



MEMORANDUM

To: Design Review Board
From: Jon Regala, Senior Planner
Date: May 13, 2013
File No.: DRV12-01340
Subject: 450 CENTRAL WAY MIXED USE PROJECT DESIGN RESPONSE CONFERENCE
(CONTINUED FROM APRIL 15, 2013)

I. MEETING GOALS

At the May 20, 2013 Design Review Board (DRB) meeting, the DRB should continue the 450 Central Way Mixed Use project Design Response Conference and determine if the project is consistent with the design guidelines contained in Design Guidelines for Pedestrian Oriented Business Districts, as adopted in Kirkland Municipal Code (KMC) Section 3.30.040.

The DRB should provide feedback on the applicant's design response regarding the following topics:

- Central Way façade design
- Pedestrian arcade details
- Landscaping
- Courtyard design
 - Paving
 - Elevator tower details
 - 3rd story deck
- Materials, colors, and details
 - Use of cedar
 - Preliminary signage concept
- Project phasing plan

II. BACKGROUND INFORMATION

The subject property is located at 450 Central Way (see Attachment 1). Robert Pantley, with authorization from the property owner MISTPA LLC, has applied for a Design Response Conference for a new mixed-use development on the subject property. The majority of the project contains a mix of market rate apartment units (62) and residential suites (228). In addition, approximately 9,400 square feet of retail space is proposed at the ground floor along Central Way. The majority of the parking will be provided in a below-grade 2-level parking structure. Residential vehicular access is

proposed at the northwest corner of the property from 4th Avenue. Vehicular access for the retail component is proposed at the southeast corner of the property at Central Way.

The applicant also requested to phase construction of the project. Phase I would consist of Buildings 3, 4, and 5 which are located along/near Central Way. Buildings 1 and 2 would occur with Phase II. These buildings are located near northwest and north property lines.

The applicant also requested approval of a minor variation to the required 20' setback along 4th Avenue and Central Way.

The following timeline summarizes the design review process to date for the project:

- December 3, 2012 - Conceptual Design Conference. The DRB provided feedback and direction provided to the applicant.
- April 15, 2013 – Design Response Conference. The DRB provided additional feedback and direction to the applicant. See *Section III – DRB Recommendations* below. The meeting was continued to May 20, 2013.
- May 20, 2013 – Design Response Conference continuation.

Staff memos for the meetings listed above can be found at this web address on the City's website by the respective meeting dates:

http://www.kirklandwa.gov/depart/Planning/DRB_Meeting_Information.htm

III. DRB RECOMMENDATIONS

The Design Review Board reviews projects for consistency with design guidelines for pedestrian-oriented business districts, as adopted in Kirkland Municipal Code Chapter 3.30. The subsections below summarize the DRB's comments sorted by topic and as discussed at the April 15, 2013 meeting. The comments are followed by a brief analysis by staff.

The applicant's written response to the DRB's recommendation can be found in Attachment 2. The associated revision drawings can be found in Attachment 3.

A. Building Massing/Design

1. DRB Discussion/Recommendation

Massing/Architectural Design. The DRB agreed that the project has evolved nicely since the Conceptual Design Conference in terms of the overall massing and building design. The original design concept included a single 'U' shaped building configuration with a south-facing courtyard at or near grade. The current design exceeded expectations by breaking up the building massing into 5 separate buildings while still maintaining the south-facing courtyard concept. The site design goes further by providing other smaller courtyard areas between buildings, 3rd level decks overlooking Central Way, and a 3rd level deck area above the main courtyard for residents and special events.

The DRB also liked how the buildings were nicely proportioned as a result of a balanced approach to modulating the buildings. Vertical modulation was achieved with varying the roof forms, architectural bays, and placement of materials and colors. Horizontal modulation was achieved with a strong brick pedestrian arcade element at the ground and second floor, placement of the third level courtyard, and use of different materials and colors at the building's middle and top. At the northernmost building, the lengthy façade was mitigated by the retention of the existing trees and distance from the residences to the north.

A topic of concern was the building aesthetic along Central Way. While the DRB appreciated the residential architectural character for the majority of the project, especially where facing the condominiums to the north, the DRB was concerned that the upper stories along Central Way did little to express the urban character found at the base of the ground floor. The proposed peaked composition roof design, the material palette, and use of vinyl windows were questioned by the DRB as to whether they were appropriate to the location.

During their deliberation, the DRB asked that the Central Way façade reflect the urban aesthetic successfully created at the ground floor by bringing more of the pedestrian arcade materials to the upper stories. Another suggestion was to incorporate a similar shed roof design found on several buildings fronting the main courtyard area to the Central Way facade. Even with these potential changes, the DRB asked that the requested urban aesthetic should still integrate well with the northern residential buildings design.

See Section III.D below for additional discussion on the proposed building materials and colors.

Building Phasing. The DRB reviewed the applicant's phasing plan in which Buildings 1 and 2 (two northernmost buildings) would be constructed at a later date. The DRB requested that the applicant provide a detailed landscape/site plan that shows the interim site design.

Minor Variation. The DRB agreed that the request to modify the required setbacks along Central Way and 4th Avenue should be granted. The setback reduction along Central Way would create a superior design in that the Central Way façade would have additional visual architectural interest, add upper story activity, and provide significant plaza space at the ground level. The setback reduction along 4th Avenue would allow for additional courtyard space and a site plan designed to accommodate 5 separate buildings to address building massing concerns. No significant detrimental effects were found by the DRB with the setback reductions. Instead, the DRB felt that each façade responded well to the context of each respective street.

