



CITY OF KIRKLAND
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MEMORANDUM

To: Planning Commission

From: Janice Coogan, Project Planner
Paul Stewart, AICP, Deputy Director

Date: April 19, 2011

Subject: LAKEVIEW NEIGHBORHOOD PLAN-FOLLOW UP ON REMAINING ISSUES,
FILE ZON07-00032

RECOMMENDATION

Discuss and provide direction to staff on the following remaining issues related to the Lakeview Neighborhood Plan Update:

1. Draft Design Guidelines for the Yarrow Bay Business District. Staff will also brief you on comments received from the Houghton Community Council at their April 25, 2011 meeting.
2. The number of subareas for the Yarrow Bay Business District and proposed YBD use zone chart regulations.

It may be helpful for you to bring your packet from your April 14, 2011 meeting for reference, or use this link to see an online copy: http://www.kirklandwa.gov/depart/Planning/Planning_Commission.htm

BACKGROUND DISCUSSION

Status of Neighborhood Plan Process

At your last study session on April 14, 2011 you reviewed the preliminary recommendation from the Houghton Community Council on the draft Lakeview Neighborhood Plan and related draft code amendments. Overall the Planning Commission is in agreement with and supports the draft policies and code amendments. Below is a summary of your comments from the meeting that will be forwarded to the Houghton Community Council on April 25, 2011:

- **South Houghton Slope** rezone from RS 12.5 to PLA 3A, Policies L-3.4 and 3.6-
The Commission recommended the name of the zone be changed to PLA 3C to avoid confusion with the PLA 3A zone to be reclassified. You generally support rezoning the areas and agreed to leave the density option open at 6 or 7 dwelling units per acre until after the public hearing. You support the proposed development standards.

- **Small lot single family provisions**, Policies L-4.1 and 4.2 -
You support allowing these provisions in the RS 7.2 and RS 8.5 zones and for the smaller lots incorporating a floor area ratio.
- **PR Zone changes**, Policies L-6.3, 6.4 and 6.5-
There is consensus among the Planning Commission to support the rezone of the block located east of the alley between NE 59th ST and NE 60th ST from RM 3.6 to PR 3.6. You support allowing pedestrian oriented businesses in the PR 3.6 zone. Proposed zoning limits the size and prohibits these uses to face or take access from Lakeview Dr. A remaining issue to receive feedback on from the Houghton Community Council is which review process will be required for redevelopment of the old historic post office buildings (Process I, IIA or IIB).
- **Yarrow Bay Business District**, Goal L-7, Policies L-7.5 – 7.9
As part of the YBD 1 Park and Ride code amendment discussion, the Commission agreed to eliminate the size limitation for retail or grocery store uses in YBD 1. You also questioned whether there should be a size limitation of 15,000 sq. ft. for the rest of the business district (see discussion below). You discussed whether there should be 2 or 4 subareas to differentiate the types of uses, required yards and building height between the blocks in the district (see discussion below).
- **Nonconforming Density Policy** L-4.4-
The Commission disagrees with the Houghton Community Council on this policy. You believe non-conforming density should not be treated differently in Houghton than the rest of the City. You stated the existing non-conforming provisions should be consistent throughout the City and therefore the policy should be deleted.
- **Parks and Open Space Policy** L-9.3-
While the Commission understands the concerns raised by this policy of keeping views open at public waterfront parks, you had concerns that including this policy would result in regulating view protection from private property which the city does not regulate. Protecting private views is not provided elsewhere in the Comprehensive Plan. The Commission discussed the conflict between this policy and the goals of restoring the shoreline with the planting of vegetation in the shoreline regulations. You suggested this policy be deleted.

Staff will in turn provide you with the Houghton Community Council's response to the above comments at your April 28, 2011 meeting. The remaining topic for both the Houghton Community Council and Planning Commission are the Design Guidelines for the Yarrow Bay Business District discussed below.

I. Design Guidelines for the Yarrow Bay Business District

The Draft Design Guidelines for the Yarrow Bay Business District are enclosed in Attachment 1. The guidelines cover the YBD 2 and PR 8.5 zones (see discussion below regarding the number of subareas). Once the code amendments and design guidelines for the YBD 1 at the South Kirkland Park and Ride are adopted, the Design Guidelines for YBD 1 will be incorporated into this document. The Design Review Board will use these new design guidelines to evaluate development proposals in the District.

The Yarrow Bay Business District Guidelines are modeled after guidelines for the Totem Lake and Rose Hill Business District. The Yarrow Bay Business District is not considered a "Pedestrian Oriented Design

District” which is typically used for areas of the city where retail activity is the primary goal and mandatory on the ground floor. The City’s pedestrian oriented design districts are the CBD, Juanita, North Rose Hill and Totem Center. While not classified as a “Pedestrian Oriented Design District”, the YBD Guidelines include many pedestrian design elements to be incorporated into the design of projects to improve the pedestrian experience and encourage walking within the District.

Below is a summary of the design guidelines:

- The guidelines establish recommendations for building and site design such as:
 - building placement within the district blocks
 - how to address architectural scale
 - horizontal and vertical modulation
 - options for mitigating the visual impacts of parking structures
 - techniques to encourage greater pedestrian oriented development
 - detail elements of building materials, lighting and signage
- Development is encouraged to orient to adjacent streets or along east/west pedestrian walkways shown on Plate 34 in Attachment 2. A continuation of the public pedestrian walkway from the Linbrook property along the stream corridor is encouraged.
- Two gateways are encouraged at the intersection of Lake Washington Blvd and NE 38th PI and one on 108th Avenue NE at the South Kirkland Park and Ride lot.
- YBD zoning would allow retail uses to have a zero setback at street level along both sides of NE 38th PL. The minimum ground floor story height would be 13’ to support retail uses at the ground floor (see Attachment 6).

Houghton Community Council Comments

Staff will brief you on comments received from the Houghton Community Council on the Design Guidelines for the Yarrow Bay Business District and miscellaneous code amendments discussed at their April 25th meeting.

- *Do you support the Design Guidelines or have any recommendations for changes?*

II. Yarrow Bay Business District Subareas and Zoning

At your April 14, 2011 study session, you asked staff to explore dividing the District into 4. The map in Attachment 3 shows 4 YBD subareas, generally, one for each block in addition to the YBD 1 zone at the South Kirkland Park and Ride lot and the PR 8.5 zone. Attachment 4 is a chart that compares how each subarea would have the same permitted uses and building height (YBD 1 is excluded because you are discussing this under a separate code amendment process). Notice that the only distinguishing factor would be the front yard setback depending on the street and use. The Commission should discuss if there are other factors that would merit breaking this up into distinct subareas.

If all the blocks will have the same permitted uses and same maximum building height, staff recommends the district be divided into only two YBD subareas (YBD 1 and YBDS 2) as discussed

previously and shown in Attachment 5. The draft use zone chart for YBD 2 is included in Attachment 6. The different front yard setbacks for the different streets are listed. Building height would be measured based on average existing grade and as a result buildings would follow the elevation of the land.

In addition, you asked staff to provide a sense for how the district could redevelop in the future. Attachment 7 shows conceptual redevelopment of sites throughout the district prepared by MAKERS Architects. The Houghton Community Council discouraged the use of these illustrations in documents because they were too conceptual with very little building modulation.

- *Please provide direction to staff on how many subareas there should be and what the distinguishing factors are between them so that staff may prepare the draft zoning use zone charts.*

III. Next Steps

This is the last scheduled Planning Commission study session before the public hearing tentatively scheduled for June 23rd, 2011. We are still tentatively planning to have this be a joint public hearing with the Central Houghton Neighborhood Plan project depending on how that process plays out. One more study session with the Houghton Community Council is scheduled on April 25 to discuss the design guidelines.

ATTACHMENTS:

1. Draft Design Guidelines for YBD
2. Plate 34 pedestrian connections
3. YBD map with 4 subareas
4. Chart comparing regulations for 3 YBD subareas
5. YBD map with 2 subareas
6. Draft YBD 2 use zone charts
7. MAKER's conceptual graphics

cc: File ZON07-00032

Yarrow Bay Business District Design Guidelines

Revised Draft 4-14-2011

Design Guidelines for Yarrow Bay Business District

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Introduction

This document sets forth a series of Design Guidelines, adopted by Section 3.30 of the Kirkland Municipal Code that will be used by the City in the design review process for development in the Yarrow Bay Business District located in the Lakeview Neighborhood. The boundaries for the Yarrow Bay Business District include the YBD 1, YBD 2, and PR 8.5 zones. Other documents that should be referred to during design review are the YBD (Chapter 56) and PR (Chapter 25) Use Zone Charts found in the Kirkland Zoning Code.

*****Note: Design Guidelines unique to YBD 1 will be inserted throughout this document after adoption.***

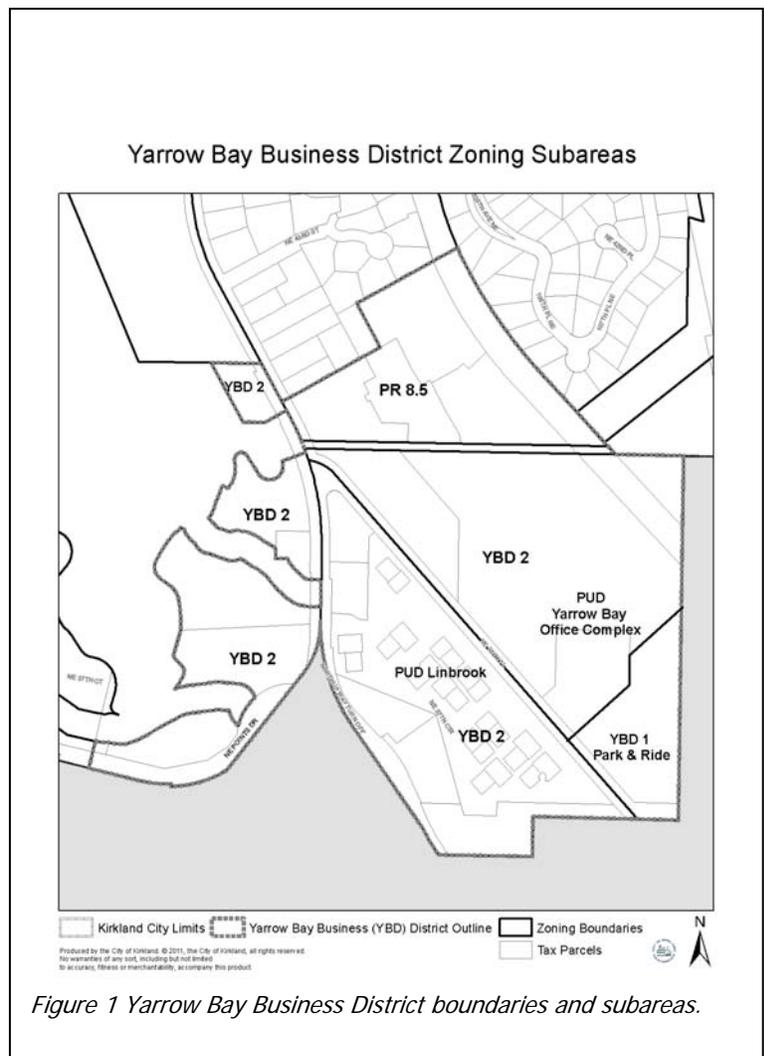
Purpose of the Design Guidelines

The Design Review Board will use these guidelines to evaluate development proposals during the design review process. The Design Guidelines are intended to establish a greater sense of quality, unity, and conformance with Kirkland's physical assets and civic identity. These guidelines are not intended to slow or restrict development, but rather to add consistency and predictability to the permit review process.

