



**CITY OF KIRKLAND**  
**Planning and Community Development Department**  
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## **MEMORANDUM**

**To:** Planning Commission

**From:** Angela Ruggieri, AICP, Senior Planner  
Paul Stewart, AICP, Deputy Director  
Eric Shields, AICP, Planning Director

**Date:** January 21, 2015

**Subject:** Parkplace Amendment Request  
FILE # CAM14-02188

### **RECOMMENDATION**

- Receive a briefing on the proposed amendments to the zoning text for CBD-5A; the proposed modifications to the Master Plan and Design Guidelines; the EIS addendum; and the Planned Action Ordinance for Parkplace.
- Hold a public hearing on the proposed zoning text amendments.
- Make a recommendation to the City Council to approve the zoning text amendments and advise the Council to approve the Master Plan and Design Guidelines changes.

### **KEY ISSUES**

#### Zoning Code – Proposed Amendments (Attachments 2 and 3)

- Increase allowed residential square footage from 10% of total gross floor area to 30%.
- Include a 10% affordable housing requirement if residential percentage is increased.
- Allow one drive through facility for a bank or related financial service.
- Increase the movie theatre incentive from 10% to 20% of required retail square footage.

#### Master Plan and Design Guidelines – Proposed Amendments (Attachment 4)

- Gateway District and Central Way District - change in modulation guidelines (see pages 26-29 of Design Guidelines).
- Park Interface District – additional wording to protect project orientation to Peter Kirk Park (see page 30 of Design Guidelines).
- Main access now off of Central Way rather than 6<sup>th</sup> Street (see pages 10 & 11 of Master Plan).

## **BACKGROUND DISCUSSION**

The City Council received a letter dated October 3, 2014 from G. Richard Hill representing Kirkland Parkplace requesting that the City consider changes to the zoning text for CBD 5A and revisions to the Master Plan and Design Guidelines. At its October 21, 2014 meeting, the City Council directed the Planning Commission to study and provide a recommendation on the Parkplace proposal. As discussed below, the most significant amendment requested is to increase the percentage of residential use allowed. On November 12, 2014, the Planning Commission received a briefing on the proposed amendments and discussed the work plan. **At the Planning Commission's** December 11, 2014 meeting the code amendments were reviewed further. The changes to the Master Plan and Design Guidelines were also presented by the project architect. The staff reports for these Planning Commission meetings can be found at:

[http://www.kirklandwa.gov/depart/planning/Boards\\_and\\_Commissions/Planning\\_Commission.htm](http://www.kirklandwa.gov/depart/planning/Boards_and_Commissions/Planning_Commission.htm)

The following items will be discussed at the January 29, 2015 meeting.

### **A. Parkplace Current and Previous Proposal:**

The Parkplace property (see Attachment 1) is now owned by KPP Development LLP and the owner has presented the City with its proposal to modify the zoning text for CBD 5A and the adopted Master Plan and Design Guidelines for Parkplace. The proposed project is expected to have approximately 1,175,000 square feet which is significantly less than the original 1,750,000 square feet in the previously approved proposal.

The existing zoning that was adopted in 2008 allows building heights up to a maximum of 8 stories (up to 115 feet) on most of the site, with lower heights adjacent to Peter Kirk Park and Central Way. No changes are being proposed to the allowed height.

### **B. Process:**

In the adopted Master Plan and Design Guidelines, major modifications to the Master Plan are required to be reviewed by staff for consistency with the Comprehensive Plan and to be approved by the City Council. KMC 3.30.040 states that the City Council shall consult with the Planning Commission prior to amending the Master Plan and Design Guidelines.

The Planning Commission will make recommendations to the City Council on the zoning text changes (Attachments 2 and 3) and advise the Council on the amendments to the Master Plan and Design Guidelines (Attachment 4). The Planning Commission will not be reviewing the actual design of the project. Section 160.60 of the Zoning Code states that the City may not consider a specific proposal site plan or project in deciding whether or not a proposal should be approved.

An addendum to the Parkplace EIS, the Supplemental Parkplace EIS and the MRM EIS has been completed and is available for review by the Planning Commission to help with its recommendation (Attachment 5). Amendments to the original Planned Action Ordinance for the Touchstone Project have also been included in this packet for information purposes (Attachment 6). The Planned Action Ordinance is a separate City Council action and is not part of the Zoning Code or Master Plan.

The project design will be reviewed by the Design Review Board (DRB) in 2015, after the City Council has made a decision on the proposed changes to the zoning text and the Master Plan and Design Guidelines. These documents will be used by the DRB in their review of the project design.

Per Zoning Code Section 142.55, the applicant must begin construction or submit to the City a complete building permit application for the development within five years after the final approval of the Design Review Board or the decision becomes void. The applicant must substantially complete construction for the development activity within seven years after the final Design Review Board approval or the decision becomes void.

For development activity with phased construction (such as this proposal), lapse of approval may be extended by the Design Review Board and made a condition of the notice of decision.

### **C. Proposed Zoning Text Changes:**

The proposed changes to the zoning text that the Planning Commission will be reviewing include the following (see Attachments 2 and 3):

- The current zoning code limits residential development to 10% of the total gross floor area of the Master Plan; a zoning amendment is requested to increase this limit to 30%.
- The current code requires that the gross floor area of retail and restaurant uses be equal to or greater than 25% of the gross floor area of office uses. This regulation is not proposed to be changed; however, the current zoning also provides an incentive to include a movie theater in the project which is proposed to be changed. The code states that a maximum of 10% of the required retail and restaurant square footage may be met by movie theater square footage. A zoning text amendment is proposed to change this incentive to 20% of the required retail and restaurant square footage.
- The current code does not allow drive through facilities. A zoning text amendment to allow a bank drive through is requested.
- Staff is proposing a 10% affordable housing requirement if the residential percentage is increased (see Attachment 3). This 10% of affordable housing is commonly required when residential development capacity is increased. The

requirement is in accordance with Comprehensive Plan Policies H-2.3 and H-2.4 which state:

Policy H-2.3: Promote the provision of affordable housing by private sector residential developments.

Policy H-2.4: Provide affordable housing units when increases to development capacity are considered.

The following Zoning Code criteria must be met when zoning text changes are proposed.

135.25 Criteria for Amending the Text of the Zoning Code

The City may amend the text of this code only if it finds that:

1. The proposed amendment is consistent with the applicable provisions of the Comprehensive Plan; and

*See Comprehensive Plan analysis in Section D of this memo.*

2. The proposed amendment bears a substantial relation to public health, safety, or welfare; and

*Much of the public comment around the current proposal has been focused on the residential component, reduction in office and retail space, open space and the existing height allowance. A review of these public welfare issues follows:*

Residential: *A discussion of the addition of residential uses in mixed use projects and of combining residential with retail uses is included in Attachment 7 to this memo. It should also be noted that the 2008 proposal had a hotel that was approximately 250,000 square feet. There is no hotel in the current proposal.*

Retail: *The importance of preserving retail in the mix of uses has been identified. The applicant has not asked for a change in the Zoning Code requirement of retail square footage equal to 25% of the office square footage on the site. Since there will be less office in this current proposal than in the 2008 proposal, the amount of required retail will also be less. The current proposal includes community serving retail such as a new grocery and a movie theatre. The proposed retail will be 225,000 square feet, which is an increase of 81,850 square feet over the existing 143,150 square feet presently on the site.*

Open space: *The original 2008 rezone for Parkplace was partially based on the creation of a network of public open space. The new proposal includes more open space than the 2008 design. Pedestrian space in the 2008 proposal was 10% of the site or approximately 51,000 square feet). It will be between 15% and 20% of the site with a minimum of 75,000 square feet in the new proposal.*

*Height Allowance:* The applicant is not proposing a change to the existing Zoning Code height allowance, but residential buildings generally have less feet per floor than office buildings. The proposed QFC with residential above will be 20 or more feet lower than a QFC building with office above, since there is typically at least a **3' difference in floor height** between office and residential uses.

Based on the mitigations incorporated into the Planned Action Ordinance, the restrictions and requirements incorporated into the CBD 5A zone, and the development requirements included in the Master Plan and Design Guidelines, the proposed Zoning text amendments bear a substantial relationship to the public welfare. As reviewed in the EIS addendum, there are no significant adverse impacts identified to public health or safety.

3. The proposed amendment is in the best interest of the residents of Kirkland.

*The proposed zoning text changes provide for a vibrant mixed use development in the existing Parkplace location which will create a strong employment and base with complementary residential, retail and cultural/entertainment uses in the downtown activity area and derive the economic development benefits that accompany that base. The project has a pedestrian orientation with increased open space, and will include an affordable housing component and an expanded grocery store to serve the surrounding area.*

#### **D. Comprehensive Plan:**

Staff has determined that Comprehensive Plan amendments will not be necessary for this proposal. A complete analysis of the Comprehensive Plan Policies that relate to the amendment request is outlined in the EIS addendum (see Attachment 5) and included below.

Vision Statement (Excerpt): Downtown Kirkland is a vibrant focal point of our hometown with a rich mix of commercial, residential, civic and cultural activities in a unique waterfront location. Our downtown maintains a human scale through carefully planned pedestrian and transit-oriented development.

*Discussion:* The Revised Proposal includes a mix of office, retail and residential uses in the Downtown, and reflects the broad mix of activities envisioned in the Vision Statement. The Parkplace site has been planned to facilitate on-site pedestrian movement and will provide connections to other portions of the Downtown. The Parkplace site is also located adjacent to and within short walking distance of the Kirkland Transit Center. Higher density mixed-use development located adjacent to a transit center can also encourage greater use of transit.

### Framework Goals

FG-3: Maintain vibrant and stable residential neighborhoods and mixed-use development, with housing for diverse income groups, age groups, and lifestyles.

*Discussion:* The Revised Proposal is a mixed-use development that would include housing. It is assumed that KZC 112.15 would apply to the revised Proposal and would require that 10 percent of residential units be affordable.

FG-4: Promote a strong and diverse economy.

*Discussion:* The Revised Proposal includes 875,000 square feet of office and retail use, and would provide approximately 3,050 total jobs which would promote the local economy. A resident population would provide support for goods and services provided on-site and within the Downtown area.

**FG-8: Maintain and enhance Kirkland's strong physical, visual and perceptual linkages to Lake Washington.**

*Discussion:* The prior Parkplace EIS and SEIS evaluated the visual impacts of the proposal and alternatives, which was approved with buildings up to 8 stories in height. The Revised Proposal includes buildings up to the same height; aesthetic impacts are discussed in greater detail in Section 4.0 of the Addendum.

FG-14: Plan for a fair share of regional growth, consistent with state and regional goals to minimize low-density sprawl and direct growth to urban areas.

*Discussion:* The Revised Proposal would provide approximately 3,050 total jobs (2,383 new jobs) and 300 housing units in a compact, high density pattern in an urban downtown. This growth would help the City to meet its adopted housing and employment growth targets. Please also refer to Section 4.0 of the Addendum regarding growth forecasts.

### Land Use

LU-1.4: Create an effective transition between different land uses and housing types.

*Discussion:* Parkplace is located in the CBD 5A zoning district which currently permits a mix of office, commercial and residential uses. Residential uses would be contained in a mixed-use building located central to the site. Adjacent land uses are residential mixed-use to the north, across Central Way; multifamily residential and office to the east; office and residential to the south; and civic uses and Peter Kirk Park to the west. The prior Parkplace EISs evaluate potential land use impacts from the approved, more intensive Parkplace development to adjacent uses. Mitigation measures to address identified impacts were incorporated into the zoning regulations and design guidelines that apply to Parkplace. The Revised Proposal is less intensive and impacts would be similar or

lower; zoning regulations may be modified, however, to reflect the change in use and revised site plan.

Similarly, the MRM SEIS discusses potential impacts associated with locating residential land uses in this general area of the Downtown. The number of housing units proposed to be included in the Revised Parkplace Proposal is similar to the number of units considered in MRM Alternative 2a, and impacts would be similar as well.

Additional information about land use compatibility is included in the Land Use section of this Addendum.

LU-3.1: Provide employment opportunities and shops and services within walking or bicycling distance of home.

*Discussion:* The Revised Parkplace Proposal would provide employment opportunities for approximately 3,050 total (2,383 net) jobs and housing for approximately 513 people. Integrating commercial and residential uses on the same site in Downtown Kirkland would facilitate walking or bicycling to shops and services within Parkplace and in the Downtown by those living on-site and in adjacent residential buildings and neighborhoods.

LU-3.2. Encourage residential development within commercial areas.

**Plan explanatory text: "Housing within commercial areas provides the opportunity for people to live close to shops, services and places of employment. Conversely, residents living within commercial areas create a localized market for nearby goods and services, provide increased security, and help to create a sense of community for those districts. Residential development within commercial areas should be compatible and complementary to business activity. Residential use should not displace existing or potential commercial use."**

*Discussion:* Parkplace is an existing shopping center and office development, and it is identified in the Comprehensive Plan as a commercial area. As originally approved, Parkplace did not include any residential units, although the zoning regulations for district CBD 5A does permit up to 10 percent of the gross floor area of the Master Plan to be developed for multifamily housing and assisted living facilities. Proposed zoning changes would increase the permitted proportion of residential development to 30 percent. Incorporating residences in a mixed-use land use pattern could achieve the benefits identified in the plan text for policy LU-3.2, such as creating a base of residences who would contribute to the market for nearby goods and services, both on-site and in the Downtown more generally.

The increase in residential use could, however, **be viewed as displacing "potential commercial use" since the approved project was entirely office and retail.** The proposed addition of residential use is also occurring in the context of a project

that is being reduced in scale overall compared to what was originally approved, **so some "potential" for commercial use is being eliminated, with or without the** addition of housing. But any residential use developed in the originally approved project, a use which is allowed by existing zoning standards, would also have **displaced some "potential commercial use."**

**The site's new owners have revised the site plan to reflect changed market** conditions; the Revised Proposal includes what is considered to be supportable and desirable land uses in the Kirkland real estate market. From this perspective, although there may **be the "potential" for additional commercial use in terms of theoretical utilization of the site's land area, additional "potential" commercial use** is not considered to be marketable.

Overall, it seems likely that the intent of LU-3.2 is to provide direction in the context of the rezoning or change in use of an individual parcel from commercial to residential. Its application in the current situation, which is a diversification and reallocation of uses within a master planned site where uses are predominantly office and retail, is less clear. The policy issue of residential use is also discussed relative to the Moss Bay Neighborhood Plan below.

LU-3.5: Incorporate features in new development projects which support transit and non-motorized travel as alternatives to the single-occupant vehicle.

*Discussion:* As noted in the discussion of LU-3.1, integrating housing with office and retail uses on the same site in Downtown Kirkland, proximate to the transit center, would facilitate walking or bicycling to shops and services – within Parkplace and in the Downtown -- by those living on-site and in adjacent residential buildings and neighborhoods. Mixed-use development proximate to transit can also encourage use of public transit as an alternative drive-alone commuting. Other transit-supportive and non-motorized features were incorporated into the original Parkplace project, including implementation of transportation management program (TMP) program, and it is assumed that the same or a similar program would apply to the Revised Proposal.

LU-3.6: Encourage vehicular and non-motorized connections between adjacent properties.

*Discussion:* Section 8 of the adopted Parkplace Master Plan and Design Guidelines (KMC 3.30) require a network of pedestrian connections from the site to existing streets and to Peter Kirk Park. .The same or a similar network would be required for the Revised Proposal.

Goal LU-4: Protect and enhance the character, quality and function of existing residential neighborhoods while accommodating the **City's growth targets.**

*Discussion:* Residential neighborhoods, some of which are mixed-use are located in Downtown Kirkland adjacent to the site. Chapter 3.1 of the Parkplace Draft

EIS (City of Kirkland, 2008) discusses the potential for significant impacts to these neighborhoods from redevelopment of Parkplace. Most potential impacts to adjacent residential areas were related to the intensification of development/activity on the site and from increased height, and these impacts were addressed through a combination of building height limits, increased setbacks, and application of design guidelines. Proposed changes would add a residential component to the project and would reduce the intensity of redevelopment overall, but would maintain the same building heights. The inclusion of housing would help the City to achieve its population growth targets, while the reduction in employment would require increased growth in other commercial areas, primarily the Totem Lake Urban Center. Impacts would likely be the same or less, and the Revised Proposal would be consistent with Goal LU-4.

LU-4.2: Locate the most dense residential areas close to shops and services and transportation hubs.

*Discussion:* **In general, Kirkland's Downtown, which includes the Parkplace site, contains the City's highest residential densities, and this concentration of housing** is close to a concentration of shops and services. The high-density residential building proposed for Parkplace would be integrated within a mixed-use retail and office project that is within a short walk of the Downtown transit center.

LU-4.3: Continue to allow for new residential growth throughout the community, consistent with the basic pattern of land use in the City.

*Discussion:* **The City's "basic pattern of land use" in the Downtown, as expressed** in the Vision Statement and policy LU-5.3 below, includes a mix of residential, office and retail uses; the Revised Proposal is currently part of and would continue this basic pattern. The question of the appropriate mix of land uses on the Parkplace site in relationship to the Moss Bay Neighborhood is discussed below.

LU-4.4: Consider neighborhood character and integrity when determining the extent of land use changes.

*Discussion:* The amended proposal will increase employment significantly in Downtown, but by less than half as much as the 2008 proposal (approximately 2,383 new employees rather than approximately 5,318 new employees). The 2014 Proposal will also add up to 300 units and 300,000 square feet of multifamily residential. This addition of housing to the Parkplace site represents a change from the existing land use on the site, which is retail and office. Housing would be designed for compatibility with the overall master plan and is expected to be placed above commercial uses in a traditional mixed use pattern. Housing mixed with commercial and office would be compatible with surrounding uses.

To the south and north of the Parkplace site are residential developments with ground floor commercial; to the east is a mix of employment and multifamily

uses; to the west is Peter Kirk Park. The addition of residential use at the site will increase night-time use, which is not expected to have an impact on adjacent properties, particularly where there is like mixed use patterns; in the case of adjacent employment areas, these would typically be unused in the night time and would not be affected by Parkplace residential uses.

LU-5.1: Reflect the following principles in development standards and land use plans for commercial areas:

- Create lively and attractive districts with a human scale.
- Support a mix of retail, office and residential uses in multistory structures.
- Create effective transitions between commercial uses and surrounding residential neighborhoods.
- Protect residential areas from excessive noise, exterior lighting, glare, visual nuisances, and other conditions which detract from the quality of the living environment.

*Discussion:* Standards and guidelines for redevelopment of Parkplace were adopted in 2010 and were incorporated in the Comprehensive Plan, zoning regulations for CBD 5A (KZC 50.38) and the Parkplace Master Plan and Design Guidelines. While the Zoning Code and master plan/design guidelines will likely change somewhat to reflect the Revised Proposal, the basic elements of the redevelopment plan will remain the same. Adopted design guidelines will help create an attractive area, and the addition of housing will complement the commercial elements of the project (1<sup>st</sup> policy bullet). Proposed uses include a mix of office, retail, and residential in multi-story structures (2<sup>nd</sup> policy bullet). Transitions to adjacent residential and park/recreation uses would be achieved by the organization of building heights and setbacks (3<sup>rd</sup> policy bullet). Mitigation measures to protect adjacent residential area from potential impacts of office and retail development were identified in the prior Parkplace EIS and are incorporated into the policy and regulatory changes referenced above. Changes to the Project would result in similar or reduced impacts.

