



CITY OF KIRKLAND

Planning and Community Development Department

123 Fifth Avenue, Kirkland, WA 98033

425.587-3225 - www.kirklandwa.gov

MEMORANDUM

To: Kurt Triplett, City Manager

From: Angela Ruggeri, Senior Planner
Janice Coogan, Senior Planner
Paul Stewart, Deputy Planning Director
Eric R. Shields, Planning Director

Date: November 3, 2011

Subject: ADOPTION OF CENTRAL HOUGHTON AND LAKEVIEW NEIGHBORHOOD PLANS (FILE NOS. ZON09-00016 and ZON07-00032)

RECOMMENDATION

Approve the following elements of the Central Houghton and Lakeview Neighborhood Plan updates by adopting the five attached Ordinances.

Central Houghton:

- Ordinance 0-4329 adopting Comprehensive Plan amendments including a new neighborhood plan chapter for the Central Houghton Neighborhood and related amendments to the Land Use Element and Implementation Strategies Chapter. The ordinance also includes a minor adjustment to the neighborhood boundary between the Central Houghton and Lakeview Neighborhoods.

Prior to adoption, determine final wording for policy CH 8.1 of the Central Houghton Neighborhood Plan (see page 2 of this memo).

- Ordinance 0-4330 adopting a Municipal Code amendment which adds the Central Houghton Neighborhood to the small lot single family regulations.

Lakeview:

- Ordinance 0-4331 adopting Comprehensive Plan amendments including a new neighborhood plan chapter for the Lakeview Neighborhood and related amendments to the Land Use Map (corresponding to legislative rezones in the Lakeview Neighborhood) and to the Implementation Strategies Chapter.

Prior to adoption, determine final wording for Policy L-11.3 of the Lakeview Neighborhood Plan (see page 4 of this memo).

- Ordinance 0-4332 adopting Municipal Code amendments which add the Lakeview Neighborhood to the small lot single family regulations and indicate that lot averaging and small lot single family allowances do not apply in the new PLA 3C zone. The ordinance also includes amendments to Section 3.30 adding Design Guidelines for the Yarrow Bay Business District.
- Ordinance 0-4333 adopting new and revised sections to the Zoning Code to implement the new Lakeview Neighborhood Plan with revisions to the Zoning Map to reflect proposed rezones.

FINAL WORDING DISCUSSION FOR TWO POLICIES

The Houghton Community Council (HCC) and Planning Commission (PC) were in agreement on all of the proposed amendments with only a few exceptions that were discussed at the City Council study session on September 20, 2011. At that time, the Council gave direction on all but two of the differences, one in the Lakeview plan and one in the Central Houghton plan. To help with the final decision on these two policy differences, staff was asked to provide information on view policies in other cities (see Attachment 1 for a summary memo) and on Kirkland Parks Department policies (see Attachment 2 for a memo from Michael Cogle, Deputy Director of the Parks Department).

The two policies are listed below along with the recommendations of the HCC and the PC. A staff recommendation follows each.

In order to identify the differences between the HCC and PC recommendations on these two remaining policies:

- Blue is used for HCC recommended wording.
- Brown is used for PC recommended wording.

Central Houghton Neighborhood Plan Policy CH-8.1:

HCC recommended Policy CH-8.1 text:

Policy CH-8.1: Provide opportunities for early and continuing community involvement in any expansion plans, modifications, or changes in use or intensity of ancillary uses for schools and places of worship.

Early community involvement is important in addressing issues that may affect the surrounding area and the neighborhood as a whole. Issues such as parking, and public safety should be taken into account when considering additional ancillary uses, expansion of facilities, or the addition of new facilities. Public and private views should also be taken into account when considering options for buffering schools and places of worship from adjacent residential uses.

PC recommended Policy CH-8.1 text:

Policy CH-8.1: Provide opportunities for early ~~and continuing~~ community involvement in any expansion plans, modifications, or changes in use ~~of intensity of ancillary uses~~ for schools and places of worship.

Early community involvement is important in addressing issues that may affect the surrounding area and the neighborhood as a whole. Issues such as parking, and public safety should be taken into account when considering additional ancillary uses, expansion of facilities, or the addition of new facilities. ~~Public and private views should also be taken into account when considering options for buffering schools and places of worship from adjacent residential uses.~~

Staff recommendation on Policy CH-8.1:

Policy wording:

The HCC accepted policy wording suggested by the Central Houghton Advisory Group. The advisory group was concerned with impacts on the neighborhood they had seen due to changes in schools and places of worship, particularly ancillary uses within these facilities. Additional regulations to implement the policy were not proposed.

The PC was concerned that the additional wording would require additional regulations and that the existing regulations relating to schools and places of worship are adequate. The proposed wording removes "continuing" from community involvement since the normal review process does not address ongoing involvement. It also removes "intensity of ancillary uses"

Staff agrees with the PC recommended wording, but proposes the minor edits shown below. By making the word "use" plural, the policy would implicitly cover all uses with a facility, including ancillary uses.

Policy CH-8.1: Provide opportunities for early community involvement in any expansion plans for, modifications to, or changes in uses within schools and places of worship.

Narrative wording:

The HCC included a sentence in the narrative for this policy that was to help protect views when considering buffering options for schools and places of worship.

The PC recommended removal of the sentence because they strongly disagree with the protection of private views and they felt that the issue was already adequately covered in the Zoning Code. Zoning Code section 95.46 allows for modification of landscape buffering standards if certain criteria are met including written approval by adjoining property owners.

Staff basically agrees with the PC recommendation, but feels it is reasonable to take the impacts of buffering into account. A new sentence is proposed which relates to the concept that buffering should be used to mitigate existing impacts, but not to create new impacts. Proposed wording is below:

Early community involvement is important in addressing issues that may affect the surrounding area and the neighborhood as a whole. Issues such as parking, and public safety should be taken into account when considering additional ancillary uses, expansion of facilities, or the addition of new facilities. Required buffering should be designed to minimize impacts to and be compatible with neighboring uses.

Lakeview Neighborhood Plan Policy L-11.3:

HCC recommended Policy L-11.3 text:

Policy L-11.3: Maintain wide, expansive views of Lake Washington through waterfront parks. Prevent view obstruction by vegetation or placement of structures.

A high priority for the neighborhood is to maintain the wide expansive views of Lake Washington and beyond, especially at waterfront parks. Ongoing maintenance of existing vegetation at parks to retain views of the Lake from Lake Washington Boulevard and properties to the east is a priority. Shoreline regulations also encourage planting of shoreline vegetation and trees. A balance must be achieved between shoreline restoration with the planting of vegetation with retaining views.

PC recommended Policy L-11.3 text:

Policy L-11.3: Maintain public views of ~~wide, expansive views~~ Lake Washington through waterfront parks. ~~Prevent view obstruction by vegetation or placement of structures.~~

~~A high priority for the neighborhood is to maintain the wide expansive views of Lake Washington and beyond, especially at waterfront parks. Ongoing maintenance of existing vegetation at parks to retain views of the Lake Washington and beyond from Lake Washington Boulevard and ~~properties to the east~~ is a priority. Shoreline regulations also encourage planting of shoreline vegetation and trees. A balance must be achieved between shoreline restoration using vegetation ~~with the planting of vegetation~~ while maintaining public views.~~

Staff recommendation on Policy L-11.3:

The main difference in the recommendations is the focus on public vs. private views. The HCC included protection of private views in its recommendation. The PC recommended wording that protected only public views.

Staff generally agrees with the PC recommendation and feels that the policy should be oriented toward public views. A sentence has been added to the narrative for the policy which acknowledges that consideration should be given to the impact of vegetation on neighboring properties. Proposed edits to the PC proposed wording are below.

Policy L-11.3: Maintain public views of Lake Washington through waterfront parks.

Ongoing maintenance of existing vegetation at parks to retain views of Lake Washington and beyond from Lake Washington Boulevard is a priority. Shoreline regulations also encourage planting of shoreline vegetation and trees. A balance

must be achieved between planting of vegetation for shoreline restoration using vegetation while and maintaining public views. Consideration also should be given to the impact of vegetation on views from nearby properties.

BACKGROUND DISCUSSION

The City Council held a study session and received the PC and HCC unanimous recommendation of approval for both the Central Houghton and Lakeview Neighborhood Plans on September 20, 2011. The video and all information provided for this study session, including the PC and HCC recommendations, is available for viewing at: <http://www.kirklandwa.gov/depart/council/Agendas/agenda092011.htm>

The Central Houghton Plan does not include any legislative rezones, but does have one Municipal Code amendment relating to small lots. The Lakeview Neighborhood plan includes the rezoning of several parcels, new Zoning and Municipal Code amendments and new Design Guidelines.

Council Direction on Differences in the Recommendation of the HCC and PC

The following differences in the recommendations of the HCC and the PC were discussed at the City Council study session at which time the Council directed staff to make changes for the final ordinances. The differences are summarized below along with the direction given by the City Council for the final ordinances.

Lakeview Neighborhood Plan Differences

- 1. Policy L-4.3: In the north portion of the neighborhood west of Lakeview Drive, allow multifamily use at medium density 12 dwelling units per acre.***

In the northern portion of the neighborhood west of Lakeview Drive, medium density residential is appropriate. Some parcels have multi-family development that was constructed under previous higher density development which is non-conforming under the current zoning. The Zoning Code (~~Section 162.60~~) contains the regulations governing nonconforming density.