Urban Design Goals and Objectives

The key design objectives promoted in the Lakeview Neighborhood Plan for the Yarrow Bay Business District include:

- *Promote quality architectural and site design by establishing design standards that apply to commercial and multifamily development.*
- *Establish architectural vertical and horizontal modulation guidelines along all street frontages and perimeter of district.*
- *Preserve public scenic views and natural features that contribute to Lakeview's visual identity.*



- *Provide interconnected street and pedestrian improvements throughout the district that tie uses together, contribute to a sense of identity and enhance visual quality. Included in this concept are pedestrian linkages between uses on site, to adjacent properties, transit facilities and the Transit Oriented Development at the South Kirkland Park and Ride. Provide directional signs that indicate path locations.*
- *Enhance streetscapes distinctive to the neighborhood along Lake Washington Blvd. and NE 38th PI with street trees, decorative pedestrian lighting, benches, and street furniture.*
- *Incorporate gateway features to the neighborhood to strengthen neighborhood identity at the locations identified in the Lakeview Neighborhood Plan.*
- *Promote pedestrian oriented design such as minimizing blank walls, providing generous window treatment, awnings, superior building materials, open space plazas, and pedestrian amenities especially around retail uses.*
- *Locate development away from streams and wetlands. Enhance stream corridors for both habitat and as a natural amenity.*

Vision for the Yarrow Bay Business District

The Lakeview Neighborhood Plan vision for the Yarrow Bay Business District is to transform the large suburban style office park development into a more integrated, mixed use residential and commercial district. Several strategies will help achieve this vision such as allowing a broader range of uses, and improving pedestrian connections between properties, businesses, the Transit Oriented Development at the South Kirkland Park and Ride facility and the Eastside Rail Corridor. Incorporating public plazas, green spaces and pedestrian amenities into new development will help create an inviting environment for employees, residents and visitors. New design standards and design review for development will ensure quality architecture, site design and identity for the district.

The Lakeview Neighborhood Plan envisions improvements to NE 38th PI, Lake Washington Blvd. NE and Northup Way to upgrade the streets with wider sidewalks, street trees and decorative pedestrian lighting, directional signs, benches and varying pavement textures. On street parking should be provided along portions of NE 38th PI that contain retail frontage such as near the South Kirkland Park and Ride Transit Oriented Development.

Larger sites within the Yarrow Bay Business District present opportunities for master planning to provide coordinated development. Within YBD 2, for example, mixed-use developments could be created, combining retail, office and residential uses. Property development can provide an attractive face along the major traffic corridors or provide their own interior vehicular and pedestrian pathways and open space as focal points for pedestrians.

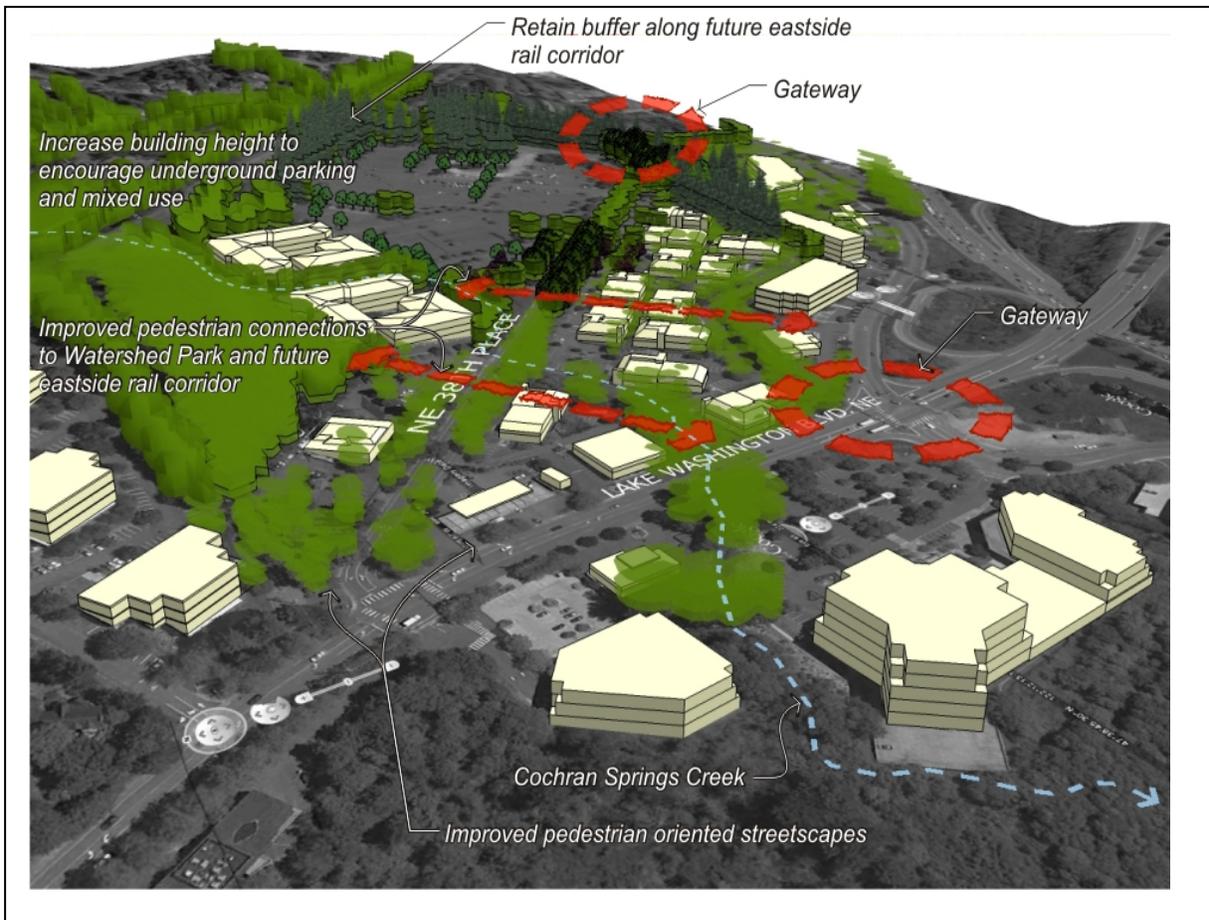


Figure 2 illustrates the future urban design concept for the Yarrow Bay Business District

These may include plazas surrounded by shops, offices, services or wide sidewalk areas along an interior access street.

Buildings are encouraged to front along NE 38th Pl. and Lake Washington Blvd. Storefronts may be clustered around major entry points to the development providing a welcoming entry. Buildings are encouraged to use a variety of materials and colors and modulated walls and rooflines to reduce their architectural scale. Residential uses are encouraged to feature prominent building entries and individual balconies. Residential buildings should be clustered around a courtyard, small park, or natural features.

Vision for YBD 1

****Insert text for YBD 1 after adoption*

Design Guidelines

The following design guidelines for the Yarrow Bay Business District (YBD) are intended to help guide the future development of the neighborhood toward the vision described in the Lakeview Neighborhood Plan and in this document.

1. Building Location and Orientation

Objectives

- To enhance the character and identity of the Yarrow Bay Business District.
- To upgrade the appearance of streets in the Yarrow Bay Business District.
- To enhance pedestrian circulation.
- To create focal points, particularly on large sites.
- To coordinate development with adjacent property where desirable.

Insert new photo

Figure 3. Encourage developments to place parking lots to the side or rear, as accomplished here.

Guidelines

- a. Locate and orient buildings toward streets, plazas, common open spaces or major internal pathways. Provide pedestrian entrances along the sidewalk. Provide windows along sidewalks. Locating parking to the side and/or rear of buildings is preferred.
- b. Configure development to provide focal points and opportunities for coordinated pedestrian and vehicular access. Where there are no current opportunities for coordinated access provide the opportunity for future coordination should adjacent site redevelop in the future.
- c. Where buildings are oriented to an interior open space or courtyard, orient primary building entries to the open space provided there is direct pedestrian access and visibility into the open space from the street sidewalk.

2. Parking Lots and Vehicular Circulation

Objectives

- To minimize the impact of parking facilities on the fronting street, pedestrian environment, and neighboring properties.
- To enhance pedestrian and vehicular safety.
- To maintain traffic flow on streets.
- To promote shared parking.
- To provide attractive and connected vehicular circulation routes.

Discussion

Parking lots can detract from the pedestrian and visual character of a commercial area. The adverse impacts of parking lots can be mitigated through sensitive design, location, and configuration. Large parking lots can be confusing unless vehicle and pedestrian circulation patterns are well organized and marked.

Guidelines

Driveways

- a. Minimize the number of driveways into a development along Lake Washington Blvd, NE 38th Pl. and Northrup Way. To the extent possible, adjacent developments should share driveways.

Parking Lot Location and Design

- a. Locate surface parking areas to the side or rear of buildings so that parking areas do not occupy the majority of a site's frontage.
- b. Avoid parking layouts that visually dominate a development. Break up large parking lots into smaller ones.
- c. Provide a clear and well organized parking lot design. Space should be provided for pedestrians to walk safely in all parking lots.



Figure 4 A good example of incorporating trees between parking lot and along pedestrian paths in front of uses.



Figure 5 An example of interior parking lot landscaping.

Parking Lot Landscaping and Screening

- a. Integrate landscaping into parking lots to reduce their visual impact. Provide planting beds with a variety of trees, shrubs, and ground cover to provide visual relief, summer shade, and seasonal interest.
- b. Provide low level perimeter landscaping where parking is adjacent to sidewalks in order to maintain a visual screen and reduce clutter. Use screening methods that maintain visibility at eye level between the street and parking area.
- c. Provide extensive screening and landscaping between parking lots, residential uses, and open spaces. A combination of a screen wall with a landscape buffer is preferred.

3. Parking Structures

Objective

- To mitigate the visual impacts of parking structures in the urban environment.

Guidelines

- a. Mitigate the intrusive qualities of parking garages along streets, pedestrian pathways and in pedestrian areas using the following design techniques:
 - Locate parking structures, service, and storage functions away from the street edge and so they are not visible from the street or sidewalks.
 - Provide intervening ground-level commercial uses incorporated into parking structures.
 - Use landscaping to screen the parking garage façade.
 - Design and locate parking garage entries to complement, not subordinate the pedestrian entry. Locate the parking entry away from the primary street, to either the side or rear of the building.
 - Use architectural forms, materials, and/or details to integrate a parking structure with the development.
 - Locate and design parking structures to obscure the view of parked cars from adjacent properties.



Figure 6. This parking garage includes streetfront retail space and landscaped trellises to mitigate visual impacts on the streetscape

4. Architectural Scale

Objectives

- To encourage an architectural scale of development that is compatible with the vision for the districts within the Yarrow Bay Business District.
- To implement the planning concepts for the distinct design districts within the Yarrow Bay Business District.
- To add visual interest to buildings.

Discussion

“Architectural scale” means the size of a building relative to the buildings or elements around it. When the buildings in a neighborhood are about the same size and proportion, we say they are “in scale.” The vision and development regulations for the Yarrow Bay Business District provide for larger buildings than currently exist. Care must be taken to design buildings so they appropriately respond to the evolution of the District from the current low rise condition to its vibrant mixed use future. For example, a new project need not step down to a one story edge condition to acknowledge an existing one story building on an adjoining site, but it can incorporate horizontal and vertical modulation that allow it to “fit” with the existing context and provide cues for future development of the adjoining site.

