



## MEMORANDUM

**To:** Design Review Board  
**From:** Tony Leavitt, Associate Planner  
**Date:** January 7, 2013  
**File No.:** DRV12-01513  
**Subject:** 324 CENTRAL WAY MIXED USE PROJECT  
CONCEPTUAL DESIGN CONFERENCE

### I. MEETING GOALS

At the January 14, 2013 Design Review Board (DRB) meeting, the DRB should review the concept design for the 324 Central Way Mixed Use project. At the meeting, the DRB should determine:

- A. How the design guidelines affect or pertain to the proposed development.
- B. Determine which guidelines apply to the proposed development.
- C. Determine what other application materials are needed for the Design Response Conference.

### II. BACKGROUND INFORMATION

The subject property is located at 324 Central Way (see Attachment 1). The applicant is proposing to construct a new mixed-use project that would consist of ground floor retail tenant space with 3 to 4 floors of market rate apartment units above. Parking is proposed within a below-grade parking structure. The exact number of residential units, parking, and the size of retail space is unknown at this time due to the conceptual nature of the project at this design review phase. The applicant has provided a program description and general project information which includes several building massing options (see Attachment 2). The applicant's preferred building massing option is shown in Option 3. This option would require a variation of the minimum front yard setback for the residential use.

### III. SITE

The subject property currently contains a gas station, car wash, and coffee stand. The site slopes downward from east to west, but has a significant upward slope along the north property line. The property has street frontage along Central Way to the south and 4<sup>th</sup> Street to the east. Central Way is a designated *Pedestrian Oriented Street*.

The following list summarizes the zoning designation, uses, and allowed heights of properties adjacent to the subject property (see Attachment 3):

- North:* PLA 7B. Single and multi-family, Office. Maximum height is 30'.  
*East:* CBD 7. Shell Gas Station. Maximum height is 41'.

*South:* Park. Peter Kirk Park.

*West:* CBD 7. Wendy's Restaurant . Maximum height is 41'.

Additional photographs prepared by the applicant that show the surrounding properties are contained in Attachment 2.

#### **IV. KEY ZONING REGULATIONS**

Zoning regulations for uses in CBD 7 are found in the use-zone chart (see Attachment 3). The following regulations are important to point out as they form the basis of any new development on the site.

- A. Permitted Uses: Permitted uses in this zone include, but are not limited to retail, restaurants, office, and stacked dwelling units. Office and residential uses may not be located on the ground floor of a structure unless there is an intervening retail use.

*Staff Comment:* The applicant is proposing ground floor retail along Central Way with residential uses behind. The upper stories will consist of only residential uses. The majority of the parking for the project is proposed in a below-grade parking structure. The proposal is consistent with the permitted uses for CBD 7.

- B. Setbacks: A minimum 20' front yard setback is required. One-story street level retail shall have a zero foot setback. There are no required setbacks for the side or rear property lines.

*Staff Comment:* A 20' front yard setback is required along Central Way and 4<sup>th</sup> Street with the exception of one-story retail.

*The DRB has the authority to approve a request for minor variations from the required setback yards pursuant to KZC Section 142.37 provided that:*

- *The request results in superior design and fulfills the policy basis for the applicable design regulations and design guidelines;*
- *The departure will not have any substantial detrimental effect on nearby properties and the City or the neighborhood.*

*The applicant's preferred massing alternative includes variations from the required 20 setback. If the applicant proceeds with a variation proposal as part of the Design Response Conference, the applicant should provide specific dimensions for the request and information that addresses the criteria listed above. The DRB will determine if the variation is "minor" and meets the applicable criteria.*

- C. Height: CBD 7 allows a maximum height of 41' measured above the average building elevation. In addition, KZC Section 50.62.2 requires that the minimum ground floor height is 15' for retail, restaurant, and other ground floor uses.

Additional height is allowed for peaked roofs and/or parapets. For structures with a peaked roof, the peak may extend five (5) feet above the height limit if the slope of the roof is greater than three (3) feet vertical to 12 feet horizontal and eight (8) feet above the height limit if the slope of the roof is equal or greater than four (4) feet vertical to 12 feet horizontal. Decorative parapets may exceed the height limit by a maximum of four feet, provided that the average height of the parapet around the perimeter of the structure shall not exceed two feet.

*Staff Comment: The applicant should submit average building elevation calculations with the Design Response Conference application. Staff will review the project for compliance with the City's height regulations.*

- D. Lot Coverage: CBD 7 zoning regulations allow 80% lot coverage. If a minimum zero-foot front yard setback is provided for one-story retail uses, then the lot coverage for the property shall be 100%.
- E. Parking: Retail uses must provide one parking space for each 350 square feet of gross floor area. Stacked dwelling units must provide a minimum of one parking stall per bedroom or studio unit and an average of at least 1.3 parking stalls per unit for the development. Guest parking shall be provided at a rate of 0.1 stalls per bedroom or studio unit with a minimum of two guest parking stalls.

*Staff Comment: Staff has not yet evaluated the proposed project for compliance with the City's parking regulations.*

- F. Landscaping. Based on the proposed uses on the subject property and the adjoining multi-family development to the north, a 15'-wide landscape buffer is required along the north property line planted pursuant to standards found in KZC Section 95.42.