2. Staff Comment

The applicant has responded to the DRB's concerns regarding the building design along Central Way (see Attachments 2 and 3). Details of the interim site improvements related to the phasing plan have also been provided (see Attachment 4).

To address the DRB's request to have a more contemporary/urban aesthetic along Central Way (building upon the pedestrian arcade) and how that aesthetic transitions to the more residential character on the northern portion of the site, the applicant has taken the approach of providing the DRB with several design options. In regards to the roof form, the applicant has proposed two design solutions. One option has a more traditional shed roof design over the vertical bays. The other option has a lateral shed roof design over the bays when viewed from Central Way. In regards to the building material transition, the applicant has provided three options for the DRB's consideration at the building's upper stories. All options draw upon the pedestrian arcade design.

The DRB should review the façade and roof design options, the phasing plan, and provide feedback to the applicant.

B. Pedestrian Access

1. DRB Discussion/Recommendation

Pedestrian Arcade. Generally speaking, the DRB thought that the pedestrian arcade was successfully designed. The arcade had enough room to have an open feel (appropriate scale), provided enough natural light, was an integral part of the building's architecture, and did not hinder the retail experience. However, the DRB requested that the applicant explore the following:

- There was some concern about the location of the landscape strip next to the on-street parking and how it could impact the flow/movement of pedestrians in and out of the arcade as well as the opening of car doors.
- The DRB requested that attention be paid to the portion of the arcade that spans the driveway and that the integrity of the arcade design continues at this area.
- Provide additional detailing and human scale elements to the arcade columns.

Through-Block Pedestrian Connection. The DRB asked the City to elaborate on the requested through-block pedestrian pathway. The DRB agreed that while a pedestrian connection would be beneficial to the City, the priority should be to retain the existing tree buffer along the north property line. The DRB was informed that any changes to the required landscape buffer standards as a result of constructing the pedestrian connection would need written approval from the property owners to the north. The DRB acknowledged the opposition to the pedestrian pathway expressed by the neighbors.

2. Staff Comment

Pedestrian Arcade. The applicant revised the arcade design to the address the DRB's concerns regarding the arcade detailing and human scale (see Attachment 3). In addition, the landscape strip next to the on-street parking was reduced in size to provide for more pedestrian movement area and reduce conflict with car doors. Also, the street trees were moved to coincide with the arcade columns to provide additional storefront visibility and to create a more inviting and open area. The DRB should provide feedback to the applicant regarding these changes.

The applicant should also continue to work with Public Works in finalizing the arcade design. Portions of the arcade walkway are narrower (8') than the required 10' minimum sidewalk width due to 2' wide columns at the arcade edge. The design guidelines suggest a 10' to 12' wide movement zone. Also, the walkway area under the arcade does not coincide with the public easement being required by the City. Structural components of the arcade are also proposed within the required public easement. For example, sight distance compliance near the project driveway as it relates to the proposed building and arcade columns should be confirmed with Public Works as well as the final location of street trees, landscaping, and lighting that would be required.

Through-Block Pedestrian Connection. KZC Section 105.19 allows the City to require a through-block pedestrian pathway if:

- A walkway is reasonably necessary to provide efficient pedestrian access to a designated activity center of the City or to transit; or
- A through-block pedestrian pathway if blocks are unusually long; or

- Pedestrian access is necessary to connect between existing or planned dead-end streets, through streets, or other pedestrian access

The location of the City requested through-block pathway coincides however with a Zoning Code mandated 15' wide landscape buffer (includes 6' tall fence at the property line) and existing row of significant evergreen trees. The existing trees would be supplemented with new trees in order to comply with the City's landscape buffering requirements. Staff had the following concerns regarding pathway and buffering requirements:

- Existing significant tree retention. To what extent should additional trees be removed to allow for the pedestrian walkway?
- Location of the pathway relative to the new residential building
- Reduced visibility of the walkway given that a 6' tall fence is required at the property line
- Any modification of the landscape buffering standards requires written approval by the neighboring owners (including the removal or relocation of the required 6' tall fence)
- Modifications to the through-block pathway would be subject to the standards found in KZC Section 105.19.2.b in which one of the modification criterion is that the modification will not have a substantial detrimental effect on nearby properties and the City as a whole.

Given these concerns, some of which were expressed also by the DRB, the City will not require the applicant to install the through-block pedestrian connection.

C. Landscaping and Courtyard Design

1. DRB Discussion/Recommendation

The DRB agreed that the landscape plan was headed in the right direction but wanted to review the details regarding landscaping within the courtyard to include the location of pots, planters, and other pedestrian features (e.g. art displays, benches, etc.) as they relate to each other and the outdoor seating areas. Additional details regarding the overall planting plan and a paving plan were also requested. The DRB also asked how the courtyard design can better reflect the pedestrian arcade design along Central Way.

The DRB also noted that having the elevator feature at the end of the courtyard was a nice focal point and requested that its design be played up since it will have a major role in the courtyard experience. Another concern expressed by the DRB was the 3rd story deck above the courtyard. Comments suggest that the deck should be made more integral to the composition of the courtyard and the surrounding buildings.