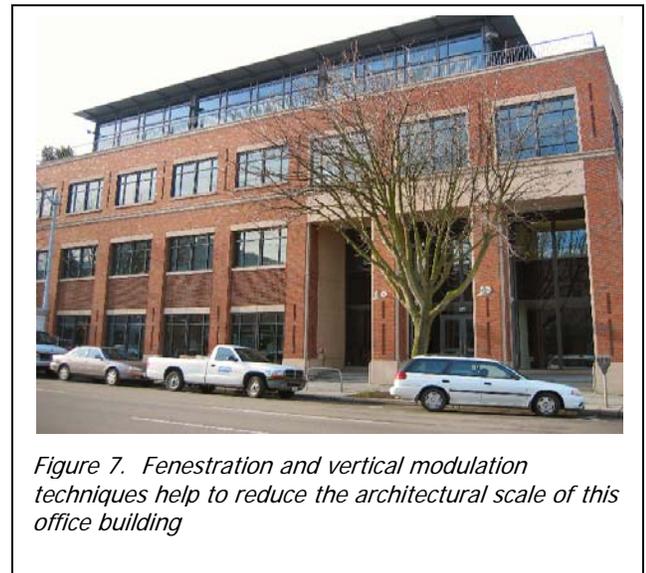


Figure 7. Fenestration and vertical modulation techniques help to reduce the architectural scale of this office building

Guidelines

A combination of techniques is desirable to reduce the architectural scale of buildings. In general the following techniques should be included at intervals of 70 feet for office uses and 30 feet for residential uses. Alternatives will be considered provided they meet the objectives.

- a. Incorporate fenestration techniques that indicate the scale of the building. The size, location, and number of windows in an urban setting create a sense of interest that relies on a mixture of proportions and patterns. This is particularly important on upper floors, where windows should be divided into individual units with each window unit separated by a visible mullion or other element. “Ribbon windows” (continuous horizontal bands of glass) or “window walls” (glass over the entire surface) do little to indicate the scale of the building and are thus discouraged, except in special circumstances where they serve as an accent element.

- b. Encourage vertical modulation on multi-story buildings to add variety and to make large buildings appear to be an aggregation of smaller buildings. Vertical modulation may be particularly effective for tall buildings adjacent to a street, plaza, or residential area to provide compatible architectural scale and to minimize shade and shadow impacts.
- c. Incorporate horizontal building modulation techniques to reduce the architectural scale of the building and add visual interest. Horizontal building modulation is the horizontal articulation or division of an imposing building façade through upper story setbacks, awnings, balconies, roof decks, eaves, and banding of contrasting materials. Elevations that are modulated with horizontal elements appear less massive than those with sheer, flat surfaces.

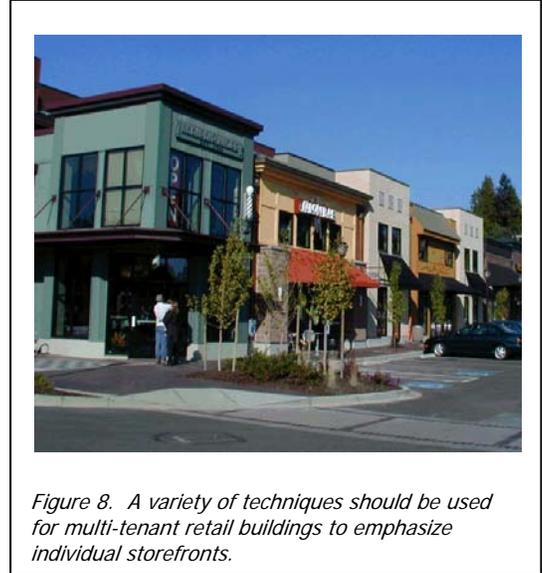


Figure 8. A variety of techniques should be used for multi-tenant retail buildings to emphasize individual storefronts.

Recommended horizontal building modulation techniques include:

- Roofline modulation and a change in building materials.
 - Step back building facades, generally above the second floor.
 - For residential uses, provide horizontal building modulation based on individual unit size, use roofline modulation, and changes in color and/or building materials. The depth and width of the modulation should be sufficient to meet the objectives of the guidelines. Avoid repetitive modulation techniques, since they may not be effective when viewed from a distance. Larger residential buildings will require greater horizontal modulation techniques to provide appropriate architectural scale.
- d. Break up long continuous walls with a combination of horizontal building modulation, change in fenestration, and/or change in building materials. This is especially important for office buildings.
- e. Encourage a variety of roofline modulation techniques such as hipped or gabled rooflines and modulated flat rooflines. As a general rule, the larger the building or unbroken roofline, the bigger the modulation should be. In determining the appropriate roof type and amount of modulation, consider at what distance the building can be viewed. For example, a large commercial building adjacent to a parking lot is capable of being viewed from a relatively large distance. Consequently the roofline modulation techniques must be sufficient to provide an appropriate architectural scale that provides visual interest.

5. Human Scale

Objectives

- To encourage the use of building components that relate to the size of the human body.
- To add visual interest to buildings.

Discussion

The term “human scale” is generally used to indicate a building’s size relative to a person, but the actual size of a building is often not as important as its perceived size. A variety of design techniques may be used to make a building less imposing.

Special elements in a building facade create a distinct character in an urban context. A bay window suggests housing, while an arcade suggests a public walkway with retail frontage. Each element must be designed for an appropriate urban setting and for public or private use. A building should incorporate special features that enhance its character and surroundings. Such features give a building a better defined “human scale.”

Guidelines

- a. Encourage a combination of architectural building elements that give the building a human scale. Examples include arcades, balconies, bay windows, roof decks, trellises, landscaping, awnings, cornices, friezes, art concepts, street front courtyards and plazas outside of retail spaces. Window fenestration techniques described in Section ___ can also be effective in giving humans clues as the size of the building. Consider the distances from which buildings can be viewed (from the sidewalk, street, parking lot, open space, etc.).

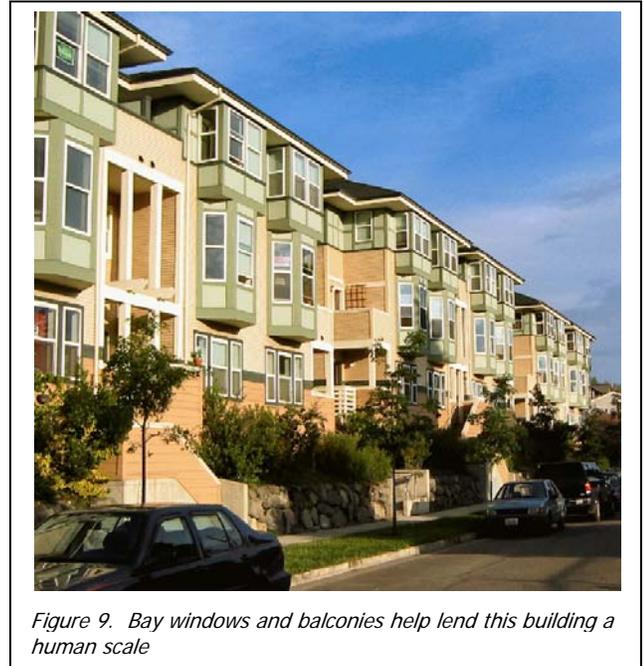


Figure 9. Bay windows and balconies help lend this building a human scale

6. Pedestrian Connections

Objectives

- To provide convenient pedestrian access.
- To reduce vehicle trips.
- To encourage pedestrian activity.

Guidelines

- a. Provide convenient pedestrian access between the street, bus stops, buildings, parking areas, and open spaces.
- b. Provide direct pedestrian access from buildings to abutting a public sidewalk or major internal pathways.
- c. Provide paved walkways through large parking lots. Separate walkways from vehicular parking and travel lanes by use of contrasting paving material which may be raised above the vehicular pavement and by landscaping.
- d. Provide safe and convenient pedestrian connections east to west through the business district. Consider installing a public trail along the stream corridor as a pedestrian connection and natural amenity.



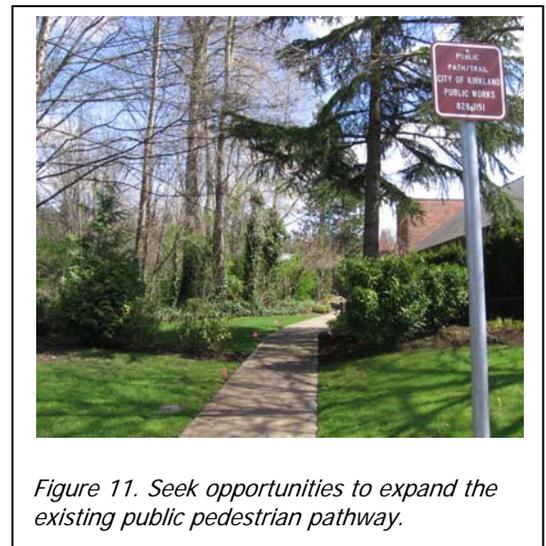
7. Natural Features

Objectives

- Establish a “greenway” corridor extending in an east/west direction across the business district from the Yarrow Bay wetlands and along stream corridors to the Houghton slope.

Guidelines

- a. Configure buildings and site features to preserve and enhance the stream corridors and steep slopes. Consider these natural features as open space amenities.
- b. Use wooded slopes as a natural site amenity and buffer by using and retaining native vegetation.
- c. Encourage buildings and rooflines to step down or be tucked against hillsides to roughly follow the slope of the existing terrain.



8. Blank Walls

Objectives

- To minimize visible blank walls.
- To enhance public safety along sidewalks and pathways.
- To encourage design elements that enhance the character of buildings at all perceived distances.

Discussion

Blank walls deaden the pedestrian environment and can break the continuity of uses along a street or pathway. Blank walls can also create a safety problem, particularly where adjacent to pedestrian areas, as they don't allow for natural surveillance of those areas. In some cases for example, fire walls require the intrusion of a flat, unadorned surface.

Guidelines

a. Avoid blank walls near sidewalks, major internal walkways, parks, and pedestrian areas. Use the following treatments to mitigate the negative effects of blank walls (in order of preference):

- Configure buildings and uses to avoid blank walls exposed to public view.
- Provide a planting bed with plant material to screen most of the wall.
- Install trellises with climbing vines or plant materials to cover the surface of the wall. For long walls, use trellises to avoid monotony.
- Provide artwork on the wall surface.
- Provide architectural techniques that add visual interest at a pedestrian scale, such as a combination of horizontal building modulation, change in building materials and/or color, and use of decorative building materials.
- Provide decorative lighting fixtures.



Figure 12. An example of treatment for blank wall or parking structure.



Figure 13. This building was a combination of alternating building materials, details, and landscaping elements to add visual interest at a close range.

9. Pedestrian-Friendly Building Fronts

Objectives

- To enhance the pedestrian environment.
- To create safe and active sidewalks and pathways.

Guidelines

- a. Incorporate transparent windows and doors and include weather protection features along all non-residential facades adjacent to a sidewalk or internal pathway. Weather protection features could include awnings, canopies, marquees, or other similar treatments.

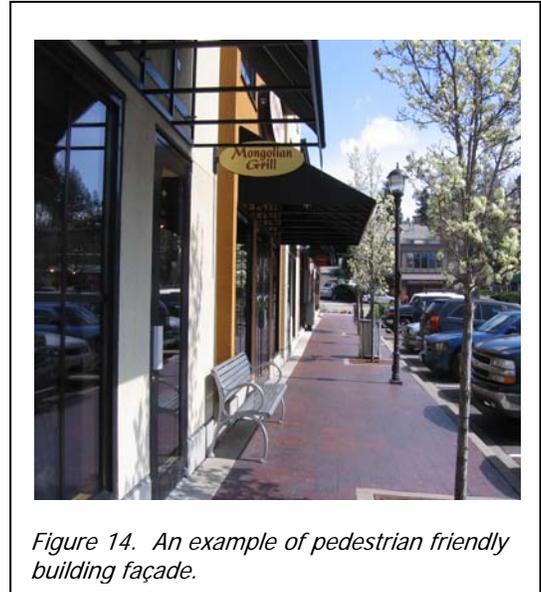


Figure 14. An example of pedestrian friendly building façade.

