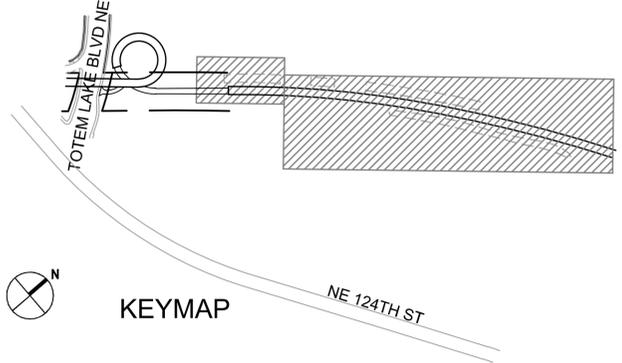


- LEGEND**
- RIGHT OF WAY LINE
 - APPROXIMATE LIMITS OF WORK
 - ▨ CLEAR & GRUB (SEE GENERAL NOTE 4)
 - ▧ CLEAR & GRUB OUTSIDE LIMITS OF HIGH VISIBILITY SILT FENCE, SEE SPECIAL PROVISIONS
 - ▩ REMOVE PAVEMENT
 - REMOVE EXISTING GRAVEL SURFACING
 - ⋯ WATTLE (CK-E.10)
 - HIGH VISIBILITY FENCE (WSDOT STD PLAN I-10.10-01)
 - SILT FENCE (CK-E.03), UNLESS NOTED OTHERWISE
 - ▬ TEMPORARY STORM DRAIN BYPASS
 - FORCE MAIN / PUMPED STORMWATER

- GENERAL NOTES**
1. ALL UTILITIES DESCRIBED AS "FUTURE" ARE PART OF SEPARATE PROJECTS. THESE PLANS ASSUME ALL COMFORT INN BYPASS INFRASTRUCTURE IS INSTALLED PRIOR TO START OF CONSTRUCTION.
 2. SEE SHEET C-170 FOR CITY OF KIRKLAND STANDARD EROSION/SEDIMENTATION CONTROL NOTES.
 3. PROTECT EXISTING INFRASTRUCTURE, SITE FEATURES AND STRUCTURES NOT IDENTIFIED FOR REMOVAL.
 4. INSTALL TEMPORARY SLOPE STABILIZATION (CK-E.06) WHERE SLOPE IS GREATER THAN 3H:1V.
 5. NOT USED.
 6. CONTRACTOR TO LOCATE AND PROTECT ALL EXISTING UTILITIES TO REMAIN.
 7. SEE SPECIFICATIONS FOR HANDLING, CHARACTERIZATION, AND DISPOSAL OF POTENTIALLY IMPACTED SOILS.
 8. CONFIRM STOCKPILING, LAYDOWN AREAS, AND ALL WORK WITHIN VICINITY OF KING COUNTY SANITARY SEWERS WITH ENGINEER PRIOR TO CONSTRUCTION.
 9. REDIRECT DITCH FLOW AND SIZE TEMPORARY STORM DRAIN BYPASS TO CONVEY 100YR FLOW 13.9 CFS TO DOWNSTREAM STORM DRAIN SYSTEM.
 10. SEE SPECIAL PROVISIONS AND HAZARDOUS MATERIALS REPORT IN PROJECT MANUAL APPENDIX FOR REQUIREMENTS FOR REMOVAL, HANDLING, AND DISPOSAL OF CONTAMINATED SOIL.



- CONSTRUCTION NOTES**
- 1 REMOVE TREE
 - 2 INLET PROTECTION (KC E.11)
 - 3 TEMPORARY STORM DRAIN BYPASS, SEE GENERAL NOTE 9
 - 4 REMOVE & SALVAGE EXISTING
 - 5 REMOVE EXISTING

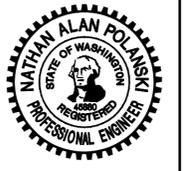


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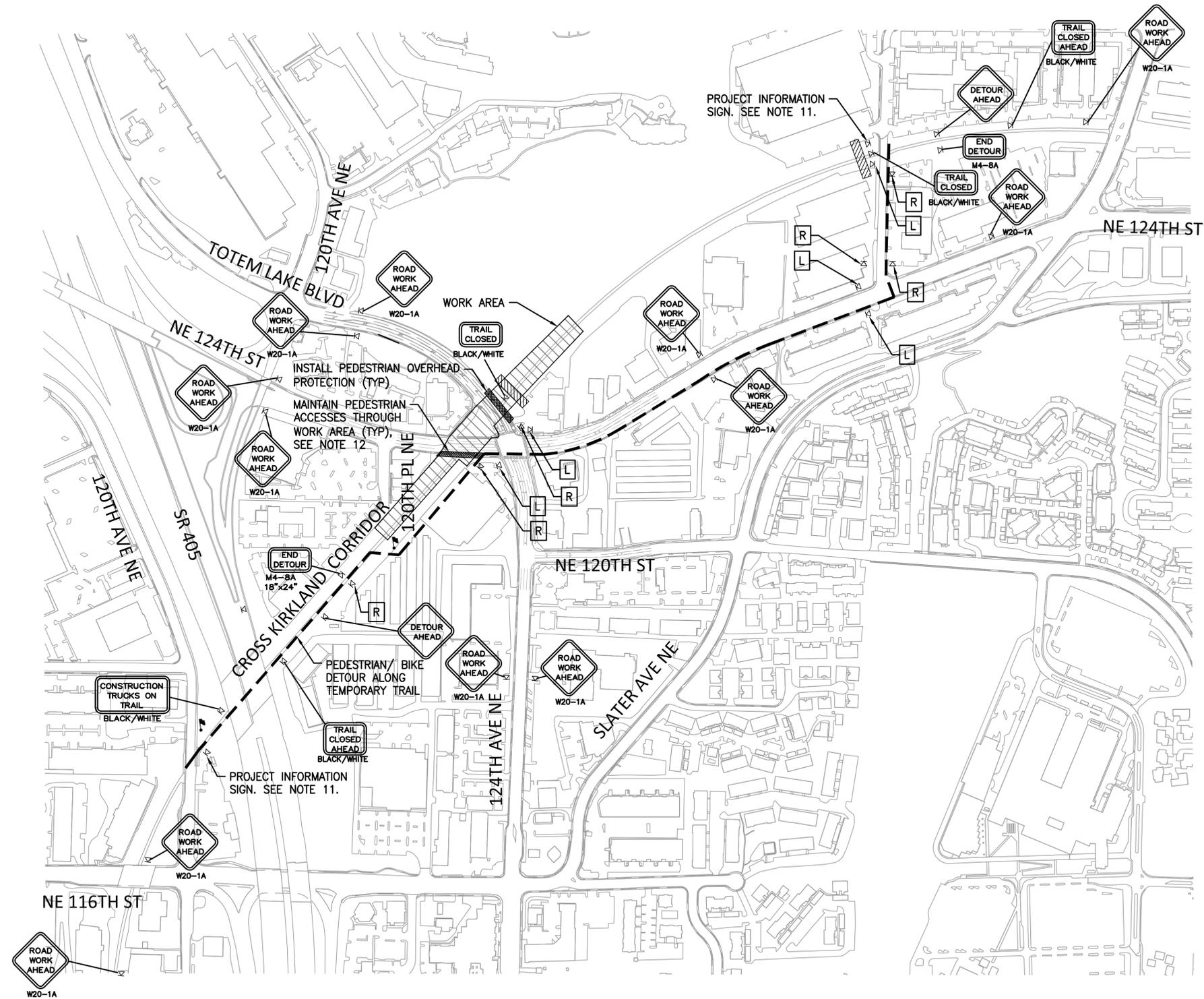
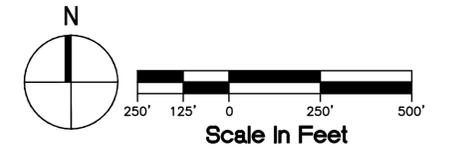


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CITY OF KIRKLAND
TOTEM LAKE PEDESTRIAN BRIDGE
 DEMOLITION AND EROSION CONTROL PLAN 7

SHEET: **TLC - C - 177**
 SCALE: **AS SHOWN**
 FILENAME: TLPB_170_TESC-Demo.dwg



- LEGEND**
- FLAGGING STATION
 - CHANNELIZING DEVICE
 - TEMPORARY SIGN LOCATION
 - WORK AREA
 - TYPE 3 BARRICADE
 - PEDESTRIAN AND BIKE DETOUR ROUTE
 - M4-9AR, 30"x24", BLACK/ORANGE
 - M4-9AL, 30"x24", BLACK/ORANGE

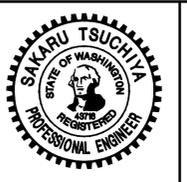
- NOTES**
1. SEE PROJECT SPECIFICATIONS FOR ADDITIONAL TRAFFIC CONTROL REQUIREMENTS INCLUDING ACCESS FOR USERS AND NIGHT TIME WORK REQUIREMENTS.
 2. ALL SIGNS ARE BLACK ON ORANGE UNLESS OTHERWISE NOTED.
 3. CONTRACTOR SHALL PROVIDE FLAGGERS AS NEEDED TO FACILITATE INGRESS AND EGRESS AT DRIVEWAYS.
 4. CONTRACTOR SHALL PROVIDE FLAGGERS WHERE NEEDED FOR PEDESTRIANS.
 5. FLAGGING STATIONS SHALL BE STAFFED ONLY WHEN FLAGGING IS REQUIRED.
 6. PHASE CONSTRUCTION TO MAINTAIN A MINIMUM OF ONE VEHICULAR AND ONE PEDESTRIAN ACCESS POINT FOR EACH PROPERTY WITHIN WORK AREA LIMITS AT ALL TIMES.
 7. DISTANCES MAY VARY AS APPROVED BY THE ENGINEER.
 8. SIGN SPACING, CHANNELIZATION DEVICE SPACING, AND TAPER LENGTH TABLES PER WSDOT WORK ZONE TRAFFIC CONTROL PLANS TCP-1 - TCP-8.
 9. ALL BIKE DETOUR ROUTES SHALL HAVE PAVED SURFACING.
 10. FOR NIGHT TIME DETOURS, MAINTAIN PAVEMENTS NECESSARY FOR DETOUR TURNING MOVEMENTS.
 11. COORDINATE WITH CITY FOR PREPARATION OF PROJECT SIGN FABRICATED BY CITY INSTALLED BY CONTRACTOR.
 12. PROVIDE PEDESTRIAN ACCESS THROUGH TRAFFIC ISLAND BETWEEN NE 124TH ST AND TOTEM LAKE BLVD IF PEDESTRIAN ROUTES ON SOUTH SIDE OF NE 124TH ST OR NORTH SIDE OF TOTEM LAKE BLVD ARE CLOSED. CONTRACTOR SHALL COORDINATE TEMPORARY PEDESTRIAN CLOSURES WITH CITY FOR SAFE PEDESTRIANS CROSSING OF 124TH AVE NE AND TOTEM LAKE BLVD.

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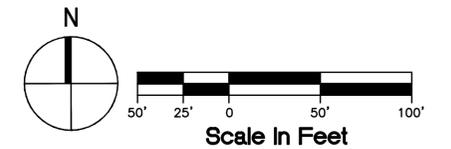


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CITY OF KIRKLAND
 TOTEM LAKE PEDESTRIAN BRIDGE
 TEMPORARY TRAFFIC CONTROL PLAN
 DETOUR FOR PEDESTRIANS AND BIKES

SHEET: **TLC - C - 181**
 SCALE: **AS SHOWN**
 FILENAME: TLPB_180_TRAFFIC_CNTRL.dwg



LEGEND

- FLAGGING STATION
- CHANNELIZING DEVICE
- TEMPORARY SIGN LOCATION
- WORK AREA
- TYPE 3 BARRICADE
- DRIVEWAY ACCESS

NOTES

1. SEE PROJECT SPECIFICATIONS FOR ADDITIONAL TRAFFIC CONTROL REQUIREMENTS INCLUDING ACCESS FOR USERS AND NIGHT TIME WORK REQUIREMENTS.
2. ALL SIGNS ARE BLACK ON ORANGE UNLESS OTHERWISE NOTED.
3. CONTRACTOR SHALL PROVIDE FLAGGERS AS NEEDED TO FACILITATE INGRESS AND EGRESS AT DRIVEWAYS.
4. CONTRACTOR SHALL PROVIDE FLAGGERS WHERE NEEDED FOR PEDESTRIANS.
5. FLAGGING STATIONS SHALL BE STAFFED ONLY WHEN FLAGGING IS REQUIRED.
6. PHASE CONSTRUCTION TO MAINTAIN A MINIMUM OF ONE VEHICULAR AND ONE PEDESTRIAN ACCESS POINT FOR EACH PROPERTY WITHIN WORK AREA LIMITS AT ALL TIMES.
7. DISTANCES MAY VARY AS APPROVED BY THE ENGINEER.
8. SIGN SPACING, CHANNELIZATION DEVICE SPACING, AND TAPER LENGTH TABLES PER WSDOT WORK ZONE TRAFFIC CONTROL PLANS TCP-1 - TCP-8.
9. ALL BIKE DETOUR ROUTES SHALL HAVE PAVED SURFACING.
10. FOR NIGHT TIME DETOURS, MAINTAIN PAVEMENTS NECESSARY FOR DETOUR TURNING MOVEMENTS.
11. SEE C-181 FOR PEDESTRIAN DETOUR ROUTES.

SIGN SPACING = X (1)

RURAL HIGHWAYS	60 / 65 MPH	800' ±
RURAL ROADS	45 / 55 MPH	500' ±
RURAL ROADS & URBAN ARTERIALS	35 / 40 MPH	350' ±
RURAL ROADS, URBAN ARTERIALS, RESIDENTIAL & BUSINESS DISTRICTS	25 / 30 MPH	200' ± (2)
URBAN STREETS	25 MPH OR LESS	100' ± (2)

(1) ALL SPACING MAY BE ADJUSTED TO ACCOMMODATE INTERSECTIONS AND DRIVEWAYS.
 (2) THIS SPACING MAY BE REDUCED IN URBAN AREAS TO FIT ROADWAY CONDITIONS.

CHANNELIZATION DEVICE SPACING (feet)

MPH	TAPER	TANGENT
50/70	40	80
35/45	30	60
25/30	20	40

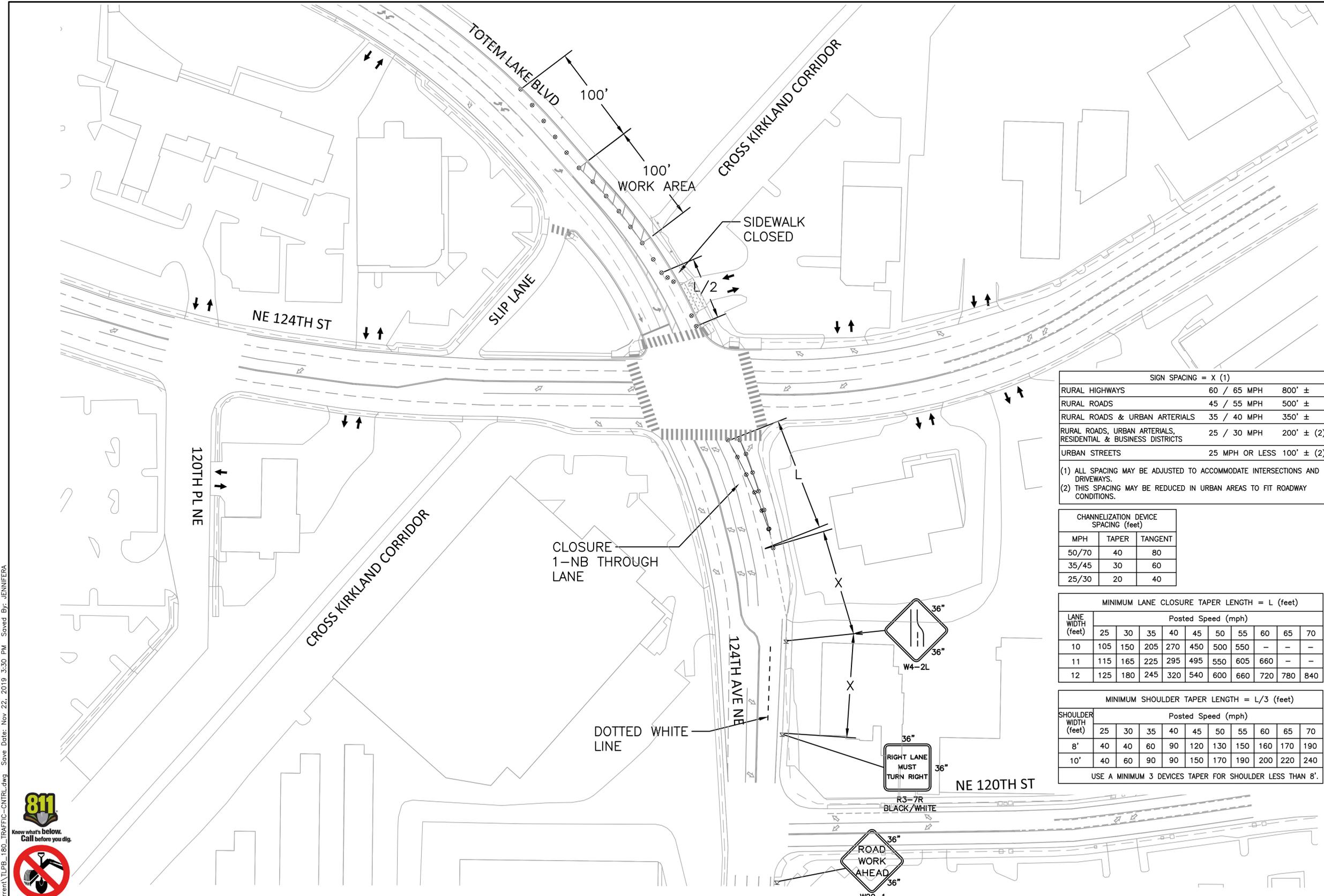
MINIMUM LANE CLOSURE TAPER LENGTH = L (feet)

LANE WIDTH (feet)	Posted Speed (mph)									
	25	30	35	40	45	50	55	60	65	70
10	105	150	205	270	450	500	550	-	-	-
11	115	165	225	295	495	550	605	660	-	-
12	125	180	245	320	540	600	660	720	780	840

MINIMUM SHOULDER TAPER LENGTH = L/3 (feet)

SHOULDER WIDTH (feet)	Posted Speed (mph)									
	25	30	35	40	45	50	55	60	65	70
8'	40	40	60	90	120	130	150	160	170	190
10'	40	60	90	90	150	170	190	200	220	240

USE A MINIMUM 3 DEVICES TAPER FOR SHOULDER LESS THAN 8'.

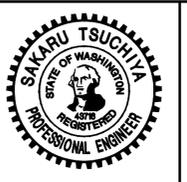


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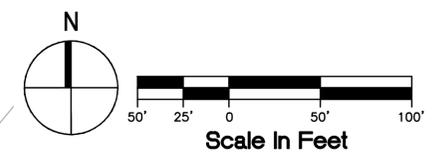


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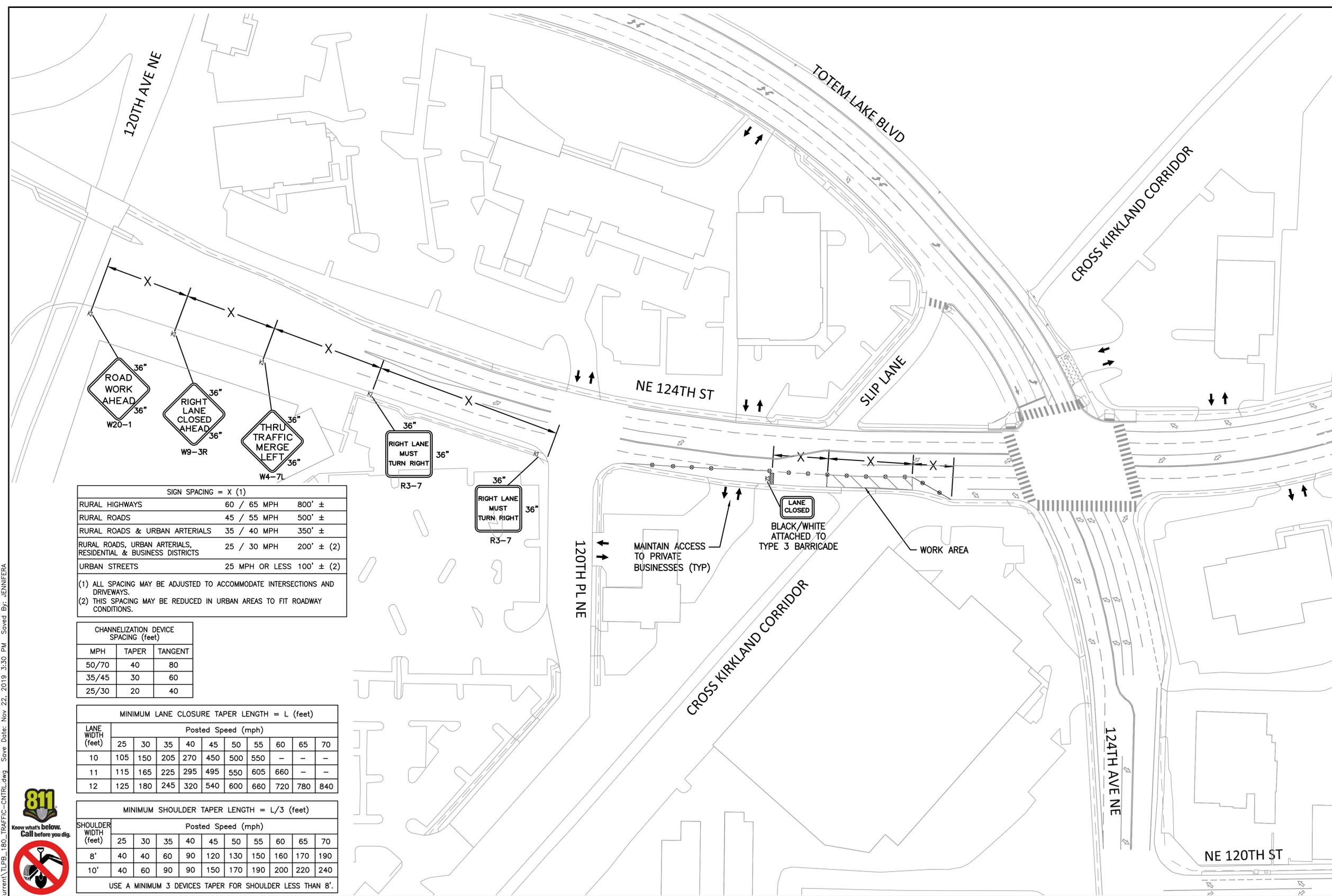
CITY OF KIRKLAND
 TOTEM LAKE PEDESTRIAN BRIDGE
 TEMPORARY TRAFFIC CONTROL PLAN
 LANE CLOSURE ON TOTEM LAKE BLVD
 (NORTH SIDE)

SHEET: **TLC - C - 182**
 SCALE: **AS SHOWN**
 FILENAME: TLPB_180_TRAFFIC-CNTRL.dwg



- LEGEND**
- FLAGGING STATION
 - CHANNELIZING DEVICE
 - TEMPORARY SIGN LOCATION
 - WORK AREA
 - TYPE 3 BARRICADE
 - DRIVEWAY ACCESS

- NOTES**
1. SEE PROJECT SPECIFICATIONS FOR ADDITIONAL TRAFFIC CONTROL REQUIREMENTS INCLUDING ACCESS FOR USERS AND NIGHT TIME WORK REQUIREMENTS.
 2. ALL SIGNS ARE BLACK ON ORANGE UNLESS OTHERWISE NOTED.
 3. CONTRACTOR SHALL PROVIDE FLAGGERS AS NEEDED TO FACILITATE INGRESS AND EGRESS AT DRIVEWAYS.
 4. CONTRACTOR SHALL PROVIDE FLAGGERS WHERE NEEDED FOR PEDESTRIANS.
 5. FLAGGING STATIONS SHALL BE STAFFED ONLY WHEN FLAGGING IS REQUIRED.
 6. PHASE CONSTRUCTION TO MAINTAIN A MINIMUM OF ONE VEHICULAR AND ONE PEDESTRIAN ACCESS POINT FOR EACH PROPERTY WITHIN WORK AREA LIMITS AT ALL TIMES.
 7. DISTANCES MAY VARY AS APPROVED BY THE ENGINEER.
 8. SIGN SPACING, CHANNELIZATION DEVICE SPACING, AND TAPER LENGTH TABLES PER WSDOT WORK ZONE TRAFFIC CONTROL PLANS TCP-1 - TCP-8.
 9. ALL BIKE DETOUR ROUTES SHALL HAVE PAVED SURFACING.
 10. FOR NIGHT TIME DETOURS, MAINTAIN PAVEMENTS NECESSARY FOR DETOUR TURNING MOVEMENTS.
 11. SEE C-181 FOR PEDESTRIAN DETOUR ROUTES.



SIGN SPACING = X (1)

RURAL HIGHWAYS	60 / 65 MPH	800' ±
RURAL ROADS	45 / 55 MPH	500' ±
RURAL ROADS & URBAN ARTERIALS	35 / 40 MPH	350' ±
RURAL ROADS, URBAN ARTERIALS, RESIDENTIAL & BUSINESS DISTRICTS	25 / 30 MPH	200' ± (2)
URBAN STREETS	25 MPH OR LESS	100' ± (2)

(1) ALL SPACING MAY BE ADJUSTED TO ACCOMMODATE INTERSECTIONS AND DRIVEWAYS.
 (2) THIS SPACING MAY BE REDUCED IN URBAN AREAS TO FIT ROADWAY CONDITIONS.

CHANNELIZATION DEVICE SPACING (feet)

MPH	TAPER	TANGENT
50/70	40	80
35/45	30	60
25/30	20	40

MINIMUM LANE CLOSURE TAPER LENGTH = L (feet)

LANE WIDTH (feet)	Posted Speed (mph)									
	25	30	35	40	45	50	55	60	65	70
10	105	150	205	270	450	500	550	-	-	-
11	115	165	225	295	495	550	605	660	-	-
12	125	180	245	320	540	600	660	720	780	840

MINIMUM SHOULDER TAPER LENGTH = L/3 (feet)

SHOULDER WIDTH (feet)	Posted Speed (mph)									
	25	30	35	40	45	50	55	60	65	70
8'	40	40	60	90	120	130	150	160	170	190
10'	40	60	90	90	150	170	190	200	220	240

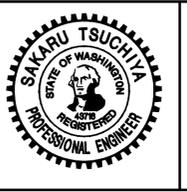
USE A MINIMUM 3 DEVICES TAPER FOR SHOULDER LESS THAN 8'.

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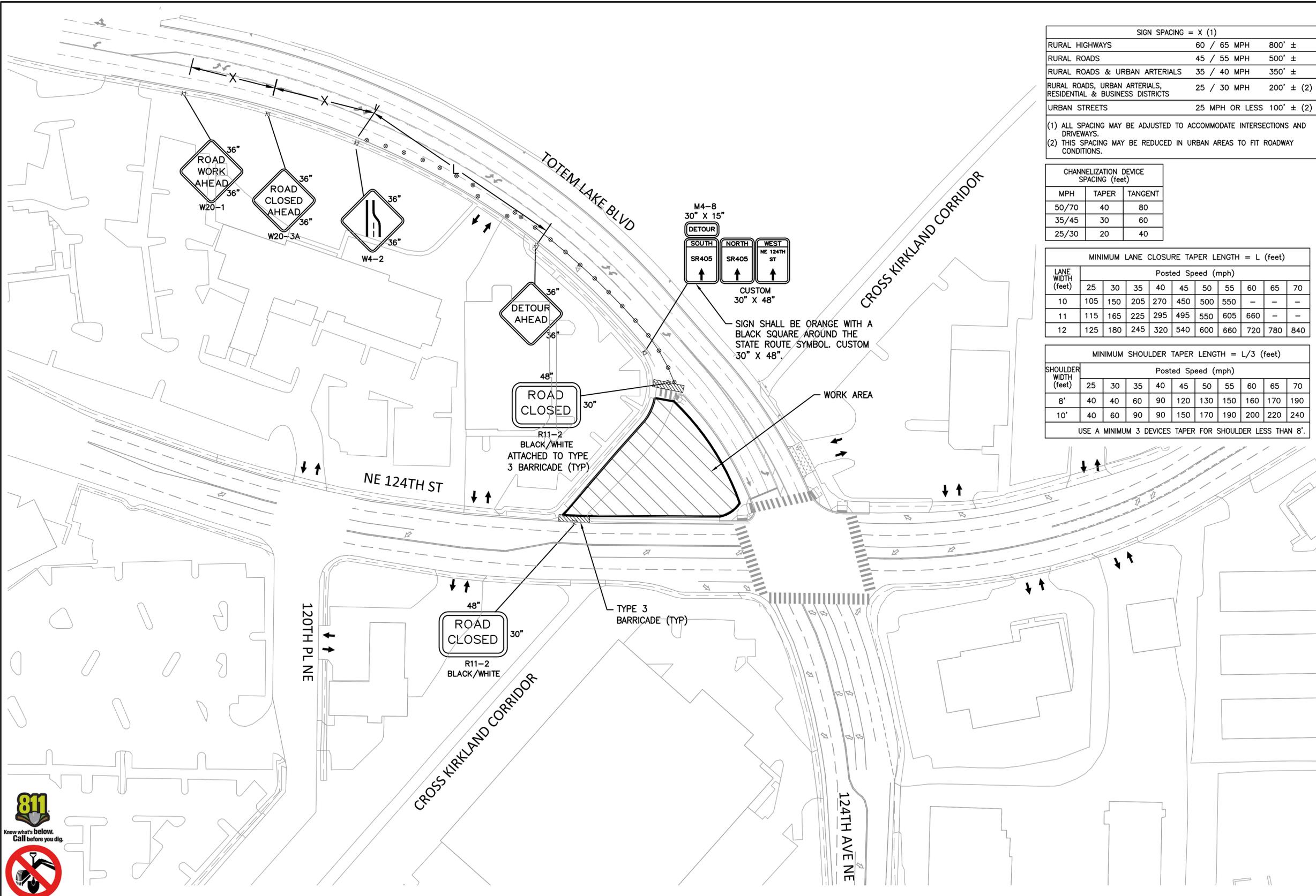
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CITY OF KIRKLAND
 TOTEM LAKE PEDESTRIAN BRIDGE
 TEMPORARY TRAFFIC CONTROL PLAN
 LANE CLOSURE ON NE 124TH STREET
 (SOUTH SIDE)

SHEET: **TLC - C - 183**
 SCALE: **AS SHOWN**
 FILENAME: TLPB_180_TRAFFIC-CNTRL.dwg

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SIGN SPACING = X (1)

RURAL HIGHWAYS	60 / 65 MPH	800' ±
RURAL ROADS	45 / 55 MPH	500' ±
RURAL ROADS & URBAN ARTERIALS	35 / 40 MPH	350' ±
RURAL ROADS, URBAN ARTERIALS, RESIDENTIAL & BUSINESS DISTRICTS	25 / 30 MPH	200' ± (2)
URBAN STREETS	25 MPH OR LESS	100' ± (2)

(1) ALL SPACING MAY BE ADJUSTED TO ACCOMMODATE INTERSECTIONS AND DRIVEWAYS.
 (2) THIS SPACING MAY BE REDUCED IN URBAN AREAS TO FIT ROADWAY CONDITIONS.