2. Staff Comment

The applicant has submitted a design response to the concerns expressed by the DRB in regards to the 3rd story deck, the elevator feature at the end of the courtyard, and the landscape plan (see Attachments 2 to 5). Attachment 6 contains detailed information regarding the hanging planters attached to the pedestrian arcade. The DRB should provide input on the proposed revisions.

D. Building Materials, Color, and Detail

1. DRB Discussion/Recommendation

The DRB liked the proposed material and color palette for the project. The DRB agreed that the colors and materials used were successful in reflecting a northwest beach and Kirkland aesthetic. However, the DRB requested that the applicant explore breaking up the larger vertical expanses of cedar used at various areas around the project. The main concern the DRB had was the eventual appearance of cedar after 5+ years.

2. Staff Comment

The applicant submitted two design options to address the DRB's concern regarding the large areas of cedar. One option is to provide steel banding at the floor lines to help break up the vertical expanse of cedar siding. The other option organizes cedar siding into smaller areas with similar patterns. There is no change in materials however with this second approach. The DRB should provide feedback to the applicant regarding this topic.

The applicant has also submitted a preliminary signage concept (see Attachment 3). The DRB should confirm that the placement of future signs are integrated with the building's architecture and should not detract from the building's design. In terms of background code information, KZC Chapter 100 regulates signage. Based on the proposed uses on the subject property, the applicant may be allowed to install wall mounted, marquee/awning, and ground mounted signs. KZC Chapter 100 also regulates the amount of sign area allowed for a development. Projecting and under marquee/awning (blade) signs are prohibited unless 4 square feet in size or smaller.

IV. KEY ZONING REGULATIONS

The applicant's proposal is also subject to the applicable requirements contained in the Kirkland Municipal Code, Zoning Code, Fire and Building Code, and Public Works Standards. It is the responsibility of the applicant to ensure compliance with the various provisions contained in these ordinances. These regulations and standards are not under the review authority of the DRB and will be reviewed for compliance as part of the building permit review for the project.

In terms of zoning, development on the subject property is subject to the regulations in CBD 7 as well as other applicable KZC sections. The following regulations are important to point out as they form the basis of any new development on the site. Below are some of the key zoning standards that apply to the development followed by staff comment in italics.

- A. Permitted Uses: Permitted uses in this zone include, but are not limited to retail, restaurants, office, and stacked dwelling units. Office and residential uses may not be located on the ground floor of a structure unless there is an intervening retail use. Residential suites are also allowed in CBD 7 as another type of residential use. Residential suites are defined as:

KZC Section 5.10.778 - A structure containing single room living units with a minimum floor area of 120 square feet and maximum floor area of 350 square feet offered on a monthly basis or longer where residents share bathroom and/or kitchen facilities. "Residential Suites" does not include dwelling units, assisted living facility, bed and breakfast house, convalescent center, nursing home, facility housing individuals who are incarcerated as the result of a conviction or other court order, or secure community

transition facility. For purposes of zones where minimum density or affordable housing is required, each living unit shall equate to one dwelling unit.

Staff Comment: The applicant is proposing ground floor retail along Central Way with residential uses behind. The upper stories will consist of only residential uses. The residential component of the project will consist of a mix of market rate apartment units and residential suites. The majority of the parking for the project is proposed in a below-grade parking structure. The proposal is consistent with the permitted uses for CBD 7.

- B. Setbacks: A minimum 20' front yard setback is required. One-story street level retail shall have a zero foot setback. There are no required setbacks for the side or rear property lines.

Staff Comment: A 20' front yard setback is required along Central Way with the exception of one-story retail. In addition, a 20' front yard setback is also required along the portion of the north property line adjacent to 4th Avenue. The applicant has requested a minor variation to the setback requirement pursuant to KZC Section 142.37. See Section III.A above.

- C. Height: CBD 7 allows a maximum height of 41' measured above the average building elevation. In addition, KZC Section 50.62.2 requires that the minimum ground floor height is 15' for retail, restaurant, and other ground floor uses.

Additional height is allowed for peaked roofs and/or parapets. For structures with a peaked roof, the peak may extend five (5) feet above the height limit if the slope of the roof is greater than three (3) feet vertical to 12 feet horizontal and eight (8) feet above the height limit if the slope of the roof is equal or greater than four (4) feet vertical to 12 feet horizontal. Decorative parapets may exceed the height limit by a maximum of four feet, provided that the average height of the parapet around the perimeter of the structure shall not exceed two feet.

Additional allowances for height in regards to rooftop appurtenances are found in KZC Section 115.120.

Staff Comment: The applicant must demonstrate compliance with the City's height requirements as part of any building permit.

- D. Lot Coverage: CBD 7 zoning regulations allow 80% lot coverage. If a minimum zero-foot front yard setback is provided for one-story retail uses, then the lot coverage for the property shall be 100%.

- E. Parking: Retail uses must provide one parking space for each 350 square feet of gross floor area. Stacked dwelling units must provide a minimum of one parking stall per bedroom or studio unit and an average of at least 1.3 parking stalls per unit for the development. Guest parking shall be provided at a rate of 0.1 stalls per bedroom or studio unit with a minimum of two guest parking stalls.

Residential suites must provide either 0.5 or 1 parking stalls per living unit (depending on how parking is managed per code) and 1 stall per on-site employee.