10. Pedestrian Plazas

Objectives

- To provide a variety of pedestrian-oriented areas to attract shoppers and employees to commercial areas and enrich the pedestrian environment.
- To create gathering spaces for the community.
- To configure buildings to encourage pedestrian activity and pedestrian focal points.

Discussion

Pedestrian plazas can serve multiple purposes such as, amenities, architectural emphasis, relieve long facades.

Guidelines

- a. Provide pedestrian plazas in conjunction with uses that can be used by the general public, residents and transit users.
- b. Position plazas in visible locations on major streets such as along NE 38th PI, major internal circulation routes, or where there are strong pedestrian flows on neighboring sidewalks. For large sites,

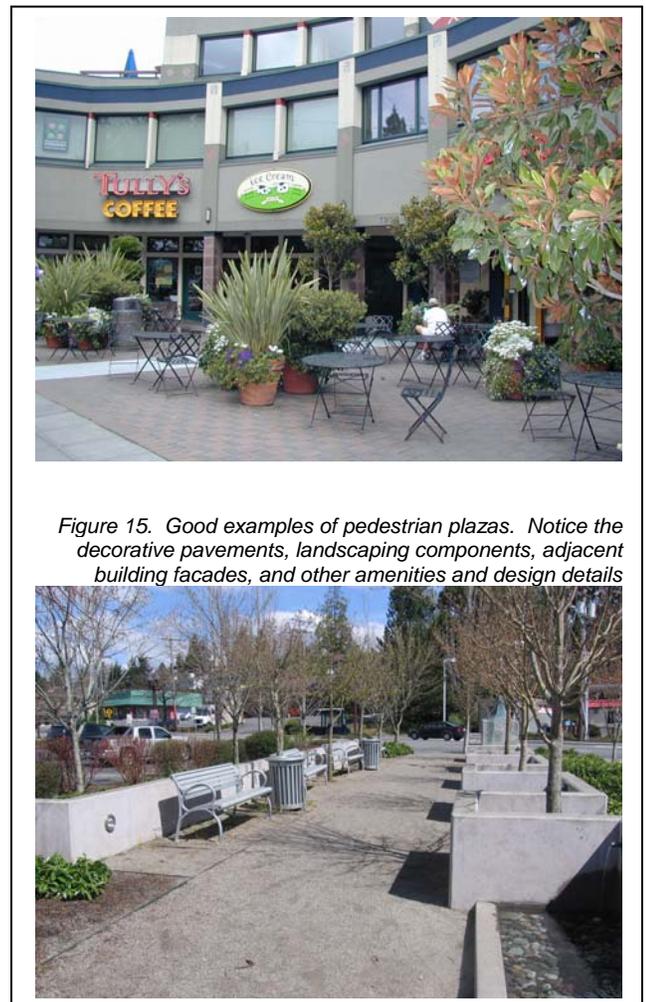


Figure 15. Good examples of pedestrian plazas. Notice the decorative pavements, landscaping components, adjacent building facades, and other amenities and design details

development should be configured to create a focal plaza or plazas. Plazas should be no more than 3' above or below the adjacent sidewalk or internal pathway to enhance visibility and accessibility.

- c. Locate building entrances that open on to plazas.
- d. Provide landscaping elements that add color and seasonal interest. This can include trees, planting beds, potted plants, trellises, and hanging plants.
- e. Incorporate pedestrian amenities, as described in Section ___.
- f. Locate plazas in sunny locations.
- g. Provide transitional zones along building edges to allow for outdoor seating areas and a planted buffer.

11. Residential Open Space

Objectives

- To create useable space that is suitable for leisure or recreational activities for residents.
- To create open space that contributes to the residential setting.

Guidelines

- a. Incorporate common open space for use by residents. In some developments, multiple smaller spaces may be more useful than one, larger space. Guidelines for common open space include:
 - Design space as a focal point of the residential development.
 - Provide space large enough to provide functional leisure or recreational activity. For example, long narrow spaces rarely, if ever, can function as usable common space.
 - Provide space for a range of activities and age groups. Children's play areas should be visible from dwelling units and positioned near pedestrian activity.
 - Separate common space from ground floor windows, streets, service areas, and parking lots with landscaping and/or low-level fencing. However, care should be used to maintain visibility from dwelling units towards open space for safety.
- b. Provide private open space for individual residential units. For townhouses and other ground-based housing units, provide patios, decks, and/or landscaped front or rear yards adjacent to the units. For all other units, provide balconies large enough to allow for human activity.



Figure 16. Good examples of common open space, including street-level courtyards (left), a children's play area (top right), and a pedestrian corridor (lower right)

12. Pedestrian Amenities

Objectives

- To provide amenities that enrich the pedestrian environment.
- To increase pedestrian activity.

Discussion

Site features and pedestrian amenities, such as lighting, benches, paving, waste receptacles, and other site elements, are an important aspect of a business district's character. These elements reduce apparent walking lengths and unify the district's visual character.

Guidelines

Provide pedestrian amenities along all sidewalks, interior pathways and within plazas and other open spaces. Examples include:

- Pedestrian-scaled lighting less than 15' above the ground.
- Seating space such as benches, steps, railings and planting ledges. Ideal heights are between 12" to 18". An appropriate seat depth ranges from 6" to 24".
- Pedestrian furniture such as trash receptacles, consolidated newspaper racks, and drinking fountains.
- Planting beds and/or potted plants.
- Unit paving such as stones, bricks, or tiles.
- Decorative pavement patterns and tree grates.
- Water features.
- Informational kiosks.
- Transit shelters.
- Decorative clocks.
- Artwork.
- Bicycle racks



Figure 17. Consolidated newspaper racks



Figure 18. Bicycle racks



Figure 19. Potted plants



Figure 21. Decorative pavement patterns (top), benches and pedestrian-scale lighting (middle), and informational kiosk (bottom)



Figure 20. This example combines a sculptural water feature with

13. Pedestrian Coverings

Objectives

- To provide shelter for pedestrians.
- To provide spatial enclosure and add design interest to a retail or office streetscapes.

Discussion

The design and width of pedestrian coverings should be determined by their function, the building's use and the type of street.

As a general rule, the more traffic an entry is expected to accommodate, the larger the covered area at the entry should be.

The width of the sidewalk should also be considered when sizing the pedestrian covering (wider sidewalks can accommodate wider pedestrian coverings). Canopies and awnings should be appropriately dimensioned to allow for tree growth, where applicable. The architecture of the building and the spacing of individual storefronts should help determine the appropriate placement and style of the canopy or awning. Continuous, uniform awnings or canopies, particularly for multi-tenant retail buildings, can create a monotonous visual environment and are discouraged.

Guidelines

- a. Provide weather protection along the primary exterior entrance of all businesses, residential units, and other buildings.
- b. Design weather protection features to provide adequate width and depth at building entries.
- c. Pedestrian covering treatments may include: covered porches, overhangs, awnings, canopies, marquees, recessed entries or other similar features. A variety of styles and colors should be considered and be compatible with the architectural style of the building and the ground floor use.
- d. Back lit, plastic awnings are not appropriate.

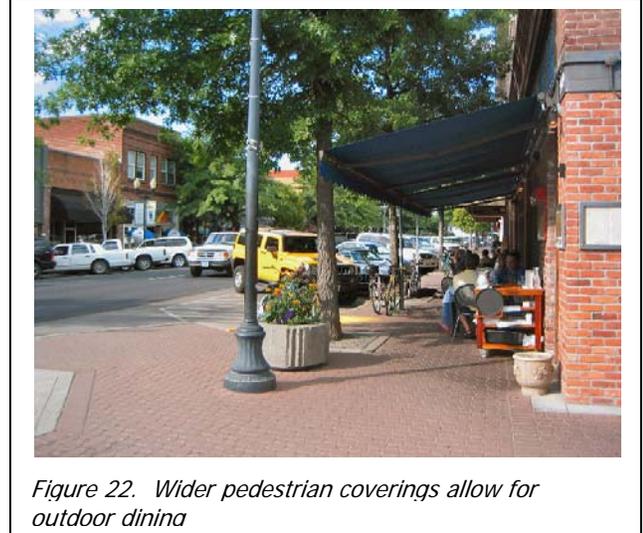


Figure 22. Wider pedestrian coverings allow for outdoor dinina



Figure 23. Note how these awnings have been integrated into the building's storefront spaces

14. Building Details and Materials

Objectives

- To use building and site design details that add visual interest to buildings/sites at a pedestrian scale.
- To use a variety of quality building materials such as brick, stone, glass, timber, and metal appropriate to the Pacific Northwest climate.

Guidelines

- a. Encourage the integration of ornament and applied art with the structures and the site environment. For example, significant architectural features should not be hidden, nor should the urban context be overshadowed.

Emphasis should be placed on highlighting building features such as doors, windows, eaves, and on materials such as wood siding and ornamental masonry. Ornament may take the form of traditional or contemporary elements. Original artwork or hand-crafted details should be considered in special areas. Ornament and applied art can be used to emphasize the edges and transition between public and private space, and between walls to ground, roof to sky, and architectural features to adjacent elements. Ornament may consist of raised surfaces, painted surfaces, ornamental or textured banding, changing of materials, or lighting.

- b. Use a variety of quality building materials such as brick, stone, timber, and metal to add visual interest to the buildings and reduce their perceived scale. Use masonry or other durable materials near the ground level (first 2 feet above sidewalk or ground level).
- c. Avoid use of concrete block and large expansive tilt up concrete facades.



Figure 24. Consider changes in building materials with modulation techniques



Figure 25. A combination of materials is preferred

15. Entry Gateway Features

The Lakeview Neighborhood Plan calls for gateway features at two key entry points into neighborhood and the business district:

- Intersection of SR 520 and Lake Washington Blvd. NE
- Intersection of 108th Avenue NE and NE 38th PL at the Transit Oriented Development (Note: Guidelines for this gateway in YBD 1 will be added to this section when SKPR review is complete).



Figure 26 Existing gateway city entrance sign

Objectives

- To enhance the character and identity of the Lakeview Neighborhood.
- To provide a welcoming statement for visitors entering the City.

Guideline

Incorporate entry gateway features in new development in the vicinity of gateways/nodal intersections identified in the Lakeview Neighborhood Plan. A new gateway sign with the City logo located on Lake Washington Blvd should be relocated to a more prominent location and the design improved. Gateway features may include some or all of the following:

- a. Distinctive landscaping.
- b. Artwork (e.g. vertical sculpture incorporating historical information about the Lakeview Neighborhood).
- c. Decorative lighting elements.
- d. Distinctive architectural feature that is unique to the neighborhood or provide open space.
- e. Incorporation of the Cochrane Springs Creek crossing into a gateway feature as a softer, green entrance to the City.

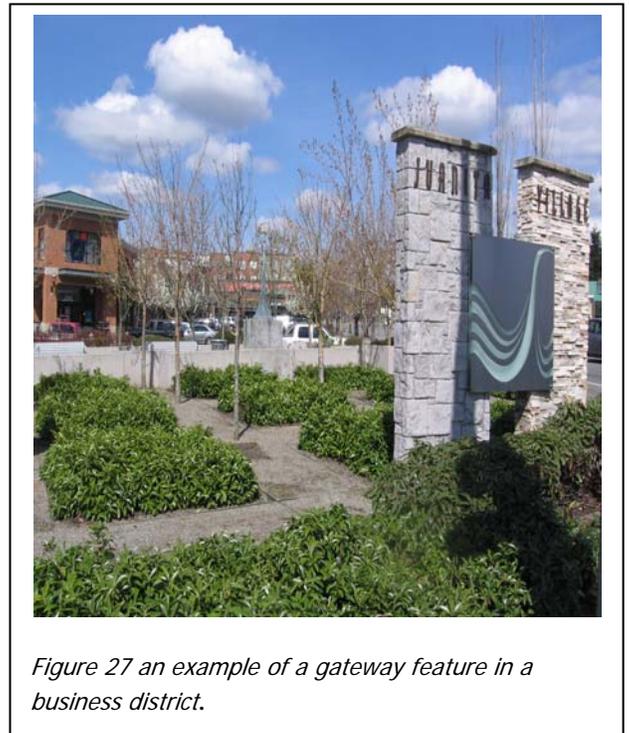


Figure 27 an example of a gateway feature in a business district.

