



## **MEMORANDUM**

**DATE:** June 20, 2012

**To:** Planning Commission

**FROM:** Jeremy McMahan, Planning Supervisor  
Eric Shields, AICP, Planning Director

**SUBJECT:** Commercial Codes, KZC, Comprehensive Plan and Design Guideline Amendments, File No. ZON11-00042

## **RECOMMENDATION**

- Conduct public hearing to receive public testimony on the proposed amendments.
- Deliberate and make a recommendation on the amendments that will be transmitted to the City Council for their consideration.

## **BACKGROUND**

Based on the approved Planning Work Program, in October 2011 the Planning Commission began work on a package of amendments to the Kirkland Zoning Code (KZC) related to clarifying and improving commercial regulations.

On January 3, 2012 the City Council extended a moratorium on development in BN zones for a total of six months and directed the Planning Commission to include additional review of the BN zoning and related Comprehensive Plan policies. In the Ordinance, the Council entered the following specific Findings of Fact that the Commission has used as guidance on issues to be addressed:

- While mixed used development with residential and commercial uses is encouraged in the City's commercial districts, development should also be compatible in scale and character so as to fit well with surrounding uses.
- Existing Neighborhood Business (BN) zoning regulations are perceived as being inadequate to address the scale and density of development consistent with Comprehensive Plan policies.
- A planning process including significant opportunities for participation by property owners, residents and other stakeholders is underway and the moratorium is required to maintain current conditions while the planning process progresses.

Following this direction from the City Council, the Planning Commission expanded the scope of amendments to address the issues identified in BN and related zones. On May 15<sup>th</sup>, the City Council reviewed the Planning Commission's initial direction and provided the following feedback on the Lake Street South BN zone to the Planning Commission:

- Consider a Comprehensive Plan amendment to change the commercial designation for the Lake Street South commercial area from "Residential Market" to "Neighborhood Center".
- Consider a maximum residential density limit.
- Do not favor requirement for commercial space to be at street level.
- Do not favor an additional maximum three story height limit.
- Do not favor limiting the size of building floor plates.
- Require minimum 13' height for commercial space.
- Make buffers for retail and office consistent at five feet (Commission has proposed consistency with a 15 foot buffer).
- Prohibit auto-oriented commercial uses.
- Establish minimum ground floor commercial frontage requirements.
- Require commercial use to be oriented to the street
- Limit the size of commercial uses to 4,000 square feet.
- Maintain 80% lot coverage.
- Establish a design review process and supporting guidelines or regulations.

The Planning Commission has taken that feedback into consideration in scoping the public hearing. The Planning Commission has now conducted six study sessions to scope the amendments and provide direction on draft codes, policies, and guidelines. The meeting packets, recordings, and minutes are available on the City's Planning Commission webpage and are entered into the hearing record. The Commission's most recent direction to staff for preparation of drafts for the public hearing is summarized in Attachment 1.

### **Key Issues and Considerations**

One of the fundamental reasons for this work plan item was to address mixed use development of the identified commercial zones to ensure that the rules are clear, will achieve the desired outcome, and will preserve these commercial zones for the commercial needs of the community.

Key considerations in review of the draft codes, policies, and guidelines should be internal consistency between the Comprehensive Plan and Zoning Code and consistency of regulations between similar zones where appropriate.

### **Proposed Comprehensive Plan Amendments**

The Comprehensive Plan establishes a hierarchy of commercial districts; with Urban Center being the most intensive and Residential Market being the least intensive (see Attachment 2). For example, Totem Center is designated as an Urban Center and the BN zone on Lake Street South and the Super 24 site (zoned RM 3.6) on Lake Washington Boulevard are the only designated Residential Markets. Other BN and BNA zones are designated as Neighborhood Centers.

A Residential Market is currently defined as:

*Individual stores or very small, mixed-use buildings/centers focused on local pedestrian traffic. Residential scale and design are critical to integrate these uses into the residential area. Uses may include corner grocery stores, small service businesses (social service outlets, daycares), laundromats, and small coffee shops or community gathering places.*

A Neighborhood Center is currently defined as:

*Areas of commercial activity dispensing commodities primarily to the neighborhood. A supermarket may be a major tenant; other stores may include a drug store, variety, hardware, barber, beauty shop, laundry, dry cleaning, and other local retail enterprises. These centers provide facilities to serve the everyday needs of the neighborhood. Residential uses may be located on upper stories of commercial buildings in the center.*

Proposed draft Comprehensive Plan amendments are included as Attachment 3 and are summarized and discussed below:

1. In the Land Use Element, consider changing the "Residential Market" area on Lake St. S. within the Moss Bay Neighborhood to a "Neighborhood Center"

This option was suggested by the City Council in the feedback provided to the Planning Commission at the May 15<sup>th</sup> Council meeting. The intent was likely:

- To bring the BN zoned area on Lake Street S (designated Residential Market) into consistency with other neighborhood business areas
- To acknowledge that the commercial area on Lake Street S encompasses a relatively large area, making it difficult to reconcile the "very small" part of the Residential Market definition
- To fill in some of the gaps left by the Residential Market definition, including clarifying that a "mixed-use building/center" includes residential use

The Planning Commission preference has been to retain and adjust the Residential Market designation and expand it to the similar BN and MSC 2 zones areas. However, the Neighborhood Center option is included in the scope of potential amendments to allow public comment.

2. In addition or in the alternative, consider amendments to the definition of "Residential Market" or development of an intermediate category between "Residential Market" and "Neighborhood Center"

The Planning Commission has indicated a preference for adjusting the definition of Residential Market to address the issues noted above. The Commission suggested deleting the word "very" from the concept of a small building or center. In the attached draft amendments (see Attachment 3), staff has suggested amending the definition as follows:

- Delete the word "very"
- Include language that specifically includes residential as part of mixed use development
- Include a reference to residential density limits, as established in the neighborhood plan

If the Planning Commission or City Council decides that any of these neighborhood business districts do not fit cleanly into one category or the other, the option would be to create a new category to encompass the objectives for that particular commercial area.

3. Consider changing the following "Neighborhood Center" areas to "Residential Market" (or intermediate category):
  - a. Neighborhood Center area on the north side of NE 70<sup>th</sup> PI within the South Rose Hill Neighborhood
  - b. Neighborhood Center area on the west side of Market Street between 14<sup>th</sup> Ave W and 16<sup>th</sup> Ave W within the Market Street Corridor

In reviewing other commercial areas within the neighborhood business "family" of zones, the Commission noted similarities in land use patterns where the MSC 2 zoned area on Market Street and the BN zoned area in South Rose Hill do not neatly fit into the "Neighborhood Center" definition because neither are large enough to accommodate a supermarket as the major tenant. By contrast, other Neighborhood Centers are anchored by a supermarket with the exception of Juanita Village which is anchored by a large drug store and a high concentration of smaller retail and restaurant tenants.

To create internal consistency within the Comprehensive Plan, the proposed amendments would change the map of commercial areas (see Attachment 3) to change the areas on Market Street and in South Rose Hill from Neighborhood Center to Residential Market.

The Commission may wish to discuss the RM 3.6 zoned Residential Market area at 64<sup>th</sup> and Lake Washington Boulevard to review how it fits into the refined hierarchy. In the attached Comprehensive Plan edits, staff suggests deleting the Residential Market reference to that RM 3.6 area. The policy base for that area would then be in the recently adopted Lakeview Neighborhood Plan, which supports the limited commercial use. Regulation would be through the RM 3.6 zone, which allows commercial use if approved through a conditional use permit on sites identified in the Comprehensive Plan.

4. Consider miscellaneous amendments to the maps and text of the affected neighborhood plans to ensure consistency with these amendments

Staff has proposed the following amendments to the Comprehensive Plan to implement the changes under consideration:

- Change Policy LU-5.9 to detach the concept of small markets in residential zones from the defined term "Residential Market". The intent is to preserve the concept that neighborhoods that do not have convenient neighborhood retail or services may wish to come to the City and to permit small individual stores or services within a residential area. This could entail something like a conditional use process to allow commercial use in a residential zone. This concept is fundamentally different than allowing commercial use of commercially zoned and designated property.
- Amend the neighborhood Land Use Maps to specify a density range for commercial areas with density limits (see Attachment 3). The intent is to be consistent with other land use designations so that if residential density limits exist, they are indicated on the Land Use Maps. For example, attached Figure MS-2 designates much of the Market Street Corridor for office/multifamily use and, because there is a residential density limit, the density (in units per acre) is specified on the maps. The Commission has left open the appropriate density limit pending consideration at the public hearing.

- Expand the text of the Moss Bay Neighborhood Plan to include a vision for the future of the commercial area at Lake Street South and 10<sup>th</sup> Street South (see Attachment 3). Currently, the text is limited to an explanation of current conditions in the area and does not address the facts that over 75% of the commercial area is either vacant or in single family use and that the existing commercial buildings are over 50 years old. Staff has prepared an initial draft of text to begin to establish some vision for the future of this area.

### Proposed KZC Amendments

Proposed draft Kirkland Zoning Code amendments are included as Attachment 4 and are summarized and discussed below:

1. Within Neighborhood Business (BN), Neighborhood Business A (BNA), Market Street Corridor 2 (MSC 2) zones:

- a. Consider establishing maximum residential density limits

Amendments to the Use Zone Charts for these zones would establish maximum residential density limits, expressed below in terms of how many units are allowed based on the parcel size (e.g. – x units per y square feet of land).

For the BNA zone, the draft amendments would establish residential density limits and a restriction of the overall percentage of gross floor area for residential use. The Commission's direction was to push the regulations for residential development back to something comparable to what the County had in place prior to annexation until the Comprehensive Plan is amended to establish a vision for these commercial areas. The draft charts establish a higher density (1 unit/1,800 square feet of land) for the northern BNA zone on Finn Hill based on the higher density surroundings of that area. The charts establish a lower density (1/2,400 s.f.) for the southern BNA zone on Finn Hill based on that area's surrounding zoning.

For the BN and MSC 2 zone, the Commission has discussed residential density limits ranging from 1/1,800 s.f. to 1/900 s.f., but deferred a specific recommendation until public comment is received.

Planned density adjoining various commercial zones is illustrated in Attachment 5. Attachment 6 illustrates actual built densities adjoining the Moss Bay BN zone.

Note that while affordable housing is typically required in density restricted zones, the City's affordable housing provisions have been written in as optional incentives. This is because legal requirements for mandatory affordable housing programs anticipate that the Code is providing additional development incentives to achieve that objective. In the case of the current amendments, because the City is contemplating density restrictions where none currently exist, the conditions do not exist to mandate affordable housing. However, by enabling a property owner to utilize the City's standard incentives, the opportunity to create affordable housing would be available.

Note also that draft charts suggest adding "Attached" dwelling units to the currently allowed "Stacked" dwelling units. Staff could not identify a reason why townhouse style (attached) development would not be appropriate in these zones.

*Note that the regulations b-d noted below only apply to new mixed use development containing residential uses. The Commission's considerations are to preserve commercial zones for predominantly commercial uses with redevelopment and to avoid creating nonconformances within existing development.*

b. Replace requirement for 75% commercial on ground floor with minimum commercial frontage requirements

The KZC currently requires 75% of the ground floor of all structures to be commercial use and prohibits residential use on the ground floor. The Commission has discussed that this method of measuring ground floor commercial is flawed because: the amount of commercial is dictated solely based on the size of the structure being built, it may force more commercial space than the market will bear or the neighborhood desires, and it does not adequately address other ground floor uses such as parking and residential.

For these neighborhood business zones, the draft charts replace the minimum percentage measurement with a requirement that with new mixed-use development, the property have commercial frontage along the streets. The intent is to preserve the neighborhood-serving commercial basis of these zones and work with the proposed setbacks and design standards (see below) to create pedestrian-oriented business districts.

c. Allow residential use on ground floor behind commercial frontage and limited residential lobbies within commercial frontage

In conjunction with the commercial frontage requirements noted above, residential, parking and other uses allowed in the zone could be located on the ground floor but would have to be placed behind the intervening commercial frontage. Lobbies could be located along the building's commercial frontage, but are limited to 20% of the frontage to preserve the predominant commercial character.

d. Require a minimum height for ground floor commercial and allow increase in building height (in BN and MSC 2 zones) if needed to accommodate ground floor commercial

The draft codes require the ground floor commercial space to provide a minimum height of 13'. The intent is to provide space that is adaptable to various office, retail, and restaurant tenants over time. The height works in conjunction with proposed design guidelines to create transparent building frontages for pedestrian interest.

One concern identified by the Planning Commission was that if the ground floor is required to be taller, it may compromise a property owner's ability to achieve a three-story building within current height limits. Therefore, the draft code would allow a three

foot increase in total building height (e.g. – from a maximum of 30 feet to a maximum of 33 feet), but only for a three story building. That provision has not been included for the BNA zone because the current height limit is 35 feet in that zone.

Note that within all City zones subject to design review, KCZ 115.60.2.d also allows a five foot height bonus for pitched roofs.

e. Reduce required front yards and establish requirements for improved pedestrian orientation

The draft codes reduce the required front yard setback from 20 feet to 0 feet for commercial and mixed use developments. Working in concert with the commercial frontage requirements discussed above; and requirements for wide sidewalks, pedestrian weather protection, and design review discussed below; the intent is to provide the tools to create pedestrian-oriented neighborhood business districts with commercial uses rather than parking lots lining the streets.

The setbacks for the BNA zone are not included in the draft code because they are currently at 10 feet (rather than 20 feet) and the Commission discussed waiting for future neighborhood planning.

f. Standardize setbacks for retail and office uses

The current codes establish different side yards setbacks for office uses (5' minimum, 15' combined) than those for retail uses (10' minimum). The unintended result is that a new commercial building could get built with ground floor office uses and space could not be converted to a retail use in the future due to the different setback. The draft code would establish minimum require side yard setbacks for office and commercial uses at 10 feet.

g. Standardize required land use buffers for retail and office uses

As with the setbacks discussed above, the current code establishes different land use buffer standards for office and retail uses, posing the same potential issues with changes in use over time and creating a disincentive to build retail project in these zones. The draft code standardizes these buffers, with a 10' width corresponding to the required setbacks.

h. Reduce maximum store size (applies to BN zone only)

The current code establishes a 10,000 square foot maximum size for retail uses to ensure the zone attracts stores that provide neighborhood rather than broader regional services. There are currently exceptions for some retail uses in the BNA zone to accommodate retail uses that existed at the time of annexation. The MSC 2 zone currently has a 4,000 square foot limit.

Based on the hierarchy of commercial zones established in the Land Use Element of the Comprehensive Plan, with Residential Market being the least intensive and intended to serve the immediate neighborhood, the draft code establishes a 4,000 square foot store size limit in the BN zone similar to the MSC 2 zone. Policies in the South Rose Hill Neighborhood Plan also emphasize the neighborhood orientation of commercial uses in that BN zoned area. Store size limits for the BNA zone would not be changed. Staff has reviewed business license data for existing uses in these zones and has not identified any uses that would exceed this limit, so no nonconformance will be created with this amendment.

i. Prohibit office use on upper floors of BN zone in the Moss Bay Neighborhood

The Moss Bay Neighborhood Plan acknowledges the existing market in this zone that serves primarily as a convenience to the surrounding residential area and encourages that limited commercial use to remain. The Plan also discusses issues with vehicular ingress and egress in the area. The Plan does not mention any employment need or function of the area. To limit the intensity of new mixed use development in the area and impacts on surrounding residential areas, in accordance with the Comprehensive Plan the Planning Commission discussed limiting the upper floors of mixed use development to residential use. Therefore, the draft code would preclude office use above the street level floor of buildings in the Moss Bay Neighborhood BN zone. This would allow residential as the preferred mixed use component of a mixed use development, which is typically less intensive in terms of land use compatibility and trip generation than office use.

j. Prohibit auto-oriented uses such as vehicle service stations and drive-through (applies to BN and MSC 2 zones only)

The emphasis of the Residential Market designation in the Comprehensive Plan is on pedestrian orientation of commercial uses. The MSC 2 zone currently contains restrictions on auto-oriented uses such as gas stations, drive through, and drive in facilities. Similarly, the South Rose Hill Neighborhood Plan discourages those auto-oriented uses and the existing land use permit for the BN zone in that neighborhood established appropriate restrictions. The draft code would adopt those policies into specific prohibitions within the BN zone.

Similar policies are not in place for the BNA zone and there are a number of auto-oriented uses in existence. Therefore, the restrictions are not proposed for the BNA zone.

k. Require design review process for new development and establish design guidelines/regulations

The draft code would require new development to be reviewed through design review. Based on current processes established in KZC 142, new development taller than one story or exceeding 10,000 square feet would be reviewed by the Design Review Board. Smaller project are reviewed administratively. The current code establishes an

exemption from Design Board Review for projects in MSC 2 and the draft code would eliminate this exemption to make it consistent with other neighborhood business districts.

The proposal would utilize the City's existing Design Guidelines for Pedestrian-Oriented Business Districts as the guidelines for new projects in these zones. The existing guidelines have been adapted to include a purpose statement for design of these districts and a number of special considerations for the projects (see Attachment 7). The following new special considerations and guidelines are included based on previous concerns identified by the Planning Commission:

- Addressing the grade relationship between the commercial space and the adjoining sidewalk
- Addressing the avoidance of long facades
- Addressing the need for upper story step backs

2. Within the Community Business X (BCX), Community Business 1 (BC 1), and Community Business 2 (BC 2) zones:

a. Replace requirement for 75% commercial on ground floor with a minimum commercial floor area ratio

The KZC currently requires 75% of the ground floor of all structures to be commercial use and prohibits residential use on the ground floor. The Commission has discussed that this method of measuring ground floor commercial is flawed because: the amount of commercial is dictated solely based on the size of the structure being built, it may force more commercial space than the market will bear or the neighborhood desires, and it does not adequately address other ground floor uses such as parking and residential. However, the Commission also noted the importance that the Comprehensive Plan places on preserving commercially zones properties for commercial use.

For these community business zones, the draft charts replace the minimum percentage measurement with a minimum floor area ratio requirement and commercial frontage requirements along the streets. The floor area ratio provision establishes that a new mixed use development must provide a minimum commercial floor area equal to 25% of the parcel size (e.g. – development of a 10,000 square foot parcel must include 2,500 square feet of commercial use).

A map of existing floor area ratios within Kirkland's commercial zones is included as Attachment 8.

b. Allow residential use on ground floor behind commercial frontage and limited residential lobbies within commercial frontage

Working in conjunction with the minimum floor area ratio requirement, the draft code addresses placement of the residential within mixed use development by requiring the

commercial space to be oriented to the street and the residential only allowed on the ground floor if placed behind an intervening commercial frontage.

- c. Require a minimum height for ground floor commercial and allow increase in building height (in BCX zone) if needed to accommodate ground floor commercial

The draft codes require the ground floor commercial space to provide a minimum height of 13'. The intent is to provide space that is adaptable to various office, retail, and restaurant tenants over time.

One concern identified by the Planning Commission was that if the ground floor is required to be taller, it may compromise a property owner's ability to achieve a three-story building within current height limits. Therefore, the draft code for the BCX zone would allow a three foot increase in total building height (e.g. – from a maximum of 30 feet to a maximum of 33 feet), but only for a three story building. That provision has not been included for the BC 1 and BC 2 zones because the current height limit is 35 feet in BC 1 and 35-60 feet in BC 2.

*Note that the regulations noted in a-c above only apply to new mixed use development containing residential uses. The Commission's consideration was to preserve commercial zones for predominantly commercial uses and to not create any nonconformances with these regulations.*

## Public Notice

Pursuant to KZC 160.40, notice of the hearing was published in the official City newspaper, posted on office notice boards, and posted on the City website. In addition, the notice was sent to:

- All owners of property affected by the propose changes
- Each affected neighborhood association with a description of the changes for the neighborhood
- Individuals who were previously invited to address the Commission on the Moss Bay Neighborhood BN zone
- Subscribers to the City's email updates for the pending project in the Moss Bay Neighborhood BN zone

The City has also created a project webpage for the process at

[http://www.kirklandwa.gov/depart/Planning/Code\\_Updates/Commercial\\_Code\\_and\\_Plan\\_Amendments\\_2012.htm](http://www.kirklandwa.gov/depart/Planning/Code_Updates/Commercial_Code_and_Plan_Amendments_2012.htm) and the City has directed interested parties to visit that page for detailed information.

## Public Comment

The City has received extensive correspondence on the proposed amendments. Correspondence that was submitted during the course of the Planning Commission study session has previously been forwarded to and reviewed by the Planning Commission. Attachment 10 provides copies of all public comment previously received by the Commission during the study sessions (prior to May 23, 2012). Attachment 9 provides copies of public comment received since May 23, 2012.

**Attachments**

1. Planning Commission Matrix of Potential Amendments for Hearing
2. Land Use Map LU-2 and Related Definitions
3. Draft Comprehensive Plan Amendments
4. Draft Zoning Code Amendments
5. Planned Residential Densities Map
6. Built Density Map – Moss Bay BN Area
7. Draft Design Guideline Amendments
8. Map of Commercial Floor Area Ratios
9. Public Comment received after May 23, 2012
10. Public Comment received prior to May 23, 2012



## Potential Comprehensive Plan & Zoning Code Amendments for Neighborhood Business Family of Zones

Chart shows current policies/regulations and the Planning Commission (PC) direction for amendments to be considered at the June 28<sup>th</sup> Public hearing

Zone	BN (Lake St S)		BN (1) (NE 70 <sup>th</sup> PI)		BNA (Juanita Dr)		MSC 2 (Market St)	
	Current	PC Direction	Current	PC Direction	Current	PC Direction	Current	PC Direction
<b>Comprehensive Plan</b>	Residential Market	Consider Council suggestion for redesignation to Neighborhood Center Initial preference to retain Residential Market designation and adjust definition (i.e. - clarify scale so the designation is appropriate for the size of sites with the designation)	Neighborhood Center	Change to Residential Market	Neighborhood Center	No change	Neighborhood Center	Change to Residential Market
<b>Residential Density</b>	None	Establish density limit between 24-48 units/acre (t.b.d. following hearing)	None	Establish density limit between 24-48 units/acre (t.b.d. following hearing)	None	<ul style="list-style-type: none"> <li>24 units/acre for north area, 18 units/acre for south area</li> <li>Residential square feet not to exceed 50% of the site's total square feet of floor area</li> </ul>	None	Establish density limit between 24-48 units/acre (t.b.d. following hearing)
<b>Minimum Commercial Floor Area</b>	75% of ground floor	Minimum commercial frontage	75% of ground floor	Minimum commercial frontage	75% of ground floor	None	75% of ground floor	Minimum commercial frontage
<b>Residential on Ground Floor of Structure</b>	Prohibited	<ul style="list-style-type: none"> <li>Allow behind commercial frontage</li> <li>Res. lobby allowed in comm. frontage</li> </ul>	Prohibited	<ul style="list-style-type: none"> <li>Allow behind commercial frontage</li> <li>Res. lobby allowed in comm. frontage</li> </ul>	Prohibited	Allow, subject to 50% requirement above	Prohibited	<ul style="list-style-type: none"> <li>Allow behind commercial frontage</li> <li>Res. lobby allowed in comm. frontage</li> </ul>
<b>Commercial Orientation</b>	Toward arterial or sidewalk	<ul style="list-style-type: none"> <li>Toward arterial or sidewalk</li> <li>Minimum 13' ground floor height</li> <li>Design guidelines to address grade</li> </ul>	Toward arterial or sidewalk	<ul style="list-style-type: none"> <li>Toward arterial or sidewalk</li> <li>Minimum 13' ground floor height</li> <li>Design guidelines to address grade</li> </ul>	Toward arterial or sidewalk	<ul style="list-style-type: none"> <li>Toward arterial or sidewalk</li> <li>Minimum 13' ground floor height</li> <li>Design guidelines to address grade</li> </ul>	Toward arterial or sidewalk	<ul style="list-style-type: none"> <li>Toward arterial or sidewalk</li> <li>Minimum 13' ground floor height</li> <li>Design guidelines to address grade</li> </ul>
<b>Maximum Height</b>	30'	Allow 3' increase only as needed to accommodate the proposed 13' ground floor height	30'	Allow 3' increase only as needed to accommodate the proposed 13' ground floor height	35'	No change	30'	Allow 3' increase only as needed to accommodate the proposed 13' ground floor height

## Potential Comprehensive Plan & Zoning Code Amendments for Neighborhood Business Family of Zones (cont.)

Zone	BN (Lake St S)		BN (1) (NE 70 <sup>th</sup> Pl)		BNA (Juanita Dr)		MSC 2 (Market St)	
	Current	PC Direction	Current	PC Direction	Current	PC Direction	Current	PC Direction
<b>Required Yards<sup>1</sup></b>	20' front <sup>2</sup> 10' side (retail) 5' min/15 total (office) 10' rear (typ.)	<ul style="list-style-type: none"> <li>0' front where commercial at grade, 10' sidewalks &amp; ped. weather protection</li> <li>Design guidelines address massing above ground floor</li> <li>10' side &amp; rear for all uses</li> </ul>	20' front 10' side (retail) 5' min/15 total (office) 10' rear (typ.)	<ul style="list-style-type: none"> <li>0' front where commercial at grade with 10' sidewalks &amp; ped. weather protection</li> <li>Design guidelines address massing above ground floor</li> <li>10' side &amp; rear for all uses</li> </ul>	10' front 10' side (retail) 5' min/15 total (office) 10' rear (typ.)	<ul style="list-style-type: none"> <li>No change to front</li> <li>10' sides for office</li> </ul>	20' front 10' side (retail) 5' min/15 total (office) 10' rear (typ.)	<ul style="list-style-type: none"> <li>0' front where commercial at grade, 10' sidewalks &amp; ped. weather protection</li> <li>Design guidelines address massing above ground floor</li> <li>10' side &amp; rear for all uses</li> </ul>
<b>Land Use Buffer</b>	Retail=15' adjoining SF or MF Office=15' adjoining SF, 5' adjoining MF	10' for all commercial uses adjoining residential, develop planting standards	Retail=20' adjoining SF, 15' adjoining MF Office=20' adjoining SF, 5' adjoining MF <sup>3</sup>	10' for all commercial uses adjoining residential, develop planting standards	Retail=15' adjoining SF or MF Office=15' adjoining SF, 5' adjoining MF	10' for all commercial uses adjoining residential, develop planting standards	Retail=15' adjoining SF or MF Office=15' adjoining SF, 5' adjoining MF	10' for all commercial uses adjoining residential, develop planting standards
<b>Maximum Store Size</b>	10,000 s.f. per establishment	4,000 per establishment	10,000 s.f. per establishment	4,000 per establishment, check that no nonconformances are created	10,000 s.f. per establishment (excluding grocery, drug, hardware...)	No change	4,000 s.f. per establishment	No change
<b>Use Limitations</b>	Use Zone Charts	<ul style="list-style-type: none"> <li>Prohibit Office use on upper floors</li> <li>Prohibit non-pedestrian oriented (e.g. vehicle service station &amp; drive-thru) with mixed use redevelopment</li> </ul>	Use Zone Charts	Prohibit non-pedestrian oriented (e.g. vehicle service station & drive-thru) with mixed use redevelopment	Use Zone Charts	No change	Limited in Use Zone Charts	Prohibit non-pedestrian oriented (e.g. vehicle service station & drive-thru) with mixed use redevelopment
<b>Maximum Building Length<sup>4</sup></b>	None	Address through design guidelines	None	Address through design guidelines	None	Address through design guidelines	Design regulations	Address through design guidelines
<b>Review Process</b>	None	Design Board Review	Process IIA	<ul style="list-style-type: none"> <li>Design Board Review</li> <li>Incorporate Comp Plan criteria into special regulations</li> </ul>	None	Design Board Review	Administrative Design Review	Design Board Review

<sup>1</sup> Note that office has 5' minimum side (15' combined)

<sup>2</sup> Required yard along Lake St S or LWB increased 2' for each 1' that the structure exceeds 25' (applies to RM along Boulevard as well)

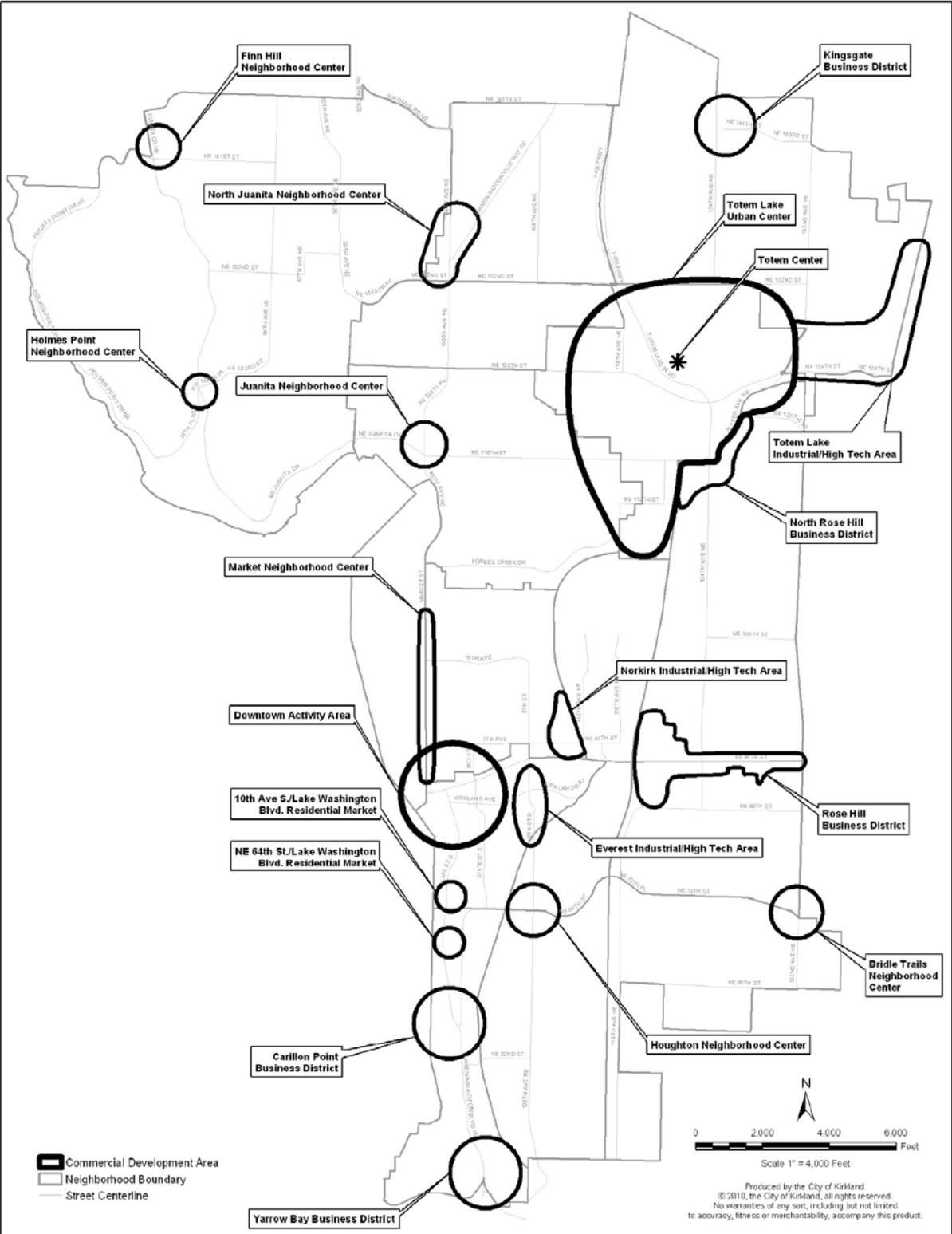
<sup>3</sup> 20' landscaped berm/topographic change required by (1) suffix

<sup>4</sup> Used in Design Regulations. Depending on Business District, regulations may require full building separation, a significant modulation break, or change in building definition and materials

## Potential Zoning Code Amendments for Community Business (BC) Family of Zones

Zone	BCX		BC 1		BC 2	
	Current	PC Direction	Current	PC Direction	Current	PC Direction
<b>Minimum Commercial Floor Area</b>	75% of ground floor	Minimum commercial FAR of 25% for new mixed use	75% of ground floor	Minimum commercial FAR of 25% for new mixed use	75% of ground floor	Minimum commercial FAR of 25% for new mixed use
<b>Residential on Ground Floor of Structure</b>	Prohibited	Allowed, but must have intervening commercial frontage along street	Prohibited	Allowed, but must have intervening commercial frontage along street	Prohibited	Allowed, but must have intervening commercial frontage along street
<b>Commercial Orientation</b>	Toward arterial or sidewalk	<ul style="list-style-type: none"> <li>• Toward arterial or sidewalk</li> <li>• Minimum 13' ground floor height (Allow 3' increase in 30' total building height only as needed to accommodate the proposed 13' ground floor height)</li> </ul>	Toward arterial or sidewalk	<ul style="list-style-type: none"> <li>• Toward arterial or sidewalk</li> <li>• Minimum 13' ground floor height</li> </ul>	Toward arterial or sidewalk	<ul style="list-style-type: none"> <li>• Toward arterial or sidewalk</li> <li>• Minimum 13' ground floor height</li> </ul>





Map LU-2

Pg VI-13: Commercial land uses are a critical part of the Kirkland community. They provide shopping and service opportunities for Kirkland residents, and also create employment within the City. The tax revenues generated by business help fund the capital facilities and public services that residents enjoy.

In return, the quality of life in the City's neighborhoods provides a main attraction for both businesses and their patrons. The proximity to Lake Washington, the fine system of parks, the availability of a regional medical center with good medical care, top notch educational facilities, the environmental ethic of the community, and quality infrastructure attract outsiders to Kirkland and make the City a good place to do business – for employers, employees, and customers.

Problems that the community faces – traffic congestion, particularly – create concerns for commercial land uses. Ease of transporting goods and adequate parking are especially important. An underlying premise of the Land Use Element, expressed in the Vision Statement, is that, in the future, residents of the City will not drive as much as they do presently to minimize traffic congestion and reduce parking needs. To that end, the Element attempts to promote commercial land use patterns that support alternative transportation modes and locate housing in commercial areas where appropriate.

Along with the need to provide new housing units for future residents, the City will need to designate adequate land area for commercial uses, some of which may employ Kirkland residents. If the opportunity for local employment is increased, the high proportion of residents who work outside the community may be reduced. This in turn would ease traffic congestion by shortening commute trips and making other modes of travel to work more feasible.

Currently, a hierarchy of "commercial development areas" exists in the City, based primarily on size and relationship to the regional market and transportation system (see Figure LU-2: Commercial Areas).

Some of Kirkland's commercial areas serve primarily the surrounding neighborhood; others have a subregional or regional draw. Most of the larger commercial areas are centered around major intersections. They depend on principal arterials, the freeway, or the railroad for goods transport and for bringing in workers or customers. Smaller commercial areas, Neighborhood Centers, for example, have a more localized draw. Residents depend on their neighborhood grocery store, dry cleaners, bank, etc., for everyday needs.

The Land Use Element provides general direction for development standards in commercial areas and describes the future of specific commercial areas in Kirkland. The following terms are used in the discussion of commercial land uses:

#### **Urban Center**

An Urban Center is a regionally significant concentration of employment and housing, with direct service by high-capacity transit and a wide range of land uses, such as retail, recreational, public facilities, parks and open space. An Urban Center has a mix of uses and densities to efficiently support transit as part of the regional high-capacity transit system.

#### **Activity Area**

An Activity Area is an area of moderate commercial and residential concentration that functions as a focal point for the community and is served by a transit center.

#### **Business District**

A Business District is an area that serves the subregional market, as well as the local community. These districts vary in uses and intensities and may include office, retail, restaurants, housing, hotels and service businesses.

### **Neighborhood Center**

A Neighborhood Center is an area of commercial activity dispensing commodities primarily to the neighborhood. A supermarket may be a major tenant; other stores may include a drug store, variety, hardware, barber, beauty shop, laundry, dry cleaning, and other local retail enterprises. These centers provide facilities to serve the everyday needs of the neighborhood. Residential uses may be located on upper stories of commercial buildings in the center.

### **Residential Market**

A residential market is an individual store or very small, mixed-use building/center focused on local pedestrian traffic. Residential scale and design are critical to integrate these uses into the residential area. Uses may include corner grocery stores, small service businesses (social service outlets, daycares), laundromats, and small coffee shops or community gathering places.



## VI. LAND USE

identified. In the Land Use Element, the general notion of protection of community character is promoted. However, this Element also acknowledges that the community will be growing and that a balance must be struck between providing more housing units and preserving the neighborhoods as they are today.

Several of the most important housing issues – affordability, special needs housing, and accessory units – are not addressed in this Element. They are discussed, instead, in the Housing Element.

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***Goal LU-4: Protect and enhance the character, quality, and function of existing residential neighborhoods while accommodating the City's growth targets.***

---

***Policy LU-4.1: Maintain and enhance Kirkland's single-family residential character.***

The community vision, as described in the Vision Statement of this Plan, is that Kirkland's residential areas are diverse with a variety of housing choices including single-family detached, attached, stacked, cottage, carriage styles and accessory dwelling units.

***Policy LU-4.2: Locate the most dense residential areas close to shops and services and transportation hubs.***

Denser residential areas such as apartments and condominiums should continue to be sited close to or within commercial areas and transportation hubs to increase the viability of the multimodal transportation system.

***Policy LU-4.3: Continue to allow for new residential growth throughout the community, consistent with the basic pattern of land use in the City.***

Although the Land Use Element states that opportunities for new housing units should be dispersed throughout the community, significantly greater densities are not targeted for low-density neighborhoods. Instead, infill development is expected in these areas based on availability of developable land, while higher densities are clustered near existing commercial areas.

***Policy LU-4.4: Consider neighborhood character and integrity when determining the extent and type of land use changes.***

Protection of community character is a theme woven throughout the Land Use Element. Community character is most clearly expressed through the Neighborhood Plans. It is the intent of this policy to direct specific consideration of the unique characteristics of neighborhoods, as described in the Neighborhood Plans, before committing to major area-wide residential land use changes.

### COMMERCIAL LAND USES

Commercial land uses are a critical part of the Kirkland community. They provide shopping and service opportunities for Kirkland residents, and also create employment within the City. The tax revenues generated by business help fund the capital facilities and public services that residents enjoy.