CHANNELIZATION DEVICE SPACING (feet)

MPH	TAPER	TANGENT
50/70	40	80
35/45	30	60
25/30	20	40

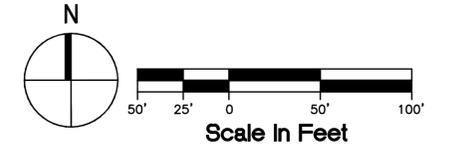
MINIMUM LANE CLOSURE TAPER LENGTH = L (feet)

LANE WIDTH (feet)	Posted Speed (mph)									
	25	30	35	40	45	50	55	60	65	70
10	105	150	205	270	450	500	550	-	-	-
11	115	165	225	295	495	550	605	660	-	-
12	125	180	245	320	540	600	660	720	780	840

MINIMUM SHOULDER TAPER LENGTH = L/3 (feet)

SHOULDER WIDTH (feet)	Posted Speed (mph)									
	25	30	35	40	45	50	55	60	65	70
8'	40	40	60	90	120	130	150	160	170	190
10'	40	60	90	90	150	170	190	200	220	240

USE A MINIMUM 3 DEVICES TAPER FOR SHOULDER LESS THAN 8'.



- LEGEND**
- FLAGGING STATION
 - CHANNELIZING DEVICE
 - TEMPORARY SIGN LOCATION
 - WORK AREA
 - TYPE 3 BARRICADE
 - DRIVEWAY ACCESS

- NOTES**
1. SEE PROJECT SPECIFICATIONS FOR ADDITIONAL TRAFFIC CONTROL REQUIREMENTS INCLUDING ACCESS FOR USERS AND NIGHT TIME WORK REQUIREMENTS.
 2. ALL SIGNS ARE BLACK ON ORANGE UNLESS OTHERWISE NOTED.
 3. CONTRACTOR SHALL PROVIDE FLAGGERS AS NEEDED TO FACILITATE INGRESS AND EGRESS AT DRIVEWAYS.
 4. CONTRACTOR SHALL PROVIDE FLAGGERS WHERE NEEDED FOR PEDESTRIANS.
 5. FLAGGING STATIONS SHALL BE STAFFED ONLY WHEN FLAGGING IS REQUIRED.
 6. PHASE CONSTRUCTION TO MAINTAIN A MINIMUM OF ONE VEHICULAR AND ONE PEDESTRIAN ACCESS POINT FOR EACH PROPERTY WITHIN WORK AREA LIMITS AT ALL TIMES.
 7. DISTANCES MAY VARY AS APPROVED BY THE ENGINEER.
 8. SIGN SPACING, CHANNELIZATION DEVICE SPACING, AND TAPER LENGTH TABLES PER WSDOT WORK ZONE TRAFFIC CONTROL PLANS TCP-1 - TCP-8.
 9. ALL BIKE DETOUR ROUTES SHALL HAVE PAVED SURFACING.
 10. FOR NIGHT TIME DETOURS, MAINTAIN PAVEMENTS NECESSARY FOR DETOUR TURNING MOVEMENTS.
 11. SEE C-185 FOR DETOUR.
 12. SEE C-181 FOR PEDESTRIAN DETOUR ROUTES.



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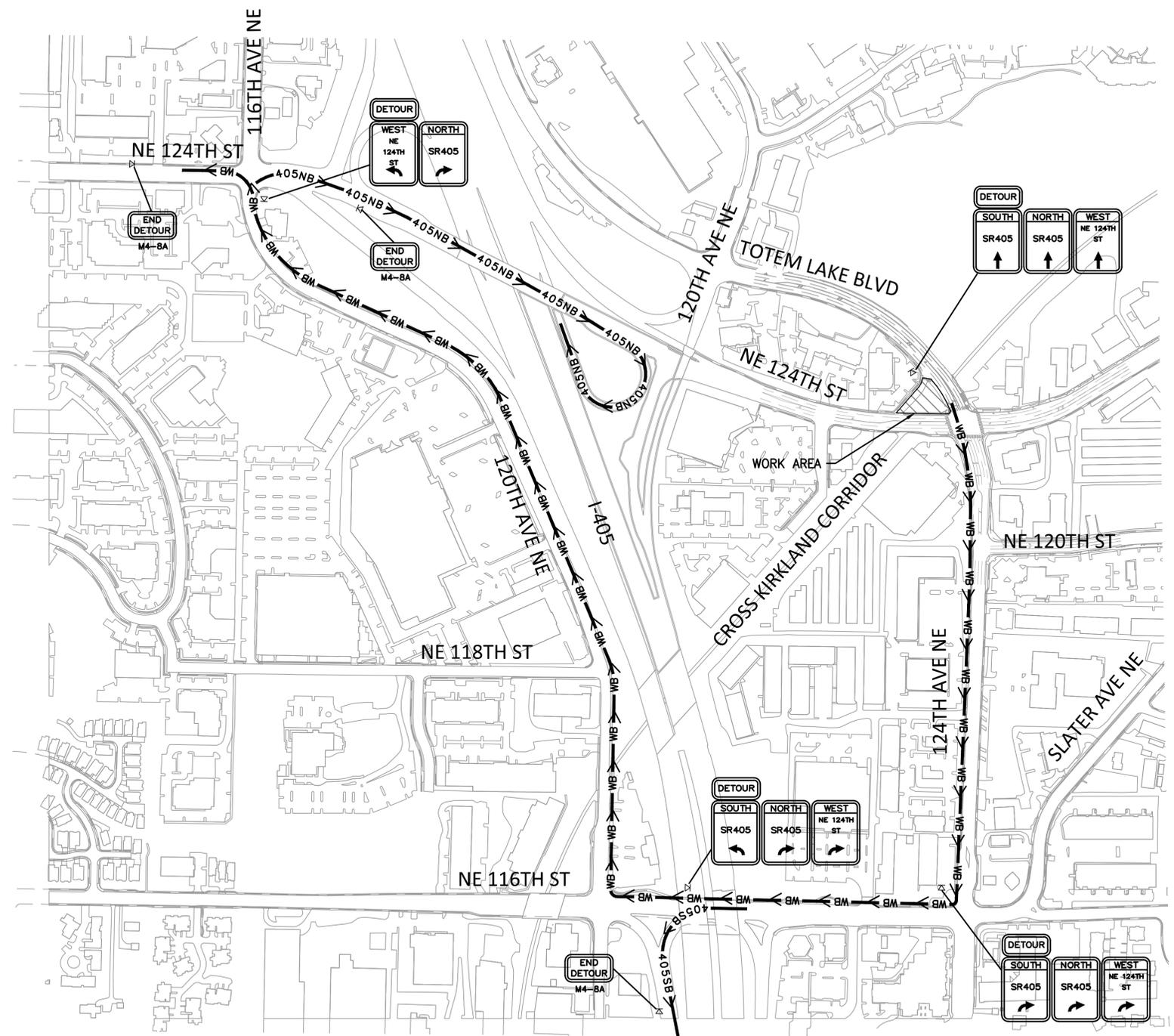
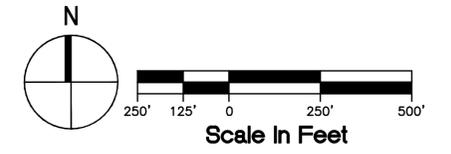


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CITY OF KIRKLAND
 TOTEM LAKE PEDESTRIAN BRIDGE
 TEMPORARY TRAFFIC CONTROL PLAN
 SLIP LANE CLOSURE FROM TOTEM LAKE BLVD TO NE 124TH ST

SHEET: **TLC - C-184**
 SCALE: **AS SHOWN**
 FILENAME: TLPB_180_TRAFFIC-CNTRL.dwg



- LEGEND**
- FLAGGING STATION
 - CHANNELIZING DEVICE
 - TEMPORARY SIGN LOCATION
 - WORK AREA
 - TYPE 3 BARRICADE
 - NB TRAFFIC ON TOTEM LAKE BLVD
 - SB TRAFFIC ON TOTME LAKE BLVD
 - EB TRAFFIC ON NE 124TH ST
 - WB TRAFFIC ON NE 124TH ST
 - DETOUR FOR SR405 NB ON-RAMP
 - DETOUR FOR SR405 SB ON-RAMP
 - W20-1 ROAD WORK AHEAD
 - W20-3A ROAD CLOSED AHEAD
 - W20-2A DETOUR AHEAD
 - M4-8A END DETOUR

- NOTES**
1. SEE PROJECT SPECIFICATIONS FOR ADDITIONAL TRAFFIC CONTROL REQUIREMENTS INCLUDING ACCESS FOR USERS AND NIGHT TIME WORK REQUIREMENTS.
 2. ALL SIGNS ARE BLACK ON ORANGE UNLESS OTHERWISE NOTED.
 3. CONTRACTOR SHALL PROVIDE FLAGGERS AS NEEDED TO FACILITATE INGRESS AND EGRESS AT DRIVEWAYS.
 4. CONTRACTOR SHALL PROVIDE FLAGGERS WHERE NEEDED FOR PEDESTRIANS.
 5. FLAGGING STATIONS SHALL BE STAFFED ONLY WHEN FLAGGING IS REQUIRED.
 6. PHASE CONSTRUCTION TO MAINTAIN A MINIMUM OF ONE VEHICULAR AND ONE PEDESTRIAN ACCESS POINT FOR EACH PROPERTY WITHIN WORK AREA LIMITS AT ALL TIMES.
 7. DISTANCES MAY VARY AS APPROVED BY THE ENGINEER.
 8. SIGN SPACING, CHANNELIZATION DEVICE SPACING, AND TAPER LENGTH TABLES PER WSDOT WORK ZONE TRAFFIC CONTROL PLANS TCP-1 - TCP-8.
 9. ALL BIKE DETOUR ROUTES SHALL HAVE PAVED SURFACING.
 10. FOR NIGHT TIME DETOURS, MAINTAIN PAVEMENTS NECESSARY FOR DETOUR TURNING MOVEMENTS.
 11. SEE C-181 FOR PEDESTRIAN DETOUR ROUTES.

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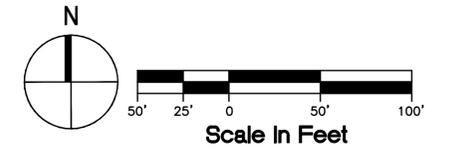


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**CITY OF KIRKLAND
 TOTEM LAKE PEDESTRIAN BRIDGE
 TEMPORARY TRAFFIC CONTROL PLAN
 DETOUR FOR SLIP LANE CLOSURE FROM
 TOTEM LAKE BLVD TO NE 124TH ST**

SHEET: **TLC - C-185**
 SCALE: **AS SHOWN**
 FILENAME: TLPB_180_TRAFFIC-CNTRL.dwg



- LEGEND**
- FLAGGING STATION
 - CHANNELIZING DEVICE
 - TEMPORARY SIGN LOCATION
 - WORK AREA
 - TYPE 3 BARRICADE
 - DRIVEWAY ACCESS

- NOTES**
1. SEE PROJECT SPECIFICATIONS FOR ADDITIONAL TRAFFIC CONTROL REQUIREMENTS INCLUDING ACCESS FOR USERS AND NIGHT TIME WORK REQUIREMENTS.
 2. ALL SIGNS ARE BLACK ON ORANGE UNLESS OTHERWISE NOTED.
 3. CONTRACTOR SHALL PROVIDE FLAGGERS AS NEEDED TO FACILITATE INGRESS AND EGRESS AT DRIVEWAYS.
 4. CONTRACTOR SHALL PROVIDE FLAGGERS WHERE NEEDED FOR PEDESTRIANS.
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 6. PHASE CONSTRUCTION TO MAINTAIN A MINIMUM OF ONE VEHICULAR AND ONE PEDESTRIAN ACCESS POINT FOR EACH PROPERTY WITHIN WORK AREA LIMITS AT ALL TIMES.
 7. DISTANCES MAY VARY AS APPROVED BY THE ENGINEER.
 8. SIGN SPACING, CHANNELIZATION DEVICE SPACING, AND TAPER LENGTH TABLES PER WSDOT WORK ZONE TRAFFIC CONTROL PLANS TCP-1 - TCP-8.
 9. ALL BIKE DETOUR ROUTES SHALL HAVE PAVED SURFACING.
 10. FOR NIGHT TIME DETOURS, MAINTAIN PAVEMENTS NECESSARY FOR DETOUR TURNING MOVEMENTS.
 11. SEE C-187 FOR DETOUR.
 12. SEE C-181 FOR PEDESTRIAN DETOUR ROUTES.

SIGN SPACING = X (1)

RURAL HIGHWAYS	60 / 65 MPH	800' ±
RURAL ROADS	45 / 55 MPH	500' ±
RURAL ROADS & URBAN ARTERIALS	35 / 40 MPH	350' ±
RURAL ROADS, URBAN ARTERIALS, RESIDENTIAL & BUSINESS DISTRICTS	25 / 30 MPH	200' ± (2)
URBAN STREETS	25 MPH OR LESS	100' ± (2)

(1) ALL SPACING MAY BE ADJUSTED TO ACCOMMODATE INTERSECTIONS AND DRIVEWAYS.
 (2) THIS SPACING MAY BE REDUCED IN URBAN AREAS TO FIT ROADWAY CONDITIONS.

CHANNELIZATION DEVICE SPACING (feet)

MPH	TAPER	TANGENT
50/70	40	80
35/45	30	60
25/30	20	40

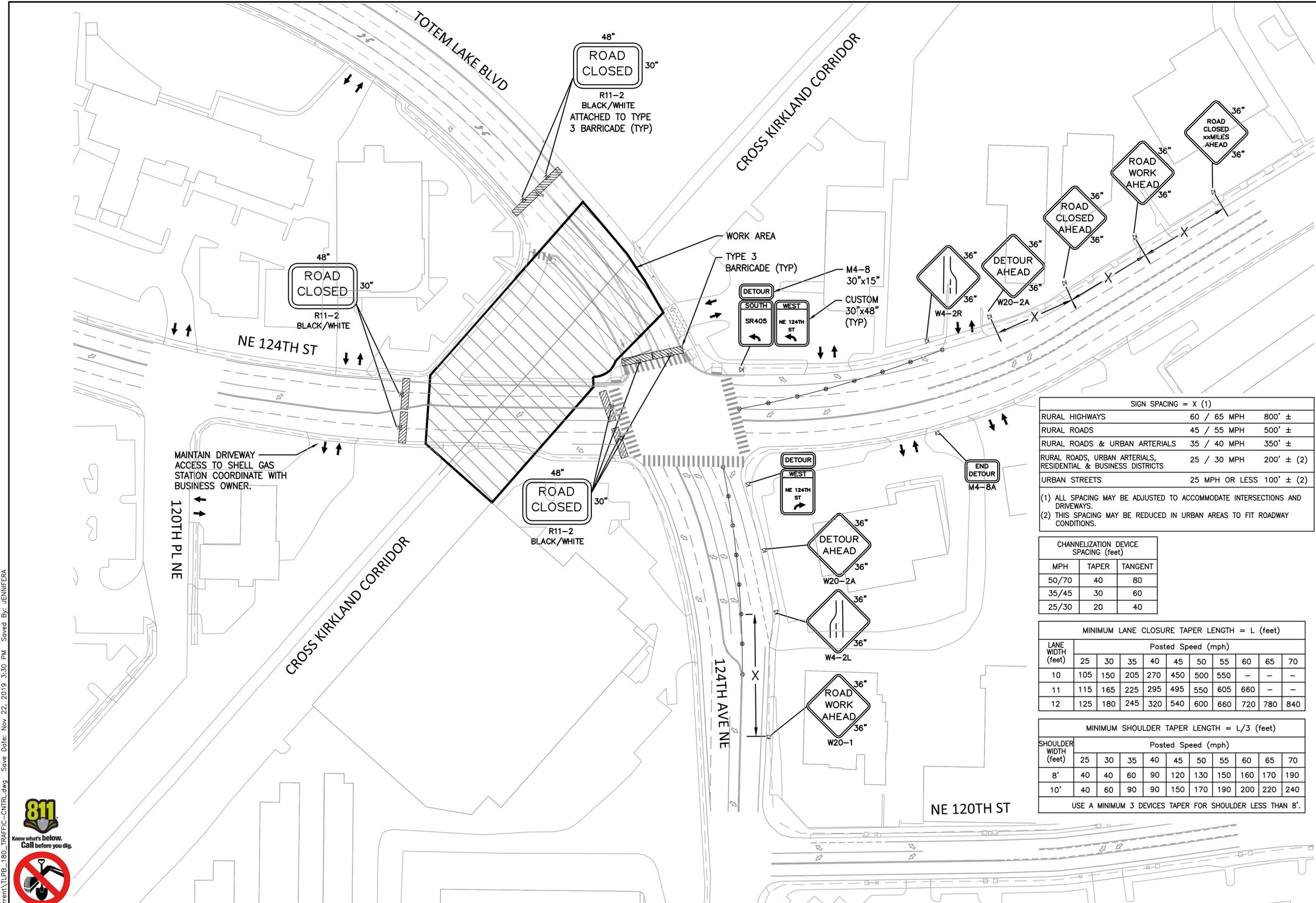
MINIMUM LANE CLOSURE TAPER LENGTH = L (feet)

LANE WIDTH (feet)	Posted Speed (mph)									
	25	30	35	40	45	50	55	60	65	70
10	105	150	205	270	450	500	550	-	-	-
11	115	165	225	295	495	550	605	660	-	-
12	125	180	245	320	540	600	660	720	780	840

MINIMUM SHOULDER TAPER LENGTH = L/3 (feet)

SHOULDER WIDTH (feet)	Posted Speed (mph)									
	25	30	35	40	45	50	55	60	65	70
8'	40	40	60	90	120	130	150	160	170	190
10'	40	60	90	90	150	170	190	200	220	240

USE A MINIMUM 3 DEVICES TAPER FOR SHOULDER LESS THAN 8'.



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CITY OF KIRKLAND
TOTEM LAKE PEDESTRIAN BRIDGE
TEMPORARY TRAFFIC CONTROL PLAN
NIGHT TIME CLOSURE OF TOTEM LAKE BLVD
AND NE 124TH STREET

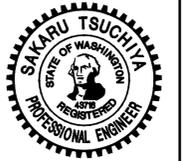
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 SCALE: **AS SHOWN**
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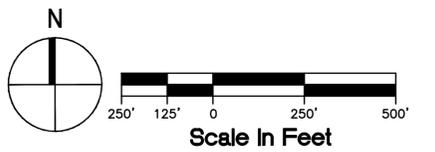
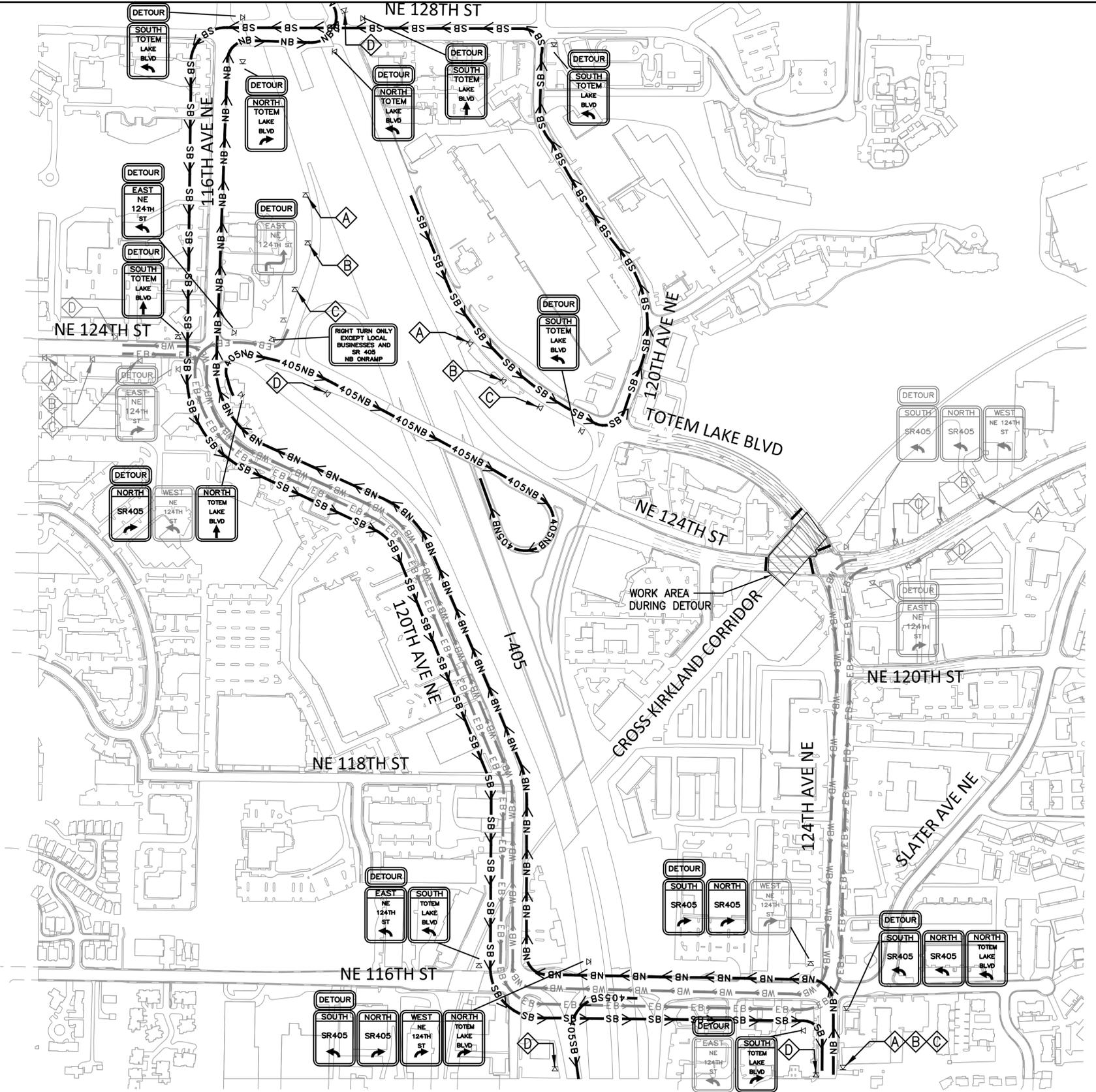


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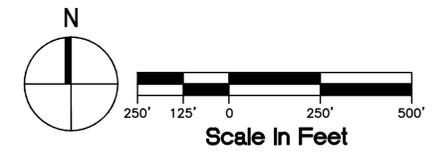
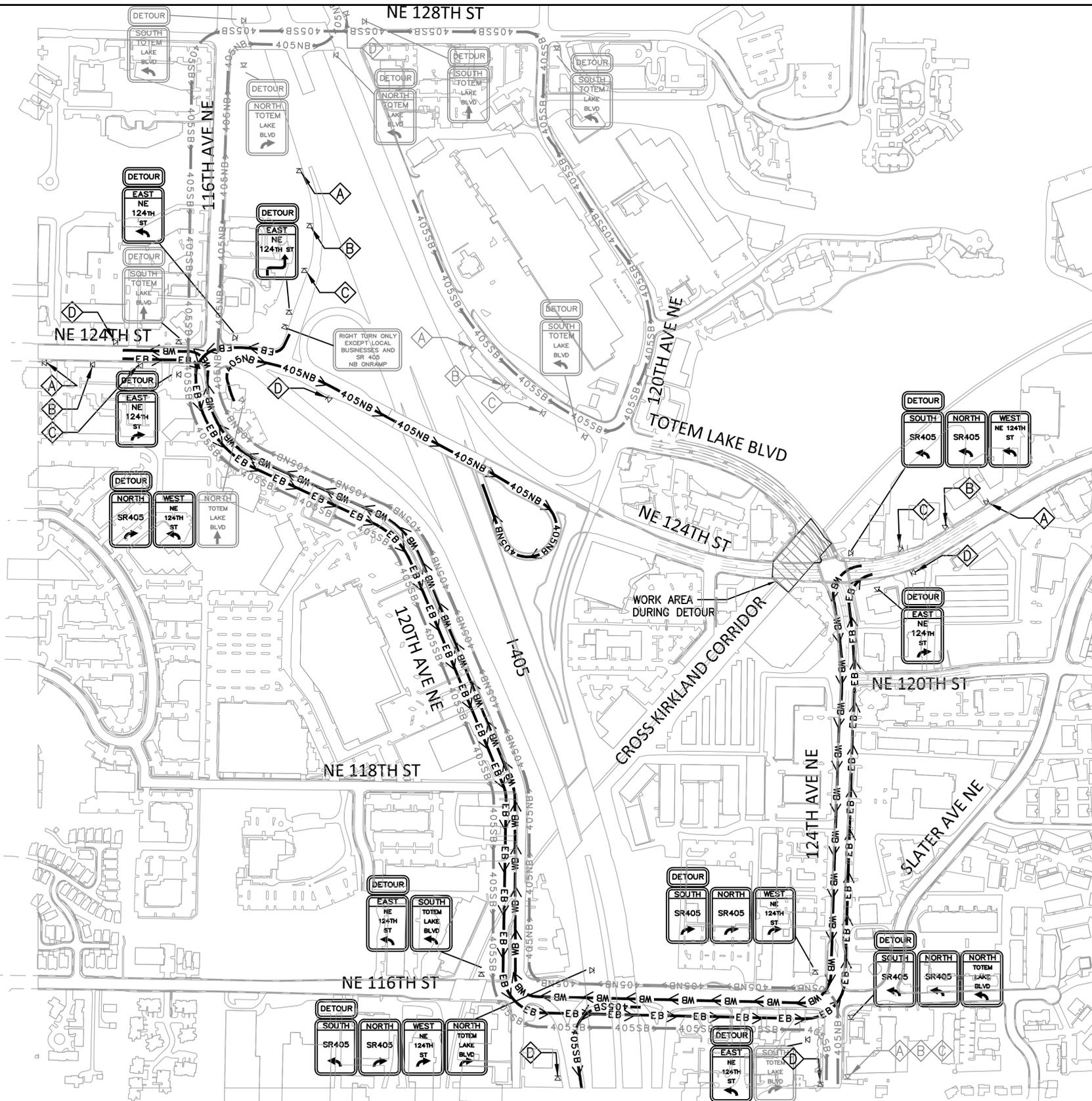
CITY OF KIRKLAND
TOTEM LAKE PEDESTRIAN BRIDGE
TEMPORARY TRAFFIC CONTROL PLAN
DETOUR FOR NIGHT TIME CLOSURE OF TOTEM LAKE BLVD AND NE 124TH STREET (DETOUR FOR TOTEM LAKE BLVD)

SHEET: **TLC - C - 187A**
 SCALE: **AS SHOWN**
 FILENAME: TLPB_180_TRAFFIC-CNTRL.dwg



- LEGEND**
- FLAGGING STATION
 - CHANNELIZING DEVICE
 - TEMPORARY SIGN LOCATION
 - WORK AREA
 - TYPE 3 BARRICADE
 - NB TRAFFIC ON TOTEM LAKE BLVD
 - SB TRAFFIC ON TOTEM LAKE BLVD
 - EB TRAFFIC ON NE 124TH ST
 - WB TRAFFIC ON NE 124TH ST
 - DETOUR FOR SR405 NB ON-RAMP
 - DETOUR FOR SR405 SB ON-RAMP
 - W20-1 ROAD WORK AHEAD
 - W20-3A ROAD CLOSED AHEAD
 - W20-2A DETOUR AHEAD
 - M4-8A END DETOUR

- NOTES**
1. SEE PROJECT SPECIFICATIONS FOR ADDITIONAL TRAFFIC CONTROL REQUIREMENTS INCLUDING ACCESS FOR USERS AND NIGHT TIME WORK REQUIREMENTS.
 2. ALL SIGNS ARE BLACK ON ORANGE UNLESS OTHERWISE NOTED.
 3. CONTRACTOR SHALL PROVIDE FLAGGERS AS NEEDED TO FACILITATE INGRESS AND EGRESS AT DRIVEWAYS.
 4. CONTRACTOR SHALL PROVIDE FLAGGERS WHERE NEEDED FOR PEDESTRIANS.
 5. FLAGGING STATIONS SHALL BE STAFFED ONLY WHEN FLAGGING IS REQUIRED.
 6. PHASE CONSTRUCTION TO MAINTAIN A MINIMUM OF ONE VEHICULAR AND ONE PEDESTRIAN ACCESS POINT FOR EACH PROPERTY WITHIN WORK AREA LIMITS AT ALL TIMES.
 7. DISTANCES MAY VARY AS APPROVED BY THE ENGINEER.
 8. SIGN SPACING, CHANNELIZATION DEVICE SPACING, AND TAPER LENGTH TABLES PER WSDOT WORK ZONE TRAFFIC CONTROL PLANS TCP-1 - TCP-8.
 9. ALL BIKE DETOUR ROUTES SHALL HAVE PAVED SURFACING.
 10. FOR NIGHT TIME DETOURS, MAINTAIN PAVEMENTS NECESSARY FOR DETOUR TURNING MOVEMENTS.
 11. SEE C-187B FOR NE 124TH ST DETOUR.
 12. SEE C-181 FOR PEDESTRIAN DETOUR ROUTES.



- LEGEND**
- FLAGGING STATION
 - CHANNELIZING DEVICE
 - TEMPORARY SIGN LOCATION
 - WORK AREA
 - TYPE 3 BARRICADE
 - NB TRAFFIC ON TOTEM LAKE BLVD
 - SB TRAFFIC ON TOTEM LAKE BLVD
 - EB TRAFFIC ON NE 124TH ST
 - WB TRAFFIC ON NE 124TH ST
 - DETOUR FOR SR405 NB ON-RAMP
 - DETOUR FOR SR405 SB ON-RAMP
 - W20-1 ROAD WORK AHEAD
 - W20-3A ROAD CLOSED AHEAD
 - W20-2A DETOUR AHEAD
 - M4-8A END DETOUR

- NOTES**
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 10. FOR NIGHT TIME DETOURS, MAINTAIN PAVEMENTS NECESSARY FOR DETOUR TURNING MOVEMENTS.
 11. SEE C-187A FOR TOTEM LAKE BLVD DETOUR.
 12. SEE C-181 FOR PEDESTRIAN DETOUR ROUTES.

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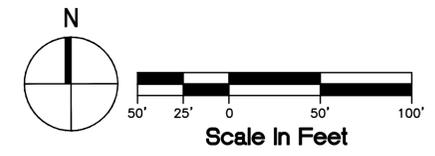


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CITY OF KIRKLAND
 TOTEM LAKE PEDESTRIAN BRIDGE
 TEMPORARY TRAFFIC CONTROL PLAN
 DETOUR FOR NIGHT TIME CLOSURE OF TOTEM
 LAKE BLVD AND NE 124TH STREET
 (DETOUR FOR NE 124TH ST)

SHEET: TLC - C - 187B
 SCALE: AS SHOWN
 FILENAME: TLPB_180_TRAFFIC-CNTRL.dwg



- LEGEND**
- FLAGGING STATION
 - CHANNELIZING DEVICE
 - TEMPORARY SIGN LOCATION
 - WORK AREA
 - TYPE 3 BARRICADE
 - DRIVEWAY ACCESS

- NOTES**
1. SEE PROJECT SPECIFICATIONS FOR ADDITIONAL TRAFFIC CONTROL REQUIREMENTS INCLUDING ACCESS FOR USERS AND NIGHT TIME WORK REQUIREMENTS.
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 9. ALL BIKE DETOUR ROUTES SHALL HAVE PAVED SURFACING.
 10. FOR NIGHT TIME DETOURS, MAINTAIN PAVEMENTS NECESSARY FOR DETOUR TURNING MOVEMENTS.
 11. SEE C-189 FOR DETOUR.
 12. SEE C-181 FOR PEDESTRIAN DETOUR ROUTES.