PC revision: The Planning Commission recommended deleting the reference noted above.

City Council Direction: Delete reference to Section 162.60.

- 2.** The PC recommended the following minor wording adjustments to the HCC's recommendation for the YBD Design Guidelines Lighting Section:

A blend of lighting directed downward onto walking surfaces and up ~~into tree canopies is desired~~ **onto trees can be used** to define these spaces. Care should be taken to avoid spilling excessive glare into adjacent properties **and to avoid sky directed light pollution.**

City Council Direction: Incorporate the PC's changes into the final ordinance.

Central Houghton Neighborhood Plan Differences

1. Policy CH-4.1: *Allow a variety of development styles that provide housing choice in low density areas.*

Providing housing options for a wide spectrum of households is an important value to support and encourage. Alternative housing provides more housing choice to meet changing housing demographics such as smaller households and an aging population. Allowing design innovations can help lower land and development costs and improve affordability. Compatibility with the predominant traditional detached single-family housing style in the neighborhood will determine the acceptance of housing alternatives. **HCC last sentence:** Alternative housing types such as cottage, compact single-family, accessory dwelling units, and clustered dwellings are appropriate options to serve a diverse population and changing household size and composition.

PC revision to last sentence: Alternative housing types such as cottage, compact single-family, two/three unit homes, accessory dwelling units, and clustered dwellings are appropriate options to serve a diverse population and changing household size and composition.

City Council Direction: Use HCC wording which does not include "two/three unit homes".

2. Policy CH-12.2: *Support future development of the Eastside Rail Corridor as a multipurpose trail for pedestrian and bicycles with access points along the corridor.*

The unused BNSF railroad right-of-way, known as the Eastside Rail Corridor, provides an opportunity for a bicycle, pedestrian and rail transportation corridor. **HCC wording:** Pedestrian and bicycle transportation is the highest priority, but regardless of the function of the Corridor it should be designed so that it will:

- Serve as a gateway to the City.
- Provide neighborhood pedestrian and bicycle connections, with the highest priority access points at NE 52nd, NE 60th and NE 68th Streets.
- Be compatible with adjacent neighborhoods.
- Ensure a high degree of safety.
- Show environmental stewardship.

PC wording: Pedestrian and bicycle transportation is a high priority, but regardless of the function of the Corridor it should be designed so that it will:

City Council Direction: Use PC wording "a high priority".

3. Policy CH-13.1: *Pursue acquisition of property and partnerships with schools and other institutions in Central Houghton.*

HCC: The City should seek opportunities to acquire land to expand parks as properties adjacent to existing parks become available. It is also important to provide and maintain a diversity of park recreation types for the neighborhood. In addition, street ends should be developed and expanded into park and open space areas for public enjoyment.

PC revision: The City should seek opportunities to acquire land to expand parks as properties adjacent to existing parks become available. It is also important to provide and maintain a diversity of park recreation types for the neighborhood. The City should pursue cooperative agreements for use of the facilities at Northwest University and International Community School. In addition, street ends should be developed and expanded into park and open space areas for public enjoyment.

City Council Direction: Prepare alternative wording that addresses institutions and schools more generally. The following sentence was drafted by staff and put in the plan for the final ordinance:

The City should pursue cooperative agreements for joint use of the facilities at schools and other institutions.

Other Minor Wording Differences: Several other wording differences occurred because the PC met to make their recommendation after the HCC had made theirs. The PC saw some of the HCC recommended wording for the first time and so had these minor edits to propose in an attempt to clarify the wording further. Edits by the PC are underlined below. As directed by the Council at the study session, these changes have been incorporated into the final ordinance.

1. **Goal CH-7:** Support the transition of the Houghton Center into a pedestrian-oriented mixed use development, including retail, with office and/or residential uses and other compatible uses.
2. **Policy CH-9.2:** Require all development in PLA 1 to conform to an approved master plan.

The master plan approved in 1999 is the guiding document for Northwest University in PLA 1. Any variations from this master plan must be reviewed and approved by the City.

3. **Policy CH-10.1:** Mitigate negative ~~minimize~~ impacts of commercial and institutional development on residential areas to protect neighborhood character.

Regulating building height, building mass, building placement, vehicular access and traffic impacts and/or providing landscape buffers can be used to reduce negative impacts of commercial and institutional uses on surrounding residential uses. Mitigate adverse impacts through environmental review, development regulations and appropriate conditions imposed through development review.

4. Description of ICS in the Parks section of the plan.

International Community School (ICS) is located at the north end of the neighborhood. This approximately 11- acre site provides recreation space for the neighborhood including both outdoor and indoor recreation space ~~on a limited~~ basis. All facilities on the property are maintained by LWSD.

FINAL STEPS

A postcard was sent to all property owners and residents in both the Lakeview and Central Houghton Neighborhoods notifying them of the dates that the City Council and HCC would be taking final action on the plans.

The HCC is scheduled to take final action at its meeting on November 28, 2011.

Attachments:

1. Research on view regulations for other cities
2. Memo from Michael Cogle dated October 24, 2011

cc: File No. ZON09-00016
File No. ZON07-00032
Houghton Community Council
Planning Commission
Central Houghton Advisory Group
Lakeview Advisory Group

VIEW PROTECTION

The Municipal Research and Services Center (MRSC) of Washington has gathered information on Scenic View Protection and made it available on their site.

<http://www.mrsc.org/subjects/planning/view.aspx>

Cities and counties may consider aesthetics in regulating the use of land per MRSC. Zoning for aesthetics includes **view protection regulations**, historic preservation ordinances, design review standards, sign controls, and **tree protection** measures. MRSC recommends that the city or county support any height restrictions or other view protection provisions on as many grounds as possible, since it is not clear in this state if aesthetics alone will justify a view protection ordinance. Sample code sections have been included below.

Clyde Hill Municipal Code – *View Obstruction & Tree Removal*

17.38.025 - Preservation of views

In order to resolve the conflict between trees and views as defined herein, no tree, (except as exempted under CHMC 17.38.020(C)) whether native or planted and whenever or wherever planted in Clyde Hill is immune from complaint if the height of the tree unreasonably obstructs the view or access to sunlight of a neighbor. No owner of a tree shall allow the same to grow or tolerate or permit the presence on the owner's premises of such a tree. (Ord. 805 § 1, 1999; Ord. 648 § 1, 1991)

The code goes on to explain how a property owner can file a complaint in order to resolve the conflict between trees and views.

Section CHMC 17.38.020(C): *referenced above provides a definition of exempt trees:*

“Tree” a woody perennial plant which usually, but not necessarily, has a single trunk and a height of 15 feet or more, or has a circumference of 20 inches measured at 24 inches above the ground and for purposes of this chapter includes any shrubbery capable in its normal growth to exceed a height of eight feet; references herein to “tree” shall include the plural; provided, however, any tree whose age antedates the incorporation of the city shall be deemed a “historic” tree and be exempted from this chapter. For purposes of this chapter “tree” shall include any plant material, or shrubbery planted or growing in a dense continuous line so as to form a thicket barrier or naturally grown fence. (See CHMC 17.37.020 for regulations pertaining to naturally grown fences.)

Kent Municipal Code – *View corridor protection regulations on hillside development*

These regulations are related to buildings only. Trees are not regulated.

15.08.060 View corridor protection regulations on hillside development.**A. Purpose.**

1. The visual environment of the city of Kent is strongly characterized by scenic vistas to the Green River valley from the slopes of the East and West Hills which frame the valley. The purpose of the view regulations set out in this section is to regulate the height and location of buildings on hillsides in order to protect view corridors to and from these hillsides which are on the visual forefront of the city and encourage placement of residences in ways which are compatible with the preservation of such public vistas.

2. The criteria of this section establish procedures for determining heights and locations of buildings on hillsides so that views will not be destroyed by site development plans that fail to consider the topography of the lot and the orientation of adjacent properties. The regulations of this section shall be interpreted and enforced at the time of development plan review. The regulations of this section shall not, however, prohibit construction of any building with a building height of twenty-five (25) feet or less, as measured from natural or finished grade, whichever is lower, on any legal lot of record.

La Conner Municipal Code – *regarding view protection***10.10.225 View protection – Goals.**

- (1) Protect the visual quality of the town's shoreline.
- (2) Maintain view corridors to and from the Swinomish Channel and adjacent shoreline features.

[Ord. 705 § 2(7.2.2.A), 1997.]

10.10.230 View protection – Policies.

- (1) Development uses and activities on or near the shoreline should protect the public's visual access to the water.
- (2) Public views from the shoreline upland areas should be enhanced and preserved.

[Ord. 705 §2(7.2.2.B), 1997.]

The applicable regulations say that shoreline uses and activities shall be designed and operated to provide visual access to the water and shorelines. They also state that public lands such as street ends, rights-of-way and utilities should provide visual access to the water and shoreline. Trees are not specifically referenced.

Lake Forest Park Municipal Code – *Public view protection*

*The Lake Forest Park Code states that, “Adopted land use codes attempt to protect private views through height and bulk controls and other zoning regulations but it is impractical to protect **private** views through project-specific review.” (#4 below)*

16.06.200 Public view protection.

A. Policy Background.

1. Lake Forest Park has a magnificent natural setting of greenery, mountains and water; visual amenities and opportunities are an integral part of the city’s environmental quality.
2. The city has developed particular sites for the public’s enjoyment of views of mountains, water and skyline.
3. Obstruction of public views may occur when a proposed structure is located in close proximity to the street property line, when development occurs on lots situated at the foot of a street that terminates or changes direction because of a shift in the street grid pattern, or when development along a street creates a continuous wall separating the street from the view.
4. Adopted land use codes attempt to protect private views through height and bulk controls and other zoning regulations but it is impractical to protect private views through project-specific review.