In return, the quality of life in the City's neighborhoods provides a main attraction for both businesses and their patrons. The proximity to Lake Washington, the fine system of parks, the availability of a regional medical center with good medical care, top notch educational facilities, the environmental ethic of the community, and quality infrastructure attract outsiders to Kirkland and make the City a good place to do business – for employers, employees, and customers.

Problems that the community faces – traffic congestion, particularly – create concerns for commercial land uses. Ease of transporting goods and adequate parking are especially important. An underlying premise of the Land Use Element, expressed in the Vision Statement, is that, in the future, residents of the City will not drive as much as they do presently to minimize traffic congestion and reduce parking needs. To that end, the Element attempts to promote commercial land use patterns that support alternative transportation modes and locate housing in commercial areas where appropriate.

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## VI. LAND USE

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The Land Use Element provides general direction for development standards in commercial areas and describes the future of specific commercial areas in Kirkland. The following terms are used in the discussion of commercial land uses:

### Urban Center

An Urban Center is a regionally significant concentration of employment and housing, with direct service by high-capacity transit and a wide range of land uses, such as retail, recreational, public facilities, parks and open space. An Urban Center has a mix of uses and densities to efficiently support transit as part of the regional high-capacity transit system.

### Activity Area

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### Neighborhood Center

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### Light Industrial/High Technology Area

A Light Industrial/High Technology area serves both the local and regional markets and may include office, light manufacturing, high technology, wholesale trade, storage facilities and limited retail.

Residential uses may be located above or behind commercial uses in the center, at densities specified in the applicable neighborhood plan.

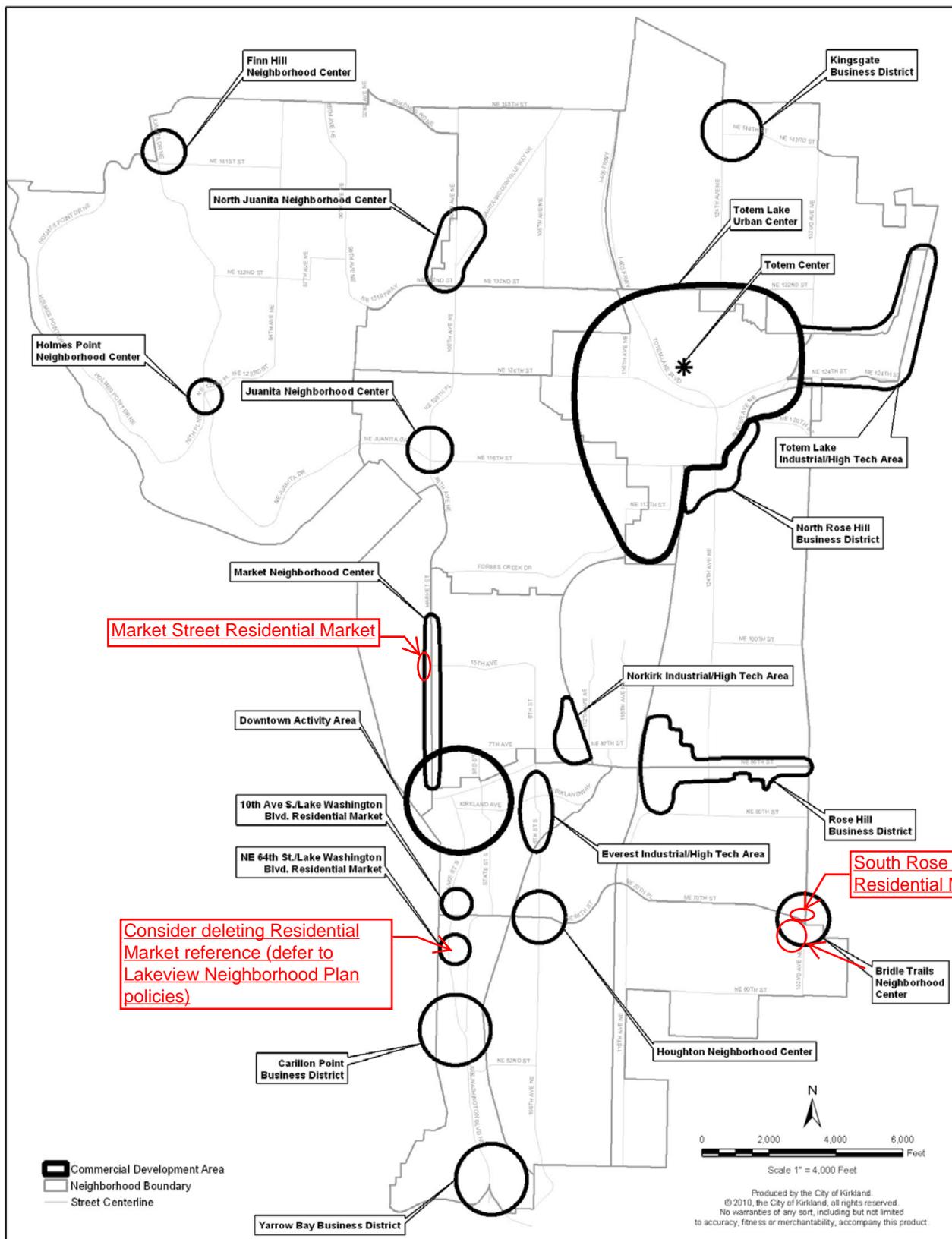


Figure LU-2: Commercial Areas

## VI. LAND USE

**Goal LU-5:** *Plan for a hierarchy of commercial development areas serving neighborhood, community, and/or regional needs.*

**Policy LU-5.1:** *Reflect the following principles in development standards and land use plans for commercial areas:*

### *Urban Design*

- ◆ *Create lively and attractive districts with a human scale.*
- ◆ *Support a mix of retail, office, and residential uses in multistory structures.*
- ◆ *Create effective transitions between commercial area and surrounding residential neighborhood.*
- ◆ *Protect residential areas from excessive noise, exterior lighting, glare, visual nuisances, and other conditions which detract from the quality of the living environment.*

### *Access*

- ◆ *Encourage multimodal transportation options, especially during peak traffic periods.*
- ◆ *Promote an intensity and density of land uses sufficient to support effective transit and pedestrian activity.*
- ◆ *Promote a street pattern that provides through connections, pedestrian accessibility and vehicular access.*
- ◆ *Encourage pedestrian travel to and within the commercial area by providing:*
  - *Safe and attractive walkways;*
  - *Close groupings of stores and offices;*
  - *Structured and underground parking to reduce walking distances and provide overhead weather protection; and*

- *Placement of off-street surface parking to the back or to the side of buildings to maximize pedestrian access from the sidewalk(s).*

- ◆ *Promote non-SOV travel by reducing total parking area where transit service is frequent.*

Each commercial area has its own unique attributes, although generalized development guidelines which work to preserve community character and support a multimodal transportation system are described in the above policies. Particular emphasis is placed on improving pedestrian accessibility in commercial areas.

These policies recognize that urban design is important, and that well-designed commercial areas, in partnership with Kirkland's residential neighborhoods, will project a positive community image.

Good urban commercial design complements and enhances adjacent residential areas.

**Policy LU-5.2:** *Maintain and strengthen existing commercial areas by focusing economic development within them and establishing development guidelines.*

The intent of this policy is that future economic development be concentrated in existing commercial areas. This concentration can help to maintain and strengthen these areas and also promote orderly and efficient growth that minimizes impacts and service expansion costs. Concentration also allows businesses to benefit from proximity to each other.

Intensification, rather than expansion of the boundaries of existing commercial areas into surrounding residential neighborhoods, is desirable. Infilling is preferred, particularly when it would create a denser pattern of development that is focused less on the private automobile and more on the opportunity for multiple transportation modes. Redevelopment may also provide new opportunities, especially in commercial areas where the community vision has changed over time.

## VI. LAND USE

**LU-5.8:** *Promote development within the Bridle Trails, Houghton, and Juanita Neighborhood Centers that becomes part of the neighborhood in the way it looks and in the functions it serves.*

Neighborhood centers provide services to surrounding residential neighborhoods so that residents may shop close to home. They also may function as the focal point for a community. Because of these important ties to their neighborhood, neighborhood centers should develop in ways that provide goods and services needed by the local residents, enhance physical connections to the surrounding neighborhoods, foster good will and provide an opportunity for people to mingle and converse.

**Policy LU-5.9:** ~~Allow residential markets,~~ subject to the following development and design standards:

- ◆ *Locate small-scale neighborhood retail and personal services where local economic demand and local citizen acceptance are demonstrated.*
- ◆ *Provide the minimum amount of off-street parking necessary to serve market customers.*
- ◆ *Ensure that building design is compatible with the neighborhood in size, scale, and character.*

The intent of this policy is to permit small individual stores or service businesses in residential areas on a case-by-case basis. These businesses should cater to nearby residents, be oriented to pedestrian traffic, and require very little customer parking. They should be designed and located in a manner that is compatible with adjacent residences and that will not encourage the spread of commercial uses into residential areas. They should be located where local economic demand and neighborhood acceptance can be demonstrated.

**Goal LU-6:** *Provide opportunities for a variety of employment.*

**Policy LU-6.1:** *Provide opportunities for light industrial and high technology uses.*



405 Corporate Center

While Kirkland is not interested in recruiting heavy industry, the City is supportive of existing industrial enterprises and wants to encourage new high-technology businesses to locate here.

Policies that encourage residential and retail encroachment in industrial areas drive up the cost of land and promote conflicts which may force displacement of industrial operations. The strategy in the Land Use Element is to maintain industrial uses, while acknowledging that, in some parts of the City, industrial lands may be considered for conversion to other land uses.

Recognizing that each industrial area in the City has its own distinct character, the range of uses may vary between districts and may include some nonindustrial uses. Factors which should be taken into account when determining appropriate land uses include existing uses, surrounding uses, the local transportation system, and the effect on maintenance of primary jobs in the local job market.

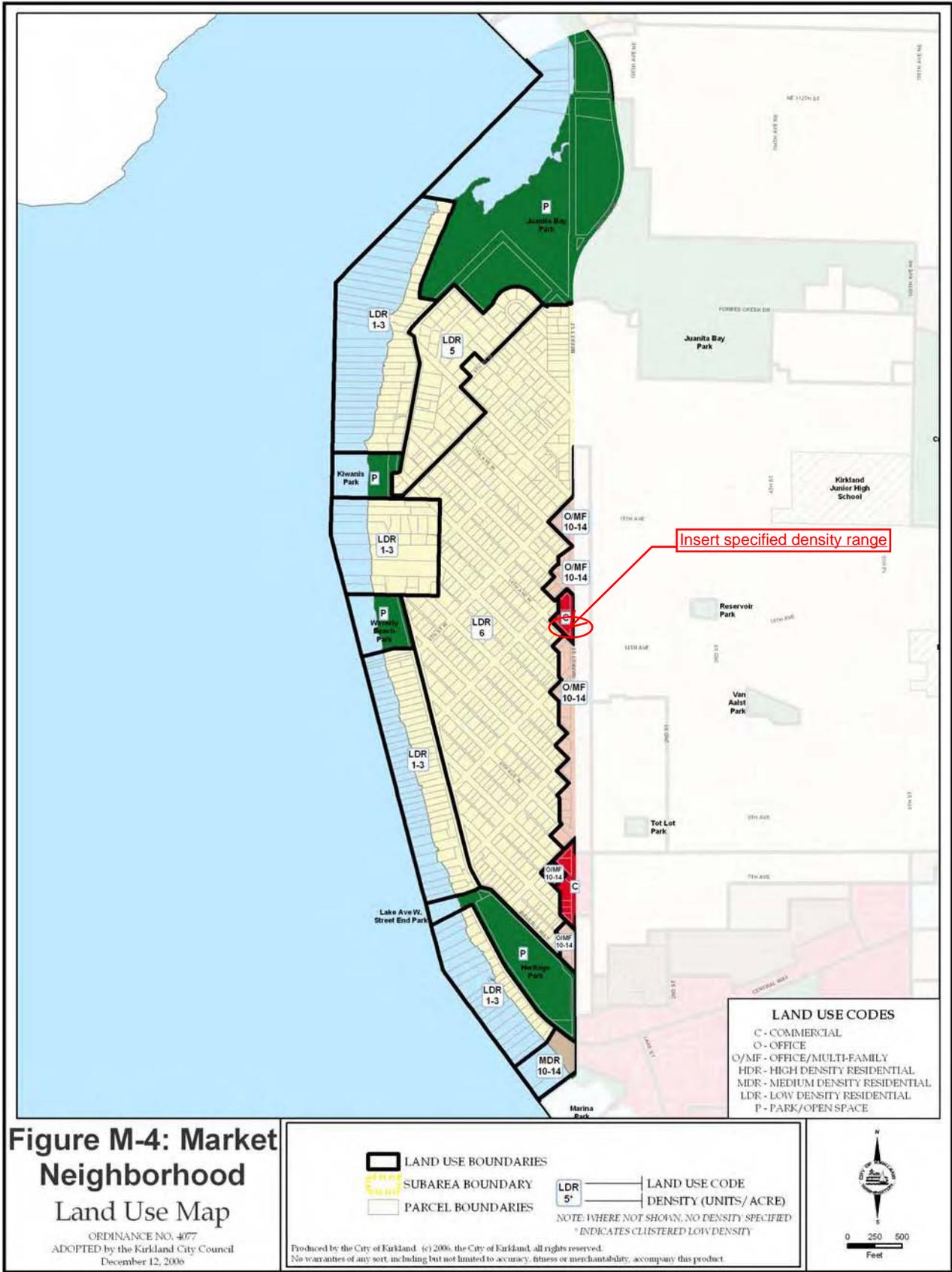


Figure M-4: Market Neighborhood Land Use

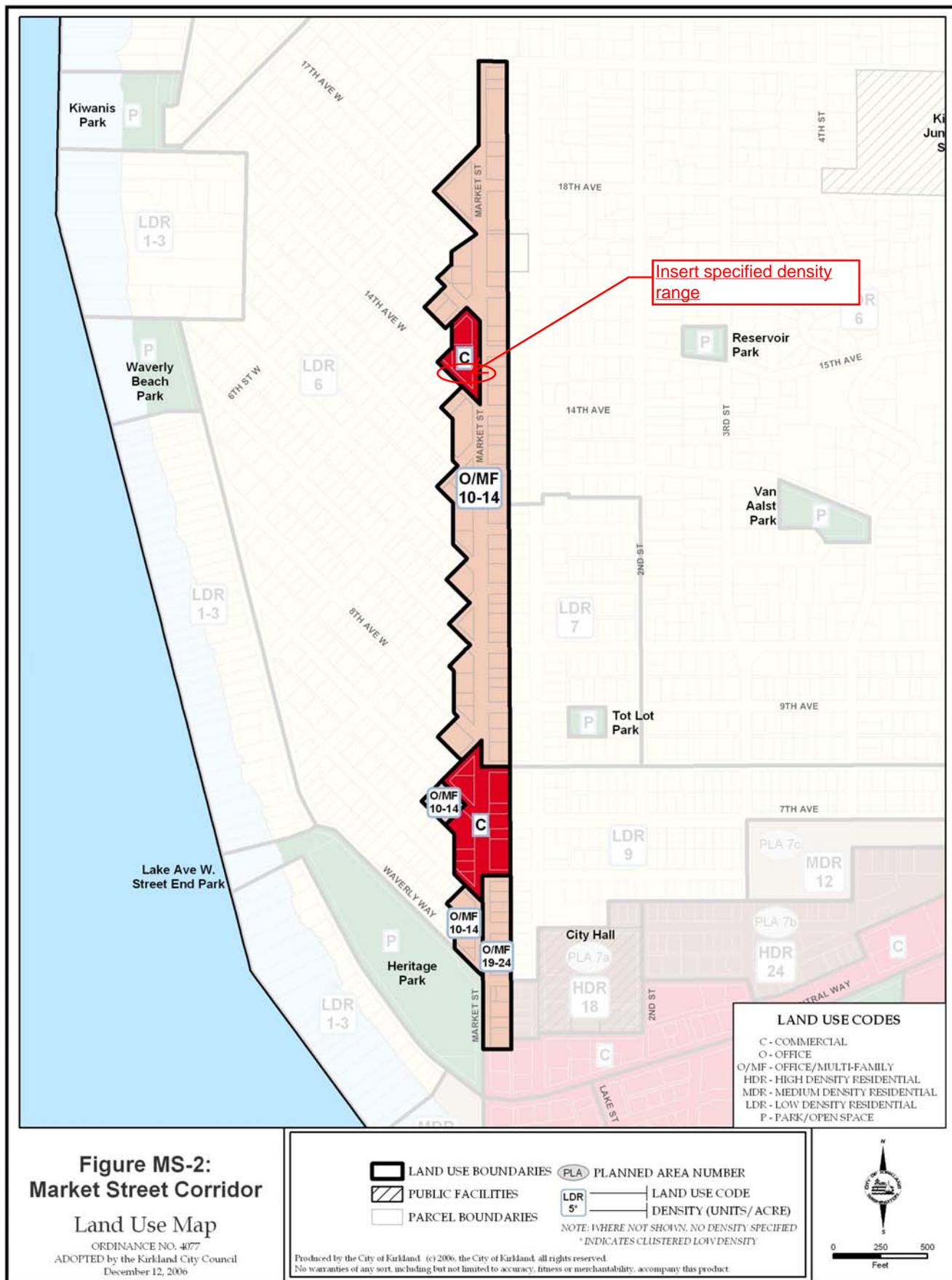


Figure MS-2: Market Street Corridor Land Use

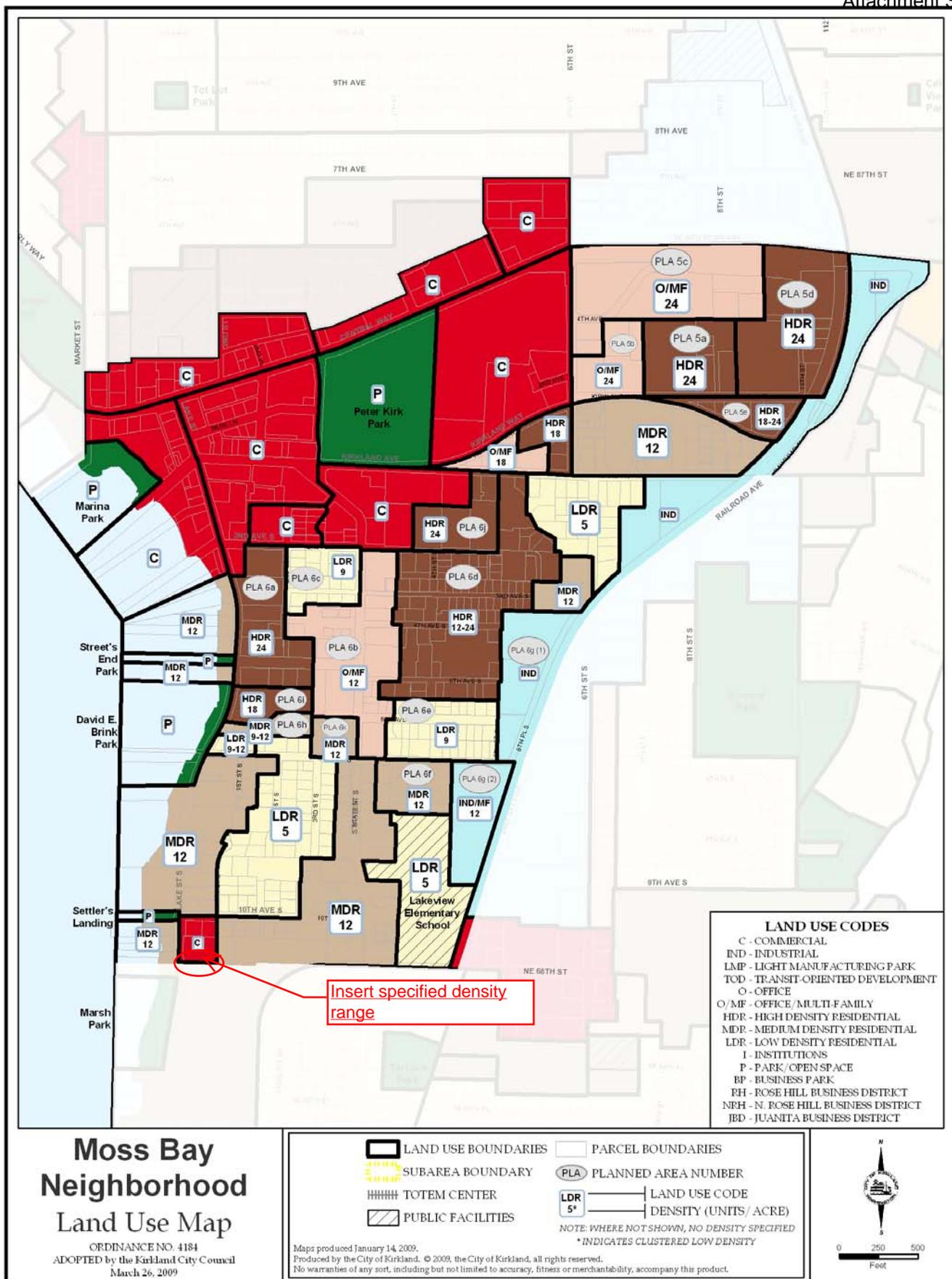


Figure MB-2: Moss Bay Area Land Use

## XV.D. MOSS BAY NEIGHBORHOOD

### 4. PERIMETER AREAS

*Land on the east side of Lake Street South is generally not suitable for commercial development.*

Most of the land on the east side of Lake Street South appears to be unsuitable for commercial use because of steep slope conditions, as well as problems concerning vehicular ingress and egress. The southeast quadrant of the 10th Street South and Lake Street intersection, however, is developed with a market which serves as a convenience to the surrounding residences. Limited commercial use of this location, therefore, should be allowed to remain.

*Industrial activities east of the railroad tracks described.*

The strip of land located east of the railroad tracks, south of Central Way and west of Kirkland Way, contains an existing light industrial use. While the area's proximity to I-405 and NE 85th Street makes it attractive for commercial development, the area is also near residential uses, and should be subject to greater restrictions than other industrial areas. Buildings should be well screened by a landscaped buffer, and loading and outdoor storage areas should be located away from residential areas. In addition, the number and size of signs should be strictly limited, with only wall- and ground-mounted signs permitted. Pole signs, such as the one currently located in this gateway area, are inappropriate. Finally, it is noted in the Everest Neighborhood Plan that there is a major territorial view at the intersection of NE 85th Street and Kirkland Way. This view of Lake Washington, Seattle, the Olympic Mountains and Downtown Kirkland falls over property in this area.

### C. PLANNED AREA 5

*High-density residential and office uses permitted in Planned Area 5.*

The eastern portion of the Moss Bay Neighborhood has been designated as Planned Area 5. Due to topographic conditions and circulation patterns, land in Planned Area 5 is relatively secluded. The area has been designated for high-density residential and office uses because of the ability to buffer such high-density development from other uses in the area. The area is developed primarily in high-density residential development while limited office uses

To mitigate impacts to the adjoining residential area, new development should be subject to the following standards:

- (1) Commercial uses should be oriented to serving the neighborhood. Uses should not include vehicle service stations, drive-in or drive through businesses, auto service and sales, or storage facilities.
- (2) As part of mixed use development, upper floors should be limited to residential uses rather than office uses and residential should be limited to a density of \_\_\_\_\_ units per acre.
- (3) Design review should be used to address scale, context, and pedestrian orientation of new development.

to develop with high-density residential uses (up to 24 dwellings/acre).

#### West B Subarea

The southern portion of Subarea B is adjacent to 6th Street and the entire subarea is south of 4th Avenue. Subarea B is heavily impacted by traffic, as well as existing and future commercial uses and offices to the west. The noise and traffic make this area inappropriate for single-family use, while its ease of access and proximity to the Downtown makes it appropriate for both offices and multifamily uses at a density of up to 24 dwelling units per acre. New development in this subarea should minimize access points directly onto 6th Street. Access for offices, however, should be provided exclusively from 6th Street or 4th Avenue and precluded from Kirkland Way. Structures should be limited to three stories in height.

## XV.G. SOUTH ROSE HILL NEIGHBORHOOD

- (8) Vegetative buffering (preferably with native, drought-tolerant plants) should be provided next to single-family areas.

***Existing multifamily areas south of NE 80th Street and east of 116th Avenue NE should remain zoned as low-density development due to impacts. Redevelopment should focus on vegetation preservation and access.***

Existing multifamily housing located south of NE 80th Street and east of 116th Avenue NE is impacted by existing Planned Area 13 office and multifamily uses to the north, the freeway, and Lake Washington High School. Its designation of low-density development to a maximum of seven dwelling units per acre should continue. If redevelopment occurs, the existing vegetative buffer along the southern border should be preserved. Access should be located so as to maximize sight distances along 116th Avenue NE and NE 80th Street by keeping the access away from the curve formed by their junction. Therefore, the access should be aligned with 118th Avenue NE.

### 4. ECONOMIC ACTIVITIES

***NE 85th Street is a regional transportation and commercial corridor, featuring retail, office, and business park uses. Some medium- and high-density multifamily development is also present.***

The only area of economic activity in South Rose Hill is within the commercial district along NE 85th Street (see Figure SRH-3). It is recognized as both a regional transportation and commercial corridor. This area includes retail, office, and business park uses, and, to a lesser degree, some medium- and high-density multifamily development. From I-405 east to the Kirkland city limits, the commercial corridor generally tapers from a depth of over 1,100 feet to about 150 feet at 132nd Avenue NE on both sides of NE 85th Street. See the NE 85th Street Subarea Plan for more information about the commercial corridor.

***Commercial development is permitted on the north side of NE 70th Street, across from the Bridle Trails Shopping Center. Medium-density detached single-family residential development is also appropriate in the immediate vicinity.***

The northwest corner of NE 70th Street and 132nd Avenue NE contains a small-scale commercial development. Development should not extend into the surrounding low-density residential neighborhood, however.

The northern boundary of the commercial area lies south of the existing single-family development along 132nd Avenue NE. The western boundary lies east of the existing single-family development along NE 70th Street. In the northwestern portion of the site, the boundary generally follows the toe of the existing slope.

To mitigate impacts to the adjoining residential area, development is subject to the following standards:

- (1) Commercial uses should be oriented to serving the neighborhood. Uses should not include vehicle service stations, drive-in businesses, auto service and sales, or storage facilities.
- (2) Building height, bulk, modulation, and roofline design should reflect the scale and character of single-family development. Blank walls should be avoided.
- (3) New structures should be substantially buffered from nearby low-density residential uses. Such buffering should consist of an earthen berm a minimum of 20 feet wide and five feet high at the center. In some places, the existing slope may replace the berm. The berm or slope should be planted with trees and shrubbery in sufficient size, number, and spacing to achieve a reasonable obstruction of views of the subject property. Alternatively, an equal or superior buffering technique may be used.

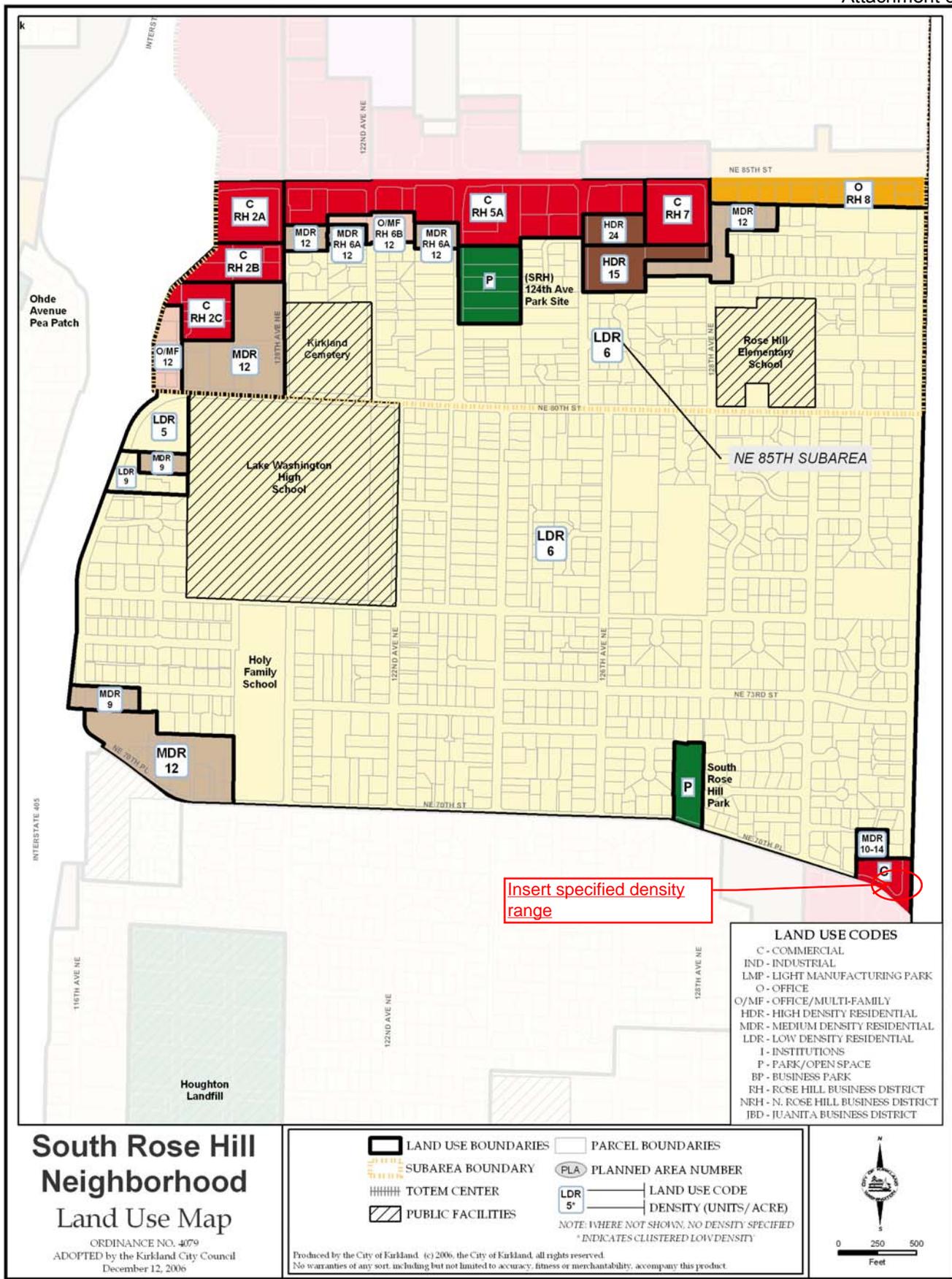


Figure SRH-3: South Rose Hill Land Use

## XV.G. SOUTH ROSE HILL NEIGHBORHOOD

- (4) Businesses must be oriented to NE 70th Street or 132nd Avenue NE and must be directly connected, with on-site sidewalks, to sidewalks in adjacent rights-of-way.
- (5) Commercial access must be taken only from NE 70th Street and/or 132nd Avenue NE. Turning movements may be restricted to promote public safety.
- (6) Parking areas should be landscaped and visually screened from adjoining residential development.
- (7) The number and size of signs should be minimized to avoid a cluttered, intensive, commercial appearance. A master sign plan should be implemented. Back-lit or internally-lit translucent awnings should be prohibited. Only wall- or ground-mounted signs should be permitted.
- (8) Noise impacts to surrounding residential development should be minimized.
- (9) Hours of operation of businesses on the site should be limited to no more than 16 hours per day, ending at 10 p.m.

Immediately to the north of the commercial area, medium-density residential development is appropriate. Units should be small-lot detached single-family residences, however.

### 5. OPEN SPACE/PARKS

South Rose Hill has a number of publicly owned areas that currently provide park and open space opportunities for neighborhood residents. They are briefly described below.

**South Rose Hill Neighborhood Park** is a 2.5-acre site that was purchased as a result of a successful Park Bond in 1989. This park is located on NE 70th Street, at approximately 128th Avenue NE (see Figure SRH-4). Improvements in this park are typical of a neighborhood park facility, including pedestrian access.

**Lake Washington High School** is a 38.31-acre site located at NE 80th Street and 122nd Avenue NE. Improvements to this site include school buildings, a playfield, tennis courts, and track.

**Rose Hill Elementary School** is a 9.75-acre site located at NE 80th Street and 128th Avenue NE. Improvements to this site include school buildings and a playground.

**Kirkland Cemetery** is a 5.75-acre site located at NE 80th Street and 122nd Avenue NE. The cemetery is an important public historic landmark and open space feature in the neighborhood. Future funded improvements include irrigation, planting, relocation and improvement of cemetery entry, additional parking, new cemetery services, improved pedestrian and vehicular circulation, and expansion to the southeast corner of the property.

*Efforts should be made to acquire additional parkland for this neighborhood, including smaller parcels.*

Despite these parks and open space facilities, the neighborhood is deficient in parkland based on the standard of 15 acres per 1,000 population, because much of this land is owned by the Lake Washington School District. As a result, every effort should be made to acquire additional parkland for this neighborhood, including smaller parcels for use as “pocket parks.” These parks serve limited park needs where neighborhood park opportunities are lacking. Pocket parks are typically less than one acre in size

## APPENDIX B – GLOSSARY

**Mode Split:** The statistical breakdown of travel by alternate modes, usually expressed as a percentage of travel by single-occupant automobile, carpool, transit, etc. Mode-split goals are used to evaluate the performance of transportation systems.

**Moderate-Income Household:** One or more adults and their dependents whose income exceeds 50 percent, but does not exceed 80 percent, of the median household income for King County, adjusted for household size, as published by the United States Department of Housing and Urban Development.

**Multifamily:** Residential use of land where a structure provides shelter for two or more households at medium to high densities.

**Multimodal Transportation:** Means of transport by multiple ways or methods, including automobiles, public transit, walking, bicycling, and ride-sharing.

**Neighborhood Centers:** Areas of commercial activity dispensing commodities primarily to the neighborhood. A supermarket may be a major tenant; other stores may include a drug store, variety, hardware, barber, beauty shop, laundry, dry cleaning, and other local retail enterprises. These centers provide facilities to serve the everyday needs of the neighborhood. Residential uses may be located on upper stories of commercial buildings in the center.

**Office:** Uses providing services other than production, distribution, or sale or repair of goods or commodities. Depending on the location, these uses may range from single-story, residential-scale buildings to multistory buildings and/or multibuilding complexes.

**Office/Multifamily:** Areas where both office and medium- or high-density residential uses are allowed. Uses may be allowed individually or within the same building.

**Parks/Open Space:** Natural or landscaped areas used for active or passive recreational needs, to protect environmentally sensitive areas, and/or to preserve natural landforms and scenic views.

**Planning Period:** The 20-year period following the adoption of a comprehensive plan or such longer period as may have been selected as the initial planning horizon by the planning jurisdiction.

**Policy:** Principle that reflects a method or course of action to achieve an identified goal.

**Primary Jobs:** Jobs which produce goods and services that bring income into the community.

**Public Facilities:** Include streets, roads, highways, sidewalks, street and road lighting systems, traffic signals, domestic water systems, storm and sanitary sewer systems, parks and recreational facilities, fire stations, libraries, and schools. These physical structures are owned or operated by a public entity that provides or supports a public service.

**Public Services:** Include fire protection and suppression, emergency medical services, law enforcement, public health, library, solid waste, education, recreation, environmental protection, and other governmental services.

**Queue Bypass Lane:** A lane provided for the movement of certain vehicles, typically transit or HOVs, which allows those vehicles to bypass queues at a traffic signal.

**Regional Facilities:** Public capital facilities of a regional or Statewide nature, such as wastewater treatment plants, airports, or in-patient treatment facilities. These facilities may be privately owned but regulated by public entities.

**Regional Transportation Plan:** The transportation plan for the regionally designated transportation system which is produced by the Regional Transportation Planning Organization (RTPO).

**Regional Transportation Planning Organization (RTPO):** The voluntary organization conforming to RCW 47.80.020, consisting of local governments

## APPENDIX B – GLOSSARY

within a region containing one or more counties which have common transportation interests, such as the Puget Sound Regional Council.

**Residential Markets:** Individual stores or very small, mixed-use buildings/centers focused on local pedestrian traffic. Residential scale and design are critical to integrate these uses into the residential area.

**Right-of-Way:** Land in which the State, a county, or a municipality owns the fee simple title or has an easement dedicated or required for a transportation or utility use.

**Runoff:** The overland or subsurface flow of water.

**Sanitary Sewer Systems:** All facilities, including approved on-site disposal facilities, used in the collection, transmission, storage, treatment, or discharge of any waterborne waste, whether domestic in origin or a combination of domestic, commercial, or industrial waste.

**Sensitive Areas:** Wetlands, streams, lakes, excluding Lake Washington, and frequently flooded areas.

**Shorelines:** Lake Washington, its underlying land, associated wetlands, those lands extending landward 200 feet from its OHWM and critical area buffers within 200 feet of the OHWM. These are lands within state shorelines jurisdiction, pursuant to RCW 90.58.030.

**Single-Family:** Residential use of land where dwelling units provide shelter and living accommodations for one family.

**Single-Room Occupancy (SRO) Hotels:** Typically a small room with a sink and a closet. Occupant shares bathroom, shower, and kitchen with other rooms.

**Sustainable Building Practices:** Various techniques to reduce construction and maintenance costs and to benefit the environment, such as using recycled building materials, reusing water and installing alternative heating and cooling systems.

**Townhouse:** Attached dwelling units (that is, having one or more walls in common) with each unit having its own exterior entrance.

**Transit-Oriented Development (TOD):** Development intended to maximize the use of transit.

**Transportation Facilities:** Includes capital facilities related to air, water, or land transportation.

Residential uses may be located above or behind commercial uses in the center, at densities specified in the applicable neighborhood plan.

**Development Strategies**  
ing travel behavior rather than at expanding the transportation network to meet travel demand. Such strategies can include the promotion of work-hour changes, ride-sharing and vanpooling options, transit flex passes, preferential parking for carpools, charge for parking, guaranteed ride home program, available showers and lockers and telecommuting.