SIGN SPACING = X (1)

RURAL HIGHWAYS	60 / 65 MPH	800' ±
RURAL ROADS	45 / 55 MPH	500' ±
RURAL ROADS & URBAN ARTERIALS	35 / 40 MPH	350' ±
RURAL ROADS, URBAN ARTERIALS, RESIDENTIAL & BUSINESS DISTRICTS	25 / 30 MPH	200' ± (2)
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 (2) THIS SPACING MAY BE REDUCED IN URBAN AREAS TO FIT ROADWAY CONDITIONS.

CHANNELIZATION DEVICE SPACING (feet)

MPH	TAPER	TANGENT
50/70	40	80
35/45	30	60
25/30	20	40

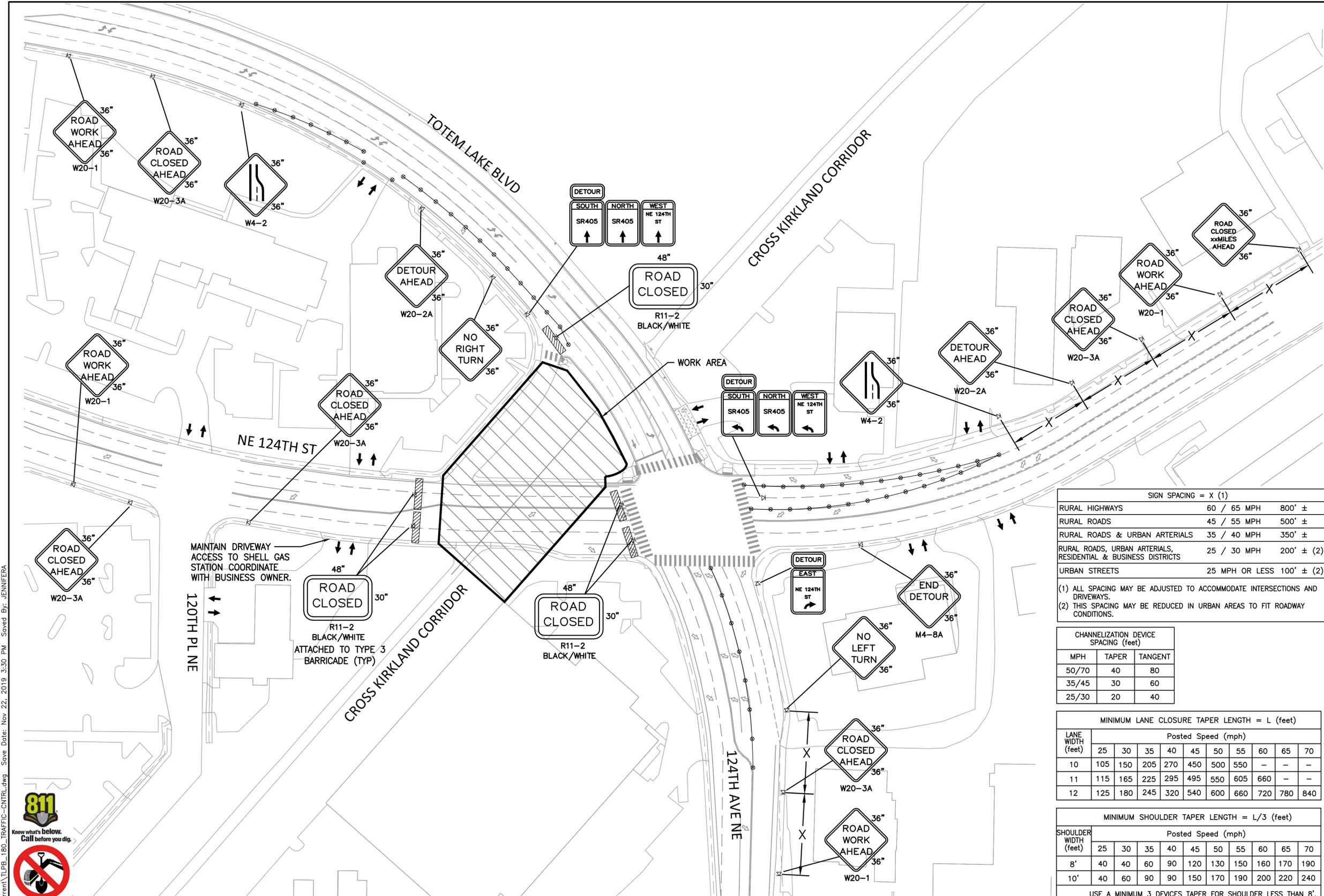
MINIMUM LANE CLOSURE TAPER LENGTH = L (feet)

LANE WIDTH (feet)	Posted Speed (mph)									
	25	30	35	40	45	50	55	60	65	70
10	105	150	205	270	450	500	550	-	-	-
11	115	165	225	295	495	550	605	660	-	-
12	125	180	245	320	540	600	660	720	780	840

MINIMUM SHOULDER TAPER LENGTH = L/3 (feet)

SHOULDER WIDTH (feet)	Posted Speed (mph)									
	25	30	35	40	45	50	55	60	65	70
8'	40	40	60	90	120	130	150	160	170	190
10'	40	60	90	90	150	170	190	200	220	240

USE A MINIMUM 3 DEVICES TAPER FOR SHOULDER LESS THAN 8'.



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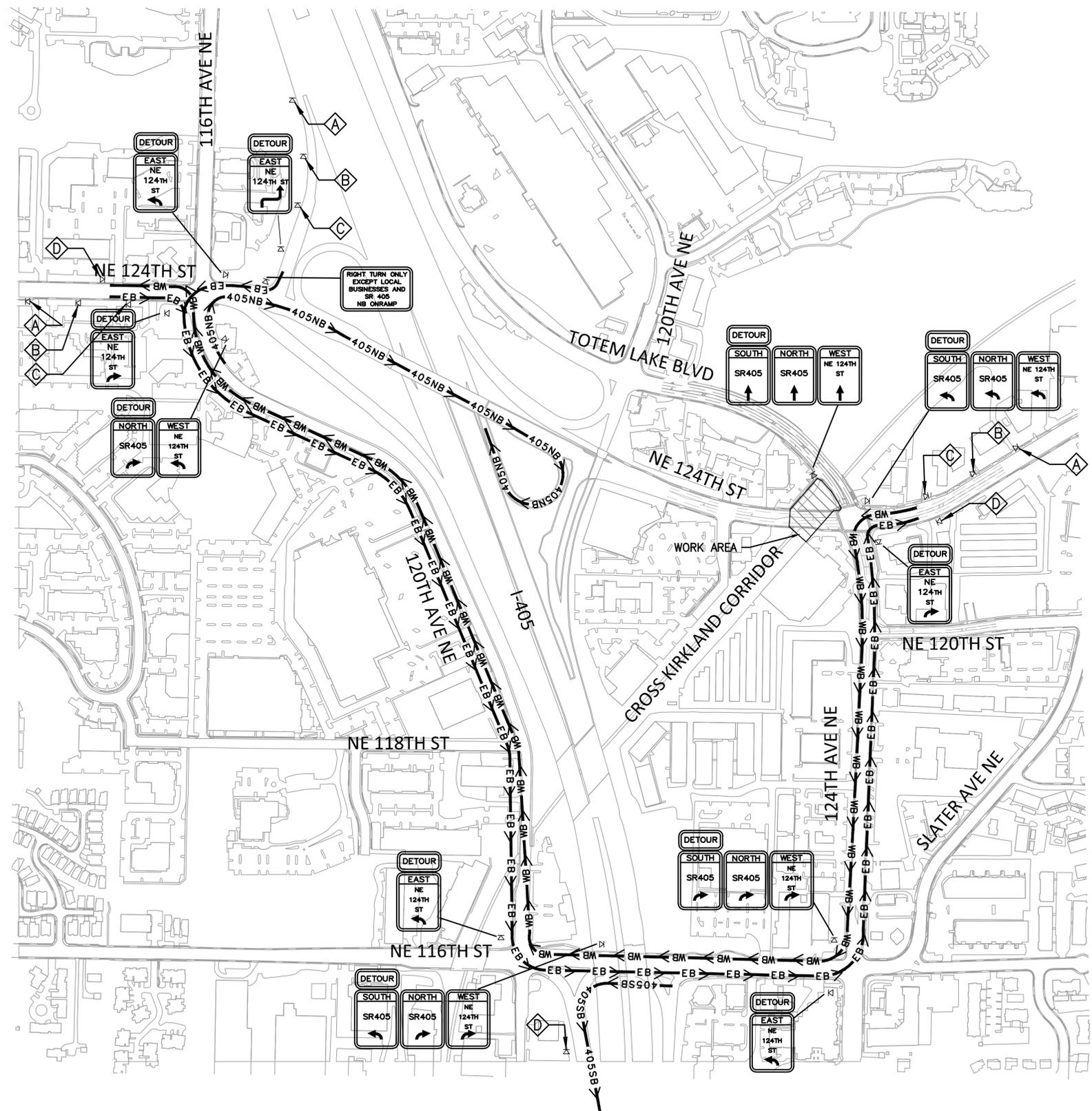
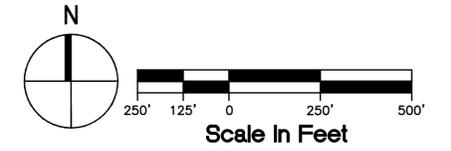


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CITY OF KIRKLAND
 TOTEM LAKE PEDESTRIAN BRIDGE
 TEMPORARY TRAFFIC CONTROL PLAN
 NIGHT TIME CLOSURE OF
 NE 124TH STREET

SHEET: TLC - C - 188
 SCALE: AS SHOWN
 FILENAME: TLPB_180_TRAFFIC-CNTRL.dwg



- LEGEND**
- FLAGGING STATION
 - CHANNELIZING DEVICE
 - TEMPORARY SIGN LOCATION
 - WORK AREA
 - TYPE 3 BARRICADE
 - NB TRAFFIC ON TOTEM LAKE BLVD
 - SB TRAFFIC ON TOTEM LAKE BLVD
 - EB TRAFFIC ON NE 124TH ST
 - WB TRAFFIC ON NE 124TH ST
 - DETOUR FOR SR405 NB ON-RAMP
 - DETOUR FOR SR405 SB ON-RAMP
 - W20-1 ROAD WORK AHEAD
 - W20-3A ROAD CLOSED AHEAD
 - W20-2A DETOUR AHEAD
 - M4-8A END DETOUR

- NOTES**
1. SEE PROJECT SPECIFICATIONS FOR ADDITIONAL TRAFFIC CONTROL REQUIREMENTS INCLUDING ACCESS FOR USERS AND NIGHT TIME WORK REQUIREMENTS.
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 8. SIGN SPACING, CHANNELIZATION DEVICE SPACING, AND TAPER LENGTH TABLES PER WSDOT WORK ZONE TRAFFIC CONTROL PLANS TCP-1 - TCP-8.
 9. ALL BIKE DETOUR ROUTES SHALL HAVE PAVED SURFACING.
 10. FOR NIGHT TIME DETOURS, MAINTAIN PAVEMENTS NECESSARY FOR DETOUR TURNING MOVEMENTS.
 11. SEE C-188 FOR TCP DETAIL NEAR WORK AREA.
 12. SEE C-181 FOR PEDESTRIAN DETOUR ROUTES.

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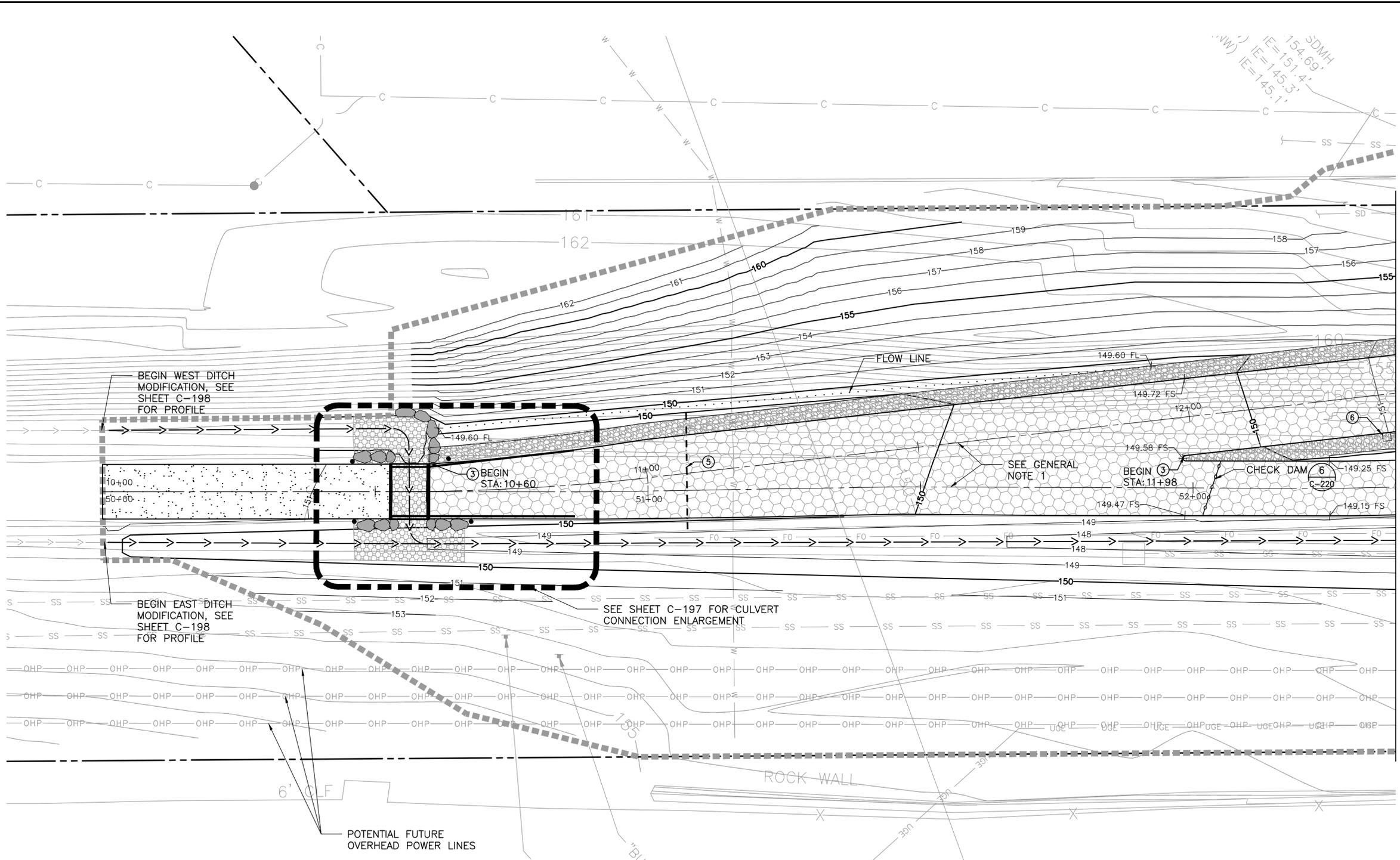
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CITY OF KIRKLAND
TOTEM LAKE PEDESTRIAN BRIDGE
TEMPORARY TRAFFIC CONTROL PLAN
DETOUR FOR NIGHT TIME CLOSURE OF
NE 124TH STREET

SHEET:	TLC - C - 189
SCALE:	AS SHOWN
FILENAME:	TLPB_180_TRAFFIC-CNTRL.dwg

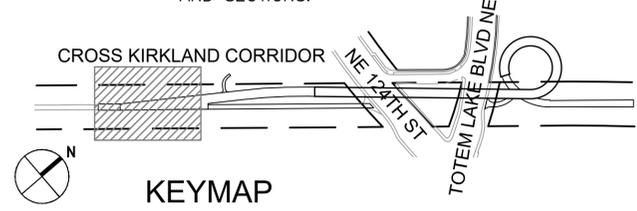
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- LEGEND**
- PROPERTY LINE
 - APPROXIMATE LIMITS OF WORK
 - ▨ MSE WALL, SEE NOTE 2
 - RESTORED DITCH (1/3) C-221
 - INFILTRATION POND (4-6) C-221
 - STORM SEWER PIPE, TYPE AND SIZE PER PLANS
 - ▨ POROUS ASPHALT TRAIL (2) C-220
 - ▨ CRUSHED SURFACING FOR TRAIL, 4" DEPTH
 - ▨ GRAVEL SHOULDER (2) C-221
 - x-xxx.xx SPOT ELEVATION
 - SWALE (1/4) C-221
 - FS/FL FINISHED SURFACE/FLOW LINE
 - CONC/STEEL PIER, CONC ABUTMENT WALL, SEE STRUCTURAL PLANS
 - MAJOR CONTOUR (5')
 - MINOR CONTOUR (1')
 - * * * SPLIT RAIL FENCE (2) C-222
 - GUARDRAIL, SEE SHEET S-223

- CONSTRUCTION NOTES**
- ① 1' GRAVEL SHOULDER
 - ② 2' GRAVEL SHOULDER
 - ③ 3' GRAVEL SHOULDER
 - ④ REINSTALL SIGN PER COK STD PLAN CK-R.43
 - ⑤ INSTALL IRRIGATION SLEEVE PER L-210A/B, MAX SPACING 200'
 - ⑥ LIGHT FOUNDATION/JUNCTION BOX, SEE ELECTRICAL
 - ⑦ CRITICAL AREA SIGN ON SPLIT RAIL FENCE (1/2) C-222

- GENERAL NOTES**
1. SEE S-103 TO S-107 FOR TRAIL ALIGNMENT DATA AND TYPICAL TRAIL SECTIONS.
 2. SEE S-108 TO S-114 FOR MSE WALL PROFILE AND SECTIONS.



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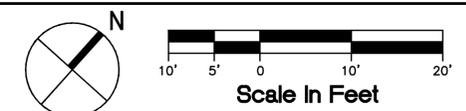
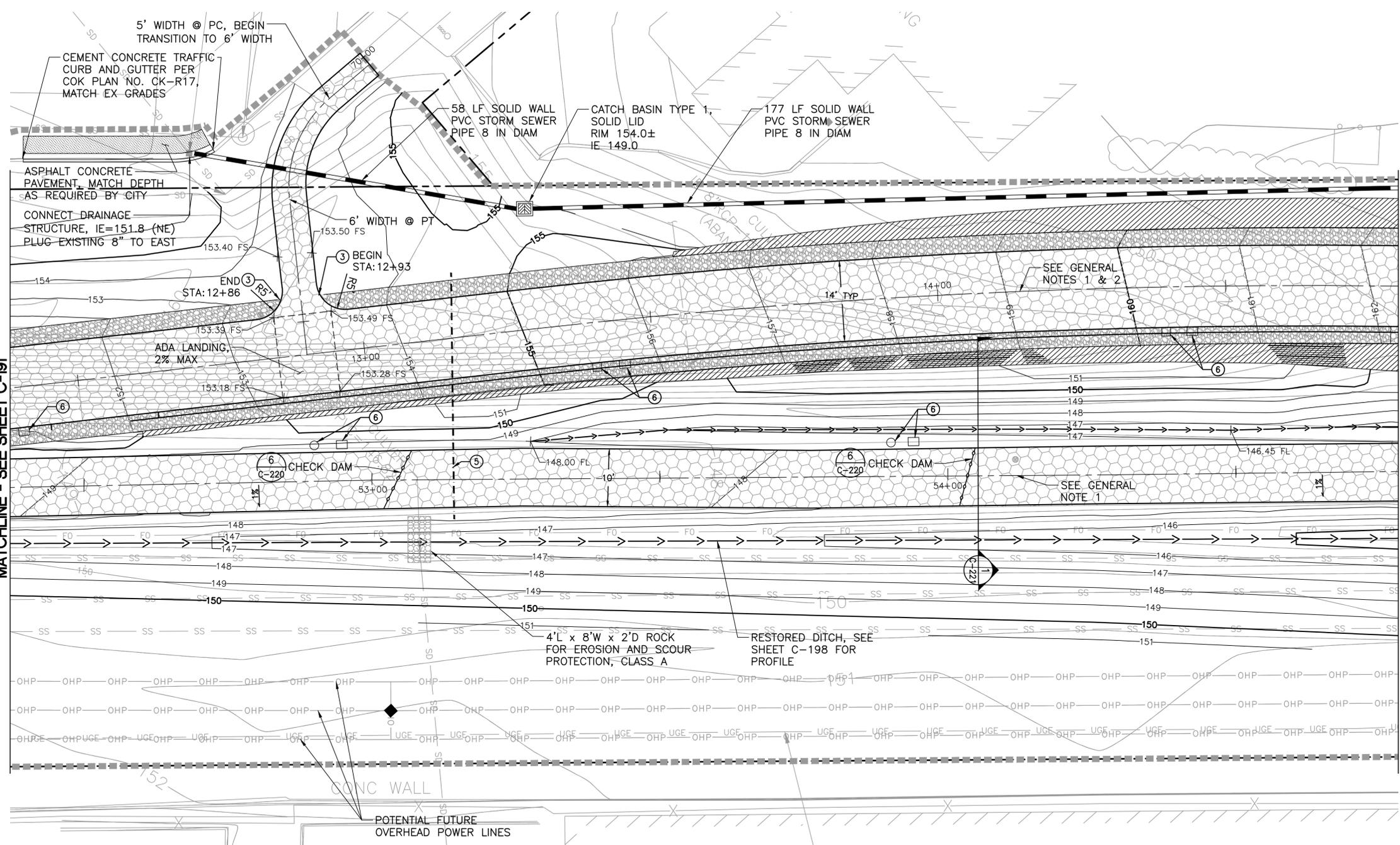
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CITY OF KIRKLAND
 TOTEM LAKE PEDESTRIAN BRIDGE
 GRADING AND DRAINAGE PLAN 1

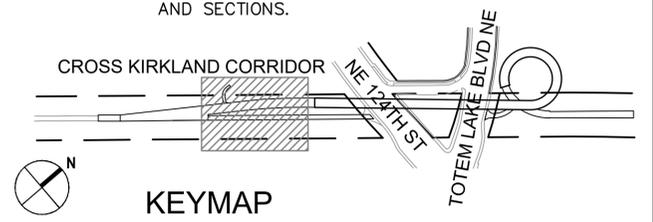
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- LEGEND**
- PROPERTY LINE
 - APPROXIMATE LIMITS OF WORK
 - ▨ MSE WALL, SEE NOTE 2
 - RESTORED DITCH (1/3 C-221)
 - INFILTRATION POND (4-6 C-221)
 - STORM SEWER PIPE, TYPE AND SIZE PER PLANS
 - ▨ POROUS ASPHALT TRAIL (2 C-220)
 - ▨ CRUSHED SURFACING FOR TRAIL, 4" DEPTH
 - ▨ GRAVEL SHOULDER (2 C-221)
 - ×XXX.XX SPOT ELEVATION
 - SWALE (1/4 C-221)
 - FS/FL FINISHED SURFACE/FLOW LINE
 - CONC/STEEL PIER, CONC ABUTMENT WALL, SEE STRUCTURAL PLANS
 - MAJOR CONTOUR (5')
 - MINOR CONTOUR (1')
 - × SPLIT RAIL FENCE (2 C-222)
 - GUARDRAIL, SEE SHEET S-223

- CONSTRUCTION NOTES**
- ① 1' GRAVEL SHOULDER
 - ② 2' GRAVEL SHOULDER
 - ③ 3' GRAVEL SHOULDER
 - ④ REINSTALL SIGN PER COK STD PLAN CK-R.43
 - ⑤ INSTALL IRRIGATION SLEEVE PER L-210A/B, MAX SPACING 200'
 - ⑥ LIGHT FOUNDATION/JUNCTION BOX, SEE ELECTRICAL
 - ⑦ CRITICAL AREA SIGN ON SPLIT RAIL FENCE (1/2 C-222)
- GENERAL NOTES**
1. SEE S-103 TO S-107 FOR TRAIL ALIGNMENT DATA AND TYPICAL TRAIL SECTIONS.
 2. SEE S-108 TO S-114 FOR MSE WALL PROFILE AND SECTIONS.



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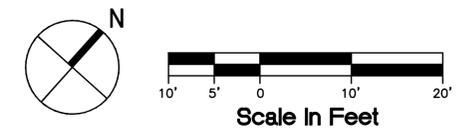
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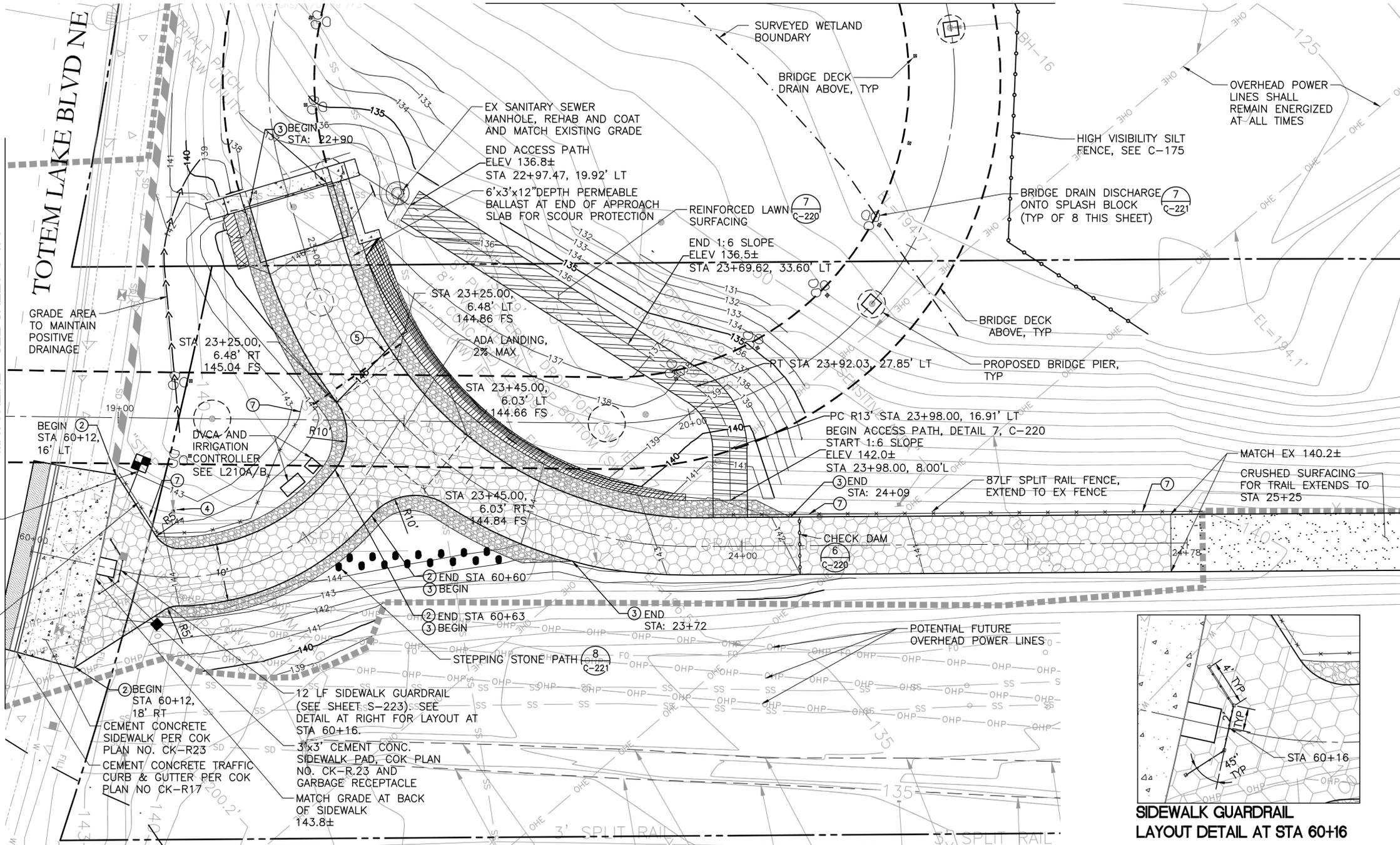
KG 2019/12/06
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CITY OF KIRKLAND
 TOTEM LAKE PEDESTRIAN BRIDGE
 GRADING AND DRAINAGE PLAN 2

SHEET: TLC - C-192
 SCALE: AS SHOWN
 FILENAME: TLPB_190_GRADE-DRAIN.dwg



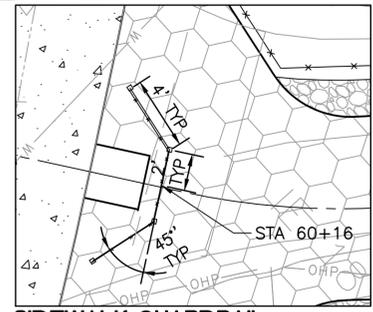
MATCHLINE - SEE SHEET C-196



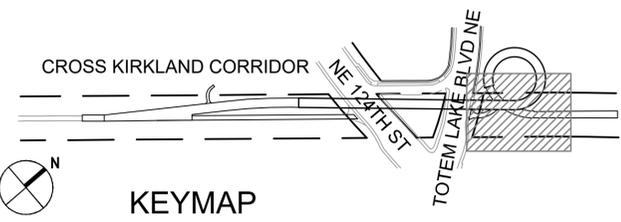
LEGEND

- PROPERTY LINE
- APPROXIMATE LIMITS OF WORK
- MSE WALL, SEE NOTE 2
- RESTORED DITCH (1/3 C-221)
- INFILTRATION POND (4-6 C-221)
- STORM SEWER PIPE, TYPE AND SIZE PER PLANS
- POROUS ASPHALT TRAIL (2 C-220)
- CRUSHED SURFACING FOR TRAIL, 4" DEPTH
- GRAVEL SHOULDER (2 C-221)
- SPOT ELEVATION (xxx.xx)
- SWALE (1/4 C-221)
- FINISHED SURFACE/FLOW LINE (FS/FL)
- CONC/STEEL PIER, CONC ABUTMENT WALL, SEE STRUCTURAL PLANS
- MAJOR CONTOUR (5')
- MINOR CONTOUR (1')
- SPLIT RAIL FENCE (2 C-222)
- GUARDRAIL, SEE SHEET S-223

- CONSTRUCTION NOTES**
- 1' GRAVEL SHOULDER
 - 2' GRAVEL SHOULDER
 - 3' GRAVEL SHOULDER
 - REINSTALL SIGN PER COK STD PLAN CK-R.43
 - INSTALL IRRIGATION SLEEVE PER L-210A/B, MAX SPACING 200'
 - LIGHT FOUNDATION/JUNCTION BOX, SEE ELECTRICAL
 - CRITICAL AREA SIGN ON SPLIT RAIL FENCE (1/2 C-222)



SIDEWALK GUARDRAIL LAYOUT DETAIL AT STA 60+16



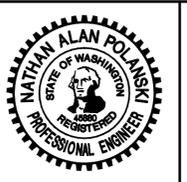
SURVEY NOTE:

- 1) EXISTING CONDITIONS SHOWN ALONG TOTEM LAKE BLVD NE (CURB ALIGNMENT, SIDEWALK, STRIPING AND DRAINAGE STRUCTURES) ARE BASED ON PROPOSED IMPROVEMENTS SHOWN IN THE BID SET FOR THE CITY'S TOTEM LAKE GATEWAY PROJECT (CIP NO. STC006050).