LU-5.2: Maintain and strengthen existing commercial areas by focusing economic development within them and establishing development guidelines.

**Explanatory text: "The intent of this policy is that future economic development be concentrated in existing commercial areas. This concentration can help to maintain and strengthen these areas and also promote orderly and efficient growth that minimizes impacts and service expansion costs. Concentration also allows businesses to benefit from proximity to each other. Intensification, rather than expansion of the boundaries of existing commercial areas into surrounding residential neighborhoods, is desirable. Infilling is preferred, particularly when it would create a denser pattern of development that is focused less on the private automobile and more on the opportunity for multiple transportation modes. Redevelopment may also provide new opportunities, especially in commercial areas where the community vision has changed over time."**

*Discussion:* Parkplace is an existing retail and office development in Downtown Kirkland, and is identified as a commercial area in the Comprehensive Plan. The goals of redeveloping the site include updating, revitalizing, diversifying and strengthening its economic performance, which is harmonious with the intent of LU-5.2. The site would be developed more intensively with a broader mix of uses than at present; no change in site boundaries would occur. Development guidelines are established in the Zoning Code, and in the Parkplace Master Plan and Design Guidelines; these are proposed to be revised to reflect a reduction in development scale, the inclusion of housing and revisions to the site plan.

Proposed redevelopment would also achieve many of the benefits described in **the plan's explanatory text. For example, the site is located within a short walk of** the Kirkland Transit Center and redevelopment would provide pedestrian connections to the surrounding area. These features would encourage transit use and non-motorized transit modes, respectively.

**LU-5.3: Maintain and enhance Kirkland's Central Business District (CBD) as a regional Activity Area**, reflecting the following principles in development standards and land use plans:

- Create a compact area to support a transit center and promote pedestrian activity.
- Promote a mix of uses, including retail, office and housing.
- Encourage uses that will provide both daytime and evening activities.
- Support civic, cultural and entertainment activities.
- Provide sufficient public open space and recreational activities.
- Enhance, and provide access to, the waterfront.

**Explanatory text: "The Central Business District (CBD) has** historically been the center of commercial activity in Kirkland. As Framework Goal 3 states, Downtown is also a residential, civic, cultural and entertainment focal point and has the most **dominant role in contributing to the City's identity. These prominent** roles of the CBD **should be maintained and enhanced."**

*Discussion:* The Revised Parkplace would be an intensively developed commercial area within the CBD, located within a short walk of the Downtown Transit Center. It would contain a mix of office, retail and residential uses; this is identical to the existing and desired mix of uses in the Downtown area overall. Retail uses, including restaurants and entertainment, would attract people during the day and evening. Public and private open spaces are included in the site plan. Redevelopment and the resulting provision of 3,050 total jobs (2,383 new jobs) would help to reinforce the central commercial function of Downtown Kirkland.

## Housing

H-2.4: Provide affordable housing units when increases to development capacity are considered.

*Discussion:* Although the Revised Proposal would decrease the overall development capacity of the Parkplace site, it would also increase residential development capacity, from 10 percent of gross floor area to up to 30 percent of gross floor area. It is assumed that the affordable housing requirement of KZC 112.15 would apply to the revised project, which could result in up to 30 affordable units.

## Economic Development

ED-1: Foster a strong and diverse economy consistent with community values, goals and policies.

*Discussion:* The Revised Proposal would contain approximately 875,000 gross square feet of office and retail use and would provide approximately 3,050 total jobs. Planned employment land uses would support the local economy and would advance relevant goals and policies of the Comprehensive Plan.

ED-1.5: Encourage clusters of complementary businesses.

*Discussion:* Parkplace is an existing retail shopping center and office development. The essence of a retail shopping center is that complimentary uses are clustered together to make shopping convenient and to create synergy among activities. The retail component of the center would be expanded, from an existing 143,150 gross square feet to approximately 225,000 gross square feet. The specific types of office-based businesses that would locate in Parkplace are not known at this time.

ED-1.6: Strive to maintain a balance of jobs and housing.

*Discussion:* Policy ED-1.6 speaks to a desired balance of jobs and housing for the City as a whole; it does not suggest that each individual project needs to provide a balance. The Revised Proposal would contribute a substantial quantity of jobs and some housing which would help the City to achieve its growth targets. At build-out, Parkplace as proposed would provide approximately 3,050 total jobs (2,383 new jobs) and 300 housing units. As a result of the reduction in scale of the overall project, there would be 2,935 fewer jobs compared to the previously approved project and more similar to the 2008 No Action Alternative; see Table in the EIS addendum. Due to the diversification in uses, there would also be 300 more housing units compared to the previously approved project.

**The effect of the Revised Parkplace proposal is that the City's capacity for housing would increase and provide further cushion to meet its 2031 and 2035**

estimated growth targets. It would reduce the **City's** job capacity but given the **excess capacity at Totem Center, the City's high range job capacity estimate** would continue to have excess capacity for growth targets. See Table in addendum.

ED-2.4: Consider the economic effects on businesses and the economic benefit to the community when making land use decisions.

*Discussion:* The Revised Proposal would expand Parkplace from an existing 238,450 gross square feet to 1,175,000 square feet of office, retail and residential use. Employment would increase from approximately 668 at present (applying standard employment square footage ratios) to an estimated 3,050 office and retail jobs. An economic objective of redevelopment is to increase spending for goods and services, which would generate additional tax revenues **to the City. The project's residential population would provide economic support** for businesses within Parkplace and within the Downtown generally.

ED-3.3: Encourage infill and redevelopment of existing commercial areas consistent with the role of each commercial area.

*Discussion:* The Proposal is redevelopment, intensification and diversification of an existing retail shopping center in Downtown Kirkland. Redevelopment would revitalize the existing development, expand employment opportunities and enhance **the center's economic function within the City.**

ED-3.5: Encourage mixed-use development within commercial areas.

**Explanatory text: "A mix of uses improves the vitality of commercial areas.** Mixed-use residential and commercial development provides the opportunity for residents to live, shop and work in commercial areas. Mixed-use development encourages one-stop shopping when a variety of businesses are located in close proximity to each other and shared parking is provided. Mixed-use development, when combined with multi-story structures, promotes a more compact and sustainable land use pattern and encourages walking and transit use to reduce **dependence on automobiles."**

*Discussion:* Changes to the Proposal would embody the principles stated in ED-3.5 and explanatory text. Redevelopment of Parkplace would result in a mix of office, retail and residential uses being located on an existing commercial site in Downtown Kirkland. Complementary retail uses would be located in proximity to one another. The site would be intensively developed in a compact pattern with multi-story buildings, ranging in height 3 to 8 stories. The project would provide pedestrian connections to surrounding development in the Downtown and to the nearby Transit Center.

Moss Bay Neighborhood Plan

Land Use. The Downtown area is appropriate for a wide variety of permitted uses. The **area's economic vitality and identity as a commercial center will depend upon its ability** to establish and retain a critical mass of retail uses and services, primarily located west of 3<sup>rd</sup> Street.

The enhancement of the area for retail and service businesses will best be served by concentrating such uses in the pedestrian core and shoreline districts and by encouraging a substantial increase in the amount of housing and office floor area either within or adjacent to the core.

*Discussion:* Although the statement above is focused primarily on the pedestrian core of the Downtown, which west of Parkplace, the principles are not limited geographically to the pedestrian core, and presumably include retaining and enhancement of retail and service businesses in the Downtown more generally. The Revised Proposal would redevelop and expand retail and service businesses and office uses in an important commercial area within the Downtown, which would enhance the area and contribute to its vitality.

The Land Use text also encourages a substantial increase in the amount of housing and office use within or adjacent to the core. The revised Parkplace Proposal, which is located adjacent to the core, includes both a substantial amount of office use and some housing.

As identified in the MRM SEIS, there is currently strong demand for housing in Downtown Kirkland. The SEIS notes a documented market trend that has preferred to develop housing in Downtown zoning districts which allow either housing or office use.

East Core Frame. Development in the East Core Frame should be in large, intensively developed mixed-use projects.

The East Core Frame is located east of Peter Kirk Park, extending from Kirkland Way northerly to 7<sup>th</sup> Avenue. The area includes the Parkplace shopping center as well as several large office buildings and residential complexes. South of Central Way, the area is largely commercial and provides significant opportunities for redevelopment. Because this area provides the best opportunities in Downtown for creating a strong employment base, redevelopment for office use should be emphasized. Within the Parkplace Center site, however, retail uses should be a significant component of a mixed-use complex. Limited residential use should be allowed as a complimentary use.

*Discussion:* This plan text addresses both the larger East Core Frame, which includes Parkplace, and the Parkplace site more specifically. The proposed development is consistent with the statement about the preferred form of development in the East Core Frame: it is a large, intensively developed mixed-use project. The explanatory text goes on to establish a hierarchy of preferred

uses: office use **should be "emphasized" (in the East Core Frame generally);** retail use should be a significant component of mixed-use development in **Parkplace; and "limited" residential use should be allowed in Parkplace.** This preferred hierarchy is generally reflected in the proposed development. Offices would be the most extensive use, at 55 percent of total gross floor area. Retail, theater and health club uses would comprise 19 percent of the total project. Together, office and retail uses would make up almost 75 percent of the total site. Retail and restaurant uses would comprise more than 25 percent of office use, which would meet the requirements of the CBD 5A zoning regulations (KMC 50.38.010, Special Regulation 2). While these amounts of development, and the overall project, are reduced from what was contained in the adopted Parkplace Master Plan, the emphasis of the revised redevelopment plan is still on office development, and retail use is still a significant component of the project. Retail uses would increase by approximately 50 percent compared to what exists in Parkplace today.

The text is clear that office and retail uses should be emphasized and that **residential use should be "limited."** The word "limited" is defined in Webster's New World College Dictionary (4<sup>th</sup> Edition) to mean "confined within bounds, restricted, narrow in scope or extent." The existing CBD 5A regulations limit residential development within Parkplace to 10 percent of the gross floor area (KZC 50.38, Special Regulation 3.d). The proposed code amendments would allow an increase of up to 30% residential square footage. While this proposed change would increase the amount of housing, this use would still be limited by regulation and secondary to commercial uses. The specific proportion of housing that is allowed is a legislative decision that will be made by the City Council. The CBD-5A zone (KZC 50.38.010) permits a variety of uses and it is not an exclusive office or retail district. As noted previously, the code limits the amount of residential use and includes some requirements for different types of retail/commercial uses.

Design District 5A. Redevelopment of this area should be governed by the Kirkland Parkplace Master Plan and Design Guidelines as set forth in the Municipal Code. Heights of up to eight stories are appropriate as an incentive to create a network of public open spaces around which is organized a dynamic retail destination. Development under the Master Plan and Design Guidelines should guide the transformation of this district from an auto-oriented center surrounded by surface parking into a pedestrian-oriented center **integrated into the community...Residential development could be designed to integrate** into both the office/retail character of the zone and the active urban nature of Peter Kirk Park.

*Discussion:* The 2014 Proposal would increase total office and commercial square footage in the analysis area, in addition to adding residential space, with larger buildings and greater area coverage than currently exist. However, because this proposal has approximately 34 percent less development space than the 2008 proposal, impacts are likely to be reduced. The revised proposal continues to propose eight stories, and the potential for view impacts of the 2008 EIS would

not change; design standards proposed for mitigation have been adopted and would apply to reduce impacts.

### **E. Proposed Changes to Master Plan and Design Guidelines**

Staff has been working with the applicants and their architect on proposed changes to the Master Plan and Design Guidelines (see Attachment 4). A representative from Collins Woerman, the architectural firm for the project, explained the key changes to the Master Plan and Design Guidelines at the Planning Commission meeting on December 11, 2014. Additional updates since that meeting are shown in Attachment 4. In response to a question posed at the last Planning Commission meeting, surface parking is addressed on page 7 of the Master Plan: Development Standards (see E. Parking).

The following description is taken from the EIS addendum:

Master Plan & Design Guidelines. City regulations establish a design review process for many types of projects. The process includes review and approval of proposals by the Design Review Board (KZC 142.35.9), and allows design departures and minor variations in design pursuant to established criteria (KZC 142.37) in appropriate circumstances. The City adopted a Master Plan and Design Guidelines for Parkplace in 2008 (KMC 3.30.040(4)), and that document establishes a framework for the design and development of the project, and provides a means to gauge design compliance during project review. Topics addressed in the Master Plan and Design Guidelines include basic project parameters (amounts and types of uses), site planning, building design, public access and amenities, and the design of streets. The heart of the document provides statements of design intent and graphic illustrations of design objectives for various components of the project.

*Discussion:* The Revised Project proposal includes modifications to the adopted Master Plan and Design Guidelines to reflect the revised site plan and development concept. These changes generally include the following:

- Updating of project parameters to reflect the decreased amount of development and proposed mix of uses (i.e., addition of residential use) of the Revised Proposal;
- New discussion of residential use which was not an element of the approved Parkplace project;
- New graphics to illustrate the intent of the design standards and guidelines;
- **Minor changes in phraseology (e.g., “pedestrian weather protection” replaces “covered walkway”);**
- For some of the design parameters, such as modulation and building design in the Central Way and Gateway districts, a greater emphasis on design intent and elimination of a specific quantitative/prescriptive

standard (e.g., the depth of building modulation, upper story stepbacks);

- Some minor reconfigurations of street sections (e.g., sidewalks, parking lanes) on some streets, although sidewalks are generally the same or wider.
- A change in the primary site access to Central Way; and
- An increase in required open space, from 10 percent/50,000 s.f., to 15 percent/75,000 s.f.

Overall, the revised Design Guidelines are substantially the same as the adopted Design Guidelines. Like the adopted guidelines, they are intended to ensure that project design is consistent with its physical context and the intent of adopted City policy. The proposed changes would not be likely to result in substantially different or greater impacts compared to the adopted Guidelines.

## **F. EIS Addendum**

The Addendum to the Parkplace EIS, Parkplace Supplemental EIS and MRM EIS is included as Attachment 5. EIS Consultants, Richard Weinman and Jennifer Barnes will present the highlights of the EIS Addendum at the Planning Commission meeting on 1/29/2015 and a brief summary is included below.

The Addendum includes the following sections which compare impacts and integrate mitigation measures of the 2008 proposal and the current revised proposal.

- 1.** Fact Sheet
- 2.** Introduction
- 3.** Description of Proposal and Prior Alternatives
- 4.** Environmental Review
- 5.** References
- 6.** Appendices

The Appendices include technical reports prepared regarding: Transportation, Water Service, Sewer Service, Public Services, Land Use and Aesthetics, and Plans and Policies.

In general, since the new proposal is smaller than the 2008 proposal, impacts of the new proposal are the same or reduced from those of the 2008 proposal.

Although employment in the Downtown will increase by approximately 2,383, the increase is 55 percent less than the 2008 proposal. The revised proposal will also add up to 300 residential units and 300,000 square feet of multifamily residential use. The housing and employment mix in the new proposal has changed.

Traffic impacts would be less with the new proposal as is shown by the following trip generation figures.

- Total PM Peak Hour Trips reduced by 788 (2056 in 2008 – 1268 present proposal)
- Total AM Peak Hour Trips reduced by 1865 (3545 in 2008 – 1680 present proposal)

The new proposal is expected to have similar or reduced visual impacts to views for residents and motorists, as it is 34 percent smaller than the 2008 Proposal, contains fewer buildings and would have more open space.

### **G. Planned Action Ordinance**

The 2008 EIS review alternative included the adoption of a Planned Action Ordinance (see Attachment 6) designating the Parkplace request as a Planned Action for the purposes of SEPA compliance. A Planned Action is intended to conduct early environmental review under SEPA so that impacts and mitigation measures for the planned development are identified up front. When a permit application and environmental checklist are submitted for Parkplace, the City will first verify that:

- the project meets the description of the project designated as a Planned Action by the ordinance;
- the probable significant adverse environmental impacts have been adequately addressed in the EIS; and
- the project includes any conditions or mitigation measures outlined in the ordinance.

If the project meets the above requirements, it qualifies as a Planned Action project and a SEPA threshold determination is not required. The City will monitor the development levels approved in the planned action areas as follows:

- Determine if the proposed land uses are within categories of land use studied in the EIS.
- Establish the maximum development potential for the request as reviewed in the EIS. Development potential can be expressed in square feet of development and in total vehicle trips.
- There is also a requirement for implementation of a transportation management plan for the development and associated monitoring.

Revisions to the 2008 Planned Action Ordinance have been made to reflect the new proposal with somewhat reduced mitigations due to the reduced project size. The Altom site (shown as Area C) has also been removed to clarify Parkplace requirements.

## **H. Retail and Residential Uses**

In response to a request from the Planning Commission, BERK Consulting has provided a memo (see Attachment 7) describing the value of mixed uses in centers. This memo describes broadly the value of mixed uses in a city core and describes the effects of combining residential, retail, and office uses. It concludes with a review of similar case study projects and some trends and considerations regarding the combination of residential and grocery uses in a downtown context.

### **PUBLIC OUTREACH**

Section 160.40.2 of the Zoning Code requires that a notice of the proposal and the public hearing be distributed at least 14 calendar days before the public hearing as follows:

- 1) The notice, or a summary thereof, will be published in the official newspaper of the City.
- 2) The notice, or a summary thereof, will be posted on each of the official notification boards of the City.
- 3) **The notice will be posted on the City's website.**

In addition to these requirements, the following notice was also given.

- 3 public notice signs have been placed on the site.
- A notice has been emailed to all interested parties from the 2008 Parkplace list.
- A notice has been sent to the previous 2008 Parkplace listserv.
- There was a mailing to the surrounding property owners and residents in advance of the public hearing.
- The proposal was presented to the Parks Board on 1/14/2015 and to the Transportation Commission on 1/28/2015.

### **SCHEDULE**

The following is a tentative schedule for the project.

01/29/2015 – Planning Commission review of EIS addendum, public hearing on the proposal and recommendation to the City Council.