**Transportation System Management (TSM):** Improvements that increase the capacity of the transportation network, but that do not include projects, such as adding additional lanes to streets. TSM strategies include, but are not limited to, signalization, channelization, and bus turnouts.

**Urban Center:** An area that has a regionally significant concentration of employment and housing, with direct service by high-capacity transit and a wide range of land uses, such as retail, recreational, public facilities, parks and open space. An Urban Center has a mix of uses and densities to efficiently support transit as part of the regional high-capacity transit system. An area must be designated by the King County Countywide Planning Policies to be an Urban Center.

**Urban Growth:** Refers to growth that makes intensive use of land for the location of buildings, structures, and impermeable surfaces to such a degree as to be incompatible with the primary use of such land for the production of food, other agricultural products, or fiber, or the extraction of mineral resources. When allowed to spread over wide areas, urban growth typically requires urban governmental services. “Characterized by urban growth” refers to land hav-

**CHAPTER 45 – COMMUNITY BUSINESS**  
45.05 User Guide. The charts in KZC 45.05 reading down the left hand column

**Section 45.08**



Section 45.08 – GEN  
The following regulat

- 1. Refer to Chapter
- 2. If any portion of
  - a. The height o
  - b. The maximu
 See KZC 115.30

In the BC zone,

3. At least 75 percent of the total gross floor area located on the ground floor of air structures on the subject property must contain retail uses, fishments, restaurants, taverns, hotels or motels, or offices. These uses shall be oriented to an adjacent arterial, a major pedestrian sidewalk, a through-block pedestrian pathway or an internal pathway.

5 4. In BC 1 and BC 2 zones, developments creating four or more new dwelling units shall provide at least 10 percent of the units as affordable housing units as defined in Chapter 5 KZC. Two additional units may be constructed for each affordable housing unit provided. See Chapter 112 KZC for additional affordable housing incentives and requirements.

6 5. In the BC 1 and BC 2 zones, side and rear yards abutting a residential zone shall be 20 feet.

7 6. In the BC 1 and BC 2 zones, all required yards for any portion of a structure must be increased one foot for each foot that any portion of the structure exceeds 35 feet above average building elevation (does not apply to Public Park uses).

8 7. Maximum height of structure is as follows:

a. In the BC zone, if adjoining a low density zone other than RSX, then 25 feet above average building elevation. Otherwise, 30 feet above average building elevation. Except if adjoining a low density zone, structure height may be increased above 30 feet in height through a Process IIA, Chapter 150 KZC, if:

- i. It will not block local or territorial views designated in the Comprehensive Plan;
- ii. The increased height is not specifically inconsistent with the applicable neighborhood plan provisions of the Comprehensive Plan; and

iii. The required front yard of any portion of the structure is increased one foot for each foot that any portion of the structure exceeds 30 feet above average building elevation (does not apply to Public Park uses).

b. In the BC 1 zone, 35 feet above average building elevation.

c. In the BC 2 zone, 35 feet above average building elevation. Structure height may be increased to 60 feet in height if:

- i. At least 50 percent of the floor area is residential;
- ii. Parking is located away from the street by placing it behind buildings, to the side of buildings, or in a parking structure; and
- iii. The ground floor is a minimum 15 feet in height for all retail, restaurant, or office uses (except parking garages); and
- iv. The required yards of any portion of the structure are increased one foot for each foot that any portion of the structure exceeds 30 feet above average building elevation (does not apply to Public Park uses).

4. In the BC 1 and BC 2 zones, the following requirements shall apply to all development that includes residential or assisted living uses:

a. The development must include commercial use(s) with gross floor area on the ground floor equal to or greater than 25 percent of the parcel size for the subject property. Commercial floor area shall be one or more of the following uses: Retail; Restaurant or Tavern; Entertainment, Cultural and/or Recreational Facility; or Office.

b. The commercial floor shall be a minimum of 13 feet in height. The height of the structure may exceed the maximum height of structure by three feet for a three story building with the required 13 foot commercial floor.

c. Commercial uses shall be oriented to adjoining arterials.  
d. Residential uses, assisted living uses, and parking for those uses shall not be located on the street level floor unless an intervening commercial frontage is provided between the street and those other uses or parking subject to the standards above. The intervening commercial frontage shall be a minimum 20 feet in depth. The Planning Director may approve a minor reduction in the depth requirements if the applicant demonstrates that the requirement is not feasible given the configuration of existing or proposed improvements and that the design of the commercial frontage will maximize visual interest. Lobbies for residential or assisted living uses may be allowed within the commercial frontage provided they do not exceed 20 percent of the building's linear commercial frontage along the street.

e. The maximum height of the structure shall be limited to the maximum height of air structures on the subject property most contain retail uses.



USE ZONE CHART

Section 45.10 REGULATIONS		DIRECTIONS: FIRST, read down to find use... THEN, across for REGULATIONS										
		Required Review Process	Lot Size	MINIMUMS			MAXIMUMS		Landscape Category (See Ch. 95)	Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)	Special Regulations (See also General Regulations)
Section 45.10	USE REGULATIONS			REQUIRED YARDS (See Ch. 115)			Lot Coverage	Height of Structure	A	E	See KZC 105.25.	1. May not be more than two vehicle service stations at any intersection. Gas pump islands may extend 20 feet into the front yard. Canopies or covers over gas pump islands may not be closer than 10 feet to any property line. Outdoor parking and service areas may not be closer than 10 feet to any property line. See KZC 115.105, Outdoor Use, Activity and Storage, for further regulations. 2. Outdoor vehicle or boat parking or storage areas must be buffered as required for a parking area in KZC 95.45. See KZC 115.105, Outdoor Use, Activity and Storage, for further regulations. 3. Vehicle and boat rental are allowed as part of this use.
				Front	Side	Rear						
.010	Vehicle Service Station	Process I, Chapter 145 KZC.	22,500 sq. ft.	40'	15' on each side	15'	80%	See Gen. Regs. 5-and-6. <b>7 &amp; 8</b>	A	E	See KZC 105.25.	1. May not be more than two vehicle service stations at any intersection. Gas pump islands may extend 20 feet into the front yard. Canopies or covers over gas pump islands may not be closer than 10 feet to any property line. Outdoor parking and service areas may not be closer than 10 feet to any property line. See KZC 115.105, Outdoor Use, Activity and Storage, for further regulations. 2. Outdoor vehicle or boat parking or storage areas must be buffered as required for a parking area in KZC 95.45. See KZC 115.105, Outdoor Use, Activity and Storage, for further regulations. 3. Vehicle and boat rental are allowed as part of this use.
.020	A Retail Establishment providing vehicle or boat sales or vehicle or boat service or repair. See Spec. Reg. 2.	None	None	BC: 20'	See Gen. Regs. 4-and 5.	0'	See Gen. Regs. 4-and 5.	See Gen. Regs. 4-and 5. <b>6 &amp; 7</b>	B		1 per each 100 sq. ft. of gross floor area.	1. For restaurants with drive-in or drive-through facilities: a. One outdoor waste receptacle shall be provided for every eight parking stalls. b. Access for drive-through facilities shall be approved by the Public Works Department. Drive-through facilities shall be designed so that vehicles will not block traffic in the right-of-way while waiting in line to be served. c. Landscape Category A shall apply.
.030	Restaurant or Tavern											
.050	A Retail Establishment providing storage services. See also Spec. Regs. 1 and 2.								A		See KZC 105.25.	1. May include accessory living facilities for resident security manager. 2. This use not permitted in BC 1 and BC 2 zones.

**Section 45.10**

**Zone**  
BC, BC 1,  
BC 2

**USE ZONE CHART**

Section 45.10		DIRECTIONS: FIRST, read down to find use... THEN, across for REGULATIONS										
		Required Review Process	Lot Size	MINIMUMS			MAXIMUMS			Landscape Category (See Ch. 95)	Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)
REQUIRED YARDS (See Ch. 115)				Lot Coverage	Height of Structure							
USE	REGULATIONS	Front	Side			Rear	Front	Side	Rear	Front	Side	Rear
 <b>USE</b>  <b>REGULATIONS</b>	<p><b>.060</b> Any Retail Establishment other than those specifically listed in this zone, selling goods, or providing services including banking and related financial services</p>	None	None	None	BC: 20'	0'	0'	80%	See Gen. Regs. 5-and-6.	E	1 per each 300 sq. ft. of gross floor area.	<p>1. Ancillary assembly and manufacture of goods on the premises of this use are permitted only if:</p> <ul style="list-style-type: none"> <li>a. The assembled or manufactured goods are directly related to and are dependent upon this use, and are available for purchase and removal from the premises.</li> <li>b. The outward appearance and impacts of this use with ancillary assembly or manufacturing activities must be no different from other retail uses.</li> </ul> <p>2. Access from drive-through facilities must be approved by the Public Works Department. Drive-through facilities must be designed so that vehicles will not block traffic in the right-of-way while waiting in line to be served.</p> <p>3. A delicatessen, bakery, or other similar use may include, as part of the use, accessory seating if:</p> <ul style="list-style-type: none"> <li>a. The seating and associated circulation area does not exceed more than 10 percent of the gross floor area of the use; and</li> <li>b. It can be demonstrated to the City that the floor plan is designed to preclude the seating area from being expanded.</li> </ul>
		See Gen. Regs. 4 and 5.	See Gen. Regs. 4 and 5.	See Gen. Regs. 4 and 5.	See Gen. Regs. 4 and 5.	See Gen. Regs. 4 and 5.	See Gen. Regs. 4 and 5.	See Gen. Regs. 4 and 5.	See Gen. Regs. 4 and 5.	See Gen. Regs. 4 and 5.	See Gen. Regs. 4 and 5.	
	<b>.070</b> Office Use	None	None	None	BC: 20'	0'	0'	80%	See Gen. Regs. 5-and-6.	D	If a medical, dental or veterinary office, then 1 per each 200 sq. ft. of gross floor area. Otherwise, 1 per each 300 sq. ft. of gross floor area.	<p>1. The following regulations apply to veterinary offices only:</p> <ul style="list-style-type: none"> <li>a. May only treat small animals on the subject property.</li> <li>b. Outside runs and other outside facilities for the animals are not permitted.</li> <li>c. Site must be designed so that noise from this use will not be audible off the subject property. A certification to this effect, signed by an Acoustical Engineer, must be submitted with the development permit application.</li> </ul> <p>2. Ancillary assembly and manufacture of goods on the premises of this use are permitted only if:</p> <ul style="list-style-type: none"> <li>a. The ancillary assembled or manufactured goods are subordinate to and dependent on this use.</li> <li>b. The outward appearance and impacts of this use with ancillary assembly or manufacturing activities must be no different from other office uses.</li> </ul>



USE ZONE CHART

DIRECTIONS: FIRST, read down to find use... THEN, across for REGULATIONS												
Section 45.10	USE ↓ ↑ REGULATIONS	Required Review Process	MINIMUMS			MAXIMUMS		Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)	Special Regulations (See also General Regulations)		
			Lot Size	REQUIRED YARDS (See Ch. 115)			Lot Coverage				Height of Structure	
				Front	Side	Rear						
.080	Hotel or Motel	None	None	BC: 20' BC 1 and BC 2: 10'	0' See Gen. Regs. 4 and 5. 6 & 7	0' See Gen. Regs. 4 and 5. 6 & 7	80%	See Gen. Regs. 5 and 6. 7 & 8	B	E	1 per each room. See also Spec. Reg. 2. 1 per every 4 fixed seats.	1. May include ancillary meeting and convention facilities. 2. Excludes parking requirements for ancillary meeting and convention facilities. Additional parking requirement for these ancillary uses shall be determined on a case-by-case basis.
.090	A Retail Establishment providing entertainment, recreational or cultural activities	None	None	BC: 20' BC 1 and BC 2: 10'	0' See Gen. Regs. 4 and 5. 6 & 7	0' See Gen. Regs. 4 and 5. 6 & 7	80%	See Gen. Regs. 5 and 6. 7 & 8	B	C	1 per each 300 sq. ft. of gross floor area.	In the BC zone. In the BC 1 and BC 2 zones, this use is only allowed subject to the provisions of General Regulation 4
.100	Private Lodge or Club	None	None	BC: 20' BC 1 and BC 2: 10'	0' See Gen. Regs. 4 and 5. 6 & 7	0' See Gen. Regs. 4 and 5. 6 & 7	80%	See Gen. Regs. 5 and 6. 7 & 8	C	A	1 per every 4 fixed seats.	1. This use, with the exception of a lobby, may not be located on the ground floor of a structure. 2. Chapter 115 KZC contains regulations regarding home occupations and other accessory uses, facilities and activities associated with this use.
.110	Stacked Dwelling Unit. See Special Regulation 1.	900 square feet per unit in BC 1 and BC 2, otherwise none.	Same as the regulations for the ground floor use. See Spec. Reg. 1.	BC: 20' BC 1 and BC 2: 10'	0' See Gen. Regs. 4 and 5. 6 & 7	0' See Gen. Regs. 4 and 5. 6 & 7	80%	See Gen. Regs. 5 and 6. 7 & 8	A	B	1.7 per unit.	1. This use, with the exception of a lobby, may not be located on the ground floor of a structure. 2. Chapter 115 KZC contains regulations regarding home occupations and other accessory uses, facilities and activities associated with this use.
.120	Church	None	None	BC: 20' BC 1 and BC 2: 10'	0' See Gen. Regs. 4 and 5. 6 & 7	0' See Gen. Regs. 4 and 5. 6 & 7	80%	See Gen. Regs. 5 and 6. 7 & 8	B	C	1 for every four people based on maximum occupancy load of any area of worship. See also Special Reg. 2.	1. May include accessory living facilities for staff persons. 2. No parking is required for day-care or school ancillary to this use.

**Section 45.10**

**USE ZONE CHART**

Zone  
BC, BC 1,  
BC 2

USE REGULATIONS



**DIRECTIONS: FIRST, read down to find use... THEN, read across for REGULATIONS**

Section 45.10	Required Review Process	MINIMUMS			MAXIMUMS			Landscape Category (See Ch. 95)	Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)	Special Regulations (See also General Regulations)
		Lot Size	REQUIRED YARDS (See Ch. 115)			Lot Coverage	Height of Structure				
			Front	Side	Rear						
.130	None	None	BC: 20' BC 1 and BC 2: 10'	0' See Gen. Regs. 4 and 5: 6 & 7	0' See Gen. Regs. 4 and 5: 6 & 7	80%	See Gen. Regs. 5 and 6: 7 & 8	D	B	See KZC 105.25.	1. A six-foot-high fence is required only along the property lines adjacent to the outside play areas. 2. Hours of operation may be limited to reduce impacts on nearby residential uses. 3. Structured play areas must be setback from all property lines as follows: a. 20 feet if this use can accommodate 50 or more students or children. b. 10 feet if this use can accommodate 13 to 49 students or children. 4. An on-site passenger loading area must be provided. The City shall determine the appropriate size of the loading areas on a case-by-case basis, depending on the number of attendees and the extent of the abutting right-of-way improvements. Carpooling, staggered loading/unloading time, right-of-way improvements or other means may be required to reduce traffic impacts on nearby residential uses. 5. May include accessory living facilities for staff persons. 6. The location of parking and passenger loading areas shall be designed to reduce impacts on nearby residential uses. 7. <del>These uses are subject to the requirements established by the Department of Social and Health Services (WAC Title 388).</del>
.140	None	None	BC: 20' BC 1 and BC 2: 10'	0' See Gen. Regs. 4 and 5: 6 & 7	0' See Gen. Regs. 4 and 5: 6 & 7	80%	See Gen. Regs. 5 and 6: 7 & 8	D	B	See KZC 105.25.	1. A six-foot-high fence is required along the property lines adjacent to the outside play areas. 2. Hours of operation may be limited by the City to reduce impacts on nearby residential uses. 3. Structured play areas must be setback from all property lines by five feet. 4. An on-site passenger loading area may be required depending on the number of attendees and the extent of the abutting right-of-way improvements. 5. The location of parking and passenger loading areas shall be designed to reduce impacts on nearby residential uses. 6. May include accessory living facilities for staff persons. 7. <del>These uses are subject to the requirements established by the Department of Social and Health Services (WAC Title 388).</del>



USE ZONE CHART

DIRECTIONS: FIRST, read down to find use...THEN, across for REGULATIONS										
Section 45.10	USE ↓ ↑ REGULATIONS	Required Review Process	MINIMUMS			MAXIMUMS		Sign Category (See Ch. 100)	Landscape Category (See Ch. 95)	Special Regulations (See also General Regulations)
			Lot Size	REQUIRED YARDS (See Ch. 115)		Height of Structure				
				Front	Side		Rear			
.150	Assisted Living Facility	None	For BC 1 and BC 2, see Spec. Reg. 5, otherwise none.	Same as the regulations for the ground floor use. See Spec. Reg. 4.	A	1.7 per independent unit. 1 per assisted living unit.	In the BC zone. Required Parking Spaces (See Ch. 105)	1. A facility that provides both independent dwelling units and assisted living units shall be processed as an assisted living facility. 2. If a nursing home use is combined with an assisted living facility use in order to provide a continuum of care for residents, the required review process shall be the least intensive process between the two uses. 3. This use may not be located on the ground floor of a structure. 4. Chapter 115 KZC contains regulations regarding home occupations and other accessory uses, facilities, and activities associated with this use. 5. In BC 1 and BC 2, subject to density limits listed for attached and stacked dwelling units. For density purposes, two assisted living units constitute one dwelling unit.		
.160	Convalescent Center or Nursing Home	BC: 20'	See Gen. Regs. 4 and 5.	80%	See Gen. Regs. 5 and 6.	1 for each bed.		1. If a nursing home use is combined with an assisted living facility use in order to provide a continuum of care for residents, the required review process shall be the least intensive process between the two uses.		
.170	Public Utility	BC 1 and BC 2: 10'	See Gen. Regs. 4 and 5.	0'	See Gen. Regs. 4 and 5.	See KZC 105.25.		1. Landscape Category A or B may be required depending on the type of use on the subject property and the impacts associated with the use on the nearby uses.		
.180	Government Facility Community Facility		See Gen. Regs. 4 and 5.	0'	7 & 8					
.190	Public Park		6 & 7	6 & 7	See Spec. Reg. 1			In the BC 1 and BC 2 zones, this use is only allowed subject to the provisions of General Regulation 4		
Development standards will be determined on a case-by-case basis. See Chapter 49 KZC for required review process.										

**CHAPTER 47 – COMMUNITY BUSINESS X (BCX) ZONES**

**47.05** User Guide. The charts in KZC 47.10 contain the basic zoning regulations that apply in each of the BCX zones of the City. Use these charts by reading down the left hand column entitled Use. Once you locate the use in which you are interested, read across to find the regulations that apply to that use.

**Section 47.08**



**Section 47.08 – GENERAL REGULATIONS**

The following regulations apply to all uses in this zone unless otherwise noted:

1. Refer to Chapter 1 KZC to determine what other provisions of this code may apply to the subject property.
2. If any portion of a structure is adjoining a low density zone, then either:
  - a. The height of that portion of the structure shall not exceed 15 feet above average building elevation, or
  - b. The maximum horizontal facade shall not exceed 50 feet in width.
 See KZC 115.30, Distance Between Structures/Adjacency to Institutional Use, for further details.
3. The required yard of any portion of the structure must be increased one foot for each foot that any portion of the structure exceeds 30 feet above average building elevation (does not apply to Public Park uses).
4. Except if adjoining a low density zone, structure height may be increased above 30 feet in height through a Process IIA, Chapter 150 KZC if:
  - a. It will not block local or territorial views designated in the Comprehensive Plan; and
  - b. The increased height is not specifically inconsistent with the applicable neighborhood plan provisions of the Comprehensive Plan. (Does not apply to Public Park and Automotive Service Center uses).
5. At least 75 percent of the total gross floor area located on the ground floor of all structures on the subject property must contain retail establishments, restaurants, taverns, hotels or motels, or offices. These uses shall be oriented to an adjacent arterial, a major pedestrian sidewalk, a through-block-pedestrian pathway or an internal pathway.

5. The following requirements shall apply to all development that includes residential or assisted living uses:

- a. The development must include commercial use(s) with gross floor area on the ground floor equal to or greater than 25 percent of the parcel size for the subject property. Commercial floor area shall be one or more of the following uses: Retail; Restaurant or Tavern; Entertainment, Cultural and/or Recreational Facility; or Office.
- b. The commercial floor shall be a minimum of 13 feet in height. The height of the structure may exceed the maximum height of structure by three feet for a three story building with the required 13 foot commercial floor.
- c. Commercial uses shall be oriented to adjoining arterials.
- d. Residential uses, assisted living uses, and parking for those uses shall not be located on the street level floor unless an intervening commercial frontage is provided between the street and those other uses or parking subject to the standards above. The intervening commercial frontage shall be a minimum 20 feet in depth. The Planning Director may approve a minor reduction in the depth requirements if the applicant demonstrates that the requirement is not feasible given the configuration of existing or proposed improvements and that the design of the commercial frontage will maximize visual interest. Lobbies for residential or assisted living uses may be allowed within the commercial frontage provided they do not exceed 20 percent of the building's linear commercial frontage along the street.

Section 47.10

Zone  
BCX

USE ZONE CHART

Attachment 4

Editors note: deleted 25' reference because the BCX zone is only adjoined by RSX zone

Section 47.10 REGULATIONS		MINIMUMS		MAXIMUMS		Sign Category (See Ch. 100)	Landscape Category (See Ch. 95)	Required Parking Spaces (See Ch. 105)	Special Regulations (See also General Regulations)
		Required Review Process	Lot Size	REQUIRED YARDS (See Ch. 115)					
	USE		Front	Side	Rear		A		
.010	Vehicle Service Station	Process I, Chapter 145 KZC.	22,500 sq. ft.	40'	15' on each side	15'	E	See KZC 105.25.	1. May not be more than two vehicle service stations at any intersection. 2. Gas pump islands may extend 20 feet into the front yard. Canopies or covers over gas pump islands may not be closer than 10 feet to any property line. Outdoor parking and service areas may not be closer than 10 feet to any property line. See KZC 115.105, Outdoor Use, Activity and Storage, for further regulations.
.020	Automotive Service Center See Spec. Reg. 1.	None	None	20'	0'	0'		1 per each 250 sq. ft. of gross floor area. See Spec. Reg. 3.	1. This use specifically excludes new or used vehicle or boat sales or rentals. 2. No openings (i.e., doors, windows which open, etc.) shall be permitted in any facade of the building adjoining to any residentially zoned property. Windows are permitted if they are triple-paned and unable to be opened. 3. Ten percent of the required parking spaces on site must have a minimum dimension of 10 feet wide by 30 feet long for motor home/travel trailer use. 4. Storage of used parts and tires must be conducted entirely within an enclosed structure. Outdoor vehicle parking or storage areas must be buffered as required for a parking area in KZC 95.45. See KZC 115.105, Outdoor Use, Activity and Storage, for additional regulations. 5. Prior to occupancy of the structure, documentation must be provided and stamped by a licensed professional verifying that the expected noise to be emanating from the site adjoining to any residential zoned property complies with the standards set forth in WAC 173-60-040(1) for a Class B source property and a Class A receiving property.
.030	Restaurant or Tavern						B	1 per each 100 sq. ft. of gross floor area.	1. For restaurants with drive-in or drive-through facilities: a. One outdoor waste receptacle shall be provided for every eight parking stalls. b. Access for drive-through facilities shall be approved by the Public Works Department. Drive-through facilities shall be designed so that vehicles will not block traffic in the right-of-way while waiting in line to be served. c. Landscape Category A shall apply.

DIRECTIONS: FIRST, read down to find use... THEN, across for REGULATIONS

Section 47.10

Zone  
BCX

USE ZONE CHART

Section 47.10		USE REGULATIONS		DIRECTIONS: FIRST, read down to find use... THEN, across for REGULATIONS									
				Required Review Process	Lot Size	MINIMUMS			MAXIMUMS		Landscape Category (See Ch. 95)	Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)
Front	Side	Rear	Lot Coverage			Height of Structure							
.050	A Retail Establishment providing storage services. See also Spec. Reg. 1.	None	None	20'	0'	0'	80%	If adjoining a low-density zone other than RSX, then 25' above average building elevation. Otherwise, 30' above average building elevation.	A	E	See KZC 105.25.	1. May include accessory living facilities for resident security manager.	
.060	Any Retail Establishment, other than those specifically listed in this zone, selling goods or providing services including banking and related financial services. See Spec. Reg. 1.	None	None						A		1 per each 300 sq ft. of gross floor area.	1. The sale, service and/or rental of motor vehicles, sailboats, motor boats, and recreational trailers is not permitted. Motorcycle sales, service, or rental is permitted if conducted indoors. 2. Ancillary assembly and manufacture of goods on the premises of this use are permitted only if: a. The assembled or manufactured goods are directly related to and removal from the premises. b. The outward appearance and impacts of this use with ancillary assembly or manufacturing activities must be no different from other retail uses. 3. Access from drive through facilities must be approved by the Public Works Department. Drive through facilities must be designed so that vehicles will not block traffic in the right-of-way while waiting in line to be served. 4. A delicatessen, bakery, or other similar use may include, as part of the use, accessory seating if: a. The seating and associated circulation area does not exceed more than 10 percent of the gross floor area of the use; and b. It can be demonstrated to the City that the floor plan is designed to preclude the seating area from being expanded.	
				B									



Section 47.10 USE REGULATIONS		DIRECTIONS: FIRST, read down to find use... THEN, across for REGULATIONS									
		MINIMUMS			MAXIMUMS			Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)	Special Regulations (See also General Regulations)	
		Lot Size	REQUIRED YARDS (See Ch. 115)		Lot Coverage	Height of Structure	Landscaping Category (See Ch. 95)				
Required Review Process	None	Front	Side	Rear	80%	C	D	If a medical, dental or veterinary office, then 1 per each 200 sq. ft. of gross floor area. Otherwise, 1 per each 300 sq. ft. of gross floor area.	1. The following regulations apply to veterinary offices only: a. May only treat small animals on the subject property. b. Outside runs and other outside facilities for the animals are not permitted. c. Site must be designed so that noise from this use will not be audible off the subject property. A certification to this effect, signed by an Acoustical Engineer, must be submitted with the development permit application. 2. Ancillary assembly and manufacture of goods on the premises of this use are permitted only if: a. The ancillary assembled or manufactured goods are subordinate to and dependent on this use. b. The outward appearance and impacts of this use with ancillary assembly or manufacturing activities must be no different from other office uses.		
<b>.070</b> Office Use	None	20'	0'	0'	80%	C	D	If a medical, dental or veterinary office, then 1 per each 200 sq. ft. of gross floor area. Otherwise, 1 per each 300 sq. ft. of gross floor area.	1. The following regulations apply to veterinary offices only: a. May only treat small animals on the subject property. b. Outside runs and other outside facilities for the animals are not permitted. c. Site must be designed so that noise from this use will not be audible off the subject property. A certification to this effect, signed by an Acoustical Engineer, must be submitted with the development permit application. 2. Ancillary assembly and manufacture of goods on the premises of this use are permitted only if: a. The ancillary assembled or manufactured goods are subordinate to and dependent on this use. b. The outward appearance and impacts of this use with ancillary assembly or manufacturing activities must be no different from other office uses.		
<b>.080</b> Hotel or Motel						B	E	1 per each room. See also Spec. Reg. 2.	1. May include ancillary meeting and convention facilities. 2. Excludes parking requirements for ancillary meeting and convention facilities. Additional parking requirement for these ancillary uses shall be determined on a case-by-case basis.		
<b>.090</b> A Retail Establishment providing entertainment, recreational or cultural activities								1 per every 4 fixed seats.			
<b>.100</b> Private Lodge or Club						C	B	1 per each 300 sq. ft. of gross floor area.			
<b>.110</b> Stacked Dwelling Unit. See Special Regulation 1.					Same as the regulations for the ground floor use. See Spec. Reg. 1.		A	1.7 per unit.	1. <del>This use, with the exception of a lobby, may not be located on the ground floor of a structure.</del> 2. Chapter 115 KZC contains regulations regarding home occupations and other accessory uses, facilities and activities associated with this use.		

1. This use is only allowed subject to the provisions of General Regulation 5.

Section 47.10

Zone  
BCX

USE ZONE CHART

DIRECTIONS: FIRST, read down to find use... THEN, across for REGULATIONS													
Section 47.10	USE ↑	REGULATIONS ↑	Required Review Process	MINIMUMS			MAXIMUMS			Special Regulations (See also General Regulations)			
				Lot Size	REQUIRED YARDS (See Ch. 115)			Lot Coverage	Height of Structure		Landscape Category (See Ch. 95)	Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)
					Front	Side	Rear						
.120	Church		None	20'	0'	0'	80%	If adjoining a low-density zone other than RSX, then 25' above average building elevation. Otherwise, 30' above average building elevation.	C	B	1 for every four people based on maximum occupancy load of any area of worship. See also Spec. Reg. 2.	1. May include accessory living facilities for staff persons. 2. No parking is required for day-care or school ancillary to this use.	

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Section 47.10

USE ZONE CHART

Zone  
BCX

Section 47.10		DIRECTIONS: FIRST, read down to find use... THEN, across for REGULATIONS									
		Required Review Process	Lot Size	MINIMUMS			MAXIMUMS		Landscape Category (See Ch. 95)	Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)
REQUIRED YARDS (See Ch. 115)				Lot Coverage	Height of Structure						
USE REGULATIONS		Front	Side			Rear					
 USE  REGULATIONS	.130 School or Day-Care Center	None	20'	0'	0'	80%	If adjoining a low-density zone other than RSX, then 25' above average building elevation. Otherwise, 30' above average building elevation.	D	B	See KZC 105.25.	1. A six-foot-high fence is required only along the property lines adjacent to the outside play areas. 2. Hours of operation may be limited to reduce impacts on nearby residential uses. 3. Structured play areas must be setback from all property lines as follows: a. 20 feet if this use can accommodate 50 or more students or children. b. 10 feet if this use can accommodate 13 to 49 students or children. 4. An on-site passenger loading area must be provided. The City shall determine the appropriate size of the loading areas on a case-by-case basis, depending on the number of attendees and the extent of the abutting right-of-way improvements. Carpooling, staggered loading/unloading time, right-of-way improvements or other means may be required to reduce traffic impacts on nearby residential uses. 5. May include accessory living facilities for staff persons. 6. The location of parking and passenger loading areas shall be designed to reduce impacts on nearby residential uses. 7. <del>These uses are subject to the requirements established by the Department of Social and Health Services (WAC Title 388).</del>
	.140 Mini-School or Mini-Day-Care	None									1. A six-foot-high fence is required along the property lines adjacent to the outside play areas. 2. Hours of operation may be limited by the City to reduce impacts on nearby residential uses. 3. Structured play areas must be setback from all property lines by five feet. 4. An on-site passenger loading area may be required depending on the number of attendees and the extent of the abutting right-of-way improvements. 5. The location of parking and passenger loading areas shall be designed to reduce impacts on nearby residential uses. 6. May include accessory living facilities for staff persons. 7. <del>These uses are subject to the requirements established by the Department of Social and Health Services (WAC Title 388).</del>



Section 47.10 USE REGULATIONS  		DIRECTIONS: FIRST, read down to find use... THEN, across for REGULATIONS									
		MINIMUMS			MAXIMUMS			Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)	Special Regulations (See also General Regulations)	
		Lot Size	REQUIRED YARDS (See Ch. 115)		Lot Coverage	Height of Structure	Landscape Category (See Ch. 95)				
Required Review Process	None	Front	Side	Rear	80%	20'	0'	0'	A	1.7 per independent unit. 1 per assisted living unit.	
<b>.150</b> Assisted Living Facility See Spec. Reg. 3.	None	Same as the regulations for the ground floor use. See Spec. Reg. 3.				A	1. A facility that provides both independent dwelling units and assisted living units shall be processed as an assisted living facility. 2. If a nursing home use is combined with an assisted living facility use in order to provide a continuum of care for residents, the required review process shall be the least intensive process between the two uses. <del>3. This use may not be located on the ground floor of a structure.</del> 4. Chapter 115 KZC contains regulations regarding home occupations and other accessory uses, facilities, and activities associated with this use.				
<b>.160</b> Convalescent Center or Nursing Home		20'	0'	0'	80%	If adjoining a low density zone other than RS-X, then 25' above average building elevation. Otherwise, 30' above average building elevation.	C	1 for each bed.	1. If a nursing home use is combined with an assisted living facility use in order to provide a continuum of care for residents, the required review process shall be the least intensive process between the two uses.		
<b>.170</b> Public Utility							A	See KZC 105.25.	1. Landscape Category A or B may be required depending on the type of use on the subject property and the impacts associated with the use on the nearby uses.		
<b>.180</b> Government Facility Community Facility							C				
<b>.190</b> Public Parks	Development standards will be determined on a case-by-case basis. See Chapter 49 KZC for required review process.										

3. This use is only allowed subject to the provisions of General Regulation 5.

**CHAPTER 40 – NEIGHBORHOOD BUSINESS (BN) AND NEIGHBORHOOD BUSINESS A (BNA) ZONES**

**40.05** User Guide. The charts in KZC 40.10 contain the basic zoning regulations that apply in each of the BN and BNA zones of the City. Use these charts by reading down the left hand column entitled Use. Once you locate the use in which you are interested, read across to find the regulations that apply to that use.

**Section 40.08**



**Section 40.08 – GENERAL REGULATIONS**

The following regulations apply to all uses in this zone unless otherwise noted:

1. Refer to Chapter 1 KZC to determine what other provisions of this code may apply to the subject property.
2. If any portion of a structure is adjoining a low density zone, then either:
  - a. The height of that portion of the structure shall not exceed 15 feet above average building elevation, or
  - b. The maximum horizontal facade shall not exceed 50 feet in width.
 See KZC 115.30, Distance Between Structures/Adjacency to Institutional Use, for further details.
3. ~~The required yard of a structure abutting Lake Washington Blvd. or Lake Street South must be increased two feet for each one foot that structure exceeds 25 feet above average building elevation (does not apply to Public Park uses).~~
4. At least 75 percent of the total gross floor area located on the ground floor of all structures on the subject property must contain retail establishments, restaurants, taverns, or offices. These uses shall be oriented to an adjacent arterial, a major pedestrian sidewalk, a through-block pedestrian pathway or an internal pathway.
5. May also be regulated under the Shoreline Master Program; refer to Chapter 83 KZC.

Some development standards or design regulations may be modified as part of the design review process. See Chapters 92 and 142 KZC for requirements.

4. The following commercial frontage requirements shall apply to all development that includes dwelling units or assisted living uses:
  - a. The street level floor of all buildings shall be limited to one or more of the following uses: Retail; Restaurant or Tavern; Entertainment, Cultural and/or Recreational Facility; or Office. These uses shall be oriented toward fronting arterial and collector streets and have a minimum depth of 20 feet and an average depth of at least 30 feet (as measured from the face of the building along the street). The Design Review Board (or Planning Director if not subject to D.R.) may approve a minor reduction in the depth requirements if the applicant demonstrates that the requirement is not feasible given the configuration of existing or proposed improvements and that the design of the commercial frontage will maximize visual interest. The Design Review Board (or Planning Director if not subject to D.R.) may modify the frontage requirement where the property abuts residential zones in order to create a more effective transition between uses.
  - b. The commercial floor shall be a minimum of 13 feet in height. The height of the structure may exceed the maximum height of structure by three feet for a three story building with the required 13 foot commercial floor.
  - c. Other uses allowed in this zone and parking shall not be located on the street level floor unless an intervening commercial frontage is provided between the street and those other uses or parking subject to the standards above. Lobbies for residential or assisted living uses may be allowed within the commercial frontage provided they do not exceed 20 percent of the building's linear commercial frontage along the street.
6. Where Landscape Category B is specified, the width of the required landscape strip shall be 10 feet for properties within the Moss Bay Neighborhood and 20' for properties within the South Rose Hill Neighborhood. All other provisions of Chapter 95 shall apply.
7. Developments may elect to provide affordable housing units as defined in Chapter 5 KZC subject to the voluntary use provisions of Chapter 112 KZC.

Section 40.10 USE REGULATIONS		DIRECTIONS: FIRST, read down to find use... THEN, across for REGULATIONS				Lot Size	MINIMUMS		MAXIMUMS		Density	Floor Area	Other
		Required Review Process	Front	Side	Rear		Lot Coverage	Other					
Section 40.10 USE REGULATIONS	.010	Retail Establishment selling groceries and related items	None	BN zone: 20' on each side BNA zone: 10'	10'	80%	If adjoining a low density zone other than RSX or RSA, then 25' above average building elevation.	B	1 per each 300 sq. ft. of gross floor area.	(See Ch. 105)	1. Except for retail establishments selling groceries and related items in the BNA zone, gross floor area for this use may not exceed 10,000 square feet. 2. Access from drive-through facilities must be approved by the Public Works Department. Drive-through facilities must be designed so that vehicles will not block traffic in the right-of-way while waiting in line to be served. 3. A delicatessen, bakery, or other similar use may include, as part of this use, accessory seating if: a. The seating and associated circulation area does not exceed more than 10 percent of the gross floor area of this use; and b. It can be demonstrated to the City that the floor plan is designed to preclude the seating area from being expanded.	Exceptions: a. Retail establishments selling groceries and related items in the BNA zone are not subject to this limit. b. In the BN zone, the limit shall be 4,000 square feet. (See also General Regulations)	
	.020	Retail Establishment selling drugs, books, flowers, liquor, hardware supplies, garden supplies or works of art	D.R., Chapter 142, KZC	20'	10'	80%	Otherwise, for BN zone, 30' above average building elevation and for BNA zone, 35' above average building elevation.	see Gen. Reg. 6				2. Gross floor area for this use may not exceed 10,000 square feet. Access from drive-through facilities must be approved by the Public Works Department. Drive-through facilities must be designed so that vehicles will not block traffic in the right-of-way while waiting in line to be served. 3. Ancillary assembly and manufactured goods on the premises of this use are permitted only if: a. The assembled or manufactured goods are directly related to and are dependent upon this use, and are available for purchase and removal from the premises. b. The outward appearance and identity of manufacturing activities uses.	except in the BN zone the limit shall be 4,000 square feet.
.030	Retail Variety or Department Store												
.040	Retail Establishment providing banking and related financial services												
.050	Retail Establishment providing laundry, dry cleaning, barber, beauty or shoe repair services												
.055	Retail Establishment providing entertainment, recreational or cultural activities See Spec. Reg. 5.								1 per every 4 fixed seats.				
.060	Restaurant or Tavern								1 per each 100 sq. ft. of gross floor area.				