*Staff Comment: A landscape plan and Tree Retention Plan should be submitted with the Design Response Conference application. Existing significant trees that are located within the required landscape buffer and determined to be viable should be retained to the maximum extent possible.*

- G. Sidewalks. Central Way is a designated as a Pedestrian Oriented Street. Therefore the sidewalk standards require a minimum 10' wide sidewalk along the entire frontage of the subject property abutting Central Way. An 8' wide sidewalk is required along 4<sup>th</sup> Street as it is a Major Pedestrian Sidewalk. The final sidewalk configuration shall be approved through the design review process.

## V. PEDESTRIAN ORIENTED DESIGN GUIDELINES

In addition to the standard guidelines contained in the *Design Guidelines for Pedestrian-Oriented Business Districts*, the list in Attachment 4 summarizes some of the key guidelines or regulations which apply specifically to the project or project area. The following is a list of key design issues and/or design techniques that should be addressed with this project.

- Pedestrian-oriented space and plazas
- Blank wall treatment
- Vertical and horizontal definition
- Architectural scale
- Horizontal modulation
- Change in roofline
- Human scale
- Building material, color, and detail
- Signage

*See adopted Design Guidelines for Pedestrian-Oriented Business Districts for complete text and explanations.*

## **VI. CONTEXT**

The context or setting in which the proposed development will be located is important in determining the appropriate design regulations that would apply. The following are several questions that are geared towards identifying the physical environment around and on the subject property. These questions will help supplement the discussion on the key design guidelines appropriate for the proposed project.

### **A. How does the site relate to its surroundings?**

The applicant and Design Review Board should discuss the physical and built environment on and around the subject property. Topics include height of neighboring structures, topography, and landscaping.

### **B. What are the Opportunities and Constraints of the Site and Vicinity given the following topics?**

- Streetscape
- Urban Form
- Activities and Uses in the area
- Pedestrian Patterns and Environment
- Character of Adjacent Buildings
- Landscaping/Open Space

## **VII. DISCUSSION ISSUES**

The role of the DRB at the Conceptual Design Conference is to help determine how the design guidelines found in the Pedestrian-Oriented Design Guidelines apply to the proposed development. The following sections and questions below are representative of the City's design guidelines. These questions are to be used as a tool to help identify how design guidelines would apply to the proposed project.

### **A. Scale**

1. What are the key vantages of the project?
2. Identify appropriate mitigation techniques for building massing of the proposed buildings. Possible techniques include vertical and horizontal modulation, corner treatment, and roof forms. The applicant has provided several massing schemes including a preferred option (Proposal 1) for the DRB's review and comment (see Attachment 2).

### **B. Pedestrian Access**

1. How does the proposed massing and location of structures relate or respond to the pedestrian environment?
2. What are opportunities for pedestrian oriented spaces at the street level (plazas, outdoor dining)?
3. What are the key pedestrian connections?
4. How would the project engage pedestrians?

### **C. Open Space and Landscaping**

What are opportunities for landscaping and/or open space on the subject property?

**D. Minor Variations**

The Board may wish to give some initial, conceptual, feedback on the applicant's preferred option including minor variations from the 20' setbacks from Central Way and 4<sup>th</sup> Street. Any final direction on a request would occur following the detailed design and public comment occurring at the Design Response Conference.