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CITY OF KIRKLAND
 TOTEM LAKE PEDESTRIAN BRIDGE
 GRADING AND DRAINAGE PLAN 5

SHEET: **TLC - C-195**
 SCALE: **AS SHOWN**
 FILENAME: TLPB_190_GRADE-DRAIN.dwg

SURVEY NOTE:

1) EXISTING CONDITIONS SHOWN ALONG TOTEM LAKE BLVD NE (CURB ALIGNMENT, SIDEWALK, STRIPING AND DRAINAGE STRUCTURES) ARE BASED ON PROPOSED IMPROVEMENTS SHOWN IN THE BID SET FOR THE CITY'S TOTEM LAKE GATEWAY PROJECT (CIP NO. STC006050).

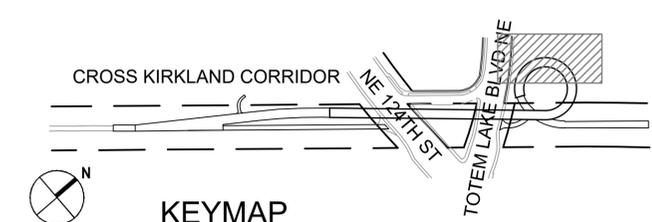
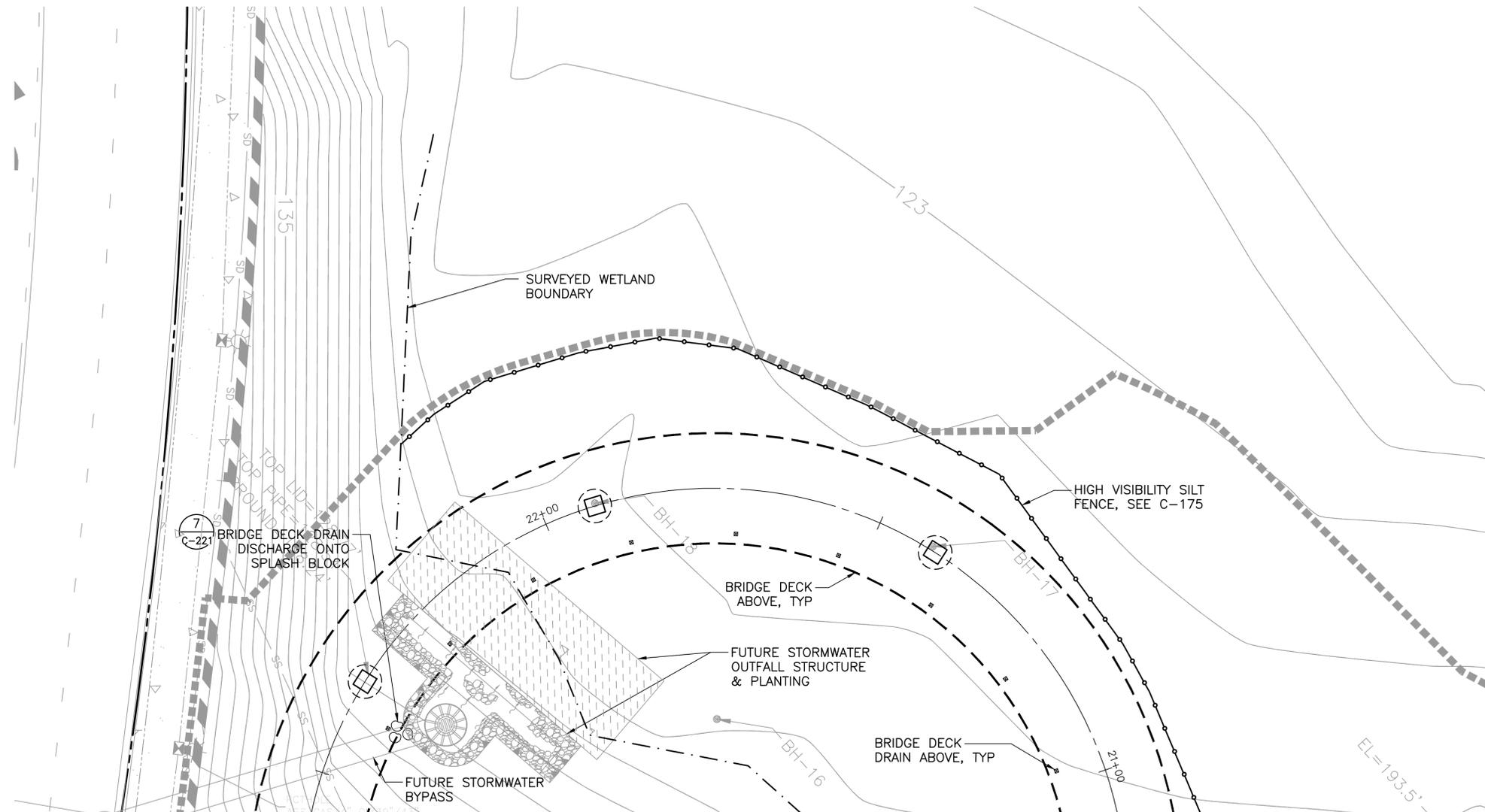


LEGEND

- PROPERTY LINE
- ▬▬▬▬▬▬ APPROXIMATE LIMITS OF WORK
- ▨ MSE WALL, SEE NOTE 2
- RESTORED DITCH (1/3 C-221)
- ⊖ INFILTRATION POND (4-6 C-221)
- STORM SEWER PIPE, TYPE AND SIZE PER PLANS
- ⬢ POROUS ASPHALT TRAIL (2 C-220)
- ⬢ CRUSHED SURFACING FOR TRAIL, 4" DEPTH
- ⬢ GRAVEL SHOULDER (2 C-221)
- x-xxx.xx SPOT ELEVATION
- SWALE (1/4 C-221)
- FS/FL FINISHED SURFACE/FLOW LINE
- ⊠ CONC/STEEL PIER, CONC ABUTMENT WALL, SEE STRUCTURAL PLANS
- MAJOR CONTOUR (5')
- MINOR CONTOUR (1')
- x-x-x-x SPLIT RAIL FENCE (2 C-222)
- GUARDRAIL, SEE SHEET S-223

CONSTRUCTION NOTES

- ① 1' GRAVEL SHOULDER
- ② 2' GRAVEL SHOULDER
- ③ 3' GRAVEL SHOULDER
- ④ REINSTALL SIGN PER COK STD PLAN CK-R.43
- ⑤ INSTALL IRRIGATION SLEEVE PER L-210A/B, MAX SPACING 200'
- ⑥ LIGHT FOUNDATION/JUNCTION BOX, SEE ELECTRICAL
- ⑦ CRITICAL AREA SIGN ON SPLIT RAIL FENCE (1/2 C-222)



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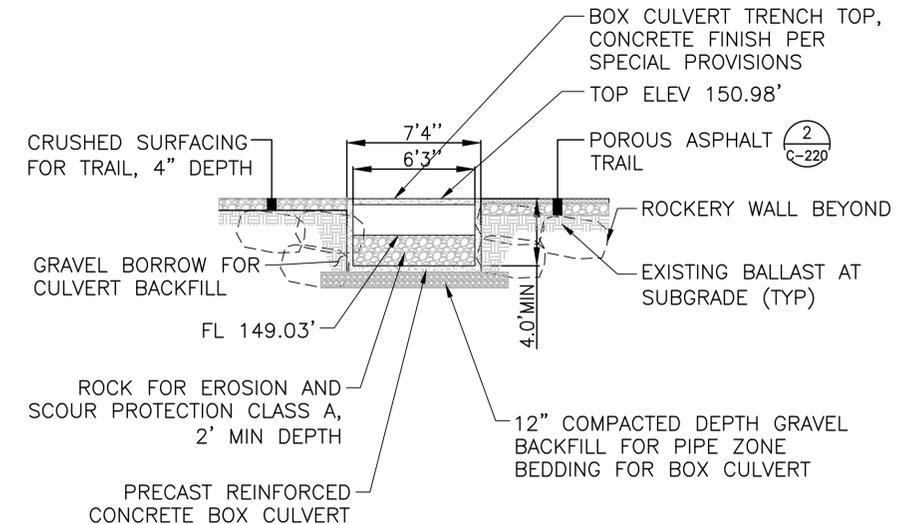
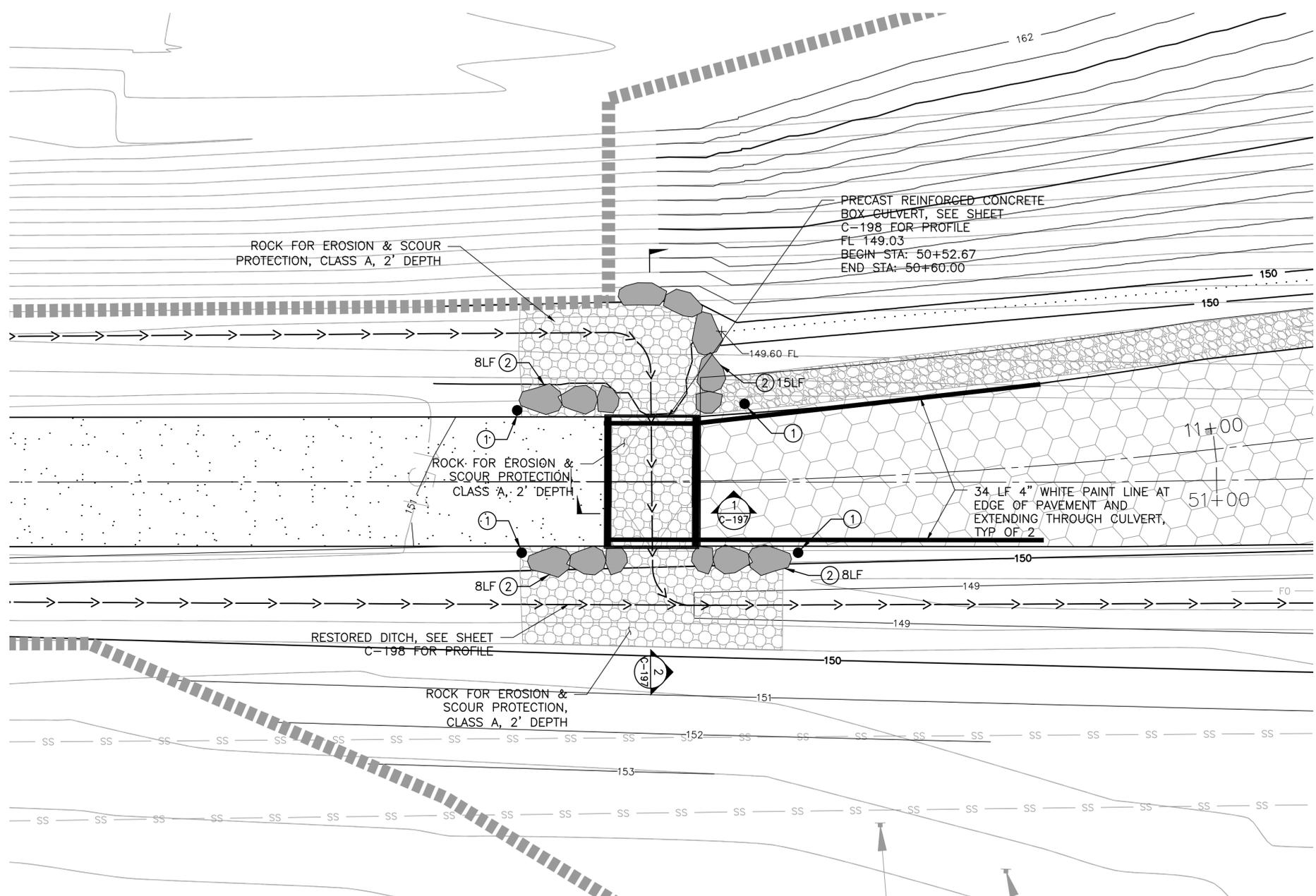


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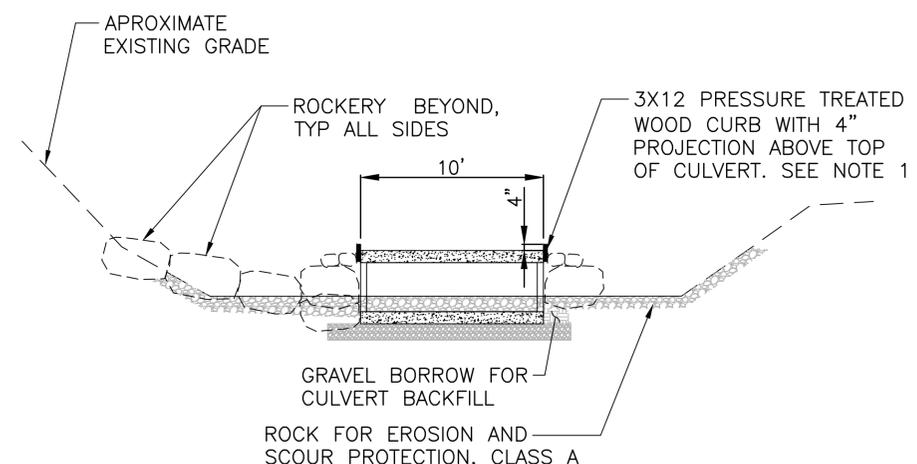
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JA	DRAWN BY:	2019/12/06	DATE
KG	CHECKED BY:	2019/12/06	DATE

CITY OF KIRKLAND
TOTEM LAKE PEDESTRIAN BRIDGE
 GRADING AND DRAINAGE PLAN 6

SHEET: **TLC - C-196**
 SCALE: **AS SHOWN**
 FILENAME: TLPB_190_GRADE-DRAIN.dwg



Box Culvert Section ^{NTS} 1



NOTE:
 1. ATTACH PRESSURE TREATED WOOD CURB WITH 15-1/4" DIAMETER COATED CONCRETE SCREWS WITH STAGGERED 11"x3" PATTERN

Box Culvert Section ^{NTS} 2

Culvert Connection Enlargement ^{NTS} 3

CONSTRUCTION NOTES

- ① INSTALL FLEXIBLE GUIDE POST WITH TYPE WW REFLECTIVE SHEETING PER WSDOT STANDARD PLAN M-40.10-03. TOP OF POST SET 36" ABOVE TRAIL GRADE. LOCATE POST AT EDGE OF TRAIL 2' IN ADVANCE OF ROCKERY WALL
- ② INSTALL ROCKERY WALL PER COK PLAN NO CK-R.52

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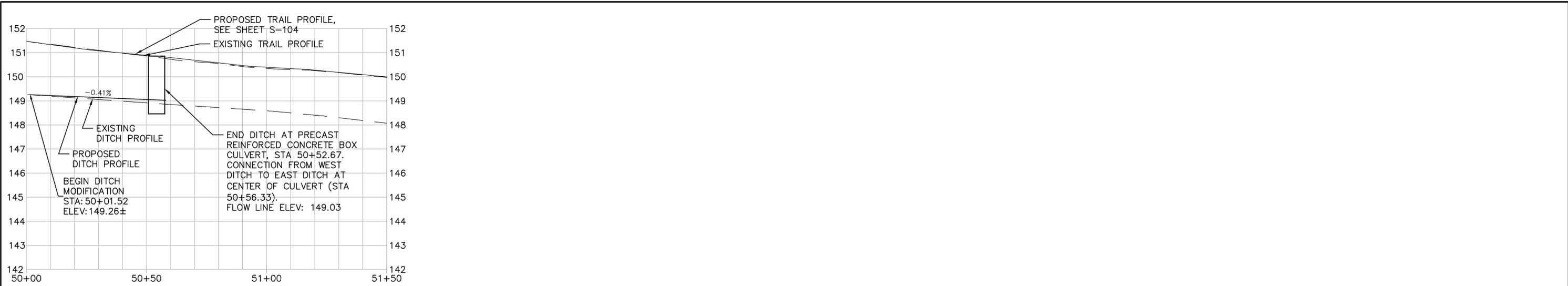
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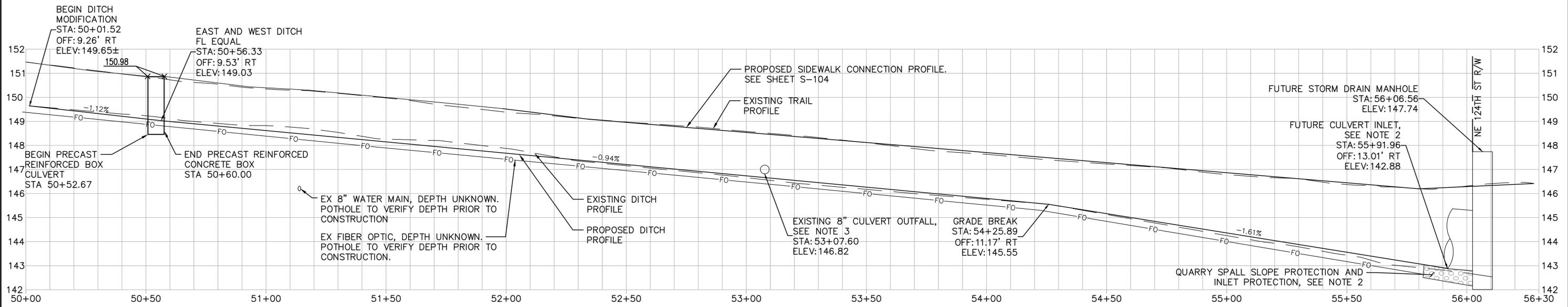
CITY OF KIRKLAND
 TOTEM LAKE PEDESTRIAN BRIDGE
 WEST DITCH TO EAST DITCH CULVERT CONNECTION

SHEET: TLC - C - 197
 SCALE: AS SHOWN
 FILENAME: TLPB_190_GRADE-DRAIN.dwg

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WEST DITCH PROFILE



EAST DITCH AND SIDEWALK CONNECTION PROFILE

NOTES

1. SEE GRADING AND DRAINAGE PLANS, SHEETS C-191 TO C-197, FOR ADDITIONAL INFORMATION.
2. END RESTORED DITCH AT CULVERT INLET. INSTALL QUARRY SPALL SLOPE PROTECTION AND INLET PROTECTION PER APPROVED RECORD DRAWINGS
3. INSTALL 4'L x 8'W x 2'D PAD OF ROCK FOR EROSION AND SCOUR PROTECTION, CLASS A AT CULVERT OUTFALL

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CITY OF KIRKLAND
TOTEM LAKE PEDESTRIAN BRIDGE
PROFILE-SIDEWALK CONNECTION AND RESTORED EAST DITCH

SHEET: TLC - C-198
SCALE: AS SHOWN
FILENAME: TLPB_190_GRADE-DRAIN.dwg

RESTORATION PLANT SCHEDULE DT=DROUGHT TOLERANT, NWN=NORTHWEST NATIVE, EG=EVERGREEN, S=FULL SUN TOLERANT, PS=PART SHADE TOLERANT

	DT	NWN	EG	BOTANICAL NAME	COMMON NAME	SIZE / SPACING	QTY.	DETAIL	EXPOSURE	NOTES
WETLAND RESTORATION / ENHANCEMENT SHRUB MIX										
	NWN			LONICERA INVOLUCRATA	BLACK TWINBERRY	40 CU. IN. PLUG/ 5' O.C.	255	WSDOT STANDARD PLAN H-10.10-00	S, PS	INTERMIX SPECIES IN GROUPS OF 3-5 PLANTS EACH
	NWN			RUBUS SPECTABILIS	SALMONBERRY	40 CU. IN. PLUG/ 5' O.C.	255	WSDOT STANDARD PLAN H-10.10-00	S, PS	
	NWN			SYMPHORICARPOS ALBUS	SNOWBERRY	LIVE STAKES/ 5' O.C.	255	WSDOT STANDARD PLAN H-10.10-00	S, PS	
WETLAND RESTORATION GROUNDCOVER MIX										
	NWN	EG		ATHYRIUM FILIX-FEMINA	LADY FERN	4" POTS/ 1' O.C.	1670	WSDOT STANDARD PLAN H-10.10-00	PS	INTERMIX SPECIES IN GROUPS OF 3-5 PLANTS EACH
	NWN	EG		CAREX OBNUPTA	SLOUGH SEDGE	4" POTS/ 1' O.C.	1670	WSDOT STANDARD PLAN H-10.10-00	S, PS	
BUFFER RESTORATION / ENHANCEMENT SHRUB MIX										
	NWN			AMELANCHIER ALNIFOLIA	SERVICEBERRY	4" POTS/ 5' O.C.	495	WSDOT STANDARD PLAN H-10.10-00	S, PS	INTERMIX SPECIES IN GROUPS OF 3-5 PLANTS EACH
	NWN			LONICERA INVOLUCRATA	BLACK TWINBERRY	4" POTS/ 5' O.C.	495	WSDOT STANDARD PLAN H-10.10-00	S, PS	
	NWN			PHILADELPHUS LEWISII	MOCK ORANGE	4" POTS/ 5' O.C.	495	WSDOT STANDARD PLAN H-10.10-00	S, PS	
BUFFER RESTORATION GROUNDCOVER MIX										
	NWN			FRAGARIA CHILOENSIS	BEACH STRAWBERRY	4" POTS/ 2' O.C.	1385	WSDOT STANDARD PLAN H-10.10-00	S, PS	INTERMIX EQUALLY
	NWN	EG		POLYSTICHUM MUNITUM	SWORD FERN	2 GAL./ 3' O.C.	615	WSDOT STANDARD PLAN H-10.10-00	PS	
TREES										
	NWN			ACER CIRCINATUM	VINE MAPLE	5 GAL.	21	WSDOT STANDARD PLAN H-10.10-00		
	NWN			ALNUS RUBRA	RED ALDER	BARE ROOT 12-18"	47	WSDOT STANDARD PLAN H-10.10-00	S, PS	
						5 GAL.	15			
	NWN			CALOCEDRUS DECURRENS	INCENSE CEDAR	BARE ROOT 12-18"	40	WSDOT STANDARD PLAN H-10.15-00		
	NWN			SALIX LUCIDA	PACIFIC WILLOW	LIVE STAKES/ AS SHOWN	32	WSDOT STANDARD PLAN H-10.15-00	S	
	NWN	EG		THUJA PLICATA	WESTERN RED CEDAR	BARE ROOT 12-18"	13	WSDOT STANDARD PLAN H-10.10-00	S, PS	
					6' HT	15				

CORRIDOR PLANT SCHEDULE

	DRAINAGE DITCH SEEDING MIX						SQUARE YARDS	565		
INFILTRATION AND DRAINAGE DITCH POND MIX : ZONE 1										
	NWN			CAREX STIPATA	SAWBREAK SEDGE	10 CU. IN. PLUGS/9" OC.	570	WSDOT STANDARD PLAN H-10.10-00	S, PS	INTERMIX SPECIES IN GROUPS OF 3-5 PLANTS EACH
	DT	NWN		JUNCUS ENSIFOLIUS	DAGGERLEAF RUSH	10 CU. IN. PLUGS/9" OC.	570	WSDOT STANDARD PLAN H-10.10-00	S, PS	
		NWN		SCIRPUS MICROCARPUS	SMALL-FRUITED BALRUSH	10 CU. IN. PLUGS/9" OC.	570	WSDOT STANDARD PLAN H-10.10-00	S, PS	
INFILTRATION POND MIX : ZONE 2										
	NWN			CORNUS SERICEA 'KELSEYI'	KELSEY REDSTEM DOGWOOD	2 GAL./ 24" OC.	70	WSDOT STANDARD PLAN H-10.10-00	S, PS	INTERMIX SPECIES IN GROUPS OF 3-5 PLANTS EACH, PLANT ALL 2 GALLON DOGWOODS IN INFILTRATION POND.
						BARE ROOT 12"-15"/ 24" OC.	140			
	DT	NWN			PENSTEMON SERRULATUS	CASCADE BEARD-TONGUE	4" POTS/ 24" O.C.	210	WSDOT STANDARD PLAN H-10.10-00	
INFILTRATION POND MIX : ZONE 3										
	DT	NWN		ACHILLEA MILLEFOLIUM	COMMON YARROW	4" POTS/ 18" O.C.	195	WSDOT STANDARD PLAN H-10.10-00	S, PS	INTERMIX SPECIES IN GROUPS OF 3-5 PLANTS EACH
	DT	NWN	EG	MAHONIA REPENS	CREEPING OREGON HOLLY-GRAPE	4" POTS/ 18" O.C.	195	WSDOT STANDARD PLAN H-10.10-00	S, PS	
	DT	NWN		PENSTEMON SERRULATUS	CASCADE BEARD-TONGUE	4" POTS/ 18" O.C.	195	WSDOT STANDARD PLAN H-10.10-00	S, PS	
SEEDED LAWN						SQUARE YARDS	610			
SOD INSTALLATION						SQUARE YARDS	665			
TRAIL EDGE SEDUM MIX										
	DT	NWN	EG	SEDUM DIVERGENS	STONECROP	4" POTS/ 12" O.C.	1490	WSDOT STANDARD PLAN H-10.10-00	S, PS	INTERMIX EQUALLY
	DT	NWN	EG	SEDUM OREGANUM	OREGON STONECROP	4" POTS/ 12" O.C.	1490	WSDOT STANDARD PLAN H-10.10-00	S	
	NWN			SYMPHORICARPOS ALBUS	SNOWBERRY	2 GAL.	14	WSDOT STANDARD PLAN H-10.10-00	S, PS	
	TREES									
	NWN			ACER CIRCINATUM	VINE MAPLE	2 GAL.	11	WSDOT STANDARD PLAN H-10.10-00		
	NWN			CALOCEDRUS DECURRENS	INCENSE CEDAR	2 GAL.	6	WSDOT STANDARD PLAN H-10.10-00		
ARBORIST WOOD CHIP MULCH						SQUARE FEET	810			

NOTE

1. ALL PLANTINGS, OTHER THAN LAWNS, ARE BID AS PSIZE BASED UPON SIZES.

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Know what's below. Call before you dig.



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CITY OF KIRKLAND
TOTEM LAKE PEDESTRIAN BRIDGE

PLANT SCHEDULE

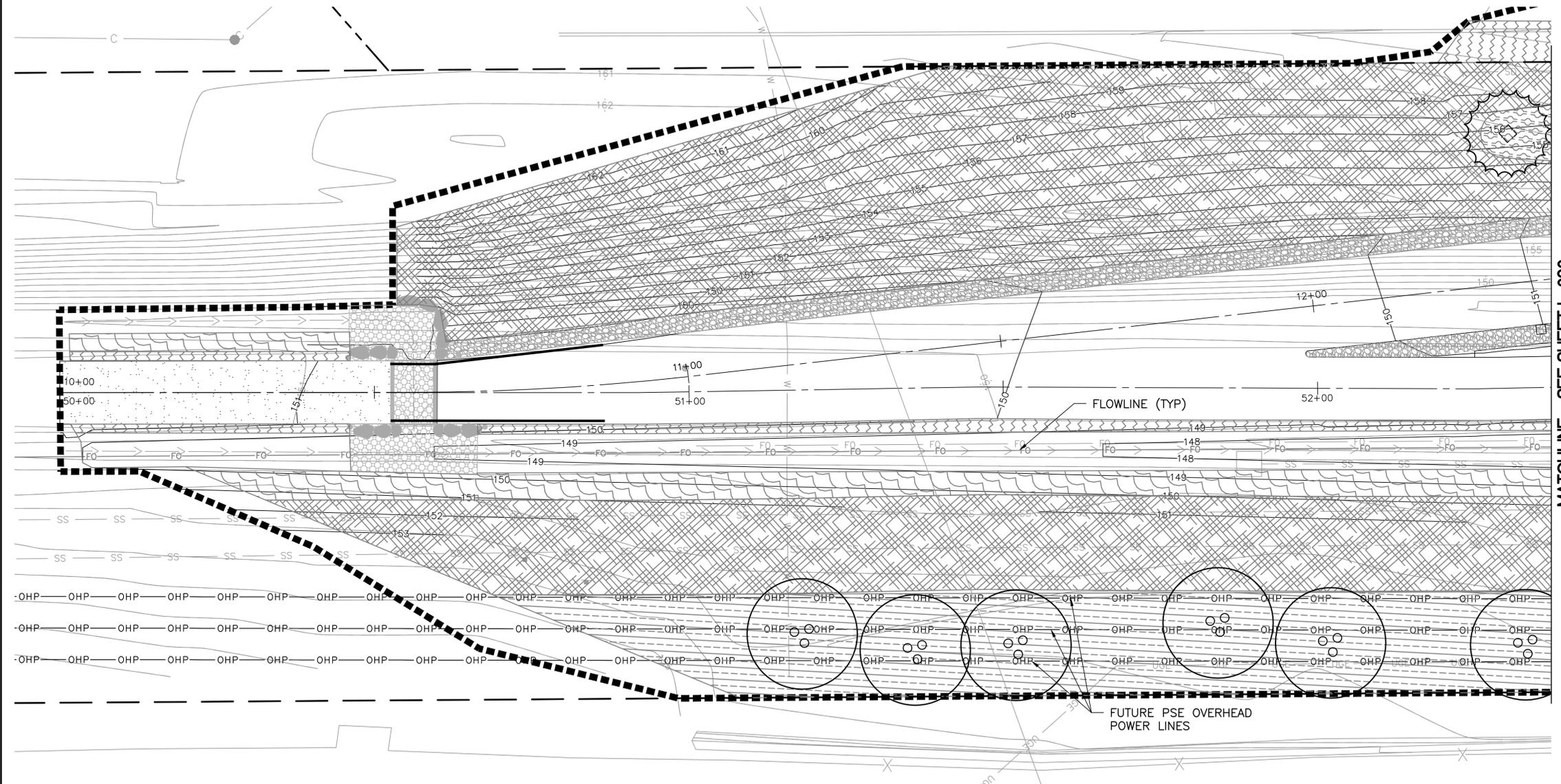
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FILENAME: TLPB_200_LANDSCAPE.dwg

- LEGEND**
- PROPERTY LINE
 - PROJECT LIMITS
 - BRIDGE AND RAILING OVERHEAD, TYP
 - RESTORED/ REPLACED SNAG
 - MSE WALL, SEE SHEET L-208 FOR MSE WALL PLANTING LAYOUT
 - ARBORIST WOOD CHIP MULCH

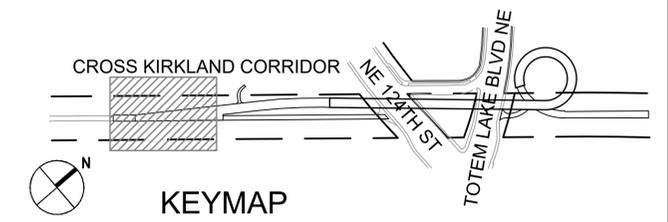
- NOTES**
1. INVASIVE PLANTS TO BE REMOVED TWICE FROM THE ENTIRE PROJECT SITE THROUGHOUT THE COURSE OF THE PROJECT:
REMOVAL #1 SHALL OCCUR AT ONSET OF CONSTRUCTION, AND REMOVAL #2 PRIOR TO INSTALLING NEW PROJECT PLANTING.
 2. DISPERSE SALVAGED LARGE WOODY DEBRIS AND INSTALL SALVAGED SNAGS WITHIN THE WETLAND AND WETLAND BUFFER PRIOR TO PLANT INSTALLATION. COORDINATE LOCATIONS WITH THE ENGINEER.
 3. CONTRACTOR SHALL DESIGN AND PROVIDE IRRIGATION TO THESE AREAS. SEE SPECS. AND SHEETS L-210A AND L-210B FOR DESIGN BUILD IRRIGATION NOTES.
 4. CONTRACTOR SHALL PROTECT POROUS ASPHALT PAVEMENT FROM DEBRIS AND SEDIMENT. SEE SPECIFICATIONS.