Section 40.10

Zone  
BN, BNA

USE ZONE CHART

USE REGULATIONS		DIRECTIONS: FIRST, read down to find use... THEN, across for REGULATIONS											
		Required Review Process		MINIMUMS		MAXIMUMS		Landscape Category (See Ch. 95)		Sign Category (See Ch. 100)		Required Parking Spaces (See Ch. 105)	
Section 40.10	Private Lodge or Club	None	REQUIRED YARDS (See Ch. 115)			Lot Coverage	Height of Structure	Landscape Category (See Ch. 95)	Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)	Special Regulations (See also General Regulations)		
			Lot Size	Front	Side								Rear
.070	Private Lodge or Club	None	BN zone: 20' BNA zone: 10'	10' on each side	10'	80%	If adjoining a low density zone other than RSX or RSA, then 25' above average building elevation.	B  see Gen. Reg. 6	B	1 per each 300 sq. ft. of gross floor area.	4. This use not allowed in the BN zone.		
.080	Vehicle Service Station  See Spec. Reg. 4	22,500 sq. ft.	40'	15' on each side. See Spec. Reg. 3.	15'		Otherwise, for BN zone, 30' above average building elevation and for BNA zone, 35' above average building elevation.	A	D	See KZC 105.25.	1. Hours of operation may be limited to reduce impact on residential areas. 2. May not be more than two vehicle service stations at any intersection. 3. Gas pump islands may extend 20 feet into the front yard. Canopies or covers over gas pump islands may not be closer than 10 feet to any property line. Outdoor parking and service areas may not be closer than 10 feet to any property line. See KZC 115.105, Outdoor Use, Activity and Storage, for further regulations.		
.090	Office Use  See Spec. Reg. 4	None	BN zone: 20' BNA zone: 10'	5'-but 2-side yards must equal at least 15'-  10' on each side	10'		See Gen. Reg. 4.b	C  B see Gen. Reg. 6		If a medical, dental or veterinary office, then one per each 200 sq. ft. of gross floor area. Otherwise one per each 300 sq. ft. of gross floor area.	1. The following regulations apply to veterinary offices only: a. May only treat small animals on the subject property. b. Outside runs and other outside facilities for the animals are not permitted. c. Site must be designed so that noise from this use will not be audible off the subject property. A certification to this effect, signed by an Acoustical Engineer, must be submitted with the development permit application. 2. Ancillary assembly and manufacture of goods on the premises of this use are permitted only if: a. The ancillary assembled or manufactured goods are subordinate to and dependent on this use. b. The outward appearance and impacts of this use with ancillary assembly or manufacturing activities must be no different from other office uses. 3. At least 75 percent of the total gross floor area located on the ground floor of all structures on the subject property must contain retail establishments, restaurants, taverns, hotels or motels, or offices. These uses shall be oriented to an adjacent arterial, a major pedestrian sidewalk, a through-block pedestrian pathway or an internal pathway.		

4. For properties located within the Moss Bay Neighborhood, this use not allowed above the street level floor of any structure.

Section 40.10



USE ZONE CHART

Section 40.10		USE REGULATIONS		DIRECTIONS: FIRST, read down to find use...THEN, across for REGULATIONS							
		Attached or USE ↓	REGULATIONS	MINIMUMS			MAXIMUMS		Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)	Special Regulations (See also General Regulations)
Required Review Process	Lot Size			Front	Side	Rear	Lot Coverage	Height of Structure			
.100	Stacked Dwelling Unit. See Special Regulation 1.	None	None	Same as the regulations for the ground floor use. See Special Regulation 4.							
.110	Church	D.R., Chapter 142 KZC	None See Spec. Reg. 3	BN zone: 20' BNA zone: 10'	10' on each side	10'	80%	C	B	1 for every 4 people based on maximum occupancy load of any area of worship. See also Special Reg. 2.	1. This use, with the exception of a lobby, may not be located on the ground floor of a structure. 2. Chapter 115 KZC contains regulations regarding home occupations and other accessory uses, facilities and activities associated with this use. 1. May include accessory living facilities for staff persons. 2. No parking is required for day-care or school ancillary to this use.

See Gen. Reg. 4.b

1. This use is only allowed on the street level floor subject to the provisions of General Regulation 4.  
 3. The minimum amount of lot area per dwelling unit is as follows:  
 a. In the BN zone, (((1,800/900))) square feet.  
 b. In the BNA zone:  
     i. North of NE 140th Street 1,800 sq. ft.  
     ii. South of NE 124th Street 2,400 sq. ft.  
 4. In the BNA zone, the gross floor area of this use shall not exceed fifty percent of the total gross floor area on the subject property.

**Section 40.10**



**USE ZONE CHART**

Section 40.10		MINIMUMS		MAXIMUMS		Landscape Category (See Ch. 95)	Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)	Special Regulations (See also General Regulations)
		Lot Size	REQUIRED YARDS (See Ch. 115)		Lot Coverage				
USE REGULATIONS School or Day-Care Center		None	If this use can accommodate 50 or more students or children, then: 50' 50' on each side	80	If adjoining a low density zone other than RSX or RSA, then 25' above average building elevation.  Otherwise, for BN zone, 30' above average building elevation and for BNA zone, 35' above average building elevation.	D B. see Gen. Reg. 6	B	See KZC 105.25.	1. A six-foot-high fence is required only along the property lines adjacent to the outside play areas. 2. Hours of operation may be limited to reduce impacts on nearby residential uses. 3. Structured play areas must be setback from all property lines as follows: a. 20 feet if this use can accommodate 50 or more students or children. b. 10 feet if this use can accommodate 13 to 49 students or children. 4. An on-site passenger loading area must be provided. The City shall determine the appropriate size of the loading areas on a case-by-case basis, depending on the number of attendees and the extent of the abutting right-of-way improvements. Carpooling, staggered loading/unloading time, right-of-way improvements or other means may be required to reduce traffic impacts on nearby residential uses. 5. The location of parking and passenger loading areas shall be designed to reduce impacts on nearby residential uses. 6. May include accessory living facilities for staff persons. 7. <del>These uses are subject to the requirements established by the Department of Social and Health Services (WAAC title 389).</del> 8. For school use, structure height may be increased, up to 35 feet, if: a. The school can accommodate 200 or more students; and b. The required side and rear yards for the portions of the structure exceeding the basic maximum structure height are increased by one foot for each additional one foot of structure height; and c. The increased height is not specifically inconsistent with the applicable neighborhood plan provisions of the Comprehensive Plan. d. The increased height will not result in a structure that is incompatible with surrounding uses or improvements. This special regulation is not effective within the disapproval jurisdiction of the Houghton Community Council.
			If this use can accommodate 13 to 49 students or children, then: 20' 20' on each side	See Spec. Reg. 8.	See Gen. Reg. 4.b				



USE ZONE CHART

Section 40.10		MINIMUMS		MAXIMUMS		Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)	Special Regulations (See also General Regulations)			
		Lot Size	REQUIRED YARDS (See Ch. 115)		Lot Coverage				Height of Structure	Landscaping (See Ch. 95)	
USE  REGULATIONS 	.130	None	BN zone: 20' BNA zone: 10'	Front: 5'-, but 2-side yards must equal at least 15'-  10' on each side	10'	80%	If adjoining a low density zone other than RSX and RSA, then 25' average building elevation.  Otherwise, for BN zone, 30' above average building elevation and for BNA zone, 35' above average building elevation.	D  B. see Gen. Reg. 6	B	See KZC 105.25.	1. A six-foot-high fence is required along the property lines adjacent to the outside play areas. 2. Hours of operation may be limited by the City to reduce impacts on nearby residential uses. 3. Structured play areas must be setback from all property lines by five feet. 4. An on-site passenger loading area may be required depending on the number of attendees and the extent of the abutting right-of-way improvements. 5. The location of parking and passenger loading areas shall be designed to reduce impacts on nearby residential uses. 6. May include accessory living facilities for staff persons. 7. These uses are subject to the requirements established by the Department of Social and Health Services (WAC Title 388).
	.140	Assisted Living Facility See Spec. Reg. 3. 4 & 5	Same as the regulations for the ground floor use. See Spec. Reg. 3.				A	1.7 per independent unit. 1 per assisted living unit.	1. A facility that provides both independent dwelling units and assisted living units shall be processed as an assisted living facility. 2. If a nursing home use is combined with an assisted living facility use in order to provide a continuum of care for residents, the required review process shall be the least intensive process between the two uses. 3. This use may not be located on the ground floor of a structure. 4. Chapter 115 KZC contains regulations regarding home occupations and other accessory uses, facilities, and activities associated with this use.		

3. This use is only allowed on the street level floor subject to the provisions of General Regulation 4.

4. In the BNA zone, the gross floor area of this use shall not exceed fifty percent of the total gross floor area on the subject property.

5. For density purposes, two assisted living units shall constitute one dwelling unit. Total dwelling units may not exceed the number of stacked dwelling units allowed on the subject property.

See Gen. Reg. 4.b

Section 40.10

USE ZONE CHART

Zone  
BN, BNA

USE REGULATIONS		DIRECTIONS: FIRST, read down to find use... THEN, across for REGULATIONS										
		Required Review Process	Lot Size	MINIMUMS			MAXIMUMS		Landscape Category (See Ch. 95)	Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)	Special Regulations (See also General Regulations)
REQUIRED YARDS (See Ch. 115)				Height of Structure	Lot Coverage							
USE	REGULATIONS	Convalescent Center or Nursing Home	None			Front	Side	Rear	If adjoining a low density zone other than RSX or RSA, then 25' average building elevation.	80%	G	B
				20'	10' on each side	10'	20'	10'				
Section 40.10	Public Utility	D.R., Chapter 142 KZC	Process IIA, Chapter 150 KZC	20'	10' on each side	10'	Otherwise, for BN zone, 30' above average building elevation and for BNA zone, 35' above average building elevation.	80%	C	See Spec. Reg. 1	See KZC 105.25.	1. Landscape Category A or B may be required depending on the type of use on the subject property and the impacts associated with the use on the nearby uses. 2. One pedestal sign with a readerboard having electronic programming is allowed at a fire station only if: a. It is a pedestal sign (see Plate 12) having a maximum of 40 square feet of sign area per sign face; b. The electronic readerboard is no more than 50 percent of the sign area; c. Moving graphics and text or video are not part of the sign; d. The electronic readerboard does not change text and/or images at a rate less than one every seven seconds and shall be readily legible given the text size and the speed limit of the adjacent right-of-way; e. The electronic readerboard displays messages regarding public service announcements or City events only; f. The intensity of the display shall not produce glare that extends to adjacent properties and the signs shall be equipped with a device which automatically dims the intensity of the lights during hours of darkness; g. The electronic readerboard is turned off between 10:00 p.m. and 6:00 a.m. except during emergencies; h. It is located to have the least impact on surrounding residential properties. If it is determined that the electronic readerboard constitutes a traffic hazard for any reason, the Planning Director may impose additional conditions.
				10'	10' on each side	10'						
.150	Convalescent Center or Nursing Home	D.R., Chapter 142 KZC	Process IIA, Chapter 150 KZC	20'	10' on each side	10'	Otherwise, for BN zone, 30' above average building elevation and for BNA zone, 35' above average building elevation.	80%	C	See Spec. Reg. 1	See KZC 105.25.	1. Landscape Category A or B may be required depending on the type of use on the subject property and the impacts associated with the use on the nearby uses. 2. One pedestal sign with a readerboard having electronic programming is allowed at a fire station only if: a. It is a pedestal sign (see Plate 12) having a maximum of 40 square feet of sign area per sign face; b. The electronic readerboard is no more than 50 percent of the sign area; c. Moving graphics and text or video are not part of the sign; d. The electronic readerboard does not change text and/or images at a rate less than one every seven seconds and shall be readily legible given the text size and the speed limit of the adjacent right-of-way; e. The electronic readerboard displays messages regarding public service announcements or City events only; f. The intensity of the display shall not produce glare that extends to adjacent properties and the signs shall be equipped with a device which automatically dims the intensity of the lights during hours of darkness; g. The electronic readerboard is turned off between 10:00 p.m. and 6:00 a.m. except during emergencies; h. It is located to have the least impact on surrounding residential properties. If it is determined that the electronic readerboard constitutes a traffic hazard for any reason, the Planning Director may impose additional conditions.
.160	Public Utility	D.R., Chapter 142 KZC	Process IIA, Chapter 150 KZC	20'	10' on each side	10'	Otherwise, for BN zone, 30' above average building elevation and for BNA zone, 35' above average building elevation.	80%	C	See Spec. Reg. 1	See KZC 105.25.	
.170	Government Facility Community Facility	D.R., Chapter 142 KZC	Process IIA, Chapter 150 KZC	20'	10' on each side	10'	Otherwise, for BN zone, 30' above average building elevation and for BNA zone, 35' above average building elevation.	80%	C	See Spec. Reg. 1	See KZC 105.25.	1. Landscape Category A or B may be required depending on the type of use on the subject property and the impacts associated with the use on the nearby uses. 2. One pedestal sign with a readerboard having electronic programming is allowed at a fire station only if: a. It is a pedestal sign (see Plate 12) having a maximum of 40 square feet of sign area per sign face; b. The electronic readerboard is no more than 50 percent of the sign area; c. Moving graphics and text or video are not part of the sign; d. The electronic readerboard does not change text and/or images at a rate less than one every seven seconds and shall be readily legible given the text size and the speed limit of the adjacent right-of-way; e. The electronic readerboard displays messages regarding public service announcements or City events only; f. The intensity of the display shall not produce glare that extends to adjacent properties and the signs shall be equipped with a device which automatically dims the intensity of the lights during hours of darkness; g. The electronic readerboard is turned off between 10:00 p.m. and 6:00 a.m. except during emergencies; h. It is located to have the least impact on surrounding residential properties. If it is determined that the electronic readerboard constitutes a traffic hazard for any reason, the Planning Director may impose additional conditions.
.180	Public Park	Development standards will be determined on a case-by-case basis. See Chapter 49 KZC for required review process.										

**51.15** User Guide. The charts in KZC 51.20 contain the basic zoning regulations that apply in the MSC 2 zone of the City. Use these charts by reading down the left hand column entitled Use. Once you locate the use in which you are interested, read across to find the regulations that apply to that use.

**Section 51.18**



**Section 51.18 – GENERAL REGULATIONS**

The following regulations apply to all uses in this zone unless otherwise noted:

1. Refer to Chapter 1 KZC to determine what other provisions of this code may apply to the subject property.
2. If any portion of a structure is adjoining a low density zone, then either:
  - a. The height of that portion of the structure shall not exceed 15 feet above average building elevation, or
  - b. The maximum horizontal facade shall not exceed 50 feet in width.
 See KZC 115.30, Distance Between Structures/Adjacency to Institutional Use, for further details.
3. Some development standards or design regulations may be modified as part of the design review process. See Chapters 92 and 142 KZC for requirements.
4. ~~At least 75 percent of the total gross floor area located on the ground floor of all structures on the subject property must contain retail establishments, restaurants, taverns, or offices. These uses shall be oriented to an adjacent arterial, a major pedestrian sidewalk, a through-block pedestrian pathway or an internal pathway.~~

4. The following commercial frontage requirements shall apply to all development that includes dwelling units or assisted living uses:
  - a. The street level floor of all buildings shall be limited to one or more of the following uses: Retail; Restaurant or Tavern; Entertainment, Cultural and/or Recreational Facility; or Office. These uses shall be oriented toward Market Street and have a minimum depth of 20 feet and an average depth of at least 30 feet (as measured from the face of the building along Market Street).  
The Design Review Board (or Planning Director if not subject to D.R.) may approve a minor reduction in the depth requirements if the applicant demonstrates that the requirement is not feasible given the configuration of existing or proposed improvements and that the design of the commercial frontage will maximize visual interest.
  - b. The commercial floor shall be a minimum of 13 feet in height. The height of the structure may exceed the maximum height of structure by three feet for a three story building with the required 13 foot commercial floor.
  - c. Other uses allowed in this zone and parking shall not be located on the street level floor unless an intervening commercial frontage is provided between the street and those other uses or parking subject to the standards above. Lobbies for residential or assisted living uses may be allowed within the commercial frontage provided they do not exceed 20 percent of the building's linear commercial frontage along Market Street.
5. Where Landscape Category B is specified, the width of the required landscape strip shall be 10 feet and all other provisions of Chapter 95 shall apply.
6. Developments may elect to provide affordable housing units as defined in Chapter 5 KZC subject to the voluntary use provisions of Chapter 112 KZC.



USE ZONE CHART

Section 51.20		REGULATIONS		MINIMUMS		MAXIMUMS		Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)	Special Regulations (See also General Regulations)
		USE	↑	Lot Size	REQUIRED YARDS (See Ch. 115)	Lot Coverage	Height of Structure			
				Front	Side	Rear				
.010	Any Retail Establishment, other than those specifically listed, prohibited in this zone, selling goods or providing services, including banking and related financial services.	↑	None	20'	10' on each side	10'	80%	B	1 per each 300 sq. ft. of gross floor area.	<p>1. Gross floor area for this use may not exceed 4,000 square feet.</p> <p>2. The following uses are not permitted in this zone:</p> <ul style="list-style-type: none"> <li>a. Vehicle service stations.</li> <li>b. Automotive service centers.</li> <li>c. Uses with drive-in facilities or drive-through facilities, except those existing as of June 15, 2007.</li> <li>d. Retail establishments providing storage services unless accessory to another permitted use.</li> <li>e. Retail establishments involving the sale, service or repair of automobiles, trucks, boats, motorcycles, recreational vehicles, heavy equipment and similar vehicles.</li> <li>f. Storage and operation of heavy equipment, except delivery vehicles associated with retail uses.</li> <li>g. Storage of parts unless conducted entirely within an enclosed structure.</li> <li>3. A delicatessen, bakery, or other similar use may include, as part of this use, accessory seating if:                             <ul style="list-style-type: none"> <li>a. The seating and associated circulation area does not exceed more than 10 percent of the gross floor area of this use; and</li> <li>b. It can be demonstrated to the City that the floor plan is designed to preclude the seating area from being expanded.</li> </ul> </li> <li>4. Ancillary assembly and manufacture of goods on the premises of this use are permitted only if:                             <ul style="list-style-type: none"> <li>a. The assembled or manufactured goods are directly related to and are dependent upon this use, and are available for purchase and removal from the premises.</li> <li>b. The outward appearance and impacts of this use with ancillary assembly or manufacturing activities must be no different from other retail uses.</li> </ul> </li> <li>5. Prior to issuance of a development permit, documentation must be provided by a qualified acoustical consultant, for approval by the Planning Official, verifying that the expected noise to be emanating from the site adjoining any residentially zoned property complies with the standards set forth in WAC 173-60-040(1) for a Class B source property and a Class A receiving property.</li> </ul>

Section 51.20

Zone MSC 2

USE ZONE CHART

USE REGULATIONS		DIRECTIONS: FIRST, read down to find use... THEN, across for REGULATIONS									
Section 51.20	USE REGULATIONS	Required Review Process	MINIMUMS			MAXIMUMS		Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)	Special Regulations (See also General Regulations)	
			Lot Size	REQUIRED YARDS (See Ch. 115)		Lot Coverage	Height of Structure				
			Front	Side	Rear						
.020	Restaurant or Tavern	D.R., Chapter 142 KZC.	None	20'	10' on each side	10'	80%	B	D	1 per each 100 sq. ft. of gross floor area.	1. Restaurants and taverns are limited to 4,000 sq. ft. maximum. 2. Drive-in and drive-through facilities are not permitted. 3. Prior to issuance of a development permit, documentation must be provided by a qualified acoustical consultant, for approval by the Planning Official, verifying that the expected noise to be emanating from the site adjoining any residentially zoned property complies with the standards set forth in WAC 173-60-040(1) for a Class B source property and a Class A receiving property.
.030	Private Lodge or Club	0' along Market Street, otherwise						See Gen. Reg. 5	B	1 per each 300 sq. ft. of gross floor area.	1. Prior to issuance of a development permit, documentation must be provided by a qualified acoustical consultant, for approval by the Planning Official, verifying that the expected noise to be emanating from the site adjoining any residentially zoned property complies with the standards set forth in WAC 173-60-040(1) for a Class B source property and a Class A receiving property.
.040	Office Use				5'-but 2-side yards must equal at least 15'-	20'		C	D	If a medical, dental or veterinary office, then one per each 200 sq. ft. of gross floor area. Otherwise one per each 300 sq. ft. of gross floor area.	1. The following regulations apply to veterinary offices only: a. May only treat small animals on the subject property. b. Outside runs and other outside facilities for the animals are not permitted. c. Prior to issuance of a development permit, documentation must be provided by a qualified acoustical consultant, for approval by the Planning Official, verifying that the expected noise to be emanating from the site adjoining any residentially zoned property complies with the standards set forth in WAC 173-60-040(1) for a Class B source property and a Class A receiving property. 2. Ancillary assembly and manufacture of goods on the premises of this use are permitted only if: a. The ancillary assembled or manufactured goods are subordinate to and dependent on this use. b. The outward appearance and impacts of this use with ancillary assembly or manufacturing activities must be no different from other office uses.
.050	Attached or Stacked Dwelling Unit. See Spec. Reg. 4.								A	1.7 per unit.	1. This use, with the exception of a lobby, may not be located on the ground floor of a structure. 2. Chapter 115 KZC contains regulations regarding home occupations and other accessory uses, facilities and activities associated with this use.

(Revised 4/08)

Kirkland Zoning Code 217

Minimum lot area per dwelling unit is (((1,800/900))) square feet

Section 51.20 REGULATIONS		DIRECTIONS: FIRST, read down to find use... THEN, across for REGULATIONS										
Section 51.20	USE	Required Review Process	MINIMUMS			MAXIMUMS				Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)	Special Regulations (See also General Regulations)
			Lot Size	REQUIRED YARDS (See Ch. 115)		Lot Coverage	Height of Structure	Landscape Category (See Ch. 95)				
.060	Church	D.R., Chapter 142 KZC.	None	20'	10' on each side	10'	80%	If adjoining a low density zone, then 25' above average building elevation. Otherwise, 30' above average building elevation.	C	B	1 for every 4 people based on maximum occupancy load of any area of worship. See also Spec. Reg. 2.	1. May include accessory living facilities for staff persons. 2. No parking is required for day-care or school ancillary to this use.
.070	School or Day-Care Center				If this use can accommodate 50 or more students or children, then: 50' on each side If this use can accommodate 13 to 49 students or children, then: 20' on each side	20'		If adjoining a low density zone, then 25' above average building elevation. Otherwise, 30' above average building elevation. See Spec. Reg. 7	D B. See Gen. Reg. 5	See KZC 105.25.	1. A six-foot-high fence is required only along the property lines adjacent to the outside play areas. 2. Hours of operation may be limited to reduce impacts on nearby residential uses. 3. Structured play areas must be setback from all property lines as follows: a. 20 feet if this use can accommodate 50 or more students or children. b. 10 feet if this use can accommodate 13 to 49 students or children. 4. An on-site passenger loading area must be provided. The City shall determine the appropriate size of the loading areas on a case-by-case basis, depending on the number of attendees and the extent of the abutting right-of-way improvements. Carpooling, staggered loading/unloading time, and other means may be required to reduce traffic impacts on nearby residential uses. 5. The location of parking and passenger loading areas shall be designed to reduce impacts on nearby residential uses. 6. May include accessory living facilities for staff persons. 7. <del>These uses are subject to the requirements established by the Department of Social and Health Services (WAC Title 388).</del> 8. For school use, structure height may be increased, up to 35 feet, if: a. The school can accommodate 200 or more students; and b. The required side and rear yards for the portions of the structure exceeding the basic maximum structure height are increased by one foot for each additional one foot of structure height; and c. The increased height is not specifically inconsistent with the applicable neighborhood plan provisions of the Comprehensive Plan. d. The increased height will not result in a structure that is incompatible with surrounding uses or improvements.	

0' along Market Street, otherwise

10'

7

7

Section 51.20

Zone MSC 2

USE ZONE CHART

USE REGULATIONS		DIRECTIONS: FIRST, read down to find use... THEN, across for REGULATIONS									
Section 51.20	USE	Required Review Process	MINIMUMS			MAXIMUMS		Landscape Category (See Ch. 95)	Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)	Special Regulations (See also General Regulations)
			Lot Size	REQUIRED YARDS (See Ch. 115)		Lot Coverage	Height of Structure				
			Front	Side	Rear						
.080	Mini-School or Mini-Day-Care	D.R., Chapter 142 KZC.	20'	5'- but 2-side yards must equal atleast 45'-	10'	80%	D	B	See KZC 105.25.	<ol style="list-style-type: none"> <li>A six-foot-high fence is required along the property lines adjacent to the outside play areas.</li> <li>Hours of operation may be limited by the City to reduce impacts on nearby residential uses.</li> <li>Structured play areas must be set back from all property lines by five feet.</li> <li>An on-site passenger loading area may be required depending on the number of attendees and the extent of the abutting right-of-way improvements.</li> <li>The location of parking and passenger loading areas shall be designed to reduce impacts on nearby residential uses.</li> <li>May include accessory living facilities for staff persons.</li> <li><del>These uses are subject to the requirements established by the Department of Social and Health Services (AWAC Title 388).</del></li> </ol>	
.090	Assisted Living Facility See Spec. Reg-2. see Gen. Reg. 4		0' along Market Street, otherwise			Same as the regulations for the ground floor use. See Spec. Reg-2.					
.100	Convalescent Center or Nursing Home		20'	10' on each side	10'	80%	C	B	1 for each bed.		
.110	Public Utility			20' on each side	20'		A		See KZC 105.25.	<ol style="list-style-type: none"> <li>Landscape Category A or B may be required depending on the type of use on the subject property and the impacts associated with the use on the nearby uses.</li> </ol>	
.120	Government Facility Community Facility			10' on each side	10'		C	See Spec. Reg. 1			
.130	Public Park	Development standards will be determined on a case-by-case basis. See Chapter 49 KZC for required review process.									

For density purposes, two assisted living units shall constitute one dwelling unit. Total dwelling units may not exceed the number of attached or stacked dwelling units allowed on the subject property.

**KZC Text Amendments  
File No. ZON11-00042**

**92.05 Introduction**

1. General – This chapter establishes the design regulations that apply to development in Design Districts including the Central Business District (CBD), Market Street Corridor (MSC), Neighborhood Business Districts (BN, BNA), Juanita Business District (JBD), Rose Hill Business District (RHBD), Totem Lake Neighborhood (TLN), North Rose Hill Business District (NRHBD), Totem Center (TC), and in areas indicated on the use zone charts for PLA 5C.

**92.15 Pedestrian-Oriented Improvements on or Adjacent to the Subject Property**

1. All Zones – Pedestrian-Oriented Space and Plazas in Parking Areas – The applicant must provide at least 175 square feet of pedestrian-oriented space at the main building entrance in a central location, or adjacent to a parking area. This area must be raised at least six (6) inches above the parking lot surface and must be paved with concrete or unit pavers.

2. Pedestrian-Oriented Space and Plazas in TC, CBD, - BN, BNA, MSC 2, NRHBD, RHBD and TLN Zones

a. In the CBD, BN, BNA, MSC 2, or in TC – If the subject property abuts a pedestrian-oriented street (see Plate 34 in Chapter 180 KZC) or public park, the space, if any, between the sidewalk and the building must be developed consistent with the following criteria:

- 1) Enhance visual and pedestrian access, including handicapped access, onto the subject property from the sidewalk.
- 2) Contain paved walking surface of either concrete or approved unit pavers.
- 3) Contain on-site or building-mounted lighting which provides adequate illumination.
- 4) Contain two (2) linear feet of seating area or one (1) individual seat per 65 square feet of area between the sidewalk and the building.
- 5) Contain landscaping such as trees, shrubs, trellises, or potted plants.
- 6) It may not include asphalt or gravel pavement or be adjacent to an unscreened parking area, a chain link fence or a blank wall which does not comply with the requirements of subsection (3) of this section, Blank Wall Treatment.
- 7) An alternative solution for the pedestrian-oriented space may be established through a Conceptual Master Plan in TL 2.

**92.30 Architectural and Human Scale**

6. Achieving Human Scale in All Zones

a. General

- 1) CBD – Except as provided in subsection (6)(a)(3) of this section, the applicant shall use at least two (2) of the elements or techniques listed in subsection (6)(b) of this section in the design and construction of each facade of a building facing a street or public park.
- 2) BN, JBD, NRHBD, RHBD, MSC, TC, YBD and TLN – Except as provided in subsection (6)(a)(3) of this section, the applicant shall use at least one (1) of the elements or techniques listed in subsection (6)(b) of this section in the design and construction of each facade of a 1-story building facing a street or through-block pathway, and at least two (2) of the elements or techniques for a 2-story building facing a street or through-block pathway (see Plate 34 in Chapter 180 KZC).
- 3) All Zones – The applicant shall use at least three (3) of the elements or techniques listed in subsection (6)(b) of this section in the design and construction of any facade of a building facing a street, through-block pathway or public park, if:
  - a) The facade has a height of three (3) or more stories; or
  - b) The facade is more than 100 feet long.

### 105.18 Pedestrian Access

#### 3. Pedestrian Access – Required Improvements

##### b. Overhead Weather Protection – Location – The applicant shall provide pedestrian overhead weather protection in the following locations:

- 1) Along any portion of the building which is adjacent to a pedestrian walkway or sidewalk;
- 2) Over the primary exterior entrance to all buildings including residential units.
- 3) Exceptions in Design Districts:

In CBD Zones: Along at least 80 percent of the frontage of the subject property on each pedestrian-oriented street.

In RHBD, BN, BNA, MSC 2 and TLN Zones: Along at least 75 percent of a pedestrian-oriented building facade.

In JBD Zones: Along 100 percent of a building facade abutting a street or through-block pathway.

For more information regarding designated pedestrian-oriented streets see Plate 34 in Chapter 180 KZC, and pedestrian-oriented facades in Chapter 92 KZC.

### 142.15 Development Activities Requiring D.R. Approval

#### 1. Design Board Review (D.B.R.)

- a. The following development activities shall be reviewed by the Design Review Board pursuant to KZC 142.35:

- 1) New buildings greater than one (1) story in height or greater than 10,000 square feet of gross floor area, or in the Market Street Corridor Historic District (MSC 3 Zone).
  - 2) Additions to existing buildings where:
    - a) The new gross floor area is greater than 10 percent of the existing building's gross floor area; and
    - b) The addition is greater than 2,000 square feet of gross floor area; and
    - c) Either:
      - 1) The existing building and addition total more than 10,000 square feet of gross floor area; or
      - 2) The addition adds another story; or
      - 3) Is in the Market Street Corridor Historic District (MSC 3 zone).
  - 3) Renovations to existing facades, where the building is identified by the City as an historic structure or is in the Market Street Corridor Historic District (MSC 3 zone).
- b. Exemptions from D.B.R. – The following development activities shall be reviewed through the administrative design review process in KZC [142.25](#):
- 1) Any development where administrative design review is indicated in the applicable Use Zone Chart.
  - 2) Any development in the following zones within the NE 85th Street Subarea: RH 8, PR 3.6, RM, PLA 17A.
  - 3) Any development in the MSC 1, ~~MSC 2~~, and MSC 4 zones located within the Market Street Corridor.
2. Administrative Design Review (A.D.R.) – All other development activities not requiring D.B.R. review under subsection (1) of this section shall be reviewed through the A.D.R. process pursuant to KZC [142.25](#).

### **142.37 Design Departure and Minor Variations**

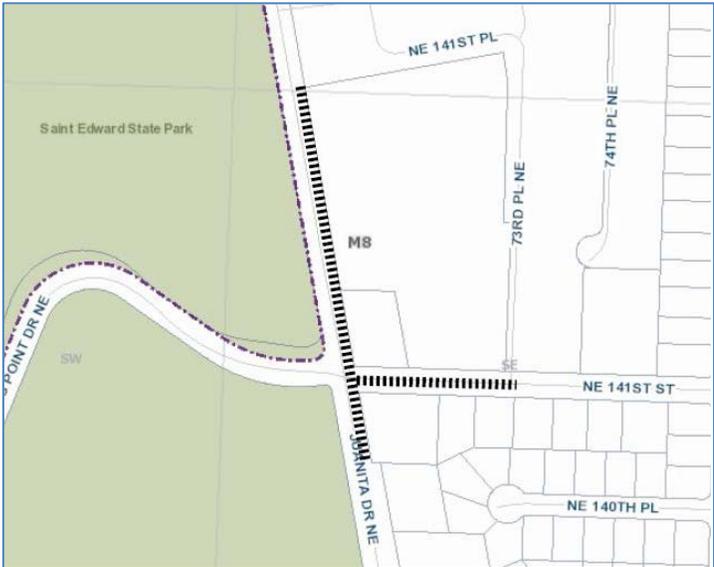
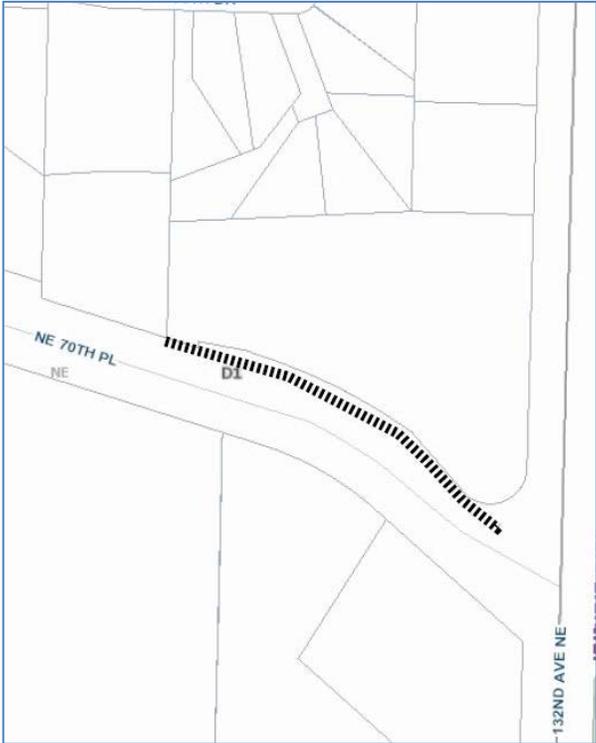
1. General – This section provides a mechanism for obtaining approval to depart from strict adherence to the design regulations or for requesting minor variations from requirements in the following zones:
  - a. In the CBD: minimum required yards; and
  - b. In the Totem Center: minimum required yards, floor plate maximums and building separation requirements; and
  - c. In the RHBD and the TLN: minimum required yards, landscape buffer and horizontal facade requirements; and

- d. In the MSC 1 and MSC 4 zones of the Market Street Corridor: minimum required front yards and horizontal facade requirements; and
- e. In the MSC 2 zone of the Market Street Corridor: height (up to an additional five (5) feet), minimum required front yards and horizontal facade requirements; and
- f. In the MSC 3 zone of the Market Street Corridor: horizontal facade requirements; ~~and-~~
- g. In the BN and BNA zones: horizontal façade requirements.