Staff Comment: The applicant must demonstrate compliance with the City's parking requirements as part of any building permit.

- F. Landscaping & Tree Retention. Based on the proposed uses on the subject property and the adjoining multi-family development to the north, a 15'-wide

landscape buffer is required along the north property line planted pursuant to standards found in KZC Section 95.42.

Staff Comment: KZC Section 95.40.2 gives the City the authority to require the retention of existing trees and vegetation in order for a project to comply with City landscape requirements. Since the existing trees along the north property line coincide with the required 15' wide landscape buffer, as many of the existing trees should be retained depending on the Tree Retention Plan review.

Based on the City arborist's recommendations, the Tree Retention Plan submitted by the applicant should be confirmed for accuracy by the applicant and re-evaluated. The City arborist made notes as to the correct location of existing trees. In general however, the City arborist agrees with the applicant's arborist in that all of the trees along the north property line, east of the cul-de-sac, should remain. The trees should survive the construction process if protected correctly. As the existing Douglas Fir trees mature and thin-out at the lower branches, the City arborist also recommends the landscape plan be updated with tree species and shrubs to enhance the tree understory and supplement the buffer.

- G. Sidewalks. Central Way is a designated Pedestrian Oriented Street. Therefore the sidewalk standards require a minimum 10' wide sidewalk along the entire frontage of the subject property abutting Central Way. The final sidewalk configuration should be approved through the design review process.

KZC Section 110.52.1 requires states that if the required sidewalk improvements cannot be accommodated within the existing right-of-way, the difference may be made up with a public easement over private property; provided, that a minimum of five (5) feet from the curb shall be retained as public right-of-way and may not be in an easement.

Staff Comment: The applicant should continue to work with Public Works regarding the sidewalk design, easement location in regards to the 10' wide sidewalk location, and confirm compliance with KZC Section 110.52.1.

V. STATE ENVIRONMENTAL POLICY ACT

SEPA is the state law that requires an evaluation of a development proposal for environmental impacts. The issue most frequently addressed through SEPA is traffic. Design Review is not a project action and thus SEPA review is not required at the time of Design Review.

A SEPA application has not been submitted by the applicant. SEPA review is required to occur prior to the issuance of any building permit for the project.

VI. PUBLIC COMMENT

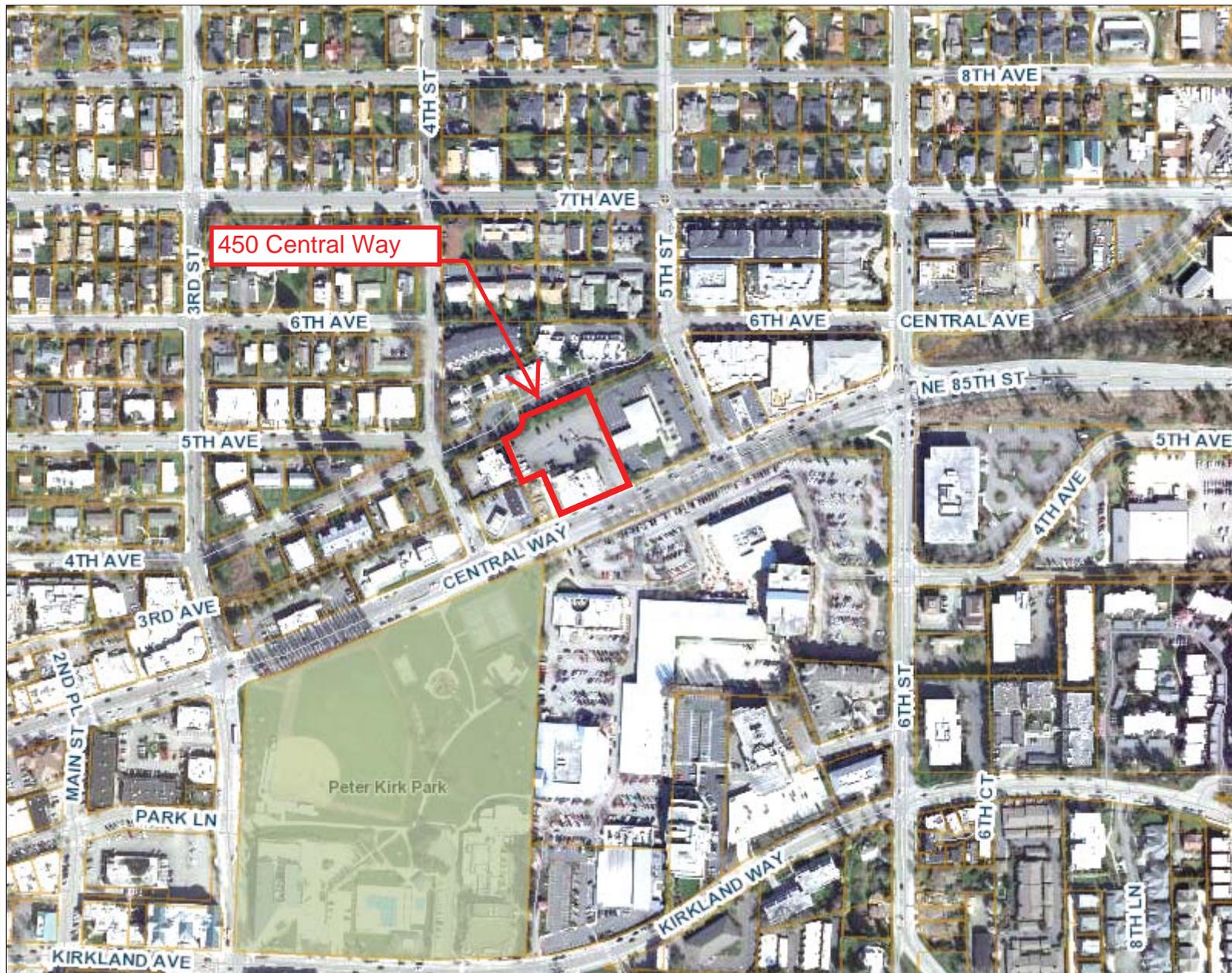
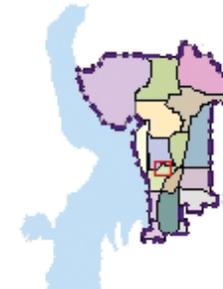
Prior to the finalization and distribution of this staff memo, no comments from the general public were received.