CORRIDOR UNDERSTORY PLANT SCHEDULE
SEE SHEET L-200 FOR THE PLANTING LEGEND

- WETLAND RESTORATION/ENHANCEMENT SHRUB MIX
- WETLAND RESTORATION GROUND COVER MIX
- BUFFER RESTORATION/ENHANCEMENT SHRUB MIX (SEE NOTE 3)
- BUFFER RESTORATION GROUND COVER MIX (SEE NOTE 3)
- DRAINAGE DITCH SEEDING MIX (SEE NOTE 3)
- INFILTRATION POND AND DRAINAGE DITCH MIX ZONE 1 (SEE NOTE 3)
- INFILTRATION POND MIX ZONE 2 (SEE NOTE 3)
- INFILTRATION MIX ZONE 3 (SEE NOTE 3)
- HYDROSEED LAWN (SEE NOTE 3)
- SOD LAWN (SEE NOTE 3)
- TRAIL EDGE SEDUM MIX (SEE NOTE 3)



MATCHLINE - SEE SHEET L-202



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CITY OF KIRKLAND
TOTEM LAKE PEDESTRIAN BRIDGE
CORRIDOR RESTORATION 1

SHEET: **TLC - L - 201**
SCALE: **AS SHOWN**
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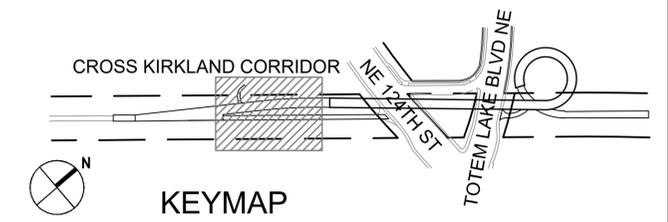
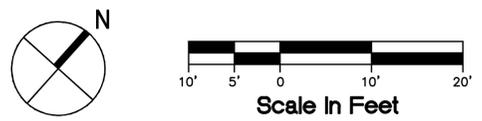
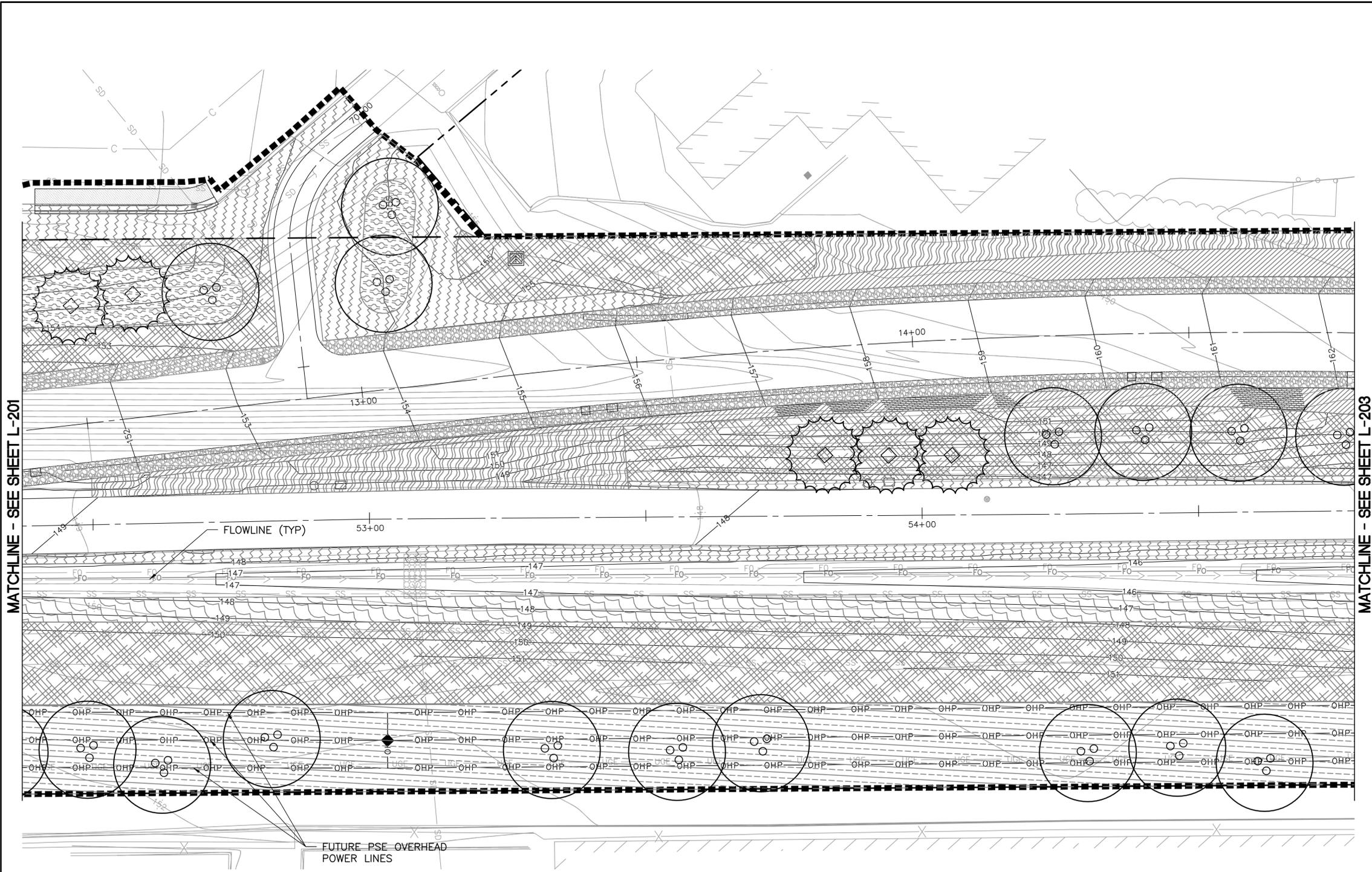
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- LEGEND**
- PROPERTY LINE
 - PROJECT LIMITS
 - - - BRIDGE AND RAILING OVERHEAD, TYP
 - ⊕ RESTORED/ REPLACED SNAG
 - [Hatched Box] MSE WALL, SEE SHEET L-208 FOR MSE WALL PLANTING LAYOUT
 - [Dotted Box] ARBORIST WOOD CHIP MULCH

- NOTES**
- INVASIVE PLANTS TO BE REMOVED TWICE FROM THE ENTIRE PROJECT SITE THROUGHOUT THE COURSE OF THE PROJECT:
REMOVAL #1 SHALL OCCUR AT ONSET OF CONSTRUCTION, AND REMOVAL #2 PRIOR TO INSTALLING NEW PROJECT PLANTING.
 - DISPERSE SALVAGED LARGE WOODY DEBRIS AND INSTALL SALVAGED SNAGS WITHIN THE WETLAND AND WETLAND BUFFER PRIOR TO PLANT INSTALLATION. COORDINATE LOCATIONS WITH THE ENGINEER.
 - CONTRACTOR SHALL DESIGN AND PROVIDE IRRIGATION TO THESE AREAS. SEE SPECS. AND SHEETS L-210A AND L-210B FOR DESIGN BUILD IRRIGATION NOTES.
 - CONTRACTOR SHALL PROTECT POROUS ASPHALT PAVEMENT FROM DEBRIS AND SEDIMENT. SEE SPECIFICATIONS.

CORRIDOR UNDERSTORY PLANT SCHEDULE
SEE SHEET L-200 FOR THE PLANTING LEGEND

- [Wavy Hatched Box] WETLAND RESTORATION/ENHANCEMENT SHRUB MIX
- [Diagonal Hatched Box] WETLAND RESTORATION GROUND COVER MIX
- [Cross-hatched Box] BUFFER RESTORATION/ENHANCEMENT SHRUB MIX (SEE NOTE 3)
- [Horizontal Hatched Box] BUFFER RESTORATION GROUND COVER MIX (SEE NOTE 3)
- [Wavy Box] DRAINAGE DITCH SEEDING MIX (SEE NOTE 3)
- [Vertical Hatched Box] INFILTRATION POND AND DRAINAGE DITCH MIX ZONE 1 (SEE NOTE 3)
- [Grid Box] INFILTRATION POND MIX ZONE 2 (SEE NOTE 3)
- [X-pattern Box] INFILTRATION MIX ZONE 3 (SEE NOTE 3)
- [Wavy Box] HYDROSEED LAWN (SEE NOTE 3)
- [Rectangular Box] SOD LAWN (SEE NOTE 3)
- [Zigzag Box] TRAIL EDGE SEDUM MIX (SEE NOTE 3)



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CITY OF KIRKLAND
TOTEM LAKE PEDESTRIAN BRIDGE
CORRIDOR RESTORATION 2

SHEET: **TLC - L - 202**
SCALE: **AS SHOWN**
FILENAME: TLPB_200_LANDSCAPE.dwg

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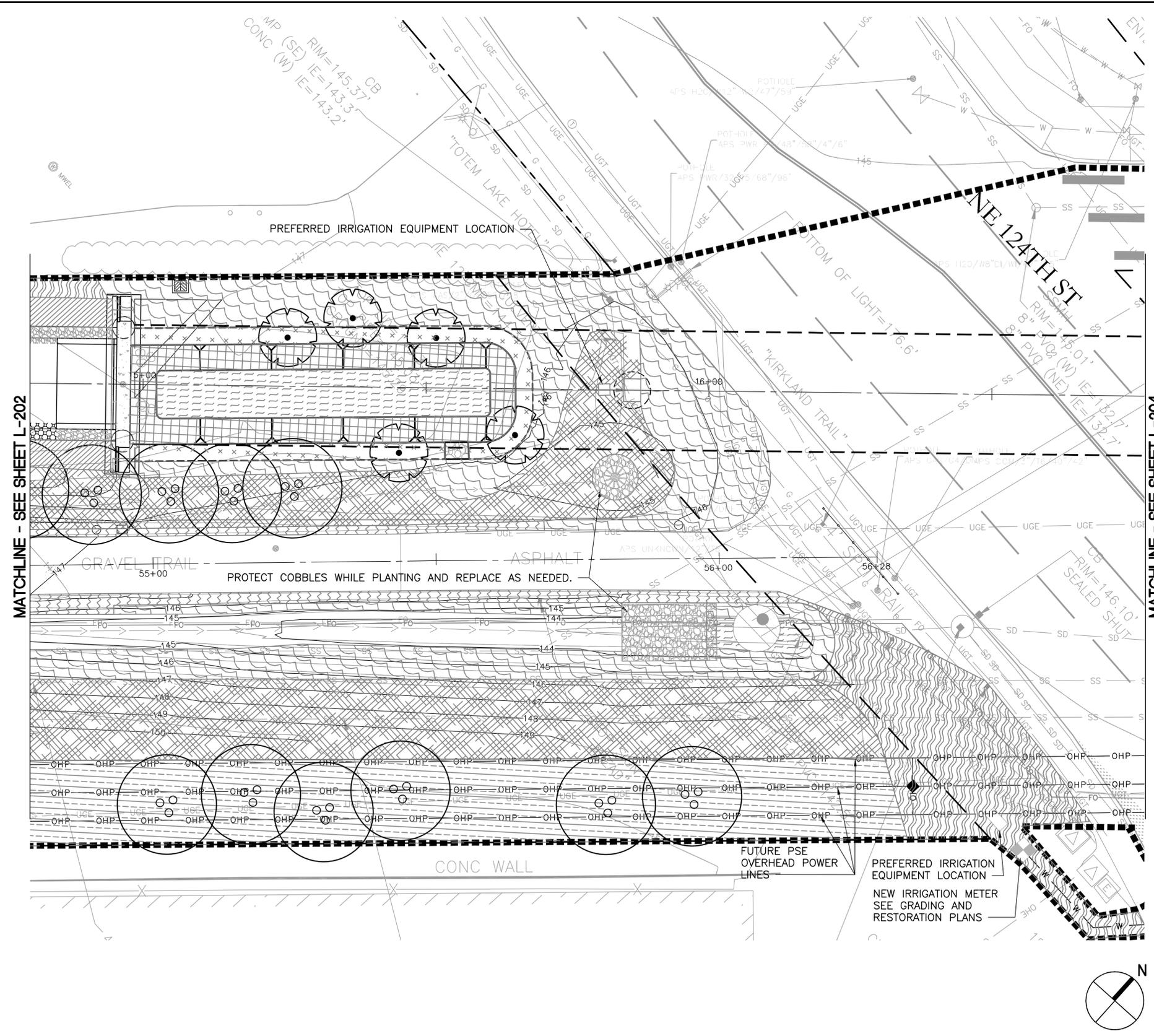
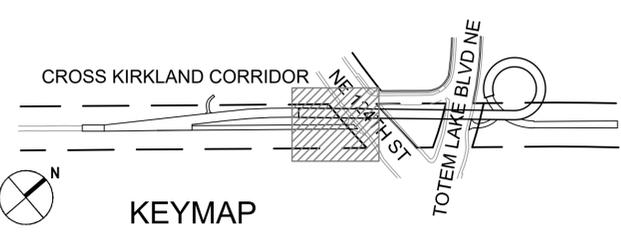
LEGEND

- PROPERTY LINE
- PROJECT LIMITS
- BRIDGE AND RAILING OVERHEAD, TYP
- RESTORED/ REPLACED SNAG
- MSE WALL, SEE SHEET L-208 FOR MSE WALL PLANTING LAYOUT
- ARBORIST WOOD CHIP MULCH

- NOTES**
- INVASIVE PLANTS TO BE REMOVED TWICE FROM THE ENTIRE PROJECT SITE THROUGHOUT THE COURSE OF THE PROJECT:
REMOVAL #1 SHALL OCCUR AT ONSET OF CONSTRUCTION, AND REMOVAL #2 PRIOR TO INSTALLING NEW PROJECT PLANTING.
 - DISPERSE SALVAGED LARGE WOODY DEBRIS AND INSTALL SALVAGED SNAGS WITHIN THE WETLAND AND WETLAND BUFFER PRIOR TO PLANT INSTALLATION. COORDINATE LOCATIONS WITH THE ENGINEER.
 - CONTRACTOR SHALL DESIGN AND PROVIDE IRRIGATION TO THESE AREAS. SEE SPECS. AND SHEETS L-210A AND L-210B FOR DESIGN BUILD IRRIGATION NOTES.
 - CONTRACTOR SHALL PROTECT POROUS ASPHALT PAVEMENT FROM DEBRIS AND SEDIMENT. SEE SPECIFICATIONS.

CORRIDOR UNDERSTORY PLANT SCHEDULE
SEE SHEET L-200 FOR THE PLANTING LEGEND

	WETLAND RESTORATION/ENHANCEMENT SHRUB MIX
	WETLAND RESTORATION GROUND COVER MIX
	BUFFER RESTORATION/ENHANCEMENT SHRUB MIX (SEE NOTE 3)
	BUFFER RESTORATION GROUND COVER MIX (SEE NOTE 3)
	DRAINAGE DITCH SEEDING MIX (SEE NOTE 3)
	INFILTRATION POND AND DRAINAGE DITCH MIX ZONE 1 (SEE NOTE 3)
	INFILTRATION POND MIX ZONE 2 (SEE NOTE 3)
	INFILTRATION MIX ZONE 3 (SEE NOTE 3)
	HYDROSEED LAWN (SEE NOTE 3)
	SOD LAWN (SEE NOTE 3)
	TRAIL EDGE SEDUM MIX (SEE NOTE 3)



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CITY OF KIRKLAND
TOTEM LAKE PEDESTRIAN BRIDGE
CORRIDOR RESTORATION 3

SHEET: TLC - L - 203
SCALE: AS SHOWN
FILENAME: TLPB_200_LANDSCAPE.dwg

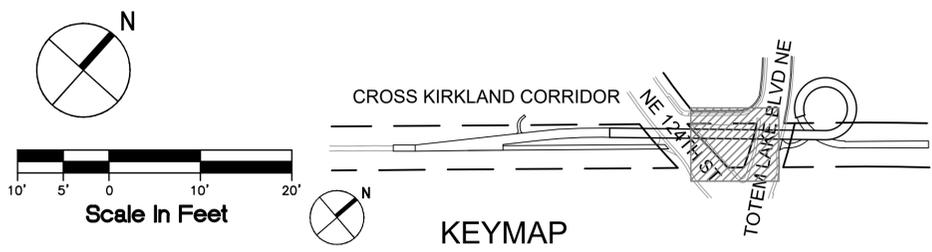
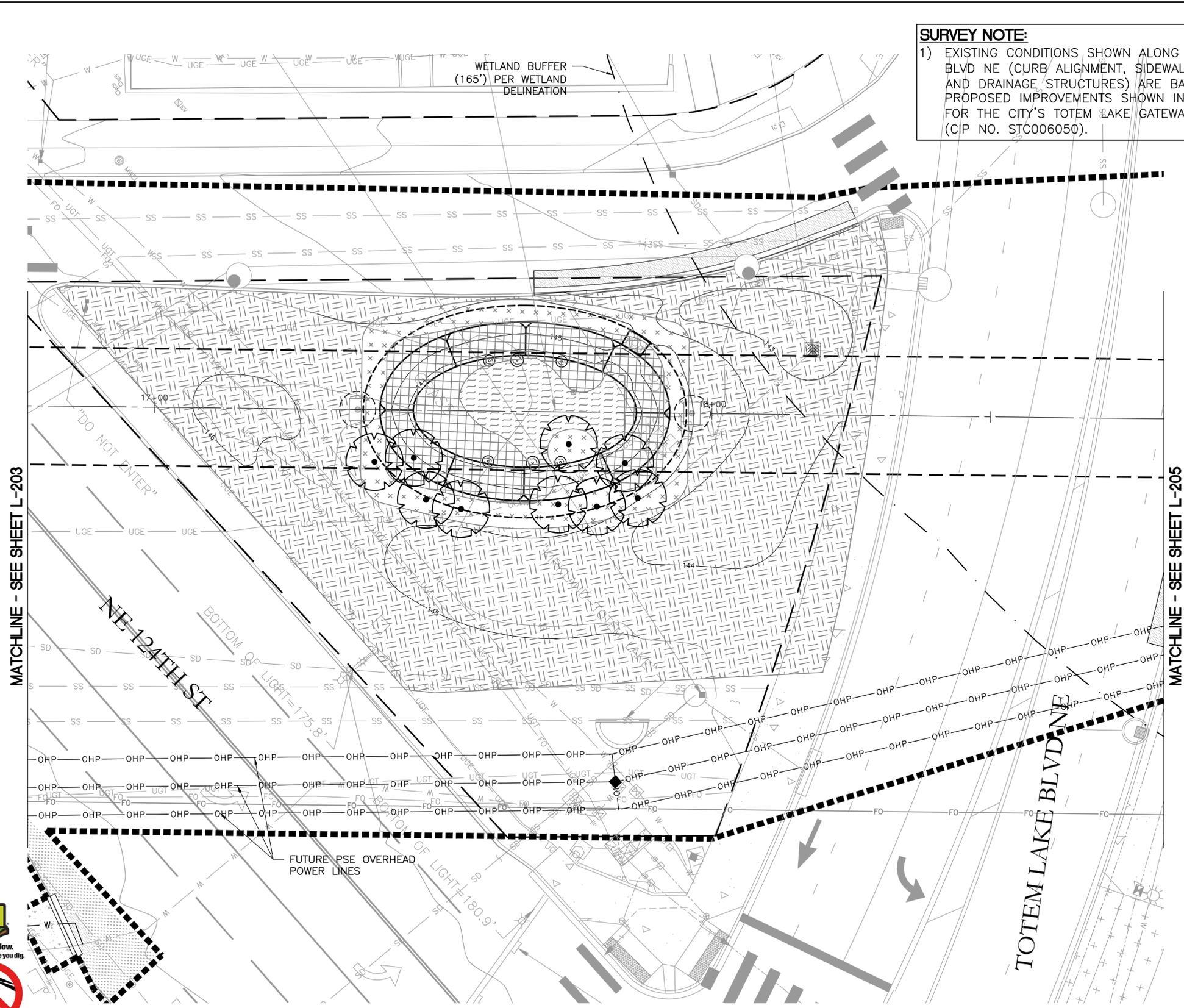
SURVEY NOTE:
 1) EXISTING CONDITIONS SHOWN ALONG TOTEM LAKE BLVD NE (CURB ALIGNMENT, SIDEWALK, STRIPING AND DRAINAGE STRUCTURES) ARE BASED ON PROPOSED IMPROVEMENTS SHOWN IN THE BID SET FOR THE CITY'S TOTEM LAKE GATEWAY PROJECT (CIP NO. STC006050).

- LEGEND**
- PROPERTY LINE
 - PROJECT LIMITS
 - BRIDGE AND RAILING OVERHEAD, TYP
 - RESTORED/ REPLACED SNAG
 - MSE WALL, SEE SHEET L-208 FOR MSE WALL PLANTING LAYOUT
 - ARBORIST WOOD CHIP MULCH

- NOTES**
1. INVASIVE PLANTS TO BE REMOVED TWICE FROM THE ENTIRE PROJECT SITE THROUGHOUT THE COURSE OF THE PROJECT:
 REMOVAL #1 SHALL OCCUR AT ONSET OF CONSTRUCTION, AND REMOVAL #2 PRIOR TO INSTALLING NEW PROJECT PLANTING.
 2. DISPERSE SALVAGED LARGE WOODY DEBRIS AND INSTALL SALVAGED SNAGS WITHIN THE WETLAND AND WETLAND BUFFER PRIOR TO PLANT INSTALLATION. COORDINATE LOCATIONS WITH THE ENGINEER.
 3. CONTRACTOR SHALL DESIGN AND PROVIDE IRRIGATION TO THESE AREAS. SEE SPECS. AND SHEETS L-210A AND L-210B FOR DESIGN BUILD IRRIGATION NOTES.
 4. CONTRACTOR SHALL PROTECT POROUS ASPHALT PAVEMENT FROM DEBRIS AND SEDIMENT. SEE SPECIFICATIONS.

CORRIDOR UNDERSTORY PLANT SCHEDULE
 SEE SHEET L-200 FOR THE PLANTING LEGEND

- WETLAND RESTORATION/ENHANCEMENT SHRUB MIX
- WETLAND RESTORATION GROUND COVER MIX
- BUFFER RESTORATION/ENHANCEMENT SHRUB MIX (SEE NOTE 3)
- BUFFER RESTORATION GROUND COVER MIX (SEE NOTE 3)
- DRAINAGE DITCH SEEDING MIX (SEE NOTE 3)
- INFILTRATION POND AND DRAINAGE DITCH MIX ZONE 1 (SEE NOTE 3)
- INFILTRATION POND MIX ZONE 2 (SEE NOTE 3)
- INFILTRATION MIX ZONE 3 (SEE NOTE 3)
- HYDROSEED LAWN (SEE NOTE 3)
- SOD LAWN (SEE NOTE 3)
- TRAIL EDGE SEDUM MIX (SEE NOTE 3)



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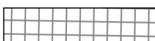
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CITY OF KIRKLAND
 TOTEM LAKE PEDESTRIAN BRIDGE
 CORRIDOR RESTORATION 4

SHEET: TLC - L - 204
 SCALE: AS SHOWN
 FILENAME: TLPB_200_LANDSCAPE.dwg

SURVEY NOTE:
 1) EXISTING CONDITIONS SHOWN ALONG TOTEM LAKE BLVD NE (CURB ALIGNMENT, SIDEWALK, STRIPING AND DRAINAGE STRUCTURES) ARE BASED ON PROPOSED IMPROVEMENTS SHOWN IN THE BID SET FOR THE CITY'S TOTEM LAKE GATEWAY PROJECT (CIP NO. STC006050).

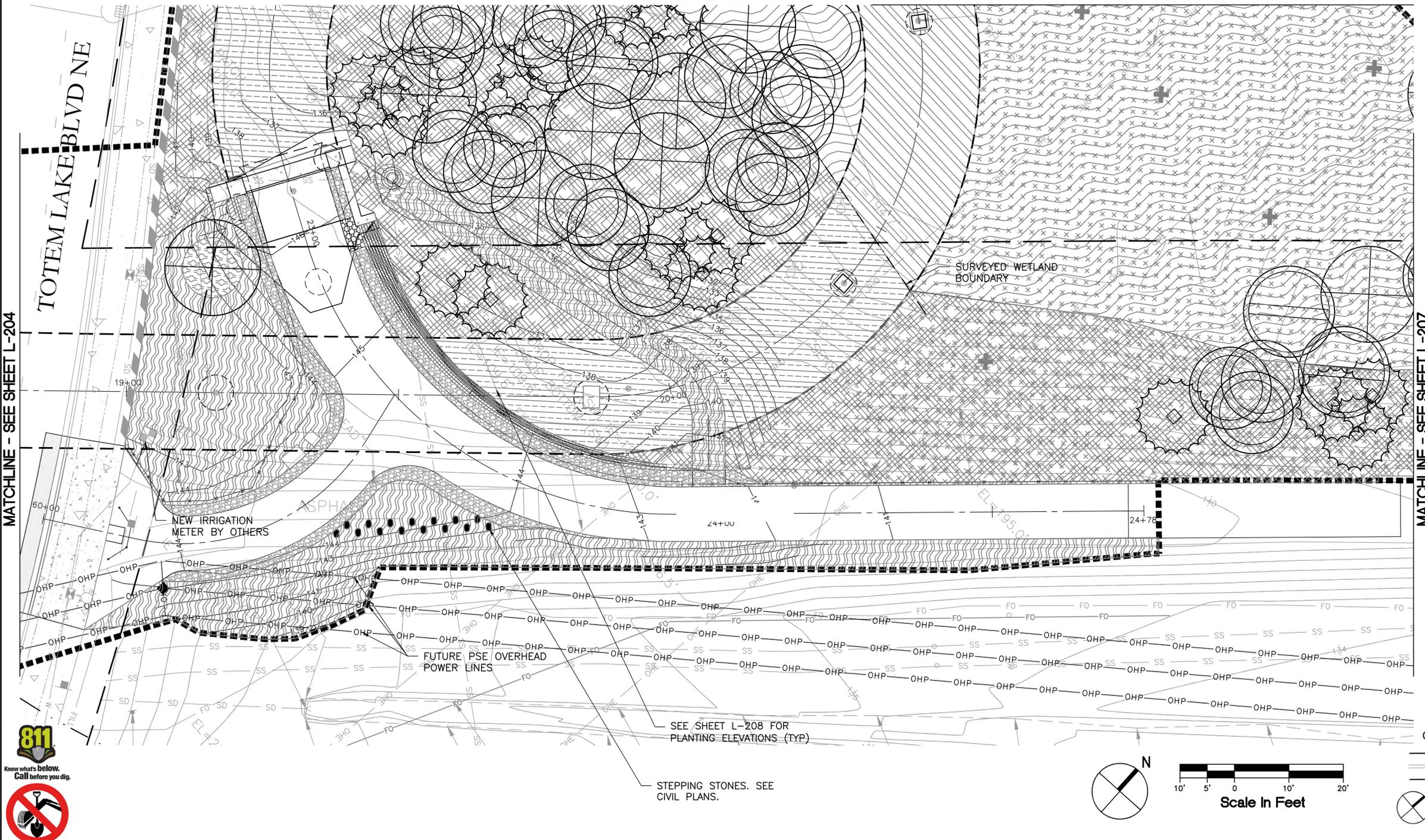
CORRIDOR UNDERSTORY PLANT SCHEDULE
 SEE SHEET L-200 FOR THE PLANTING LEGEND

-  WETLAND RESTORATION/ENHANCEMENT SHRUB MIX
-  WETLAND RESTORATION GROUND COVER MIX
-  BUFFER RESTORATION/ENHANCEMENT SHRUB MIX (SEE NOTE 3)
-  BUFFER RESTORATION GROUND COVER MIX (SEE NOTE 3)
-  DRAINAGE DITCH SEEDING MIX (SEE NOTE 3)
-  INFILTRATION POND AND DRAINAGE DITCH MIX ZONE 1 (SEE NOTE 3)
-  INFILTRATION POND MIX ZONE 2 (SEE NOTE 3)
-  INFILTRATION MIX ZONE 3 (SEE NOTE 3)
-  HYDROSEED LAWN (SEE NOTE 3)
-  SOD LAWN (SEE NOTE 3)
-  TRAIL EDGE SEDUM MIX (SEE NOTE 3)

- LEGEND**
-  PROPERTY LINE
 -  PROJECT LIMITS
 -  BRIDGE AND RAILING OVERHEAD, TYP
 -  RESTORED/ REPLACED SNAG
 -  MSE WALL, SEE SHEET L-208 FOR MSE WALL PLANTING LAYOUT
 -  ARBORIST WOOD CHIP MULCH

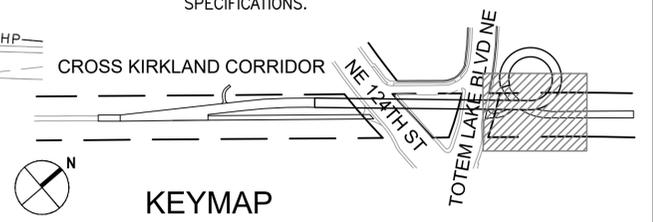
- NOTES**
1. INVASIVE PLANTS TO BE REMOVED TWICE FROM THE ENTIRE PROJECT SITE THROUGHOUT THE COURSE OF THE PROJECT:
 REMOVAL #1 SHALL OCCUR AT ONSET OF CONSTRUCTION, AND REMOVAL #2 PRIOR TO INSTALLING NEW PROJECT PLANTING.
 2. DISPERSE SALVAGED LARGE WOODY DEBRIS AND INSTALL SALVAGED SNAGS WITHIN THE WETLAND AND WETLAND BUFFER PRIOR TO PLANT INSTALLATION. COORDINATE LOCATIONS WITH THE ENGINEER.
 3. CONTRACTOR SHALL DESIGN AND PROVIDE IRRIGATION TO THESE AREAS. SEE SPECS. AND SHEETS L-210A AND L-210B FOR DESIGN BUILD IRRIGATION NOTES.
 4. CONTRACTOR SHALL PROTECT POROUS ASPHALT PAVEMENT FROM DEBRIS AND SEDIMENT. SEE SPECIFICATIONS.

MATCHLINE - SEE SHEET L-206



MATCHLINE - SEE SHEET L-204

MATCHLINE - SEE SHEET L-207



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CITY OF KIRKLAND
 TOTEM LAKE PEDESTRIAN BRIDGE
 CORRIDOR RESTORATION 5

SHEET: TLC - L - 205
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SURVEY NOTE:

1) EXISTING CONDITIONS SHOWN ALONG TOTEM LAKE BLVD NE (CURB ALIGNMENT, SIDEWALK, STRIPING AND DRAINAGE STRUCTURES) ARE BASED ON PROPOSED IMPROVEMENTS SHOWN IN THE BID SET FOR THE CITY'S TOTEM LAKE GATEWAY PROJECT (CIP NO. STC006050).

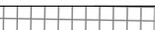
LEGEND

-  PROPERTY LINE
-  PROJECT LIMITS
-  BRIDGE AND RAILING OVERHEAD, TYP
-  RESTORED/ REPLACED SNAG
-  MSE WALL, SEE SHEET L-208 FOR MSE WALL PLANTING LAYOUT
-  ARBORIST WOOD CHIP MULCH

NOTES

1. INVASIVE PLANTS TO BE REMOVED TWICE FROM THE ENTIRE PROJECT SITE THROUGHOUT THE COURSE OF THE PROJECT:
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4. CONTRACTOR SHALL PROTECT POROUS ASPHALT PAVEMENT FROM DEBRIS AND SEDIMENT. SEE SPECIFICATIONS.