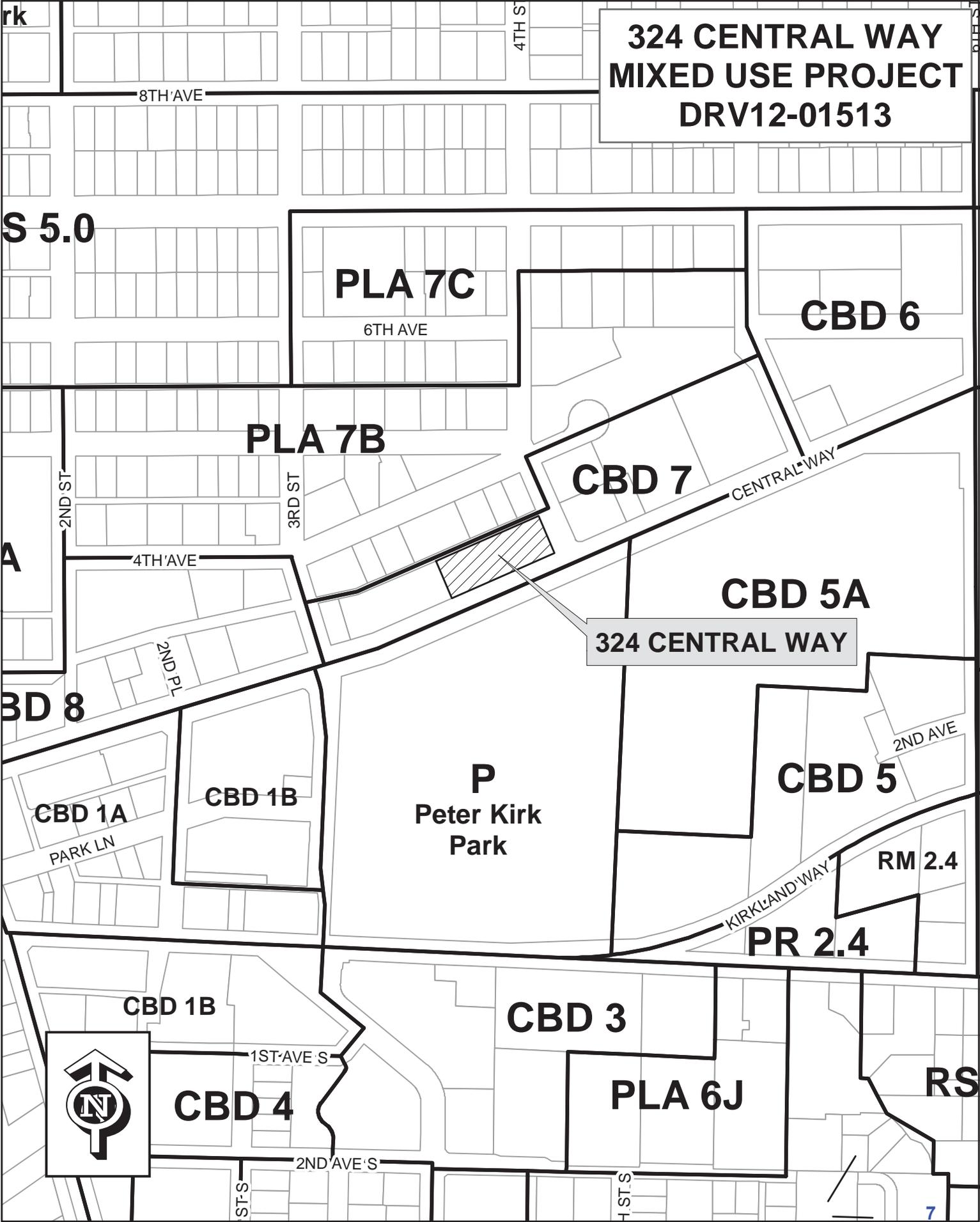
**VIII. ITEMS REQUIRED FOR DESIGN RESPONSE CONFERENCE**

The Design Review Board shall determine what models, drawings, perspectives, 3-D CAD/Sketchup model, or other application materials the applicant will need to submit with the design review application.

**IX. ATTACHMENTS:**

1. Vicinity Map
2. Applicant's Proposal
3. CBD 7 Use Zone Chart
4. Design Guidelines – Special Considerations for Downtown Kirkland





**324 CENTRAL WAY  
MIXED USE PROJECT  
DRV12-01513**

**324 CENTRAL WAY**



# Conceptual Design Conference Analytic Design Proposal Packet

324 Central Way, Kirkland WA 98033

A Proposed Apartment Development  
for Continental Properties, LLC.

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Conceptual Design Conference  
324 CENTRAL WAY

11/30/12

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## SHEET INDEX

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## PROJECT INFORMATION

<b>ADDRESS:</b>	324 CENTRAL WAY 98033	<b>BLDG. MAX. HT.:</b>	41'-0"
<b>PARCEL #:</b>	390010-1275	<b>BASE PLANE:</b>	48'-0"
<b>ZONING:</b>	CBD7 (CENTRAL BUSINESS)	<b>MAX. HEIGHT PLANE:</b>	89'-0"
<b>LAND USE:</b>	C (COMMERCIAL)	<b>FRONT SETBACK:</b>	20'-0" (0'-0" WITH RETAIL)
<b>FAR:</b>	NO REQUIREMENTS	<b>SIDE SETBACK:</b>	0'-0"
<b>LOT SIZE:</b>	27,458.75 SF	<b>REAR SETBACK:</b>	0'-0"
<b>LOT COVERAGE:</b>	80% (100% WITH RETAIL)	<b>RETAIL SETBACK:</b>	0'-0"

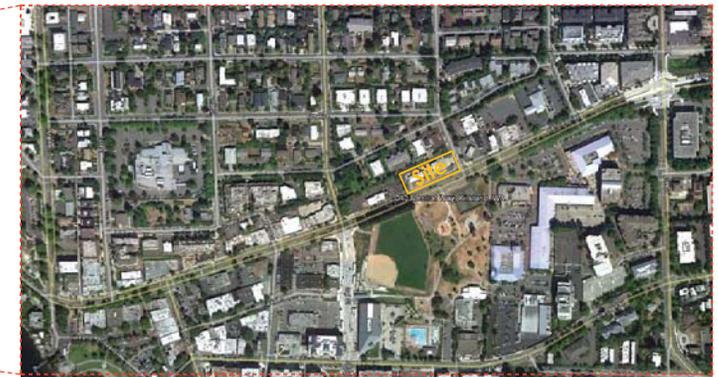
## LEGAL DESCRIPTION

390010-1275:  
(324 Central Way)  
LOTS 24, 25, 26, 27, 28, 29, 30, 31, 32, 33 AND 34, BLOCK 95, KIRKLAND TERRACE, ACCORDING TO THE PLAT THEREOF RECORDED IN VOLUME 21 OF PLATS, PAGE 42, IN KING COUNTY, WASHINGTON.