02/10/2015 – Community meeting to discuss Addendum and Planned Action Ordinance

03/3/2015 – City Council considers Planning Commission recommendation and takes action on the proposed zoning text amendments. They also take action

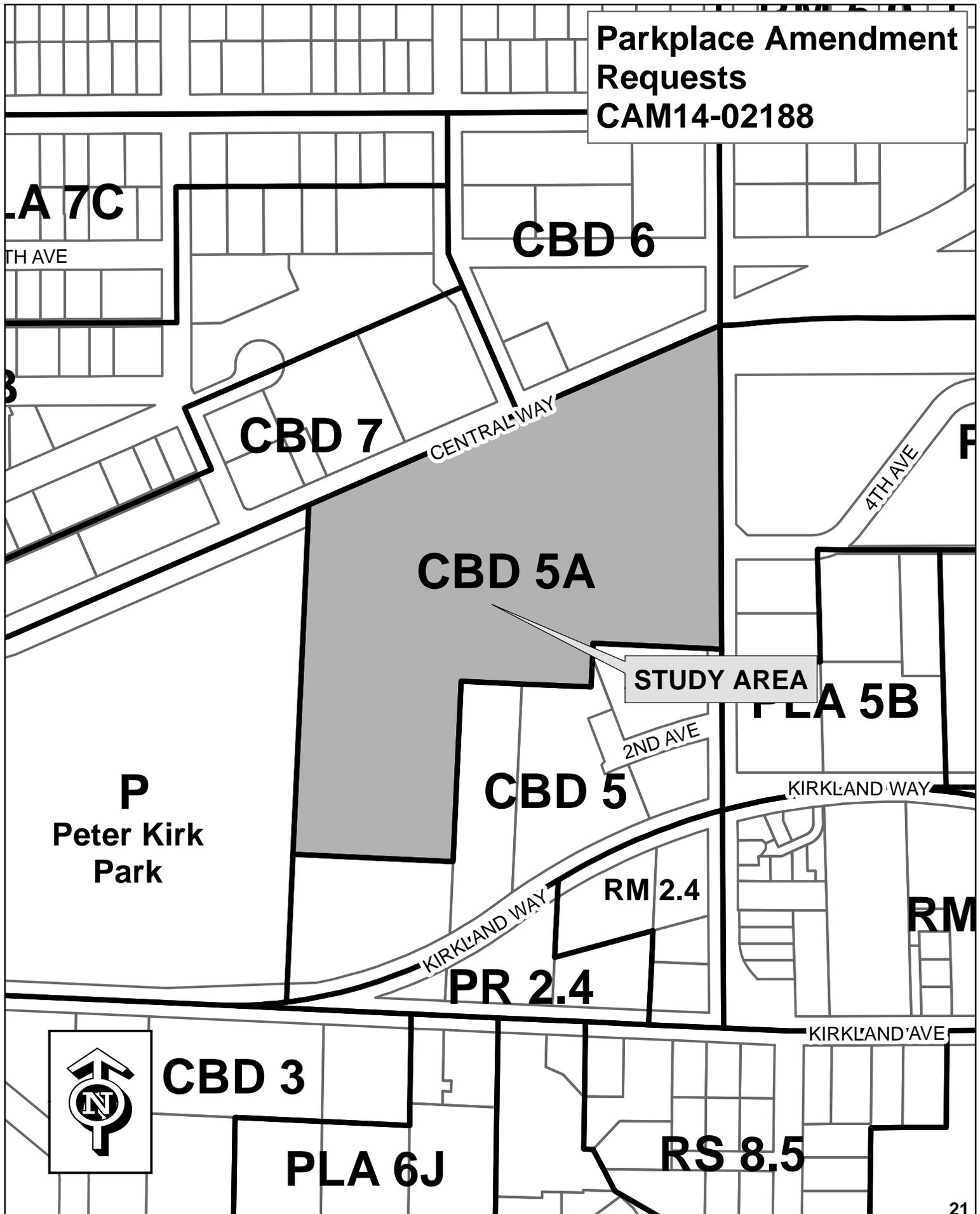
on the Design Guidelines and Master Plan amendments and the Planned Action Ordinance after taking public comment.

**Attachments:**

1. Study Area Map
2. Use Zone Chart for CBD 5A
3. Changes to the affordable housing chapter of the Zoning Code.
4. Summary of Changes to the Master Plan and Design Guidelines
5. Addendum
6. Planned Action Ordinance
7. Memo on Retail and Residential Uses

Cc: Rich Hill, Attorney for KPP Development LLP  
Bill Pollard, Talon Private Capital  
Jim Neal, Talon Private Capital  
Joe Razore, MRM Kirkland, LLC  
Moss Bay Neighborhood Association  
KAN

**Parkplace Amendment  
Requests  
CAM14-02188**





**50.36 User Guide – CBD 5A zones.**

The charts in KZC 50.38 contain the basic zoning regulations that apply in the CBD 5A zones of the City. Use these charts by reading down the left hand column entitled Use. Once you locate the use in which you are interested, read across to find the regulations that apply to that use.

<p><b>Section 50.37</b></p> 	<p>Section 50.37 – GENERAL REGULATIONS</p> <p>The following regulations apply to all uses in this zone unless otherwise noted:</p> <ol style="list-style-type: none"><li>1. Refer to Chapter 1 KZC to determine what other provisions of this code may apply to the subject property.</li><li>2. See KZC 50.62 for additional building height provisions.</li></ol>
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link to Section 50.38 table



**3. Developments creating four or more new dwelling units shall provide at least 10 percent of the units as affordable housing units as defined in Chapter 5 KZC. See Chapter 112 KZC for additional affordable housing incentives and requirements.**

DIRECTIONS: FIRST, read down to find use...THEN, across for REGULATIONS

Section 50.38	USE ↓ REGULATIONS ↓	Required Review Process	MINIMUMS			MAXIMUMS		Landscape Category (See Ch. 95)	Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)	Special Regulations (See also General Regulations)	
			Lot Size	REQUIRED YARDS (See Ch. 115)			Lot Coverage					Height of Structure
				Front	Side	Rear						
.010	Mixed Use Development Containing Office, Retail and Restaurant Uses. See Spec. Regs. 1, 2, 3 and 4.	D.R., Chapter 142 KZC. See Spec. Reg. 1.	None	See Plate 5, Chapter 180 KZC.			100%	See Spec. Reg. 5 and Plates 6 and 7, Chapter 180 KZC.	None	E See Spec. Reg. 6.	See Spec. Reg. 7.	<ol style="list-style-type: none"> <li>Development under this use shall be pursuant to the Parkplace Master Plan and Design Guidelines contained in Chapter 3.30 KMC. Compliance with the Master Plan and Design Guidelines shall be determined through DR, Chapter 142 KZC.</li> <li>The gross floor area of retail and restaurant uses in this zone shall be equal to or greater than 25 percent of the gross floor area of office uses in this zone. Retail uses may include accessory short term drop-off children's play facilities.</li> <li>The following additional uses are allowed subject to restrictions listed:                             <ol style="list-style-type: none"> <li>Hotel or Athletic Club. Accessory retail or restaurant uses shall be included as retail uses under Special Regulation 2, provided they are open to the public.</li> <li>Movie theater. This use may be included as a retail use under Special Regulation 2; provided, that the gross floor area of this use shall not count toward more than 10 percent of the required minimum gross floor area of retail and restaurant uses.</li> <li>Private Lodge or Club; Church; School, Day-care Center, or Mini-School or Day-care Center; Public Utility, Government Facility, or Community Facility; Public Park.</li> <li>Assisted Living Facility (including a nursing home if part of the facility); Stacked or Attached Dwelling Units; provided, that the gross floor area of these uses does not exceed 10 percent of the total gross floor area for the Master Plan.</li> </ol> </li> <li>The following uses are prohibited:                             <ol style="list-style-type: none"> <li>Any retail establishment exceeding 70,000 square feet.</li> <li>At grade drive-through facilities.</li> <li>The outdoor storage, sale, service and/or rental of motor vehicles, sailboats, motor boats, and recreational trailers.</li> </ol> </li> <li>Rooftop appurtenances may exceed the applicable height limitation by a maximum of 16 feet if the area of all appurtenances and screening does not exceed 25 percent of the total area of the building rooftop. All other regulations for rooftop appurtenances in Chapter 115 KZC shall apply.</li> <li>Prior to installation of permanent signs, the development must submit and receive approval of a Master Sign Plan pursuant to Chapter 100 KZC.</li> </ol>

provided, that the gross floor area does not exceed 10 percent of the total gross floor area for the Master Plan; and

20%

30%

Exception: one drive thru facility for banking or related financial services is permitted if the facility does not compromise the pedestrian orientation or traffic circulation of the development. The location and design of the facility requires Planning Official and Public Works Department approval.

REGULATIONS CONTINUED ON NEXT PAGE

USE ZONE CHART

**DIRECTIONS: FIRST, read down to find use...THEN, across for REGULATIONS**

Section 50.38		Required Review Process	MINIMUMS			MAXIMUMS		Landscape Category (See Ch. 95)	Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)	Special Regulations (See also General Regulations)	
			Lot Size	REQUIRED YARDS (See Ch. 115)			Lot Coverage					Height of Structure
				Front	Side	Rear						
.010	Mixed Use Development Containing Office, Retail and Restaurant Uses (continued)									<p>REGULATIONS CONTINUED FROM PREVIOUS PAGE</p> <p>7. The following establishes the number of parking spaces required:</p> <ul style="list-style-type: none"> <li>a. Residential uses must provide 1.7 parking spaces for each dwelling unit and one parking space for each assisted living unit.</li> <li>b. Restaurants and taverns must provide one parking space for each 125 square feet of gross floor area.</li> <li>c. All other uses must provide one parking space for each 350 square feet of gross floor area.</li> </ul> <p>A mix of uses with different peak parking times makes a project eligible for applying a shared parking methodology to parking calculations. Further parking reductions may be appropriate through a transportation management plan (TMP) and parking management measures. The development may propose and the Planning Official may permit a reduction in the required number of parking spaces based on a demand and utilization study prepared by a licensed transportation engineer. The study shall include an analysis of shared parking demonstrating that the proposed parking supply is adequate to meet the peak parking demand of all uses operating at the same time. A TMP and parking management measures shall be incorporated into the analysis. An analysis of the effectiveness of the TMP and parking management measures shall be provided for City review. The City's transportation engineer shall approve the scope and methodology of the study as well as the effectiveness of the TMP and parking management measures.</p>		



**Chapter 5 – DEFINITIONS****Sections:**

- 5.05 User Guide
- 5.10 Definitions

**5.05 User Guide**

The definitions in this chapter apply for this code. Also see definitions contained in Chapter 83 KZC for shoreline management, Chapter 90 KZC for drainage basins, Chapter 95 KZC for tree management and required landscaping, and Chapter 113 KZC for cottage, carriage and two/three-unit homes that are applicable to those chapters.

**5.10 Definitions**

The following definitions apply throughout this code unless, from the context, another meaning is clearly intended:

**.023 Affordable Housing Unit**

1. An owner-occupied dwelling unit reserved for occupancy by eligible households and affordable to households whose household annual income does not exceed the following percent of the King County median household income, adjusted for household size, as determined by the United States Department of Housing and Urban Development (HUD), and no more than 30 percent of the monthly household income is paid for monthly housing expenses (mortgage and mortgage insurance, property taxes, property insurance and homeowners dues):

- a. Eighty percent in the CBD 5A, RH, TL and PLA 5C zoning districts: where additional building height is allowed in exchange for the creation of affordable housing units; or
- b. One hundred percent in density limited zoning districts. where additional dwelling units are allowed in exchange for the creation of affordable housing units.

2. A renter-occupied dwelling unit reserved for occupancy by eligible households and affordable to households whose household annual income does not exceed 50 percent of the King County median household income, adjusted for household size, as determined by HUD, and no more than 30 percent of the monthly household income is paid for monthly housing expenses (rent and an appropriate utility allowance).

In the event that HUD no longer publishes median income figures for King County, the City may use any other method for determining the King County median income, adjusted for household size.

## Chapter 112 – AFFORDABLE HOUSING INCENTIVES – MULTIFAMILY

### Sections:

- 112.05 User Guide
- 112.10 Purpose
- 112.15 Affordable Housing Requirement
- 112.20 Basic Affordable Housing Incentives
- 112.25 Additional Affordable Housing Incentives
- 112.30 Alternative Compliance
- 112.35 Affordability Provisions
- 112.40 Regulatory Review and Evaluation

### 112.05 User Guide

This chapter offers dimensional standard flexibility and density and economic incentives to encourage construction of affordable housing units in commercial zones, high density residential zones, medium density zones and office zones.

If you are interested in proposing four (4) more residential units in commercial zones, high density residential zones, medium density zones or office zones, or you wish to participate in the City's decision on such a project, you should read this chapter.

### 112.10 Purpose

There is a limited stock of land within the City zoned and available for residential development and there is a demonstrated need in the City for housing which is affordable to persons of low and moderate income. Therefore, this chapter provides development incentives in exchange for the public benefit of providing affordable housing units in commercial zones, high density residential zones, medium density zones and office zones.

### 112.15 Affordable Housing Requirement

1. Applicability –
  - a. Minimum Requirement – All developments creating four (4) or more new dwelling units in commercial, high density residential, medium density and office zones shall provide at least 10 percent of the units as affordable housing units and comply with the provisions of this chapter as established in the General Regulations for the Use Zone or the Special Regulations in the Use Zone Chart for the specific use. This subsection is not effective within the disapproval jurisdiction of the Houghton Community Council.
  - b. Voluntary Use – All other provisions of this chapter are available for use within the disapproval jurisdiction of the Houghton Community Council and in developments where the minimum requirement does not apply; provided, however, the provisions of this chapter are not available for use in developments located within the BN zone.
2. Calculation in Density-Limited Zones – For developments in density-limited zones, the required amount of affordable housing shall be calculated based on the number of dwelling units proposed prior to the addition of any bonus units allowed pursuant to KZC 112.20.
3. Calculation in **CBD 5A**, RH, TL and PLA 5C Zones – For developments in the **CBD 5A**, RH, TL and PLA 5C Zones, the required amount of affordable housing shall be calculated based on the total number of dwelling units proposed.
4. Rounding and Alternative Compliance – In all zones, the number of affordable housing units required is determined by rounding up to the next whole number of units if the fraction of the whole number is at least 0.66. KZC 112.30 establishes methods for alternative compliance, including payment in lieu of construction for portions of required affordable housing units that are less than 0.66 units.

**112.20 Basic Affordable Housing Incentives**

1. Approval Process – The City will use the underlying permit process to review and decide upon an application utilizing the affordable housing incentives identified in this section.

2. Bonus

a. Height Bonus. In RH, PLA 5C, and TL use zones where there is no minimum lot size per dwelling unit, additional building height has been granted in exchange for affordable housing, as reflected in each Use Zone Chart.

b. Development Capacity Bonus. In the CBD 5A use zone where there is no minimum lot size per dwelling unit, additional residential development capacity has been granted in exchange for affordable housing as reflected in the Use Zone Chart.

b.c. Bonus Units. In use zones where the number of dwelling units allowed on the subject property is determined by dividing the lot size by the required minimum lot area per unit, two (2) additional units (“bonus units”) may be constructed for each affordable housing unit provided. (See Plate 32 for example of bonus unit calculations.)

e.d. Maximum Unit Bonuses. The maximum number of bonus units achieved through a basic affordable housing incentive shall be 25 percent of the number of units allowed based on the underlying zone of the subject property.

d.e. Density Bonus for Assisted Living Facilities. The affordable housing density bonus may be used for assisted living facilities to the extent that the bonus for affordable housing may not exceed 25 percent of the base density of the underlying zone of the subject property.

3. Alternative Affordability Levels – An applicant may propose affordability levels different from those defined in Chapter 5 KZC for the affordable housing units.

a. In use zones where a density bonus is provided in exchange for affordable housing units, the ratio of bonus units per affordable housing unit for alternative affordability levels will be as follows:

Affordability Level	Bonus Unit to Affordable Unit Ratio
<i>Renter-Occupied Housing</i>	
60% of median income	1.9 to 1
70% of median income	1.8 to 1
<i>Owner-Occupied Housing</i>	
90% of median income	2.1 to 1
80% of median income	2.2 to 1

b. In the CBD 5A, RH, TL and PLA5C use zones where additional height is provided in exchange for affordable housing units, the percent of affordable units required for alternative affordability levels will be as follows:

Affordability Level	% of Project Units Required to Be Affordable
<i>Renter-Occupied Housing</i>	
60% of median income	13%
70% of median income	17%

Affordability Level	% of Project Units Required to Be Affordable
<i>Owner-Occupied Housing</i>	
70% of median income	8%
90% of median income	13%
100% of median income	21%

c. To encourage “pioneer developments” in the Rose Hill and Totem Lake business districts, the definition of affordable housing for projects in the RH and TL zones shall be as provided in the following table. This subsection shall apply only to those projects which meet the affordability requirements on site or off site. This subsection shall not apply to those projects which elect to use a payment in lieu of constructing affordable units as authorized in KZC 112.30(4).

The affordable housing requirements for projects vested on or after the effective date of the ordinance codified in this section must be targeted for households whose incomes do not exceed the following:

Number of Total Units		Affordability Level	
<i>RH Zones</i>	<i>TL Zones</i>	<i>Renter-Occupied</i>	<i>Owner-Occupied</i>
First 50 units	First 150 units	70% of median income	100% of median income
Second 50 units	Second 150 units	60% of median income	90% of median income
All subsequent units	All subsequent units	50% of median income	80% of median income

“Number of Total Units” shall mean the total number of housing units (affordable and otherwise) permitted to be constructed within the RH and TL zones where affordable housing units are required and which have not received funding from public sources.

d. Depending on the level of affordability provided, the affordable housing units may not be eligible for the impact fee waivers described in subsections (5)(a) and (5)(b) of this section.

4. Dimensional Standards Modification – To the extent necessary to accommodate the bonus units allowed under subsection (2)(b)(c) of this section on site, the following requirements of the Kirkland Zoning Code may be modified through the procedures outlined in this subsection. These modifications may not be used to accommodate the units resulting from the base density calculation.

a. Maximum Lot Coverage. The maximum lot coverage may be increased by up to five (5) percentage points over the maximum lot coverage permitted by the underlying use zone. Maximum lot coverage may not be modified through this provision on properties with streams, wetlands, minor lakes or their buffers. In addition, this modification would require a shoreline variance as set forth in Chapter 141 KZC for properties within jurisdiction of the Shoreline Management Act. See Chapter 83 KZC.

b. Parking Requirement. The required parking may be reduced to 1.0 space per affordable housing unit. No additional guest parking is required for affordable housing units. If parking is reduced through this provision, the owner of the affordable housing unit shall sign a covenant, in a form acceptable to the City Attorney, restricting the occupants of each affordable housing unit to a maximum of one (1) automobile.

c. Structure Height. Maximum height for structures containing affordable housing units may be increased by up to six (6) feet for those portions of the structure(s) that are at least 20 feet from all property lines. Maximum structure height may not be modified through this provision for any portion of a structure that is adjoining a low density zone. This modification may be permitted or may require a shoreline variance as set forth in Chapter 141 KZC for properties within jurisdiction of the Shoreline Management Act. See Chapter 83 KZC.

- d. Required Yards. Structures containing affordable housing units may encroach up to five (5) feet into any required yard except that in no case shall a remaining required yard be less than five (5) feet. A modification to the shoreline setback would require a shoreline variance set forth in Chapter 141 KZC for properties within jurisdiction of the Shoreline Management Act. See Chapter 83 KZC.
  - e. Common Recreational Space. Common recreational open space per unit, when required, may be reduced by 50 square feet per affordable housing unit.
5. Impact Fee and Permit Fee Calculation
- a. Applicants providing affordable housing units may request an exemption from payment of road impact fees for the affordable housing units as established by KMC 27.04.050.
  - b. Applicants providing affordable housing units may request an exemption from payment of park impact fees for the affordable housing units as established by KMC 27.06.050.
  - c. Applicants providing affordable housing units are eligible for exemption from various planning, building, plumbing, mechanical and electrical permit fees for the bonus units allowed under subsection (2)(b)(c) of this section as established in KMC 5.74.070 and KMC Title 21.
6. Property Tax Exemption – A property providing affordable housing units may be eligible for a property tax exemption as established in Chapter 5.88 KMC.