VII. ATTACHMENTS

1. Vicinity Map
2. Applicant Design Response - Letter
3. Applicant Design Response - Plans
4. Landscape Response Letter
5. Landscape Plan
6. Hanging Planter Detail



GIS MAPPING PORTAL ~ City of Kirkland, Washington ~ Department of Information Technology



Legend

- Streets
- Parcels
- Parks
- Schools
- z_Image09**
- Red: Band_1
- Green: Band_2
- Blue: Band_3

1:4,199



ATTACHMENT 1
DRY12-01349
VICINITY MAP

0.1 0 0.07 0.13 Miles

©NAD_1983_StatePlane_Washington_North_FIPS_4601_Feet
Produced by the City of Kirkland. © 2011 City of Kirkland, Washington, all rights reserved.

No warranties of any sort, including but not limited to accuracy, fitness or merchantability, accompany this product.

THIS MAP IS NOT TO BE USED FOR NAVIGATION

Notes

Vicinity Map
436 Central Way



Memo

~~May 1, 2013~~ May 6, 2013

Mr. Jon Regala
City of Kirkland
City Hall
123 5th Ave
Kirkland WA 98033-6189

Re: Response to 4.15.2013 DRB guidance meeting

Jon,

We have taken the guidance received in the April 15th DRB meeting under consideration and developed a comprehensive, well-thought response (please see the enclosed drawing supplement). There were specific guidance items that the board wanted to see developed for the May 20th meeting as follows:

1. More contemporary expression along Central Way, roof forms in particular
2. Create cohesion of the colonnade with the upper stories
3. Mitigate the larger expanses of singular siding materials with banding or other forms of differentiation.
4. Create cohesion of the elevated deck at the central courtyard with the balance of the design
5. More brick detailing of the colonnade
6. Signage concept
7. More expression/celebration of the steel on the street front
8. More detailed landscape information
9. Landscape plan showing interim uses between phase one and two

Item 1: Roof forms

The team has studied several different options. Two alternate solutions were deemed appropriate with one option clearly preferred.

- a. The first alternate is a traditional bay expression carried to the fifth story and terminated with a reverse shed at 4:12 pitch. The team feels this would be an acceptable solution but feels it to be a ubiquitous urban form and not as sophisticated or "Kirkland authentic" as alternate "b".
- b. Using the beach as inspiration for our material palette we returned to that source for inspiration of alternate two. We are expressing the roof forms as lateral pitched shed roofs at minimum 4:12 pitch. The resulting geometry evokes the form of a sail on boat, open to the prevailing wind and consequently the views to the southwest. We find this expression to be unique, urban and well-suited for the balance of the building design. As the building turns into the courtyard the end gable has been tapered to create a dynamic form supporting the geometry of the sails, metaphorically

resembling the prow of ship. This form also bridges the shed roof expressions at the upper central courtyard with the Central Way façade.

After consulting with our LEED verification team, it was determined that there was not a significant benefit in utilizing a metal roof in the reduction of the heat island effect and the glare produce by a high albedo, metal roof will negatively affect the neighbors. Given the relationship and support of our neighbors to the north, we are proposing architectural grade composition roofing to minimize visual glare and maintain high albedo. As the roof finishes will not be seen from the pedestrian level, we are confident this is an appropriate response.

Item 2: Upper story compatibility:

Some board members suggested more compatibility between the arcade and the upper stories along Central Way. We believe much of this will be resolved with the preferred solution of item 1 above. To further support the pursuit of compatibility, the team assembled three material distribution studies:

- a. This alternate maintains the cedar main field with brick accents at the bay projections that match the colonnade finish below. This supports contrast with the base below by honestly expressing the differing functions of the commercial space below and the residential above.
- b. We have "married" the upper stories with the base by utilizing the same brick material as the main field material. Cedar accents at the bay projections maintain some of the warmth and residential expression.
- c. This alternate is similar to option B but with more contrast at the bay projections. The resulting design embodies a more "coastal" design response.

Item 3: Siding Scale and Detail

The team analyzed several options to mitigate the larger expanses of singular siding. In celebration of the historic Peter Kirk steel ambitions (Pittsburg of the west), we have incorporated metal banding expressions at the floor lines to break up the larger expanses of siding. The profile provides shade, shadow, relief and overall visual interest.