## VICINITY MAP



## SITE MAP



TITLE PAGE

CONCEPTUAL DESIGN CONFERENCE- 324 CENTRAL WAY

T1

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# DEVELOPMENT OBJECTIVES

## Existing site

The project site is located on busy Central Way, northwest of the intersection of Central Way and 4th street. The site is 27,459 square feet of contiguous land. Currently a gas station with a car wash facility and a coffee stand.

## Zoning and any other overlay designations

The project site is zoned CBD7. Land Use dedication is C (Commercial).

## Neighborhood

The project site is adjacent to the Peter Kirk Park. The neighborhood is a mix of high density residential (HDR 24) and commercial zoning (C), with buildings types ranging from three to four-story apartments/condominiums, single family houses, one story office, retail, and restaurant buildings. Directly south of the project site is the Peter Kirk Par, Peter Kirk Community Center and King County Public Library. Southeast of the project is the Kirkland Park Place shopping center. A few blocks Southwest of the project the the Kirkland downtown core and the city dock & public moorage. North of the project is a mixture of single family and multi-family residential building with a few office buildings. The Kirkland City Hall is located a few blocks northeast of the project.

## Development objectives

The owner's aim is to create a market rate rental community that appeals to a wide range of Kirkland city dwellers. The development will be designed in context with the distinguished character of the surrounding neighborhood in architectural elements, building scale, and massing. We are committed to using quality, long-lasting materials, and an aesthetic design that appeals to and blends with the neighborhood.

The building proposed is a 72-unit, four-level wood frame over concrete. The design will include a subterranean level of parking accessed via Central way. The parking level will include 119 parking stalls.

## SITE AERIAL MAP



## DEPARTURE REQUEST

#	Code Requirement	Departure Requested	Explanation for Departure
1	KZC Section 50.47 use zone chart- minimum required front yard setback= 20'-0"	Decrease allowed minimum required front setback from 20'-0" to 10'-0".	Due to Central way street improvement (property loss of 3'-8" along Central way plus 20'-0" required setback by code) on the south side of the property and 15'-0" buffer between commercial and residential zone on the north side of the property, created a long and narrow building footprint. To obtain the balance of the development goal and code requirements, the development team is forced to create a long single building with a shallow modulated flat elevation. This departure allows greater modulation flexibility and depth, make building frontages more consistent with the scale of surrounding buildings in downtown Kirkland.

**DEVELOPMENT OBJECTIVES**  
CONCEPTUAL DESIGN CONFERENCE- 324 CENTRAL WAY

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# CONTEXT ANALYSIS

## KEY

-  RETAIL
-  HOUSE
-  RESTAURANT
-  APARTMENTS/  
CONDO
-  OFFICE
-  MAJOR  
PEDESTRIAN  
SIDEWALK
-  PEDESTRIAN  
ORIENTED  
STREET
-  BUS STOP
-  ZONING



CONTEXT MAP & ZONING MAP  
CONCEPTUAL DESIGN CONFERENCE- 324 CENTRAL WAY

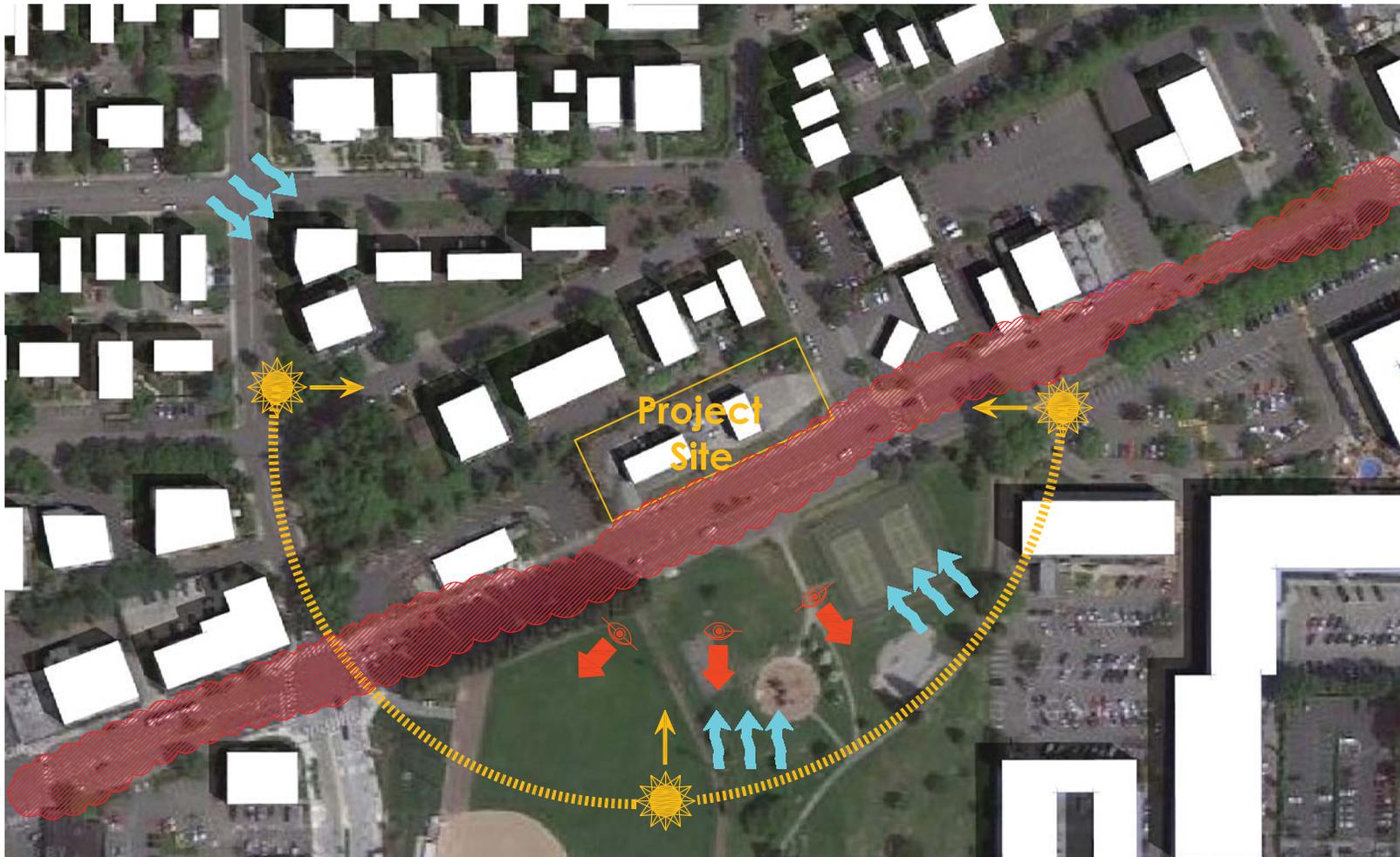
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# SITE ANALYSIS