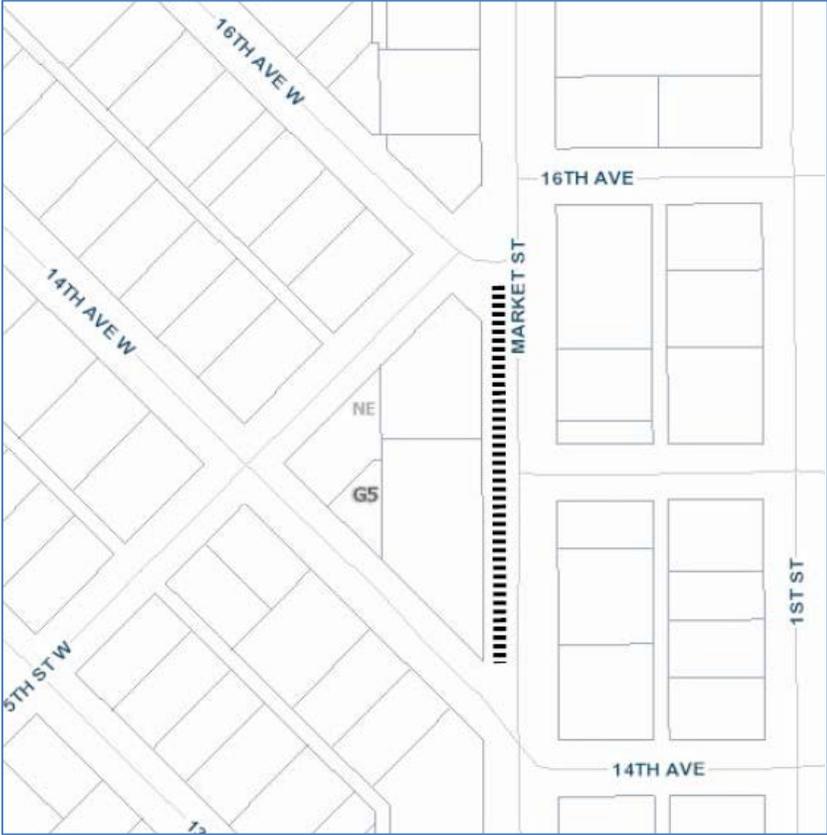
Plate 34M

Pedestrian Circulation in Neighborhood Business Zones (BN, BNA & MSC 2)

----- Pedestrian-Oriented Street



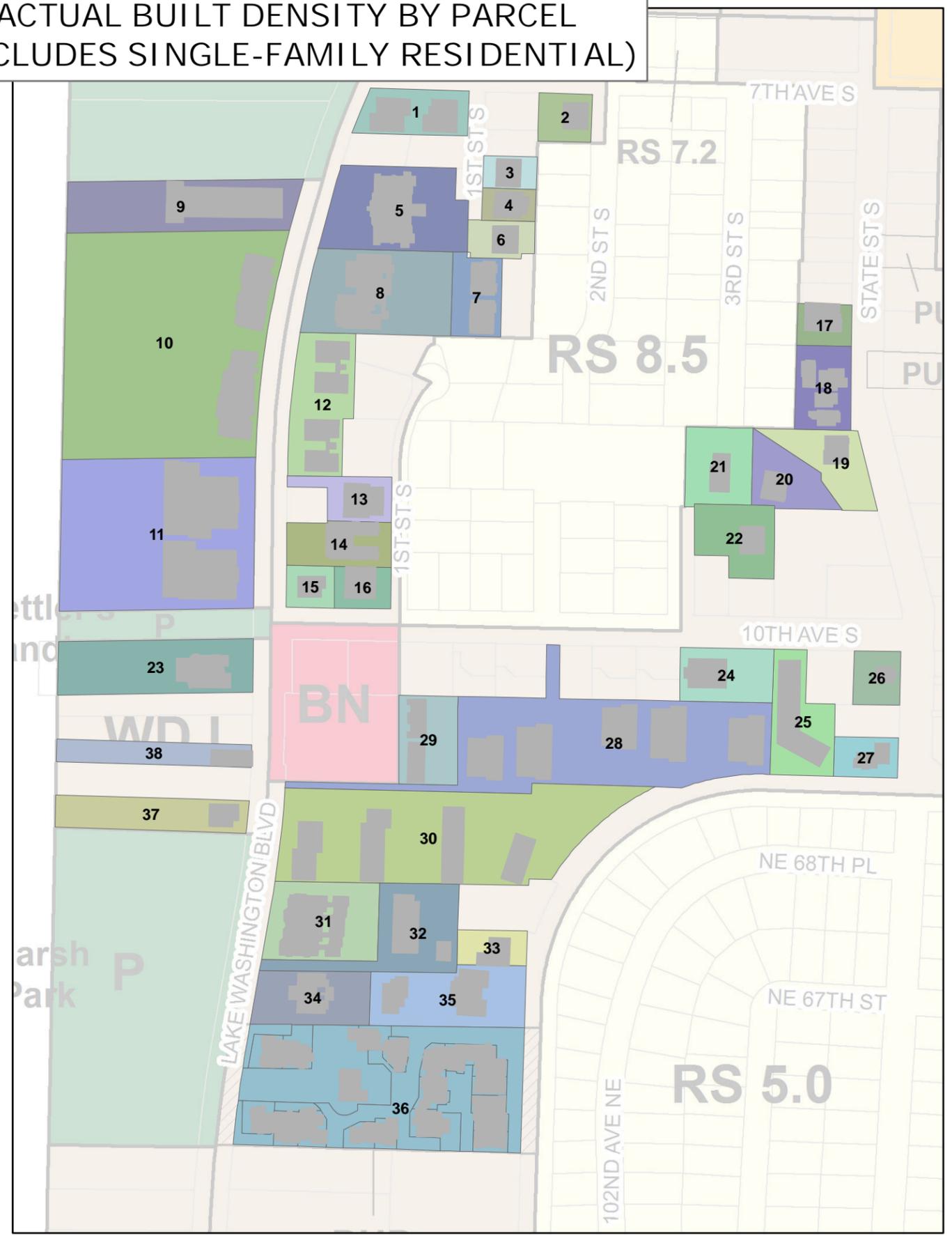
**Plate 34M (continued)**







**ACTUAL BUILT DENSITY BY PARCEL  
(EXCLUDES SINGLE-FAMILY RESIDENTIAL)**



No.	PIN	No. of Units	Lot Size	Units Per Acre	SqFt per Unit
1	5555000000	4	16,695	10.4	4,174
2	1720800400	4	9,000	19.4	2,250
3	1720800335	3	6,000	21.8	2,000
4	2560880000	2	6,002	14.5	3,001
5	4098500000	11	38,938	12.3	3,540
6	8937000000	4	8,400	20.7	2,100
7	2560900000	4	13,868	12.6	3,467
8	3810950000	11	42,233	11.3	3,839
9	7698200000	38	9,343	177.2	246
10	8127900000	23	42,833	23.4	1,862
11	9197570000	13	58,469	9.7	4,498
12	1924100000	8	27,900	12.5	3,488
13	2286600000	4	11,100	15.7	2,775
14	3298580000	4	16,078	10.8	4,020
15	0825059209	4	7,365	23.7	1,841
16	0825059272	7	8,772	34.8	1,253
17	7698320000	2	7,492	11.6	3,746
18	7981500000	4	15,874	11.0	3,969
19	0825059276	4	16,624	10.5	4,156
20	3888350000	4	14,754	11.8	3,689
21	0825059238	2	17,939	4.9	8,970
22	9354900055	4	17,998	9.7	4,500
23	9195250000	6	20,299	12.9	3,383
24	9354900370	9	17,500	22.4	1,944
25	1419780000	12	22,330	23.4	1,861
26	9354900430	2	9,000	9.7	4,500
27	0825059244	3	8,880	14.7	2,960
28	0825059024	60	101,750	25.7	1,696
29	6641300000	8	18,150	19.2	2,269
30	6818000000	56	102,700	23.8	1,834
31	7804260000	12	29,486	17.7	2,457
32	8662700000	7	28,687	10.6	4,098
33	0825059219	2	8,450	10.3	4,225
34	6640800000	16	21,621	32.2	1,351
35	9320450000	9	30,928	12.7	3,436
36	Mutp @	21	80,593	11.4	3,838
37	1310400000	5	5,493	39.7	1,099
38	0825059114	2	3,780	23.0	1,890



# Design Guidelines

## For Pedestrian-Oriented Business Districts



Adopted by the City Council pursuant to  
Kirkland Municipal Code Section 3.30.040.  
Updated March 3, 2009, R-4739.



Attest:

James Lauinger,  
Mayor

Eric Shields  
Director,  
Planning & Community  
Development

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The Illustrations throughout this document are provided by MAKERS.

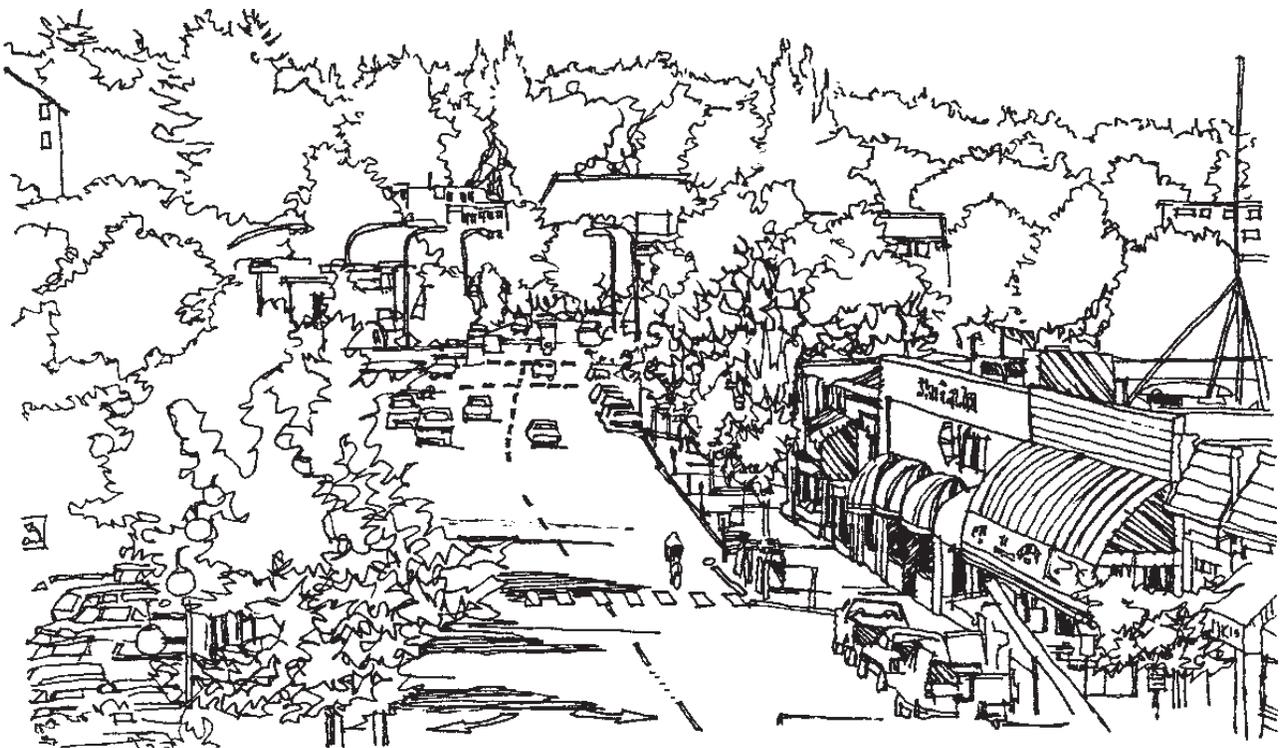
# Introduction

This document sets forth a series of Design Guidelines, adopted by Section 3.30 of the Kirkland Municipal Code, that will be used by the City in the design review process. For Board Design Review (BDR), the Design Review Board will use these guidelines in association with the Design Regulations of the Kirkland Zoning Code. To the extent that the standards of the Design Guidelines or Design Regulations address the same issue but are not entirely consistent or contain different levels of specificity, the Design Review Board will determine which standard results in superior design. For Administrative Design Review (ADR), the Planning Official will use these guidelines when necessary to interpret the Design Regulations. They are also intended to assist project developers and their architects by providing graphic examples of the intent of the City's guidelines and regulations.

Most of the concepts presented in the Design Guidelines are applicable to any pedestrian-oriented business district.\* “Special Considerations” have been added, such as for Downtown Kirkland, to illustrate how unique characteristics of that pedestrian-oriented business district relate to the Guideline.

The Design Guidelines do not set a particular style of architecture or design theme. Rather, they will establish a greater sense of quality, unity, and conformance with Kirkland's physical assets and civic role.

The Design Guidelines will work with improvements to streets and parks and the development of new public facilities to create a dynamic setting for civic activities and private development. It is important to note that these Guidelines are not intended to slow or restrict development, but rather to add consistency and predictability to the permit review process.



*\* The guidelines also apply to residential development in the Central Business District (CBD), the Juanita Business District (JBD), the North Rose Hill Business District, the Market Street Corridor (MSC), Totem Center and to mixed use development throughout the City.*



## Kirkland Design Guidelines

The drawing below illustrates many of the design Guidelines described in this appendix

- 1 Pedestrian plazas and places for vendors encouraged through several regulations.
- 2 Buildings on corner lots may be required to incorporate an architectural or pedestrian-oriented feature at the corner. Many options are possible including plazas, artwork, turrets, curved corners, etc.

**Special architectural requirements placed on use of concrete block and metal siding.**

- 3 “Architectural scale” requirements direct large buildings to fit more comfortably with neighboring development. This example employs building setbacks, decks, curved surfaces, and recessed entries to reduce appearance of building mass.
- 4 Parking garages on pedestrian-oriented streets or through-block sidewalks may incorporate pedestrian-oriented uses or pedestrian-oriented space into front facades.

**Street trees required along certain streets.**

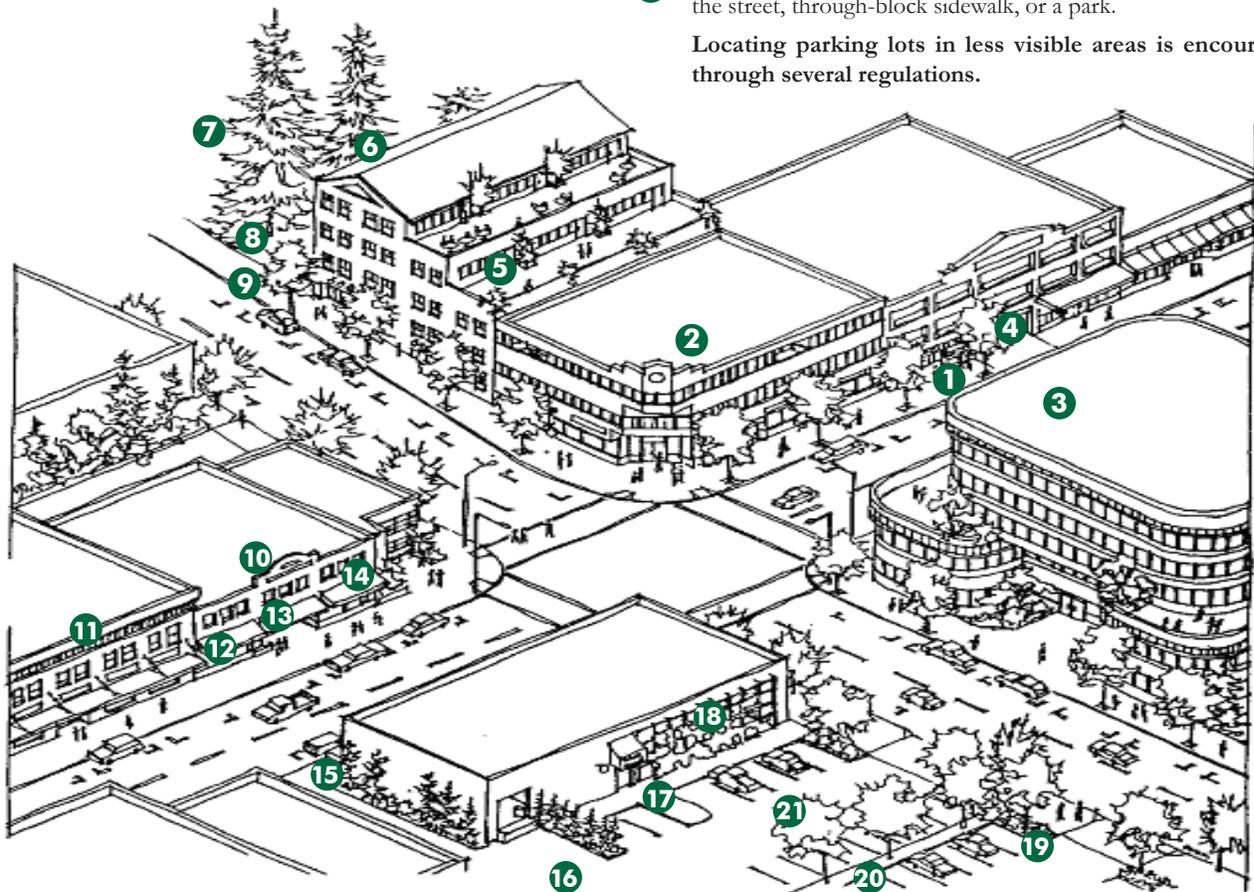
- 5 Human scale features such as balconies or decks, bay windows, covered entries, gable or hipped rooflines, multiple paned windows, or pedestrian-oriented space may be required.
- 6 More flexible method of measuring building height on slopes.
- 7 New policies regarding tree protection and enhancement of wooded slopes. Standards for size, quantity, quality, and maintenance of landscape plant materials are set by the Zoning Code.

- 8 Standards for size, quantity, quality, and maintenance of landscape plant materials are set by the Zoning Code.
- 9 Standards are set for pathway width, pavement, lighting, and site features on required major pathways and public properties.
- 10 A building cornerstone or plaque may be required.
- 11 Covering up existing masonry or details with synthetic materials is restricted.
- 12 Ground story facades of buildings on pedestrian-oriented streets or adjacent to parks may be required to feature display windows, artwork, or pedestrian-oriented space.
- 13 Pedestrian weather protection required on pedestrian-oriented streets.
- 14 Architectural detail elements such as decorative or special windows, doors, railings, grillwork, lighting, trellises, pavements, materials, or artwork to add visual interest may be required.

**Size of parking lots abutting pedestrian-oriented streets may be restricted.**

- 15 Quantity and locations of driveways are regulated.
- 16 Visible service areas and loading docks must be screened.
- 17 Provision for pedestrian circulation is required in large parking lots.
- 18 Blank walls near streets or adjacent to through-block sidewalks must be treated with landscaping, artwork, or other treatment.
- 19 Screening of parking lots near streets is required.
- 20 Standards for curbs, signing, lighting, and equipment are set for parking lots.
- 21 Internal landscaping is required on large parking lots visible from the street, through-block sidewalk, or a park.

**Locating parking lots in less visible areas is encouraged through several regulations.**



## Purpose of the Design Guidelines for Downtown Kirkland

In 1989 the Kirkland City Council adopted Kirkland's Downtown Plan which set a vision for the downtown's future and outlined policies and public actions to make that vision a reality. One of the recommended actions is the adoption of a set of Downtown Design Guidelines to be used in reviewing all new development and major renovations in the downtown area. The goal of the Design Guidelines as stated in the plan is to

*... balance the desired diversity of project architecture with the equally desired overall coherence of the downtown's visual and historic character. This is to be achieved by injecting into each projects' creative design process a recognition and respect of design guidelines and methods which incorporate new development into downtown's overall pattern.*

In addition, the guidelines are intended to further the following urban design goals stated in the plan:

- ◆ Promote a sense of community identity by emphasizing Kirkland's natural assets, maintaining its human scale, and encouraging activities that make downtown the cultural, civic, and commercial heart of the community.
- ◆ Maintain a high-quality environment by ensuring that new construction and site development meet high standards.
- ◆ Orient to the pedestrian by providing weather protection, amenities, human scale elements, and activities that attract people to downtown.
- ◆ Increase a sense of continuity and order by coordinating site orientation, building scale, and streetscape elements of new development to better fit with neighboring buildings.
- ◆ Incorporate parks and natural features by establishing an integrated network of trails, parks, and open spaces and maintaining existing trees and incorporating landscaping into new development.
- ◆ Allow for diversity and growth through flexible guidelines that are adaptable to a variety of conditions and do not restrict new development.

## Purpose of the Design Guidelines for Juanita Business District

The Juanita Business District Plan was adopted in 1990 by the City Council. It states that "the underlying goal of redevelopment in the business district is to create a neighborhood-scale, pedestrian district which takes advantage of the amenities offered by Juanita Bay."

As part of the Juanita Business District Plan, Design Regulations and Design Guidelines were established for new development and major renovations in the Business District (JBD). These guidelines and regulations are intended to further the following urban design features stated in the plan:

- ◆ Pedestrian pathways from the surrounding residential areas to and through the business district and on to Juanita Beach Park should be acquired and improved.
- ◆ View corridors to the lake should be explored through new development in the business district.
- ◆ Entry features, such as signs or sculpture, should be established in the locations shown in the Juanita Business District Plan.
- ◆ Coordinated streetscape improvements should be used throughout the business district, including street trees, street furniture, and other amenities, like flowers, banners, and signs.

## Purpose of the Design Guidelines for the Market Street Corridor, including the Market Street Historic District

The City Council adopted the Market Street Corridor Plan in December of 2006 as part of the Market and Norkirk Neighborhood planning process. The new plan was created for commercial and multifamily properties adjoining Market Street extending from the Central Business District at the south end to 19th Avenue at the north end. The plan includes a vision for the corridor of an attractive, economically healthy area that accommodates neighborhood oriented businesses, office uses and multifamily housing in a way that complements and protects the adjacent residential neighborhoods.

The historic 1890's buildings at the intersection of Market Street and 7th Avenue create a unique sense of place that represents the original town center of Kirkland. The plan establishes an historic district in this area that will reflect the City's past through both its old and new buildings and its streetscape. New development and renovation within



this historic district should reflect the scale and design features of the existing historic resources in the district.

As part of the Market Street Corridor Plan, Design Regulations and Guidelines are established for new development and major renovations in the Market Street Corridor (MSC). These guidelines and regulations are intended to further the following design objectives that are stated in the plan:

- ◆ Encourage preservation of structures and locations that reflect Kirkland’s heritage.
- ◆ Support a mix of higher intensity uses along the Market Street Corridor while minimizing impacts on adjacent residential neighborhoods.
- ◆ Maintain and enhance the character of the historic intersection at 7th Avenue and Market Street.
- ◆ Provide streetscape, gateway and public art improvements that contribute to a sense of identity and enhanced visual quality.
- ◆ Provide transitions between low density residential uses within the neighborhoods and the commercial and multifamily residential uses in the Market Street.

Except for the MSC 2 zone,

The following guidelines, which suggest wider sidewalks, do not apply since there are no “pedestrian oriented streets” or “major pedestrian sidewalks” designated in the Zoning Code for the Market Street Corridor.

- ◆ Sidewalk Width: Movement Zone
- ◆ Sidewalk Width: Storefront Activity Zone

Additional guidelines that do not apply to the Market Street Corridor include:

- ◆ Protection and Enhancement of Wooded Slopes
- ◆ Height Measurement on Hillsides
- ◆ Culverted Creeks

## Purpose of the Design Guidelines for North Rose Hill Business District

The North Rose Hill Business District goals and policies were adopted in 2003 as part of the North Rose Hill Neighborhood Plan. Development in the North Rose Hill Business District (NRHBD) is to complement the Totem Lake neighborhood and encourage increased residential capacity to help meet housing needs. Commercial uses are to be limited to those that are compatible with the residential focus of the NRHBD.

As part of the NRH plan, design regulations and guidelines were established for new development and major renovations in the Business District (NRHBD). These guidelines and regulations are intended to further the following urban design goals and policies stated in the plan:

- ◆ Ensure that public improvements and private development contribute to neighborhood quality and identity in the Business District through:
  - *Establishment of building and site design standards.*
  - *Utilization of the design review process.*
  - *Location and sharing of parking lots .*
  - *Utilization of high quality materials, public art, bicycle and pedestrian amenities, directional signs on all arterials, and other measures for public buildings and public infrastructure, such as streets and parks.*
- ◆ Provide transitions between commercial and residential uses in the neighborhood.
- ◆ Provide streetscape improvements that contribute to a sense of neighborhood identity and enhanced visual quality.

Since the focus of the NRHBD is on increasing residential capacity while accommodating supportive commercial uses, rather than developing into a destination retail business district, the following guidelines do not apply to this business district.

- ◆ Sidewalk Width – Movement Zone
- ◆ Sidewalk Width – Curb Zone
- ◆ Sidewalk Width – The Storefront Activity Zone
- ◆ Pedestrian Coverings
- ◆ Pedestrian-Friendly Building Fronts
- ◆ Upper-Story Activities Overlooking the Street

In addition, the following do not apply:

- ◆ Protection and Enhancement of Wooded Slopes
- ◆ Height Measurement on Hillsides
- ◆ Views of Water
- ◆ Culverted Creeks

## Purpose of the Design Guidelines for Totem Center

The Kirkland City Council adopted a new neighborhood plan for Totem Lake in early 2002. The vision set forth in the Plan for Totem Center is of a dense, compact community, with a mix of business, commercial and residential uses and a high level of transit and pedestrian activity.



The Plan establishes key overall design principles for Totem Center, as well as specific design objectives for the Totem Lake Mall (TL 2), Evergreen Hospital campus (TL 3), and the mixed-use area west of the campus (TL 1). Design objectives promoted in the plan for Totem Center include:

- ◆ Accommodate high density, transit-oriented development, consistent with the district's position in an Urban Center.
- ◆ Ensure that public and private development contribute to a lively and inviting character in Totem Center.
- ◆ Reinforce the character of Totem Center through public investments
- ◆ Produce buildings that exhibit high quality design, incorporate pedestrian features and amenities and display elements of both continuity and individuality
- ◆ Provide public spaces that are focal points for the community
- ◆ Provide visual and functional connections between adjacent developments through landscaping, public spaces and pedestrian connections.

Design considerations specific to the three subareas within the district include:

#### Mixed-Use Area (TL 1)

- ◆ Break up the mass of larger buildings through techniques such as towers over podiums, to create a varied building footprint and the perception of a smaller overall building mass.
- ◆ Incorporate features that create distinctive roof forms, to contribute to a skyline that is visually interesting throughout the district.
- ◆ Ensure appropriate transitions from lower density uses north of Totem Center through providing residentially scaled façades and centered building masses in development along NE 132nd Street.

#### Retail Center (TL 2)

The Totem Lake Neighborhood Plan direction for the TL2 area is to support its growth as a vibrant, intensive retail center for the Kirkland community and surrounding region. These guidelines are intended to promote the vision of this area as a "village-like" community gathering place, with high-quality urban and architectural design in redevelopment. To provide for flexibility and increased development potential, while ensuring coordinated development and design integrity over time, redevelopment should occur within the context of an overall site development or Master Plan for the entire property.

#### Evergreen Hospital Medical Center Campus (TL 3)

The Totem Lake Neighborhood Plan acknowledges the important role the hospital plays in the Kirkland community, and supports growth on the campus to strengthen this role. Design objectives stated in the Plan for the Evergreen Hospital campus are consistent with those expressed in the Master Plan approved for the site:

- ◆ Taller buildings should be located toward the center of the site and designed to minimize shadowing and transition impacts on residential areas.
- ◆ Public access to usable green spaces on the campus can help to offset the impacts of taller buildings on the site.
- ◆ Ensure campus edges are compatible with neighboring uses.
- ◆ Enhance and improve pedestrian access with the campus and to surrounding uses, particularly the transit center and to TL 2.

The approved Master Plan for the hospital campus includes additional, unique design guidelines that apply to institutional development in a campus environment:

- ◆ *Respond to Physical Environment:* New buildings should be attractive as well as functional additions to the campus.
- ◆ *Enhance the Skyline:* The upper portion of buildings should be designed to promote visual interest and variety on the skyline, except where building function dictates uninterrupted vertical mass.
- ◆ Avoid blank facades in buildings located on the perimeter of the campus.

- ◆ Use materials and forms that reinforce the visual coherence of the campus.
- ◆ Provide inviting and useable open space.
- ◆ Enhance the campus with landscaping.
- ◆ Guidelines for the transit center to be located on the hospital campus should be developed and incorporated with guidelines for the rest of the campus.

The following guidelines do not apply to Totem Center:

- ◆ Height Measurement on Hillsides
- ◆ Views of Water

### **Purpose of the Design Guidelines for Neighborhood Business Districts**

The Comprehensive Plan establishes a hierarchy of commercial districts, with regional goods and services at the upper end and neighborhoods goods and services at the lower end. Kirkland's Neighborhood Business Districts (BN, BNA, and MSC 2) are designated as Residential Markets, with an emphasis on providing neighborhood goods and services. Given the more localized draw for residents to meet their everyday needs, an emphasis on convenient and attractive pedestrian connections and vehicular access is important. In addition, because these districts are surrounded by the residential land uses they serve, the design character and context of new development is critical to ensure that it integrates into the neighborhood.

The design guidelines are intended to further the following design objectives that are stated in the Plan:

- Establish development standards that promote attractive commercial areas and reflect the distinctive role of each area.
- Encourage and develop places and events throughout the community where people can gather and interact.
- Moss Bay neighborhood: Ensure that building design is compatible with the neighborhood in size, scale and character.
- South Rose Hill neighborhood: Residential scale and design are critical to integrate these uses into the residential area.

The following guidelines do not apply to these districts:

- Protection and Enhancement of Wooded Slopes
- Height Measurement on Hillsides
- Culverted Creeks

# Pedestrian-Oriented Elements

## Introduction

Successful pedestrian-oriented business districts, as opposed to “commercial strips,” depend upon making pedestrian circulation more convenient and attractive than vehicular circulation, because the retail strategy for such districts is to encourage the customer to visit often and for more than one purpose at a time. The desired shopping pattern is for the customer to park in a convenient location and walk to several different businesses or attractions. The guidelines in this section focus on creating a high-quality pedestrian environment, especially along *pedestrian-oriented streets*. *Pedestrian-oriented streets* are specific streets defined for each business district.

This section also deals with building elements that detract from pedestrian qualities. One such detraction is a large expanse of blank wall, which, when adjacent or near to neighboring properties or overlooking public areas, can be intrusive and create undesirable conditions for pedestrians and neighbors. Therefore, the guidelines direct new development to treat blank walls with landscaping, building modulation, or other elements to reduce the impact of blank walls on neighboring and public properties.

The guidelines dealing with the spatial and functional integration of sidewalk areas and building elements address several issues:

- ◆ Width of sidewalk to accommodate pedestrian flow, building entrances, and other sidewalk activities.
- ◆ Pedestrian weather protection.
- ◆ “Pedestrian-friendly” building fronts.
- ◆ Other building facade elements that improve pedestrian conditions along the sidewalk.
- ◆ Mitigation of blank walls and screening of service areas.





*On the following pages are described urban design guidelines relating to pedestrian circulation and amenities. The guidelines outline the general issues and present design information, concepts, and solutions to address the issues. The guidelines serve as a conceptual foundation and support the regulations included in the Kirkland Zoning Code.*

## Sidewalk Width: Movement Zone

### Issue

Pedestrian movement is a primary function of sidewalks. The sidewalk has three overlapping parts with different functions: the curb zone, the movement zone, and the storefront or activity zone.

A well-sized and uncluttered movement zone allows pedestrians to move at a comfortable pace. People can window-shop comfortably and enjoy a relaxed atmosphere without bumping into street signs, garbage cans, or other people.

### Discussion

An adult person measures approximately 2' across the shoulders, but a pedestrian carrying grocery bags, pushing a baby carriage or bicycle, or walking a dog measures 3' across. A window-shopper will require a minimum of 2'-6" to 3' wide space to avoid being pushed or having their view obstructed.

The movement zone should be at least 10' to 12' wide so that two couples can comfortably pass one another. This same space also will allow one person to pass a couple while another person passes from the opposite direction. In business districts add 3' to the storefront activity zone for window-shopping.

The width of the sidewalk movement zone should consider the function of sidewalks, the level of pedestrian traffic, and the general age groups of the pedestrians (children and the elderly slow traffic on sidewalks that are too narrow).



### Guideline

*A sidewalk should support a variety and concentration of activity yet avoid overcrowding and congestion. The average sidewalk width should be between 10' and 18'. New buildings on pedestrian-oriented streets should be set back a sufficient distance to provide at least 10' of sidewalk. If outdoor dining, seating, vending, or displays are desired, an additional setback is necessary.*

### Special Consideration for Downtown Kirkland

Most of the business core of Kirkland is already developed with fairly narrow sidewalks. New development should provide sidewalks at the recommended width. Providing wider sidewalks throughout downtown is a long-term endeavor.

### Special Consideration for Juanita Business District

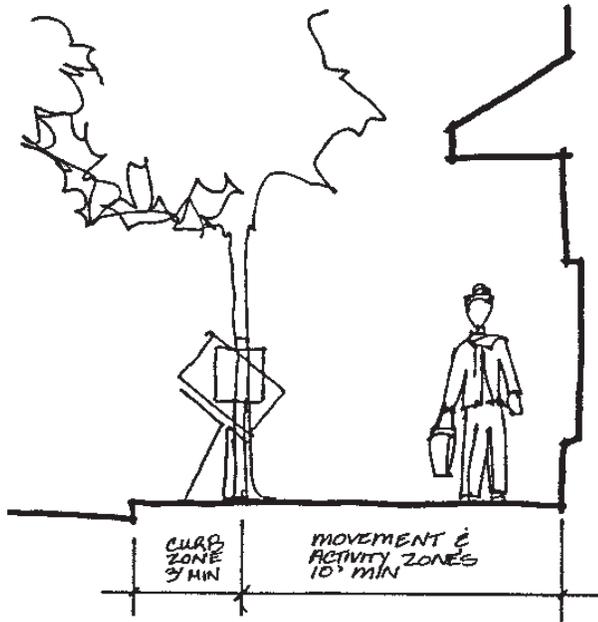
A concentrated, organized, retail-oriented core with a unified pedestrian circulation network is a goal of the Juanita Business District. The pedestrian system will also serve to connect the perimeter of the district to the core.

### Special Consideration for Totem Center

New development in TL2 should provide sidewalks at the recommended width, to contribute to the pedestrian-orientation of new development. Public gathering places, such as pedestrian-oriented plazas linked to the sidewalk, should be encouraged.



## Sidewalk Width – Curb Zone



### Issue

The curb zone contains parking meters, garbage cans, newspaper stands, street signs, light poles, mail boxes, phone booths, bus stops, and trees. The curb zone is also a buffer between vehicular traffic and pedestrians.

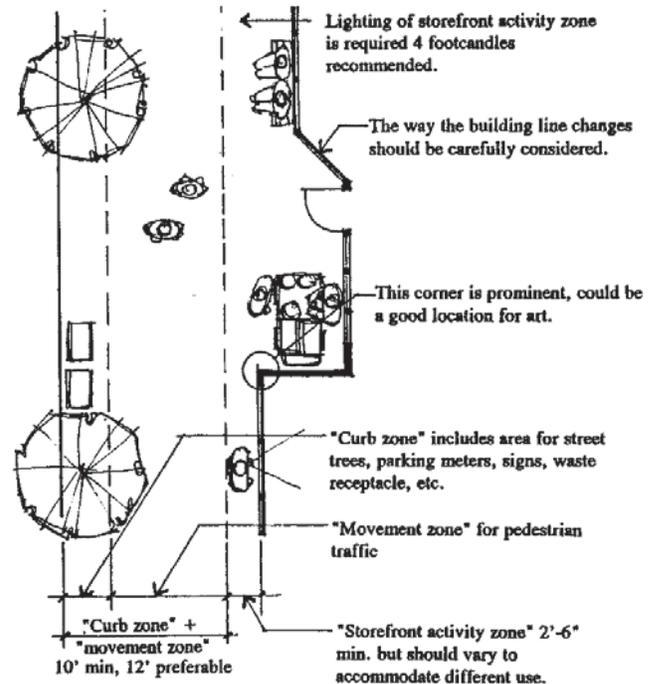
### Discussion

The curb zone may be integrated into the sidewalk design in a number of ways.

- ◆ *A curb zone with parallel parking.* Getting in and out of parked cars requires 2'-6"; so the curb zone width should be between 4'-6" and 5'-6".
- ◆ *A curb zone without parallel parking.* Space is not needed to park cars; the curb zone width should be between 3' and 4'.
- ◆ *A curb zone with street furniture clustered in sidewalk bulbs along the street; parking is allotted in the pockets between the bulbs.* Clusters of street elements – benches, newspaper stands, covered bus stops – require a sidewalk width of about 8' to 12'.

The curb zone may be visually separated from the movement zone by changes in color or surface material. Street furniture and other elements may be grouped and unified by color and shape to give the street a less cluttered appearance.

The design of the curb zone and street elements provides an opportunity for Kirkland to develop a visual identity that differs from street to street yet is still characteristic of Kirkland.



### Guidelines

*Street elements – trees, parking meters, signs – should be organized in the curb zone to reduce congestion. During busy periods, pedestrians may use the curb zone for walking.*

*Where pedestrian traffic is the heaviest, sidewalk bulbs can be constructed to accommodate bike racks, waste receptacles, and newspaper racks. Corner bulbs also increase pedestrian visibility.*

## Sidewalk Width – The Storefront Activity Zone

### Issue

The storefront activity zone is the most important area for improving pedestrian amenities because it offers protection, provides space for sidewalk activities, and is a transition from the public space of the sidewalk to the private space of the building.

### Discussion

At least 10' of the sidewalk must be kept for pedestrian movement. In addition, there must be room for other activities that add life and interest to the street. Window shopping requires a minimum of 2'-6". Other activities require:

- ◆ Bench for sitting: 4' min.
- ◆ Vendor: 4' min. (6' preferable)
- ◆ Outdoor dining: 6' min. (one table)
- ◆ Outdoor displays: 4' min. (6' preferable)

The activity desired in the storefront activity zone can vary from property to property. This may result in a more animated sidewalk environment with protected alcoves and niches.

### Guideline

*New buildings should be set back a sufficient distance from the front property line a minimum of 10' to allow enough room for pedestrian movement. Wider setbacks should be considered to accommodate other sidewalk uses that would benefit their businesses and the pedestrian environment. Lighting and special paving of the storefront activity zone are also beneficial.*

## Pedestrian Coverings

### Issue

Pedestrian coverings such as awnings and canopies offer shelter, provide spatial enclosure, and add design interest to a retail streetscape.

### Discussion

The design of awnings and canopies should be coordinated with a number of factors:

*The width of a canopy or awning depends on its function. A 3' to 4' canopy will provide rain cover for window-shopping. A 5' or greater canopy will provide cover for a street sale, and a 7' to 8' canopy will provide room for a window shopper and a passing couple.*

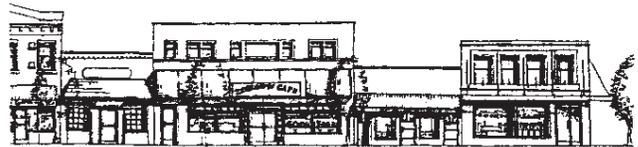
*The width of the sidewalk should be considered when sizing the awning. Water spilling down the edges of awnings is unpleasant; thus the awning should be either extended or shortened if there is not room for two people to pass one another either under the awning or outside the awning.*

*The architecture of the building determines the appropriate placement and style of the canopy or awning. A canopy should be continuous in shape, design, and placement throughout a building.*

*The overall style of a street should guide the choice of type, color, and size of coverings. The quality of light emanating from awnings or canopies should be controlled. The back-lit plastic awning typical of fast food chains is inappropriate on pedestrian streetscapes.*

*The crown of trees can be a canopy in its own right by defining space and providing shelter. Canopies and awnings should be appropriately dimensioned to allow for tree growth.*

*The street type. A rich variety of canopies and awnings is particularly desirable on pedestrian-oriented streets and less important on automobile-oriented streets.*



**Nonuniform Awnings and Facades  
(Recommended for Pedestrian Oriented Streets)**



### Guideline

*Awnings or canopies should be required on facades facing pedestrian-oriented sidewalks. A variety of styles and colors should be encouraged on pedestrian-oriented streets, and a more continuous, uniform style encouraged for large developments on entry arterial streets.*

## “Pedestrian-Friendly” Building Fronts

### Issue

Building setbacks were originally developed to promote “pedestrian-friendly” building fronts by providing light, air, and safety. But dull building facades and building setbacks that are either too wide or too narrow can destroy a pedestrian streetscape. A successful pedestrian business district must provide interesting, pedestrian-friendly building facades and sidewalk activities.

### Discussion

Building fronts should have pedestrian-friendly features transparent or decorative windows, public entrances, murals, bulletin boards, display windows, seating, or street vendors that cover at least 75 percent of the ground-level storefront surface between 2’ and 6’ above the sidewalk.