At the cedar siding areas of buildings 1, 2 & 3, we again turn to history and were inspired by Native American use of the cedar bark into "textiles" for bags, rugs, and clothing. The resulting "parquet" or "weave" breaks down façade into subtle compartments demonstrating craft and visual interest.

Item 4 & 7 Upper deck compatibility and metal expression

We have enhanced the design of the bridge structure to be more expressive and tectonic. The result is a rich blending of the brick and steel evoking an historic palette of utilitarian, honest uses. The bridge form reinforces passage to the rear courtyard, a gateway, yet maintains a light, tracery element to maximize the passage of light, air and visual access.

Item 5 Brick & Colonnade detailing

The brick colonnade has been refined to show a rich composition of soldier and stack bond brick coursing to provide shadow, relief, texture and overall visual interest. The detailing is simple with great clarity which is compatible with the balance of the design response. The storefront has been updated to show a simple mullion rhythm with larger expanses of glass at

the pedestrian range. This establishes variety with less rigor than the previous design and supports the pedestrian scale.

We propose a passive water feature at the column lines that will be animated during the rain. Small volumes of water will be diverted to gently cascade down a lightly corrugated face into a decorative drain at the base of the storefront wall.

The landscape valance at the upper colonnade is being animated by an elevated and irrigated plant "trough". The steel expression provides another layer of shadow/relief and ties into the historical reference theme.

Item 6 & 7 Signage and metal expression

The signage concept is simple and integrates architecturally into the balance of the design. The metal reinforces the material palette elsewhere in the project. The proposed lettering size and placement meets the requirements of KZC 100.0 with an allowable per-business-sign area of 30 sf with an allowable aggregate total of 111sf per table 100.45. for mixed-use applications.

Items 8 & 9: Landscape information

Please see Landscape written response under separate cover.

Feel free to call with any questions or comments.

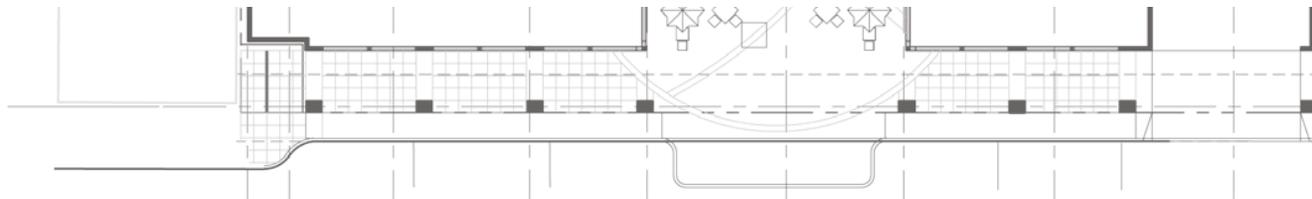
Respectfully,



Jay Janette



SOUTH ELEVATION





AERIAL VIEW FROM CENTRAL



STREET VIEW FROM CENTRAL



COURTYARD VIEW FROM DECK



VIEW FROM CENTRAL "BAY" OPTION (A)



VIEWS OF "BAY" OPTION (A)



VIEW FROM CENTRAL OF "SAIL" OPTION (B)



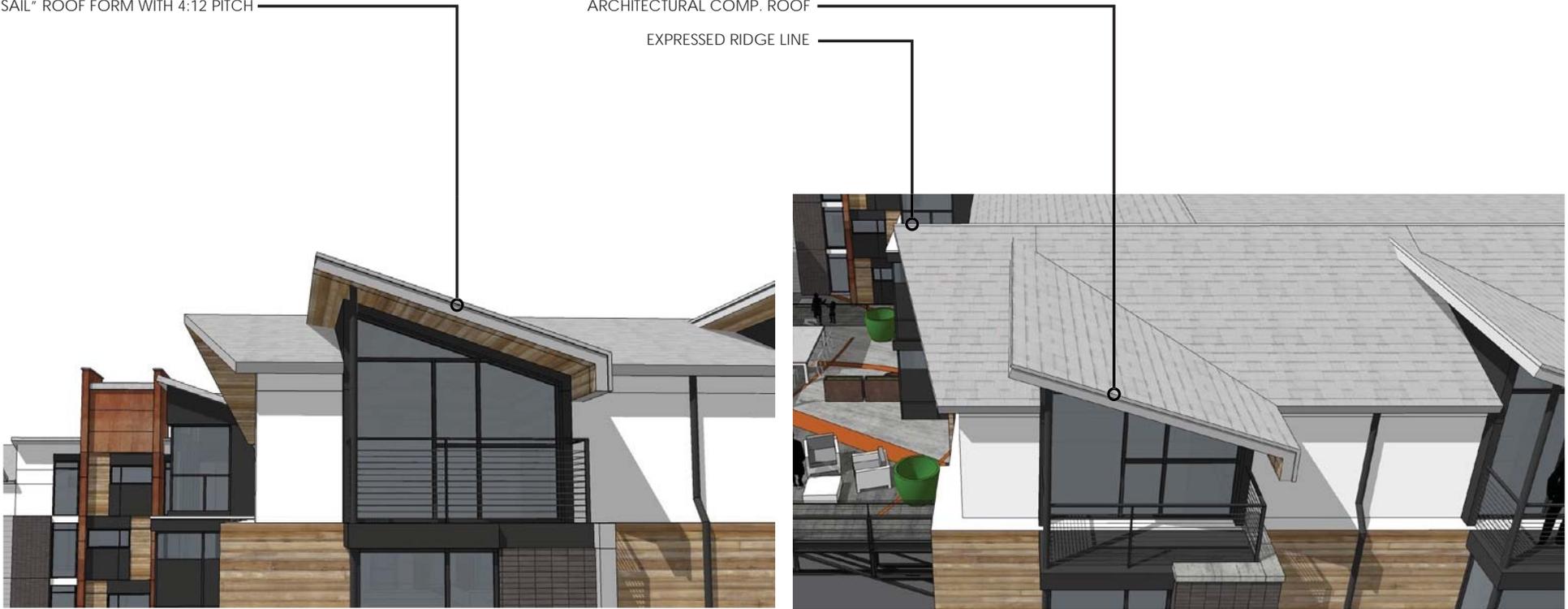
VIEWS OF "SAIL" OPTION (B)