16. Sidewalk and Pathway Widths

Objectives

- To provide wide sidewalks and pathways that accommodates pedestrian movement and provides a pleasing pedestrian experience.

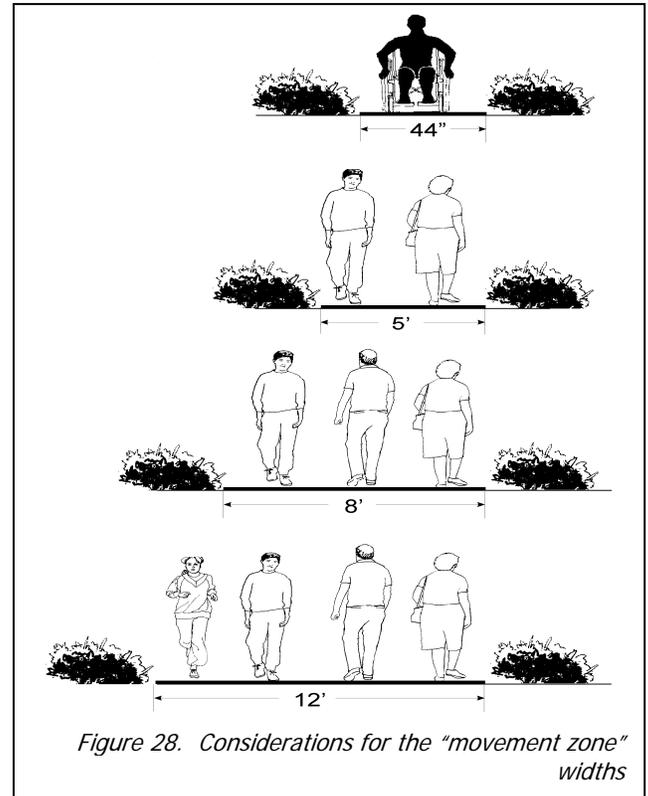
Discussion

Sidewalks have three overlapping parts with different functions: the curb zone, the movement zone, and the storefront or activity zone. A well-sized and uncluttered movement zone allows pedestrians to move at a comfortable pace.

Sidewalks or pathways adjacent to moving vehicular traffic need generous buffers to make them safer and more inviting. Landscaping elements are particularly important physical and visual buffers between walkways and streets or other vehicle access areas. As a general rule, the higher the travel speed, the greater the buffer should be between moving cars and pedestrians.

Guidelines

- a. Integrate a "curb zone" into the sidewalk or pathway width to separate the pedestrian from the street. This space can include street trees in landscape strip or trees in tree grates. Subtle changes in paving patterns between the curb zone and the movement zone can be effective and should be considered.
- b. Design sidewalks and pathways to support a variety and concentration of activities and provide a separation for the pedestrian from the busy street. Provide decorative pedestrian lighting and amenities described in the pedestrian amenities section below.
- c. For the movement or storefront activity zone design sidewalks to be wide enough to allow for pedestrians to pass those window shopping or for café seating.



17. Street Trees

Objectives

- To utilize street trees to upgrade the character and identity of the Yarrow Bay Business District.
- To enhance the pedestrian environment in the Business District.
- To use trees that provide seasonal interest.
- To use trees that will not obscure views of businesses from the street.

Discussion

The repetition of trees bordering streets, internal roadways, and pathways can unify the District. Trees can add color, texture, and form to the urban environment and provide a respite from the weather.

The health and species of the existing street trees along NE 38th Pl, Lake Washington Blvd and Northup Way should be evaluated to determine if they are the appropriate variety for the location or in need of replacement.



Figure 30. Provide street trees along all streets and internal access roads

Guidelines

- a. Incorporate street trees along all streets, internal access roads, and pathways.
- b. Encourage developments to use street trees as a unifying feature for the District.
- c. Select and maintain tree species that will accommodate pedestrian and vehicular traffic, and maintain visibility into and through sites for safety purposes.

18. Landscaping

Objective

- To enhance the visual quality of the urban environment.

Discussion

Landscaping can soften the hard edges and improve the visual quality of the urban environment. Landscaping treatment in the urban environment can focus on the automobile, the pedestrian, or building landscape.

Along high speed and high volume traffic areas raised planting strips can be used to protect the pedestrian from traffic. The pedestrian landscape offers variety at the ground level through the use of shrubs, ground cover, and trees. Pedestrian circulation, complete with entry and resting points, should be emphasized. Landscaping around buildings particularly along blank walls can reduce scale and add diversity through pattern, color, and form.

Examples of how landscaping is used to soften and enhance the visual quality of the urban environment include:

- Screening of parking lots;
- Tall cylindrical trees to mark an entry;
- Continuous street tree plantings to protect pedestrians;
- Clusters of dense trees along long building facades;
- Cluster plantings at focal points;
- Parking lots with trees and shrubs planted internally as well as on the perimeter.

Guidelines

- a. Design landscaping for the purpose and context of where it will be located. The auto oriented landscaping requires strong plantings of a structural nature to act as buffers or screens for pedestrians. The pedestrian landscape should emphasize the subtle characteristics of the plant materials. The building landscape should use landscaping that complements the building's qualities and screens its faults while not blocking views of the business or signage.
- b. Encourage a colorful mix of drought tolerant and low maintenance trees, shrubs and perennials. Except in special circumstances, ivy and grass lawn should be avoided.
- c. Consider the on-site topography to hide parking and enhance views.
- d. Use wooded slopes and streams as a natural site amenity and to screen unwanted views, where applicable.

19. Service Areas

Objectives

- To mitigate adverse impacts of service areas.
- To locate and design site service and storage areas to promote ease of use, safety, and visual cohesion.

Guidelines

- a. Locate and design service and storage areas (such as refuse, recycling, loading or mechanical equipment areas) to minimize visibility by and impacts on the pedestrian environment and adjacent uses.
- b. Locate service elements where they are accessible to service vehicles and convenient for tenant use.
- c. Design service enclosures to be compatible with the design of adjacent buildings. This may be accomplished by the use of similar building materials, details, and architectural styles. Such enclosures should be made of masonry, ornamental metal, heavy wood timber, or other durable materials.
- d. Locate roof-mounted mechanical equipment so as not to be visible from the street, public open space, parking areas, or from the ground level of adjacent properties. Equipment screening should blend with the architectural character of the building.
- e. Include both the location and screening of all equipment and service areas in the early design of a building.

20. Lighting

Objectives

- To enhance safety.
- To create inviting pedestrian areas.
- To provide adequate lighting without creating excessive glare or light levels.

Discussion

Overpowering and uniform illumination from commercial uses creates glare and destroys the quality of night light, especially for adjacent residential areas. Well placed light fixtures will form individual pools of light and maintain sufficient lighting levels for security and safety purposes.

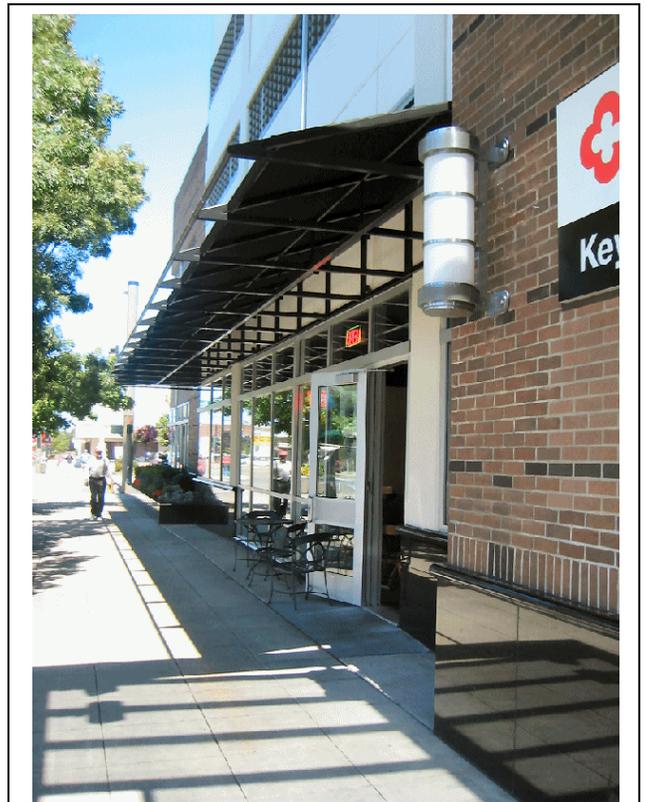


Figure 31. Building-mounted lighting is encouraged to enhance the pedestrian environment

Guidelines

- a. Provide adequate lighting levels in all areas used by pedestrians and automobiles, including building entries, walkways, parking areas, circulation areas, and open spaces. Recommended minimum light levels:
 - Building entries: 4 foot candles
 - Primary pedestrian walkway: 2 foot candles
 - Secondary pedestrian walkway: 1-2 foot candles
 - Parking lot: .60 -1 foot candle
 - Enclosed parking garages for common use: 3 foot candles
- b. Provide lighting for walkways and sidewalks through building mounted lights, canopy- or awning-mounted lights, and display window lights. Building-mounted light fixtures are encouraged to give visual variety from one facade to the next.
- c. Provide parking lot light fixtures that are non-glare. Lower level lighting fixtures are preferred to maintain a human scale. Lights up to 20' may be used for safety, when needed.
- d. Prohibit flood illumination of entire building facades

21. Signs

Objectives

- To encourage the use of creative, well-crafted signs that will contribute to the character of the district.

Discussion

Kirkland's Zoning Code regulates signs throughout the city in order to create a high-quality urban environment. The type and design of a sign should depend on if it is geared toward the passing motorist, pedestrians or a commercial center. Signs should be an integral part of a building's façade or center identification. The location, architectural style, and mounting of signs should conform to a building's architecture and not cover up or conflict with its prominent architectural features. A sign's design and mounting should be appropriate for the setting.