B. Policies.

1. It is the city’s policy to protect public views of significant natural and human-made features: the Olympic and Cascade Mountains, and major bodies of water.
2. Mitigating measures may include, but are not limited to: a. Requiring a change in the height of the development; b. Requiring a change in the bulk of the development; c. Requiring a redesign of the profile of the development; d. Requiring on-site view corridors or requiring enhancements to off-site view corridors; e. Relocating the project on the site; f. Requiring a reduction or rearrangement of walls, fences, or plant material; and g. Requiring a reduction or rearrangement of accessory structures including, but not limited to towers, railings and antennae. (Ord. 426§ 6(15), 1989)

Redmond Zoning Code – *Public View Corridors and Gateways*

Public view corridors are discussed and displayed in maps. There is also a discussion of appropriate landscape materials for each of the public view corridors.

21.42.060 Identification of Citywide Public View Corridors

Map 42.1 identifies areas from which there are significant views from public spaces. Each area identified corresponds to a more detailed map/diagram and a narrative description that may be supplemented with photos/diagrams of the views to be preserved together with design guidelines to be used to protect that particular view.

San Juan County Code – *View Protection*

“C” Below protects natural vegetation from excess removal for view access to the shoreline.

18.50.140 View protection.

- A. Shoreline uses and activities must be designed and operated to avoid blocking or adversely interfering with visual access from public areas to the water and shorelines except as provided for in SJCC 18.50.130. (section 18.50.130 has to do with Vegetation management for shorelines).
- B. The vacation of public road ends and rights-of-way which provide visual access to the water and shoreline may be allowed only in accordance with RCW 36.87.130 and local rules.
- C. In providing visual access to the shoreline, the natural vegetation shall not be excessively removed either by clearing or by topping.
- D. In order to limit interference with views from surrounding properties to the shoreline and adjoining waters, development on or over the water shall be constructed only as far seaward as necessary for the intended use.
- E. Development on or over the water must be constructed of materials that are compatible in color with the surrounding area.



CITY OF KIRKLAND
 Department of Parks & Community Services
 505 Market Street, Suite A, Kirkland, WA 98033 425.587.3300
 www.kirklandwa.gov

MEMORANDUM

To: Paul Stewart, Deputy Director, Planning & Community Development Dept.

From: Michael Cogle, Deputy Director

Date: October 24, 2011

Subject: Lakeview Neighborhood Plan: View Issues and Waterfront Parks

The Parks and Community Services Department has been asked to provide an additional perspective regarding proposed language included in the draft update to the Lakeview Neighborhood Plan. Specifically, we have been asked to consider the policy and management issues surrounding the differing text suggested by the Houghton Community Council (HCC) and the Planning Commission (PC) regarding maintaining views of Lake Washington and its surrounding shoreline, as detailed in proposed Policy L-11.3:

HCC recommended Policy L-11.3 text:

Policy L-11.3: Maintain wide, expansive views of Lake Washington through waterfront parks. Prevent view obstruction by vegetation or placement of structures.

A high priority for the neighborhood is to maintain the wide expansive views of Lake Washington and beyond, especially at waterfront parks. Ongoing maintenance of existing vegetation at parks to retain views of the Lake from Lake Washington Boulevard and properties to the east is a priority. Shoreline regulations also encourage planting of shoreline vegetation and trees. A balance must be achieved between shoreline restoration with the planting of vegetation while still retaining views.

PC recommended Policy L-11.3 text:

The Planning Commission recommended alternative text to emphasize that it is public views not private views that should be protected:

Policy L-11.3: Maintain public views of wide, expansive views of Lake Washington through waterfront parks. ~~Prevent view obstruction by vegetation or placement of structures.~~

~~A high priority for the neighborhood is to maintain the wide expansive views of Lake Washington and beyond, especially at waterfront parks.~~ Ongoing maintenance of existing vegetation at parks to retain views of the Lake Washington and beyond from Lake Washington Boulevard ~~and properties to the~~

~~east~~ is a priority. Shoreline regulations also encourage planting of shoreline vegetation and trees. A balance must be achieved between shoreline restoration using vegetation with the planting of vegetation with retaining while maintaining public views.

At the outset let me affirm that the Parks and Community Services Department appreciates and takes pride in the role the city's waterfront parks play in defining community character and contributing to the defined vision of Kirkland *"as an attractive, vibrant, and inviting place to live, work and visit. Our lakefront community, with its long shoreline, provides views and access to the lake and is a destination place for residents and visitors (Comprehensive Plan)."*

As well, we strive to be good neighbors with adjacent property owners and are attuned to the daily dynamics of managing our vital public resources within a complex urban environment.

For our Department the following issues arise from the proposed language:

Maintenance of existing vegetation. The suggested language by the Houghton Community Council to maintain vegetation so as to *"retain views of the Lake from.....properties to the east"* raises three concerns which we believe should be considered.

One, the language suggests that the existing height and width of existing vegetation in waterfront parks should be maintained by the City at essentially current measurements in order to ensure that existing views from adjacent private properties are retained. The implication is that upon adoption of the Lakeview Neighborhood Plan a baseline is established for measuring whether or not the City is adhering to the language suggested by the policy. We believe that this creates a difficult maintenance standard to meet and in fact runs counter to best management practices (BMP's) particularly for tree maintenance (e.g. the practice of "topping" trees has long-since been discontinued).

Second, the use of the word "retain" also suggests that the *specific* private views of *specific* property owners to the east are to be protected. The reality is that some private properties currently enjoy a greater degree of scenic views through waterfront parks than others. By proposing language which emphasizes retention of certain existing private views we are concerned that the City could be severely constrained in its ability to maintain, renovate, and/or redevelop waterfront parks to meet the needs of the larger community over time.

Finally, we would like to point out that the proposed language would apply to "waterfront parks", of which the Lakeview neighborhood currently has three – Marsh Park, Houghton Beach Park, and Yarrow Bay Wetlands. Implementation of a proposed policy intended to ensure retention of existing private views for properties east of the Yarrow Bay Wetlands would be a particular challenge given the park's natural characteristics and habitat value. The maintenance regimen for this park is far different than that of the highly-used Marsh Park and Houghton Beach Park. Some clarification of the proposed policy as it relates to Yarrow Bay Wetlands seems warranted.

cc: Jennifer Schroder, Director
Jason Filan, Park Maintenance and Operations Manager

ORDINANCE NO. O-4329

AN ORDINANCE OF THE CITY OF KIRKLAND RELATING TO COMPREHENSIVE PLANNING AND LAND USE AND AMENDING THE COMPREHENSIVE PLAN, ORDINANCE 3481 AS AMENDED, TO IMPLEMENT THE CENTRAL HOUGHTON NEIGHBORHOOD PLAN UPDATE; AMEND THE LAND USE ELEMENT AND IMPLEMENTATION STRATEGIES CHAPTERS; ADJUST THE NEIGHBORHOOD BOUNDARY LINE BETWEEN CENTRAL HOUGHTON AND LAKEVIEW; AND APPROVE A SUMMARY FOR PUBLICATION, FILE NO ZON09-00016.

WHEREAS, the City Council has received a recommendation from the Kirkland Planning Commission and the Houghton Community Council to amend certain portions of the Comprehensive Plan for the City, Ordinance 3481 as amended, all as set forth in that certain report and recommendation of the Planning Commission and Houghton Community Council for the Central Houghton Neighborhood dated September 7, 2011 and bearing Kirkland Department of Planning and Community Development File No. ZON09-00016; and

WHEREAS, prior to making said recommendation the Planning Commission and Houghton Community Council, following notice thereof as required by RCW 35A.63.070 and RCW 36.70A.035, held public hearings on June 23, 2011 on the amendment proposals and considered the comments received at said hearing; and

WHEREAS, pursuant to the State Environmental Policy Act (SEPA), there has accompanied the legislative proposal and recommendation through the entire consideration process, a SEPA Addendum to Existing Environmental Documents, issued on June 15, 2011 by the responsible official pursuant to WAC 197-11-625; and

WHEREAS, in regular public meeting the City Council considered the environmental documents received from the responsible official, together with the report and recommendation of the Planning Commission and Houghton Community Council;

NOW, THEREFORE, the City Council of the City of Kirkland do ordain as follows:

Section 1. Comprehensive Plan Text and Graphics amended: The following specific portions of the text of the Comprehensive Plan, Ordinance 3481 as amended, be and they hereby are amended to read as follows:

A. Section I. Introduction:

Amendment to Figure I-2: City of Kirkland Neighborhoods as set forth in Exhibit A attached to this ordinance and incorporated by reference.

- B. Section VI. Land Use Element:
Map amendment to Figure LU-2: Commercial Areas as set forth in Exhibit B attached to this ordinance and incorporated by reference.
- C. Section VI. Land Use Element:
Text amendment to Policy LU-5.8 as set forth in Exhibit C attached to this ordinance and incorporated by reference.
- D. Section XIV. Implementation Strategies:
Amendment to Table IS-1: Implementation Tasks as set forth in Exhibit D attached to this ordinance and incorporated by reference.
- E. Section XV.B Central Houghton Neighborhood Plan:
Updated Central Houghton Neighborhood Plan as set forth in Exhibit E attached to this ordinance and incorporated by reference.