CORRIDOR UNDERSTORY PLANT SCHEDULE
SEE SHEET L-200 FOR THE PLANTING LEGEND

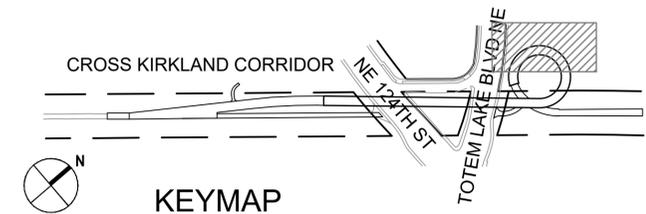
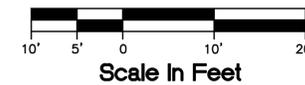
-  WETLAND RESTORATION/ENHANCEMENT SHRUB MIX
-  WETLAND RESTORATION GROUND COVER MIX
-  BUFFER RESTORATION/ENHANCEMENT SHRUB MIX (SEE NOTE 3)
-  BUFFER RESTORATION GROUND COVER MIX (SEE NOTE 3)
-  DRAINAGE DITCH SEEDING MIX (SEE NOTE 3)
-  INFILTRATION POND AND DRAINAGE DITCH MIX ZONE 1 (SEE NOTE 3)
-  INFILTRATION POND MIX ZONE 2 (SEE NOTE 3)
-  INFILTRATION MIX ZONE 3 (SEE NOTE 3)
-  HYDROSEED LAWN (SEE NOTE 3)
-  SOD LAWN (SEE NOTE 3)
-  TRAIL EDGE SEDUM MIX (SEE NOTE 3)

TOTEM LAKE BLVD NE

SURVEYED WETLAND BOUNDARY

MATCHLINE - SEE SHEET L-205

PROTECT COBBLES WHILE PLANTING AND REPLACE AS NEEDED.



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CITY OF KIRKLAND
TOTEM LAKE PEDESTRIAN BRIDGE
CORRIDOR RESTORATION 6

SHEET: TLC - L - 206
SCALE: AS SHOWN
FILENAME: TLPB_200_LANDSCAPE.dwg

SURVEY AND SHEET NOTES:

- 1) NO SURVEY NORTH OF TRAIL IMPROVEMENTS; ACTUAL LIMITS OF CLEARING AND GRUBBING TO BE COORDINATED WITH CITY. FOR PURPOSES OF BIDDING, THE AREAS SHOWN ARE INCLUDED IN THE 2.6 ACRES OF CLEARING AND GRUBBING.
- 2) NOTE SCALE CHANGE FROM 10-SCALE TO 40-SCALE NORTH OF MATCHLINE
- 3) KING COUNTY SEWER MAINS SHOWN ON THIS PLAN ARE BEYOND SURVEY LIMITS AND BASED ON GIS INFORMATION. LOCATION SHOWN IS APPROXIMATE. SEE NOTE 8.

NOTES

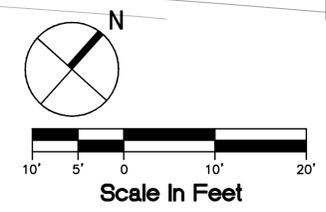
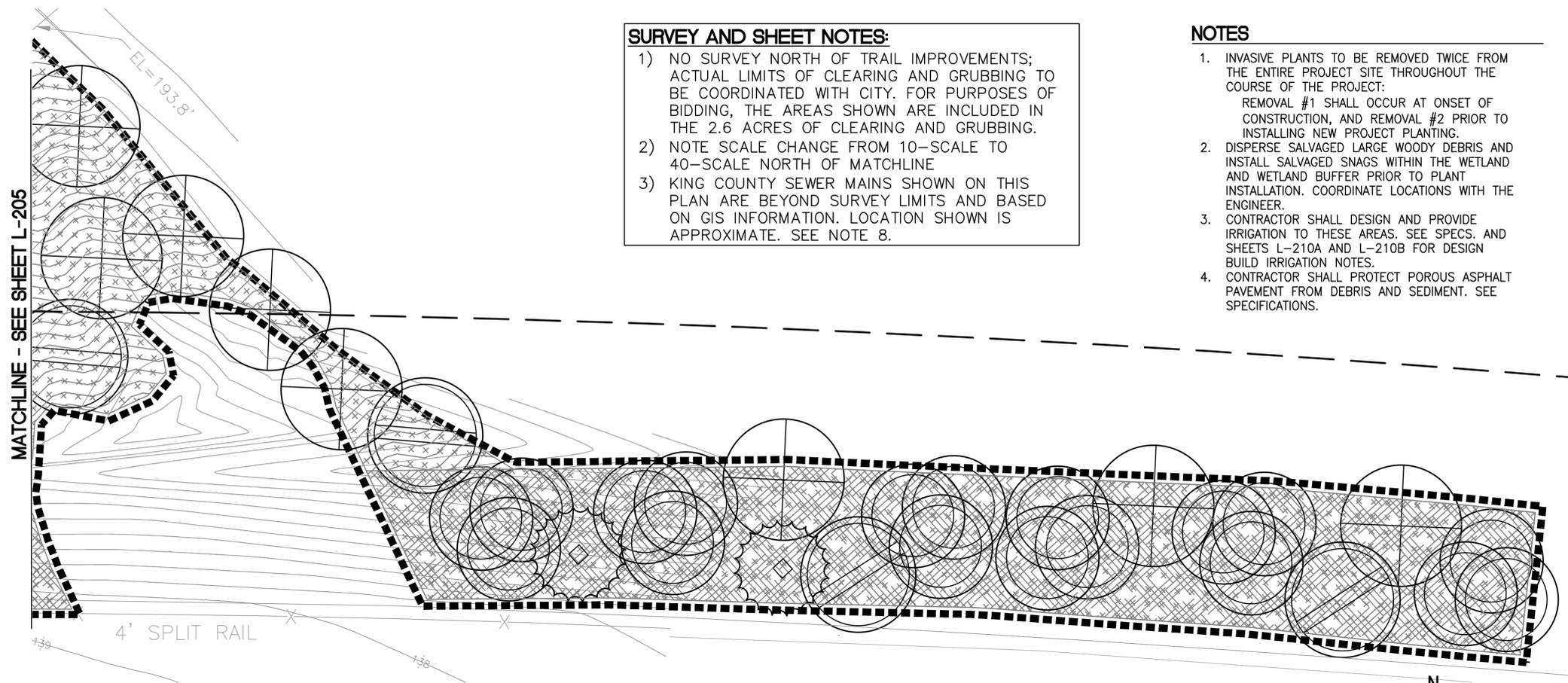
1. INVASIVE PLANTS TO BE REMOVED TWICE FROM THE ENTIRE PROJECT SITE THROUGHOUT THE COURSE OF THE PROJECT:
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4. CONTRACTOR SHALL PROTECT POROUS ASPHALT PAVEMENT FROM DEBRIS AND SEDIMENT. SEE SPECIFICATIONS.

LEGEND

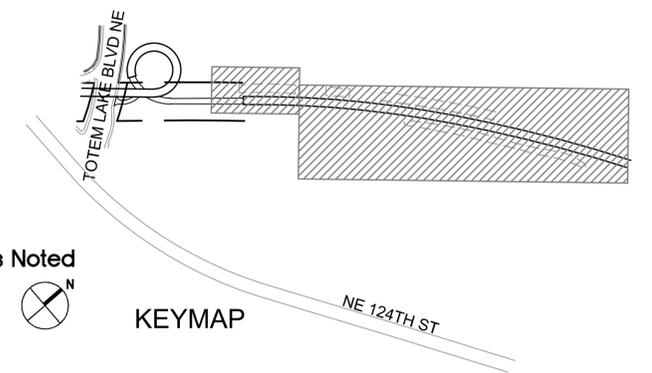
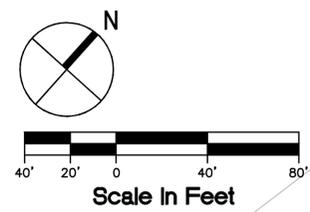
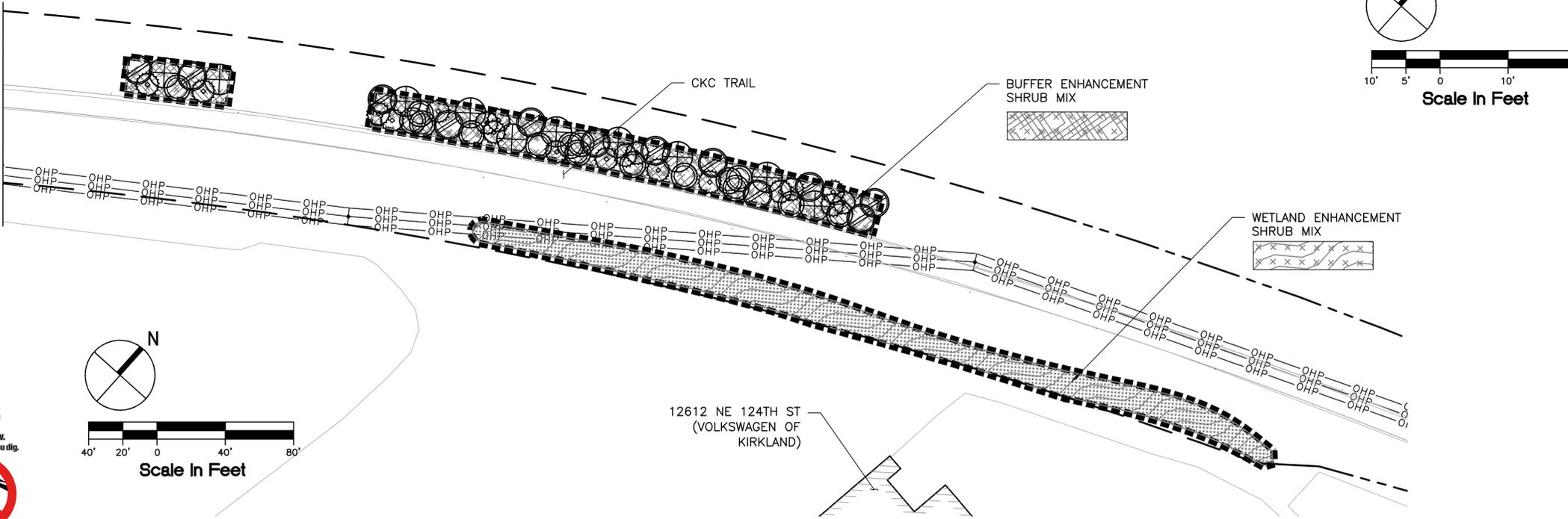
- PROPERTY LINE
- PROJECT LIMITS
- BRIDGE AND RAILING OVERHEAD, TYP
- RESTORED/ REPLACED SNAG
- MSE WALL, SEE SHEET L-208 FOR MSE WALL PLANTING LAYOUT
- ARBORIST WOOD CHIP MULCH

CORRIDOR UNDERSTORY PLANT SCHEDULE
SEE SHEET L-200 FOR THE PLANTING LEGEND

- WETLAND RESTORATION/ENHANCEMENT SHRUB MIX
- WETLAND RESTORATION GROUND COVER MIX
- BUFFER RESTORATION/ENHANCEMENT SHRUB MIX (SEE NOTE 3)
- BUFFER RESTORATION GROUND COVER MIX (SEE NOTE 3)
- DRAINAGE DITCH SEEDING MIX (SEE NOTE 3)
- INFILTRATION POND AND DRAINAGE DITCH MIX ZONE 1 (SEE NOTE 3)
- INFILTRATION POND MIX ZONE 2 (SEE NOTE 3)
- INFILTRATION MIX ZONE 3 (SEE NOTE 3)
- HYDROSEED LAWN (SEE NOTE 3)
- SOD LAWN (SEE NOTE 3)
- TRAIL EDGE SEDUM MIX (SEE NOTE 3)



MATCHLINE - SEE ABOVE
NOTE SCALE CHANGE



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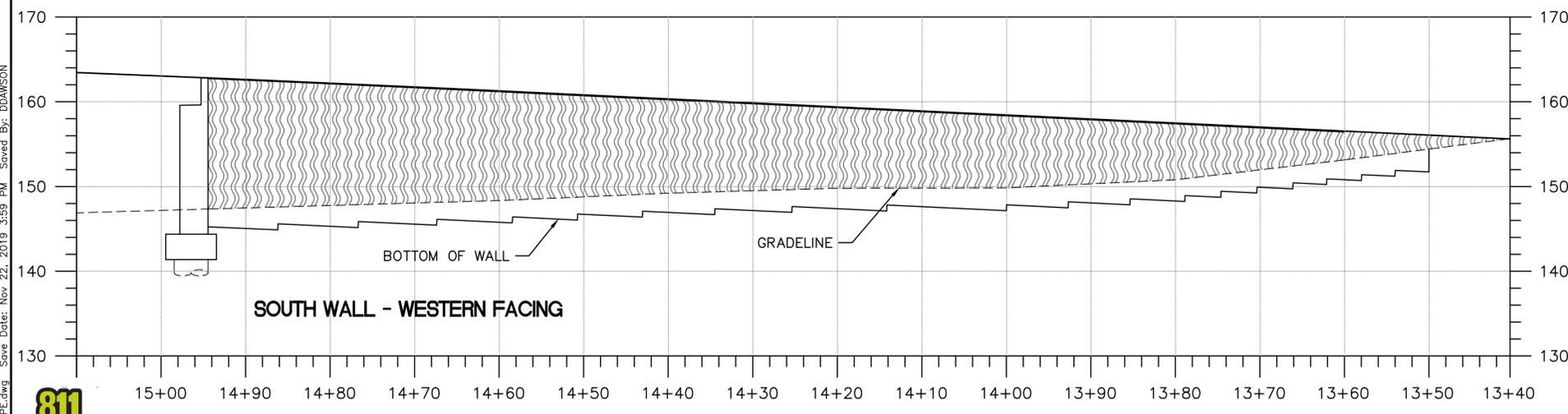
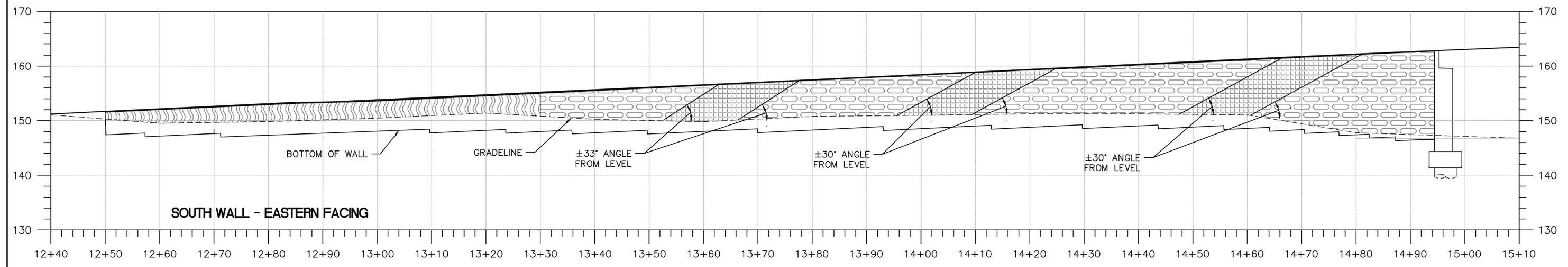
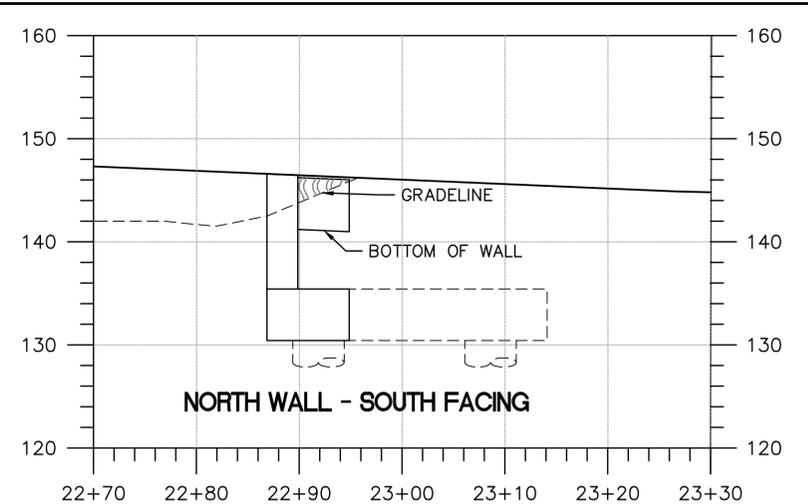
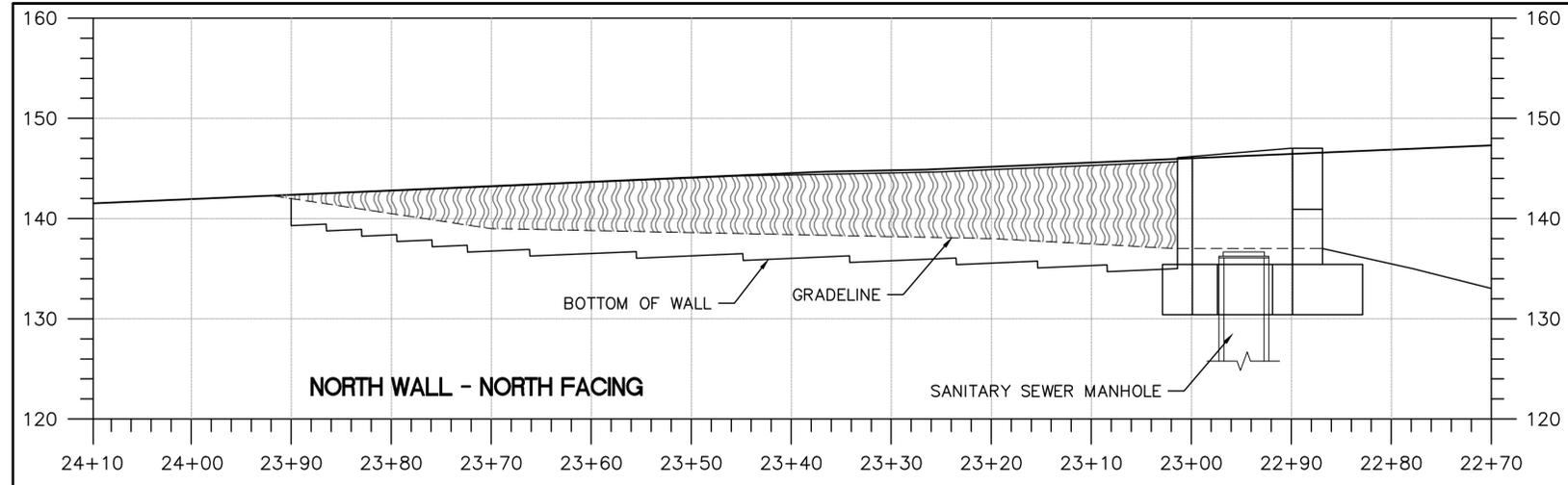


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CITY OF KIRKLAND
TOTEM LAKE PEDESTRIAN BRIDGE
CORRIDOR RESTORATION 7

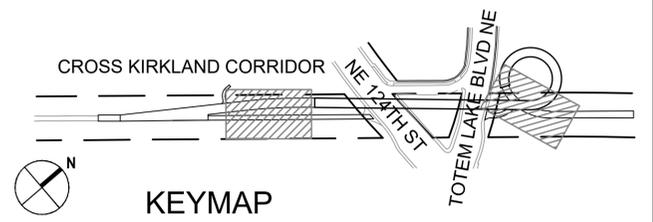
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SCALE: AS SHOWN
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- NOTES**
- IRRIGATION FOR WALLS SHALL CONFORM WITH SELECTED MSE WALL SYSTEM MANUFACTURER'S RECOMMENDATIONS. WHERE THE MSE WALL MANUFACTURER DOES NOT PROVIDE AN IRRIGATION RECOMMENDATION, THE DESIGN-BUILD IRRIGATION INSTALLER SHALL PROVIDE THE SYSTEM THAT BEST ENSURES THE PLANTS WILL ESTABLISH.

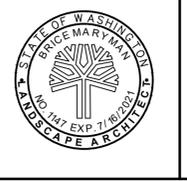
MSE WALL PLANT SCHEDULE DT=DROUGHT TOLERANT, NWN=NORTHWEST NATIVE, EG=EVERGREEN, S=FULL SUN TOLERANT, PS=PART SHADE TOLERANT

	BOTANICAL NAME	COMMON NAME	SIZE / SPACING	QTY.	DETAIL	EXPOSURE / NOTES	
DT	NWN	EG	POLYSTICHUM MUNITUM	WESTERN SWORD FERN	4" POTS/ 18" O.C.	485 PER MSE WALL MANUFACTURERS STANDARD	S, PS
DT	NWN	EG	OXAUS OREGANA	WOOD SORREL	4" POTS/ 12" O.C.	357 PER MSE WALL MANUFACTURERS STANDARD	
			SEEDED LAWN	SQUARE YARD		240	



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CITY OF KIRKLAND
TOTEM LAKE PEDESTRIAN BRIDGE
WALL PLANTING ELEVATION

SHEET: **TLC - L - 208**
SCALE: **AS SHOWN**
FILENAME: TLPB_200_LANDSCAPE.dwg

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IRRIGATION NOTES

- DESIGN BUILD: PROVIDE DESIGN, LAYOUT, MATERIALS AND INSTALLATION OF A FULLY AUTOMATIC IRRIGATION SYSTEM TO PROVIDE FIXED-SPRAY/DRIP SPRINKLER AND/OR DRIP IRRIGATION TO ALL PLANTING AREAS THROUGHOUT THE PROJECT AREA, INCLUDING MSE WALLS. INCLUDE COMMISSIONING, AS-BUILT IRRIGATION SYSTEM DRAWINGS, WARRANTY AND ONE-YEAR MAINTENANCE. CONTRACTOR SHALL OBTAIN ENGINEER'S APPROVAL OF DESIGN, MANUFACTURER AND MATERIALS PRIOR TO INSTALLATION.
- FURNISH AND INSTALL AUTOMATIC IRRIGATION SYSTEM IN CONFORMANCE WITH THE APPROVED DESIGN-BUILD DRAWINGS AND THESE NOTES, COMPLETE AND READY FOR USE. WORK CONSISTS OF FURNISHING ALL MATERIALS NECESSARY FOR A COMPLETE INSTALLATION, INCLUDING PIPE, PIPE SLEEVES, WIRE SLEEVES, FITTINGS, VALVES, VALVE BOXES, SWING JOINTS, SPRINKLER HEADS, TO TIE INTO THE EXISTING SYSTEM, AND ALL APPURTENANCES RELATED THERETO. INCLUDED SHALL BE ALL LABOR NECESSARY FOR INSTALLATION, INCLUDING TRENCHING, PLUMBING, BACK-FILLING, ELECTRICAL WORK, ADJUSTMENTS, TESTING, COMMISSIONING, AND ALL OTHER ITEMS OF LABOR NECESSARY FOR A SATISFACTORY OPERATING SYSTEM.
- THE CONTRACTOR MUST BE A CURRENT WASHINGTON STATE LANDSCAPE INDUSTRY PROFESSIONAL AND HAVE PASSED THE CERTIFIED LANDSCAPE PROFESSIONAL (CLP) EXAM WITH A MINIMUM OF 5 YEARS' EXPERIENCE. THE IRRIGATION SYSTEM MUST BE INSTALLED UNDER THE DIRECT SUPERVISION OF A CERTIFIED IRRIGATION TECHNICIAN OR JOURNEY PLUMBER WITH AT LEAST 5 YEARS EXPERIENCE. ALL POTABLE WATER SYSTEM COMPONENTS SHALL BE INSTALLED BY A WASHINGTON STATE LICENSED PLUMBER AND ALL TESTING OF THE BACKFLOW PREVENTION EQUIPMENT SHALL BE DONE BY A WASHINGTON STATE CERTIFIED BACKFLOW ASSEMBLY TESTER (BAT). CONFIRM SYSTEM IS TESTED WITHIN 60 DAYS PRIOR TO INSTALLATION.
- IRRIGATION FOR WALLS SHALL CONFORM WITH SELECTED MSE WALL SYSTEM MANUFACTURER'S RECOMMENDATIONS. WHERE THE MSE WALL MANUFACTURER DOES NOT PROVIDE AN IRRIGATION RECOMMENDATION, THE DESIGN-BUILD IRRIGATION INSTALLER SHALL PROVIDE THE SYSTEM THAT BEST ENSURES THE PLANTS WILL ESTABLISH.
- IRRIGATION ZONE CONTROL SHALL BE AUTOMATIC OPERATION WITH CONTROLLER AND AUTOMATIC CONTROL VALVES.

DELEGATED DESIGN: DESIGN 100 PERCENT COVERAGE IRRIGATION SYSTEM, INCLUDING CALCULATIONS USING PERFORMANCE REQUIREMENTS AND DESIGN CRITERIA INDICATED, BY A QUALIFIED CERTIFIED IRRIGATION DESIGNER OR LANDSCAPE ARCHITECT WITH DOCUMENTED 3 YEARS IRRIGATION DESIGN EXPERIENCE.

MINIMUM WORKING PRESSURES: THE FOLLOWING ARE MINIMUM PRESSURE REQUIREMENTS FOR PIPING, VALVES, AND SPECIALTIES UNLESS OTHERWISE INDICATED:
1. IRRIGATION MAIN PIPING: 200 PSIG (1380 KPA).
2. CIRCUIT PIPING: 150 PSIG (1035 KPA).
- SUBMIT THE FOLLOWING AT LEAST 21 WORKING DAYS PRIOR TO PERFORMING ANY WORK:
 - COMPLETE MATERIALS LIST, INCLUDING MANUFACTURER CATALOG DATA AND FULL DESCRIPTIVE LITERATURE, INCLUDING CURRENT MANUFACTURER'S PRICE LIST. SUBMITTAL SHALL BE IN ONE COMPLETE PACKAGE, EXCEPT AS-BUILT DRAWINGS.
 - INSTALLER QUALIFICATION DATA.
 - IRRIGATION DESIGNER QUALIFICATIONS.
 - IRRIGATION SYSTEM INSTALLATION SCHEDULE.
 - SLEEVING PLAN THAT COORDINATES WITH ROAD, PAVING AND UTILITY WORK.
 - IRRIGATION SYSTEM PLAN:
 - IRRIGATION PLAN WILL BE DRAWN AT EITHER 1"=20' OR 1"=10' ON 22"x34" SHEETS. DENOTE TREE, SHRUB, GROUNDCOVER AND GRASS AREAS SEPARATELY. PLANS SHALL SHOW LOCATIONS OF EXISTING, NEW, AND FUTURE UNDERGROUND UTILITIES. DESIGN IRRIGATION SYSTEM TO AVOID OR MITIGATE UTILITY CONFLICTS.
 - GROUP LIKE PLANT EXPOSURES TOGETHER IN ONE ZONE.
 - GROUP PLANTS WITH SIMILAR WATER NEEDS TOGETHER IN A ZONE.
 - EACH ZONE SHALL HAVE SIMILAR PRECIPITATION RATES AND SPRINKLERS/DRIP LINES WITHIN THE SAME FAMILY BY THE SAME MANUFACTURER.
 - CONTROLLER CAN BE SET TO RUN 2 ZONES AT ONCE PROVIDED WATER REQUIREMENTS ARE SIMILAR.

- ELEVATION DIFFERENCES WITHIN A ZONE SHALL NOT EXCEED 6 FEET.
- ENSURE COMPLETE COVERAGE AND ADEQUATE PRESSURE FOR ALL HEADS. INCLUDE CALCULATIONS TO ENSURE FURTHEST HEAD HAS ADEQUATE PRESSURE TO FUNCTION.
- INCLUDE LOCATION, HOUSING AND ELECTRICAL CONNECTION (AS APPLICABLE) FOR CONTROLLERS. ENSURE ADEQUATE POWER.
- INCLUDE POINT OF CONNECTION DRAWING DOCUMENTING PRESSURE AND BACK FLOW PREVENTION.
- IRRIGATION SYSTEM TYPICAL INSTALLATION DETAILS, TO INCLUDE MASTER VALVE AND FLOW SENSOR, GATE VALVE, REMOTE CONTROL VALVE, POP-UP ROTOR HEAD ASSEMBLY, POP-UP SPRAY HEAD ASSEMBLY, IRRIGATION PIPE TRENCH, AND OTHERS AS NECESSARY FOR PROPOSED SYSTEM. A CUSTOM DETAIL MAY BE REQUIRED TO RAISE AND PROTECT IRRIGATION SPRAY HEADS ALONG THE MSE WALL.
- APPROPRIATE BACKFLOW DEVICES AND/OR PRESSURE REDUCING VALVES SHALL BE INSTALLED ACCORDING TO THE LATEST VERSION OF THE UNIFORM PLUMBING CODE AS ADOPTED BY THE CITY OF KIRKLAND BUILDING DEPARTMENT.
- IRRIGATION SERVICE - THE CONTRACTOR SHALL UPSIZE AND CONNECT TO THE EXISTING WATER METER OR INSTALL A NEW WATER METER, AS APPROPRIATE, TO MEET THE DESIGN OF THE SYSTEM. THE SERVICE SHALL BE INSTALLED PER SPECIFICATIONS OUTLINED IN THE CURRENT EDITION OF THE PUBLIC WORKS DEPARTMENT'S PRE-APPROVED PLANS MANUAL. THE CONTRACTOR SHALL COORDINATE WITH THE CITY OF KIRKLAND PUBLIC WORKS DEPARTMENT FOR THE INSTALLATION OF THE WATER METER (FEES MAY APPLY).
- PIPE, TUBING AND FITTING - ALL PIPE AND TUBING SHALL BE 3/4" TO 1" SCHEDULE 40 PVC OR APPROVED EQUAL (ALL PIPING IN MEDIAN ISLANDS TO BE 1"). ALL PLASTIC FITTINGS SHALL BE SCHEDULE 80 PVC. ALL SLEEVING SHALL BE 2" SCHEDULE 40 PVC. 1" WATER SERVICE LINES SHALL USE 3/4" METER UNLESS OTHERWISE APPROVED. USE TEFLON TAPE ON ALL THREADED FITTINGS, REGARDLESS OF PIPE TYPE.
- POLYVINYL CHLORIDE PIPE, FITTINGS AND SLEEVING - SLEEVES REQUIRED FOR MAIN AND LATERAL LINES LOCATED UNDER PAVING SHALL BE SCHEDULE 80 PVC, WITH INNER DIAMETER OF SLEEVE EQUAL TO TWICE THE OUTSIDE DIAMETER OF INSERTED PIPE, AND MAXIMUM 1 INSERTED PIPE PER SLEEVE. ALL WIRING SHALL BE IN ITS OWN SEPARATE SCHEDULE 80 PVC SLEEVE, INDEPENDENT FROM THE PIPING SLEEVES. ALL PVC PIPE SHALL BE MARKED WITH THE MANUFACTURER'S NAME, CLASS OF PIPE AND NSF SEAL. IRRIGATION SLEEVES SHALL EXTEND A MINIMUM OF 2 FEET BEYOND THE LIMITS OF PAVEMENT.
- AUTOMATIC CONTROLLER 1, SOUTH OF NE 124TH ST. SHALL BE A BATTERY OPERATED NELSON MODEL 8030 WITH ADAPTER FOR WEATHERMATIC. IF USING A FIXED CONTROLLER WITH A DESIGNATED POWER SOURCE SUPPLY, A WEATHERMATIC SMARTLINE CONTROLLER SHALL BE USED. A SLW 10 WEATHER STATION SHOULD ALSO BE INSTALLED. CONTROLLER SHALL BE OPERABLE AND FULLY FUNCTIONING PRIOR TO FINAL INSPECTION.