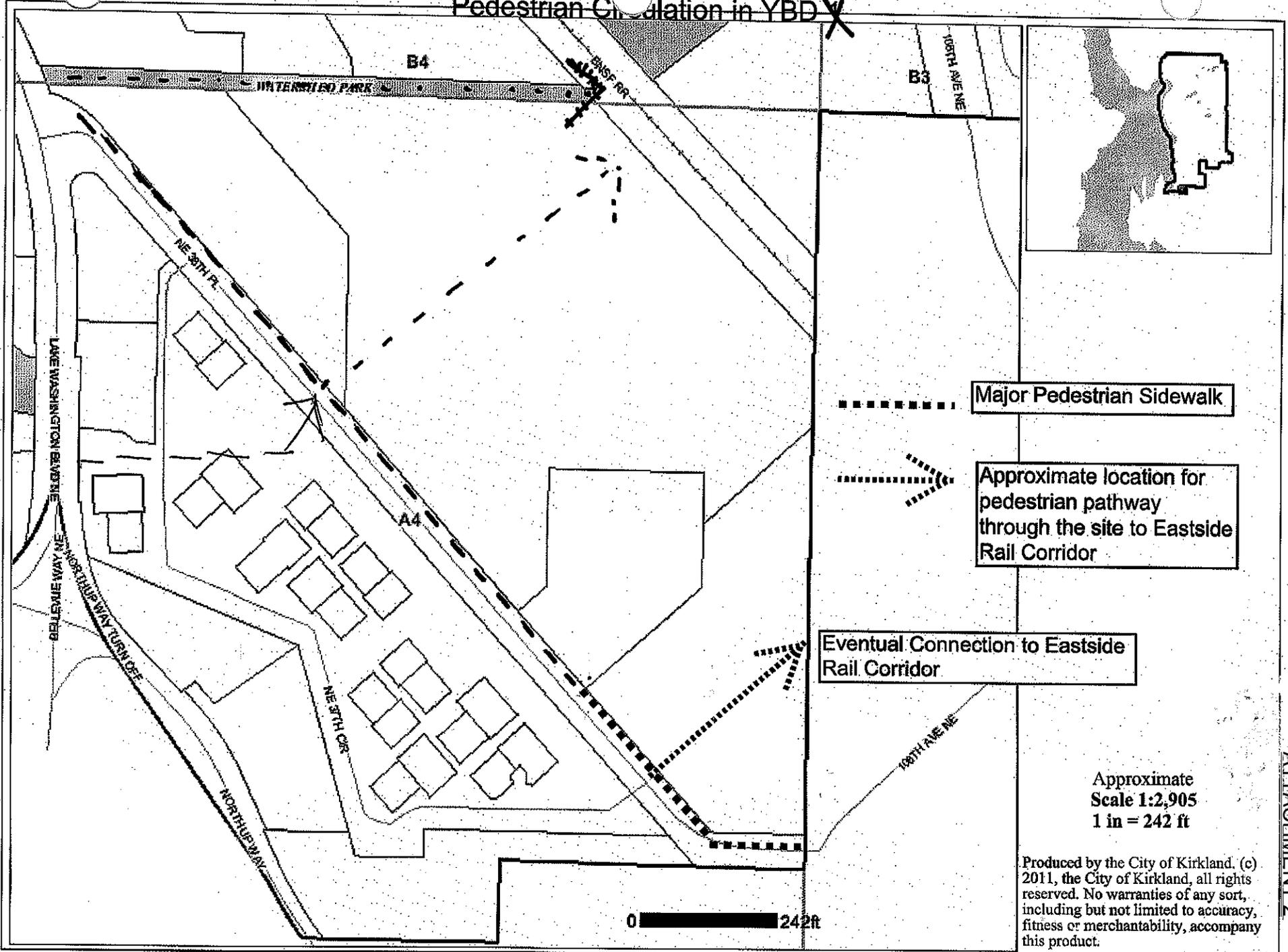


Figure 32 good example of a sign geared to automobiles for a multi use development

Guidelines

- a. Provide pedestrian oriented signs on all commercial facades where adjacent to a sidewalk or walkway. This includes signs located within 15' of the ground plane, such as "blade" signs which hang below canopies. Small signs located on canopies or awnings are also effective along building facades at the street. Sculpted signs and signs that incorporate artwork add interest.
- b. Prohibit internal lit cabinet signs. External lighting is preferred. Neon signs are appropriate when integrated with the building's architecture.
- c. For ground mounted signs provide substantial sign bases in proportion to the sign face and install low level landscaping around the sign base.
- d. Use mounting supports for signs that reflect the materials and design character of the building or site elements or both. Too much variety, too much uniformity though unified by common design elements, signs can still express the individual character of businesses.
- e. Provide master sign plans for larger commercial centers to combine signage for the whole complex that describes the general location for signs, complements the architectural design of the center and signs oriented to automobile traffic.

Plate 34L
 Pedestrian Circulation in YBD



Major Pedestrian Sidewalk

Approximate location for pedestrian pathway through the site to Eastside Rail Corridor

Eventual Connection to Eastside Rail Corridor

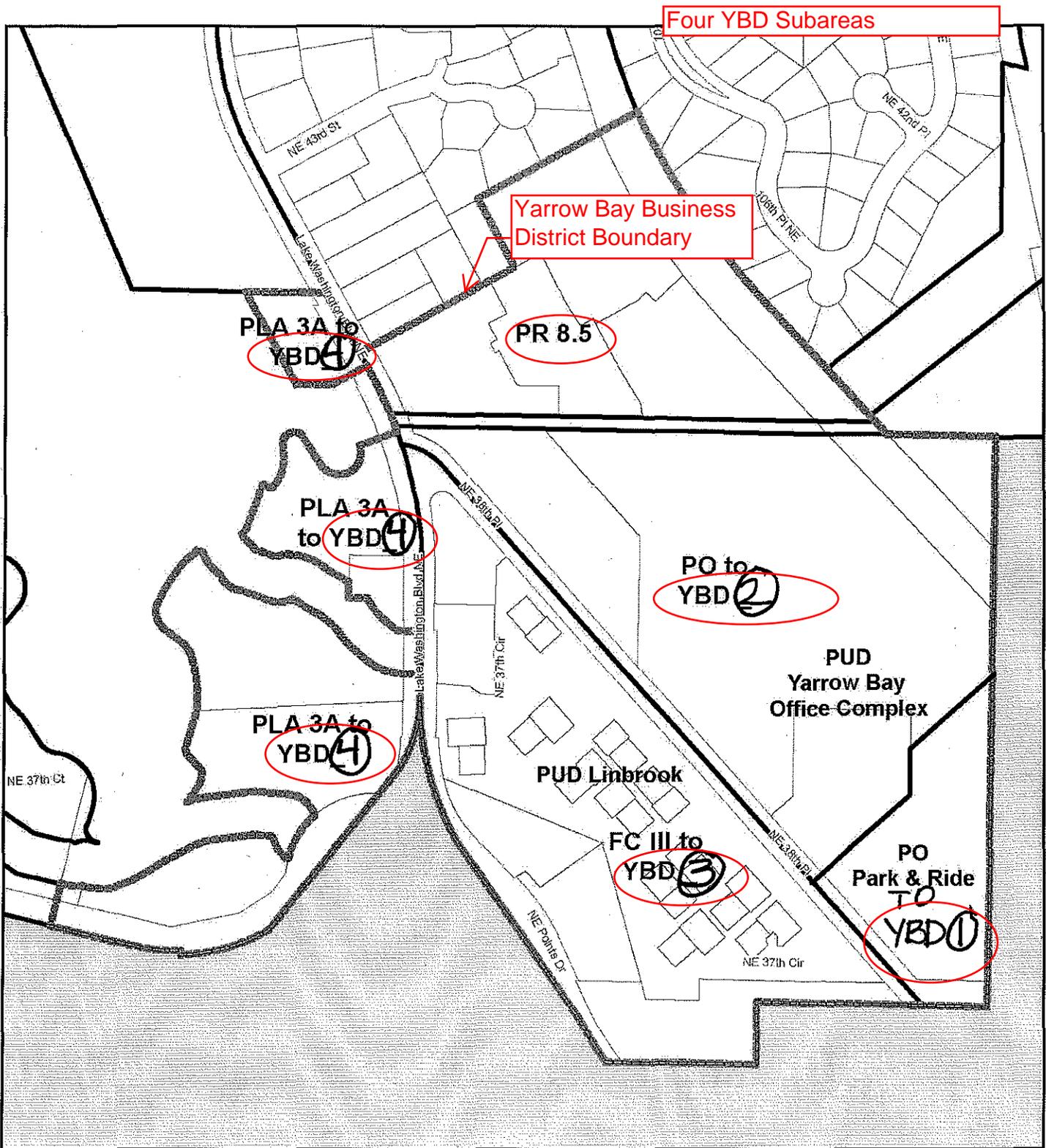
Approximate Scale 1:2,905
 1 in = 242 ft

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ATTACHMENT 2

OPTIONAL

Proposed Yarrow Bay Business District Zoning Subareas



Kirkland City Limits
 Yarrow Bay Business (YBD) District Outline
 Zoning Boundaries
 Tax Parcels

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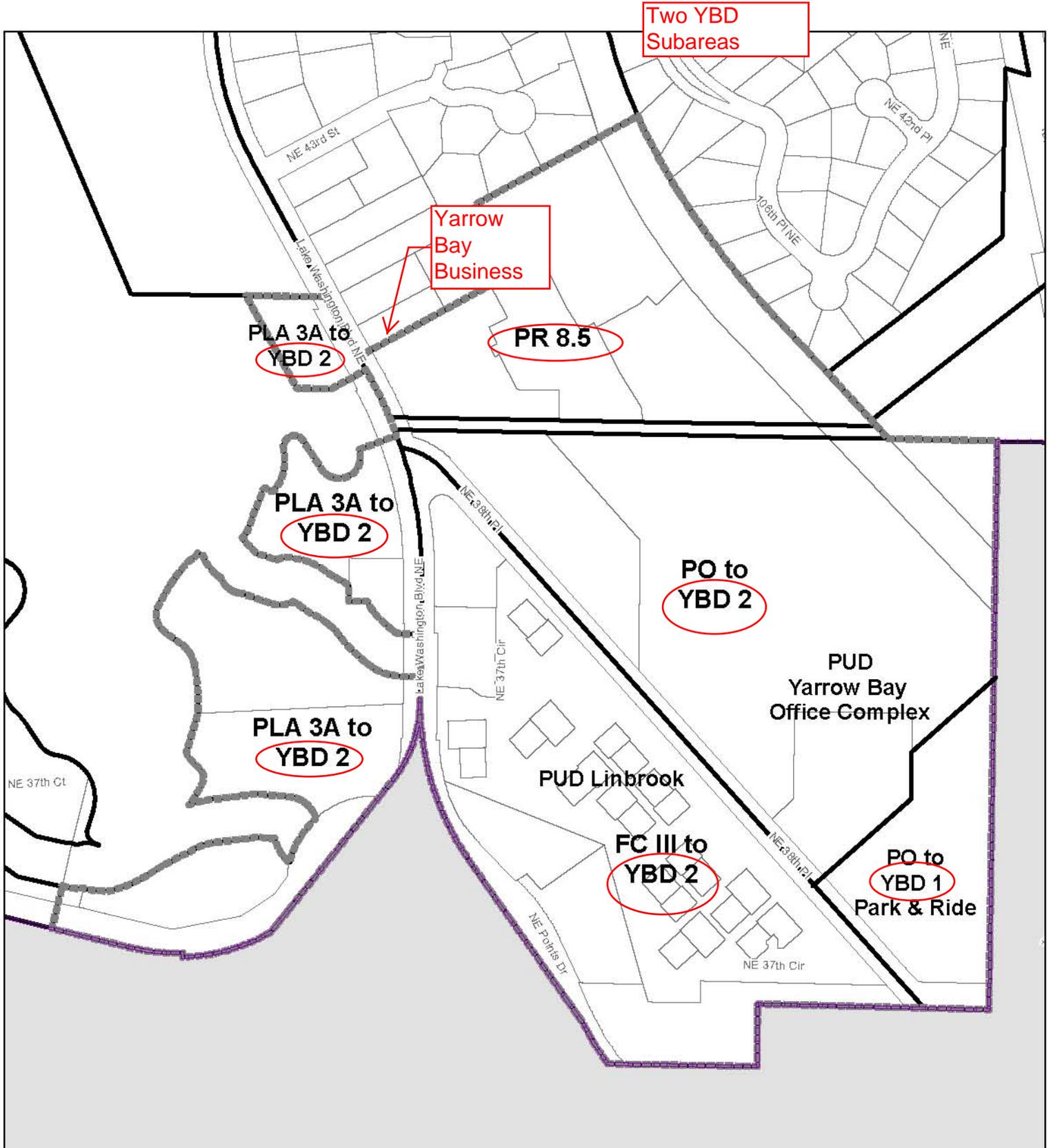
Comparison of zoning requirements for if three subareas in Yarrow Bay Business District (YBD 2, 3, 4) ATTACHMENT 4