## KEY

-  SUN
-  VIEW
-  NOISE TRAFFIC
-  PREVAILING WIND  
S (Jan-June)  
NW (July-Sept)  
SSE (Oct-Dec)



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**TRAFFIC MAP & SITE ANALYSIS**  
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# VICINITY PHOTOS



**Central Way- Looking Northeast**



**Central Way- Looking Northeast**



**Central Way- Looking Northeast**



**Central Way- Looking Southwest**



**Central Way- Looking Southwest**



**Central Way- Looking Southwest**



**4th Street- Looking Northwest**



**4th Street- Looking Southeast**



**Alley- Looking Southwest**

VICINITY PHOTOS  
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# SITE PHOTOS



**Project Site- Fronting Central Way**



**Project Site- Corner of Central Way & 4th Street**



**Project Site- Fronting 4th Street**

## SITE PHOTOS

CONCEPTUAL DESIGN CONFERENCE- 324 CENTRAL WAY

# A6

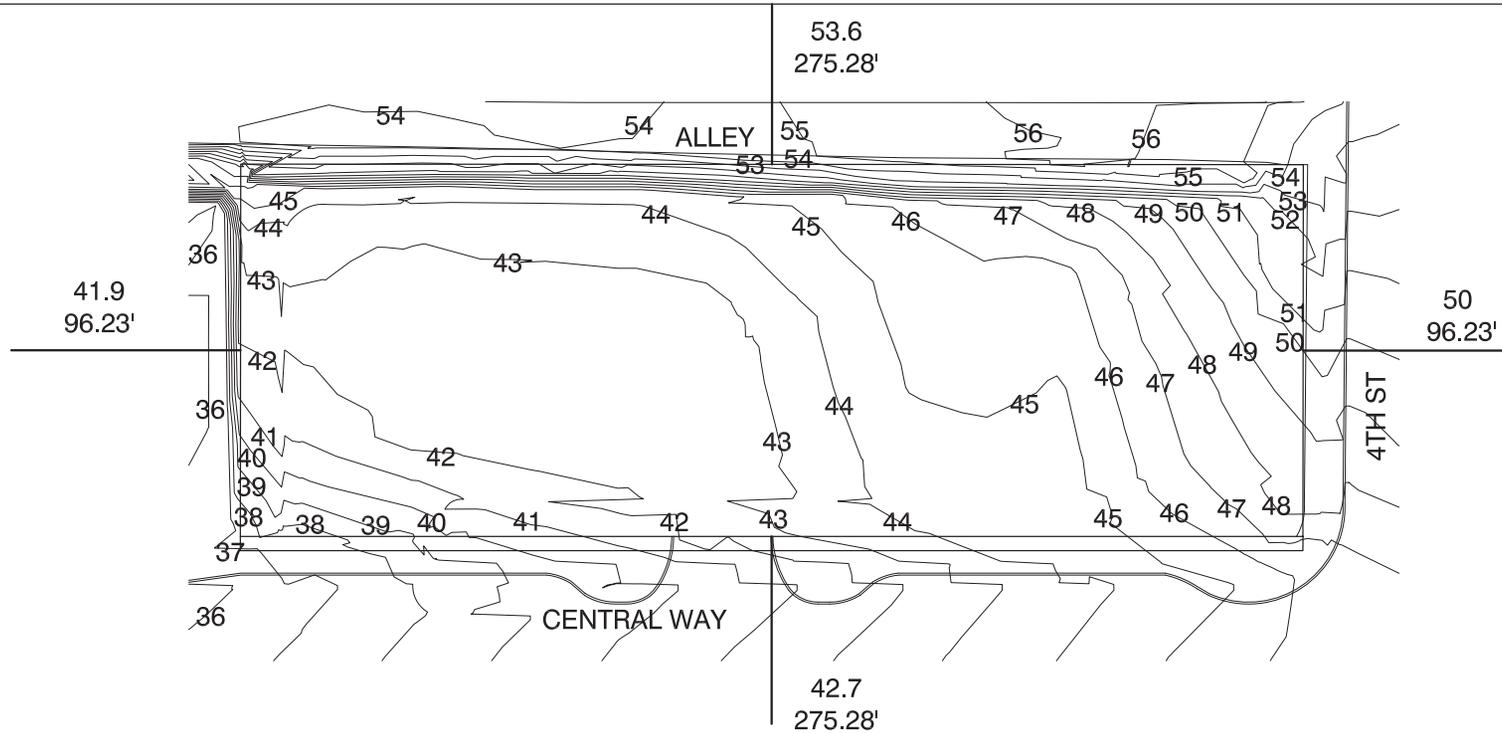
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# HEIGHT CALCULATION