AUTOMATIC CONTROLLER 2, NORTH OF TOTEM LAKE BLVD. NE SHALL BE A CALSENSE CS3000 CONTROLLER WITH WEATHER INTERFACE KIT CS3-W-KIT. CONTROLLER SHALL HAVE ENOUGH STATIONS TO MEET THE ZONES IN THE SYSTEM. MINIMUM CONTROLLER SIZE IS 8 STATIONS. USE CS38ST-KIT 8 STATION CARD PLUS TERMINAL AS REQUIRED. CONTROLLER SHALL INCLUDE ET GAGE, TIPPING RAIN BUCKET AND WIND GAGE. CONTROLLER SHALL INCLUDE A FLOW METER, SIZE BASED UPON DEMAND OF IRRIGATION SYSTEM PER MANUFACTURER'S RECOMMENDATIONS.
- SPRINKLER HEADS - POP-UP SPRAY HEADS, IMPACT HEADS AND BUBBLERS SHALL BE HUNTER OR APPROVED EQUAL. ALL HEADS SHALL HAVE BUILT-IN PRESSURE REGULATING DEVICE, AND MATCHED PRECIPITATION RATE NOZZLES WITH ADJUSTING SCREWS. IF INSTALLED ALONG CURBING, HEADS SHALL BE FLUSH AND ADJACENT TO TOP OF CURB. NOZZLES SHALL BE MP ROTATORS, OR APPROVED EQUAL.
- VALVE BOXES AND PROTECTIVE SLEEVES - VALVE BOXES FOR CONTROL VALVES SHALL BE CARSON 1419 BOX, WITH LOCKING LID OR APPROVED EQUAL. VALVE BOXES FOR DOUBLE CHECK VALVE ASSEMBLY SHALL BE CARSON 1324 WITH LOCKING LID, OR APPROVED EQUAL.

- AUTOMATIC CONTROL VALVES - AUTOMATIC CONTROL VALVES SHALL BE WEATHERMATIC 2100 SERIES. A MANUAL CONTROL SHALL BE PROVIDED FOR OPERATION WITH OR WITHOUT THE CONTROL WIRING INSTALLED.
- WIRING FOR AUTOMATIC CONTROL VALVES AND MASTER VALVES - AUTOMATIC CONTROL VALVE WIRES SHALL BE INSULATED SINGLE STRAND COPPER DESIGNED FOR 20 TO 50 VOLTS AND UL APPROVED AS TYPE U.F. (UNDERGROUND FEEDER). THE UL AND U.F. DESIGNATIONS SHALL BE CLEARLY MARKED OR INDENTED ON THE INSULATION JACKET OF THE WIRE.

EXPANSION CURLS SHALL BE PROVIDED WITHIN 3 FEET OF EACH WIRE CONNECTION TO SOLENOID AND AT LEAST EVERY 300 FEET IN LENGTH OF CONTROL WIRE.

COLOR CODED WIRES SHALL BE USED FOR ALL CONTROL WIRES AND THE FOLLOWING COLORS ARE THE ONLY COLORS THAT WILL BE ACCEPTED:
POWER (OR LEAD-IN) - BLACK OR RED
COMMON (OR NEUTRAL) - WHITE
SPARES (SEE BELOW) - ORANGE

ONE UNCONNECTED SPARE WIRE (COLOR ORANGE) FOR EACH 4 ZONES SHALL BE RUN FROM THE CONTROLLER THROUGH EACH INTERMEDIATE CONTROL VALVE BOX. PROVIDE AT LEAST A 24-INCH LONG, TIGHT LOOP FOR EACH WIRE IN EACH BOX. FOR CLARIFICATION; ZONES 1 THRU 4 REQUIRE ONE SPARE WIRE, ZONES 5 THRU 8 REQUIRE AN ADDITIONAL SPARE WIRE, AND SO ON. WHERE CONTROL VALVES RUN IN MULTIPLE DIRECTIONS FROM THE CONTROLLER, AT LEAST 1 SEPARATE SPARE WIRE SHALL BE RUN IN EACH DIRECTION. THE SPARE WIRE(S) SHALL ALWAYS BE INSTALLED TO THE FARTHEST VALVE(S) LOCATION(S) FROM THE CONTROLLER.

THE MASTER VALVE SHALL BE INSTALLED WITH SEPARATE POWER AND COMMON WIRES WHICH SHALL BE DIFFERENT COLORS THAN THE OTHER CONTROL WIRES. A SPARE WIRE SHALL ALSO BE INSTALLED TO THE MASTER VALVE. THE FOLLOWING WIRE COLORS ARE THE ONLY COLORS THAT WILL BE ACCEPTED:
POWER - BLUE
COMMON - YELLOW
SPARE - ORANGE
- QUICK COUPLING EQUIPMENT - QUICK COUPLING EQUIPMENT SHALL BE RAINBIRD #44DLRC OR APPROVED EQUAL. PROVIDE 4 OPERATING KEYS. QUICK COUPLER VALVES SHALL BE LOCATED AS SHOWN ON THE CONTRACT DOCUMENTS OR AT APPROXIMATELY 200 FOOT INTERVALS, UNLESS OTHERWISE DIRECTED BY THE ENGINEER THROUGH THE DESIGN REVIEW PROCESS. ONE ADDITIONAL QUICK COUPLING VALVE SHALL BE INSTALLED AT THE POINT OF CONNECTION (POC) FOR USE OF COMPRESSED AIR FOR WINTERIZING THE SYSTEM. ALL QUICK COUPLING VALVES SHALL BE INSTALLED IN A 10-INCH DIAMETER VALVE BOX.
- MANUAL VALVES - GATE VALVES (GV): (2-INCH O.D. AND SMALLER) SHALL BE FLANGED, IRON BODY, BRASS TRIMMED, RESILIENT DOUBLE DISC WEDGE, AND INTEGRAL TAPER SEATS WITH NON-RISING STEM AND SQUARE ACTUATOR. ALL GATE VALVES SHALL BE CLASS 150 WITH A MINIMUM 150 PSI - 300 WOG. GATE VALVES SHALL BE KENNEDY, MUELLER, OR HAMMOND, OR APPROVED EQUAL.

LATERAL (SUB-MAIN) ISOLATION VALVES (LIV): (2-INCH O.D. THROUGH 3-INCH O.D.) SHALL BE 200 PSI RATED ANGLE GLOBE VALVES WITH INTEGRALLY RESTRAINED ENDS. ENDS SHALL BE MALE SWIVEL OR INTEGRALLY RESTRAINED PUSH ON GASKETED JOINT AS REQUIRED. COMPONENTS SHALL BE DUCTILE IRON, 316 STAINLESS STEEL, AND LOW ZINC BRONZE COMPLYING WITH ASTM A536, ASTM A276, ASTM B62-C83600 RESPECTIVELY OR EQUIVALENT. DUCTILE IRON SHALL BE FUSION BOND EPOXY COATED. GASKETED JOINTS SHALL BE SBR RUBBER AND COMPLY WITH ASTM F477. INTERNAL SEALS SHALL BE EPDM RUBBER. LATERAL (SUB-MAIN) ISOLATION VALVES SHALL BE HARCO, OR APPROVED EQUAL.

MANUAL DRAIN (STOP & WASTE) VALVES: SHALL BE ALL BRONZE CONSTRUCTION, 175 PSI WATER WORKING PRESSURE, MUELLER ORISEAL MARK II, OR EQUAL. DRAIN VALVES (SPRAY SYSTEM) SHALL BE MUELLER MARK II ORISEAL H-10284, STOP AND WASTE, OR APPROVED EQUAL.
- SWING JOINTS - PRE-FABRICATED SWING JOINTS, FOR IRRIGATION HEADS, SHALL BE TRIPLE SWING JOINTS, USING SCHEDULE 80 OR

- CLASS 315 PVC THREADED FITTINGS. SWING JOINTS SHALL CONSIST OF STREET ELLS, ELLS, AND NIPPLES FOR FULL ADJUSTABILITY. FITTINGS SHALL HAVE "O" RING SEALS. ALL PVC PIPES AND FITTINGS FOR SWING JOINTS SHALL CONFORM TO ALL REQUIREMENTS OF ASTM D3139. NO ADDITIONAL THREADED PVC NIPPLES OF ANY KIND ARE ALLOWED IN SWING JOINT FABRICATIONS. ALL SWING JOINTS SHALL BE THREADED INTO SCHEDULE 80 PVC TEES OR SCHEDULE 80 PVC 90-DEGREE ELLS THAT ARE GLUED INTO THE SCHEDULE 40 MAINLINE AND LATERAL PIPES.
- BACKFILL MATERIALS - BACKFILL AROUND ALL IRRIGATION HEADS SHALL BE PER PLANS. FOR SUITABLE BEDDING MATERIAL AROUND ALL PIPES AND EQUIPMENT AS SHOWN ON THE DETAILS, USE NATIVE TOPSOIL WITH NO ROCKS OR OTHER DEBRIS MORE THAN 1 INCH DIAMETER, OR COMMON BUILDER'S SAND.
 - WORK AROUND EXISTING TREES - EXERCISE CARE WHEN EXCAVATING TRENCHES NEAR EXISTING TREES PER CONTRACT PLANS. ARBORIST REVIEW REQUIRED. WHERE ROOTS ARE 1-1/2 INCHES AND GREATER IN DIAMETER HAND EXCAVATE AND/OR AIR SPADE AND TUNNEL. WHEN LARGE ROOTS ARE EXPOSED, WRAP WITH HEAVY BURLAP FOR PROTECTION AND PREVENT EXCESSIVE DRYING. TRENCHES DUG BY MACHINES ADJACENT TO TREES HAVING ROOTS 2 INCHES AND LESS IN DIAMETER SHALL HAVE THE SIDES HAND TRIMMED MAKING A CLEAN CUT OF THE ROOTS. TRENCHES HAVING EXPOSED TREE ROOTS SHALL BE BACK-FILLED WITHIN 24 HOURS UNLESS ADEQUATELY PROTECTED WITH MOIST BURLAP OR CANVAS. MAINTAIN MOISTURE ON PROTECTIVE CLOTH UNTIL BACKFILLING.
 - TRENCHING - THE TOP 6 INCHES OF SOIL SHALL BE KEPT SEPARATE FROM SUBSOIL AND SHALL BE REPLACED AS THE TOP LAYER WHEN BACKFILLING IS CONDUCTED. TRENCHES SHALL BE EXCAVATED FOR ALL PIPE TO PROVIDE THE MINIMUM DEPTH OF COVER BELOW FINISH GRADE OF 18 INCHES MINIMUM FOR LIVE LINES (MAINS), AND 14 INCHES MINIMUM FOR LATERALS AND ALL OTHERS, NO WIDER AT ANY POINT THAN IS NECESSARY TO LAY THE PIPE OR INSTALL EQUIPMENT. TRENCHES SHALL BE EXCAVATED WITH VERTICAL SIDES. LOCATE OUTSIDE OF PAVED AREAS WHEREVER POSSIBLE. ALL PVC OR POLYETHYLENE PIPE INSTALLED UNDER AREAS TO BE PAVED SHALL BE PLACED IN IRRIGATION SLEEVES. TRENCH BOTTOM MUST BE FREE OF ROCKS OR SHARP-EDGED OBJECTS. MATERIALS UNSUITABLE FOR BEDDING OF PIPE TO BE REMOVED TO A DEPTH 4 INCHES BELOW TRENCH BOTTOM, AND REPLACED WITH SUITABLE BEDDING MATERIAL AS DIRECTED BY THE ENGINEER. SUITABLE BEDDING MATERIAL SHALL BE: EXCAVATED TRENCH MATERIAL, FREE FROM ROCKS, ROOTS, STICKS, DEBRIS OR OTHER SHARP OBJECTS OVER ONE INCH IN DIAMETER; OR SAND, AS REQUIRED. ALL TRENCHES MUST BE STRAIGHT, WITH APPROPRIATE PIPE FITTINGS USED TO ALLOW PIPE TO BE LAID WITHOUT UNDUE BENDING AND NOT HAVE ABRUPT CHANGES IN GRADE. THE USE OF AN UNDERGROUND VIBRATORY PLOW OR SIMILAR DEVICE TO PULL PIPE WILL NOT BE PERMITTED. PIPELINES SHOWN PARALLEL ON DRAWING MAY BE PLACED IN A COMMON TRENCH BUT SEPARATED BY AT LEAST 6 INCHES.
 - INSTALL PIPE, VALVES, HEADS, WIRING AND OTHER SYSTEM COMPONENTS PER APPROVED IRRIGATION SYSTEM PLAN AND DETAILS SUBMITTED PER THESE REQUIREMENTS, AND IN CONFORMANCE WITH APPLICABLE CODE.
 - BACKFILLING - BACKFILLING SHALL BE DONE WHEN PIPE IS NOT IN AN EXPANDED CONDITION DUE TO HEAT OR PRESSURE. WHILE BACKFILLING THE TRENCHES, FILL AND TAMP 4 INCHES BELOW AND 6 INCHES ABOVE THE PIPE AND FITTINGS WITH APPROVED SUITABLE BEDDING MATERIAL OR SAND AS REQUIRED. THE REMAINDER OF THE BACKFILL SHALL CONTAIN NO LUMPS OR ROCKS LARGER THAN THREE INCHES. IF NO SODDING IS REQUIRED, THE TOP 6 INCHES OF BACKFILL SHALL BE REPLACED BY TOPSOIL WHERE IT EXISTS (FREE OF ROCKS OVER ONE INCH, SUBSOIL OR TRASH) OR SELECTED FILL SOIL OR SAND IF SOIL CONDITIONS ARE ROCKY. ALL ROOTS, ROCKS

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CITY OF KIRKLAND
DEPARTMENT OF PUBLIC WORKS
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CITY OF KIRKLAND
TOTEM LAKE PEDESTRIAN BRIDGE

IRRIGATION NOTES 1

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IRRIGATION NOTES, CONTINUED

AND SURPLUS EXCAVATION SHALL BE REMOVED FROM THE SITE UNLESS OTHERWISE DIRECTED.

TRENCHES UNDER ROADS OR PAVED AREAS SHALL BE BACK-FILLED AND TAMPED WITH A MECHANICAL TAMPER IN SUCCESSIVE 6 INCH LIFTS. REPLACE PAVING TO THE SATISFACTION OF THE ENGINEER.

PRIOR TO COMPLETING BACKFILL, PLACE DETECTION TAPE 6 INCHES BELOW FINISHED GRADES AND DIRECTLY ABOVE THE INSTALLED LATERAL AND SUPPLY MAINS FOR FUTURE LINE DETECTION. PROVIDE EXTRA LENGTH TO CLEARLY EXPOSE ENDS IN THE VALVE BOXES. BEFORE COMPLETE BACK-FILLING, ALL UNDERGROUND APPURTENANCES INCLUDING RISERS, VALVES, DCVA, DRAIN VALVES, AND JOINTS MUST REMAIN EXPOSED SO THAT THEY CAN BE VIEWED DURING TESTING. IF, FOR ANY REASON, ANY PART OF THE SPRINKLER SYSTEM IS BACK-FILLED BEFORE APPROVED LOCATION, TESTING, OR INSPECTION IS AUTHORIZED, IT MUST BE COMPLETELY UNCOVERED AND EXPOSED UNTIL APPROVED FOR BACK-FILLING BY THE ENGINEER.

25. CLEAN-UP – UPON COMPLETION OF OPERATIONS AND PRIOR TO WATERING, CLEAN ALL ADJOINING AREAS SUCH AS PAVING, CURBS, AND LAWNS OF DEBRIS CAUSED BY THE WORK ON THIS PROJECT, OR ANY PART OF THIS PROJECT. ALL POROUS PAVEMENT SHALL BE VACUUMED CLEAN, AND ALL OTHER HARD SURFACED AREAS SHALL BE WASHED CLEAN. DAILY CLEAN UP SHALL BE REQUIRED ON ALL AREAS USED FOR CIRCULATION, PARKING, OR OTHER DAILY USE.

26. PRESSURE TESTING – PERFORM HYDROSTATIC TESTS ON SYSTEM AND DOCUMENT WITH PHOTOGRAPHS UNLESS THE ENGINEER IS AVAILABLE TO WITNESS THE TESTING. FURNISH NECESSARY PUMP, GAUGES AND ALL OTHER TEST EQUIPMENT.

ALL PVC MAIN LINES WITH GATE (ISOLATION) VALVES INSTALLED AND CLOSED SHALL BE FLUSHED AND PRESSURE TESTED WITH ALL JOINTS EXPOSED TO 150 PSI UNTIL WATERTIGHT. MAXIMUM PSI LOSS IN A 30 MINUTE TEST PERIOD SHALL BE 5 PSI. ALL PVC LATERAL LINES WITH RISERS INSTALLED AND CAPPED SHALL BE FLUSHED AND PRESSURE TESTED AT 60 PSI WITH ALL JOINTS EXPOSED TO SERVICE LINE PRESSURE REQUIRED IN DESIGN FOR 30 MINUTES. IF THE SYSTEM REQUIRES A PUMP, THE SERVICE LINE PRESSURE TEST IS REQUIRED WITH THE PUMP IN OPERATION. MAXIMUM PSI LOSS ALLOWED SHALL BE 5 PSI.

ALL GAUGES USED IN THE TESTING OF WATER PRESSURES SHALL BE CERTIFIED CORRECT BY AN INDEPENDENT TESTING LABORATORY IMMEDIATELY PRIOR TO USE ON THE PROJECT. AN INSPECTION REPORT SHALL BE MADE AND SUBMITTED AFTER THE COMPLETION OF TESTING.

27. RECORD DOCUMENTS – THE CONTRACTOR SHALL KEEP A CURRENT WORKING SET OF DRAWINGS WITH ALL "AS-BUILT" CHANGES OR ACTUAL CONDITIONS AS WORK PROCEEDS, TO BE SUBMITTED TO ENGINEER WITHIN 7 DAYS OF COMPLETION OF IRRIGATION SYSTEM INSTALLATION. ENGINEER MAY REQUEST TO REVIEW RECORDS DAILY. THE "AS-BUILT" PLANS SHALL BE REVIEWED AND ALL FEATURES EXPLAINED. THE "AS-BUILT" PLANS SHALL CONSIST OF RED-LINED CORRECTIONS, NOTES, COMMENTS, ETC. ON A CLEAN BOND PAPER COPY. ANY MAJOR DEVIATIONS FROM THE ORIGINAL DESIGN (AS PREVIOUSLY APPROVED) SHALL BE DOCUMENTED ON THE "AS-BUILTS" AND EXPLAINED AT THE SESSION. ALL CRITICAL DIMENSIONS SHALL BE SHOWN. THE ENGINEER SHALL REVIEW AND APPROVE THE "AS-BUILT" PLANS SUBMITTED BY THE CONTRACTOR, IF ENGINEER FINDS CONTRACTOR'S RECORDS TO BE INCOMPLETE THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING REQUIRED AS BUILT DOCUMENTATION.

SUBMIT DOCUMENTATION OF RESULTS FROM SYSTEM PRESSURE TESTING.

SUBMIT RECORD DOCUMENTS, INCLUDING AS-BUILT DRAWINGS, PRODUCT INFORMATION, CONTROLLER CHART, WATERING SCHEDULE AND OPERATION AND MAINTENANCE GUIDELINES AFTER SYSTEM HAS BEEN INSTALLED, TESTED AND APPROVED BY ENGINEER.

IRRIGATION SYSTEM COMMISSIONING AND WARRANTY

1. COVERAGE TEST – BEFORE THE IRRIGATION SYSTEM WILL BE ACCEPTED, THE CONTRACTOR, IN THE PRESENCE OF THE ENGINEER, SHALL PERFORM A WATER COVERAGE TEST FOR EACH ZONE OF THE SYSTEM. TEST SHALL BE PERFORMED UNDER AUTOMATIC OPERATION. NOTIFY THE ENGINEER AT LEAST 5 WORKING DAYS IN ADVANCE OF COVERAGE TEST. NEVER PERFORM THIS OR ANY OTHER TESTING OF THE IRRIGATION

SYSTEM EQUIPMENT USING POWER SUPPLIED BY A PORTABLE GENERATOR. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CHANGING NOZZLES, ETC., AT THE DISCRETION OF ENGINEER, IN ORDER TO OBTAIN FULL COVERAGE WITH MINIMUM OVERSPRAY. THE CONTRACTOR WILL BE REQUIRED TO ADJUST AND/OR REPLACE NOZZLES, ETC. AS NECESSARY TO MEET THIS REQUIREMENT. PRIOR TO ARRIVAL OF THE ENGINEER, THE CONTRACTOR SHALL ACCOMPLISH THE FOLLOWING: COMPLETE ALL WORK INCLUDING BALANCING, ADJUSTING THE SYSTEM (PRESSURE REDUCING VALVES, FLOW ADJUSTMENT KEYS, NOZZLES, ETC.) TO PROVIDE OPTIMUM COVERAGE WITHOUT FOGGING.

- 2. FUNCTIONALITY TEST – A FUNCTIONALITY TEST OF THE CONTROL/ COMMUNICATION SYSTEMS SHALL BE PERFORMED AND DEMONSTRATE THAT ALL PARTS OF THE SYSTEM FUNCTION AS SPECIFIED OR INTENDED. THE FUNCTIONALITY TEST FOR EACH NEW SYSTEM SHALL CONSIST OF NOT LESS THAN 30 CALENDAR DAYS OF CONTINUOUS, SATISFACTORY OPERATION. ANY MATERIALS DETERMINED TO BE FAULTY AS PART OF THE INSTALLATION SHALL BE REPLACED OR CORRECTED BY THE CONTRACTOR AT THEIR EXPENSE IN A MANNER RESPECTIVE TO THE CONTRACT DOCUMENTS. IN THE EVENT OF A SYSTEM FAILURE DUE TO FAULTY INSTALLATION OR WORKMANSHIP, THE 30 DAY PERIOD WILL BE REPEATED UNTIL TESTING IS COMPLETE.
- 3. SYSTEM COMMISSIONING, OPERATION AND TRAINING – A TRAINING AND ORIENTATION SESSION IS REQUIRED. THE CONTRACTOR, THE IRRIGATION SUB-CONTRACTOR, THE ENGINEER, AND OTHER STAFF DEEMED NECESSARY BY OWNER SHALL BE PRESENT. THE DATE AND TIME OF THE SESSION SHALL BE SUBJECT TO THE APPROVAL OF THE ENGINEER.

THE CONTRACTOR SHALL PROVIDE THE ENGINEER WITH THE NECESSARY KEYS AND/OR OTHER TOOLS NECESSARY TO OPERATE/DRAIN/ACTIVATE THE SYSTEM AND SPEND SUFFICIENT TIME WITH THE ENGINEER TO ENSURE THAT THE SYSTEM OPERATION/MAINTENANCE/WINTERIZING CAN CONTINUE AFTER DEPARTURE OF THE CONTRACTOR. THE CONTRACTOR SHALL PROVIDE 2 CONTROLLER REMOTES TO THE ENGINEER AT THIS STAGE.

FOLLOWING DE-WINTERIZATION, CONTRACTOR TO MEET WITH THE OWNER'S MAINTENANCE REPRESENTATIVE WITH THE AS-BUILT AND CONTROLLER CHART AND WALK THE ENTIRE SIGHT REVIEWING OPERATIONS AND ANSWERING QUESTIONS.

- 4. A COMPLETE OPERATIONS AND MAINTENANCE (O&M) MANUAL SHALL BE PREPARED BY THE CONTRACTOR AND 2 COPIES OF THE MANUAL SHALL BE TURNED OVER TO THE OWNER AT THE FINAL INSPECTION. THE MANUALS SHALL CONSIST OF THREE RING BINDERS CONTAINING: (A) CATALOGS OF ALL MATERIALS USED; (B) COMPLETE PARTS LIST OF ALL MATERIALS; (C) A WRITTEN SUMMARY OF ALL OPERATIONS DATA INCLUDING SPRING START-UP AND WINTERIZATION TECHNIQUES, CONTROLLER PROGRAMMING, VALVE CLEANING, IRRIGATION ADJUSTMENTS, BACKFLOW PREVENTER OPERATION AND ANY OTHER INFORMATION REQUIRED TO OPERATE AND MAINTAIN SYSTEM; (D) TWO LOCAL DISTRIBUTORS; (E) THE CONTROLLER STATION TIMING SHALL BE SUBMITTED TO ENGINEER AND SET BY THE CONTRACTOR. SMALLER ZONES WITH SIMILAR CONDITIONS CAN BE OPERATED SIMULTANEOUSLY.

PROVIDE CONTROLLER CHARTS AS FOLLOWS – THE RECORD DOCUMENTS SHALL BE APPROVED BY THE ENGINEER BEFORE CHARTS ARE PREPARED. THE CHART SHALL BE A REDUCTION OF THE ACTUAL RECORD DOCUMENTS PREPARED BY THE CONTRACTOR. PROVIDE ONE CONTROLLER CHART PER CONTROLLER. CONTROLLER CHART SHALL FIT IN BEHIND CONTROLLER DOOR. THE CHART SHALL BE A BOND COPY PRINT WITH A DIFFERENT PASTEL TRANSPARENT COLOR TO SHOW EACH SEPARATE ZONE. VERIFY THAT THE ZONE NUMBER AS SHOWN ON THE CONTROLLER CHART MATCHES THE NUMBER ON THE ACTUAL CONTROL VALVE IDENTIFICATION TAG. NUMBERING OF ZONES SHALL BE PER APPROVED PLANS. WHEN COMPLETED AND APPROVED, HERMETICALLY SEAL (LAMINATE) THE CHART BETWEEN TWO PIECES OF PLASTIC. THE CHARTS SHALL BE COMPLETED BEFORE THE PROJECT CAN BE CONSIDERED PHYSICALLY COMPLETE.

- 5. IRRIGATION SYSTEM WARRANTY AND WINTERIZATION – THE CONTRACTOR SHALL PROVIDE A ONE YEAR IRRIGATION WARRANTY

AND BE RESPONSIBLE FOR ALL WORK SHOWING DEFECTS IN MATERIALS OR WORKMANSHIP, FOR A PERIOD OF ONE YEAR FROM DATE OF PHYSICAL COMPLETION OR ONE YEAR FROM DATE OF DE-WINTERIZATION, WHICHEVER IS LATEST. THE WARRANTY SHALL INCLUDE TRENCH RESTORATION OF PLANTED OR PAVED AREAS DUE TO SETTLEMENT AND RESETTING VALVE BOXES. RESTORATION SHALL OCCUR WITHIN TWO WEEKS OF NOTIFICATION. A FINAL SITE MEETING SCHEDULED BY THE CONTRACTOR WITH THE ENGINEER SHALL TAKE PLACE 11 MONTHS AFTER THE DATE OF SUBSTANTIAL COMPLETION OR DE-WINTERIZATION, WHICHEVER IS LATER. THE SYSTEM SHALL BE EXAMINED BY THE ENGINEER TO DETERMINE IF THE SYSTEM REQUIRES ALTERATIONS OR REPLACEMENTS.

THE WARRANTY SHALL INCLUDE ONE COMPLETE WINTERIZATION AND ONE COMPLETE DE-WINTERIZATION OF THE IRRIGATION SYSTEM IN THE PRESENCE OF THE ENGINEER, OR OTHER DESIGNATED CITY STAFF.

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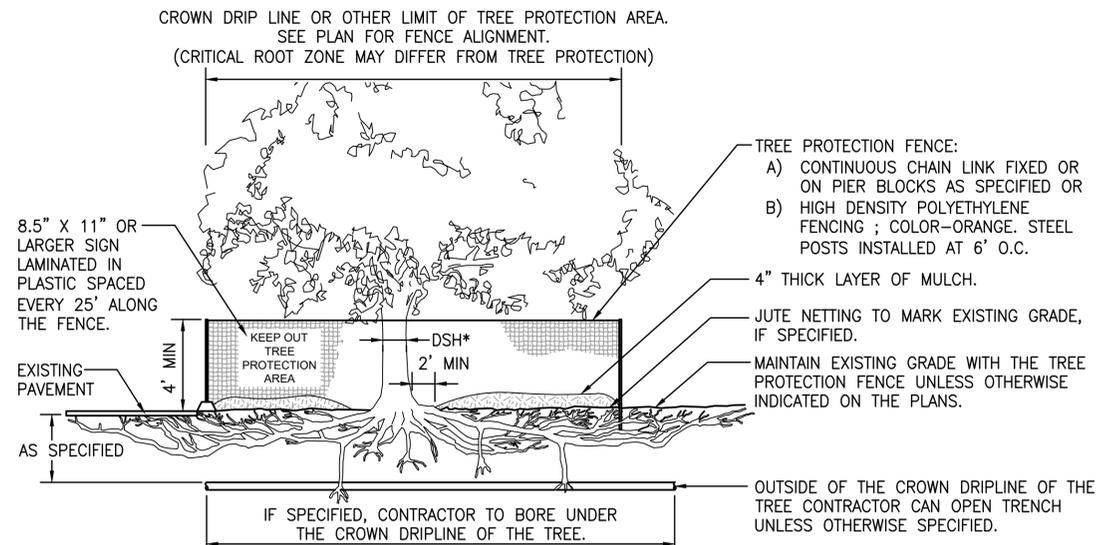
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CITY OF KIRKLAND
TOTEM LAKE PEDESTRIAN BRIDGE

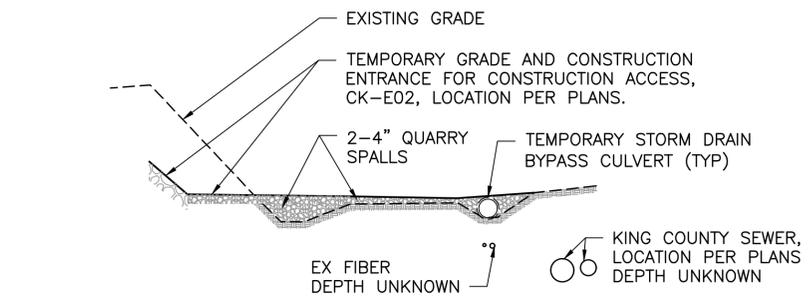
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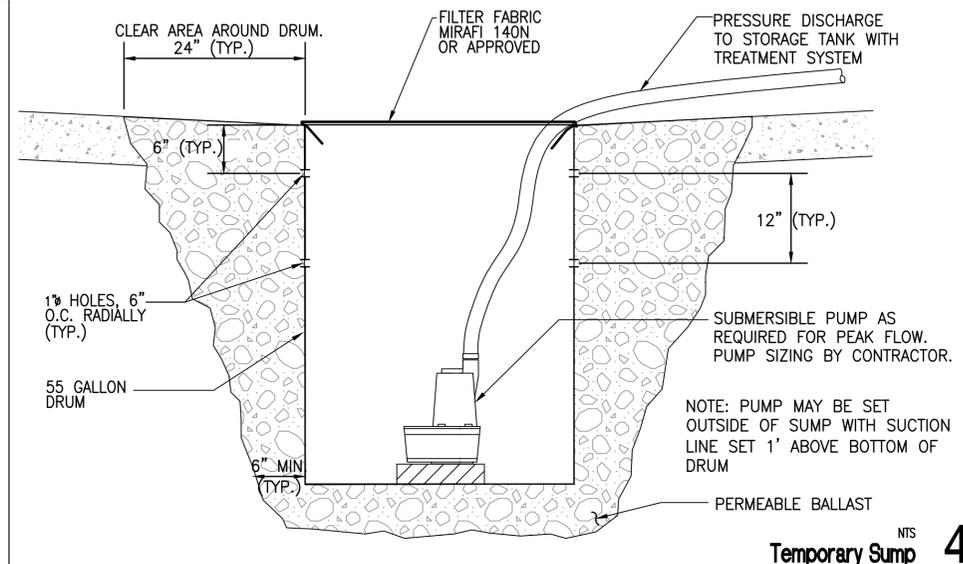


- NOTES:**
1. SEE GENERAL NOTES AND SPECIFICATIONS FOR ADDITIONAL TREE PROTECTION REQUIREMENTS.
 2. SEE SPECIFICATIONS FOR WATERING REQUIREMENTS.
 3. ALL PRUNING SHALL BE PERFORMED BY APPROVED ISA CERTIFIED ARBORIST.
 4. NO EQUIPMENT SHALL OPERATE INSIDE THE PROTECTIVE ZONE FENCE AREA INCLUDING DURING FENCE INSTALLATION AND REMOVAL.
 5. EQUIPMENT, TEMPORARY BUILDINGS, OR STORAGE SHALL NOT OCCUR WITHIN 10' OF TREE PROTECTION FENCE OR WITHIN SPECIFIED CRITICAL ROOT ZONE.
 6. SEE SITE AND CIVIL PLANS FOR ANY MODIFICATIONS WITHIN THE TREE PROTECTION AREA.
 7. DSH* - DIAMETER OF TRUNK AT STANDARD HEIGHT.