#### **112.25 Additional Affordable Housing Incentives**

1. Approval Process for Additional Affordable Housing Incentives – An applicant may request that the City grant affordable housing incentives in addition to or in place of the basic affordable housing incentives allowed in KZC 112.20 due to specific site conditions. Such a request shall be reviewed and decided upon as outlined below.
2. Density Bonus – An applicant may propose more than two (2) bonus units for every affordable housing unit or a density bonus exceeding 25 percent of the number of units allowed in the underlying zone of the subject property. However, in no event may a project receive a bonus that would result in a number of bonus units that exceeds 50 percent of the number of units allowed based on the underlying zone of the subject property. Such a request shall be reviewed and decided upon by the Planning Director. The decision of the Planning Director in approving or denying a modification under this subsection may be appealed using the appeal provision, as applicable, of Process I, KZC 145.60 through 145.110.
3. Dimensional Standards Modification – An applicant may request further modification from the dimensional standards listed in KZC 112.20(4). Approval of any further modification of the dimensional standards will be based on the applicant's demonstration that the subject property cannot reasonably achieve the permitted density, including the bonus units. Such a request shall be reviewed and decided upon using Process I, described in Chapter 145 KZC. If the development, use, or activity requires approval through Process IIA or IIB, the entire proposal will be decided upon using that other process.
4. Criteria for Approving Additional Affordable Housing Incentives – The City may approve one (1) or more of the additional affordable housing incentives listed in subsection (2) or (3) of this section, in addition to or in place of the basic affordable housing incentives, if one (1) or more of the following requirements are met:
  - a. The additional incentive is necessary to provide sufficient economic incentive to the applicant to offset the cost of providing the affordable housing units.
  - b. The additional incentive is necessary to reasonably achieve the permitted density, including the bonus units.
  - c. The additional incentive is necessary to achieve a greater number of affordable housing units than the affordable housing requirements would prescribe or a greater level of affordability than is defined by the term affordable housing unit.

In making its decision on additional incentives, the City will consider the value of any property tax exemptions available to the project from the City as established in Chapter 5.88 KMC, as well as other fee waivers or reductions as established in the Kirkland Municipal Code.

THE CITY OF KIRKLAND

# Kirkland Parkplace Mixed-Use Development

Master Plan and Design Guidelines

# Appendix I: Roadmap to Changes

Prepared by CollinsWoerman  
January 20, 2015

## Purpose of this Appendix

The newly proposed *Kirkland Parkplace Mixed-Use Development: Master Plan and Design Guidelines* will replace the earlier document with the same name created in 2008. Much of the structure and content of the 2008 version continues to be relevant and is either left unchanged or is modified to meet the current design intent in the new document.

To help reviewers understand the differences between the two documents, this Appendix is provided as a "road map". Changes are noted here and, where relevant, explanations are included.

**Appendix II: Summary of Key Changes** is a list for quick review of all changes of significant content, and complements this Appendix.

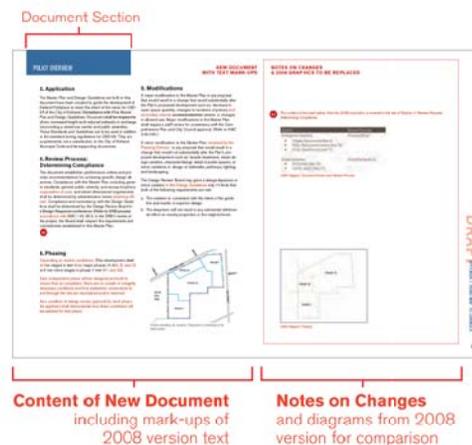
**Change in Overall Format:** The format of the new document (portrait-oriented, with two columns) is updated to be consistent with other Kirkland Design Guideline documents, such as *Design Guidelines for Yarrow Bay Business District* and *Design Guidelines for Pedestrian-Oriented Business Districts*.

**Change in Images:** All graphics in the document (diagrams, drawings, and photos) have been replaced.

## Appendix Structure

This Appendix contains each page of the newly proposed document along with notes and portions of the old document for comparison. The following diagram illustrates a sample page of this Appendix.

Note: the layout of the final document is altered slightly in this appendix in order to fit crossed out text onto the page.



### Text Color Key:

Black: Text from the existing 2008 document.

~~Text with strike-through~~: Text which is removed from the existing document appears in this appendix as crossed out.

Red: New text that was not in the 2008 document.

### Keynotes:

Some notes are added to further clarify changes between the 2008 and the proposed documents.

**7.1** Example Keynote for Section 7, note #1 of that section.

**For Review:** Content changes since Dec 3, 2014 version are in highlighted box

DRAFT APPENDIX I: ROAD MAP TO CHANGES

NEW DOCUMENT  
WITH TEXT MARK-UPS

NOTES ON CHANGES  
& 2008 GRAPHICS TO BE REPLACED

# Table of Contents

## POLICY OVERVIEW

- 1. Introduction.....3
- 2. Vision.....3
- 3. Application.....4
- 4. **Review Process: Determining Compliance**.....4
- 5. Modifications.....4
- 6. Phasing.....4
- 7. Comprehensive Plan Design Direction .....5
- 8. Design Intent.....6

## MASTER PLAN: DEVELOPMENT STANDARDS

- 9. Program Requirements.....7
- 10. **Public Amenities, Access, and Organization of Uses**.....8
  - A. **Pedestrian Connections**.....8
  - B. **Retail/Restaurant Frontage**.....9
  - C. **Organization of Uses** .....9
  - D. **Pedestrian Space** .....12
- 11. Street Classification.....13

## DESIGN GUIDELINES

- 12. Design Guidelines: All Districts.....18
- 13. Design Guidelines: District-Specific .....26
  - A. Gateway .....26
  - B. Central Way .....28
  - C. Park Interface.....30
  - D. **Mixed Use Hub**.....31

## NOTES

- Image Credits .....32

**Acknowledgment:** Written content in this document has been excerpted and/or excerpted and edited from the previously approved *Kirkland Parkplace Mixed Use Development Master Plan and Design Guidelines* document updated December 16, 2008 as created by LMN Architects.

DRAFT APPENDIX I: ROAD MAP TO CHANGES

# POLICY OVERVIEW

NEW DOCUMENT  
WITH TEXT MARK-UPS

## 1. Introduction

Located along Central Way and 6th Street, Kirkland Parkplace has the potential to offer many great amenities to Kirkland's downtown. Parkplace is a 501,000 square-foot property defined as CBD-5A in Kirkland's Zoning code. The proposed mixed-use center includes approximately ~~1.75~~ 1.175 million square feet of development consisting of retail, office, ~~hotel and sports club~~ residential, and entertainment uses that are, in effect, an extension of the existing downtown. ~~Parkplace provides components that meet the City's Comprehensive Plan, East Core Frame (2004, 2008) as described below:~~  
~~CP Policy: ...[additional text]~~

1.1

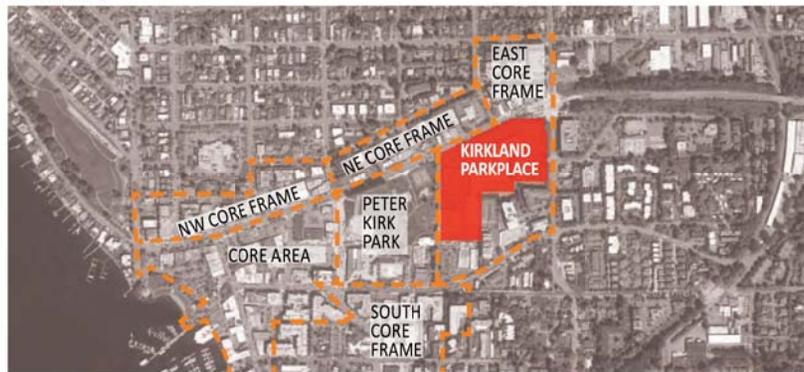
### PURPOSE

This document includes three major parts: (1) a Policy Overview that establishes a vision, procedure, and design intent; (2) a Master Plan comprised of Development Standards that establish basic programming and site planning requirements; and (3) Design Guidelines that establish detailed design standards for the site and buildings.

These Standards and Guidelines provide structure to help meet the goals outlined in the Comprehensive Plan. A discussion of relevant Comprehensive Plan directives and this document's associated responses can be found in Section 7: *Comprehensive Plan Design Direction*.

### PROJECT NAMING

While this document references the site's current name of "Kirkland Parkplace", the property owner may choose to re-brand the development and re-name it to reflect its new brand identity.



Kirkland Parkplace: Design District 5A, part of the East Core Frame in Kirkland's downtown area<sup>1</sup>

## 2. Vision

The Kirkland Parkplace Master Plan envisions a transformation of the existing suburban style office park and retail area to a lively, integrated mixed-use center.

Parkplace creates a new destination in Kirkland featuring tree-lined streets, landscaped open spaces, offices and residences overlooking public plazas, and a wide variety of shopping, dining, entertainment, and recreation experiences. Parkplace's contemporary Northwest architecture evokes Kirkland and its environs with green design, appropriate massing, and orientation. Appropriate placement of trees, fountains, benches, street lamps, and decorative sidewalk treatments add a rich texture to Parkplace's plazas and streets.

The combination of pedestrian-oriented streets, distinctive architecture, unique urban character, sensitive integration and progressive sustainable design strategies will make Kirkland Parkplace an attractive and valued gathering place for Kirkland's citizens ~~for years to come~~.

The compact design includes a diversity of spaces for gathering and bustling activity, while maintaining a human scale. This reflects and celebrates the evolution of Kirkland: balancing the need for growth and economic opportunity, but not losing touch with the comfortable, small-town roots of its past.

Kirkland Parkplace is both a home and a destination.

## NOTES ON CHANGES & 2008 GRAPHICS TO BE REPLACED

1.1

This content of Comprehensive Plan policies and Master Plan responses has moved to Section 7: *Comprehensive Plan Design Direction*.



2008 Diagram: Site Context

DRAFT APPENDIX I: ROAD MAP TO CHANGES

POLICY OVERVIEW

3. Application

The Master Plan and Design Guidelines set forth in this document have been created to guide the development of Kirkland Parkplace to meet the intent of the vision for CBD-5A of the City of Kirkland. Compliance with This Master Plan and Design Guidelines Document shall be required to allows increased height and reduced setbacks in exchange for providing a mixed-use center and public amenities. These Standards and Guidelines are to be used in addition to the standard zoning regulations for CBD-5A. They are supplemental, not a substitution, to the City of Kirkland Municipal Code and its supporting documents.

4. Review Process: Determining Compliance

This document establishes performance criteria and provides recommendations for achieving specific design objectives. Compliance with the Master Plan, including general standards; general public amenity, and access locations; organization of uses; and street dimensional requirements shall be determined by administrative review (planning official). Compliance and consistency with the Design Guidelines shall be determined by the Design Review Board in a Design Response conference. (Refer to DRB process accordance with KMC 142.35.9. In the DRB's review of the project, the Board shall respect the requirements and commitments established in this Master Plan.

4.1

6. Phasing

Depending on market conditions, this development shall be staged in two three major phases (1, 2-A, B, and C) with two minor stages in phase A one (A1 and A2).

Each independent phase will be designed and built to ensure that, at completion: there are no unsafe or unsightly temporary conditions; pedestrian connections to and through the site are maintained and/or restored; and functionality of vehicle access and circulation is maintained.

As a condition of design review approval for each phase, the applicant shall demonstrate how these conditions will be satisfied for that phase.

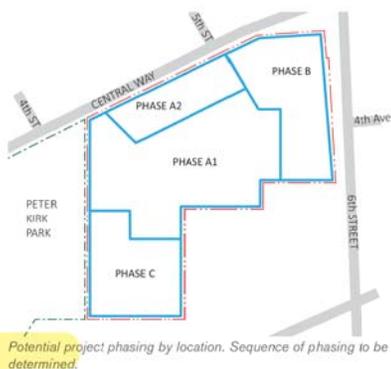
5. Modifications

A major modification to the Master Plan is any proposal that would result in a change that would substantially alter the Plan's proposed development such as: decrease in open space quantity, changes to locations of primary and secondary internal access/pedestrian streets, or changes in allowed use. Major modifications to the Master Plan shall require a staff review for consistency with the Comprehensive Plan and City Council approval. (Refer to KMC 3.30.040.)

A minor modification to the Master Plan, reviewed by the Planning Director, is any proposal that would result in a change that would not substantially alter the Plan's proposed development such as: facade treatments, street design variation, character/design detail of public spaces, or minor variations in design of sidewalks, pathways, lighting, and landscaping.

The Design Review Board may grant a design departure or minor variation in the Design Guidelines only if it finds that both of the following requirements are met:

- a. The variation is consistent with the intent of the guideline and results in superior design.
- b. The departure will not result in any substantial detrimental effect on nearby properties or the neighborhood.



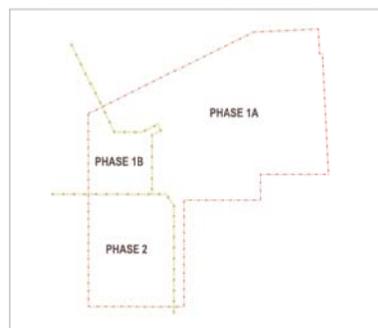
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NOTES ON CHANGES & 2008 GRAPHICS TO BE REPLACED

4.1 The content of the chart below, from the 2008 document, is covered in the text of Section 4. Review Process: Determining Compliance.

Section	Review Process
Development Standards	Planning Official
<ul style="list-style-type: none"> <li>• Program Requirements (Item 9)</li> <li>• Public Amenities and Access (Item 10)</li> <li>• Street Classifications (Item 11)</li> </ul>	
Design Guidelines	Design Review Board
<ul style="list-style-type: none"> <li>• All Districts (Item 12)</li> <li>• District Specific (Item 13)</li> </ul>	

2008 Diagram: Document Section and Review Process



2008 Diagram: Phasing

POLICY OVERVIEW

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NOTES ON CHANGES & 2008 GRAPHICS TO BE REPLACED

**7.1** **7. Comprehensive Plan Design Direction**

The City of Kirkland's Comprehensive Plan, Section XV.D, includes several policies and guidelines directly related to the Parkplace site. Four relevant Comprehensive Plan directives and associated responses are included below:

**A. CP Policy:** Heights of up to eight stories are appropriate as an incentive to create a network of public spaces around which is organized a dynamic retail destination. (CP XV.D-13).

**Response:** Parkplace is an urban, open-air retail, restaurant, entertainment, office, ~~hotel and sports club and residential~~ complex. (See ~~page 7~~ Section 10 for standards regarding networks of open space, retail frontage, and pedestrian connections.)

**B. CP Policy:** Special attention to building design, size, and location should be provided at three key locations:

- at the intersection of Central Way and Sixth Street to define and enhance this important downtown gateway;
- along Central Way to respond to the context along the north side of street;
- and facing Peter Kirk Park to provide a transition in scale to downtown's central green space (CP XV.D-14).

Pedestrian Connections to adjoining streets, Peter Kirk Park, and adjoining developments should be incorporated to facilitate the integration of the district into the neighborhood (CP XV.D-13).

**Response:** Specific design guidelines have been defined to encourage unique environments and experiences in each of these three locations. The development standards define pedestrian connection requirements.

**C. CP Policy:** Because of the intensity of land use in 5A, the design of the buildings and site should incorporate aggressive sustainability measures, including low impact development measures, deconstruction, green buildings, and transportation demand management (CP XV.D-14).

**Response:** The compact development, pedestrian-friendly, mixed-use nature of the land use in CBD-5A is fundamentally sustainable. It provides a live-work balance in downtown Kirkland and provides access to goods and services people need in proximity to where they live. Combined with a commitment to sustainable strategies in the design of the development, Kirkland Parkplace will significantly contribute to lowering carbon emissions and energy use relative to a suburban model of development.

**D. CP Policy:** Residential development could be designed to integrate into both the office/retail character of the zone and the active urban nature of Peter Kirk Park (CP XV.D-14).

**Response:** The Development Standards provide for up to 30% of building floor area to be devoted to residential use. The proposed residential component will enhance Parkplace's public and retail experience and bring after-hours activity to the development. Residents will have access to a range of services and a direct connection to Peter Kirk Park - all within walking distance.

**7.1** This section - "7. Comprehensive Plan Design Direction" - does not appear in the body of the 2008 document, though it is in the Table of Contents. The 2008 document included a discussion of relevant Comprehensive Plan design directives in Section 1, and this discussion has moved to the new Section 7.

**7.2** This existing Comprehensive Plan statement related to residential development was not referenced in the 2008 Master Plan because residential development was not proposed at that time. It is included here, along with the Master Plan response, because the new Master Plan proposes a significant residential component on the site. See Section 9. Program Requirements.

## POLICY OVERVIEW

**8. Design Intent**

This Master Plan and Design Guidelines document was created using the identified ~~eight~~ **nine** Guiding Principles for the project which were derived from input from the City staff, Design Review Board, Planning Commission, various community groups, and the residents of Kirkland.

1. Emotional ownership by the community:
  - Incorporate the project into the story of Kirkland.
  - Enable meaningful community exchanges.
  - Inspire unique experiences and discoveries.
  - Promote the coalescence of Community, Culture, and Commerce.
  - Provide a 'transforming experience' vs. a 'transactional experience'.
  - Include neighborhood retail.
2. Site planning connections:
  - Include public spaces such as plazas.
  - Create clear vehicular access and parking.
  - Create strong emphasis on the streetscape.
  - Support active public spaces.
  - Provide clear and inviting public access.
  - **Provide connections to Peter Kirk Park.**
3. Create community gathering spaces:
  - Create easily accessible public spaces.
  - Develop spaces that vary in size and offer choices for all ages.
  - Provide safety and comfort.
  - Integrate into the social life of downtown Kirkland.
4. Enhance the pedestrian environment:
  - Promote walkability: network of internal and external pedestrian connections.
  - Create visual interest along the street.
  - Incorporate a rich variety of materials.
  - Provide and enhance pedestrian circulation and retail continuity.
5. Integrate motor vehicle access and parking
  - Minimize the visual presence of parked cars.
  - Allow parking to be utilized during nights/weekends for benefit of community and downtown.