$$\begin{aligned}
 \text{Avg. Grade} &= \frac{(53.6 \times 275.28) + (50 \times 96.23) + (42.7 \times 275.28) + (41.9 \times 96.23)}{275.28 + 96.23 + 275.28 + 96.23} \\
 &= \frac{14,755.01 + 4,811.5 + 11,754.46 + 4,032.04}{743.15} \\
 &= \frac{35,353}{743.15} = 47.58
 \end{aligned}$$

HEIGHT CALCULATION  
CONCEPTUAL DESIGN CONFERENCE- 324 CENTRAL WAY

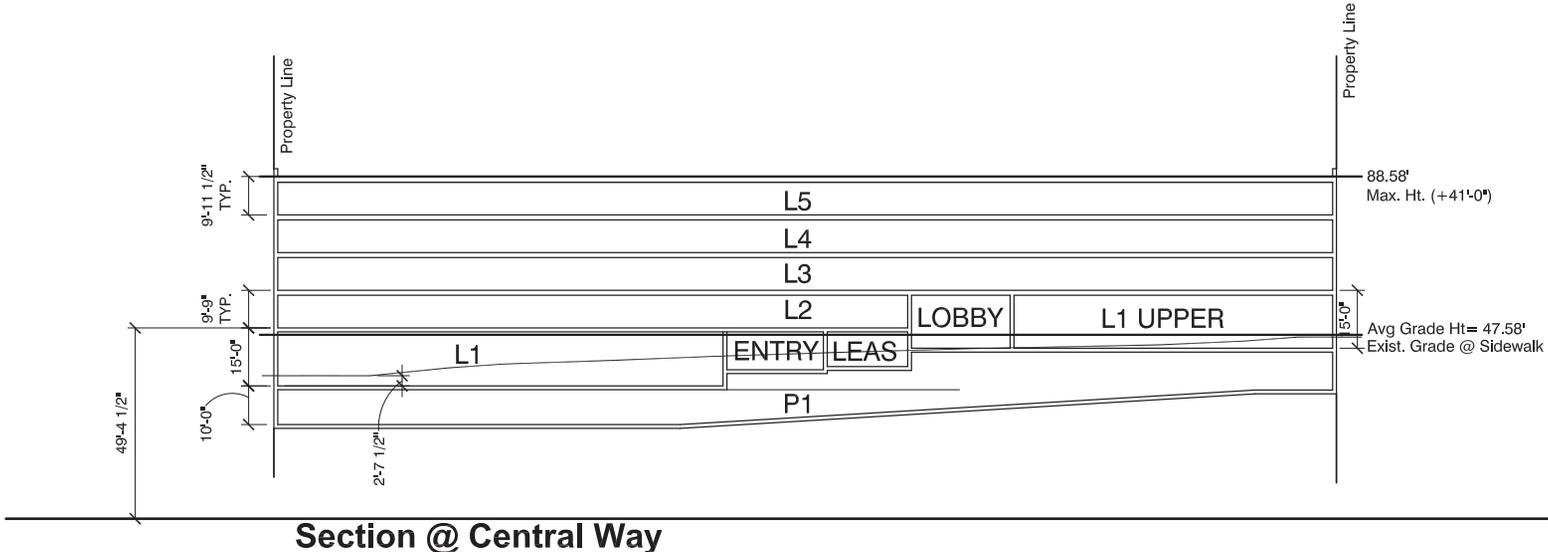
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# CONCEPTUAL SECTION