Sitting areas for restaurant and merchandise displays should allow at least a 10’ wide pavement strip for walking. Planters can define the sitting area and regulate pedestrian flow.

Blank walls severely detract from a pedestrian streetscape. To mitigate the negative effects of blank walls:

- ◆ Recess the wall with niches that invite people to stop, sit, and lean.
- ◆ Allow street vendors.
- ◆ Install trellises with climbing vines or plan materials.
- ◆ Provide a planting bed with plant material that screens at least 50 percent of the surface.
- ◆ Provide artwork on the surface.

### Guideline

*All building fronts should have pedestrian-friendly features as listed above.*

## Special Consideration for Downtown Kirkland - Glazing

Building frontages along pedestrian-oriented streets in the Central Business District should be configured to have a 15’ story height to ensure suitability for diverse retail tenants and enhance the pedestrian experience. Where these taller retail stories are required, special attention to storefront detailing is necessary to provide a visual connection between pedestrian and retail activity.

### Guideline

*Storefronts along pedestrian-oriented streets should be highly transparent with windows of clear vision glass beginning no higher than 2’ above grade to at least 10’ above grade. Windows should extend across, at a minimum, 75% of the façade length. Continuous window walls should be avoided by providing architectural building treatments, mullions, building modulation, entry doors, and/or columns at appropriate intervals.*

## Special Consideration For Non-Retail Lobbies In Central Business District 1A & 1B

Non-retail uses are generally not allowed along street frontage within Central Business District 1. However, in order to provide pedestrian access to office, hotel, or residential uses located off of the street frontage or above the retail, some allowance for lobbies is necessary.

### Guideline

*Lobbies for residential, hotel, and office uses may be allowed within the required retail storefront space provided that the street frontage of the lobby is limited relative to the property’s overall retail frontage and that the storefront design of the lobby provides continuity to the retail character of the site and the overall street.*

## Special Consideration for Totem Center

Since pedestrians move slowly along the sidewalk, the street level of buildings must be interesting and varied. Since

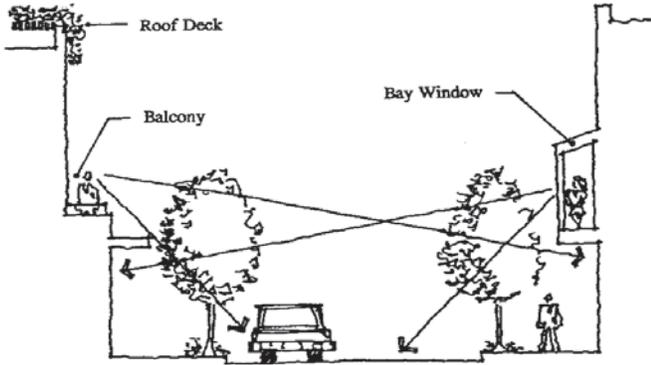
## Special Consideration for Neighborhood Business Districts

### Issue

*To create a focal point for the community and engage pedestrians, buildings are encouraged to be oriented to pedestrian-oriented streets in these zones. However, commercial space that is above or below the grade of the sidewalk can compromise the desired pedestrian orientation.*

### Guideline

*Commercial space should generally be at grade with the adjoining sidewalk. Where this is not feasible, the building should be setback from the sidewalk far enough to allow a comfortable grade transition with generous pedestrian-oriented open space.*



## Upper-Story Activities Overlooking the Street

### Issue

Upper-story architectural features such as balconies, roof decks, and bay windows improve the relation between the upper-story living and working units and the street. Upper-story activity provides additional security at night – people overlooking a street tend to “patrol” it – and give the street a more human, people-oriented quality.

### Discussion

All buildings should have either an individual balcony or bay window for each dwelling unit or a collective roof deck that overlooks the street or both. This is especially important on the second and third floors where it is easier to establish connection with people on the street level.

Retail stores, offices, and studios liven second stories, particularly at night when second story activities are silhouetted.

Balconies should have direct access from an interior room and be at least 6’ in depth so that two or three people can sit at a small table and have enough room to stretch their legs.

Plantings are encouraged on balconies and roof decks in order to bring more greenery into the City. Window seating at bay windows enables people to sit by a window and overlook the street.

## Guideline

All buildings on pedestrian-oriented streets should be encouraged to have upper-story activities overlooking the street, as well as balconies and roof decks with direct access from living spaces. Planting trellises and architectural elements are encouraged in conjunction with decks and bay windows. Upper-story commercial activities are also encouraged.

## Lighting from Buildings

### Issue

Overpowering and uniform illumination creates glare and destroys the quality of night light. Well-placed lights will form individual pools of light and maintain sufficient lighting levels for security and safety purposes.

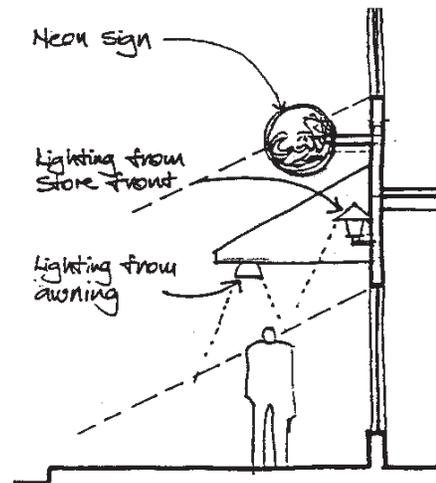
### Discussion

All building entries should be lighted to protect occupants and provide an inviting area.

Building facades, awnings, and signs should not be lighted with overpowering and uniform lights. They should be lighted with low-level building-mounted lights and placed apart from canopies, awnings, and signs to light streets.

### Recommendations

- ◆ Pri
- ◆ Sec
- ◆ Pat



## Guideline

All building entries should be well lit. Building facades in pedestrian areas should provide lighting to walkways and sidewalks through building-mounted lights, canopy- or awning-mounted lights, and display window lights. Encourage variety in the use of light fixtures to give visual variety from one building facade to the next. Back-lit or internally-lit translucent awnings should be prohibited.

## Pedestrian-Oriented Plazas

### Issue

Too often we see well-designed – but empty – plazas. There is no clear formula for designing a plaza, but a poorly designed plaza will not attract people.

### Discussion

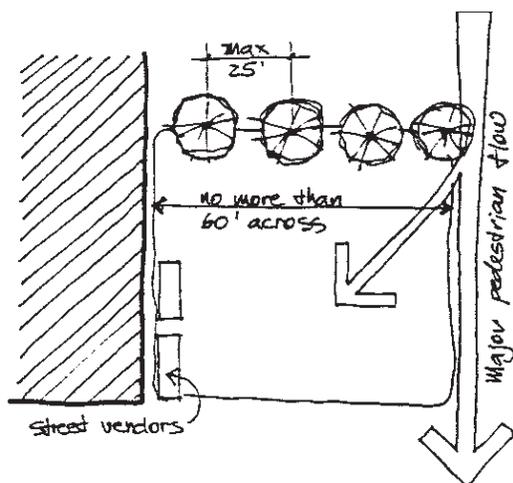
Plazas should be centrally located on major avenues, close to bus stops, or where there are strong pedestrian flows on neighboring sidewalks.

Plazas should be no more than 60' across and no more than 3' above or below the sidewalk. They must be handicapped accessible.

Plazas should have plenty of benches, steps, and ledges for seating. At least one linear foot of seating per 30 square feet of plaza area should be provided; seating should have a minimum depth of 16".

Locate the plaza in a sunny spot and encourage public art and other amenities. At least 50 percent of the total frontage of building walls facing a plaza should be occupied by retail uses, street vendors, or other pedestrian-oriented uses.

Provide plenty of planting beds for ground cover or shrubs. One tree should be required for every 200 square feet at a maximum spacing of 25' apart. Special precaution must be taken to prevent trees from blocking the sun.



## Guideline

Successful pedestrian-oriented plazas are generally located in sunny areas along a well-traveled pedestrian route. Plazas must provide plenty of sitting areas and amenities and give people a sense of enclosure and safety.

### Special Considerations for Totem Center

Public spaces, such as landscaped and/or furnished plazas and courtyards should be incorporated into the development, and be visible and accessible from either a public sidewalk or pedestrian connection. Primary pedestrian access points to retail development in TL 2 along 120<sup>th</sup> Avenue NE may be especially effective locations for public plazas.

Open spaces are especially important in TL 1, where the built environment may be dense. Well designed open spaces in front of and between buildings, visually linked with the open spaces of adjacent developments, will help to provide relief for the pedestrian.

## Pedestrian Connections

### Issue

The ability to walk directly into a commercial center from the public sidewalk or a bus stop is essential to both pedestrian and vehicular safety.

### Discussion

Well defined, direct pedestrian connections from the building to the public sidewalk are not always available in commercial centers. The connection between the internal pedestrian system on the site and the public sidewalk is often interrupted by landscaping or an automobile driveway.

Properly located landscaping can be used along with special paving to help define pedestrian links through the site



## Guideline

*Commercial developments should have well defined, safe pedestrian walkways that minimize distances from the public sidewalk and transit facilities to the internal pedestrian system and building entrances.*

## Blank Walls

### Issue

Blank walls create imposing and dull visual barriers. On the other hand, blank walls are ready “canvases” for art, murals, and landscaping.

### Discussion

*Blank walls on street fronts.* Blank walls on retail frontage deaden the surrounding space and break the retail continuity of the block. Blank walls should be avoided on street front elevations. The adverse impact of a blank wall on the pedestrian streetscape can be mitigated through art, landscaping, street vendors, signs, kiosks, bus stops, or seating. Design guidelines in New York, San Francisco, and Bellevue recommend that ground floor retail with pedestrian-oriented displays be the primary uses in commercial districts. This approach is meant to restore and maintain vitality on the street via continuous rows of retail establishments.

*Blank walls perpendicular to street fronts.* In some cases fire walls require the intrusion of a flat, unadorned surface. These conditions merit landscaping or artistic treatment. Examples of such treatment include installing trellises for vines and plant material, providing landscaped planting beds that screen at least 50 percent of the wall, incorporating decorative tile or masonry, or providing artwork (mural, sculpture, relief) on the wall.



## Guideline

*Blank walls should be avoided near sidewalks, parks, and pedestrian areas. Where unavoidable, blank walls should be treated with landscaping, art, or other architectural treatments.*

# Public Improvements and Site Features

## Introduction

Site features and pedestrian amenities such as lighting, benches, paving, waste receptacles, and other site elements are an important aspect of a pedestrian-oriented business district's character. If these features are design-coordinated and high quality, they can help to unify and upgrade the district's visual character. Development of a master plan for public spaces can provide a coordinated approach to their installation throughout the district.

The guidelines in this section apply primarily to elements associated with street right-of-ways, public parks, and required *major pedestrian pathways*. Although the standards do not apply to private property, except where a *major pedestrian pathway* is required, property owners are encouraged to utilize the standards in private development where they are appropriate. However, there may be cases where different site features, such as light fixtures and benches, should be selected to complement the architectural design of the individual site.



## Pathway Width

### Issue

Pathways must be sufficiently wide to handle projected pedestrian traffic. A pathway that is too narrow will have maintenance problems at its edges. A pathway that is too wide is unnecessarily costly and a poor use of space.

### Discussion

A pedestrian path of 10' to 12' can accommodate groups of persons walking four abreast or two couples passing each other.

A path near a major park feature or special facility like a transit center should be at least 12' wide. An 8' path will accommodate pedestrian traffic of less than 1,000 persons per hour.

Empirical Comparison:

- ◆ Green Lake path = 8'
- ◆ Burke-Gilman Path = 8'
- ◆ Typical sidewalk = 8' to 14'

### Guideline

***Design all major pedestrian pathways to be at least 8' wide. Other pathways with less activity can be 6' wide.***

### ***Special Considerations for Juanita Business District***

Through-site connections from street to street are a desirable pedestrian amenity in Land Use Area JBD-1.

The goal of these pedestrian connections will be to knit the individual developments into a more cohesive whole, providing convenient pedestrian mobility throughout even if the parcels are developed individually.

### ***Special Consideration for North Rose Hill Business District***

Buildings in the NRHBD will be setback at least ten feet from the sidewalk. Landscaping and entry features will be located within this setback yard. Therefore, the sidewalk can be somewhat narrower than on a pedestrian oriented street.

### ***Special Considerations for Totem Center***

Through-site connections from street to street, between the upper and lower portions of TL 2, and within TL 2 are needed to provide convenient pedestrian mobility, and to contribute to the village-like character desired for TL 2. Pedestrian connections to surrounding related uses, such as the hospital campus and transit center should also be provided.



Within TL 1, buildings should be set back at least ten feet from the sidewalk. Landscaping and entry features should be located within this setback yard, allowing the sidewalk to be somewhat narrower than on a pedestrian oriented street.

## Pedestrian Paths and Amenities

### Issues

Pedestrians require more detailed visual stimuli than do people in fast moving vehicles. Pedestrian paths should be safe, enjoyable, and interesting.

### Discussion

Street furniture such as benches, planters, fountains, and sculptures enhance the visual experience and reduce apparent walking lengths. Planters, curbs, rails, and other raised surfaces can also be used for seating. Any height between 12" to 20" will do with 16" to 18" being the best. An appropriate seat width ranges from 6" to 24".

Unit paving such as stones, bricks, or tiles should be installed on small plazas and areas of special interest. Asphalt can be used on minor routes to reduce cost and maintenance.

For safety reasons, lighting should be planned along all pedestrian paths. Lighting can originate either from street lights or from building-mounted lights. Street trees and shrubs should be planted along all pedestrian walkways and used to screen parking lots. For safety and appearance purposes, trees and shrubs should be pruned regularly.

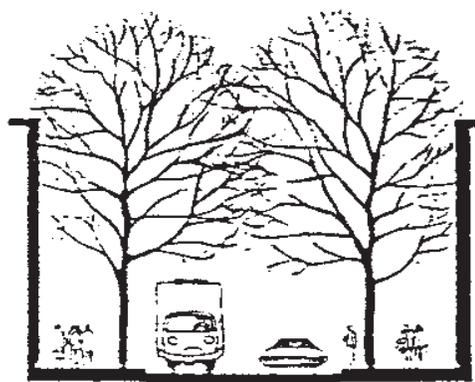
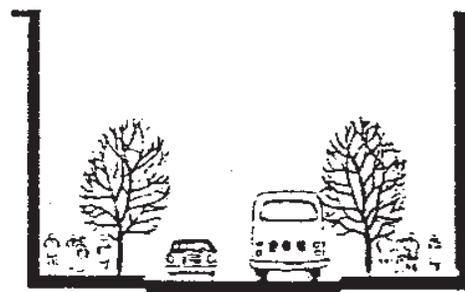
## Street Trees

### Issues

Streets are the conduits of life in a community. The repetition of trees bordering streets can unify a community's landscape. Trees add color, texture, and form to an otherwise harsh and discordant urban environment.

A strong street tree planting scheme can establish community identity and provide a respite from the weather and the built environment. Large, deciduous trees planted in rows on each side of the street can bring visual continuity to Kirkland – particularly on major entry arterials. Smaller trees should be planted in confined areas.

Street trees will not obscure businesses from the street if the appropriate trees are selected and maintained. Branches can frame ground floor businesses, allowing bus and truck movement while enhancing the pedestrian environment.



Trees should be of adequate size to create an immediate impact and have a good chance of survival. Species with invasive root systems or that are prone to disease, intolerant of pollution, or short-lived should be avoided.

### Guideline

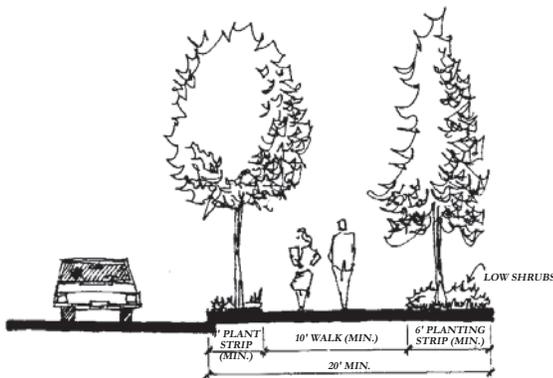
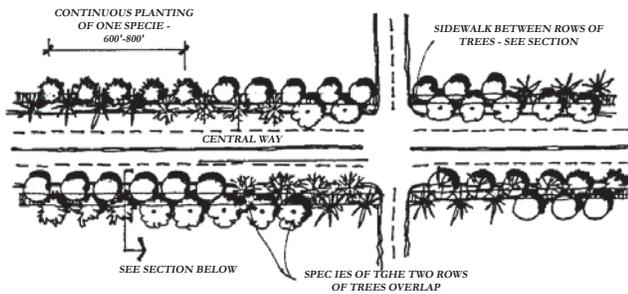
*The City should prepare a comprehensive street tree planting plan recommending species and generalized locations.*

### Special Considerations for Downtown Kirkland

A strong street tree planting scheme is especially important in downtown because of the variety of scale and architecture encouraged in private development. Major entries into Kirkland, especially along Central Way, Kirkland Avenue, Lake Street, and Market Street, should be unified by a strong street tree program.

### Some preliminary ideas for a street tree planting plan are:

*Central Way:* Two rows of trees on each side could be planted (one row near the curb and one row in the required setback on the perimeter of parking lots as in Parkplace). The two rows could feature uniform plantings of species approximately 600' to 800' long. The species could change so that different combinations of species occur along Central Way. This would provide a continuous boulevard effect and incorporate the existing trees.



Proposal for a distinctive, double-row tree planting of street trees on Central Way.

*Lake Street and other pedestrian-oriented streets with narrow sidewalks:* Flowering pear trees might be a good option since they have tight narrow shapes, attractive flowers, and dark green foliage. Photinia standards might be another option since they are small and have bright red evergreen foliage.

### Special Considerations for Juanita Business District

Street trees in the business district should be upgraded with varieties that will not block views of businesses or the lake.

Some preliminary ideas for a street tree planting plan are:

**98th Avenue NE:** Limb up existing maples and add flowering pear trees (flowers and good fall color) along the curb.

**Juanita Drive:** Choose street trees that will screen large buildings but still allow views to the lake (flowering pears for example).

**97th Avenue NE/120th Place NE:** Plant trees to screen parking lots and service entrances. Possibilities are zelkova (elm-like with good fall color) or flowering pears.

### Special Considerations for the Market Street Corridor

A consistent street tree plan should be used to add character to the Corridor. The landscape strip on the east side of Market Street adds interest and provides a more secure pedestrian environment. Additional street trees should be considered on the west side of Market Street in order to provide a similar environment.

### Special considerations for North Rose Hill Business District

Feature a diverse planting of street trees that take into account width of landscape strip, location of overhead utility lines, and maintenance requirements.

Some preliminary ideas for a street tree planting plan are:

**NE 116<sup>th</sup> Street:** Add street trees that will buffer the pedestrian corridor from traffic while providing some visual access to adjacent businesses. (*Quercus rubra* (red oak), *Tilia cordata* ‘Greenspire’ (littleleaf linden), *Zelkova serrata* ‘Village Green’ for example).

**124<sup>th</sup> Avenue NE:** Choose street trees that will buffer the pedestrian but still allow some visual access to adjoining businesses (*Carpinus japonicus* (Japanese hornbeam), *Cercidiphyllum japonicum* (Katsura), *Fraxinus pennsylvanica* ‘Summit’ (Summit ash) for example).

**Slater Avenue NE:** Add trees with flowers and good fall colors as a transition to the residential portion of the neighborhood (*Malus* sp. (flowering crab), *Styrax japonicus* (Japanese snowbell), *Crataegus phaenopyrum* (Washington hawthorn), *Prunus padus* ‘Summer Glow’ (bird cherry- red leaves) for example).

### Special Considerations for Totem Center

Street trees within this area should be selected to achieve the varying objectives of the district. Some preliminary ideas for a street tree planting plan are:

**Totem Lake Boulevard:** South of NE 128<sup>th</sup> Street, trees should be planted that balance the goals of creating a “greenway” along the boulevard, providing a safe and inviting pedestrian experience and enabling visibility of the site’s businesses to the freeway traveler. Smaller trees planted at frequent intervals anchored by larger, “boulevard” trees at primary site entrances would achieve these objectives. As an alternative or additional component, groupings of trees planted behind a meandering sidewalk may also be effective.

**North of NE 128<sup>th</sup> Street to NE 132<sup>nd</sup> Street,** plantings should be unified with those used along Totem Lake Boulevard to the south.

**120<sup>th</sup> Avenue NE:** South of NE 128<sup>th</sup> Street, choose street trees that will emphasize the pedestrian connec-



tion between the upper and lower mall, such as the use of larger trees at crossings and major points of entry. Choose spacing and varieties to create a plaza-like character to encourage pedestrian activity. Trees in planters and colorful flower beds will soften the area for pedestrians but allow visual access to adjoining businesses.

The tree planting plan used along NE 128<sup>th</sup> Street between Totem Lake Boulevard and 120<sup>th</sup> Avenue NE should be continued to the segment of 120<sup>th</sup> Avenue NE between NE 128<sup>th</sup> Street and NE 132<sup>nd</sup> Street, to provide a consistent identity throughout the district.

**NE 132<sup>nd</sup> Street:** Create a strong streetscape element, inviting to the pedestrian, with street trees proportionate to adjacent land uses.

## Public Improvements and Site Features

### Issue and Discussion

The quality and character of public improvements and site features such as street and park lights, benches, planters, waste receptacles, pavement materials, and public signs are critical components of a city's image. Standards for public improvements and site features, along with a master plan for public spaces, will assist in the development of a coordinated streetscape that will unify the variety of private development. Successful standards help assure high quality, low maintenance site features, and simplify the purchase and replacement of features for parks and public works departments.

Since public improvement standards have long-term implications for the community, relevant City



departments must be involved in their development to make sure all concerns are met. Standards should permit some flexibility and address technical issues such as cost, availability, handicapped accessibility, and durability.

## Guideline

*The Department of Planning and Community Development, along with other City departments, should develop a set of public improvement and site feature standards for use in pedestrian-oriented business districts. The standards can be the same or unique for each district. A master plan for public spaces within a district should be adopted to coordinate placement of the features and otherwise carry out the Comprehensive Plan.*

The City of Kirkland should work with interested groups to design a public sign system for gateways, pathways, information kiosks, etc., with a signature color palette and identifying logo.

### Special Considerations for the Market Street Corridor

An historic style of street light should be used to reflect the nature of the 1890's buildings in the historic district at 7th Avenue and Market Street. These lights may also be used along other stretches of the corridor, particularly in the area between the Historic District and the Central Business District.

## Entry Gateway Features

### Issue

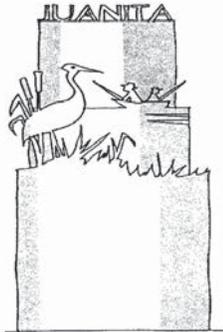
The Comprehensive Plan calls for gateway features at the key entry points into neighborhoods and business districts. Entry points differ in topography, available space, and surrounding visual character; nevertheless, gateway features should be reinforced by a unified design theme. Gateway features can be different in size or configuration, yet still incorporate similar materials, landscaping, graphics, and design elements.

### Discussion

The gateway features should frame and enhance views. Large sign bridges or flashing graphics would dominate the view and are inappropriate. Consistent elements that could be incorporated at all entry points might include:

- ◆ Distinctive landscaping such as floral displays or blue-green colored evergreen foliage.
- ◆ Multicolored masonry, perhaps forming a screen or wall on which an entry sign is placed.
- ◆ A distinctive light such as a column of glass block or cluster of globes.

- ◆ A unifying device such as the district’s logo. In Downtown Kirkland, for example, a triangular sail logo could be a metal weather vane or an actual fabric sail on a steel armature.
- ◆ A repetitive element such as a series of closely spaced sails or lights.
- ◆ A trellis incorporating landscaping. A trellis or arbor is adaptable to space constraints.
- ◆ Similar artwork such as a different animal or bird sculpture at each entry.



**Guideline**

*Construct entry gateway features at locations noted in the Comprehensive Plan. Gateways may be constructed in conjunction with commercial development. Emphasis should be placed on framing the view into the district.*

**Special Consideration for Downtown Kirkland**

The transit center is another “gateway” experience. The center should be a focal feature that provides comfort and amenities for transit users. Some form of shelter with a strong architectural identity should be pursued.

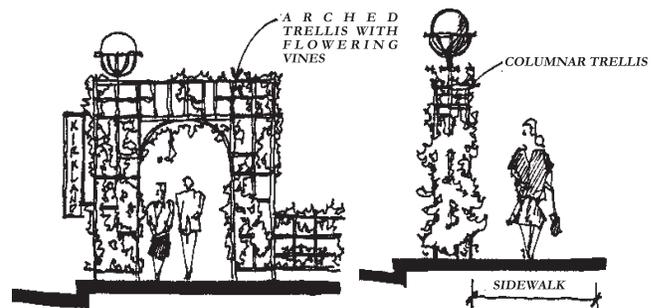
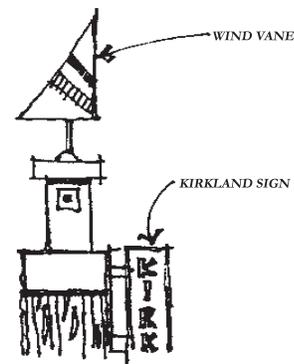
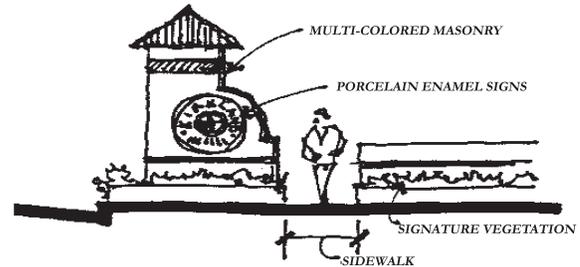
**Special Consideration for Juanita Business District**

The entry features should be “identity-giving elements” that reflect the business district and Juanita Bay. If successful they can become an identifying symbol or logo for the district and an attraction in themselves.

**Special Consideration for North Rose Hill Business District**

Use public art and private efforts to establish gateway features that strengthen the character and identity of the neighborhood. Use landscaping, signs, structures or other features that identify the neighborhood.

At the southwest corner of NE 116<sup>th</sup> Street and 124<sup>th</sup> Avenue NE a neighborhood gateway feature such as open space or plaza with signage should be integrated with a pedestrian connection linking Slater and NE 116<sup>th</sup> Street. In the alternative, a corner land mark consisting of a combination of open space and architectural building design features should be provided to identify the business district.



**Special Considerations for Totem Center**

The Transit Center on the hospital campus should be a “landmark” feature for both the Totem Center district and the hospital campus, providing a focal point for residents, employees and visitors. A combination of signs and symbols linking the transit center to the pedestrian connection along NE 128<sup>th</sup> Street, the flyer stop and the Park and Ride should be provided. Design of the transit center should be compatible with campus development yet be clearly identifiable as a facility serving the general public.

A prominent entry to the district exists at the intersection of NE 128<sup>th</sup> Street and Totem Lake Boulevard, where vehicles and pedestrians arrive from the crossing over I-405. Entry features provided in this area should contribute to the identity associated with the Totem Center district.

Public art and private efforts can be used to establish gateway features to strengthen the character and identity of Totem Center and the neighborhood. At the northern entry to Totem Center at 120<sup>th</sup> Avenue NE and NE 132<sup>nd</sup> Street, a neighborhood entry sign or other identifying neighborhood feature should be provided. Another important entry point identified in the neighborhood plan is along Totem Lake Boulevard, just east of 120<sup>th</sup> Avenue NE. A feature providing a sense of entry into the Totem Center district at this location would be appropriate.

## Public Art

### Issue

Art begins with the perceptions and expressive talents of individual artists. “Public art” applies that expression to the public realm either by its location in a public setting or by its emphasis on subjects relevant to the larger community. Public art contributes to the unique character, history, and sense of place of a community.

### Discussion

Public art is more than merely urban decoration; it can play an integral role in civic revitalization. Public art can make us more aware of our surroundings; reinforce the design character of our streets, parks, and buildings; commemorate special events; and serve as a catalyst for public activity and civic pride. At its best, art opens our eyes to new perceptions and helps us understand who we are and what is special about our community.

Public art is generally most effective when it is integrated with larger civic improvement efforts. Opportunities for art can be identified earlier and funding can be used more effectively. For example, emblems, lighting, pavement decorations, and decorative pedestrian furniture can be incorporated as part of a street improvement project at little cost to the total project such as in Seattle’s Third Avenue transit corridor, Port Angeles’s Maritime Flags, and Portland’s Transit Mall.

The involvement of an artist in the design of a park, fountain, street lighting, or signs can add a special quality that has more impact than if the artwork and the functional element were decorated separately. The famous art nouveau detailing on Paris’s metro stations is a good example.

### Guideline

*Kirkland should continue its tradition of encouraging public art pieces.*



# Parking Lot Location and Design

## Introduction

In pedestrian-oriented business districts, improperly located and poorly designed parking lots can destroy the ambiance and qualities that attract people to the district in the first place. This section contains guidelines to direct development of parking facilities. The number of required stalls is specified in the Kirkland Zoning Code. The guidelines in this section deal with:

- ◆ Parking lot location – Parking in front of buildings is discouraged, and combined lots that serve more than one business or use are encouraged.
- ◆ Parking lot entrances – The number of entries is addressed.
- ◆ Parking lot circulation and pedestrian access – Clear internal vehicular and pedestrian circulation is required, especially in large parking lots.
- ◆ Parking garages – Parking garages provide convenient, less intrusive parking. Yet, garages can themselves be intrusive since they are often large monolithic structures with little refinement, interest, or activity. The guidelines for parking garages are intended to make them fit into the scale and character of pedestrian-oriented districts.
- ◆ Parking Lot Landscaping – Parking lot landscaping should be more extensive if the lot has to be in a location that is visible from a street or public park than if the lot is located at the rear of the site hidden away from streets and neighboring properties. This provision is made to encourage parking lot development in less visible locations.

On the following pages, urban design guidelines are presented that outline design information, concepts, and solutions associated with parking lot development. They serve as a conceptual basis for the regulations in the Zoning Code.

## Parking Locations and Entrances

### Issue

Parking lots can detract from the pedestrian and visual character of a commercial area. The adverse impacts of parking lots can be mitigated through sensitive design, location, and configuration.

## Discussion

The ingress and egress of vehicles in parking lots disrupts pedestrian movement and through traffic – especially near intersections. Moreover, busy streets are a safety hazard. Parking lots that are accessed by a single curb cut reduce potential conflict and use land more efficiently. Also, combining the parking lots of individual stores into a large parking network makes it easier for patrons to find convenient parking stalls.

Parking lots should be encouraged in rear or side yards. The parking lot at Wendy's restaurant on Central Way is an example of this configuration.

The City of Seattle limits parking lot access on pedestrian-oriented streets such as Broadway on Capitol Hill.



## Guideline

***Minimize the number of driveways by restricting curb cuts and by encouraging property and business owners to combine parking lot entrances and coordinate parking areas. Encourage side and rear yard parking areas by restricting parking in front yards. Require extensive screening where there is front yard parking.***

## Special Consideration for Downtown Kirkland

Parking lot location and design is critical on busy entry streets such as Market Street, Central Way, Lake Street, Kirkland Avenue, and in the congested core area where pedestrian activities are emphasized. The *Downtown Plan* calls for limiting the number of vehicle curb cuts.

## Special Consideration for Juanita Business District and North Rose Hill Business District

Shared accesses and reciprocal vehicular easements should be established in order to reduce the number of curb cuts. The Juanita Business District Plan also encourages shared parking/service areas in Land Use Area JBD-1. This is particularly critical in TL 2, where buildings should front on 120<sup>th</sup> Avenue NE to foster the desired pedestrian-oriented environment.



## Special Consideration for Totem Center

Throughout Totem Center, parking areas located between the street and the building should be discouraged. This is particularly critical in TL 2, where buildings should front on 120<sup>th</sup> Avenue NE to foster the desired pedestrian-oriented environment.

## Circulation Within Parking Lots

### Issue

Large parking lots can be confusing unless vehicle and pedestrian circulation patterns are well organized and marked. Parking lots should be combined to reduce driveways and improve circulation.

### Discussion

*Vehicle Circulation.* Parking lots should have few dead-end parking lanes and provide drive-through configurations. The APA *Aesthetics of Parking* publication recommends channelized queuing space at the entrances and exits to parking lots to prevent cars from waiting in the street.

*Pedestrian Circulation.* Good pedestrian circulation is critical. A clear path from the sidewalk to the building entrance should be required for all sites, even through parking lots in front yards. For sites with large parking lots, clear pedestrian circulation routes within the lot from stalls to the building entrances should be provided. In addition, a raised concrete pavement should also be provided in front of the entrance as a loading or waiting area so the entrance will not be blocked by parked vehicles. Finally, pedestrian access between parking lots on adjacent properties should be provided.

### Guideline

*Parking lot design should be clear and well organized. Space should be provided for pedestrians to walk safely in all parking lots.*

## Special Consideration for Downtown Kirkland

Because land is limited in Downtown Kirkland, efficient and compact parking lot configurations are a top priority. Parking lots in the periphery of the core area that accommodate about 100 vehicles (approximately 3/4 to 1 acre) should be articulated with landscaped berms.

## Parking Lot Landscaping

### Issue

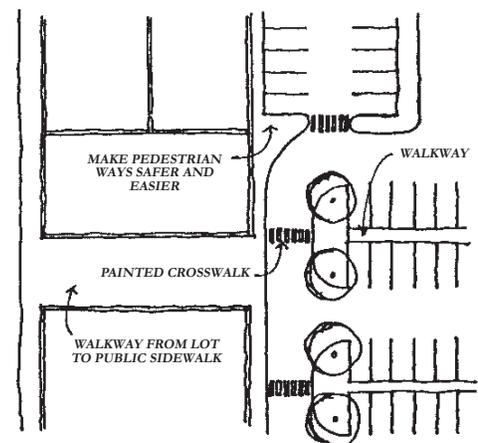
Parking lots are typically unsightly, require vast quantities of space, break the links between buildings, and destroy the continuity of streetfronts. If possible, parking lots should be located at the rear of buildings. When this is not possible, landscaping can be used to break up and screen parking lots.

### Discussion

Parking lots can be concealed by a structural screen wall or through the use of plant materials. Plant materials can create dense, hedge-like screens, separating lots from adjacent uses or public right-of-ways. Perimeter plantings must provide an adequate screen. A screen wall constructed in a similar style as adjacent development may be used in lieu of perimeter landscaping.

Trees along the edges of and within parking lots can effectively soften an otherwise barren and hostile space. Interior plantings can be consolidated to provide islands of greenery or be planted at regular intervals. Use of drought-tolerant plants can improve the likelihood that the landscaping will survive and look good.

Landscaping guidelines should be flexible and allow creative screening methods (e.g., clustering trees, berming, mixing structures, and trees). Less landscaping should be required if the lot is hidden from view.



### Guideline

*Parking lots must be integrated with the fabric of the community by creatively using landscaping to reduce their visual impact.*

### Special Considerations for the Market Street Corridor

Screening and landscaping should be required where parking is adjacent to single family residential uses in order to reduce impacts on the adjoining homes.

### Special Consideration for Juanita Business District, North Rose Hill Business District and Totem Center

Screening and landscaping should be required where parking is adjacent to sidewalks in order to improve visual qualities and reduce clutter.

Within TL 2, the provision of landscaping to soften the impacts of cars and pavement is important. Clusters of trees rather than single trees may be more effective in certain portions of the mall's parking areas. Visibility of the mall from the freeway should be considered when evaluating the locations and types of landscaping to be used.

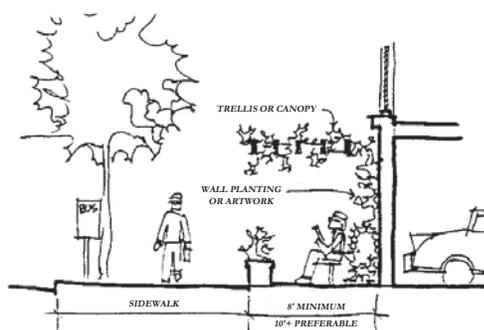
## Parking Garages Issue

Parking garages are some of the most unattractive buildings built during the past several decades. Most new parking structures are designed with little or no attention to screening or treatment of the facades.

### Discussion

There are several ways to mitigate the visual impacts of parking garages in the urban environment. A garage in a pedestrian area can contain a pedestrian-oriented retail use in the ground floor area of the garage adjacent to the street. Cafes, newsstands, or other small shops can fit well within the typical parking garage, requiring the space equivalent to only one 20' bay of parking.

Also, parking garages can be set back to provide space for a small landscaped plaza with a seating area. Moreover, the wall of the garage behind the plaza can be used as a canvas for landscaping or artwork. Also, the plaza could be covered with a glass canopy or trellis. The plaza should face south to receive sunlight. A plaza of this type is ideal for bus stops or street vendors.



In non-pedestrian areas, dense landscaping around the perimeter of parking garages can help screen their bulk. Strict standards for minimum landscaping around garages should be developed.

### Guideline

*The intrusive qualities of parking garages must be mitigated. In pedestrian areas, ground-level retail uses or appropriate pedestrian spaces should be required. Also, extensive landscaping should be required near residential areas and in highvisibility locations. On hillsides and near residential areas the stepping back or terracing of upper stories should be considered to reduce scale.*

### Special Consideration for Downtown Kirkland

Garages built on Downtown Kirkland's perimeter slopes, near residential areas, or near the waterfront can fit less obtrusively into the landscape when terraced. Treatment of the facade of the parking structure can be just as effective in mitigating the visual impacts of parking garages as pedestrian-oriented businesses, plazas, or landscaped setbacks at the ground level.

### Special Consideration for Totem Center

The development densities planned for Totem Center may result in the need for large parking structures to support them. Careful design of the structures will be important to retain a visually attractive environment.

The location of parking structures along pedestrian-oriented streets or pedestrian pathways should be discouraged. Where parking structures cannot be located underground and must be provided on the ground floor, an intervening use is desirable to retain the visual interest along the street. If parking areas are located in a separate structure from the primary use, the structure must be set back from the street, and screened with substantial landscaping.