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6. A mix of uses = a mix of building types:
  - Create a variety of building types, scales, and materials.
  - Express a three-dimensional quality to the public spaces.
7. Appropriate massing and scale:
  - Create pedestrian spaces with access to sun.
  - Address surrounding edges.
  - Consider scale, massing, and detail of individual buildings.
  - Express human-scale, detailed street level building facades.
8. Sustainability:
  - Establish macro-scale/site sustainable strategies.
  - Pursue building-specific sustainable strategies.
  - Encourage tenant-specific sustainable strategies.
9. **Mixed-use development:**
  - **Provide a residential component to the project that will support the viability of a 24-hour development and complement the other uses on the site.**



Children's play area at Peter Kirk Park<sup>2</sup>

**NOTES ON CHANGES  
& 2008 GRAPHICS TO BE REPLACED**

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NOTES ON CHANGES  
& 2008 GRAPHICS TO BE REPLACED

# MASTER PLAN: DEVELOPMENT STANDARDS

## 9. Program Requirements

The following requirements and ratios are established to quantify use types at the completion of the project, and are not a requirement for any single phase.

### A. PEDESTRIAN SPACE

The development will include a variety of public open spaces that vary in size and character. A minimum of 15+0%, or 75,000 ~~50,000~~ square feet, of the site shall be activated pedestrian-oriented space, in the form of courtyards, plazas, ~~winter atrium~~, etc. See diagram (Section 10.D) for approximate locations and dimensional requirements of specific spaces. Definition of appropriate design treatments are found in the district-specific design guidelines (Section 13).

### B. ARTS COMMITMENT

In an effort to encourage integrated art into the project, Parkplace is working in collaboration with representatives from the cultural council and local art community and will identify and create opportunities to integrate art into the project.

### C. GREEN BUILDING COMMITMENT

Section V. Natural Environment of the Comprehensive Plan outlines broad goals and policies related to environmental sustainability. Section XV.D of the Comprehensive Plan and Guiding Principle #8 (see Section 8 of this document) describe goals specific to the Parkplace site.

1. In response to these goals and policies, the following requirements will apply to the Kirkland Parkplace project:

- All ~~new~~ office buildings will be designed achieve a LEED CS Gold threshold. A USGBC Pre-Certification Application showing points meeting LEED CS Gold will be included with permit submittals to show which points will be pursued.
- The ~~hotel~~ multi-family residential building(s) will be designed to a LEED for Homes Multifamily Mid-Rise Silver ~~CS-Certified or LEED-NC-Certified~~ threshold; or to meet Built Green 4 Star certification the sustainability program of the hotel operator.

c. The applicant shall encourage all potential tenants for Kirkland Parkplace to pursue LEED-CI. To accomplish this, the applicant will create and distribute to tenants a set of Tenant Design Guidelines to show strategies tenants can use to achieve LEED-CI certification. These Tenant Design Guidelines will be made available to the City of Kirkland to inform their ongoing sustainability programs.

d. At the end of tenant build-outs ~~on~~ of the office space, the applicant ~~will~~shall prepare an executive summary for the City of Kirkland, outlining what sustainability measures were incorporated in the tenant build-outs (unless otherwise restricted by tenant confidentiality).

e. In addition, the applicant ~~will~~shall strive to make design choices in its Core and Shell buildings that are conducive to the achievement of LEED-CI by tenants.

2. In the interest of promoting a holistic sustainability approach, the applicant ~~will~~shall strive to integrate site-specific strategies identified as focus areas, such as:

- Energy efficiency strategies, like centralized cooling options and heat recovery.
- Low Impact Development (LID) strategies like stormwater planters, vegetated roofs, and bioswales.
- Materials and resource strategies like recycled materials, regional materials, and FSC certified wood.

### D. COMMUNITY-SERVING RETAIL AND SERVICES

Include neighborhood-serving retail and services. ~~such as~~ Possible examples include: grocery, childcare, bookstore, drugstore, dry cleaner, movie theater, barbershop, shoe repair, etc.

### E. PARKING

To guide the transformation described in the Comprehensive Plan from "an auto-oriented center surrounded by surface parking into a pedestrian-oriented center integrated into the community" (CP XV.D-13), the majority of parking for the development shall be placed underground. Surface parking will be provided along selected internal streets and at other selected surface parking locations to support retail uses.

**MASTER PLAN:  
DEVELOPMENT STANDARDS**

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**NOTES ON CHANGES  
& 2008 GRAPHICS TO BE REPLACED**

**SITE AREA BREAKDOWN** · TOTAL SITE AREA = 501,000 SF = 100%

Building Footprint 40 - 45%	Vehicle Areas 20 - 25%	Open Space 35 - 40%
--------------------------------	---------------------------	------------------------

**OPEN SPACE BREAKDOWN**

Sidewalks 20 - 25% of Site	<b>Pedestrian Space:</b> Plazas/Courtyards/Gardens/Elevated Terrace 15 - 20% of site (75,000 sf minimum)	Private Roof Terrace 10,000 sf
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**BUILDING USE BREAKDOWN** · Approximate 1,175,000 GROSS SF TOTAL = 100%

Commercial Office 650,000 sf	Retail / Fitness / Entertainment 225,000 sf	Residential 250-300 units 300,000 sf
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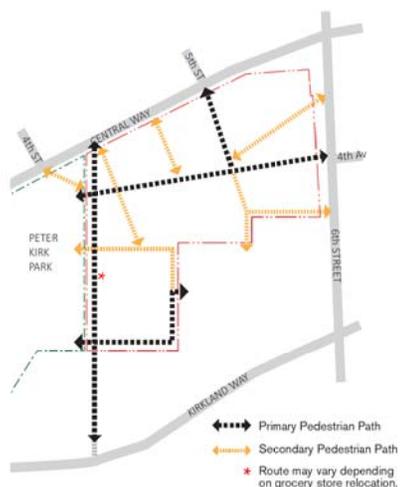
**10. Public Amenities, Access, and Organization of Uses**

**A. PEDESTRIAN CONNECTIONS**

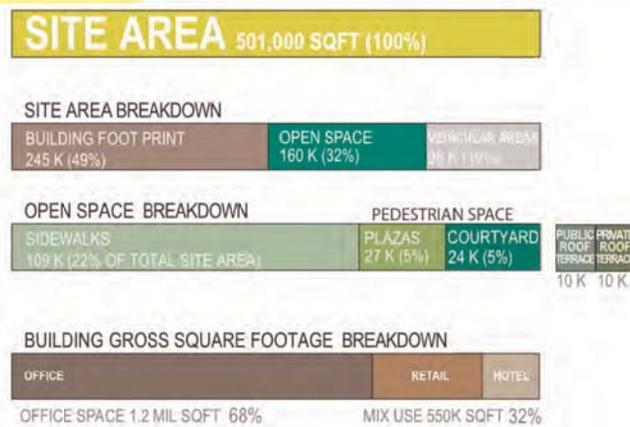
**Intent:** Create a network of identifiable linkages into and through the project site for pedestrians.

The diagram at right shows approximate pedestrian connections. Darker lines indicate primary connections required designated by the Comprehensive Plan. Lighter lines show secondary connections linking existing proposed streets as well as Peter Kirk Park. These connections are for public use.

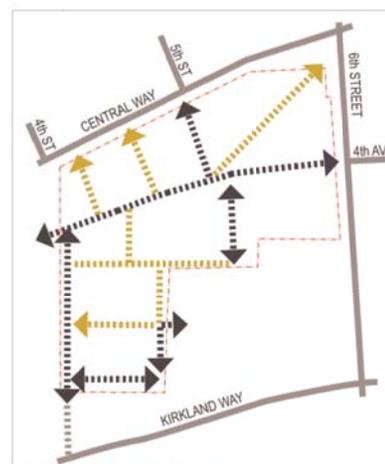
The applicant shall work with the City to define appropriate wayfinding strategies between the development and the Cross Kirkland Corridor.



Network of pedestrian connections



2008 Diagram: Area Breakdown



2008 Diagram: Pedestrian Connections

**DRAFT** APPENDIX I: ROAD MAP TO CHANGES

**MASTER PLAN:  
DEVELOPMENT STANDARDS**

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**B. RETAIL/RESTAURANT FRONTAGE**

**Intent: Encourage and contribute to the liveliness and activation of primary and secondary pedestrian paths pedestrian-oriented streets and spaces by providing retail and activating uses at ground level.**

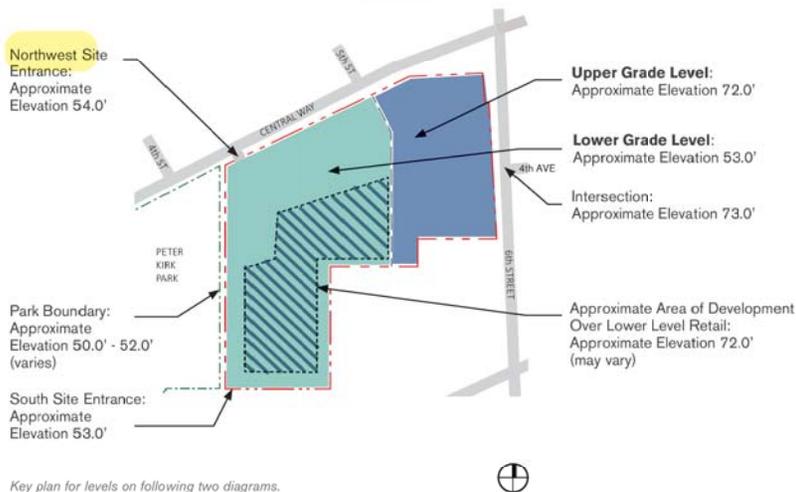
Predominant retail and other pedestrian-encouraging uses, including shops, restaurants, grocery, and a movie theater are required-encouraged along pedestrian-oriented streets and public spaces. Additional activating uses are encouraged on the ground level throughout the development where feasible.

**C. ORGANIZATION OF USES**

**Intent: Locate building and other uses to support the development goals of the project, including: ground floor retail, upper floor office space, residential space, and public gathering spaces between buildings.**

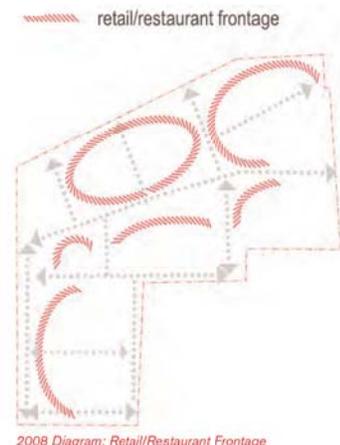
The following diagrams describe the approximate locations of various building use types, pedestrian connections, parking, and public gathering spaces.

The key plan below illustrates the two grade levels for the site: Upper Grade Level and Lower Grade Level. The Upper Grade Level relates to the existing street grades at the intersection of 6th Street and 4th Avenue. The Lower Grade Level relates to Peter Kirk Park and the grades at the northwest site entrance on Central Way.



**NOTES ON CHANGES  
& 2008 GRAPHICS TO BE REPLACED**

The Retail/Restaurant Frontage diagram is removed. The content of this 2008 diagram is folded into the Organization of Uses diagrams in Section 10.C. Retail frontage is no longer included at the northeast corner of the site. Other activating uses are encouraged there instead.



Section 10.C Organization of Uses is new to the 2014 document.

**MASTER PLAN:  
DEVELOPMENT STANDARDS**

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**NOTES ON CHANGES  
& 2008 GRAPHICS TO BE REPLACED**

**C. ORGANIZATION OF USES  
(continued)**

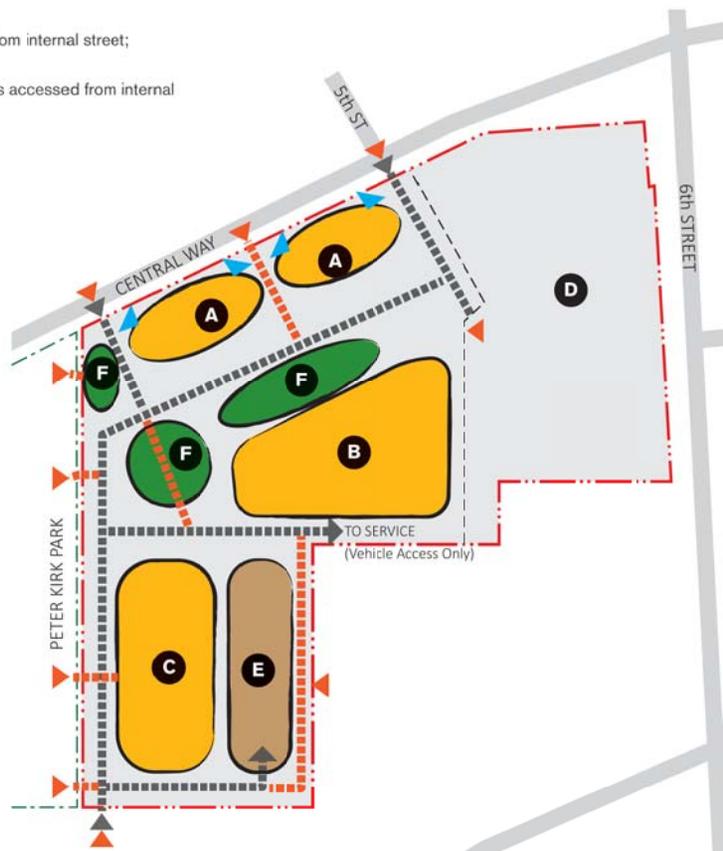
**LOWER GRADE LEVEL**

**BUILDINGS**

- A** Retail with entries accessed from internal street; Office above
- B** Retail and Grocery with entries accessed from internal street and/or open space; Residential above
- C** Retail and/or Entertainment; Office above

**SITE**

- D** Below-Grade Parking
- E** Retail Surface Parking
- F** Pedestrian Space: Plaza/Courtyard/Garden
- Vehicular and Pedestrian Circulation
- - - Pedestrian-Only Circulation
- ▲ Vehicle Site Access
- ▶ Pedestrian Site Access; Locations to be Determined
- ▶ Provide visibility into retail or other activating uses at these locations
- Retail
- Outdoor Amenity
- Parking



Section 10.C Organization of Uses is new to the 2014 document.

**DRAFT**  
APPENDIX I: ROAD MAP TO CHANGES

**MASTER PLAN:  
DEVELOPMENT STANDARDS**

**C. ORGANIZATION OF USES  
(continued)**

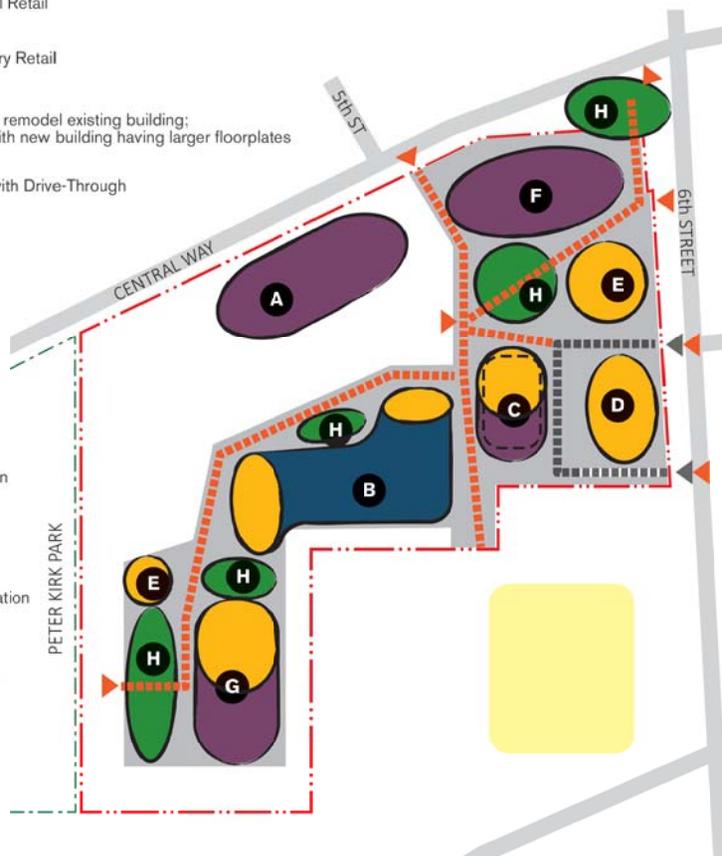
**UPPER GRADE LEVEL**

**BUILDINGS**

- A** Office over Lower Level Retail
- B** Residential with Ancillary Retail
- C** Office with Retail;  
Options: 1) Retain and remodel existing building;  
2) Replace with new building having larger floorplates
- D** Retail: Possible Bank with Drive-Through
- E** Retail
- F** Office
- G** Office with Retail

**SITE**

- H** Pedestrian Space:  
Plaza/Courtyard/Garden  
and/or  
Roof Terrace
- ..... Vehicular and  
Pedestrian Circulation
- Pedestrian-Only Circulation
- ▶ Vehicle Site Access
- ▶ Pedestrian Site Access
- Retail
- Outdoor Amenity
- Office
- Residential



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**NOTES ON CHANGES  
& 2008 GRAPHICS TO BE REPLACED**

Section 10.C Organization of Uses is new to the 2014 document.

**DRAFT**  
APPENDIX I: ROAD MAP TO CHANGES

**MASTER PLAN:  
DEVELOPMENT STANDARDS**

**D. PEDESTRIAN SPACE**

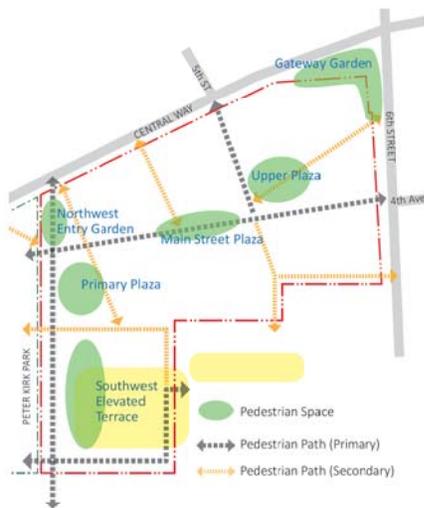
*Intent: Provide a functional and diverse pedestrian environment by creating a variety of usable pedestrian open spaces.*

The following types of public/pedestrian space are to be provided at a minimum of 15+0% of the total lot area, or 75,000 ~~50,000~~ square feet. Locations are approximate and not limited to those shown on the diagram at right below:

- a. **Central Primary plaza:** shall have a minimum area of 10+5,000 square feet with a minimum average width of 60+0 feet.
- b. **c. Atrium/breezeway- Main Street plaza:** a linear sequence of pedestrian spaces along Main Street retail shall have locations with a minimum 35-foot plaza depth from building face to curbline. (This does not include roadway. See 11.4 for building face to building face dimensional requirements along Main Street.) ~~wide separation between office floor plates.~~
- c. **Upper Plaza:** shall include a combination of landscaping and hardscaping with a minimum area of 10,000 square feet.
- d. **Northwest Entry Garden:** shall be predominantly landscaped and an extension of Peter Kirk Park.
- e. **b. Smaller courtyard/plazas:** shall have a minimum area of 2,500 square feet each. (not illustrated in diagram at right)
- f. **d. Roof-top Elevated terraces:** shall provide a minimum of 10,000 square feet total of publicly accessible pedestrian space at the Upper Grade Level. (See 10.C.) ~~rooftop terraces in one or more locations.~~

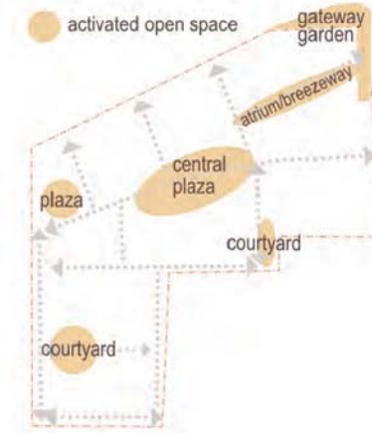
See district specific guidelines for design parameters of public space (ex. **central** plazas, Section 13.D).