**Section @ Central Way**

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**CONCEPTUAL SECTION**  
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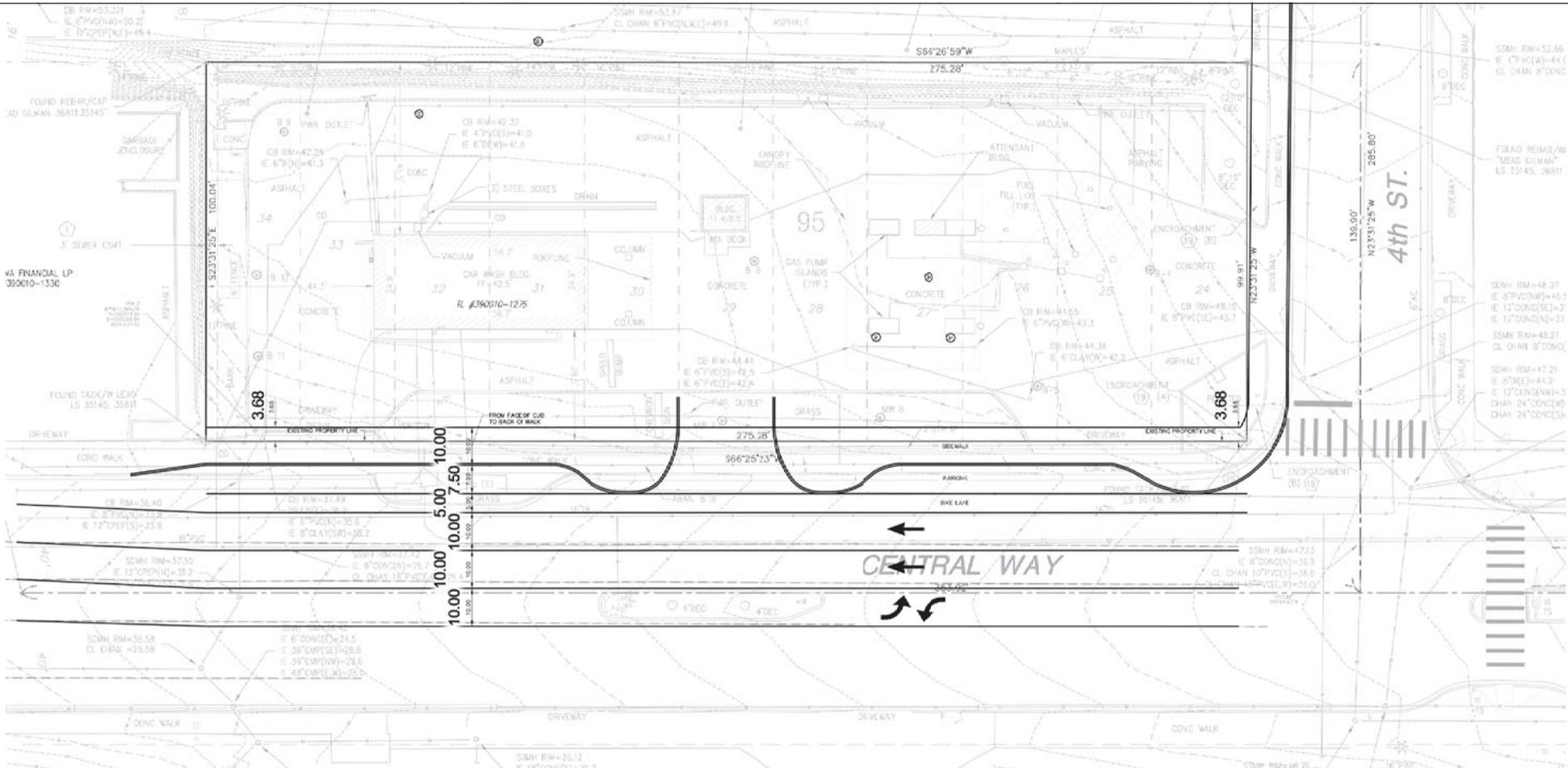
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# CENTRAL WAY STREET IMPROVEMENT- PLAN