Section 2. If any section, subsection, sentence, clause, phrase, part or portion of this ordinance, including those parts adopted by reference, is for any reason held to be invalid or unconstitutional by any court of competent jurisdiction, such decision shall not affect the validity of the remaining portions of this ordinance.

Section 3. This ordinance shall be in full force and effect January 1, 2012, which will be more than five days from and after its passage by the City Council and publication pursuant to Kirkland Municipal Code 1.08.017, in the summary form attached to the original of this ordinance and by this reference approved by the City Council as required by law.

Section 4. A complete copy of this ordinance shall be certified by the City Clerk, who shall then forward the certified copy to the King County Department of Assessments.

Passed by majority vote of the Kirkland City Council in open meeting this _____ day of _____, 2011.

Signed in authentication thereof this _____ day of _____, 2011.

MAYOR

Attest:

City Clerk

Approved as to Form:

City Attorney

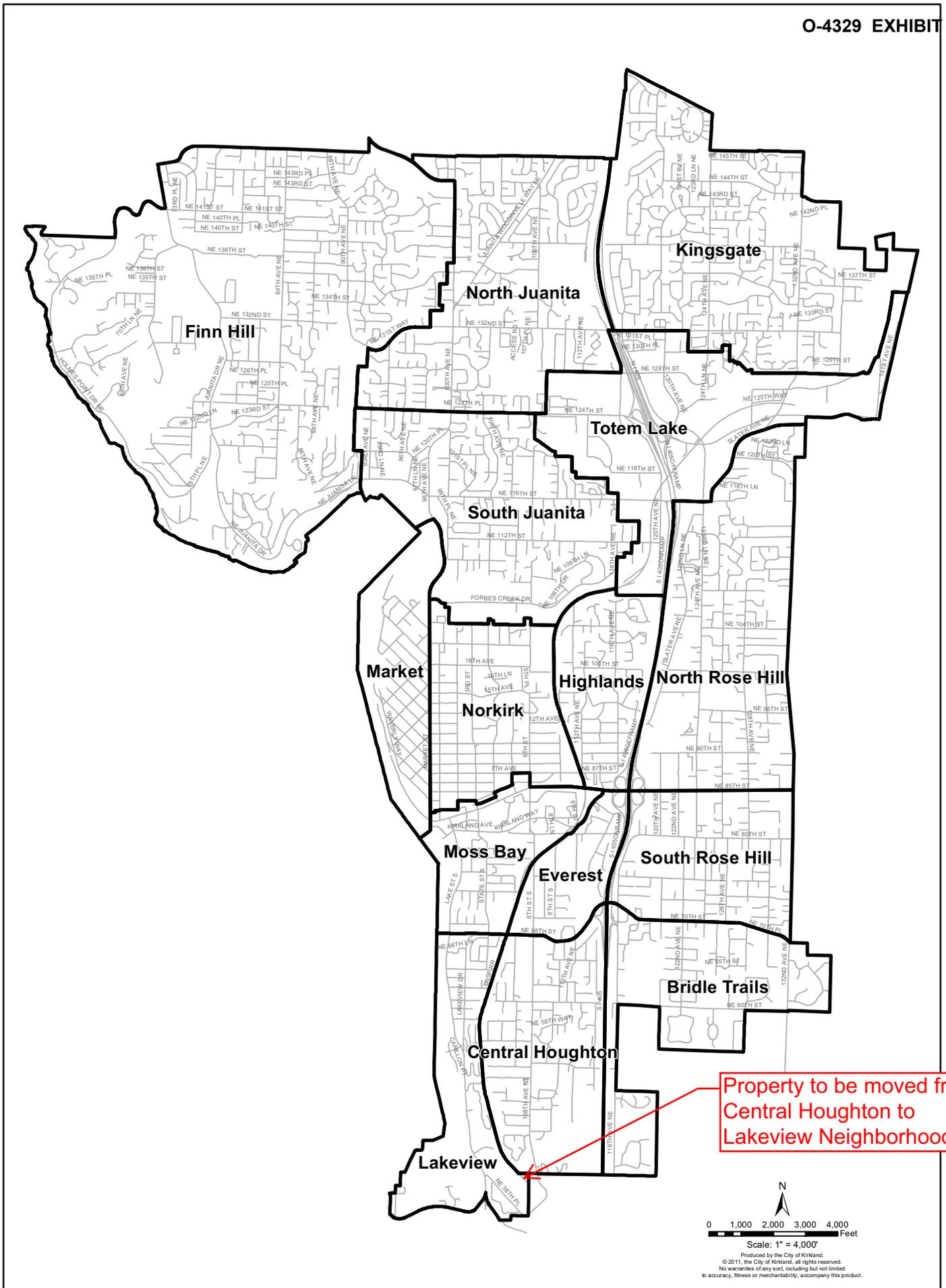


Figure I-2: City of Kirkland Neighborhoods

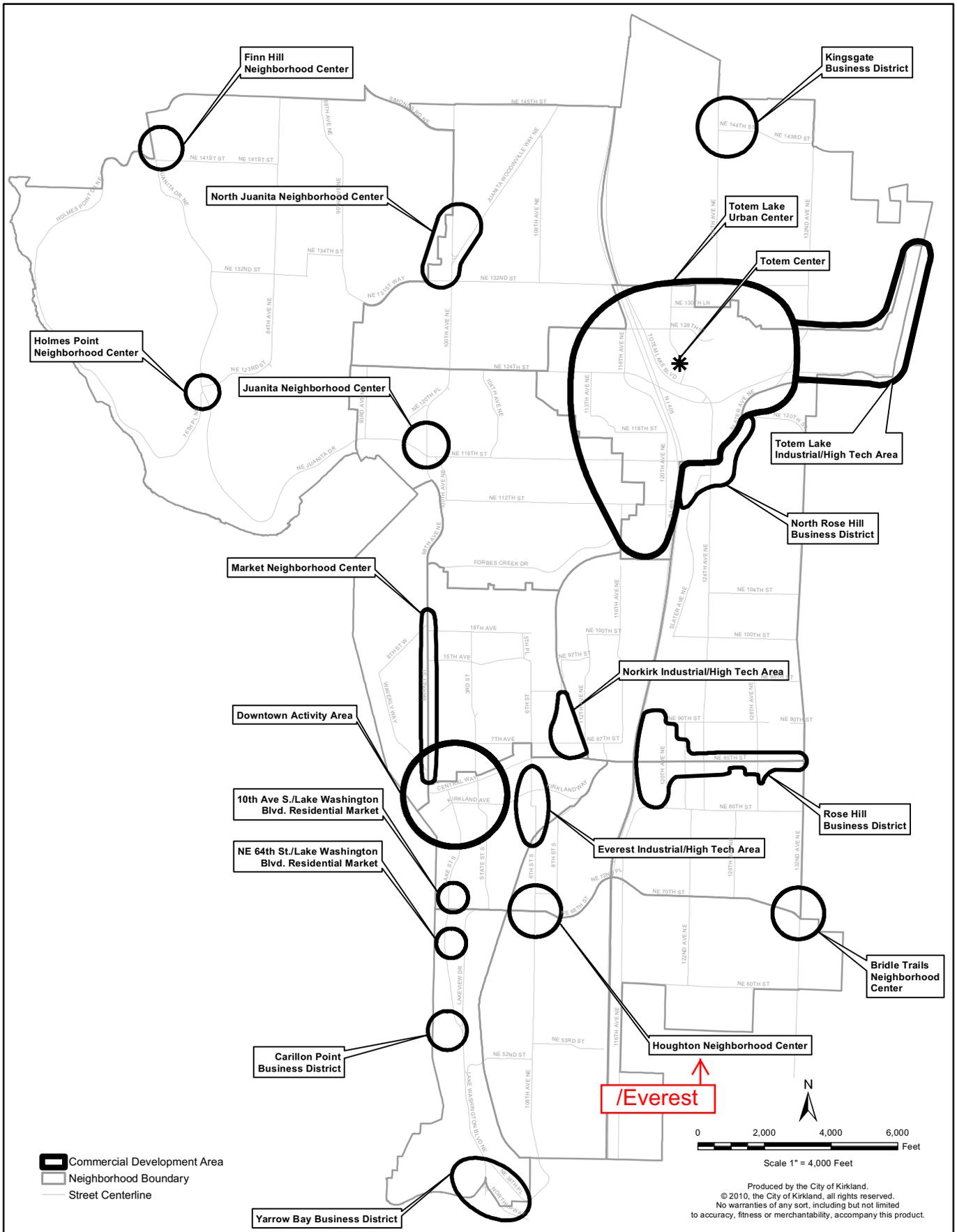


Figure LU-2: Commercial Areas

/Everest

VI. LAND USE

LU-5.8: *Promote development within the Bridle Trails, Houghton, and Juanita Neighborhood Centers that becomes part of the neighborhood in the way it looks and in the functions it serves.*

Neighborhood centers provide services to surrounding residential neighborhoods so that residents may shop close to home. They also may function as the focal point for a community. Because of these important ties to their neighborhood, neighborhood centers should develop in ways that provide goods and services needed by the local residents, enhance physical connections to the surrounding neighborhoods, foster good will and provide an opportunity for people to mingle and converse.