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*Distribution of pedestrian spaces: along paths, between buildings, and on elevated terraces. Locations are illustrative and subject to change.*

**NOTES ON CHANGES  
& 2008 GRAPHICS TO BE REPLACED**



2008 Diagram: Pedestrian Space

**DRAFT**  
APPENDIX I: ROAD MAP TO CHANGES

**MASTER PLAN:  
DEVELOPMENT STANDARDS**

**11. Street Classification**

*Intent: Create a street and sidewalk network that responds to the existing Kirkland grid pattern, creates a pedestrian-oriented environment, and allows for direct interaction with Peter Kirk Park.*

The following street classifications and diagrams represent the various types of streets and approximate locations anticipated in the project. Final location and classification of streets may be adjusted in the final design to include such design techniques as: tight turning radii to calm traffic, curb bulb outs, textured crossings, etc. Access shall be in compliance with city codes and polices for public improvements and emergency access.

Street classifications are meant to be typical sections of the roadway. Slight variations may be necessary to accommodate driveways, street furniture, structural constraints, etc.

Planting adjacent to parking or drive lane may consist of tree wells level with sidewalk or planting strips which are flush with sidewalk or raised above sidewalk. Where tree wells occur, provide minimum 12'-0" total sidewalk width including tree wells, with minimum sidewalk width of 8'-0" and tree well width of 4'-0" (except as noted on street sections).

Where continuous planting strips are provided in lieu of street tree wells, provide minimum 10'-0" sidewalk and 4'-0" minimum planting strip (unless noted otherwise).

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⇨ Indicates Possible Access to Below-Grade Parking

**ADJACENT PUBLIC STREET IMPROVEMENTS**

- 1 Central Way
- 2 6th Street

**PRIMARY INTERNAL STREETS**

- 3 Park Promenade
- 4 Main Street

**SECONDARY INTERNAL STREETS**

- 5 Access Street at Central Way near 4th Street
- 6 Access at Central Way near 5th Street
- 7 Access at 6th Street
- 8 Upper Level Internal Street
- 9 Possible Parking/Service Access at 6th Street  
*(Dependent upon traffic study, design of Upper Level, and access to below-grade parking)*
- 10 Access Street at Southern Property Line
- 11 Parking/Service Access

**NOTES ON CHANGES  
& 2008 GRAPHICS TO BE REPLACED**



2008 Diagram: Street Classification key

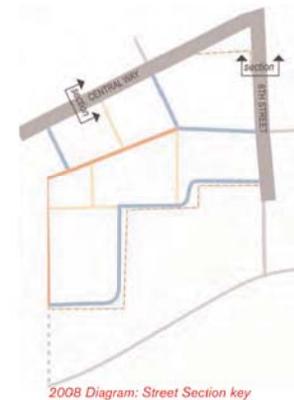
**DRAFT** APPENDIX I: ROAD MAP TO CHANGES

**MASTER PLAN:  
DEVELOPMENT STANDARDS**

**ADJACENT PUBLIC STREET  
IMPROVEMENTS**

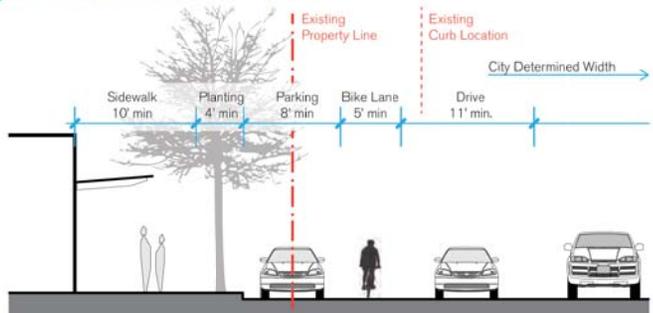
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**NOTES ON CHANGES  
& 2008 GRAPHICS TO BE REPLACED**

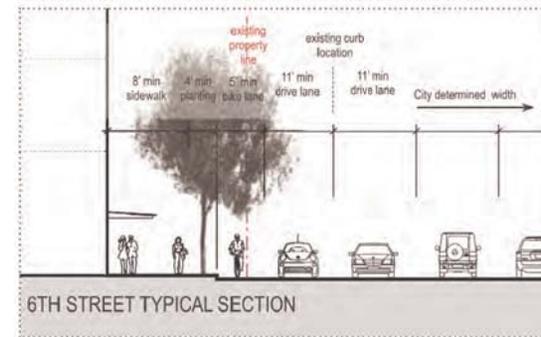
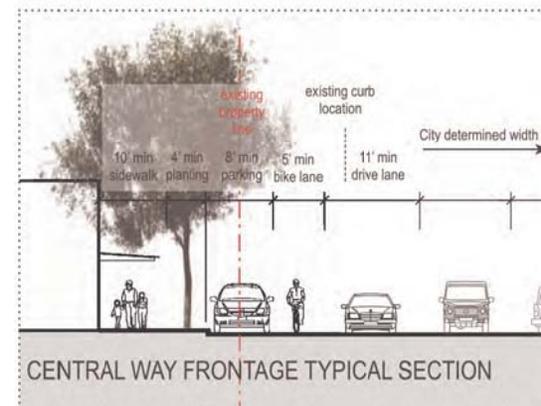
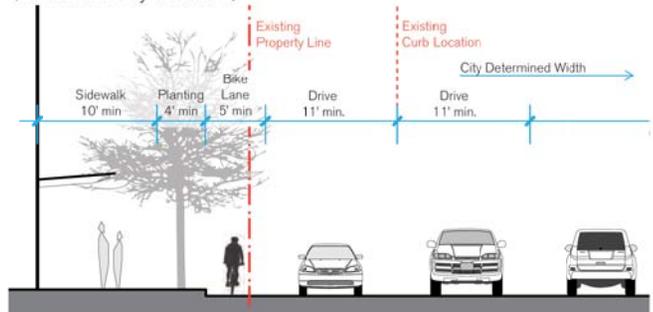


2008 Diagrams: Adjacent Public Street Improvement Street Sections

**1 Central Way Frontage Section (typical)**



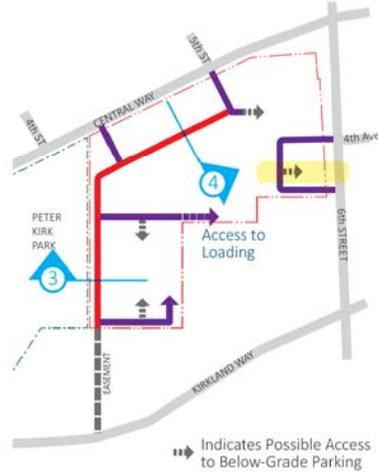
**2 6th Street Frontage Section (typical north of 4th Avenue)  
(Confirm with City of Kirkland)**



**DRAFT** APPENDIX I: ROAD MAP TO CHANGES

**MASTER PLAN:  
DEVELOPMENT STANDARDS**

**PRIMARY INTERNAL STREETS**

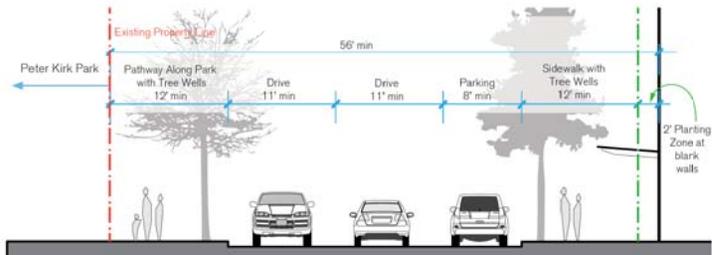


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**NOTES ON CHANGES  
& 2008 GRAPHICS TO BE REPLACED**

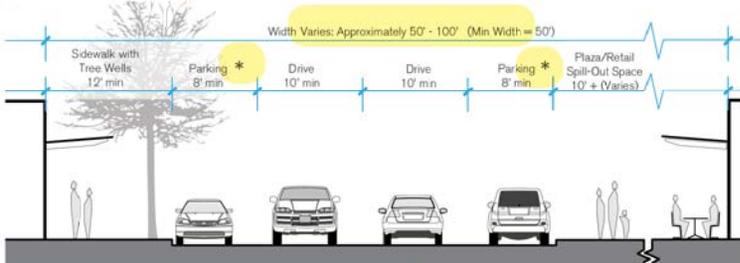


**3 Park Promenade Section (typical)**

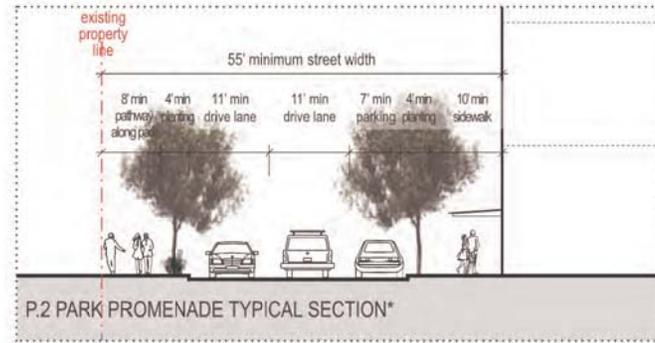


The existing easement to the south shall include a pedestrian sidewalk connecting the Park Promenade with Kirkland Way.

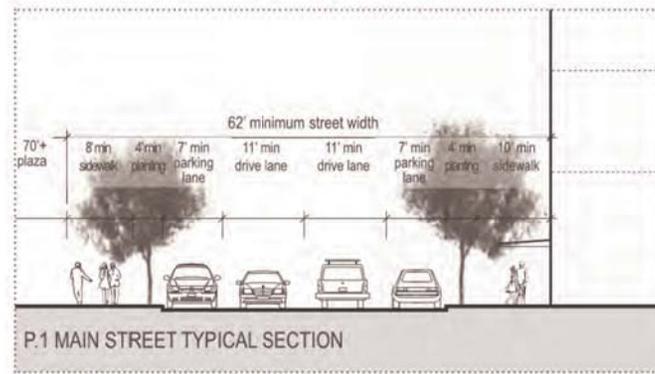
**4 Main Street Section (typical)**



\* Curbside parking may occur on one or both sides of the roadway.



\* The existing easement to the south shall include a pedestrian sidewalk connecting the Park Promenade with Kirkland Way.



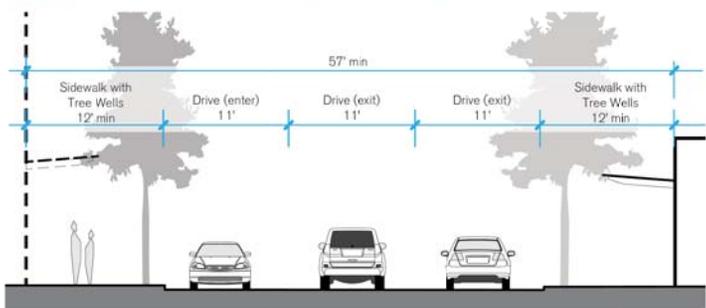
2008 Diagrams: Pedestrian Street Sections

**DRAFT** APPENDIX I: ROAD MAP TO CHANGES

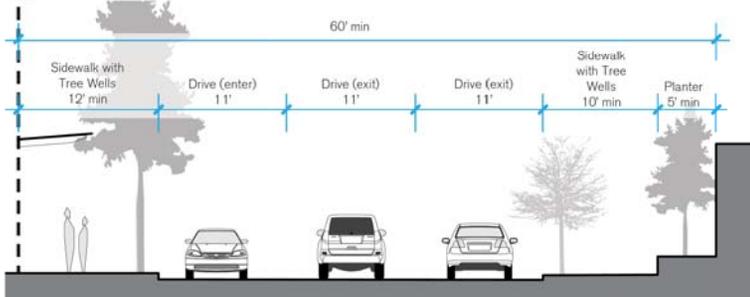
**MASTER PLAN:  
DEVELOPMENT STANDARDS**

**SECONDARY INTERNAL STREETS**

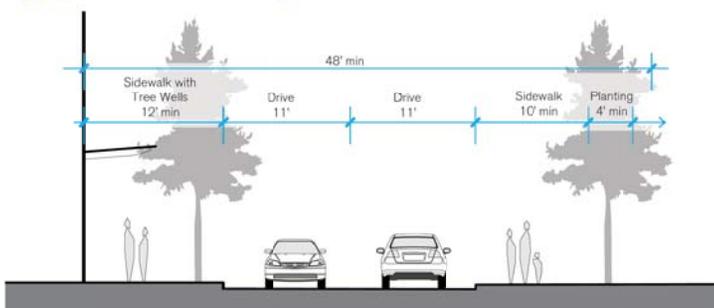
**5 7 Access Streets at Central Way, 6th Street (typical)**



**6 Access Street at Central Way and 5th Street (typical)**



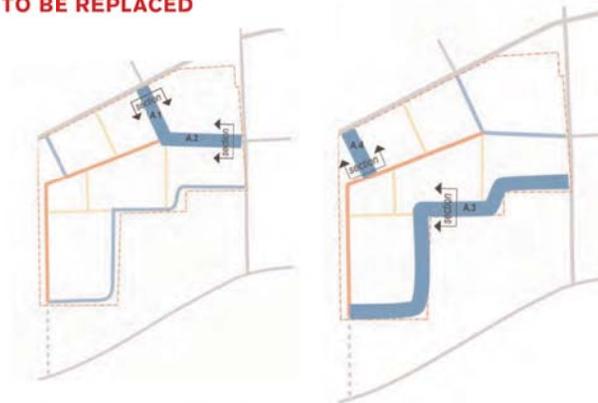
**8 Upper Level Internal Street (typical)**



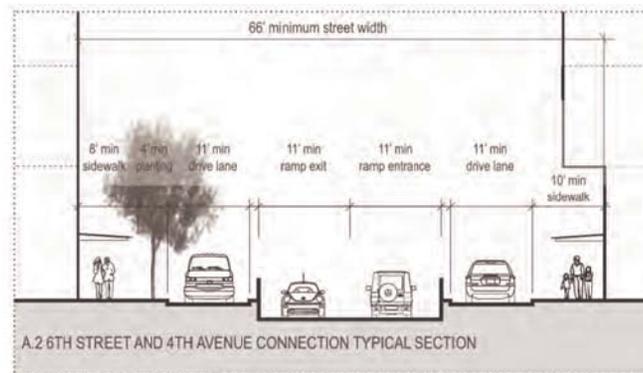
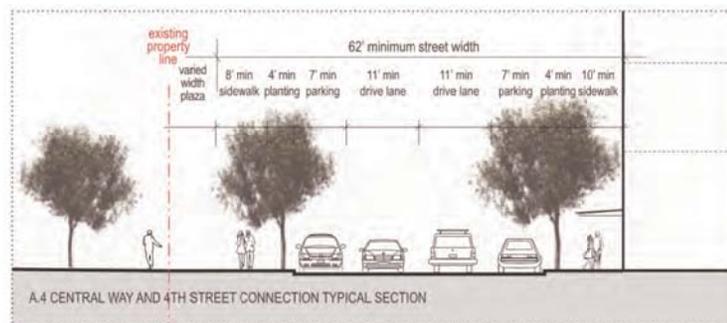
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**NOTES ON CHANGES  
& 2008 GRAPHICS TO BE REPLACED**



2008 Diagrams: Street Section keys



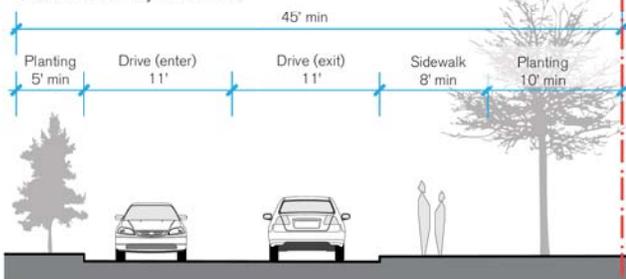
2008 Diagrams: Access Street Sections

**DRAFT** APPENDIX I: ROAD MAP TO CHANGES

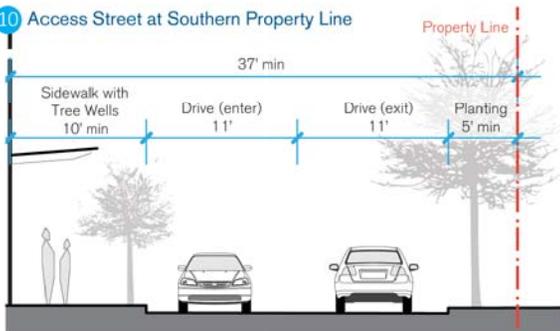
**MASTER PLAN:  
DEVELOPMENT STANDARDS**

**SECONDARY INTERNAL STREETS**

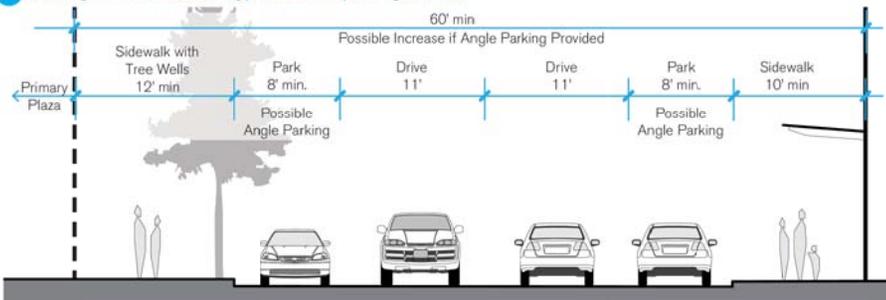
**9 Possible Parking/Service Access at 6th Street**  
*(Confirm with City of Kirkland)*



**10 Access Street at Southern Property Line**



**11 Parking/Service Access (typical where parking occurs)**

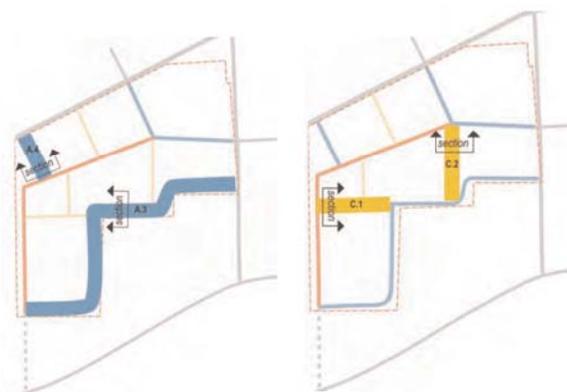


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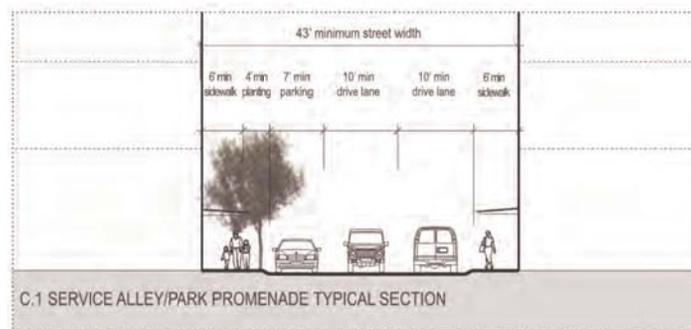


Indicates Possible Access to Below-Grade Parking

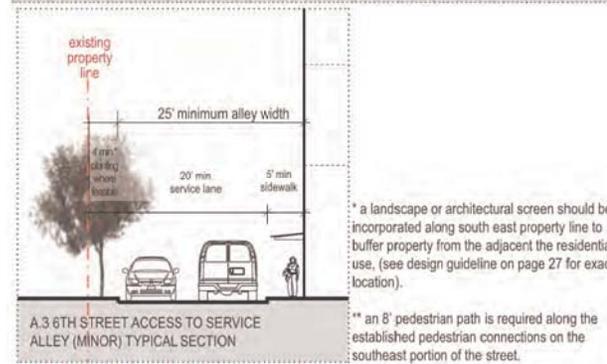
**NOTES ON CHANGES  
& 2008 GRAPHICS TO BE REPLACED**



2008 Diagrams: Street Section keys



C.1 SERVICE ALLEY/PARK PROMENADE TYPICAL SECTION



2008 Diagrams: Access/Service Street Sections

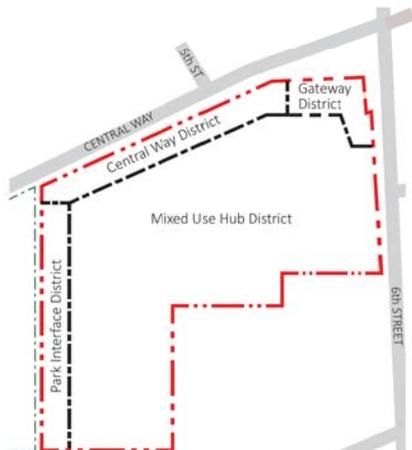
\* a landscape or architectural screen should be incorporated along south east property line to buffer property from the adjacent residential use. (see design guideline on page 27 for exact location).