"SAIL" ROOF FORM WITH 4:12 PITCH

ARCHITECTURAL COMP. ROOF

EXPRESSED RIDGE LINE





MATERIAL OPTION A VIEWS

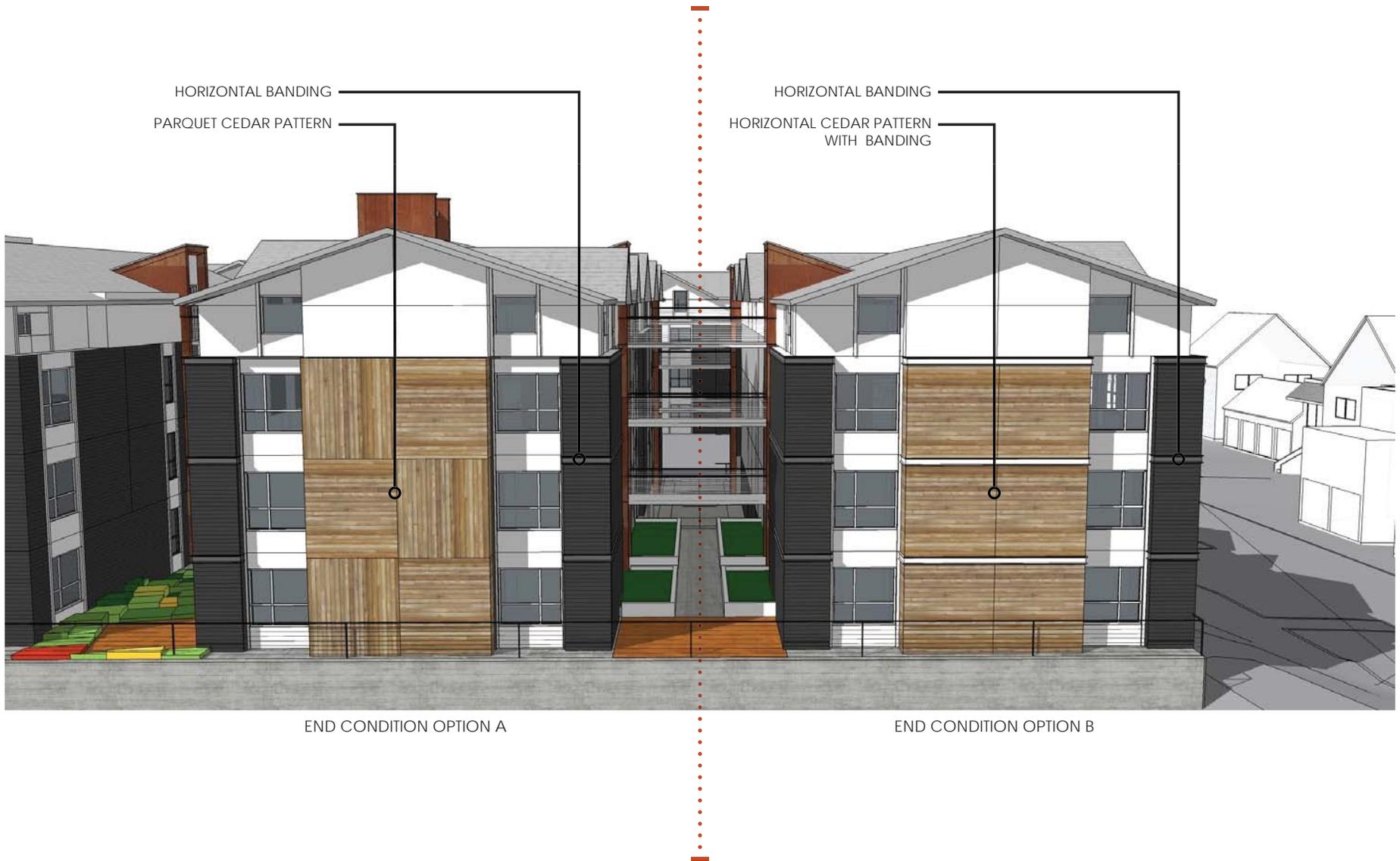


MATERIAL OPTION B VIEWS

CONTRASTING BAYS
SECONDARY FIELD OF CEDAR
PRIMARY FIELD OF BRICK



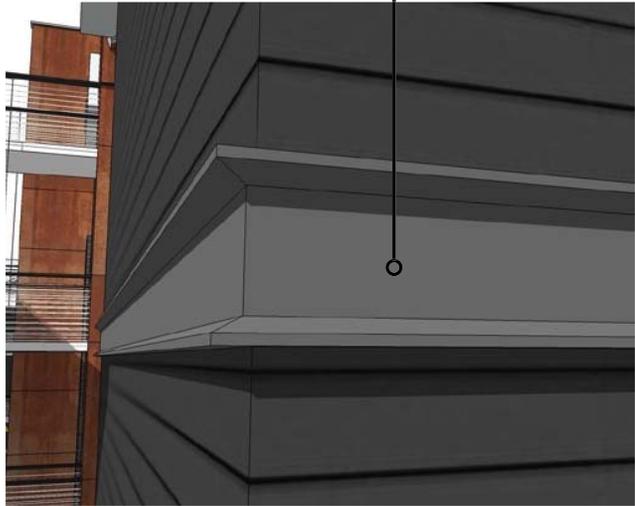
MATERIAL OPTION C VIEWS



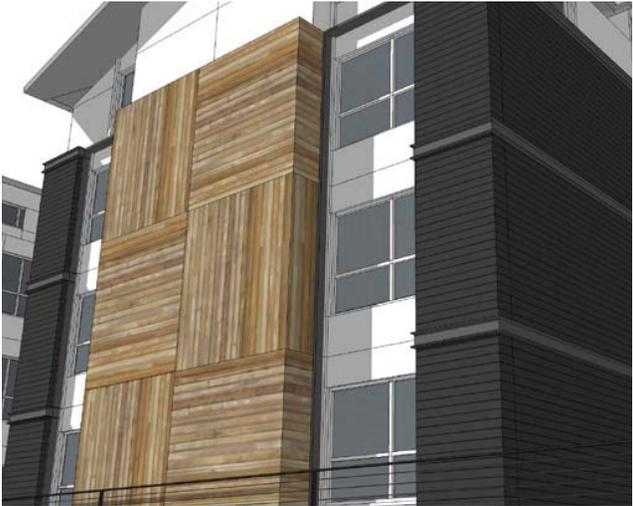
END CONDITION OPTION A