**FINAL LAYOUT DESIGN**  
 324 CENTRAL WAY, KIRKLAND, WA 98033  
 10/17/12



P12-324-FINAL FROM 2012-10-17

**IMPROVEMENT PLAN**  
 CONCEPTUAL DESIGN CONFERENCE- 324 CENTRAL WAY

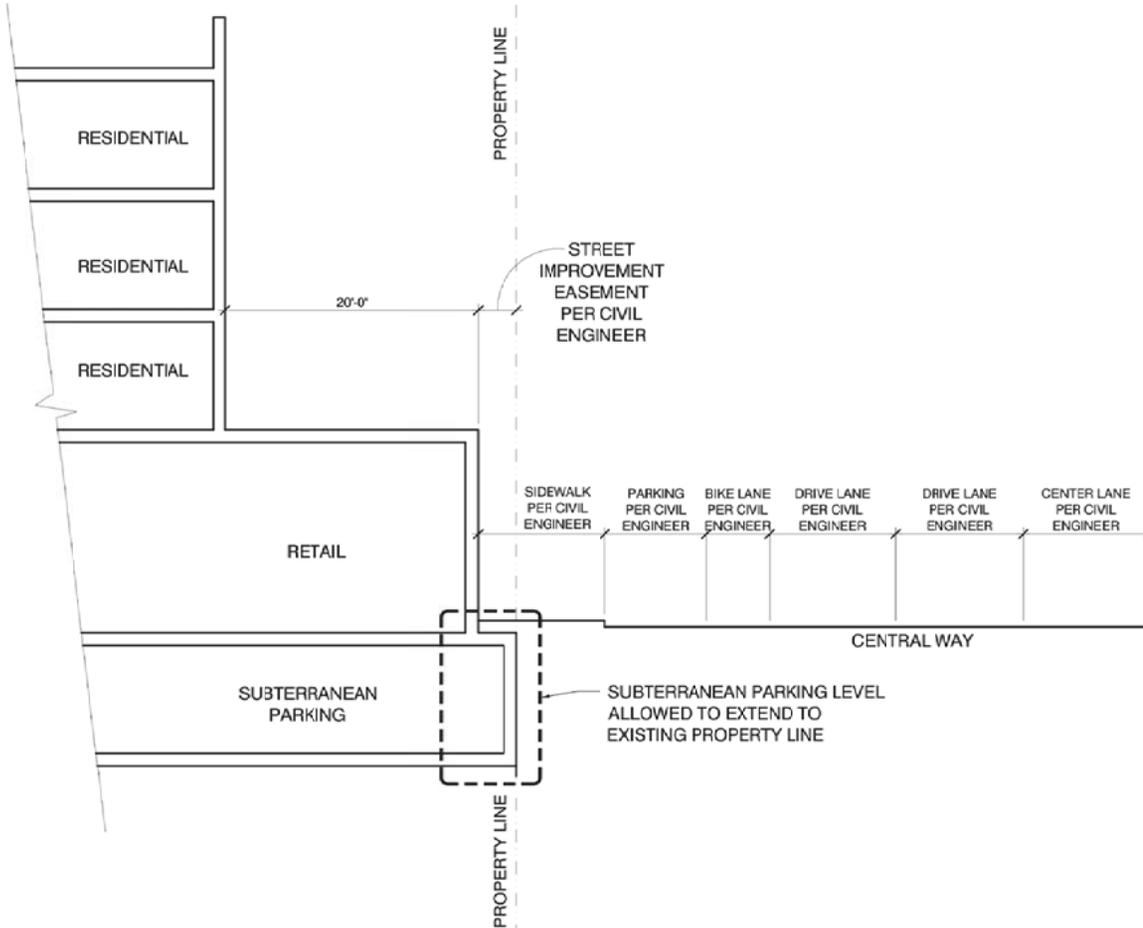
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# CENTRAL WAY STREET IMPROVEMENT- SECTION & LETTER



October 16, 2012

Charles Strazzara  
 Studio Meng Strazzara  
 2001 Western Avenue, Suite 200  
 Seattle, WA 98121

SENT VIA EMAIL – NO HARD COPY TO FOLLOW

RE: 4<sup>th</sup> and Central – Frontage Improvement Followup

Dear Mr. Strazzara:

This letter follows up on your questions regarding the required frontage improvements for redevelopment of the White Swan Carwash property in Downtown Kirkland. Public Works has the following comments:

- The 3.68' dedication (width to be verified by the project Architect/Engineer and confirmed by the City) for the sidewalk/frontage improvements may be contained within an easement. An easement would allow a portion of the parking garage to be located under the sidewalk without extending into the right-of-way.
- The on-street parking stall width can be reduced to 7.5 feet.
- The bike lane needs to be 5 feet in width.
- The 10-foot sidewalk width may include the 0.5 feet curb.

In regards to setbacks, the Zoning Code required front yard setback should be measured from the sidewalk/frontage improvement easement described above. The final setback and building configuration will be determined through the design review process since the Design Review Board has the authority to approve departures from the minimum setback requirements in the CBD.

If you have additional questions, please feel free to contact John Burkhalter, Development Engineering Supervisor at (425) 587-3846, [jburkhalter@kirklandwa.gov](mailto:jburkhalter@kirklandwa.gov) or myself at (425) 587-3255, [jregala@kirklandwa.gov](mailto:jregala@kirklandwa.gov).

Sincerely,

PLANNING AND COMMUNITY DEVELOPMENT

Jon Regala  
 Senior Planner

Cc: John Burkhalter, Development Engineering Supervisor

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IMPROVEMENT SECTION & LETTER  
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# DESIGN PROPOSAL- OPTION 1 (Code Compliant)

## View- Looking Northwest



## Option 1: Code Compliant

### Description:

- The building activates Central way and 4th St. at pedestrian level. With retail spaces and awnings at the street level, it creates a whether protected link to Central way and 4th street, dedicated pedestrian streets.
- Vehicular entrance to a below grade parking structure is located on Central way.
- The building footprint is an I-Shaped, double loaded configuration with long façade fronting Central way.
- The south side residential units will have frontage towards the Peter Kirk Park, The north side residence look out toward the adjacent residential zone with 15'-0" buffer.
- All units have plenty of natural light exposure.

### Advantages:

- Code compliant, no departure needed
- Unit frontage buffered 20'-0" from high traffic street- Central Way

### Challenges:

- Flat facade with Minimum facade modulation
- Narrow and long footprint makes the building out of scale with neighboring buildings