\*\* an 8' pedestrian path is required along the established pedestrian connections on the southeast portion of the street.

**DRAFT** APPENDIX I: ROAD MAP TO CHANGES

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## DESIGN GUIDELINES



Key Plan: on-site district locations

The Guidelines in Section 12 apply to all districts. Section 13 identifies Guidelines that are district-specific and respond to key locations defined in the City's Comprehensive Plan as requiring special attention. These design districts are defined in the diagram at left.

### 12. Design Guidelines: All Districts

**Overall Intent:** Create a rich pedestrian-oriented environment and successful mixed-use center.

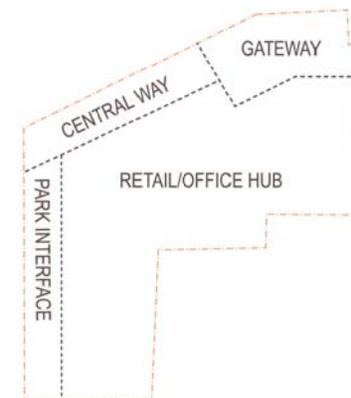
#### SITE PLANNING

##### 1. STREETScape

**Intent:** Maintain a continuous and safe streetscape with a pedestrian-friendly character.

- a. Sidewalks should maintain at least an 6-8 ft clear zone for pedestrian travel (except as noted in street sections).
- b. All streets should contribute to the physical safety and comfort of pedestrians. Provide the following where feasible to help define the sidewalk space:
  - on-street parking (see street classifications)
  - a well-defined amenity zone set to the curb for plantings, street trees, benches, trash receptacles, signs etc. (Minor deviations for street trees and major planting spaces may be necessary in some spaces due to structural constraints.) 12.1
  - wide enough sidewalk space to accommodate outdoor seating where restaurants are anticipated
- c. Use design elements such as separate storefronts, pedestrian-oriented signs, exterior light fixtures, awnings and overhangs to add interest and give a human dimension to street-level building facades.
- d. In general, buildings with active ground floor uses should be set as close as possible to sidewalk to establish active, lively uses. Maintain a continuous street wall, limiting gaps to those necessary to accommodate vehicular and pedestrian access.
- e. Encourage recessed main building and/or shop entrances consistent with a traditional "main street" design that is inviting and promotes streetscape continuity.

NOTES ON CHANGES  
& 2008 GRAPHICS TO BE REPLACED



2008 Diagram: District key plan

12.1 This note ("Minor Deviations...due to structural constraints") is at the end of the Streetscape section in the 2008 version.



Pedestrian-friendly character: on-street parking; amenity zone with street trees, signs, light fixtures; wide sidewalk to accommodate outdoor seating.

DRAFT APPENDIX I: ROAD MAP TO CHANGES

DESIGN GUIDELINES

- f. The corners of buildings located at street intersections may recess to promote visibility and allow for a collection of people.
- g. Allow larger buildings to recess from the sidewalk edge to allow for entry forecourts, provided street continuity is not interrupted along the majority of the block.

~~Minor Deviations:~~ [moved to 12.1.b.]

**2. PUBLIC SPACES: PLAZAS, COURTYARDS, TERRACES, AND SEATING AREAS GARDENS**

**Intent: Provide a friendly pedestrian environment by creating a variety of usable and interesting public and semi-public open spaces within private development.**

- a. Make plazas and courtyards comfortable for human activity and social interaction – standing, sitting, talking, eating, etc.
- b. Define and contain outdoor spaces through a combination of building and landscape. Oversized spaces that lack containment are discouraged.
- c. Establish pedestrian pathways that link public spaces to other public spaces and streets. These should be clearly identifiable for easy wayfinding.



Street bench, plantings, and recessed corner entry



Public Spaces: plazas defined by pathways and buildings include amenities such as water features, sitting spaces, landscaping, and changes in materials, colors, and textures

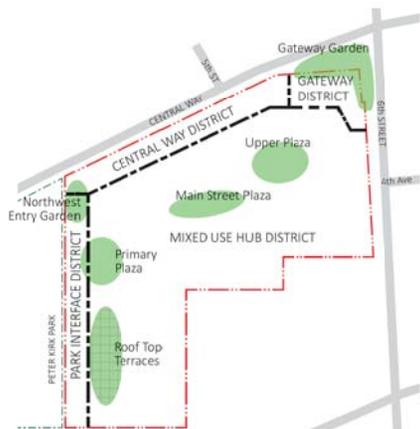


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NOTES ON CHANGES & 2008 GRAPHICS TO BE REPLACED

DRAFT APPENDIX I: ROAD MAP TO CHANGES

DESIGN GUIDELINES



Possible Organization of Pedestrian/Public Spaces as Related to Districts



Plaza with special paving, seating, planters



Pedestrian and bicycle amenities (left); Wayfinding signage and clearly defined pedestrian connections (center and right)

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- d. Plazas and courtyards should include the following:
  - planters and trees to break up space
  - seating, such as benches, tables, or low seating walls
  - special paving, such as integral colored/stained concrete, brick, or other unit pavers
  - specialty pedestrian scale bollards or other types of accent lighting
  - at least one of: public art and/or water feature
- e. Design spaces to allow for variety and individualization of temporary installations such as: lighting, banners, artwork, etc.

3. ENVIRONMENTAL CONSIDERATIONS

*Intent: Optimize pedestrian comfort using natural environmental conditions. Promote a pedestrian- and bicycle-friendly atmosphere.*

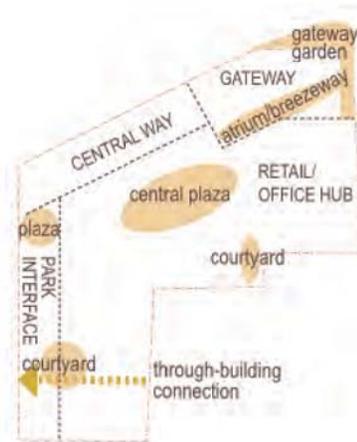
- a. Consider environmental conditions such as sun, shade, and prevailing winds when positioning courtyards and outdoor seating areas. Provide features and amenities to enhance pedestrian and bicycle access throughout the project.

4. PEDESTRIAN CONNECTIONS AND WAYFINDING

*Intent: Create a network of safe, attractive, and identifiable linkages for pedestrians.*

- a. Provide clearly defined pedestrian connections ~~shall be provided~~ at locations specified in the Pedestrian Spaces and Street Classification sections.
- b. Provide graceful grade transitions - both physical and visual - between upper grade and lower grade levels through the use of: landscaping, terraced planters, overlooking balconies, wide and inviting stairways, and other pedestrian connections.

NOTES ON CHANGES & 2008 GRAPHICS TO BE REPLACED



2008 Diagram: Pedestrian Space and Districts

DRAFT APPENDIX I: ROAD MAP TO CHANGES

DESIGN GUIDELINES

5. LIGHTING

**Intent:** Ensure that lighting contributes to the character of the project, provides personal safety, and does not disturb adjacent developments and residences.

- a. Use city-approved fixtures for street lighting along the city streets.
- b. Lighting elements throughout the project and on adjoining rights of way should be coordinated, including public open spaces, accent lighting, and streets.
- c. Accent lighting along public right-of-way should be soft in character and enrich the pedestrian street life.
- d. Accent lighting within the central pedestrian space should be congruous with the character of the project and with the arts and pedestrian space commitments. (See Section 9.)
- e. Lighting should include non-glaring design solutions, such as cut-off fixtures that avoid light spilling over onto other properties.
- f. Flood lighting of entire building facades is discouraged.
- g. Lighting on upper levels should be sensitive to Peter Kirk Park, residences, and drivers.

6. SCREENING OF TRASH AND SERVICE AREAS

**Intent:** To screen trash and service areas from public view.

- a. All service, loading, and trash collection areas shall be screened by a combination of planting and architectural treatment similar to the design of the adjacent building.
- b. Avoid wherever possible locating service, loading, and trash collection facilities in pedestrian-oriented areas.

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Integrated lighting enhances architectural character and provides pedestrian safety

Architectural and landscape elements provide screening.



NOTES ON CHANGES & 2008 GRAPHICS TO BE REPLACED

DESIGN GUIDELINES

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NOTES ON CHANGES & 2008 GRAPHICS TO BE REPLACED



7. SIGNS

Create a Master Sign Plan will be created with the city that is in keeping with the following design objectives:

**Intent: Create signs that are creative, engaging, and effective for a variety of user groups and respond to a variety of spaces.**

- a. Signsage should be complementary and integrated with the unique character of the specific areas or buildings where they are located.
- b. Signsage should be high quality and consistent with the contemporary urban character of comparable developments in similar regions.
- c. The design of buildings should identify locations, sizes, and general design for future signsage.
- d. The Master Sign Plan should include a hierarchy of elements based on function, such as:
  - site signsage for entries, wayfinding, Parkplace identity
  - building signsage for addressing and landmarking
  - tenant signsage to encourage expressive individualization

*A hierarchy of sign functions: site signs for entry and wayfinding (above, left), building signs for landmarking (below left), and tenant signs that express individual character (below center and right)*



DESIGN GUIDELINES

**BUILDING DESIGN**

**1. ORIENTATION TO THE STREET**

*Intent: Ensure that buildings contribute to the liveliness of Parkplace's public spaces, and overall community character.*

The following design treatments should apply to areas where ~~with required~~ retail frontages occur:

- a. Streets and public spaces should be enlivened by storefronts, windows, merchandise and other activity. Buildings should be designed with frequent entrances to encourage multi-tenant occupancy and walk-in traffic.
- b. Ground level retail heights should be ~~between a~~ **minimum of 14 to 18** feet in height.
- c. Entrances: Principal building entry should be visible from internal or external streets and public space. Entries should be marked by large entry doors ~~and/or~~ canopy/portico/overhang.
- d. Transparency: To help provide a visual connection between activities, ground floor facades should provide:
  - windows of clear vision glass (i.e. transparent) beginning no higher than 2' above grade to at least 10' above grade,
  - 60% minimum of facade length along Central Way, ~~P-1, P-2 and the internal Main Street,~~
  - 50% minimum of ~~retail~~ facade length along ~~A-1, A-4~~ **access streets from Central Way to the site** should provide transparency.
- e. Weather Protection: To provide pedestrians cover from weather, canopies or awnings should be:
  - a minimum of 5 feet in width unless in conflict with vehicles,
  - placed along at least 75% of facades of retail frontages, and constructed of durable materials,
  - allowed to vary in design,
  - encouraged to have continuity, minimizing gaps.



*Retail frontages with wide sidewalks, transparency, visible entries, and weather protection*

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**NOTES ON CHANGES & 2008 GRAPHICS TO BE REPLACED**

**DRAFT** APPENDIX 1: ROAD MAP TO CHANGES

DESIGN GUIDELINES



[placeholder]



Articulation, massing, and diversity to maintain a pedestrian scale.  
(middle 4)

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2. MASSING/ARTICULATION

*Intent: Create a variety of form and massing through articulation and use of materials to maintain a pedestrian scale.*

- a. In general, break down the scale and massing of larger buildings into smaller and varied volumes.
- b. All building faces should be responsive to the context of the surrounding environment and neighboring buildings.
- c. ~~Design~~ all sides of the building ~~shall be designed~~ with care (i.e. there should be no "backside" of a building.)
- d. Buildings should distinguish a "base" using articulation and materials. Include regulating lines and rhythms to create a pedestrian-scaled environment.
- e. Provide clear pattern of building openings. Windows, balconies, and bays should unify a building's street wall and add considerably to a facade's three-dimensional quality.
- f. Ribbon windows and extensive use of mirrored glass are discouraged.
- g. Employ major architectural expressions into the facade, roof form, massing, and orientation, such as tower forms, oversized windows, and entrances to demarcate gateways and intersections. Strong corner massing can function as a visual anchor at key locations within the project area. ~~See diagram for encouraged key locations:~~
- h. Building modulation should be employed to break up long facades and create a visual interest unique to each building in the project. The type of modulation should be determined by the overall design concept for each building, using dimensions from window sizes, column spacing, rain screen paneling, etc. to determine a distinct design solution.
- i. Roof Silhouettes: Express roofs in varied ways. Consider potential views of roof tops from adjacent buildings. Avoid monotonous design.
- j. Locate and/or screen rooftop equipment so that it is not visible from public spaces. Integrate rooftop screening into building's form.

NOTES ON CHANGES & 2008 GRAPHICS TO BE REPLACED



2008 Diagram: Corner treatments

DESIGN GUIDELINES

**3. BLANK WALL TREATMENTS**

*Intent: Reduce the visual impact of blank walls by providing visual interest.*

- a. Although blank walls are generally not encouraged along public streets and pedestrian spaces, there may be a few occasions in which they are necessary for functional purposes. Any blank walls longer than 20 feet should incorporate two or more of the following to provide visual interest:
  - vegetation, such as trees, shrubs, ground cover and or vines adjacent to the wall surface
  - artwork, such as bas-relief sculpture, murals, or trellis structures
  - seating area with special paving and planting
  - architectural detailing, reveals, contrasting materials, or other special visual interest



**4. ENCOURAGE HIGH-QUALITY DESIGN**

*Intent: Ensure that all buildings in the project area are constructed as a quality addition to the Kirkland Community.*

- a. Exterior architectural design and building materials should exhibit permanence and quality appropriate to an Kirkland's urban setting.



**5. BUILDING DIVERSITY**

*Intent: Ensure that project buildings in the project are distinct and respond to the unique character of their specific function and location.*

- a. Buildings should be designed to integrate with each other, while demonstrating architectural diversity. Buildings should be responsive to each specific district and its site conditions.
- b. Materials should be selected to integrate with each other and to help provide a richness of architectural diversity.
- c. Windows should incorporate variation of patterning between buildings.



Vegetation, art, and screening provide visual interest at blank walls (center image<sup>6</sup>)

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& 2008 GRAPHICS TO BE REPLACED

DRAFT APPENDIX I: ROAD MAP TO CHANGES

DESIGN GUIDELINES

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& 2008 GRAPHICS TO BE REPLACED

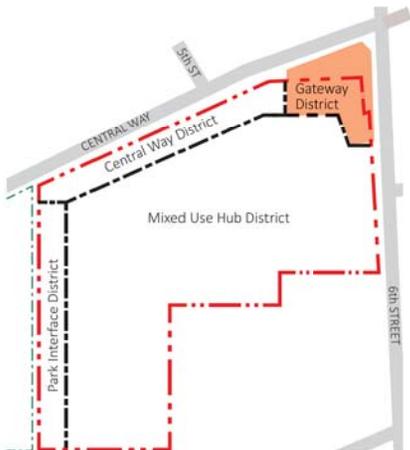
13. Design Guidelines:  
District-Specific

A. GATEWAY DISTRICT

*Intent: Create a welcoming feature to Parkplace and to downtown Kirkland. This area should create an inviting entryway that is representative of the community through the use of art, landscape, and architecture.*

SITE PLANNING

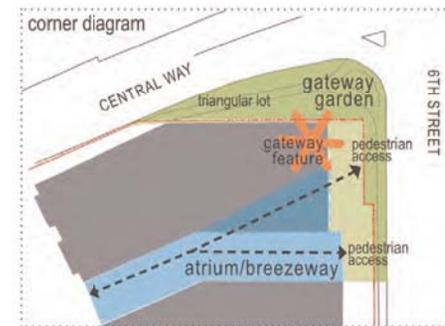
1. Incorporation of Triangular Lot "Gateway Garden": Incorporate the northeast triangular lot (excess right-of-way) into the project design to create a distinct gateway entrance that is integrated with the Parkplace development. Include:
  - a. Public Access: Public access into the site should be visible and accessible from the corner of 6th Street and Central Way.
  - b. Hardscape/Vegetation: Paving and landscaping materials should identify pedestrian spaces and access.
  - c. Trees and Other Planting: Landscaping should be of appropriate scale and species to make a significant gateway gesture. Trees should be selected to provide visibility of businesses and maintained to encourage proper growth and height.
  - d. Signage (downtown entry): Incorporate wayfinding signage directing visitors to Downtown, Peter Kirk Park, Waterfront/Marina, City Hall, and Civic District.
2. Public Space Connecting to Triangular Lot: Design of additional public space should be integrated with the triangular lot to provide a congruous pedestrian environment.
  - a. Public Access: Connect pedestrian access to the gateway garden, adjacent streets, and public open spaces.
  - b. Hardscape/Vegetation: Paving and landscaping materials should identify pedestrian spaces and access.
  - c. Seating: Incorporate seating along pedestrian pathways and gathering spaces.
  - d. Artwork: Incorporate art in an appropriate scale to distinguish the significance of this corner.
3. Atrium/Breezeway Space Pedestrian Connection: Create a pedestrian connection from the corner of 6th and Central into the heart of the project. (See Section 10.A.) The atrium/breezeway space This connection will include the following:
  - a. Covered walkway Pedestrian weather protection
  - b. public connection from 6th to central plaza the interior of the site open during regular operating hours
  - c. pedestrian lighting
  - d. seating



Key Plan: Gateway District



Distinct corner treatments: provide identity for the development and integrate pedestrian hardscape, landscaping, seating, and art



2008 Diagram: Gateway corner

DRAFT APPENDIX I: ROAD MAP TO CHANGES

DESIGN GUIDELINES

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NOTES ON CHANGES & 2008 GRAPHICS TO BE REPLACED

and may consider including:

- a. enclosed public space
- b. retail/restaurant uses
- c. covered play/activity space

4. Buildings should be separated from or differentiated from each other at this corner so that they are not perceived as one building.