Policy LU-5.9: *Allow residential markets, subject to the following development and design standards:*

- ◆ *Locate small-scale neighborhood retail and personal services where local economic demand and local citizen acceptance are demonstrated.*
- ◆ *Provide the minimum amount of off-street parking necessary to serve market customers.*
- ◆ *Ensure that building design is compatible with the neighborhood in size, scale, and character.*

The intent of this policy is to permit small individual stores or service businesses in residential areas on a case-by-case basis. These businesses should cater to nearby residents, be oriented to pedestrian traffic, and require very little customer parking. They should be designed and located in a manner that is compatible with adjacent residences and that will not encourage the spread of commercial uses into residential areas. They should be located where local economic demand and neighborhood acceptance can be demonstrated.

Goal LU-6: *Provide opportunities for a variety of employment.*

Policy LU-6.1: *Provide opportunities for light industrial and high technology uses.*



405 Corporate Center

While Kirkland is not interested in recruiting heavy industry, the City is supportive of existing industrial enterprises and wants to encourage new high-technology businesses to locate here.

Policies that encourage residential and retail encroachment in industrial areas drive up the cost of land and promote conflicts which may force displacement of industrial operations. The strategy in the Land Use Element is to maintain industrial uses, while acknowledging that, in some parts of the City, industrial lands may be considered for conversion to other land uses.

Recognizing that each industrial area in the City has its own distinct character, the range of uses may vary between districts and may include some nonindustrial uses. Factors which should be taken into account when determining appropriate land uses include existing uses, surrounding uses, the local transportation system, and the effect on maintenance of primary jobs in the local job market.

XIV. IMPLEMENTATION STRATEGIES

Table IS-1
Implementation Tasks

TASK	PRIORITY
GENERAL ELEMENT	
<u>Project</u> G.1 In 2022, review “time capsule,” located in the City Hall vault, containing the “Kirkland 2022 – Community Conversations” video and the citizen responses.	
<u>Ongoing</u> G.2. Annually update the Comprehensive Plan. G.3. Update the neighborhood plans.	
COMMUNITY CHARACTER ELEMENT	
<u>Projects</u> CC.1. Review and update Zoning Code Chapter 100: Signs. CC.2. Establish incentives to private owners for preservation, restoration, redevelopment and use of significant historic buildings and sites. CC.3. Consider public improvements for historic districts to help encourage preservation.	
<u>Ongoing</u> CC.4. Add Carillon Woods to Community Landmarks - Table CC-1	
CC.4 5 When the neighborhood plans are updated, consider design principles for new structures that respect the scale, massing, and design of existing adjacent buildings and the neighborhood context. CC. 5 6 Incorporate historic preservation into neighborhood plans as they are updated including: <ul style="list-style-type: none"> ◆ A list of each neighborhood’s historic structures and sites. ◆ Design principles for areas where historic structures are clustered. 	
NATURAL ENVIRONMENT ELEMENT	
<u>Projects</u> NE.1. Update the City’s Shoreline Master Program.	**
NE.2. Assess and amend the Comprehensive Plan, City codes, resource management practices, and other City activities as needed for consistency with the Endangered Species Act, State shoreline rules, and other natural resource requirements.	**
NE.3. Promote removal of fish barriers.	
NE.4. Amend the Zoning Code to specify criteria and procedures for handling clearing and grading violations in sensitive areas and their buffers.	**
NE.5. Study and implement methods to preserve and, where feasible, increase pervious surface in Kirkland.	*
NE.6. Review Kirkland Municipal Code, Zoning Code Definitions Chapter, and Comprehensive Plan to identify inconsistencies in natural system terminology.	*
NE.7. Review Zoning Code regulations for protection of existing landscaping and trees.	*
NE.8. Develop a City street tree program for appropriate species, planting and maintenance, and community stewardship.	**
NE.9. Develop a street tree plan for commercial and residential corridors.	

XIV. IMPLEMENTATION STRATEGIES

Table IS-1
Implementation Tasks (Continued)

TASK	PRIORITY
LAND USE ELEMENT	
<u>Projects</u>	
LU.1. Prepare zoning regulations consistent with the revised NE 85th Street Subarea Plan.	**
LU.2. Prepare zoning regulations consistent with the revised Totem Lake Neighborhood Plan.	**
LU.3. Review existing development regulations for consistency with State law on the process for essential public facilities.	
LU.4. As part of the Bridle Trails Neighborhood Plan Update project, review land use densities and zoning for consistency with the GMA.	
LU.5. Refine open space network maps, identify missing links, and develop preservation techniques.	
LU.6. Amend the Zoning Code as appropriate to establish standards for residential markets.	*
<u>Ongoing</u>	
LU.7.8 When neighborhood plans are updated, consider design principles and standards for the local commercial center(s).	
LU.8.9 Monitor and update information concerning: <ul style="list-style-type: none"> ◆ Development capacity; ◆ Development trends; and ◆ Demographics. 	
LU.7. Develop business district plan, zoning & design guidelines for Houghton/ Everest Neighborhood Center.	
HOUSING ELEMENT	
<u>Projects</u>	
H.1. Consider regulations that allow innovative housing, including compact development and cottage housing.	**
H.2. Adopt regulations for market incentives to encourage low- and medium-income housing.	**
<u>Ongoing</u>	
H.3. Adopt a housing strategy plan and work program at least every five years that outlines housing strategies to be considered in order to address the City's housing needs and goals.	
H.4. Monitor and update information concerning: <ul style="list-style-type: none"> ◆ Construction and demolition of affordable housing; ◆ Creation of accessory units and associated rent levels. 	
H.5. Continue to work with ARCH to fund low-income and special needs housing projects.	

XIV. IMPLEMENTATION STRATEGIES

**Table IS-1
Implementation Tasks (Continued)**

TASK	PRIORITY
<p>HUMAN SERVICES ELEMENT</p> <p><u>Ongoing</u></p> <p>HS.1. Review the Senior Council’s Strategic Plan every five years and revise as appropriate.</p> <p>HS.2. Review the Youth Council’s Strategic Plan every five years and revise as appropriate.</p> <p>HS.3. Continue regional collaboration of the Human Service’s grant program to increase efficiencies.</p> <p>HS.4. Identify potential funding sources and submit grant applications for Senior, Youth and Human Services programs.</p>	
<p>CAPITAL FACILITIES ELEMENT</p> <p><u>Projects</u></p> <p>CF.1. Consider new revenue sources for capital facilities and implement as appropriate, including voter-approved bond issues.</p> <p><u>Ongoing</u></p> <p>CF.2. Annually update the Capital Facilities Element to reflect capacity of facilities, land use changes, level of service standards, and financing capability.</p> <p>CF.3. Annually update the Capital Facilities Element consistent with the Capital Improvement Program.</p> <p>CF.4. Periodically update impact fees to reflect increases in road and park construction costs.</p>	<p style="text-align: center;">*</p>
<p>NEIGHBORHOOD PLANS</p> <p><u>Ongoing</u></p> <p>NP.1. Regularly review neighborhood plans and amend as appropriate.</p> <p>NP.2. Incorporate the following capital project elements into the CIP and CFP processes and/or the neighborhood connection and neighborhood grant program:</p> <p>Highlands Neighborhood</p> <ul style="list-style-type: none"> Emergency Access Bridge to Forbes Creek Drive Highlands Park facility improvement Nonmotorized street enhancements to 116th Avenue NE and NE 87th Street Cedar View Park play structure <p>Market Neighborhood</p> <ul style="list-style-type: none"> Neighborhood park development in north sector View stations at 4th and 5th Street West Improved Market Street access <p>Market Street Corridor</p> <ul style="list-style-type: none"> Pedestrian and bicycle facilities 	<p style="text-align: center;">*</p>

Central Houghton Neighborhood

1. Work with Public Works Department to have historic street names added to street signs as they are replaced.
2. Identify design standards for 108th Avenue NE

Central Houghton Neighborhood Plan

1. Overview

The Central Houghton Neighborhood is bounded by the Burlington Northern Santa Fe Railroad (BNSFR) right-of-way and the Lakeview Neighborhood on the west; Interstate 405 right-of-way on the east; and NE 68th Street on the north. The southern boundary is the Kirkland City limit (See Figure CH-1, Land Use Map). 108th Avenue NE provides the main north-south vehicular, bicycle and pedestrian connection through the neighborhood, while NE 68th Street provides an east-west connection.

Central Houghton is predominately a single family neighborhood. Other land uses within the neighborhood consist of medium density residential, offices, neighborhood oriented businesses and a variety of schools, including Northwest University.

The business district, located along NE 68th Street, is the neighborhood's only commercial area. The undeveloped 73 acre Watershed Park takes up a large area in the southeastern corner of the neighborhood. Carillon Woods Neighborhood Park is in the central part of the neighborhood and Phyllis A. Needy Neighborhood Park provides a smaller neighborhood park adjacent to 108th Avenue NE.

2. Vision Statement

The vision statement is a verbal description of the character and qualities of the Central Houghton Neighborhood at a future time when the goals and policy direction expressed in this neighborhood plan are realized.

The Central Houghton Neighborhood has a rich and unique history. The area's political history as part of a separate city until 1968 fostered a deep community identity, establishing a tradition in which residents seek opportunities for involvement and stewardship in the neighborhood's future.

The neighborhood's predominantly low density residential character has been maintained, while the changing and varied needs of the population are accommodated through a diverse housing stock. Greater housing choices, as well as efforts to preserve affordability in housing, help to expand housing opportunities for all residents within the neighborhood.