	YBD 2 Yarrow Bay Office Park Block	YBD 3 Linbrook Block	YBD 4 Plaza at Yarrow Bay Block
Maximum Building Height	60' above ABE	60' above ABE	60' above ABE
Setback Yards Setbacks are the only difference between the zones	10' front yard along 38 th PI <u>or</u> 0' for one story at street along NE 38 th PI for retail establishment or banking and related financial services or entertainment, recreational or cultural activities 0' side and rear yards For vehicle service station: front: 40' side and rear 15'	10' front yard along 38 th PI 20' along Lake Washington Blvd or Northup Way <u>or</u> 0' for one story at street along NE 38 th PI for retail establishment or banking and related financial services or entertainment, recreational or cultural activities 0' side and rear yards For vehicle service station: front: 40' side and rear 15'	20' along Lake Washington Blvd or NE Points Drive <i>Note: this zone does not front on NE 38th PL</i> 0' side and rear yards For vehicle service station: front: 40' side and rear 15'
Min. Lot Size	22,500 sq. ft. for vehicle service station otherwise none	22,500 sq. ft. for vehicle service station otherwise none	22,500 sq. ft. for vehicle service station otherwise none
Permitted Uses and limitations	-Vehicle Service Station (no more than two stations on any intersection). -Restaurant or Tavern -Office -Hotel or Motel -Retail establishment other than those specifically listed, limited, or prohibited in the zone selling goods, or providing	-Vehicle Service Station (no more than two stations on any intersection). -Restaurant or Tavern -Office -Hotel or Motel -Retail establishment other than those specifically listed, limited, or prohibited in the zone selling goods, or providing	-Vehicle Service Station (no more than two stations on any intersection). -Restaurant or Tavern -Office -Hotel or Motel -Retail establishment other than those specifically listed, limited, or prohibited in the zone selling goods, or providing

Comparison of zoning requirements for if three subareas in Yarrow Bay Business District (YBD 2, 3, 4) ATTACHMENT 4

	<p>services including banking and related financial services.</p> <ul style="list-style-type: none"> -Retail establishment providing entertainment, recreational or cultural activities -Stacked dwelling units -Assisted Living Facility, Convalescent Center or Nursing Home -Private Lodge or Club -Hospital Facility -Public Utility -Church -School or Day Care Center -Mini school or Mini Day Care -Government Facility or -Community Facility -Public Park 	<p>services including banking and related financial services.</p> <ul style="list-style-type: none"> -Retail establishment providing entertainment, recreational or cultural activities -Stacked dwelling units -Assisted Living Facility, Convalescent Center or Nursing Home -Private Lodge or Club -Hospital Facility -Public Utility -Church -School or Day Care Center -Mini school or Mini Day Care -Government Facility or -Community Facility -Public Park 	<p>services including banking and related financial services.</p> <ul style="list-style-type: none"> -Retail establishment providing entertainment, recreational or cultural activities -Stacked dwelling units -Assisted Living Facility, Convalescent Center or Nursing Home -Private Lodge or Club -Hospital Facility -Public Utility -Church -School or Day Care Center -Mini school or Mini Day Care -Government Facility or -Community Facility -Public Park
<p>Prohibited Uses</p>	<p>-Outdoor storage, sale, service or rental of motor vehicles, sailboats, motor boats and recreational trailers, vehicle repair, retail storage services, storage and operation of heavy requirement or parts unless enclosed, drive in or drive through facilities.</p> <p>-All individual establishments exceeding 15,000 sq. ft. of gross floor area are prohibited.</p>	<p>-Outdoor storage, sale, service or rental of motor vehicles, sailboats, motor boats and recreational trailers, vehicle repair, retail storage services, storage and operation of heavy requirement or parts unless enclosed, drive in or drive through facilities.</p> <p>-All individual establishments exceeding 15,000 sq. ft. of gross floor area are prohibited.</p>	<p>-Outdoor storage, sale, service or rental of motor vehicles, sailboats, motor boats and recreational trailers, vehicle repair, retail storage services, storage and operation of heavy requirement or parts unless enclosed, drive in or drive through facilities.</p> <p>-All individual establishments exceeding 15,000 sq. ft. of gross floor area are prohibited.</p>

Proposed Yarrow Bay Business District Zoning Subareas



 Kirkland City Limits
 Yarrow Bay Business (YBD) District Outline
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CHAPTER 56 – YBD 2 – Draft 4-204-2011

56.08 User Guide.

The charts in KZC 56.15 contain the basic zoning regulations that apply in each YBD 2 zone of the City. Use these charts by reading down the left hand column entitled Use. Once you locate the use in which you are interested, read across to find the regulations that apply to that use.

**Section 56.10
YBD 2****Section 56.10 – GENERAL REGULATIONS**

The following regulations apply to all uses in this zone unless otherwise noted:

1. Refer to Chapter 1 KZC to determine what other provisions of this code may apply to the subject property.
2. In addition to the height exceptions established by KZC [115.60](#), the following exceptions to height regulations in the YBD 2 are allowed:
 - a. Decorative parapets may exceed the height limit by a maximum of four feet; provided that the average height of the parapets around the perimeter of the structure shall not exceed two feet.
 - b. For structures with a peaked roof, the peak may extend eight feet above the height limit if the slope of the roof is equal to or greater than four feet vertical to 12 feet horizontal.
3. A City entry or gateway feature shall be designed and installed on the subject property adjacent Lake Washington Blvd. between the southern city limit line and NE 38th PI pursuant to the standards in KZC 110.60. The specific location and design of the gateway shall be evaluated with the Design Review Process.
4. Vehicular access points onto Lake Washington Blvd., NE 38th PI., and Northup Way must be minimized to prevent arterial congestion and traffic safety hazards. Shared access points must be utilized where feasible (does not apply to Public Park uses).
5. The minimum ground floor story height shall be 13' for the following use listings: restaurant, tavern, retail establishments including banking and related services, entertainment, recreational or cultural establishments, office, hotel or motel, all uses. This requirement does not apply to residential, assisted living, convalescent center, nursing home, public utility, government facility or vehicle service station uses.
6. Developments in parts of this zone may be limited by chapter 83 or 90 KZC, regarding development near streams, lakes, and wetlands.

DIRECTIONS: FIRST, read down to find use...THEN, across for REGULATIONS

Section 56.20	USE ⇩ REGULATIONS ⇨	Required Review Process	MINIMUMS			MAXIMUMS		Landscape Category (See Ch. 95)	Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)	Special Regulations (See also General Regulations)	
			Lot Size	REQUIRED YARDS (See Ch. 115)			Height of Structure					
				Front	Side	Rear						
.010	Vehicle Service Station	D.R., Chapter 142 KZC	22,500 sq. ft.	40'	15' on each side. See also Special Regulation 3.	15'	80 %	60' above average building elevation	A	E	See KZC 105.25.	<ol style="list-style-type: none"> The following uses and activities are prohibited: <ol style="list-style-type: none"> Individual establishments exceeding 15,000 sq. ft. of gross floor area. The outdoor storage, sale, service and/or rental of motor vehicles, sailboats, motor boats, and recreational trailers. May not be more than two vehicle service stations at any intersection, This use is only allowed if the subject property abuts Lake Washington Blvd or Northup Way. Gas pump islands may extend 20 feet into the front yard. Canopies or covers over gas pump islands may not be closer than 10 feet to any property line. Outdoor parking and service areas may not be closer than 10 feet to any property line. See KZC 115.105, Outdoor Use, Activity and Storage, for further regulations
.020	Restaurant or Tavern	D.R., Chapter 142 KZC	None	10' adjacent to NE 38 th Pl otherwise 20'	0'	0'			B		1 per each 100 sq. ft. of gross floor area.	<ol style="list-style-type: none"> The following uses and activities are prohibited: <ol style="list-style-type: none"> Individual establishments exceeding 15,000 sq. ft. of gross floor area. Drive in or drive through facilities.

.030	Office Use	D.R. Chapter 14 2, KZC	None	10 ' adjacent to NE 38 th PI otherwise 20'	0'	0'	80 %	60' above average building elevation	C	D	If Medical, Dental or Veterinary office, then one per each 200 sq. ft. of gross floor area. Otherwise, 1 per each 300 sq. ft. of gross floor area.	<ol style="list-style-type: none"> 1. The following regulations apply to veterinary offices only: <ol style="list-style-type: none"> a. May only treat small animals on the subject property. b. Outside runs and other outside facilities for the animals are not permitted. c. Site must be designed so that noise from this use will not be audible off the subject property. A certification to this effect, signed by an Acoustical Engineer, must be submitted with the development permit application. 2. Ancillary assembly and manufacture of goods on the premises of this use are permitted only if: <ol style="list-style-type: none"> a. The ancillary assembled or manufactured goods are subordinate to and dependent on this use. b. The outward appearance and impacts of this use with ancillary assembly or manufacturing activities must be no different from other office uses.
.040	Hotel or Motel	D.R. Chapter 14 2, KZC	None	10 ' adjacent to NE 38 th PI otherwise 20'	0'	0'	80 %	60' above average building elevation.	B	E	1 per each room. See also Special Regulation 2.	<ol style="list-style-type: none"> 1. May include ancillary meeting and convention facilities. 2. Excludes parking requirements for ancillary meeting and convention facilities. Additional parking requirement for these ancillary uses shall be determined on a case by case basis.
.050	A Retail Establishment other than those specifically listed, limited, or prohibited in the zone, selling goods, or providing services including banking and related financial services			10 ' adjacent to NE 38 th PI otherwise 20' See Special Regulation 3							1 per each 300 sq. ft. of gross floor area.	<ol style="list-style-type: none"> 1. The following uses and activities are prohibited: <ol style="list-style-type: none"> a. Individual retail establishments exceeding 15,000 sq. ft. in gross floor area. b. The outdoor storage, sale, service and/or rental of motor vehicles, sailboats, motor boats, and recreational trailers. c. Vehicle repair, d. Retail establishment providing storage services unless accessory to another permitted use. e. Storage and operation of heavy equipment, except delivery vehicles associated with retail uses. f. Storage of parts unless conducted entirely within an enclosed structure, g. Drive in or drive through facilities. 2. A delicatessen, bakery, or other similar use may include, as part of the use, accessory seating if: <ol style="list-style-type: none"> a. The seating and associated circulation area does not exceed more than 10 percent of the gross floor area of the use; and

												<p>b. It can be demonstrated to the City that the floor plan is designed to preclude the seating area from being expanded.</p> <p>3. The required front yard setback may be zero feet for one story at street level <u>along NE 38th PL.</u></p>
.060	A Retail Establishment providing entertainment, recreational or cultural activities	D.R. Chapter 142, KZC	None	10' adjacent to NE 38 th PI otherwise 20' See Special Regulation 2	0'	0'	80%	60' above average building elevation	B	E	1 for every four fixed seats	<p>1. Individual establishments exceeding 15,000 sq. ft of gross floor area are prohibited.</p> <p>2. The required front yard setback may be zero feet for one story at street level <u>along NE 38th PL.</u></p>
.070	Stacked Dwelling Units	D.R. Chapter 142, KZC	None	10' adjacent to NE 38 th PI otherwise 20'	0'	0'	80%	60' above average building elevation	D	A	1.7 per unit	<p>1. Chapter 115 KZC contains regulations regarding home occupations and other accessory uses, facilities and activities associated with this use.</p>