**BUILDING DESIGN: BUILDING AS GATEWAY**

1. Ground Level Treatment

- a. Setbacks from Streets - The ground floor levels of the corner building should be permitted to set back to allow for cut away view and obvious pedestrian connection into the site.
- b. ~~Active and Inviting Retail/Restaurant Uses~~ Design for an engaging pedestrian experience ~~retail and restaurant uses~~ along ground floor of the building.
- c. Details Visible at Different Movement Speeds - Incorporate details in the building along the corner that bring visual interest at the pedestrian level, as well as for vehicular traffic entering Kirkland.

2. Upper Levels

- a. Change of Expression/Material Choices: A clear visual division between upper and lower floors should be incorporated through a change in materials, colors, and forms.
- b. Modulation and Building form: Modulation and shifts in the building mass should be incorporated to decrease the apparent bulk of the building at the corner of Central Way and 6th street. Modulation of building facades should include setting back portions of the building in order to reduce the apparent length. The buildings should respond to the corner condition by shifts and/or angles in the building floor plate.
- c. Upper Level Step backs-~~The upper level should step back significantly from the floor below to reduce the apparent height of the building at the intersection of Central Way and Sixth Street. The step backs should be a minimum of 5 feet as measured from the face of the major portion of the building floor below. A modulated step back should be incorporated after the third level (approximately 50') on building facade along Central Way. This step back can vary in depths from 0-10 feet, so long the upper levels of building appear to be receding from the base. Step backs are measured from the exposed face of the building above grade, not from any property line.~~
- d. Top Floor/Roof Edge: should have a distinct profile against the sky through elements such as projections, overhangs, cornices, step backs, trellises, changes in material, or other elements.
- e. Accent Lighting: The innovative use of accent lighting incorporated into the building facade is encouraged. Lighting should include non-glaring design solutions such as cut off fixtures that avoid light spilling over onto other properties. Flood lighting of entire building facades is discouraged.



Building modulation, clear visual distinction between upper and lower floors, and details visible at different speeds

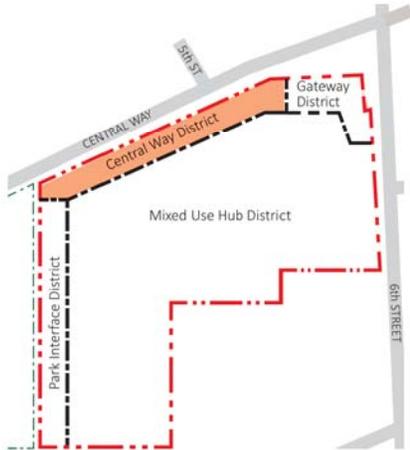


Ground floors set back to provide pedestrian connection to site



Top floor/roof edge provides distinctive profile

DESIGN GUIDELINES



Key Plan: Central Way District



Pedestrian-only connection from Central Way to interior streetscape/plaza on opposite side?



Building corners articulated with glazing, canopies, and special paving

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B. CENTRAL WAY DISTRICT

*Intent: Respond to Central Way as a major arterial linking downtown Kirkland with areas east and beyond. Parkplace must take advantage of this traffic volume to help create a multi-functioning, pedestrian-scale shopping-street that brings visual activity to the street edge.*

SITE PLANNING

1. Encourage connections and activate the street edge by incorporating:
  - on-street parking along Central Way
  - buildings located up to the edge of the sidewalk
  - storefront and hotel entrances
  - visibility into buildings in order to engage pedestrian interest
  - generous sidewalk amenity zone (trees, lights, benches)
  - street tree selection and spacing that provide visual continuity, buffer pedestrians from the busy street, and allow visibility of retail
  - pedestrian signage
2. Reduce the length of street wall by pulling back portions of the building at ground level from the street edge in key locations provided street continuity is not interrupted.
3. ~~The limited vehicular access to mid-block connection may be accessed.~~ Include a pedestrian-only connection from Central Way and into the interior of the project. Pedestrian access along this route should include pedestrian-scaled lighting and a clear connection to the streetscape/plaza space on the opposite side.
4. Activate building corners with visibility into retail and/or other inviting design features, as denoted on Organization of Uses diagram (page 10).

NOTES ON CHANGES & 2008 GRAPHICS TO BE REPLACED

DESIGN GUIDELINES



Upper levels set back from base at western edge of office building; ample glazing and canopies enhance pedestrian experience<sup>8</sup>



Two-story pedestrian pass-through to promote physical and visual connections, and to reduce apparent building bulk at grade level

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BUILDING DESIGN

1. Reduce apparent bulk of buildings along Central Way by incorporating elements such as step backs and modulation, along with shifts or angles in the building mass. Differentiate the upper portion of the building from the lower by setting the upper floors back from the building base on the western and eastern ends of the building. The step backs should create roof terraces that overlook Central Way and the interior of the site. a 20-foot upper-level step back after the third story along the majority of the facade. However in places, step backs can vary in depths from 0-20 feet, so long as the overall upper levels of building appear to be receding from the base. Step backs are measured from the exposed face of the building above grade, not from any property line.
2. The upper floor of buildings facing Central Way should step back from the floors below and incorporate a change in materials or expression to clearly differentiate the upper floor and reduce the overall visual impact of the building.
3. Facades that are stepped back should be distinguished by a change in elements such as window design, railings, trellises, details, materials, and/or color so that the result is a richly organized combination of features that face the street.
4. Provide a two-story pass-through at grade to break up the length of the building base fronting Central Way. The pass-through should be of sufficient height and width to provide views into the "main street" retail, creating a prominent and attractive visual and physical connection to the interior of the development. Balconies, terraces, and landscaping are encouraged in upper-level step backs.



Breaks in building mass reduce apparent bulk of building<sup>9</sup>

NOTES ON CHANGES & 2008 GRAPHICS TO BE REPLACED

DESIGN GUIDELINES

C. PARK INTERFACE DISTRICT

*Intent: Create a strong connection from the park and downtown core that allows for clear pedestrian flow to and into the site by incorporating engaging building frontages, plazas, gardens, and other design treatments.*

SITE PLANNING

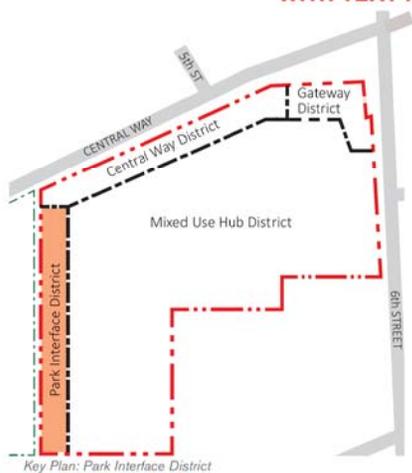
1. Incorporate ample landscaping and distinctive lighting.
2. Incorporate raised crosswalks 20' minimum in width and special paving to promote pedestrian priority along the north-south street bordering the park.
3. Encourage retail spill-out spaces and landscaped courtyards along the building edge. Bring the "indoor" out and "outdoor" in by spilling retail spaces onto the sidewalk and creating small gathering spaces along building edges.
4. Create a visual barrier for drivers between the drive lane and pedestrian walkway along the Peter Kirk Park edge using one or more elements such as: plantings, bollards, small seating walls, stone artwork, etc.
5. Carefully consider views from the park. This includes reducing apparent bulk and mass of building(s) facing the park.

BUILDING DESIGN

1. Buildings shall address park and street by incorporating:
  - terraces and balconies
  - entrances to retail along promenade
  - greater transparency at ground floor or planting zone and/or canopy at edge of buildings where transparency is not feasible, such as theater facades.
  - street front courtyards
  - retail spill-out spaces
2. Where feasible, provide rooftop terraces on lower roof levels as gathering spaces that include amenities such as:
  - seating
  - landscaping
  - canopies or coverings for weather protection
  - public access open during regular operating hours
  - retail/food service where appropriate

NEW DOCUMENT WITH TEXT MARK-UPS

NOTES ON CHANGES & 2008 GRAPHICS TO BE REPLACED



Safe, clearly marked, pedestrian-friendly crosswalks



Pedestrian-oriented park interface: trees, clear markings, ground floor retail, balconies<sup>10</sup>

DESIGN GUIDELINES

D. MIXED USE HUB

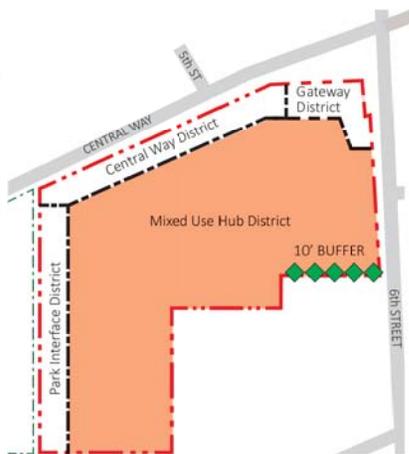
*Intent: To establish a vibrant Mixed Use Hub with activated public space and retail/window shopping experience with a mix of uses, both connected to and overlooking a common central the Main Street plaza, primary plaza, and Peter Kirk Park.*

SITE PLANNING

- The central plazas space should be integrated visually and physically with their surroundings, and should provide significant gathering and activity spaces by incorporating the following:
  - special paving
  - water feature(s)
  - special landscaping
  - seating: covered and open
  - distinct lighting
  - access to sunlight
  - accommodations for concerts/performances
- The plaza space Plazas should be supported as important activity spaces by surrounding them with active public-oriented amenities such as ground floor retail, restaurants, and cafes.
- Locate plazas at or near street grade to promote physical and visual connection to the street and adjacent buildings and their entrances.
- Design outdoor space with safety in mind; public plazas should promote visibility from the street and provide architecturally compatible lighting to enhance night time security
- A ten foot permanent landscaped edge along the southeast property line adjacent to residential uses should be incorporated within the street design. (See diagram at right.)
- The district should also consider providing:
  - small retail pavilion(s)
  - children's interactive feature
- A through building pedestrian connection on the southeastern portion of the site should be provided and include:
  - through public 24-hour access during normal business hours
  - connection to Peter Kirk Park
  - pedestrian weather protection and wayfinding signs to help guide pedestrians through parking lot and around the building.
  - gracious entries on both sides of the building
  - the design of the interior space should feel public and accommodating, so users do not feel that they are intruding into private space.

NEW DOCUMENT WITH TEXT MARK-UPS

NOTES ON CHANGES & 2008 GRAPHICS TO BE REPLACED



Key Plan: Mixed Use Hub District showing buffer at southeast property line



Pedestrian courtyards framed by retail use<sup>11</sup>

DESIGN GUIDELINES

BUILDING DESIGN

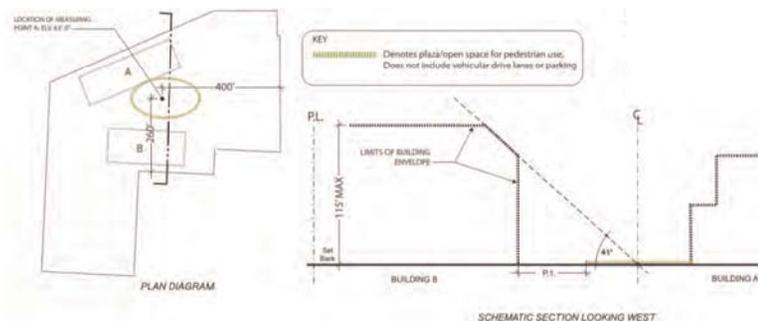
- Lower level facades with predominantly retail uses should locate entrances at the sidewalk or edge of public space to frame pedestrian spaces in key locations.
- Where feasible, provide rooftop terraces on lower roof levels as gathering spaces that include such amenities as:
  - seating
  - landscaping
  - canopies or coverings for weather protection
  - public access open during regular operating hours
  - retail/food service locations
- In order to maximize the amount of sunlight in the central primary plaza, **building B** as depicted in the diagram below buildings to the south should be contained under a line at a 41 degree angle measured from the center of the plaza per diagrams below.
- Buildings located in the southern most portion of the site should provide generous and substantial modulation in response to their proximity to neighboring buildings including:
  - creating varied edges and visual interest on long and tall buildings
  - employing modulation to visually break up long facades
  - providing patterns of windows, bays, and/or balconies that emphasize changes in modulation



Plazas providing significant gathering and activity space, framed by buildings<sup>©book 13, below 13</sup>

NEW DOCUMENT WITH TEXT MARK-UPS

NOTES ON CHANGES & 2008 GRAPHICS TO BE REPLACED



2008 Diagrams: Solar access at Plaza

**REMOVE DIAGRAMS:** The primary plaza in the new development will be closer to Peter Kirk Park and has no large buildings directly to the south that would impede solar access. The 41° angle requirement will still apply, but will be easily met and requires no diagram.

The Notes section is new to the 2014 document.

Notes

IMAGE CREDITS

CREDITS IN PROCESS

The following sources were used for end-noted images. All other images and illustrations are provided by CollinsWoerman.

- Kirkland, WA. Map. *Google Maps*. Google, 6 Aug 2014. Web. 6 Aug 2014.
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THE CITY OF KIRKLAND

# Kirkland Parkplace Mixed-Use Development

## Master Plan and Design Guidelines

# Appendix II: **DRAFT** Summary of Key Changes from Existing 2008 Version to Proposed New Version

Prepared by CollinsWoerman  
January 20, 2015

**For Review:** Content changes since Dec 3, 2014 version are in highlighted box

SECTION New Version	PAGE # New Version	PAGE # 2008 Version	CHANGE IN PROPOSED NEW DOCUMENT FROM 2008 VERSION
1	3	PO-3	Reduce: Development area from 1.75 million s.f. to 1.175 million s.f. Replace: Hotel and sports club uses with residential and entertainment uses. Add note for option of Property Owner to re-name the site.
5	4	PO-4	Add clarification: Planning Director reviews minor modifications.
6	4	PO-4	New phasing zones. Add: text to ensure no "rough edges" at the completion of each phase, including functional access and circulation, and that demonstration of this requirement will happen during Design Review.
7	5	PO-3	This section in the new document contains information that was included in the 2008 document in Section 1. Add: Comprehensive Plan policy regarding Residential Use and corresponding Master Plan response.
8	6	PO-5	Add: "Provide connections to Peter Kirk Park" Add: Guiding Principle #9: <i>Mixed Use Development</i> to reflect addition of Residential component.
9.A	7	MP-6	Increased Pedestrian Space requirement: from 50,000 to 75,000 s.f.
9.C	7	MP-6	Replace: green building requirements for hotel with green building requirements for residential (LEED for Homes: Multifamily Mid-rise; or Built Green 4 Star).
9.E	7	N/A	Add: parking program requirement: majority of parking is underground with surface parking at select locations to support retail uses.
9	8	MP-6	Revise: Area Breakdown Chart, including Site Area Breakdown, Open Space Breakdown (plazas and courtyards are combined for simplicity), and Building Gross Square Footage Breakdown. See chart for details.

SECTION New Version	PAGE # New Version	PAGE # 2008 Version	CHANGE IN PROPOSED NEW DOCUMENT FROM 2008 VERSION
10.A	8	MP-7	Revise: Pedestrian Connection diagram to reflect current development scheme. Add: note that owner shall work with the City to define appropriate wayfinding strategies between the development and the Cross Kirkland Corridor.
10.B	9	MP-7	Remove: Retail Frontage diagram. The content of the 2008 version's Retail Frontage diagram is partially folded into the new <i>Organization of Uses</i> diagrams. See Section 10C. Retail is no longer anticipated at northeast corner of site. (See changes noted for 13.A: <i>Gateway District Design Guidelines</i> .)
10.C	9-11	N/A	Add New Section: "10.C. <i>Organization of Uses</i> " describes overall layout of site including: building uses, vehicular and pedestrian access and circulation, parking, and pedestrian space.
10.D	12	MP-7	Increased Pedestrian Space requirement: 75,000 s.f. Revise: Pedestrian Space diagram reflects new development plan. Replace: Atrium Breezeway requirement with requirements for Main Street Plaza, Upper Plaza, and Northwest Entry Garden. Atrium Breezeway no longer included due to wider spacing of office buildings. Add: further definition of particular pedestrian spaces.
11	13-17	MP-8-14	New street sections correspond with new street layout. See street classification and street section diagrams for details.
12	19	DG-16	Change "Public Places" heading to correspond with other sections: "Public Places: Plazas, Courtyards, Terraces, and Gardens".
12	20	DG-17	Add: Design Guideline for graceful grade transitions.
12	23	DG-19	Remove: transparency requirement for 60% of facade facing Park Promenade due to possible entertainment uses in this location. Change: Retail height to 14' minimum, from 14'-18'. (It is anticipated that some retail/entertainment heights will be taller than 18'-0".)
12	24	DG-20	Remove: diagram of key locations for visual anchors.
13.A	26	DG-23	Replace: <i>Atrium Breezeway</i> with <i>Pedestrian Connection</i> .
13.A	27	DG-24	Replace: retail/restaurant uses at ground floor at Gateway District with more general "active and inviting" requirement. It is anticipated that office or office-related amenity uses would occupy the majority of the northeast corner. New Upper Level building design requirements at Gateway District: new "Modulation and Building Form" requirement; revised step back requirement.
13.B	28-29	DG-25	Replace: "Storefront and hotel entrances" along Central Way with "Visibility into Buildings". No hotel is planned. Retail will have primary access from internal street. New Guidelines for reducing apparent bulk of buildings along Central Way: step backs, modulation, shifts or angles in building mass, two-story pass-through into interior of site.
13.C	30	DG-27	Add: guideline to "carefully consider views from the park." Add: planting zone and/or canopy at edge of buildings facing Peter Kirk Park where transparency is not feasible (due to potential entertainment uses). Add: retail/food service at rooftop terraces.

DRAFT

SECTION New Version	PAGE # New Version	PAGE # 2008 Version	CHANGE IN PROPOSED NEW DOCUMENT FROM 2008 VERSION
13.D	31	DG-28	Primary Plaza location shifts to western area of site, so name "Central Plaza" is no longer used. For this reason, language that referred to "Central Plaza" in 2008 version has been replaced with "Primary Plaza" or "Plazas" generally.
13.D	31	DG-28	The east-west pedestrian connection at the southern portion of site is not required to pass through a building. Instead, there is a requirement for pedestrian weather protection and wayfinding signs along the pedestrian connection.