Central Houghton is a friendly, accessible neighborhood, with safe and inviting pedestrian and bicycle routes. Healthy and active living is promoted through attractive streets and trails. Traffic on the neighborhood's major streets, 108th Avenue NE and NE 68th Street, is managed well, with improvements designed to be compatible with surrounding development. The Eastside Rail Corridor provides pedestrian and bicycle connections linking the corridor to parks and other neighborhood gathering places.

Local citizens value the variety of opportunities to meet in shops and restaurants within the Houghton/Everest Business District, as well as in casual locations in the neighborhood's parks and natural areas. The Houghton/Everest Business District has evolved into a thriving, pedestrian-oriented mixed-use center, with businesses available to meet the retail and service needs of the community. Appropriate streetscapes, site layouts and building designs provide an attractive and coordinated appearance within the district. Careful attention to the placement and design of vehicle and pedestrian

access from commercial areas to surrounding streets contributes to an efficient street network, and avoids conflicts with nearby low density areas.

Several schools and the Northwest University campus add to the Central Houghton community by providing neighborhood residents with a connection to the schools' students, parents, and facilities, as well as with residents of other Kirkland neighborhoods and the larger community. These campuses are valued and supported, not only for their role in providing educational opportunities and fostering community relationships, but for the additional open space they provide and share with the neighborhood.

The Central Houghton Neighborhood provides many beautiful open space experiences including the views, tree canopy and neighborhood parks. The residents cherish and preserve the territorial views, including the expansive views of Lake Washington, Seattle and the Olympic Mountains, the slopes, and the natural watershed areas that contribute to the neighborhood's distinctive character. The tree canopy in the neighborhood has been managed and enhanced, and adds to the neighborhood's peaceful setting. The neighborhood's parks meet the needs of the neighborhood's residents. Phyllis A. Needy Park provides a place for active play for the neighborhood's youngest residents, while Carillon Woods meets the neighborhood's recreational needs with a play area and both paved and natural trails. Opportunities for residents to quietly observe and enjoy wildlife habitat and open space exist at Carillon Woods and at the south end of the neighborhood, in the Watershed Natural Area.

Central Houghton residents take great pleasure and pride in calling this beautiful neighborhood their home.

3. Historical Context

The following history includes the Central Houghton and Lakeview Neighborhoods, as well as a portion of the Bridle Trails Neighborhood, since together they made up the City of Houghton until its consolidation with the City of Kirkland on April 30, 1968.

Naming and Early Settlement of Houghton: Samuel and Caroline French along with their adult son, Harry French, settled on the eastside of Lake Washington in 1872. The French family was from Maine and had been corresponding with a friend who had settled in Seattle and praised the potential of the Eastside. Mrs. French named their new home, Pleasant Bay.

The French family is considered Houghton's first white settlers. Little has been learned about the earliest Native American inhabitants of the area, but Mrs. French reported seeing them as they rowed their canoes along the shore. The French house was built in 1874 at 10120 NE 63rd and was home to the French family for four generations. The house was moved to 4130 Lake Washington Blvd in 1978.

In 1880, all communities were required by the US Post Office to have a one-word name. The Pleasant Bay community submitted the name Edison, after Thomas Edison, but Edison was already being used in the Washington Territory. The Pleasant Bay church had been given a 600 pound Meneely & Company church bell by Mr. and Mrs. William Houghton of Boston and so the community of Houghton was named in their honor. When the congregational churches merged in 1894, the bell was relocated to the Kirkland Congregation Church on 5th Avenue in the Norkirk Neighborhood. The church has been rebuilt, but the bell remains there and rings every Sunday.

The Suffhoff home was built in 1903 by Kirkland realtor, Charles Parrish for the Morris Orton family. The young widow, Mrs. May Orton rented the home to Dr. George Hudson Davis in about 1910 and it

was then used as a hospital and dental office. The home served as Houghton's and Kirkland's first hospital for ten years.

Industry of Pleasant Bay/Houghton: The French family soon had industrious neighbors. The Jay O'Conner's who purchased the Popham/McGregor land, built the Steamer Squak and the large Lake House which was used as a hotel. The John and Abigail Fish family purchased the Lake House and continued the hotel business. The Lake House was in the family for generations and was torn down in 1984. The Curtis family built and operated ferries on Lake Washington for over fifty years. George Bartsch and his brother-in-law, Harrie Tompkins, started the Bartsch-Tompkins Transportation Company in 1904. Mr. Bartsch bought out Mr. Tompkins and then partnered with John Anderson to create the Anderson Steamboat Company in 1907. The Anderson Steamboat Company became the Anderson Shipyard which then became the Lake Washington Shipyard.

The Lake Washington Shipyard was at the site of the present Carillon Point development. One of the original buildings built in 1907 was used as a pattern shop for the wooden ships built during WW I, then as a mold shop for the steel ships built during WW II. The building was still standing until the development of Carillon Point.

The early shipyards were limited to building lake ferries or smaller ocean going boats that could be navigated down the Black River at the southern tip of Lake Washington. In 1916, the opening of the ship canal lowered Lake Washington by almost 9 feet and dried up the Black River. With the opening of the Montlake Ship Canal, the shipyard could build large ocean going ships.

Houghton and the Wars: During WW I, shipbuilding boomed in Houghton, and many wooden war ships were constructed there. Workers commuted from Seattle for the work which ended in 1918. The Second World War again brought tremendous growth for Houghton. The Lake Washington Shipyard built steel hulled ships and they were all in service at the end of the war. The U.S. Government built the Steward Heights housing project on 108th Avenue NE for the shipyard workers. This area is now the Northwest University campus.

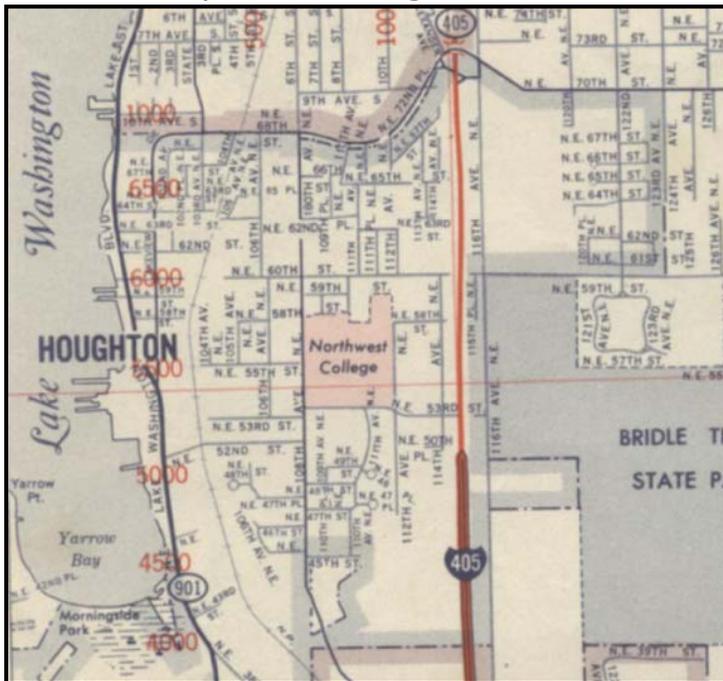
Terrace Park - Site of the Houghton City Hall: Terrace Park was originally the site for a community center for the Lakeview Neighborhood during WWII. It was built to service the needs of the Lake Washington Shipyard workers. In about 1955, the buildings were converted to house the Houghton City Hall, library, fire station and police station. The existing cement pads were used as the floor of the Houghton Police Station.

Livelihood of Houghton Residents: From the early 1870's, Houghton was settled by educated and hardworking families spreading out across the country. They purchased their homesteads and because of their isolation, they lived off their land. Their close proximity to Seattle also allowed them access to jobs, services and goods. For example, Harry French commuted to Seattle to work in Yesler's Mill. At first workers rowed weekly, returning home for the weekend, and in later years they took a steamer daily. There were jobs in the forest, the coal mines, and the lumber mills, but all required a commute by rowboat, horse or on foot. As more settlers arrived, there was a need for scheduled ferry service, a school, and a place to worship.

School and Worship: Harry French built a frame cabin, which was used by the family until their family home was ready. This cabin later became Pleasant Bay's first classroom and its first Sunday school. A church was then built and the minister, Reverend Greene, began coming from Seattle to make the rounds to the small local churches in the area.

Houghton Resident, John Cort: John Cort had the first legitimate theatre circuit and owned 117 theaters on the west coast. Mr. Cort was also an early founder of the FOE Eagles and was their first President. He was the first in the world to use the new Edison lights in his Seattle Standard Theatre which burned in the great fire of 1889. Cort’s last remaining Seattle theatre is the Moore. Cort traveled a great deal to manage his theatres, but his family lived full time on Whisker Farms, his Houghton home until 1918. Around this time he moved his empire to New York City where he later retired and then died in 1929. Whisker Farms was on Cort Road, now 108th Avenue NE. The Collins School was built on the burned out ruins of Whisker Farms, but has since been torn down and replaced by homes.

City of Houghton: Until 1968, Houghton was a separate city with a Houghton address and residents that were called “Houghtonites”. When Houghton merged with Kirkland, there was a strong emphasis to retain some authority on land use and zoning issues. State law allowed Houghton citizens to maintain control of their zoning and continued enforcement of their land use plan. The Houghton community continues to have a Houghton Community Council with veto power over land use actions of the Kirkland City Council relating to the area of the old City of Houghton. The Houghton Community Council is one of only two such community councils remaining in the State of Washington.