Within TL 2, if it is not possible or practical to locate parking structures behind a building or underground, structured parking should be developed, oriented and screened to complement adjacent buildings, reduce automobile/pedestrian conflicts, and support the pedestrian environment. Artwork, display windows, trellises and/or dense vegetation are examples of screening devices that may be successful in balancing the scale of the structure with the pedestrian environment.



# scale

## Introduction

When architects talk about a building’s “scale,” they generally mean the perceived size of the building relative to an individual person or its surroundings. The term “human scale” is used to indicate a building’s size relative to a person, but the actual size of a building or room is often not as important as its perceived size. Architects use a variety of design techniques to give a space or structure the desired effect; whether it be to make a room either more intimate or spacious, or a building either more or less imposing. Frank Lloyd Wright, for example, used wide overhangs and horizontal rooflines to make his prairie-style houses appear lower and longer, better fitting into the flat, midwestern landscape. Unless the objective is to produce a grandiose or imposing building, architects generally try to give a building a “good human scale,” meaning that the building is of a size and proportion that feels comfortable. For most commercial buildings, the objective is to attract customers and visitors by designing comfortable, inviting buildings.

Generally, people feel more comfortable in a space where they can clearly understand the size of the building by visual clues or proportions. For example, because we know from experience the size of typical doors, windows, railings, etc., using traditionally-sized elements such as these provides a sense of a building’s size. Greek temples that feature columns, but not conventional doors, windows, or other elements, do not give a sense of human scale (although the Greeks subtly modified the properties and siting of their temples to achieve the desired scale). The guidelines in this section describe a variety of techniques to give a comfortable human scale by providing building elements that help individuals relate to the building.

“Architectural scale” means the size of a building relative to the buildings or elements around it. When the buildings in a neighborhood are about the same size and proportion, we say they are “in scale.” It is important that buildings have generally the same architectural scale so that a few buildings do not overpower the others. The exception to this rule is an important civic or cultural building that has a prominent role in the community. For example, nobody accuses a beautiful cathedral in a medieval European town of being “out of scale.” Because the Comprehensive Plan encourages a variety of different uses and building heights, such as in Downtown Kirkland, the buildings’ sizes will vary widely. To achieve a more harmonious relationship between the buildings and a more consistent character, design techniques should be used to break the volume of large buildings down into smaller units. Several guidelines in this section are directed toward achieving a consistent scale within districts.

The following guidelines illustrate some design techniques to give buildings a “sense of scale.” The regulations in the Zoning Code related to scale require that project architects address the issues of human and architectural scale while providing a wide range of options to do so.

## Fenestration Patterns

### Issue

The size, location, and number of windows in an urban setting creates a sense of interest that relies on a subtle mixture of correct ratios, proportions, and patterns. Excess window glazing on a storefront provides little visual contrast; blank walls are dull and monotonous. The correct window-to-wall ratio and a mix of fenestration patterns can create an enjoyable and cohesive urban character on both pedestrian- and automobile-oriented streets.

Many local contemporary buildings have “ribbon windows” (continuous horizontal bands of glass) or “window walls” (glass over the entire surface). Although effective in many settings, these window types do little to indicate the scale of the building and do not necessarily complement the architecture of small-scaled buildings. Breaking large expanses or strips of glass with mullions or other devices can help to give the building a more identifiable scale.

## Discussion

According to an old architectural cliché, windows are a building's eyes. We look to windows for visual clues as to the size and function of the building. If the window areas are divided into units that we associate with small-scale commercial buildings, then we will be better able to judge the building's size relative to our own bodies. Breaking window areas into units of about 35 square feet or less with each window unit separated by a visible mullion or other element at least 6 inches wide would accomplish this goal. Another successful approach is multiple-paned windows with visible mullions separating several smaller panes of glass. But on the ground floor where transparency is vital to pedestrian qualities, this device may be counterproductive.

Patterns of fenestration should vary depending on whether the street is pedestrian- or automobile-oriented. A window pattern that is interesting from a car may be monotonous to a slow-moving pedestrian; likewise, a window pattern that is interesting to a pedestrian may seem chaotic from a fast-moving car. Thus, pedestrian-oriented fenestration should allow for more complex arrangements and irregularity while automobile-oriented fenestration should have more gradual changes in pattern and larger and more simple window types.

An optimum design goal would allow for varied treatment of window detailing with unifying features such as 18" to 24" sills, vertical modulation in structure, varied setbacks in elevation, and more highly ornamented upper-story windows. Excessive use of ribbon windows throughout a building does not engage the eye and should be avoided.

## Guideline



***Varied window treatments should be encouraged. Ground floor uses should have large windows that showcase storefront displays to increase pedestrian interest. Architectural detailing at all window jambs, sills, and heads should be emphasized.***

## Special Considerations for the Market Street Corridor

Window treatment in the historic district should reflect the trim detailing, size, proportions, location and number of windows in the existing historic buildings in the district.

### Special Consideration for Downtown Kirkland

Breaking larger window areas into smaller units to achieve a more intimate scale is most important in Design Districts 1, 2, 4, 8, and the southwest portion of 3 where new buildings should fit with older structures that have traditional-styled windows. Architectural Elements Decks, Bay Windows, Arcades, Porches.

## Architectural Elements: Decks, Bay Windows, Arcades, Porches Issue

Special elements in a building facade create a distinct character in an urban context. A bay window suggests housing, while an arcade suggests a public walkway with retail frontage. Each element must be designed for an appropriate urban setting and for public or private use. A building should incorporate special features that enhance its character and surroundings. Such features give a building a better defined "human scale."

## Discussion

Requirements for specific architectural features should be avoided and variety encouraged. Building designs should incorporate one or more of the following architectural elements: arcade, balcony, bay window, roof deck, trellis, landscaping, awning, cornice, frieze, art concept, or courtyard. Insistence on design control should take a back seat to encouraging the use of such elements.

## Guideline

***Architectural building elements such as arcades, balconies, bay windows, roof decks, trellises, landscaping, awnings, cornices, friezes, art concepts, and courtyards should be encouraged.***

### Special Consideration for Downtown Kirkland

Pedestrian features should be differentiated from vehicular features; thus fenestration detailing, cornices, friezes, and smaller art concepts should be concentrated in Design Districts 1 and 2, while landscaping and larger architectural features should be concentrated in Design Districts 3, 5, 7, and 8.

### Special Consideration for Totem Center

Balconies provide private open space, and help to minimize the vertical mass of structures. Residential building facades visible from streets and public spaces should provide balconies of a sufficient depth to appear integrated with the building and not “tacked on”.

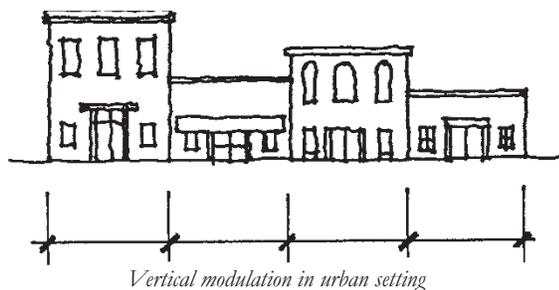
## Building Modulation – Vertical

### Issue

Vertical building modulation is the vertical articulation or division of an imposing building facade through architectural features, setbacks, or varying rooflines. Vertical modulation adds variety and visual relief to long stretches of development on the streetscape. By altering an elevation vertically, a large building will appear to be more of an aggregation of smaller buildings. Vertical modulation is well-suited for residential development and sites with steep topography.

### Discussion

Urban design guidelines should address vertical modulation in order to eliminate monotonous facades. Vertical modulation may take the form of balcony setbacks, varied rooflines, bay windows, protruding structures, or vertical circulation elements – the technique used must be integral to the architecture.



Vertical modulation is important primarily in neighborhoods where topography demands a stepping down of structures. The vertical modulation of a large development project in a residential area can make the project appear to be more in scale with the existing neighborhood. Long facades can be vertically modulated to better conform to the layout and development pattern of single-family houses. The vertical modulation of buildings on steep slopes also provides terraced development rather than one single building block, thereby better reflecting the existing terrain.

### Guideline

***Vertical building modulation should be used to add variety and to make large buildings appear to be an aggregation of smaller buildings.***



*This building uses both horizontal and vertical modulation to add interest and reduce its visual bulk.*

### Special Considerations for Totem Center

Since greater heights are allowed in TL 1 than elsewhere in the city, the impacts of increased height are a concern. Impacts associated with taller buildings are generally ones of reduced open space and privacy, shadowing and loss of light.

Massing of development in slimmer but taller towers rather than in shorter, wider buildings presents an opportunity to create open space between existing buildings, particularly when buildings step back from property lines and neighboring structures. For new buildings to fit in to the existing setting, a balance between higher and lower structures should be maintained.

To preserve openness between structures, separation between towers, both on a development site and between adjacent properties, should be provided. The specific separation should be determined based on height, relation and orientation to other tall structures, configuration of building mass and solar access to public spaces.

Taller buildings or “towers” in TL 1 should have relatively compact floor plates. The use of towers above a two-three story podium creates a varied building footprint and the perception of a smaller overall building mass. When the building’s mass is instead concentrated in lower buildings with larger floor plates, greater emphasis should be placed on open space and plazas to provide relief at the pedestrian level.

Design treatments used in the upper portion of a building can promote visual interest and variety in the Totem Center skyline. Treatments that sculpt the facades of a building, provide for variety in materials, texture, pattern or color, or provide a specific architectural rooftop element can contribute to the creation of a varied skyline.

## Building Modulation – Horizontal

### Issue

Horizontal building modulation is the horizontal articulation or division of larger building façades. The lower portion of a multi-story building should incorporate pedestrian-scale elements and a strong base. The top of the building should incorporate distinctive roof treatments. Elevations that are modulated with horizontal elements appear less massive than those with sheer, flat surfaces. Horizontal modulation is well suited to downtown areas and automobile-oriented streetscapes where the development of tall building masses is more likely.

**Horizontal building modulation may be used to reduce the perceived mass of a building and to provide continuity at the ground level of large building complexes. Building design should incorporate strong pedestrian-oriented elements at the ground level and distinctive roof treatments.**

### Discussion

A lively urban character uses a variety of architectural forms and materials that together create an integrated pattern of development with recurring architectural features. Horizontal awnings, balconies, and roof features should be incorporated into new development provided that their appearance varies through the use of color, materials, size, and location.

### Special Consideration for Neighborhood Business Districts

#### Issue

Because these districts are typically integrated into residential areas, the design should reflect the scale of neighborhood by avoiding long facades without visual relief.

#### Guideline

Facades over 120 feet in length should incorporate vertical definition including substantial modulation of the exterior wall carried through all floors above the ground floor combined with changes in color and material.

*brick banding, and window details.*

### Guideline

*Horizontal building modulation may be used to reduce the perceived mass of a building and to provide continuity at the ground level of large building complexes.*

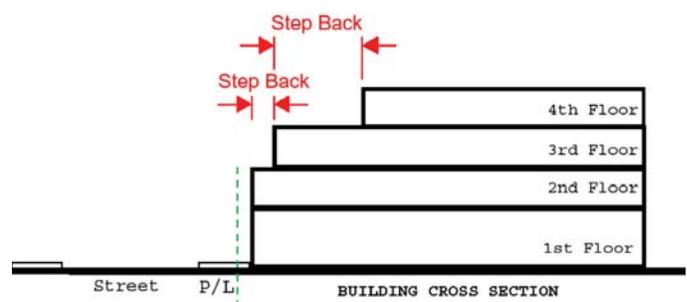
### Special Consideration for Downtown Kirkland

Large-scale developments, particularly east of the core area, should stress continuity in streetscape on the lower two floors. Setback facades and varied forms should be used above the second stories.

### Special Consideration for Building Massing in Central Business District 1 (CBD 1A & 1B) - Upper Story Step Backs

#### Issue

Taller buildings can negatively affect human scale at the street level and should be mitigated. Upper story step backs provide a way to reduce building massing for larger structures. An upper story building step back is the horizontal distance between a building façade and the building façade of the floor below.



By reducing mass at upper stories, visual focus is oriented towards the building base and the pedestrian experience. In addition, greater solar access may be provided at the street level due to the wider angle which results from the recessed upper stories



*Marina Heights*

Upper story step backs are appropriate in areas where taller buildings are allowed and imposing building facades at the sidewalk are intended to be avoided.

## Discussion

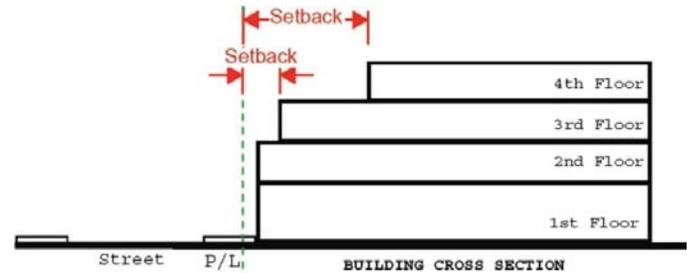
Design guidelines should address upper story step backs to improve the pedestrian experience and maintain human scale. When viewed from across the street, upper story step backs generally reduce perceived building massing and provide additional sunlight at the ground level. When viewed from the sidewalk immediately adjacent to the building, upper story step backs reduce the view of the upper stories and help maintain pedestrian scale by preventing large buildings from looming over the sidewalk.

Since the benefits of upper story step backs are primarily experienced from the public realm in front of buildings, the step backs should be located within a zone along the front property line.

Overly regimented building forms along front facades should be avoided to prevent undesirable building design. The arrangement of building step backs should create varied and attractive buildings consistent with the principles discussed in previous sections.

Upper story step backs also allow for additional eyes on the street in the form of decks and/or balconies. Upper story activities help improve the relationship of the building to the streetscape. Landscaping should also be incorporated at the upper stories to help soften building forms.

In order to quantify upper story step backs, measurement should be taken from the property line. Setback is the term used to describe the distance of a structure from the property line. By measuring from the pre-existing property line, setbacks provide for consistency in measurement and will account for projects where additional right-of-way is proposed or required along the property frontage for wider sidewalks and/or additional public open space.

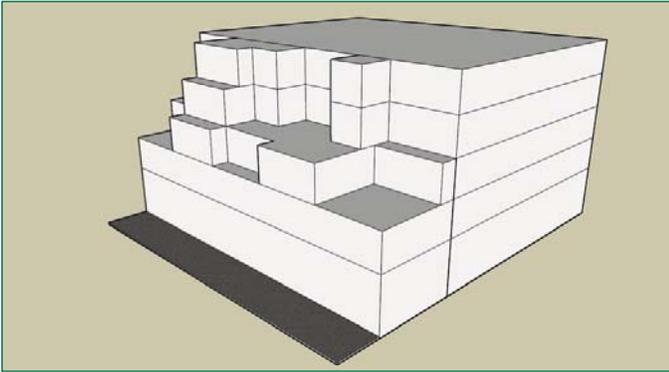


The required upper story setback should be allowed to be reduced if an equal amount of beneficial public open space is provided at the street level. A certain amount of building cantilevering over sidewalks may also be allowed if the pedestrian environment is not adversely affected.

The Kirkland Zoning Code establishes the requirements for upper story setbacks and provisions for allowing reductions to the required upper story setbacks in exchange for open space at the street level. The following guidelines are intended to provide the Design Review Board the tools to create varied and attractive buildings.

## Guidelines - Upper Story Setbacks

- ◆ ***Buildings above the second story (or third story where applicable in the Downtown Plan) should utilize upper story step backs to create receding building forms as building height increases, allow for additional solar access, and maintain human scale at the street level.***
- ◆ ***The final arrangement of building mass should be placed in context with existing and/or planned improvements, solar access, important street corners, and orientation with the public realm.***
- ◆ ***A rigid stair step or “wedding cake” approach to upper story step backs is not appropriate.***



*Varied step back approach*

- ◆ Decks and/or balconies should be designed so that they do not significantly increase the apparent mass of the building within the required upper story setback area.
- ◆ In addition to applying setbacks to upper stories, building facades should be well modulated to avoid blank walls and provide architectural interest.
- ◆ Along pedestrian oriented streets, upper story building facades should be stepped back to provide enough space for decks, balconies and other activities overlooking the street
- ◆ Landscaping on upper story terraces should be included where appropriate to soften building forms and provide visual interest.
- ◆ Continuous two or three story street walls should be avoided by incorporating vertical and horizontal modulations into the building form.
- ◆ Limited areas of vertical three, four, or five story walls can be used to create vertical punctuation at key facades. Special attention to maintain an activated streetscape is important in these areas.
- ◆ For properties on Park Lane which front multiple streets and upper story setbacks are proposed to be averaged, concentration of upper story building mass along Park Lane should be avoided.

### Guideline - Open Space at Street Level

Reductions to required upper story setbacks may be appropriate where an equal amount of beneficial public open space is created at the street level consistent with the following principles:

- ◆ Public open space should be open to the sky except where overhead weather protection is provided (e.g. canopies and awnings).
- ◆ The space should appear and function as public space rather than private space.

- ◆ Public open space should be activated with adjacent shops, outdoor dining, art, water features, and/or landscaping while still allowing enough room for pedestrian flow.
- ◆ A combination of lighting, paving, landscaping and seating should be utilized to enhance the pedestrian experience within the public open space.
- ◆ Where substantial open space “trade-offs” are proposed, site context should be the primary factor in the placement of the public open space (e.g. important corners, solar access.)

### Guideline - Building Cantilevering Over Sidewalks

Buildings may be allowed to cantilever over sidewalks if a sidewalk dedication and/or easement is required consistent with following guidelines:

- ◆ The total length of cantilevered portions of a building should be no more than 1/3rd of the entire length of the building façade. The cantilevered portions of a building should be spread out and not consolidated in a single area on the building façade.
- ◆ Unobstructed pedestrian flow should be maintained through the subject property to adjoining sidewalks.
- ◆ Space under the building cantilever should appear and function as part of the public realm.
- ◆ The sense of enclosure is minimized.

### Special Considerations for Neighborhood Business Districts

#### Issue

Where buildings are close the street in these neighborhood areas, vertical building massing can negatively affect human scale at the street level. Upper story step backs provide a way to reduce building massing. An upper story building step back is the horizontal distance between a building façade and the building façade of the floor below.

#### Guideline

Above the ground floor, buildings should utilize upper story step backs to create receding building forms as building height increases. Rather than a rigid stair step approach, varied step back depths and heights should be used to create well modulated facades and usable decks and balconies overlooking the street .

#### Issue

Within the South Rose Hill Neighborhood Plan, additional mitigation of scale impacts in called for.

#### Guideline

Building height, bulk, modulation, and roofline design should reflect the scale and character of adjoining single-family development.

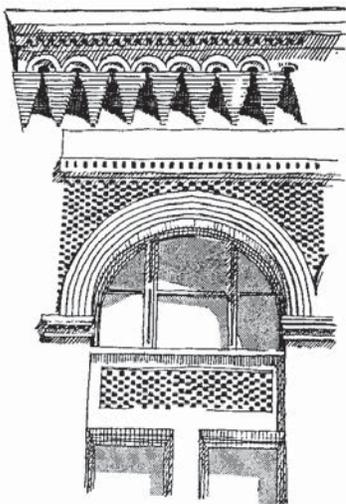
# Building Material Color and Detail

## Introduction

Many historic cities and towns owe much of their charm to a limited palette of building materials. One thinks of how the white clapboard houses of a New England village or the tile-roofed structures of an Italian hill town provide a more unified, consistent visual character. Today, there is a wide spectrum of building materials available, and modern towns such as Kirkland feature a variety of materials and colors. Architects have demonstrated that materials often considered unattractive, such as cinderblocks or metal siding, can be successfully used in attractive, high-quality buildings.

When buildings are seen from a distance, the most noticeable qualities are the overall form and color. If we take the typical building in Kirkland to be 100' wide and 35' tall, then we must be at least 200' away from the building for it to fit within our cone of vision so that we can perceive its overall shape. At that distance, windows, doors, and other major features are clearly visible.

However, as we approach the building and get within 60' to 80' from the building (approximately the distance across a typical downtown street), we notice not so much the building's overall form as its individual elements. When we get still closer, the most important aspects of a building are its design details, texture of materials, quality of its finishes, and small, decorative elements. In a pedestrian-oriented business district, it is essential that buildings and their contents be attractive up close.



Therefore, these design guidelines are intended to allow a variety of materials and colors, but direct the use of certain materials so that their application does not significantly detract from design consistency or quality. Most of the regulations in the Zoning Code deal with the application of specific materials such as metal siding and cinderblocks so that their potentially negative characteristics are minimized. In addition, the guidelines include guidelines and regulations that require all buildings to incorporate design details and small-scale elements into their facades.

## Ornament and Applied Art

### Issue

Ornament and applied art add quality, visual interest, and a sense of human scale to the built environment. It is necessary to understand the place and appropriateness of ornament in order to maintain a cohesive and integrated urban setting.

### Discussion

Ornament and applied art can be used to emphasize the edges and transition between public and private space, and between walls to ground, roof to sky, and architectural features to adjacent elements. Ornament may consist of raised surfaces, painted surfaces, ornamental or textured banding, changing of materials, or lighting. Therefore, buildings should incorporate art features that emphasize architectural elements and connections. Ornament should also maintain a cohesive relationship to its setting, emphasizing its connection to the surrounding space.

### Guideline

***Ornament and applied art should be integrated with the structures and the site environment and not haphazardly applied. Significant architectural features should not be hidden, nor should the urban context be overshadowed. Emphasis should be placed on highlighting building features such as doors, windows, eaves, and on materials such as wood siding and ornamental masonry. Ornament may take the form of traditional or contemporary elements. Original artwork or hand-crafted details should be considered in special areas.***

## Special Considerations for the Market Street Corridor

Emphasis on building features such as doors, windows, cornice treatment, bricks and ornamental masonry should be taken into consideration when designing new or remodeled buildings in the historic district. These features should be in keeping with the building materials, colors and details of the existing historic buildings.

## Color

### Issue

Color bolsters a sense of place and community identity (e.g., white New England villages, adobe-colored New Mexico towns, limestone Cotswold villages). Kirkland should consider emphasizing the existing color scheme and developing a unified design identity.

### Discussion

A variety of colors should be used in Kirkland. By no means should design be limited by overly-restrictive guidelines dictating color use. Based on Kirkland's existing color scheme, the following general guidelines can prevent garish, incongruous colors from being inappropriately applied or juxtaposed to more subdued earth tones and colors.

- ◆ Where appropriate, use the natural colors of materials such as brick, stone, tile, and stained wood (painted wood is acceptable).
- ◆ Use only high-quality coatings for concrete.
- ◆ Emphasize earth tones or subdued colors such as barn red and blue-gray for building walls and large surfaces.
- ◆ Reserve bright colors for trim or accents.
- ◆ Emphasize dark, saturated colors for awnings, and avoid garish and light colors that show dirt.
- ◆ Avoid highly-tinted or mirrored glass (except stained-glass windows).
- ◆ Consider the color of neighboring buildings when selecting colors for new buildings.

### Guideline

***Color schemes should adhere to the guidelines enumerated above. The use of a range of colors compatible within a coordinated color scheme should be encouraged.***

## Street Corners

### Issue

Street corners provide special opportunities for visual punctuation and an enhanced pedestrian environment. Buildings on corner sites should incorporate architectural design elements that create visual interest for the pedestrian and provide a sense of human proportion and scale.

### Discussion

Corners are crossroads and provide places of heightened pedestrian activity. Rob Krier notes that: "The corner of a building is one of the most important zones and is mainly concerned with the mediation of two facades." Corners may be accentuated by towers and corner building entrances.



### Guideline

***Buildings should be designed to architecturally enhance building corners.***

### Special Consideration for Downtown Kirkland

Special attention should be paid to both the design and detailing of new buildings on corner sites in the pedestrian oriented design districts. Existing buildings could incorporate some of these elements (human-scale and visual punctuation) through the use of such elements as awnings and well-designed signs at the corner.

Downtown Kirkland has several "T" intersections, and the building located at the terminus of the street view corridor presents a high-visibility opportunity for special architectural treatment.

The corner of Central Way and Third Street marks a prominent gateway to the core area as well as the Downtown Transit Center and deserves special design emphasis.

## Signs

### Issues

Kirkland's Zoning Code regulates signs throughout the city in order to create a high-quality urban environment. Automobile-oriented signs typically found on commercial strips can be overpowering and obtrusive. Pedestrian signs are smaller and closer to viewers; thus, creative, well-crafted signs are more cost effective than large signs mounted high on poles.

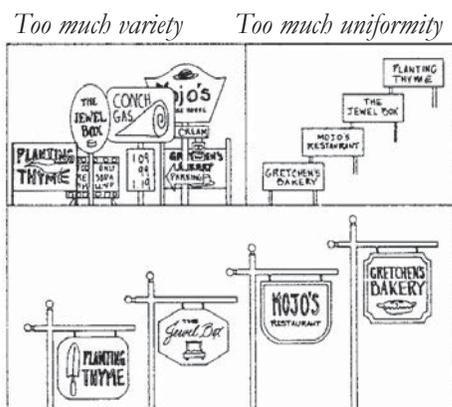
Signs should be an integral part of a building's facade. The location, architectural style, and mounting of signs should conform with a building's architecture and not cover up or conflict with its prominent architectural features. A sign's design and mounting should be appropriate for the setting.

### Discussion

Pedestrian-oriented signs are most effective when located within 15' of the ground plane. Three-inch-high letters can be read at 120' and 6" letters read at 300'. Large lettering is not necessary. The signs should be aligned to people on sidewalks and not automobile drivers. "Blade" signs or single signs hanging below canopies or small signs located on canopies or awnings are effective.

Signs with quality graphics and a high level of craftsmanship are important in attracting customers. Sculpted signs and signs that incorporate artwork add interest. Signs with front lighting and down lighting (but not internal lighting) are recommended. Neon signs are appropriate when integrated with the building's architecture.

Generic, internally-lit "can" signs that are meant to be set anywhere are not appropriate. Ground-mounted signs should feature a substantial base and be integrated with the landscaping and other site features. Mounting supports should reflect the materials and design character of the building or site elements or both.



*Though unified by common design elements, signs can still express the individual character of businesses.*

### Guidelines

- ◆ All signs should be building-mounted or below 12' in height if ground mounted. Maximum height is measured from the top of the sign to the ground plane.
- ◆ No off-premises commercial signs, except public directional signs, should be permitted. No billboards should be permitted.
- ◆ Signs for individual parking stalls should be discouraged. If necessary, they should not be higher than necessary to be seen above bumpers. Parking lot signs should be limited to one sign per entrance and should not extend more than 12' above the ground.
- ◆ Neon signs, sculptural signs, and signs incorporating artwork are encouraged.
- ◆ Signs that are integrated with a building's architecture are encouraged.
- ◆ Shingle signs and blade signs hung from canopies or from building facades are encouraged.
- ◆ Traditional signs such as barber poles are encouraged.

### Special Considerations for Downtown Kirkland

- ◆ The Downtown Plan's mandate for high-quality development should also be reflected in sign design.
- ◆ No internally lit plastic-faced or can signs should be permitted.
- ◆ All signs in the downtown should be pedestrian-oriented. Master-planned sites such as Parkplace may also include signs oriented to automobile traffic for the whole complex.

### Special Considerations for Totem Center

- ◆ Signs within the TL2 should be coordinated through a sign package for the entire property.

### Special Considerations for the Market Street Corridor

Electrical signs are not allowed along the Market Street Corridor. Signs within the historic district should reflect the historic nature of the buildings in the area.

# Natural Features

## Introduction

### General

An important aspect of a pedestrian-oriented business district is its physical setting. Natural features of a place are key to residents' and visitors' perception. This section lays out guidelines which serve to merge the design of structures and places with the natural environment. It discusses concepts behind new landscaping as well as the maintenance and protection of existing natural features.

### *Special Considerations for Downtown Kirkland*

A primary goal stated in the Downtown Plan's Vision Statement is to "clarify Downtown's natural physical setting." Besides its excellent waterfront, Downtown Kirkland's most important natural feature is its bowl-shaped topography which provides views down from the heights and views from the downtown of the wooded hillsides surrounding the district. The valley topography also helps to define the downtown's edges and facilitates the transition from largely commercial activities in the valley floor to the mostly residential areas in the uplands. Although Peter Kirk Park is a man-made open space, it too provides a naturalizing function.

### *Special Considerations for Juanita Business District*

The underlying goal of redevelopment in the business district is to create a neighborhood-scale, pedestrian district which takes advantage of the amenities offered by Juanita Bay.

### *Special Considerations for Totem Center*

An important goal in the Totem Lake Neighborhood Plan is to establish a "greenway" extending in an east/west direction across the neighborhood. Portions of the greenway follow Totem Lake Boulevard, along the western boundary of TL 2. Properties abutting the designated greenbelt should be landscaped with materials that complement the natural areas of the greenway where possible.

## Visual Quality of Landscapes

### Issue

The relationship between landscaping and architecture is symbiotic; plant materials add to a building's richness, while the building points to the architectural qualities of the landscaping.

### Discussion

Foliage can soften the hard edges and improve the visual quality of the urban environment. Landscaping treatment in the urban environment can be categorized as a *pedestrian/ auto, pedestrian, or building landscape*.



***The Pedestrian/Auto Landscape*** applies to where the pedestrian and auto are in close proximity. Raised planting strips can be used to protect the pedestrian from high-speed and high-volume traffic. Street trees help create a hospitable environment for both the pedestrian and the driver by reducing scale, providing shade and seasonal variety, and mitigating noise impacts.

***The Pedestrian Landscape*** offers variety at the ground level through the use of shrubs, ground cover, and trees. Pedestrian circulation, complete with entry and resting points, should be emphasized. If used effectively, plant materials can give the pedestrian visual cues for moving through the urban environment. Plant materials that provide variety in texture, color, fragrance, and shape are especially desirable.

***The Building Landscape.*** Landscaping around urban buildings – particularly buildings with blank walls – can reduce scale and add diversity through pattern, color, and form.

Examples of how landscaping is used to soften and enhance the visual quality of the urban environment include:

- ◆ Dense screening of parking lots;
- ◆ Tall cylindrical trees to mark an entry;



- ◆ Continuous street tree plantings to protect pedestrians;
- ◆ Several clusters of dense trees along long building facades;
- ◆ Cluster plantings at focal points;
- ◆ Parking with trees and shrubs planted internally as well as on the perimeter.

## Guidelines

*The placement and amount of landscaping for new and existing development should be mandated through design standards. Special consideration should be given to the purpose and context of the proposed landscaping. The pedestrian/auto landscape requires strong plantings of a structural nature to act as buffers or screens.*

*The pedestrian landscape should emphasize the subtle characteristics of the plant materials. The building landscape should use landscaping that complements the building's favorable qualities and screens its faults.*

### *Special Consideration for North Rose Hill Business District*

A dense landscape buffer should be utilized to provide a transition separating commercial uses from adjoining single family or multi-family residential uses.

### *Special Consideration for Totem Center*

Within TL 1, special landscaping elements such as gateways, arches, fountains and sculptures should be incorporated, in order to create a lively streetscape and provide visual interest along the street edge. Where possible, existing mature landscaping should be retained and incorporated into new development to soften the impact of increased site coverage and preserve the green character of the area.

## Protection and Enhancement of Wooded Slopes

### Issue

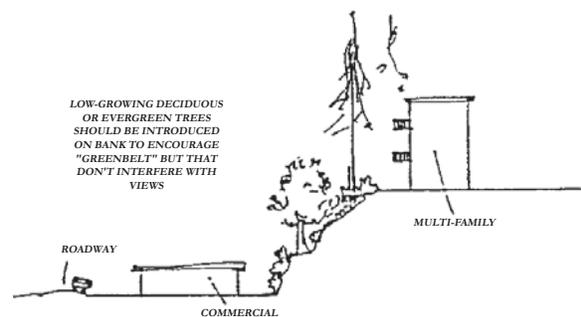
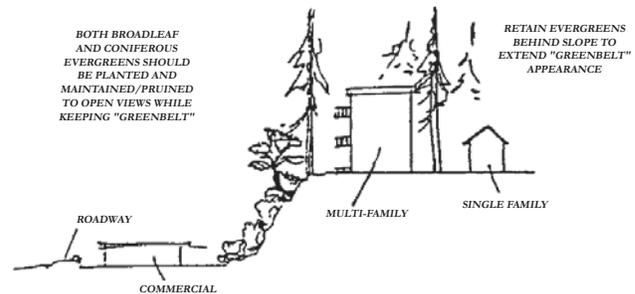
Topography provides opportunities for natural screening that maintains views.

### Discussion

New plantings on wooded slopes should be selected for their slender, open growth pattern. Limbing-up and thinning-out branches should also be allowed to maintain views while keeping the character of the wooded hillsides. Weed species should be removed and replaced with appropriate native species. Wooded slopes can:

- ◆ Reduce visual impacts of the urban environment.

- ◆ Separate uses by providing a transition zone.
- ◆ Mitigate urban noise and air pollution for upland uses.
- ◆ Provide wildlife habitat.



## Guidelines

*Vegetation on slopes should be preserved and maintained as a buffer using native vegetation wherever possible.*

*New multifamily and single-family residential developments on slopes should be required to retain about 30 percent of the site in wooded open space and inventoried significant trees. Tree removal or enhancement can be determined by the use and site design.*

*Property owners of lowlands should be sensitive to upland uses and enhance hillsides to maintain existing views. Deciduous trees should be restricted to small varieties; coniferous evergreens should be thinned-out or limbed-up to allow for views from adjoining properties.*

*In developments above view slopes, coniferous evergreens should be incorporated into the site back from the slope to give continuity with the wooded slope. The back sides of commercial lots at the base of hillsides should be planted to screen upland properties from unsightly views of rooftops.*

### ***Special Consideration for Downtown Kirkland***

Using and enhancing existing wooded slopes is especially important to Kirkland's natural setting. The hillsides surrounding Downtown Kirkland can provide a "ring of green." As vegetation ascends the slope it provides a "greenbelt" effect. The proper maintenance or enhancement of such slopes need not disrupt view corridors of upland properties.

### ***Special Consideration for Juanita Business District***

The views of wooded hillsides surrounding the Juanita Business District are a local asset that can be used to upgrade the area's visual impact.

## **Height Measurement on Hillsides**

### **Issue**

Maintaining views and enhancing natural land forms is important to the design character of Kirkland. The scale relationships of built forms to their terrain should minimize visual barriers to views and lessen the impact on surrounding neighborhoods. In order to promote responsible design, building height restrictions should permit a development envelope that conforms to the terrain. Terracing, the stepping down of horizontal elements, is an effective way to develop hillsides and maintain views.

### **Discussion**

The visual character of a landscape should be reflected in the buildings. Buildings that do not conform to steep inclines detract from the natural features of the site and should be avoided. In contrast, buildings that use the terrain as an opportunity for variation in the built form easily fit into their setting without disruption. Terracing a building to roughly parallel the slope of a site will create a building envelope that follows the contour of its property. Terraced roof decks, modulated roofs, and sloped roofs can carry out this objective.



*Terraced buildings reflect the hillside topography ringing Kirkland's Downtown.*

### **Guideline**

***The top of the building should roughly follow the slope of the existing terrain.***

## **Views of Water**

### **Issue**

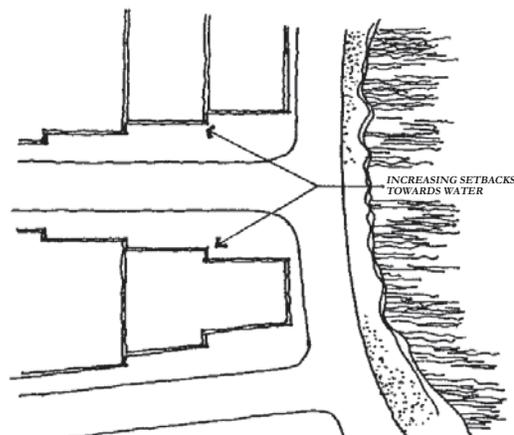
Views of Lake Washington give Kirkland its sense of place within the regional context. The waterfront remains an exceptional resource that should be better linked to nearby districts. A water view is a recurring reminder of the direction, function, and origin of Kirkland.

### **Discussion**

Views may be considered in three ways. The *distant panorama* may be seen from one-quarter to more than one mile away. Development has eliminated most of Kirkland's panoramic views; remaining views should be protected. *View corridors* are places where an avenue between buildings creates a slotted visual path allowing a glimpse of the water beyond. *Proximity views* are those adjacent to and within one block away from the waterfront; they extend the waterfront's character. Each type of view is critical to Kirkland's urban design character.

View corridors and panoramic views from higher ground can be protected by height restrictions and limitations on rooftop clutter. Existing structures in some areas block views of the Lake. With renovation of existing structures, opening up of views should be encouraged. New development should respect the existing view corridors.

Proximity views require much larger fields of vision, therefore, development should remain a comfortable distance from the shore and be set back along view corridors. This will allow views of the water to widen from increasingly closer distances and will eliminate an abrupt change between development and shoreline.



## Guideline

*Existing views should be maintained. This can be accomplished by widening setbacks as development approaches the water. Buildings should step down hillsides. Buildings and rooftop appurtenances should be placed perpendicular to the water in order to safeguard views.*

### ***Special Consideration for Juanita Business District***

View corridors to the Lake should be explored through new development in the business district. Existing residential views and view opportunities through Juanita Beach Park and down public streets should be preserved.

## Culverted Creeks

### Issue

Often stream beds fall victim to progress and their stream banks are reduced to a drain pipe. One way to further the objective of clarifying the natural physical setting is to reopen stream beds wherever possible.