END CONDITION OPTION B

METAL PROFILE



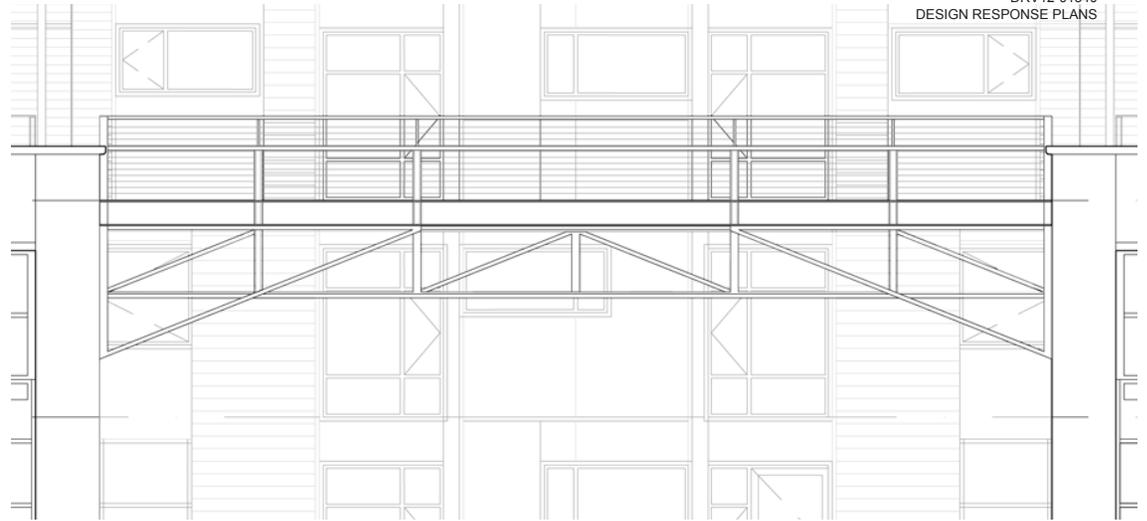
DETAIL VIEW OF BANDING



VIEW OF OPTION A END CONDITION



VIEW OF OPTION B END CONDITION



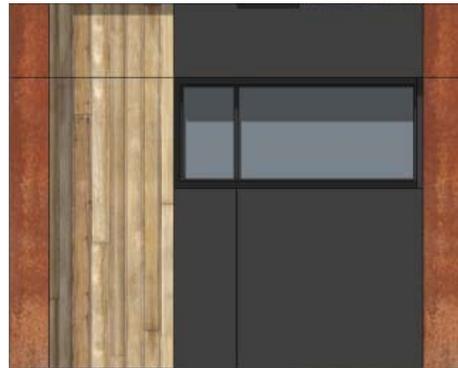
EXPRESSED STRUCTURE ESTABLISHES GATEWAY TO COURTYARD



DETAIL VIEW OF STRUCTURE



DETAIL VIEW OF STRUCTURE



ELEVATOR FACADE MODULE



VIEW OF ELEVATOR FROM COURTYARD