1968 Road Map of the Houghton Area

Although the City of Houghton and the City of Kirkland merged over 40 years ago, there is still a strong feeling of community among the residents of the Central Houghton neighborhood because of their unique history as a separate town.

History taken from Primary Sources: Family and State records, the Boston Newspaper, 1889 Kirkland Press and the French Diaries. For more information on the history of the City of Houghton please contact the Kirkland Heritage Society and see the Lakeview Neighborhood Plan.

Goal CH-1: Encourage preservation of structures, sites and objects of historical significance in the Central Houghton Neighborhood.

Policy CH-1.1: Encourage property owners to preserve buildings, structures, sites and objects of historical significance.

The Community Character Element establishes the hierarchy for designating historic buildings, structures, sites and objects in the City. Although age is an important factor in determining historical significance, other factors, such as the integrity of the building, architecture, location and relationship to notable persons or events of the past, are also important.

***Policy CH-1.2:** Provide directional signs, markers and interpretive information at structures, buildings, sites or objects of historical significance.*

Individual historic properties are encouraged to add historic plaques and interpretive signs. Additional directional signs and interpretive centers at or near structures, buildings, sites or objects of historical significance around the neighborhood will help bridge Houghton's rich history with future generations. Most of the original historic street names have been changed over the years. As street signs are replaced, the original street names could be added to recognize the neighborhood's history. The Community Character Element of this Comprehensive Plan also lists other techniques to preserve the neighborhood's history.

4. Natural Environment

***Goal CH-2:** Protect and enhance the natural environment in the Central Houghton Neighborhood.*

***Policy CH-2.1:** Undertake measures to protect and improve water quality and promote fish passage in Lake Washington and neighborhood wetlands, streams and wildlife corridors.*

The Central Houghton Neighborhood is located within the Yarrow Creek, Carillon Creek, Houghton Slope A and B, and Moss Bay drainage basins (see Figure CH-2). These drainage systems connect to Lake Washington and provide important ecological functions such as flood and storm water conveyance, water quality, fish habitat, wildlife and riparian corridors, and open space benefits.

Water quality is an important issue in the Central Houghton Neighborhood. Day lighted streams in the neighborhood should be kept clean and maintained in their natural state. Even in areas without significant streams, water from the neighborhood drains to Lake Washington and so pesticide and fertilizer use should be discouraged.

***Policy CH-2.2:** Ensure that development is designed to avoid damage to life and property on properties containing high or moderate landslide or erosion hazards areas.*

The Central Houghton Neighborhood contains medium and high landslide hazard areas (see Figure CH-3). These areas are prone to landslides that may be triggered by natural events or by manmade activities including grading operations, land clearing, irrigation, or the load characteristics of buildings on hillsides.

***Policy CH-2.3:** Protect wildlife throughout the neighborhood and encourage the creation of backyard sanctuaries for wildlife habitat.*

The National Wildlife Federation has designated the City of Kirkland as a certified Community Wildlife Habitat. The Community Wildlife Habitat Program for the City began in the Central Houghton Neighborhood. Central Houghton contains many wildlife corridors connecting parks and along stream channels to Lake Washington and Yarrow Bay Wetlands. Residents are encouraged to continue to improve wildlife habitat on their private property by planting native vegetation, and providing food, water, shelter and space for wildlife.

5. Land Use

Residential land uses occupy the majority of the Central Houghton neighborhood. Schools, including the expansive campus of Northwest University, are dispersed throughout the low-density residential core, while two large park and open space areas, Carillon Woods and the Watershed Natural Area are located in the central and southern portions of the neighborhood. Multifamily apartments and condominiums are clustered along the northern edge of Central Houghton, where they adjoin the neighborhood's only commercial area, the Houghton/Everest Neighborhood Center.

Residential

Goal CH-3: Promote and retain the residential character of the neighborhood while accommodating compatible infill development and redevelopment.

Policy CH-3.1: Retain the predominately detached single-family housing style in the Central Houghton neighborhood.

Central Houghton is a well established neighborhood that has predominately low-density (five to six dwelling units per acre) traditional single-family residential development. The land use transitions from low-density residential to medium-density multifamily and commercial development in the northern portion of the neighborhood near NE 68th Street. A mix of housing styles and sizes is important to the neighborhood's character.



Goal CH-4: Allow alternative residential development options that are compatible with surrounding development.

Policy CH-4.1: Allow a variety of development styles that provide housing choice in low density areas.

Providing housing options for a wide spectrum of households is an important value to support and encourage. Alternative housing provides more housing choice to meet changing housing demographics such as smaller households and an aging population. Allowing design innovations can help lower land and development costs and improve affordability. Compatibility with the predominant traditional detached single-family housing style in the neighborhood will determine the acceptance of housing alternatives. Alternative housing types such as cottage, compact single-family, accessory dwelling units, and clustered dwellings are appropriate options to serve a diverse population and changing household size and composition.

Policy CH-4.2: Encourage diversity in size of dwelling units by preserving and/or promoting smaller homes on smaller lots.

Diversity can be achieved by allowing properties to subdivide into lots that are smaller than the normal minimum lot size allowed in the zone if the size of houses on the small lots is limited. This encourages diversity, maintains neighborhood character, and provides more housing choice. Up to 50 percent of the single family lots in a subdivision should be allowed to be smaller than the zoning designation allows if a small house is retained or built on the small lots. The lots containing the small houses should be no less than 5,000 square feet in the RS 7.2 zones and no less than 6000 square feet in the RS 8.5 zones. The size of the houses on the small lots would be limited by a maximum floor area ratio and all other zoning regulations would apply.

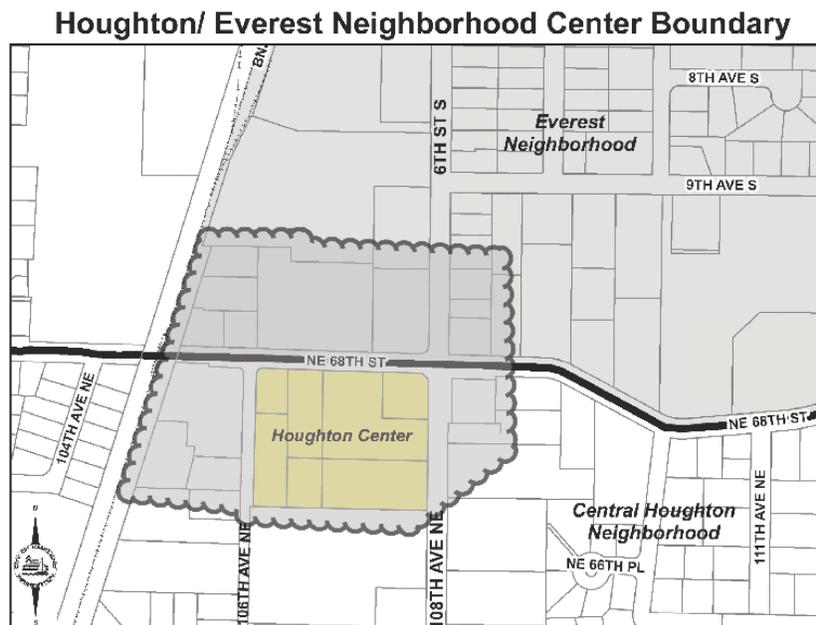
Policy CH-4.3: The residential land south of NE 68th Street and surrounding the Houghton/Everest Neighborhood Center area is suitable for medium residential densities (see MDR and O/MF land use designations on Figure CH-1).

The area south of NE 68th Street and surrounding the Houghton/Everest Center is appropriate for medium densities because of topographic features and surrounding neighborhood conditions. This area provides a good transition between the low density residential uses to the south, and the commercial shopping area to the north.

Commercial

Houghton/Everest Neighborhood Center

The Houghton/Everest Neighborhood Center is defined as a “Neighborhood Center” commercial area in the Land Use Element of the Comprehensive Plan. It includes properties on the north and south sides of NE 68th Street in both the Central Houghton and Everest Neighborhoods.



Goal CH-5: *Promote a strong and vibrant Neighborhood Center with a mix of commercial and residential uses.*

Policy CH-5.1: *Coordinate with the Everest Neighborhood to develop a plan for the Houghton/Everest Neighborhood Center, which overlays properties along the NE 68th Street corridor in both the Everest and Central Houghton neighborhoods (see inset).*

This plan should promote a coordinated strategy for the Neighborhood Center while minimizing adverse impacts on surrounding residential areas. The existing land use map designations will be used until the land use, and zoning and development regulations for the entire Neighborhood Center are re-examined.



Policy CH-5.2: *Encourage a mix of uses within the Houghton/Everest Neighborhood Center that includes commercial development such as neighborhood oriented shops, services, and offices, as well as multifamily residential use.*

A variety of uses, including retail, office and residential should be combined in order to contribute to a vibrant mixed use Neighborhood Center.

Policy CH-5.3: *Implement transportation improvements that support the existing and planned land uses in the Neighborhood Center and adjoining neighborhoods.*

A review of transportation impacts should be done for all new development in the Neighborhood Center. Transportation system improvements should be designed to encourage traffic to use existing arterials and to include traffic calming devices on neighborhood streets. Alternate modes of transportation should also be *encouraged*.