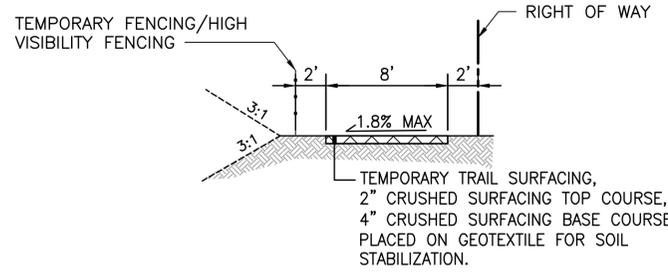
Tree Protection and Protection Fencing Detail **1**



SECTION A - TEMPORARY LAYDOWN AND CONSTRUCTION ACCESS



Temporary Sump **4**

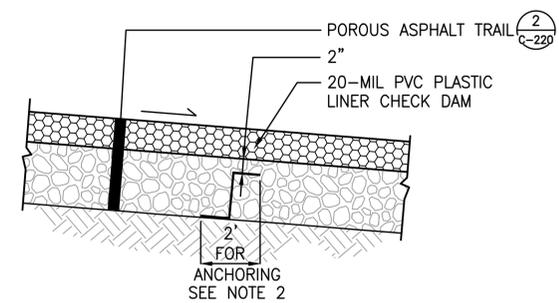


SECTION B - TEMPORARY TRAIL

NOTES: DEPTHS ARE COMPACTED DEPTHS.

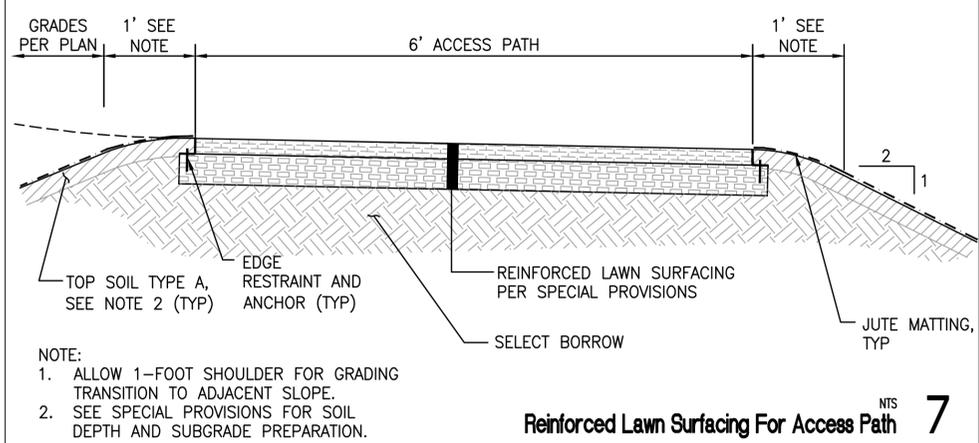


Typical Section for Construction Access and Temporary Trail **3**



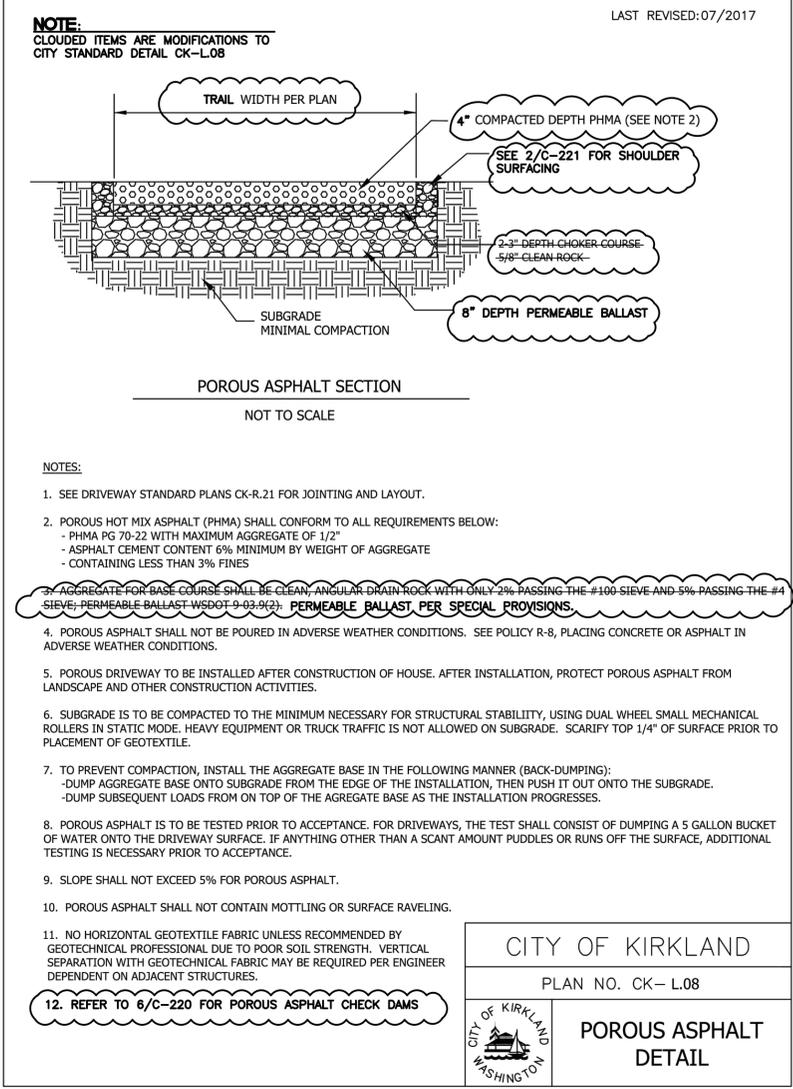
- NOTE:**
1. TOP OF PC CHECK DAM LINER IS SAME ACROSS LENGTH OF CHECK DAM.
 2. EXTEND LINER AS NEEDED TO SECURE IN PLACE.
 3. INSTALL CHECK DAMS 11' ON CENTER BETWEEN STATIONS 12+00 AND 14+85 AND STATIONS 23+00 AND 24+00. SEE PLANS FOR LOCATIONS OF CHECK DAM IN OTHER LOCATIONS

Porous Asphalt Check Dams **6**



- NOTE:**
1. ALLOW 1-FOOT SHOULDER FOR GRADING TRANSITION TO ADJACENT SLOPE.
 2. SEE SPECIAL PROVISIONS FOR SOIL DEPTH AND SUBGRADE PREPARATION.

Reinforced Lawn Surfacing For Access Path **7**



NOTE: CLOUDED ITEMS ARE MODIFICATIONS TO CITY STANDARD DETAIL CK-L08

- NOTES:**
1. SEE DRIVEWAY STANDARD PLANS CK-R.21 FOR JOINTING AND LAYOUT.
 2. POROUS HOT MIX ASPHALT (PHMA) SHALL CONFORM TO ALL REQUIREMENTS BELOW:
 - PHMA PG 70-22 WITH MAXIMUM AGGREGATE OF 1/2"
 - ASPHALT CEMENT CONTENT 6% MINIMUM BY WEIGHT OF AGGREGATE
 - CONTAINING LESS THAN 3% FINES
 3. AGGREGATE FOR BASE COURSE SHALL BE CLEAN, ANGULAR DRAIN ROCK WITH ONLY 2% PASSING THE #100 SIEVE AND 5% PASSING THE #4 SIEVE; PERMEABLE BALLAST W/SDOT 9-03-9(2); PERMEABLE BALLAST PER SPECIAL PROVISIONS.
 4. POROUS ASPHALT SHALL NOT BE POURED IN ADVERSE WEATHER CONDITIONS. SEE POLICY R-8, PLACING CONCRETE OR ASPHALT IN ADVERSE WEATHER CONDITIONS.
 5. POROUS DRIVEWAY TO BE INSTALLED AFTER CONSTRUCTION OF HOUSE. AFTER INSTALLATION, PROTECT POROUS ASPHALT FROM LANDSCAPE AND OTHER CONSTRUCTION ACTIVITIES.
 6. SUBGRADE IS TO BE COMPACTED TO THE MINIMUM NECESSARY FOR STRUCTURAL STABILITY, USING DUAL WHEEL SMALL MECHANICAL ROLLERS IN STATIC MODE. HEAVY EQUIPMENT OR TRUCK TRAFFIC IS NOT ALLOWED ON SUBGRADE. SCARIFY TOP 1/4" OF SURFACE PRIOR TO PLACEMENT OF GEOTEXTILE.
 7. TO PREVENT COMPACTION, INSTALL THE AGGREGATE BASE IN THE FOLLOWING MANNER (BACK-DUMPING):
 - DUMP AGGREGATE BASE ONTO SUBGRADE FROM THE EDGE OF THE INSTALLATION, THEN PUSH IT OUT ONTO THE SUBGRADE.
 - DUMP SUBSEQUENT LOADS FROM ON TOP OF THE AGGREGATE BASE AS THE INSTALLATION PROGRESSES.
 8. POROUS ASPHALT IS TO BE TESTED PRIOR TO ACCEPTANCE. FOR DRIVEWAYS, THE TEST SHALL CONSIST OF DUMPING A 5 GALLON BUCKET OF WATER ONTO THE DRIVEWAY SURFACE. IF ANYTHING OTHER THAN A SCANT AMOUNT PUDDLES OR RUNS OFF THE SURFACE, ADDITIONAL TESTING IS NECESSARY PRIOR TO ACCEPTANCE.
 9. SLOPE SHALL NOT EXCEED 5% FOR POROUS ASPHALT.
 10. POROUS ASPHALT SHALL NOT CONTAIN MOTTLING OR SURFACE RAVELING.
 11. NO HORIZONTAL GEOTEXTILE FABRIC UNLESS RECOMMENDED BY GEOTECHNICAL PROFESSIONAL DUE TO POOR SOIL STRENGTH. VERTICAL SEPARATION WITH GEOTECHNICAL FABRIC MAY BE REQUIRED PER ENGINEER DEPENDENT ON ADJACENT STRUCTURES.
 12. REFER TO 6/C-220 FOR POROUS ASPHALT CHECK DAMS

CITY OF KIRKLAND
PLAN NO. CK-L08
CITY OF KIRKLAND WASHINGTON
POROUS ASPHALT DETAIL

Porous Asphalt Trail **2**

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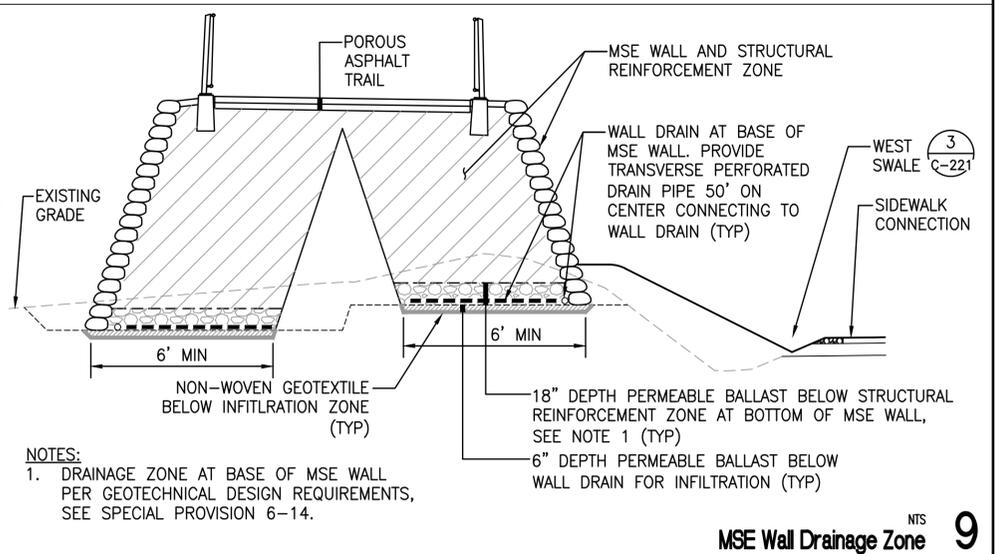
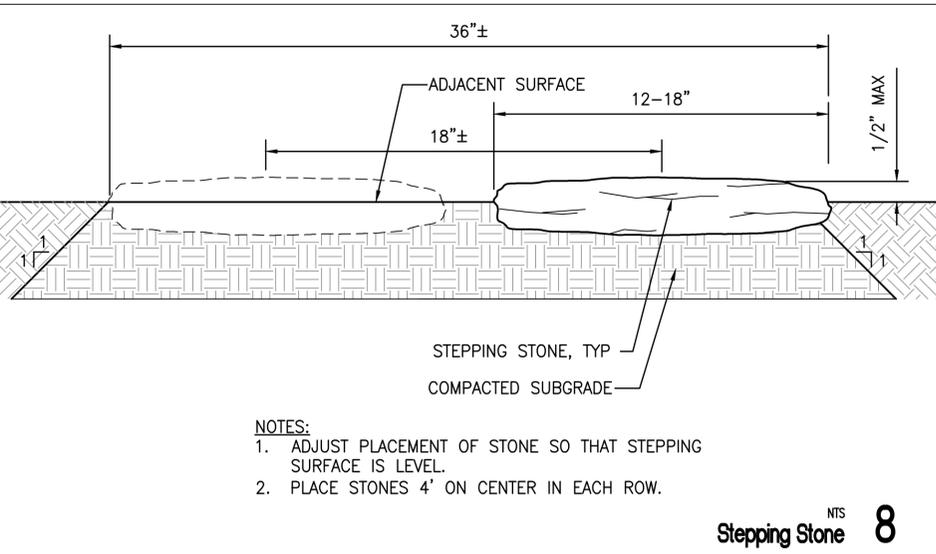
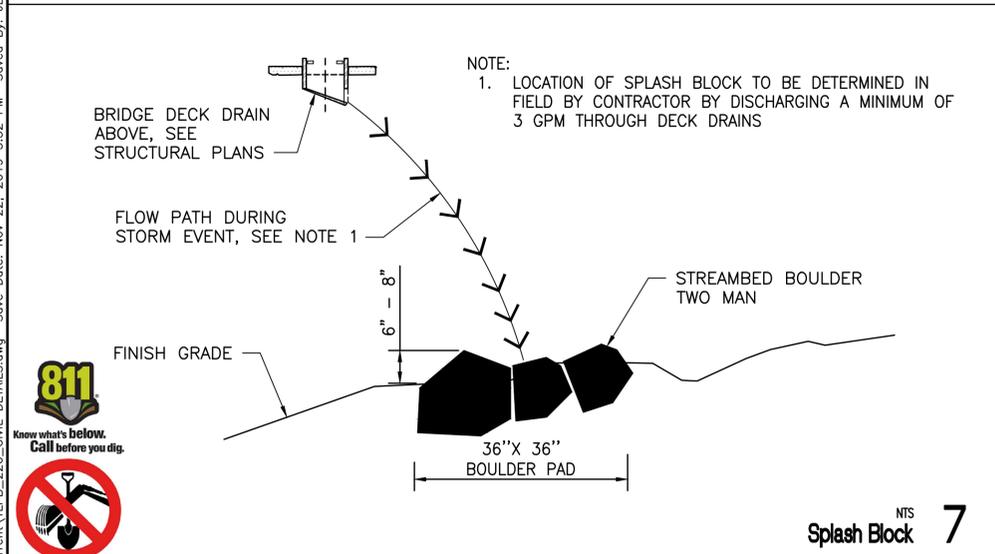
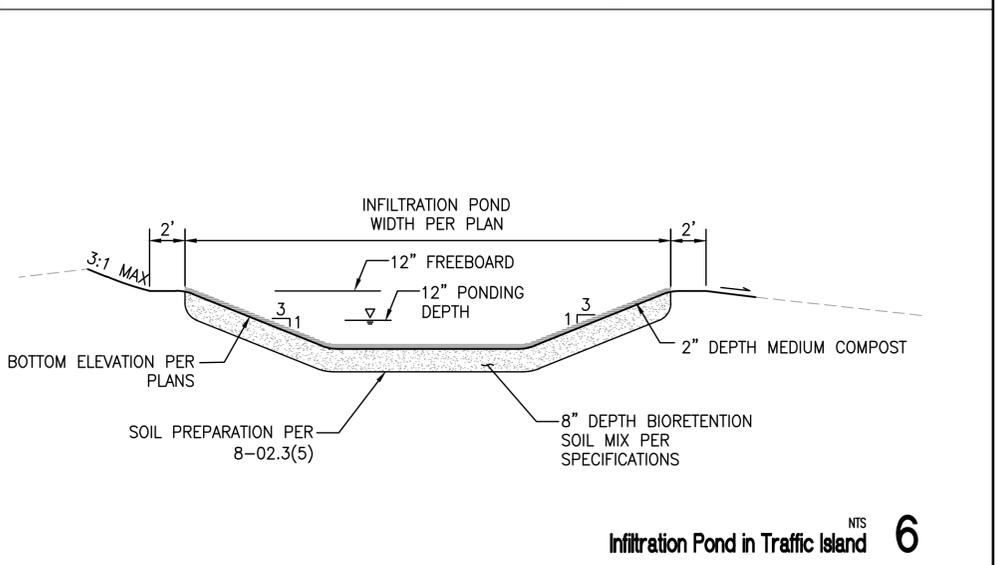
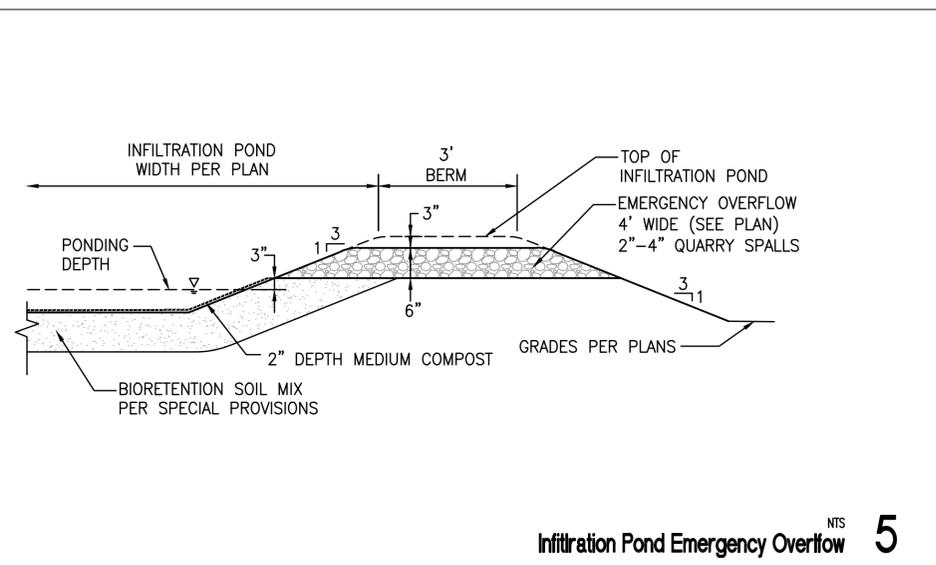
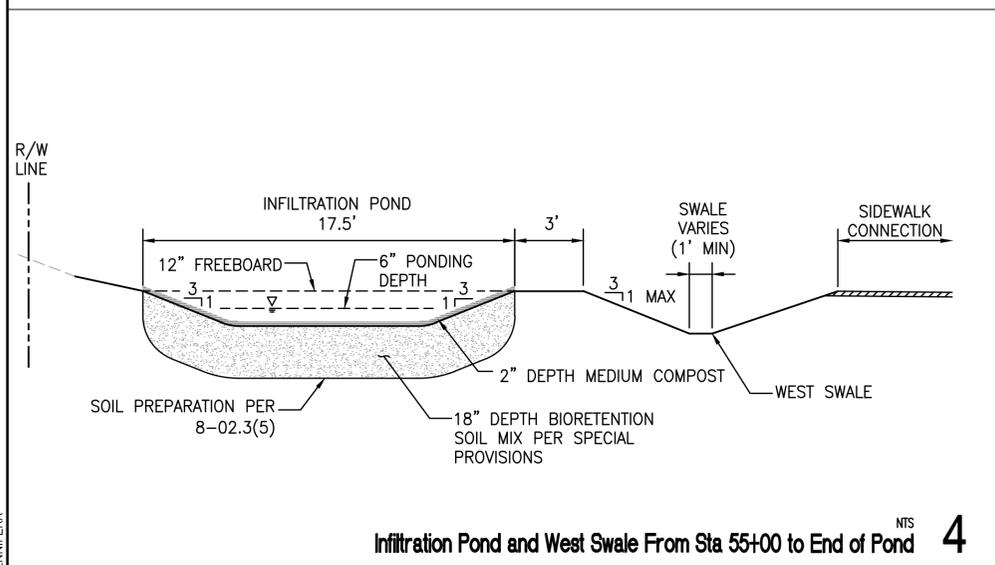
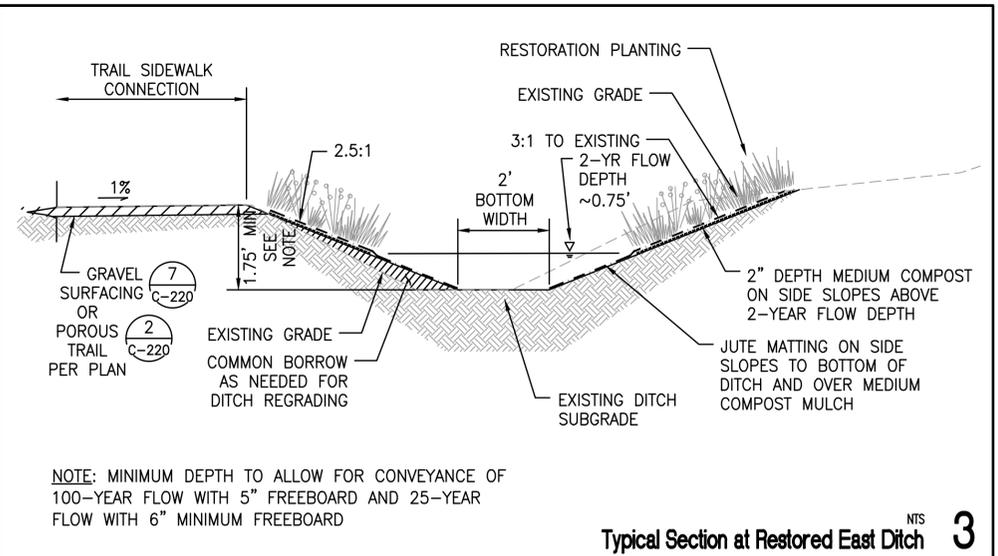
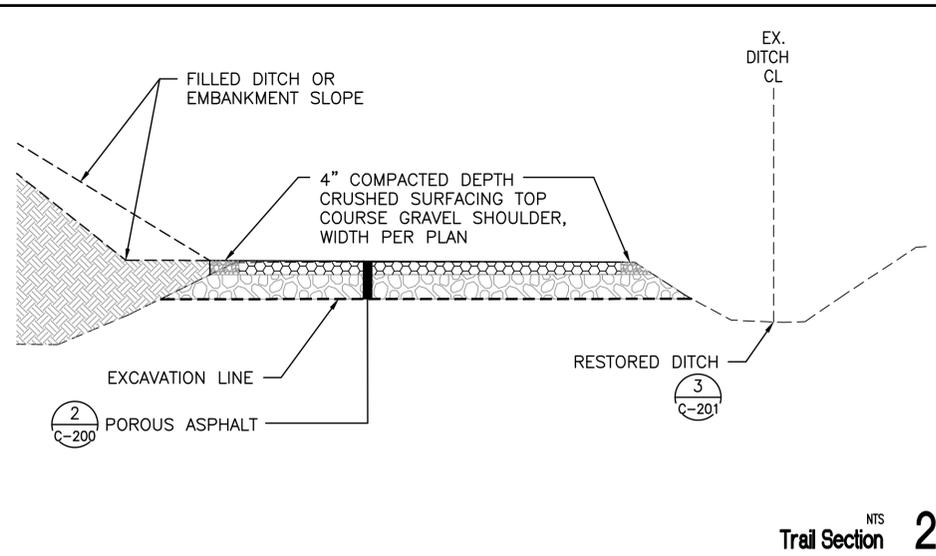
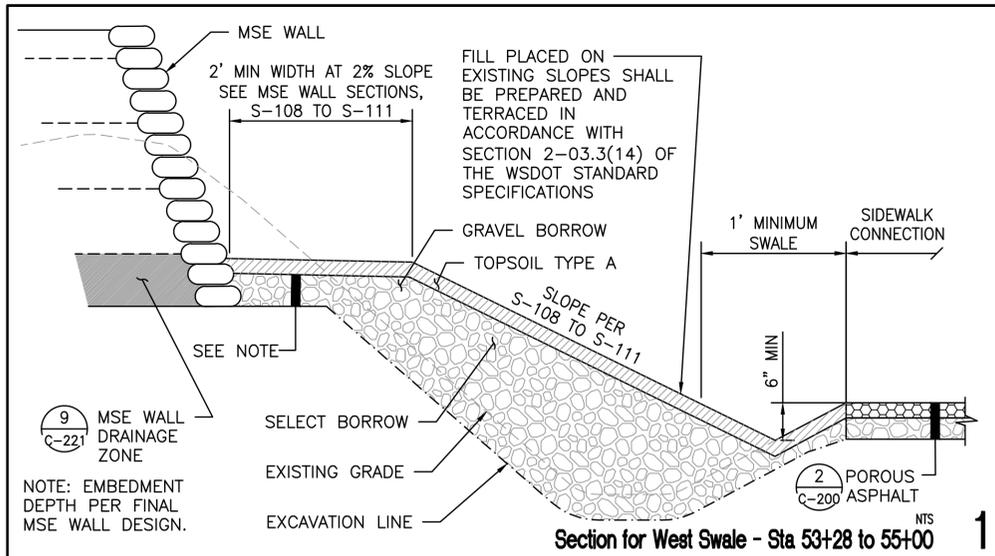


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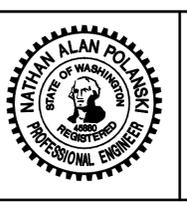
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CITY OF KIRKLAND
TOTEM LAKE PEDESTRIAN BRIDGE
CIVIL DETAILS 1

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SCALE: AS SHOWN
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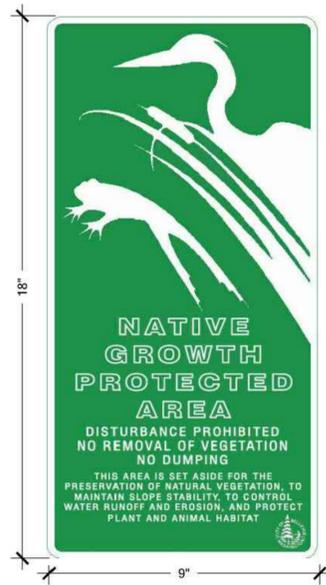
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TOTEM LAKE PEDESTRIAN BRIDGE
CIVIL DETAILS 2

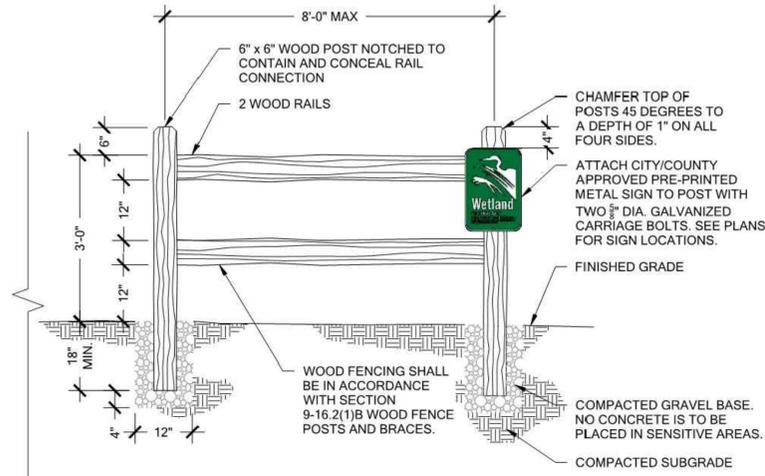
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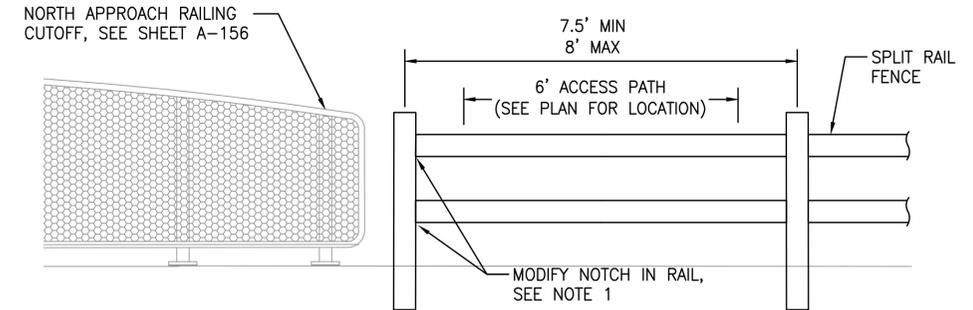
- NOTES:**
1. SIGNAGE TO APPEAR LIKE IMAGE AND SIZES NOTED ON THE LEFT WITH A GREEN BACKGROUND.
 2. SIGN TO BE FABRICATED IN ALUMINUM, BY A CITY APPROVED VENDOR, SUCH AS REDMOND SIGNS, (425) 883-9944 OR EQUIVALENT MANUFACTURER.
 3. SIGNAGE TO BE PLACED ON FENCING WHERE NOTED ON THE PLANS AND PER SPECIFICATIONS.

Critical Area Sign ^{NTS} 1



- NOTES:**
1. CENTER OF POST SHALL BE LOCATED 6" FROM THE EDGE OF SHOULDER (OR EDGE OF PATH WHEN NO SHOULDER IS PROVIDED) UNLESS SHOWN OTHERWISE IN PLANS.
 2. SEE DETAIL 3, SHEET C-222 FOR POST SPACING AND MODIFICATION TO RAILS AT ACCESS PATH (STA 23+98 LT)

Split Rail Fencing With Critical Area Signs ^{NTS} 2



NOTE: COORDINATE WITH ENGINEER TO PROVIDE MODIFICATION TO NOTCH IN RAILS AT ACCESS PATH TO ALLOW RAILS TO BE TEMPORARILY REMOVED FOR MAINTENANCE ACCESS.

Split Rail Fence at Sta 23+98 ^{NTS} 3

Not Used ^{NTS} 4

Not Used ^{NTS} 5

Not Used ^{NTS} 6

Not Used ^{NTS} 7

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CITY OF KIRKLAND
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