### Guideline

*Opportunities should be sought to restore portions of culverted creeks to their natural state.*

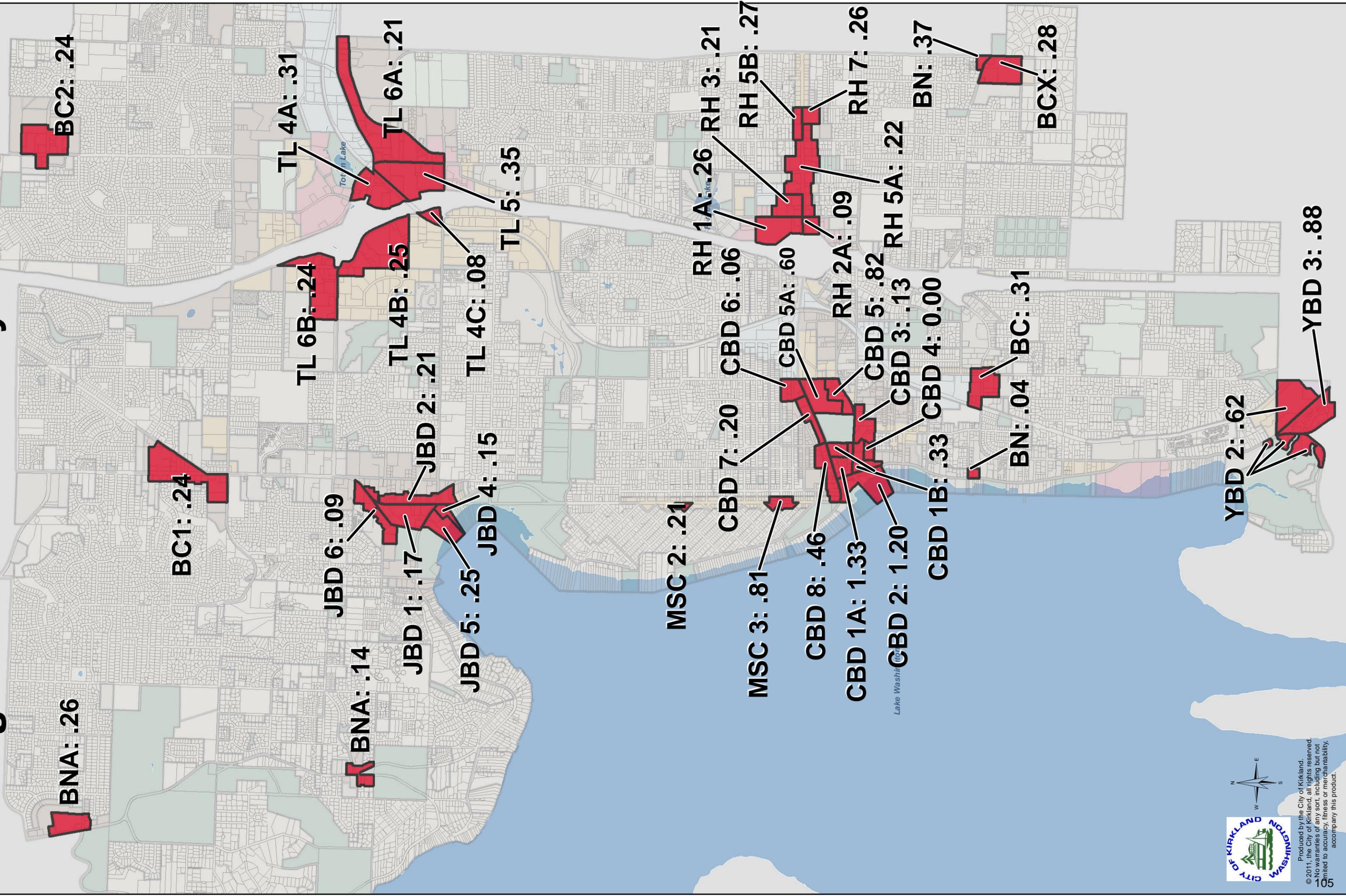
### ***Special Consideration for Downtown Kirkland***

A former stream bed, now enclosed in culverts, flows through the center of downtown from 6th Street, through Peter Kirk Park, just south of Central Way and into Marina Park. A restored stream bed could be incorporated in the parks and other public sites, and possibly on private property.

### ***Special Considerations for Totem Center***

One channel of the Totem Lake tributary extends along I-405, west of Totem Lake Boulevard in a culvert to Totem Lake. If it is feasible, restoration of this stream bed could be incorporated into the “greenway” design developed for this segment of Totem Lake Boulevard. Another tributary of Juanita Creek runs across the northwest section of Totem Center, with portions in a culvert and other portions remaining in an open stream bed. Redevelopment of these properties could include restoration of the culverted portions of the stream as an amenity provided on site.

# Existing Floor Area Ratio by Zone





**From:** [Jack Arndt](#)  
**To:** [Jay Arnold](#); [Jon Pascal](#); [Mike Miller](#); [Byron Katsuyama](#); [Glenn Peterson](#); [C Ray Allshouse](#); [Andrew Held](#); [Robin Jenkinson](#); [Eric Shields](#); [Jeremy McMahan](#)  
**Cc:** [uwkkg@aol.com](mailto:uwkkg@aol.com)  
**Subject:** May 31st Council Meeting  
**Date:** Wednesday, May 30, 2012 8:23:39 PM

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Planning Commission/Department

We would like to re-forced that you are the experts with your knowledge and expertise to come forward with the correct density level to fit into the neighborhood plan surrounding Lake Washington Blvd/10th Street.

It was very clear during the last council meeting that the opinions of McBride, Marchione, Sweet, and Walen of our city council did not have the best interest of Kirkland citizens in mind when they made their recommendation on the BN zoning. They were only focused to the interests of the developer in order to avoid a potential lawsuit by the developer. They also did not understand their role in letting you, the experts determine from a planning commission/department what the area can support.

Council Members Steroff, Nixon, and Asher supported you on moving forward with a residential zoning plan.

We trust you will come forward with your leadership and vision based on facts not opinions in what the area can support in density caps for all the concerns that have been voiced during the past 6 months. We encourage you not to back down to 4 council members and do what is right for the future of Kirkland.

Sincerely,

Jack & Christy Arndt  
6424 Lake Washington Blvd.

**From:** [Mark Taylor](#)  
**To:** [Joan McBride](#); [Penny Sweet](#); [Doreen Marchione](#); [Amy Walen](#)  
**Cc:** [Karen Levinson](#); [Chuck Pilcher](#); [Toby Nixon](#); [Bob Sternoff](#); [Dave Asher](#); [Robin Jenkinson](#); [Kurt Triplett](#); [Eric Shields](#); [Jeremy McMahan](#); [Jay Arnold](#); [Byron Katsuyama](#); [Glenn Peterson](#); [Jon Pascal](#); [Andrew Held](#); [C Ray Allshouse](#); [Mike Miller](#); [kervin@seattletimes.com](#)  
**Subject:** RE: BN zoning on Lake Washington Blvd  
**Date:** Saturday, May 26, 2012 10:44:51 AM  
**Importance:** High

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To Kirkland City Councilmembers Marchione, McBride, Sweet and Walen:

It has been 11 days since the city council meeting in which you voted in favor of turning the Lake Street and 10<sup>th</sup> parcel into a designated Neighborhood Center without density cap. You may think that you have successfully “kicked the can down the road,” but today’s Seattle Times’ article by Keith Ervin certainly belies that notion.

For many months now dozens of Kirkland residents have personally expressed their desire to maintain current neighborhood density limits, and hundreds have signed an online petition to that effect. Strong arguments with historical basis supporting this position have been offered and Mr. Dargey’s team was made aware of the legal history causing previous developers to not develop this property. But, with your respective votes you have chosen to effectively “flip-off” the residents of the Lakeview and Moss Bay neighborhoods in favor of your personal wishes for increased density in Kirkland.

What is worse is the flimsy rationale each of you offered in support of your vote. Your explanations ranged from incoherent (Ms. Walen) to ambivalent (Ms. Sweet) to parochial (Ms. McBride) to nonexistent (Ms. Marchione). If this is the best you can do to justify a vote of this significance, I can only conclude that all of you are intellectually incapable of serving on any city council this side of Mayberry.

The Kirkland City Council is supposed to be accountable to the residents of Kirkland. You know—the people who actually live here and whom you are actually supposed to represent. And, no, Ms. McBride, it is *not* the responsibility of the City of Kirkland to “have to house more people.”

The City of Kirkland deserves better than this.

Toby Nixon proposed a compromise that all of you chose to ignore during the meeting. His proposal was to allow a density cap that exceeds that of surrounding neighborhoods by an amount that does not impact their overall character. But, instead of pursuing such a constructive approach you seem to be stuck on your personal agendas for growth. I strongly urge the Kirkland City Council to pursue Mr. Nixon’s approach to achieving a compromise that is acceptable to all parties.

Absent such a constructive approach, each of you can be assured of several hundred votes immediately opposing you in the next election should you choose to remain in office. It is the least we residents of Lakeview and Moss Bay can do to prevent similar bizarre decisions being made by those elected to represent us.

Representative government—what a concept!

Mark S. Taylor  
 6202 Lake Washington Blvd NE  
 Kirkland, WA 98033

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From: mark.s.taylor@hotmail.com  
 To: rjenkinson@kirklandwa.gov; ktriplett@kirklandwa.gov; eshields@kirklandwa.gov; jmcman@kirklandwa.gov; jmcbride@kirklandwa.gov; psweet@kirklandwa.gov; dmarchio@kirklandwa.gov; awalen@kirklandwa.gov; dasher@kirklandwa.gov; bsternoff@kirklandwa.gov; tnixon@kirklandwa.gov; jarnold@kirklandwa.gov; bkatsuyama@kirklandwa.gov; gpeterson@kirklandwa.gov; jpascal@kirklandwa.gov; aheld@kirklandwa.gov; callshouse@kirklandwa.gov; mmiller@kirklandwa.gov

CC: uwkkg@aol.com; chuck@bourlandweb.com  
Subject: BN zoning on Lake Washington Blvd  
Date: Tue, 17 Apr 2012 14:13:33 -0700

Members of the Kirkland City Council,

Please add our names to the list of Kirkland residents concerned about the neighborhood business ("BN") zoning under consideration for the parcel located on Lake Washington Blvd.

We bought our property on Lake Washington Blvd in 2010 largely because we were confident in the ability of the Kirkland City Council to manage the growth of the city in a way that would benefit the community as a whole. We have previously resided in communities that did not manage growth well, and it significantly degraded both the quality of life and the economic value of owning property. As relative newcomers to Kirkland, we are not wholly familiar with the history leading to this point, but several principles seem important.

1) The interests of the community at large are far more important than those of any single developer. While we believe the community is enhanced by the ability of developers to do their thing, there must be reasonable guidelines governing development that protect the interests of those already residing in the vicinity of a proposed development. The number of residents wearing red at council meetings and expressing concern about this issue should be a clear indication of public opinion.

2) A "neighborhood business" designation should mean exactly that. Simply put, a neighborhood business is a business that serves the needs of people living in the neighborhood. A coffee shop, a dry cleaner, or a small restaurant are all examples of neighborhood businesses. Professional services such as a doctor's office, while potentially serving the community, are not really neighborhood businesses because the majority of their clientele and staff reside elsewhere.

3) A BN-zoned parcel should complement its surroundings, especially when those surroundings are zoned residential. The lack of a density cap on the BN zoning designation is especially problematic, since it introduces the possibility of a significant mismatch between a BN-zoned parcel and its surrounding residential zones. At a minimum, a BN-zoned parcel should be required to have a density cap that does not exceed those of its surroundings.

4) The last thing that Lake Washington Blvd needs is more traffic. It is inconceivable that anyone could conclude that adding a hundred-plus housing units with three hundred-plus parking spaces would not significantly worsen an already bad traffic situation. This is only common sense.

5) The buck stops with you, the city council. You are the people ultimately accountable to the citizens of Kirkland. You should not place difficult decisions on the shoulders of planning commissions or city employees. You were elected to make the hard decisions. Now is your time.

It seems that multiple errors have been made over a number of years with respect to Kirkland zoning. It is never too late to reverse a bad decision. Please do so.

Respectfully,

Mark & Betty Taylor  
6202 Lake Washington Blvd NE  
Kirkland  
206-979-8740 (cell phone)

**From:** [Chuck Pilcher](#)  
**To:** [Jay Arnold](#); [Jon Pascal](#); [Mike Miller](#); [Byron Katsuyama](#); [Glenn Peterson](#); [C Ray Allhouse](#); [Andrew Held](#); [Robin Jenkinson](#); [Kurt Triplett](#); [Eric Shields](#); [Jeremy McMahan](#)  
**Subject:** You CAN correct mistakes  
**Date:** Thursday, May 31, 2012 9:56:47 AM

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Dear Planning Folks:

I noticed at least two items in the Amendments to the Zoning Code that are up for discussion currently (see below). What this tells me is that the Planning Department

- can make a mistake
- can acknowledge that mistake
- can correct that mistake.

Please, do the same with the BN zoning issue. It's just a "mistake," plain and simple, and was "inadvertently missed," just like the examples below. (**Bold** type is my emphasis.)

The developer may not like it, but among the 500+ self-identified citizens opposed to what he plans, I know that at least 5 are local developers who say his project is just plain wrong. So any action you take to mitigate this project will be viewed positively by developers, because you have saved one of the great assets of Kirkland: the boulevard. Those developers who have built in this neighborhood, like Mr. Sternoff himself, correctly see this as a property rights issue for those who have already invested in Kirkland property expecting the City not to destroy it through a stupid oversight.

We're all human and imperfect. Let's just do the right thing, acknowledge the mistake, fix it, and move on with our lives.

Chuck Pilcher  
[chuck@bourlandweb.com](mailto:chuck@bourlandweb.com)  
 206-915-8593

**Single Family Residential RSA 4 and 6 Zones Minimum Lot Size KZC Chapter 18 Section 18.10.010**

Purpose: After the approval of the Green Code Project amendments (O-4351) on April 3, 2012, a **mistake** was discovered in Special Regulation 2 that would allow minimum lot sizes in the RSA 4 and RSA 6 Zones to be smaller than was intended. The proposed change will reverse the lot size to that which was in effect prior to the green code amendment.

**Totem Lake 9B KZC Chapter 55 Section 55.64.010**

Purpose: Add the density limitation of 5,000 square feet per dwelling unit for this zone, which was **inadvertently missed** with the adoption of Ordinance 4158 in 2008. This ordinance implemented the Gordon Hart private amendment request through codification of the TL 9A and 9B zones and established a 5,000 sq. ft. minimum lot size for the TL 9B zone. This minimum lot size is equivalent to the density being codified. In all multifamily zones, a special regulation expresses density as minimum lot area per dwelling unit and this amendment does just that.

**From:** [Phillips Michael](#)  
**To:** [Jay Arnold](#); [Jon Pascal](#); [Mike Miller](#); [Byron Katsuyama](#); [Glenn Peterson](#); [C Ray Allshouse](#); [Andrew Held](#); [Robin Jenkinson](#); [Kurt Triplett](#); [Eric Shields](#); [Jeremy McMahan](#)  
**Cc:** [Uwkkq@aol.com](#); [Phillips Chantelle](#); [Knight Ron](#)  
**Subject:** Planning for population density  
**Date:** Wednesday, May 30, 2012 10:57:57 AM

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My name is Michael J Phillips. My family lives at 905 Lake Street South, Unit 103, Kirkland, WA. I am on the Home Owners Association of my condo complex.

I am an immigrant to the US, now a US citizen and have been for 15 years. I wish to address the issue of population density. I immigrated to Washington State which has a population density of 102 persons per square mile from a country (England) with a population density of 1,000 persons per square mile. Ten times higher. England is a very livable country with significantly less urban and suburban sprawl than the US. How did the English achieve this? By very strict zoning rules, green zones, requiring developers to develop structures in keeping with their surroundings, and the provision of infrastructure and mass transit to prevent the choking of roads with traffic.

Our mayor has said she is committed to density. On its own, this is a totally meaningless statement. Unless we are in favor of compulsory birth control or a "one child policy", we all have to be committed to density. Saying you are committed to density without any qualifying statement is like saying that you are committed to happiness. A blinding glimpse of the obvious, but not really useful. Singapore has a density per square mile of 18,000, is very livable and works like clockwork. Other urban areas with high population density do not. Some examples are: Mexico City, New Delhi, Cairo. There are many more in both poor and more wealthy countries. The difference between the livable Singapore, or England, and the polluted and appalling living conditions of the examples I used is all about planning, zoning, and infrastructure. Finally, all the countries which are livable with high population density, approach planning from the standpoint of what is most culturally suitable with the surrounding area, what is environmentally appropriate, and what can be supported by existing infrastructure. None of them start with the dubious premise (argued by some council members at the Council meeting) that if you buy a piece of land you have some kind of "manifest destiny" type of right to put whatever you want on it. That idea is so ridiculous that it barely deserves comment. Actually, that is how things work in cities which quickly become polluted, overcrowded and unlivable.

Kirkland is a small town, culturally quite distinct from its neighbors. For the majority of the year it becomes choked with traffic. It is a prime example of an urban area which now needs very careful planning. Our new buildings need to reflect the neighborhoods in which they are built. And above all, we must ensure that the infrastructure we have will support new population influx and business population. This is not "anti-density" it is "lets plan responsibly for density". Please lets make it obvious to any visitor to Kirkland that we have allowed our city to grow carefully, and in a way which retains its culture and which respects all the stakeholders involved as well as the environment. In my opinion, our present direction makes it more likely that our fiduciaries of the citizens' trust will be seen as creating an example of "planning blight" and a case study on how not to plan a livable community.

Sincerely,

Michael and Chantelle Phillips

May 30, 2012

Jay Arnold, Chair  
Planning Commissioners  
City of Kirkland  
123 Fifth Ave.  
Kirkland, WA 98033

Re: *Potential Amendment Residential Market Designation in Comprehensive Plan*

Dear Chairman Arnold and Planning Commissioners:

I write again on behalf of Support The Ordinances and Plan (“STOP”) regarding your ongoing efforts to review potential changes to either the City’s zoning code or Comprehensive plan for the site of the proposed Potola Village.

The purpose of this letter is to clarify, we hope, the process that you are following after the City’s Council’s May 15, 2012, split vote recommending that you “consider” and a potential amendment to the Comprehensive Plan changing the Comprehensive Plan designation of Residential Market to that of Neighborhood Center.

As you know, under GMA and City Code the consideration of a Comprehensive Plan amendment is significantly different than the consideration of a change to zoning text. In particular, for a zoning code amendment like you were originally considering, the City was allowed to follow an abbreviated review process. KZC Chapter 161 allows abbreviated process where the code amendment is being done to correct an inconsistency to ensure that the Zoning Code is consistent with the Comprehensive Plan. KZC 161.25. Because of the inconsistency between the Residential Market Comprehensive Plan designation and the existing BN zoning, the abbreviated process was allowed.

Now that the City Council appears to be recommending that you consider a Comprehensive Plan amendment, the process you follow is quite different. First and foremost, under the Growth Management Act, the City is only allowed to consider Comprehensive Plan amendments once per year. (See also Comprehensive Plan page III-4 – attached). Further, KZC 140.10 requires that the City’s review must be conducted under the thoughtful annual review process set out in KZC Chapter 160. While the City Council is certainly free to make initial recommendations to you, it is important to note that you, the Planning Commission, are charged with thorough study and review then final recommendation for any proposed Amendments. This includes issuance and posting of official public notice (KZC 160.40), review of a staff report (KZC 160.45), conduct of a public hearing (KZC 160.55), acceptance of public comment (KZC 160.70), a

Jay Arnold, Chair  
Kirkland Planning Commission  
May 30, 2012  
Page 2

determination of whether the proposed amendment meets the decisional criteria in KZC 160.60, KZC 140.25, and KZC 140.30, followed only then by your official action voting, making a recommendation, and then transmitting that recommendation to the City Council. (KZC 160.80-85).

Your recommendation can certainly include your own additions, including for example, a significant density cap (as it appears the City Council unanimously supports). Your recommendation can also certainly be against adoption of an amendment, particularly, where the Council has not studied the matter and is currently not finding consensus.

STOP looks forward to working with you through this process. This City has devoted years to studying and adopting the Residential Market Comprehensive Plan designation for this unique property. Any amendments to this designation must be equally deliberate and ensure that the outcome is compatible with the adjacent land uses and surrounding neighborhood and adequately addresses impacts to roads and public transportation.

Please do not hesitate to contact me if you have any questions.

Very truly yours,

GENDLER & MANN, LLP



David S. Mann

Enclosure

cc: Kurt Triplett  
Eric Shields  
Robin Jenkinson  
STOP

## III. GENERAL

*Policy GP-2.4: Encourage active citizen participation in the planning and design of public facilities, particularly in affected neighborhoods, communities, and business areas.*

Many of the decisions on public facilities have significant issues that need to be addressed such as access, safety, environmental concerns, neighborhood character, and economic impacts. In the planning and design of public facilities it is important to have a process that facilitates public involvement by all parties.

### D. PLAN AMENDMENT

#### *Amendment Process*

The Growth Management Act specifies that the Comprehensive Plan and Land Use Plan Map can only be amended once a year except in emergencies. Section 365-195-630 of the Washington Administrative Code states that all amendments in any year be considered concurrently so that the cumulative effect of the various proposals can be ascertained. The intent of this requirement is to ensure that piecemeal or individual amendments do not erode the integrity of the plan and are integrated and consistent with the balance of the Plan. The Zoning Code contains the process for an emergency amendment.

Amendments are initiated in two ways: by the City or by a citizen or community group. A formal process to amend the plan, consistent with the requirements of the Growth Management Act, has been established. The process for the City-initiated and citizen-initiated amendments include opportunities for public involvement and community participation. The Kirkland Planning Commission takes the lead role as the City's citizen representative body responsible for conducting the public hearing and transmitting a recommendation to the City Council. The Houghton Community Council, Kirkland Transportation Commission and Park Board also take public comment for amendment proposals within their jurisdiction and transmit recommendations to the Planning Commission and to the

City Council. The Zoning Code contains the process for reviewing and deciding upon a proposal to amend the Comprehensive Plan.

For citizen-initiated proposals, the City has a formal application process and an established deadline for submitting an application to be considered in the next round of City-initiated plan amendments. The City has a two-step process for citizen-initiated plan amendments: first a threshold determination and then a study and final decision on the proposed amendments. For City-initiated plan amendments, the City has only one step: the study and final decision on the proposed amendments. The Zoning Code contains the criteria for evaluating a proposal to amend the Comprehensive Plan.

The City reviews the Comprehensive Plan on an annual basis to update the Transportation and Capital Facilities Elements or any other element for any needed changes, to respond to amendments to the Growth Management Act and other State legislation or Countywide planning policies, to correct any inconsistencies in the Plan and with the development regulations and any recently adopted functional plan, and to update general information. The City establishes a schedule for amending the neighborhood plans and reviews the schedule each year as part of the Planning Department's work program. In addition, the City considers citizen-initiated amendment requests generally on a biannual basis and incorporates these into the annual plan amendment process. Citizen amendment requests may either be for general amendments or for a change to the land use map and/or text change relating to a specific property or a general area.

**From:** [Laura Loomis](#)  
**To:** [Jeremy McMahan](#)  
**Subject:** RE: Public Hearing June 28, 2012 Permit No. ZON11-00042  
**Date:** Tuesday, June 12, 2012 6:00:45 PM

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June 12, 2012

RE: Permit No. ZON11-00042 Public Hearing

Attn: Mr. Jeremy McMahan

Dear Mr. McMahan,

We will not be able to attend the Public Hearing on June 28, 2012, so please accept this letter regarding proposed zoning and/or Comprehensive Plan changes.

We are NOT in favor of allowing 4 story buildings in BN - Neighborhood Market zones. The density and scale of these zones should comply with the zoning and height restrictions of neighboring properties. The maximum density should be 12-24 units per acre - no more! An increase in building height should NOT be allowed to accommodate a minimum height for ground floor commercial.

These commercially zoned properties should either be zoned entirely multi-family residential in keeping with the surrounding residential zoning or include small businesses that service the neighborhood with residences limited by the zoning of the adjacent properties. BN zones are not and were not intended to be large scale buildings. They are located in neighborhoods with single family homes, small apartment buildings and condominiums. Large scale buildings belong downtown - not in a neighborhood.

There is no precedent in Bellevue, Bothell, Seattle, Redmond or any other nearby city that allows large scale unlimited or high density buildings in neighborhoods. Kirkland should not allow them either.

We do not accept the excuse that the city is required to increase density in neighborhoods. Kirkland has many undeveloped urban areas that are appropriate for large density projects. Development must be planned - not helter skelter - or the city will become an undesirable place to live. Residents won't live in a city where they cannot trust that their investment will retain its value due to indiscriminate development.

Finally, a design review should be required for all commercial/residential zoned developments.

Best regards,

**LAURA L. LOOMIS**  
CHARLES M. LOOMIS  
100 10th Avenue South  
Kirkland, WA 98033  
Email: [lauraloomis923@gmail.com](mailto:lauraloomis923@gmail.com)

**From:** [Uwkkkg@aol.com](mailto:Uwkkkg@aol.com)  
**To:** [Robin Jenkinson](#); [Kurt Triplett](#); [Eric Shields](#); [Joan McBride](#); [Jeremy McMahan](#); [Doreen Marchione](#); [Penny Sweet](#); [Amy Walen](#); [Dave Asher](#); [Toby Nixon](#); [Bob Sternoff](#)  
**Cc:** [uwkkkg@aol.com](mailto:uwkkkg@aol.com); [neighboringproperties@gmail.com](mailto:neighboringproperties@gmail.com)  
**Subject:** 10th Ave S / Lake St S - Enforcement of prior Lawsuit - Legal Remedy/Attny fees  
**Date:** Friday, May 25, 2012 9:26:21 AM  
**Attachments:** [CITYOF~2.PDF](#)

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Hi all:

I believe that neighbors are moving forward to involve attorneys in enforcement of prior legal agreement of maximum 22 units per acre which was to be enforceable by all successors. The legal agreement provides for attorney's fees reimbursed. The pages of the lawsuit (Passed as Resolution 2639 by COK) include both a description of the property (without exclusion for property @ 10th/Lake St S) and the provisions on how to enforce the agreement. The density cap is 22 per acre.

I am not understanding what evidence the Potala Village group believes that they have to indicate that they were led to believe that super high intensity development on Lake St S and 10th Ave S was acceptable.

Public records show many instances after the purchase of the property in 2010 wherein the new architect (old one was replaced) was still proposing different alternatives and still asking if they could get written confirmation that their project would work. Public Records request show that none of these requests got any positive response or confirmation from the city.

On the flip side, there is written documentation where restrictions were placed on the property during presubmittal meetings. The applicant was told that portions of the neighborhood plan applied "specifically to subject property." Those sections were highlighted in yellow and presented as part of the presubmittal packet. This is confirmed to be on file and written. The yellow highlights show max 12 units per acre, "consistency with developments to the north and south along LWB," Restricted commercial due to traffic ingress and egress which generally make commercial development not acceptable for the site, etc. When I confronted the applicant about knowing this all along, he stated that he was not at the presubmittal meeting and the packet was addressed to his first architect.... (he did not state that he was unaware of these restrictions).

The 2009 presubmittal information also has written documentation indicating that the driveway should not terminate on the arterial (Lake St S) and that certain specified objections would not be entertained as a reason to not have 10th Ave S as the driveway.

A check of the Washington Court Records shows Mr Lobsang Dargey and Dr David Myaskovsky (his business partner and believed large part owner of property) both frequent the court system. We cannot find that Tammy Agassi Dargey is an owner (except maybe due to marriage) as her name does not appear on any materials that we find. It seems to merely bring in the famous name of Andre Aggasi, her brother. We are unclear as to what relevance this has to the matter in front of the City.

Please hold fast to the decisions that were made about residential market properties by large groups of city and citizen stakeholders. Please do not be bullied by this applicant.

I believe that it was determined and that case law has been reviewed wherein a lack of building permit means the applicant is not vested. I am aware that prior to the imposition of the moratorium, the Attorney for the City of Kirkland, Robin Jenkinson, provided the applicant a heads up which would have allowed the applicant to get the building permit filed before the moratorium. He did not do that.

Karen Levenson



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11. Lake Washington Investors, et al. v. Kirkland,  
No. 837109 -

a. That portion of the property described in plaintiff's complaint as:

Lot 1, Block 2, French's Homestead Villa, according to plat recorded in Volume 20 of Plats, page 24, in King County, Washington; EXCEPT portion conveyed to King County for road purposes by deed recorded under King County Recording No. 1126248;

shall remain on the zoning map as RM 3600 and subject to said regulations.

b. The balance of the property described in plaintiff's complaint, to wit:

That portion of Government Lot 4, in Section 8, Township 25 North, Range 5 East, W.M., in King County, Washington, lying east of Lake Washington Boulevard as now established, south of the south line of Block 2, French's Homestead Villa, according to plat recorded in Volume 20 of Plats, page 24, in King County, Washington, north of the north line of Northeast 63rd Street "formerly Walnut Street", and west of 102nd Avenue Northeast, "formerly Second Street";

may be developed subject to and consistent with the general provisions of the RM chapter of the zoning code, provided, however, that such development shall not exceed 22 residential units. Provided, that unless some other method is arrived at by agreement between the owners of the real property hereinabove described and the owners of the Shorehouse Apartment, located across Lake Washington Boulevard, as to the existing Shorehouse off-street parking facility presently located on the property of plaintiffs, provision for the continuation of the "Shorehouse off-street parking facility" shall be retained or in the alternative provided

1 for within the design for off-street parking within the  
 2 westerly half of the total parcel, above described, and  
 3 further provided that nothing herein shall be taken as  
 4 preventing the owners of the property hereinabove described  
 5 from imposing reasonable rules and regulations consistent  
 6 with the ordinances of the City of Kirkland on the use of  
 7 such off-street facilities by and for the benefit of the  
 8 "Shorehouse Apartment", until such time as a court of competent  
 9 jurisdiction shall determine that the owner of the Shorehouse  
 Apartments does not have the right and privilege of using  
 said property for off-street parking.

10 B. GENERAL STIPULATIONS APPLICABLE TO ALL  
 11 CONSOLIDATED CASES

12 It is further stipulated by all parties to all of the  
 13 consolidated cases:

14 1. In the event it becomes necessary for any party to  
 15 any of the 11 consolidated actions to bring or institute any  
 16 judicial proceeding to enforce any of the provisions of the  
 17 Stipulation, Judgment, Order and Decree the prevailing party  
 therein shall be entitled to reasonable attorney's fees,  
 expert witness fees and costs.

18 2. Except as provided in the prior paragraph, each  
 19 party hereto shall bear their own attorney's fees and costs  
 20 incurred herein.

21 3. This agreement shall be for the benefit of and be  
 22 binding upon all successors and assigns of each party hereto.

23 4. Each party agrees to approve and support for entry  
 24 a judgment in all 11 of the consolidated actions incor-  
 25 porating the provisions of this stipulation, and (except as  
 26 may in this stipulation be specifically otherwise provided)  
 dismissing all actions with prejudice and which shall be a  
 final and binding judgment on all of the parties.

**From:** [Lori Isch](#)  
**To:** [Jeremy McMahan](#)  
**Cc:** [lori.isch](#)  
**Subject:** Potala Project - traffic  
**Date:** Monday, June 04, 2012 4:00:56 PM

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My primary issue with the project, assuming that the density will be scaled back a bit more to be in alignment with other such properties along S Lake Washington Blvd, is TRAFFIC.

Does the Comprehensive plan address the traffic issues on LWB? It seems to me that we are not doing enough to disincent the drive-through commuter traffic, and that traffic is what is making living on the Blvd so painful. So, the thought of a couple hundred more cars is putting people over the deep end.

There needs to be a STRATEGIC traffic plan. The minor improvements aren't doing enough.

There should be at least 2 more stop lights - people just fly down the street (South in the mornings). Such as, one light on the corner of the Potala project, and another south of Carillon Point. There needs to be some way for people to get out of their driveways easier. And, the drive-through commuters coming from North of downtown Kirkland, should be routed to 405 or 108th Ave. I would actually block Lake Washington at the tee, where you turn to go South. Going to downtown Kirkland should be a destination. All the extra traffic and the poor parking hurts the downtown businesses. I rarely drive North to downtown Kirkland because of the hassle. It is easier to go South to Bellevue.

Some creative and radical ideas are needed. We are letting the commuters who use Lake Washington as a pass-through impact the lives and neighborhoods of the people that live here!

[Lori Isch](#)  
home owner, NE 64th Street

lori.isch@usa.net  
425 444 7321 (cell)

**From:** [jkfoster756@frontier.com](mailto:jkfoster756@frontier.com)  
**To:** [Jay Arnold](mailto:jay.arnold@kirklandwa.gov); [jpascall@kirklandwa.gov](mailto:jpascall@kirklandwa.gov); [Mike Miller](#); [Byron Katsuyama](#); [Glenn Peterson](#); [C Ray Allshouse](#); [Andrew Held](#); [Robin Jenkinson](#); [Kurt Triplett](#); [Eric Shields](#); [Jeremy McMahan](#)  
**Cc:** [uwkkg@aol.com](mailto:uwkkg@aol.com)  
**Subject:** BN-Residential Market at Lake WA Blvd & 10th St.....  
**Date:** Wednesday, May 30, 2012 4:14:51 PM

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Dear Friends,

I'm writing to urge you to keep the designation of BN-Residential Market for the property at Lk Wa Blvd and 10th St. as that is what it should be for small business on residential property. There is no where near enough room for larger businesses as could be located in a neighborhood center designation.

Development should "fit" the neighborhood. Records show surrounding properties are 20 - 30% lot coverage. Lot coverage at Lake WA Blvd and 10th St. should be similar, at most 60%.

Density should also "fit" in with the surrounding properties which are 12 - 24 per acre. Lake WA Blvd. is one of the gateways into our city. We should aim to keep it as pleasant as possible for residents and visitors. Small businesses to serve the neighborhood and perhaps a small cafe for residents and visitors. We ought not to add to the congestion on Lake WA Blvd by allowing too many residents and/or large businesses on the property. Let's keep Kirkland as a pleasant place to live as well as a nice getaway for those seeking to escape from the congestion of a Seattle or Bellevue.

Thank you for your attention to this matter.

Joan Foster  
756 State St. #A  
Kirkland, WA 98033

**From:** [Ginnie DeForest](#)  
**To:** [C Ray Allshouse](#); [Jay Arnold](#); [Andrew Held](#); [Robin Jenkinson](#); [Byron Katsuyama](#); [Jeremy McMahan](#); [Mike Miller](#); [Jon Pascal](#); [Glenn Peterson](#); [eric shield](#); [Kurt Triplett](#)  
**Cc:** [ukkg@aol.com](mailto:ukkg@aol.com)  
**Subject:** BN-residential market zone  
**Date:** Thursday, May 31, 2012 12:11:45 PM

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To Members of Kirkland Planning Commission

At a previous City Council meeting you were asked to consider changing the zoning of property at Lake St. and 10th Ave. South to neighborhood center. I live at 945 1st St. So. which is directly opposite the 10th Ave. So. side of that property. We already have a wonderful neighborhood center called Houghton Center and Houghton Village a half mile away. I walk to it all the time. In addition it is less than a mile to the edge of the downtown core, and I walk that very often. Going south on Lake Washington boulevard we have a Super 24 about a half mile away and Kid Valley and Beach House Bar and Grill, and shops, restaurants and Post Office at Carillon Point, all within a mile's walk. We do not need another neighborhood center. Although the city wants to encourage retail business, one has wonder how much can be sustained within a mile radius. A small residential market might serve a need, but too much on the above property might struggle to be viable or negatively impact what we already have. So far Houghton thrives; competition is good, but too much of the same is not necessarily good. In addition, a submerged first floor should not be allowed if you want retail to be attractive.

I think we need limits to density and development that fits the scale of surrounding neighborhood. This can be done by a cap such as Councilman Nixon suggested of perhaps 1.5 times surrounding density which would generate a maximum of under 50 units, or by controlling lot coverage by fixing a percentage compatible with surrounding properties or through use of FAR as they did where I previously lived on Mercer Island.

Lastly, I have a lot of concern about traffic due to points of ingress and egress. Too much density will overload 10th Ave. So. which can barely handle two-way traffic when larger vehicles are in the curbside parking spaces.

Thank you for your consideration of these concerns,  
Virginia DeForest  
425-739-0730  
<[ginniedeforest@yahoo.com](mailto:ginniedeforest@yahoo.com)>

**From:** [uwkkg@aol.com](mailto:uwkkg@aol.com)  
**To:** [Jeremy McMahan](mailto:Jeremy McMahan)  
**Subject:** Fwd: Ultra-high density apartment building  
**Date:** Wednesday, June 13, 2012 4:25:42 PM

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Hi Jeremy...

This letter has come through other neighbors who have asked me to forward this to you. We continue to get new folks every week.

Thanks for doing what you can.

Karen

-----Original Message-----

**From:** Laura Loomis <[lauraloomis923@gmail.com](mailto:lauraloomis923@gmail.com)>  
**To:** Karen Levenson <[Uwkkg@aol.com](mailto:Uwkkg@aol.com)>  
**Sent:** Wed, Jun 13, 2012 3:54 pm  
**Subject:** Fwd: UHtra-high density apartment building

Karen,

Can you forward this letter to John McMahan? I don't have access to his email address at the moment.

Laura

Sent from my iPad

Begin forwarded message:

**From:** Chang Sophie &lt;[popoasia@gmail.com](mailto:popoasia@gmail.com)&gt;  
**Date:** June 13, 2012 1:07:42 PM PDT  
**To:** Laura Loomis &lt;[lauraloomis923@gmail.com](mailto:lauraloomis923@gmail.com)&gt;;  
[tswan@kirklandwa.gov](mailto:tswan@kirklandwa.gov), [eshields@kirklandwa.gov](mailto:eshields@kirklandwa.gov), [ktriplett@kirklandwa.gov](mailto:ktriplett@kirklandwa.gov)  
**Subject:** UHtra-high density apartment building

Hi,

My name is Sophie Chang. I live at where near by downtown Kirkland. Almost every good weather days I walked around lake from Marina Park to Carillon Point with my dog, I enjoyed it very much. I saw the flyer mention about this huge neighborhood alert and I didn't put much action for it before. Now, I would like to write an email to city hall regarding to keep our beautiful city as quiet and quality environment. We only have one Earth, such as we only have one Kirkland city in Washington State. I hope whoever in charge this project can "seriously" thinking about the units size and amount of units. I work for properties management company, pretty much figure what business men's mind. One thing I would like to point out is making money is important, but how about keep quality environment? If the investor also live in the building, I think the whole project would be totally different. Whoever charge this project from city, PLEASE reconsidering this project and make things to right direction. Thank you very much.

Sophie Chang