.080	Assisted Living Facility, Convalescent Center or Nursing Home			10 ' adjacent to NE 38 th PI otherwise 20	0"	0"			C		Independent unit: 1.7 per unit. Assisted living unit: 1 per unit. Convalescent Center or Nursing Home: 1 per each bed.	<ol style="list-style-type: none"> 1. A facility that provides both independent dwelling units and assisted living units shall be processed as an assisted living facility. 2. If a nursing home use is combined with an assisted living facility use in order to provide a continuum of care for residents the required review process shall be the least intensive process between the two uses.
.090	Private Lodge or Club	D.R. Chapter 142, KZC	None	10 ' adjacent to NE 38 th PI otherwise 20	0'	0'	80 %	60' average building elevation	C	B	1 per each 300 sq. ft. of gross floor area	
.100	Hospital Facility	D.R. Chapter 142, KZC	None	'10 ' adjacent to NE 38 th PI otherwise 20'	0'	0'	80 %	60' average building elevation	B	B	See KZC 105.25	
.110	Public Utility	D.R. Chapter 142, KZC	None	10 ' adjacent to NE 38 th PI otherwise 20'	0'	0'	80 %	60' above average building elevation	A	B	See KZC 105.25	

.120	Church			10' adjacent to NE 38 th PI otherwise 20'	0'	0'			C	B	1 for every 4 people based on maximum occupant load of any area of worship. See Special Reg 2	<ol style="list-style-type: none"> 1. May include accessory living facilities for staff persons. 2. No parking is required for day-care or school ancillary to the use.
.130	School or Day-Care Center	D.R. Chapter 142, KZC	None	10' adjacent to NE 38 th PI otherwise 20'	0'	0'	80%	60' above average building elevation	D	B	See KZC 105.25.	<ol style="list-style-type: none"> 1. A six-foot-high fence is required only along the property lines adjacent to the outside play areas. 2. Structured play areas must be setback from all property lines by at least five feet. 3. An on-site passenger loading area must be provided depending on the number of attendees and the extent of the abutting right-of-way improvements. 4. May include accessory living facilities for staff persons.
.140	Mini-School or Mini-Day-Care	D.R. Chapter 142, KZC	None	10' adjacent to NE 38 th PI otherwise 20'	0 ₃	0 ₃	80%	60' above average building elevation	E	B	See KZC 105.25.	<ol style="list-style-type: none"> 1. A six-foot-high fence is required along the property lines adjacent to the outside play areas. 2. Structured play areas must be setback from all property lines by at least five feet. 3. An on-site passenger loading area must be provided depending on the number of attendees and the extent of the abutting right-of-way improvements. 4. May include accessory living facilities for staff persons.
.150	Government Facility Community Facility								C See Spec. Reg. 1			<ol style="list-style-type: none"> 1. Landscape Category A or B may be required depending on the type of use on the subject property and the impacts associated with the use on the nearby uses.
.160	Public Park	Development standards will be determined on a case-by-case basis. See Chapter 49 KZC for required review process.										

Visualizations for Possible Future Development within Yarrow Bay Business District & South Kirkland Park and Ride

Image 1: Looking North-Northeast focused on the South Kirkland Park and Ride, with proposed mixed-use transit-oriented development (first two buildings on the east side of NE 38th Place). The light green roofed buildings represent possible new buildings, whereas the off-white colored buildings are existing structures. The image also shows some redevelopment on the parcel to the north of the park and ride, and across the street. The arrows show the internal circulation system, in this case, both pedestrian and vehicular. The dashed lines highlight a pedestrian corridor linking the TOD development and transit center with potential future Eastside Rail Corridor along the current BNSF line.

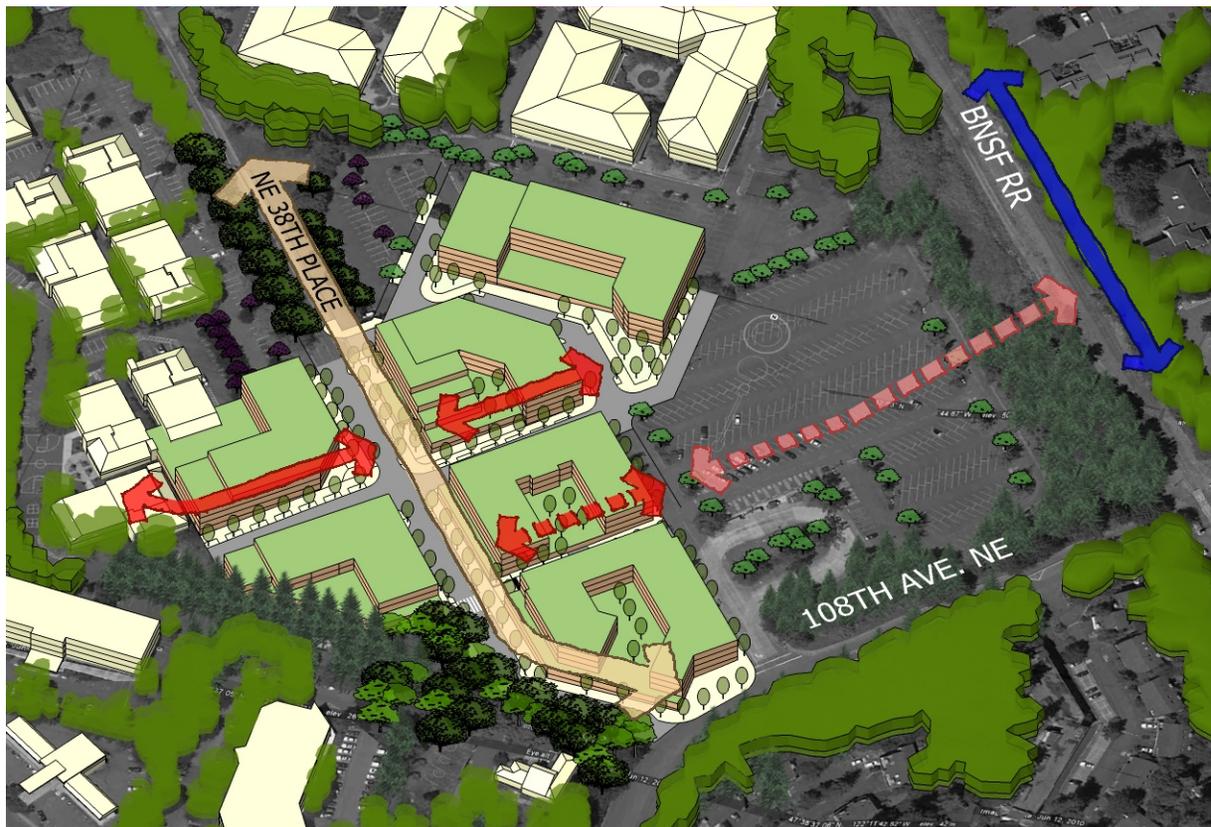


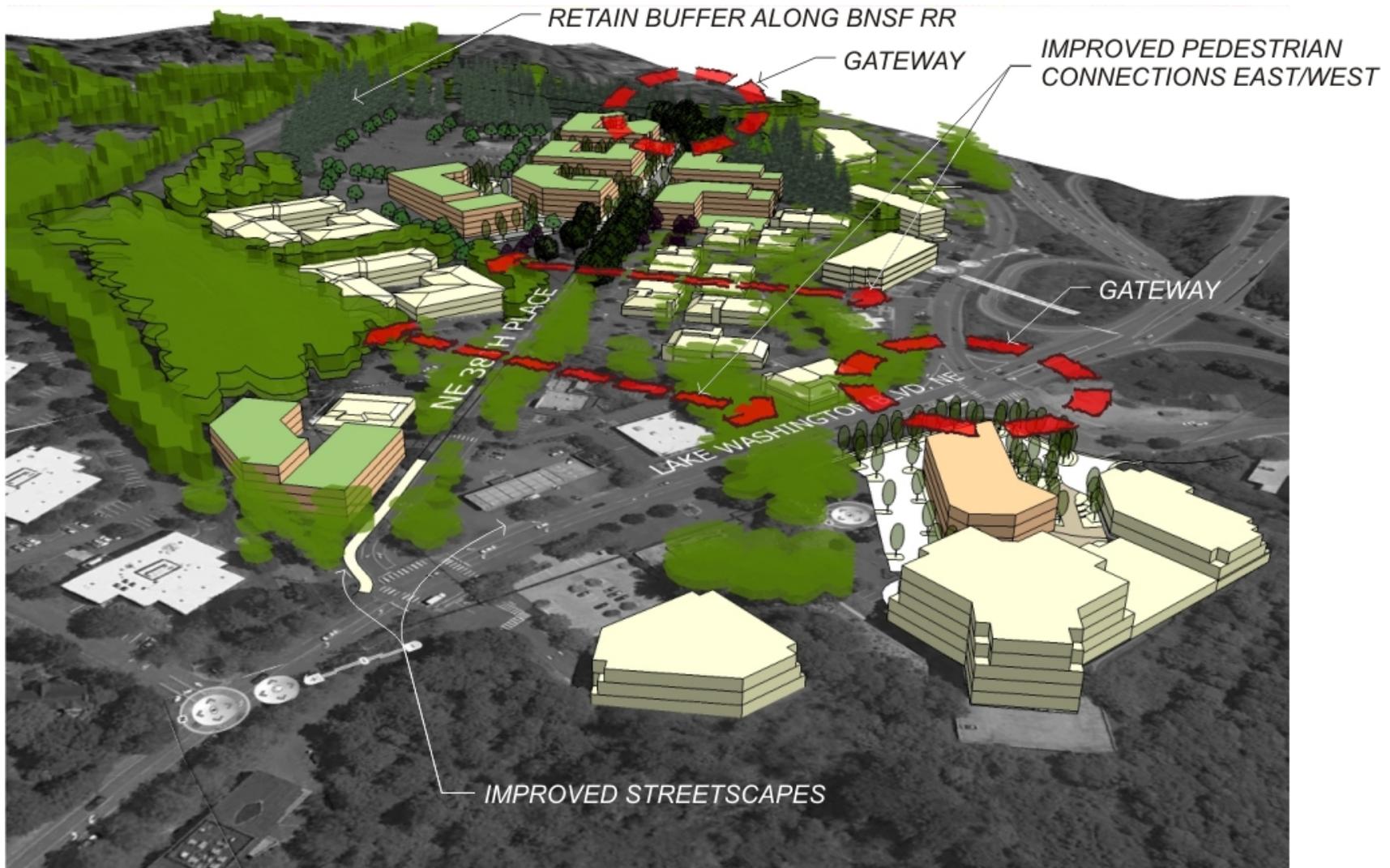
Image 2: This view is similar to image 1, except the view is towards the northwest and with a vantage point closer to the ground. Note the height of the buildings compared to the trees in the foreground. These trees are sized consistent with the size/height of these large evergreen and deciduous trees currently on the site – which appear to be typically 30-60' tall.



Image 3: View towards the east, north-east from near the SR-520/Lake Washington Boulevard interchange. Possible new buildings



Image 4: View towards the southeast. A possible new building at the Plaza at Yarrow Bay development is shown in the foreground on the right. Towards the left side of the image is a possible future building shown on the Keg's north parking lot. The park and ride site is in the background, just left of the center.



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Image 5: View towards the southwest with the Yarrow Bay Office Complex in the lower right corner and the South Kirkland Park and Ride and proposed TOD development just left of center. The image also shows other possible new buildings north and west of the park and ride site (along 38th Place in the center of the image).



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Image 6: Here's a street level view looking south along 38th Place, showing a mixed-use building with street level retail with residential above on the park and ride site. The cross-walk and pedestrian corridor, extending to the left, leads to the transit station and possibly towards a future Eastside Rail Corridor along the BNSF rail line.



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Image 7: View similar to Image 6, except from the rooftop of a possible new building across the street from the current park and ride. Again, note that the five-story buildings here are roughly equal to, or less than the size of current trees along 38th Place and Street.