Policy CH-5.4: *Expand the area designated for higher intensity use to properties west of Houghton Center and south of NE 68th Street.*

Land located west of the Houghton Center shopping area, directly east of the Eastside Rail Corridor, has the potential to provide higher density residential use within walking distance of retail and business services. The rail corridor provides a wide buffer between this area and the low density residential area to the west.

Goal CH-6: *Promote high quality design by establishing building, site, and pedestrian design standards that apply to commercial and multifamily development in the Houghton/Everest Neighborhood Center.*

Policy CH-6.1: Establish design guidelines and regulations that apply to all new, expanded or remodeled commercial, multifamily or mixed use buildings in the Houghton/Everest Neighborhood Center.

These design guidelines and regulations should support appropriate building scale and massing, produce buildings that exhibit high quality design with a sense of permanence, and incorporate site design which includes pedestrian features and amenities that contribute to the livability of the surrounding area. They should also strengthen the visual identity of the neighborhood center by addressing streetscape improvements and public views to the lake along NE 68th Street.

Houghton Center: The shopping center development located at the southwest corner of NE 68th Street and 108th Avenue NE (shown in yellow on the map) is known as the “Houghton Center.” This large strip retail development sits on several parcels occupying approximately five acres. Redevelopment to a more cohesive, pedestrian-oriented concept may be feasible since a single owner controls the bulk of the site. In addition to its potential to serve the community through expanded neighborhood commercial uses, Houghton Center can contribute to the livability and vitality of the neighborhood by providing residents and visitors with a welcoming place to shop, congregate and relax.



Goal CH-7: Support the transition of the Houghton Center into a pedestrian-oriented mixed use development, including retail, with office or residential and other compatible uses.

Policy CH-7.1: Promote a pedestrian-oriented development concept through standards for a coordinated master plan for Houghton Center including retail, with office and or residential and other compatible uses.

A master plan for the Houghton Center should provide for a complementary arrangement of facilities, pedestrian amenities, open spaces, and linkages, as well as shared parking that meets the needs of Houghton Center and a coordinated sign system.

Policy CH-7.2: Reduce ingress and egress conflicts within and around Houghton Center through creation of a circulation system for vehicles and pedestrians as part of a master plan for development of the property.

The circulation system for both pedestrians and vehicles should provide the minimum amount of ingress and egress locations necessary for an effective circulation system into and through Houghton Center.

Policy CH-7.3: Allow building heights to step up to five stories if careful attention is given to building modulation, upper story step backs, and use of materials to reduce the appearance of bulk and mass.

Specific design guidelines should be developed to ensure that modulation is used to break down scale and massing of buildings into smaller and varied volumes, and to provide upper story step backs from the sidewalks to improve the pedestrian experience and maintain human scale.

Policy CH-7.5: Provide gathering spaces and relaxation areas within Houghton Center.

Houghton Center is an important community meeting place within the Central Houghton Neighborhood. Gathering spaces should be provided when Houghton Center redevelops as a way to provide places to meet neighbors and enjoy the facilities.

Schools and Places of Worship

A strong relationship between schools, places of worship and the surrounding community is a key factor to ensuring compatibility and minimizing conflicts.

Goal CH-8: Acknowledge the value to the community of schools and places of worship. Encourage interaction between these institutions and the residents of the Central Houghton Neighborhood.

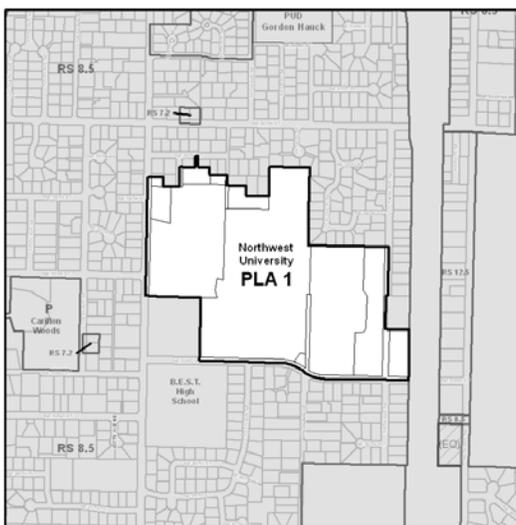
Policy CH-8.1: Provide opportunities for early **(and continuing)** community involvement in any expansion plans, modifications, or changes in use **(or intensity of ancillary uses)** for schools and places of worship.

Early community involvement is important in addressing issues that may affect the surrounding area and the neighborhood as a whole. Issues such as parking, and public safety should be taken into account when considering additional ancillary_uses, expansion of facilities, or the addition of new facilities. **(Public and private views should also be taken into account when considering**

options for buffering schools and places of worship from adjacent residential uses.)

Northwest University (Planned Area 1)

Northwest University is designated as a Planned Area because of its unique conditions including large parcel ownership, interface with the surrounding community, traffic patterns, and topographic conditions. The complex issues related to this planned area can best be dealt with through the master plan for the university.





The planned area designation permits the application of special development procedures and standards to minimize adverse impacts resulting from the natural growth and operation of the facility.

Goal CH-9: Ensure that the growth and development planned for Northwest University is reviewed and approved by the City.

Policy CH-9.1: *Limit Planned Area 1 to the boundaries designated in Figure CH-1.*

The boundaries shown in Figure CH-1 are consistent with the 1999 Northwest University Master Plan as shown below.

Policy CH-9.2: *Require all development in PLA 1 to conform to an approved master plan.*

The master plan approved in 1999 is the guiding document for Northwest University in PLA 1. Any variations from this master plan must be reviewed and approved by the City.

Policy CH-9.3: *Structures on campus should be located to minimize impacts on single family residential areas adjacent to the University.*

It is important to consider the location of new buildings on campus in relationship to the surrounding single family residential areas. New structures should be placed far enough away from single-family residential uses to minimize impacts.

Policy CH-9.4: *Traffic should be routed away from local residential streets to the extent possible.*

Traffic routing can have a great impact on the surrounding neighborhood. Primary access to the University should continue to be off of 108th Avenue NE.

Policy CH-9.5: *University activities should be buffered on all sides to protect adjacent single family residential development.*

The university should be buffered from surrounding areas to reduce visual and noise impacts and protect the privacy of those living within the surrounding single family neighborhood.

Transitional Areas

When locating institutional and commercial uses adjacent to residential areas, techniques should be used to minimize impacts on adjacent residential areas such as ensuring there is adequate parking on neighborhood streets for residents and businesses, minimizing noise in evening hours, and minimizing glare from commercial lighting.

Goal CH-10: *Minimize impacts between residential uses and adjoining institutional and commercial uses.*

Policy CH-10.1: *Mitigate negative impacts of commercial and institutional development on residential areas to protect neighborhood character.*

Regulating building height, building mass, building placement, vehicular access and traffic impacts and/or providing landscape buffers can be used to reduce negative impacts of commercial and institutional uses on surrounding residential uses. Mitigate adverse impacts through environmental review, development regulations and appropriate conditions imposed through development review.

6. Transportation

The circulation patterns in the Central Houghton Neighborhood are well established. 108th Avenue NE, a designated minor arterial, provides the primary north-south route through the Central Houghton Neighborhood. It also provides local access for a substantial number of residences, schools and businesses.



NE 68th Street which forms the northern boundary of the neighborhood is also a minor arterial. NE 52nd Street is designated a collector street providing an east-west connection between 108th Avenue NE and Lake Washington Blvd. NE 53rd Street between 108th Avenue NE and 114th Avenue NE is also a collector street. All other streets within the neighborhood are classified as neighborhood access

streets. They provide access to adjacent residences and connect to the collectors and minor arterial.

Nonmotorized transportation is addressed in the City's Active Transportation Plan and implemented through the Capital Improvement Program or through private development. The design of these improvements should enhance neighborhood access while fitting into the unique areas they traverse.

Goal CH-11: *Maintain mobility along 108th Avenue NE as a major vehicle, transit, pedestrian and bicycle corridor through the neighborhood.*

Policy CH-11.1: *Retain the existing three lane configuration for 108th Avenue NE.*

Traffic on 108th Avenue NE is often heavy, particularly during morning and evening commute periods. Congestion restricts local access to and from 108th Avenue NE and creates conflicts for bicyclists, adjacent residents, and pedestrians, including children arriving at and leaving the schools. Future traffic levels should be monitored and appropriate measures should be considered to mitigate impacts.

Policy CH-11.2: Enhance attractiveness and accessibility of 108th Avenue NE for all modes of transportation.

A master plan for 108th Avenue NE should be established through a public process. The plan should consider installation of streetscape amenities such as pedestrian lighting, street furniture, and low level landscaping to enhance the pedestrian experience and the continuation, widening and signing of bicycle lanes.

Policy CH-11.3: Implementation of street improvements should occur through both the City's Capital Improvement Program process and through site specific private development.

The means to implement improvements should be determined on a comprehensive area-wide basis and to the extent possible, on an incremental basis by encouraging or requiring the incorporation of improvements into private developments.

Policy CH-11.4: Support transportation measures that will reduce commuter or pass through traffic through the neighborhood.

The City should support and encourage the following measures:

1. Alternatives to single-occupancy vehicles for commuting purposes, such as public transportation, commuter pools, high-occupancy vehicles (HOV), and potentially other transportation modes such as light rail.



2. Improvements to the I-405/SR 520 corridors.

Goal CH-12: Encourage mobility and the use of nonmotorized transportation by providing improvements for pedestrians and bicyclists.

Policy CH-12.1: Improve the pedestrian and bicycle circulation systems both as a recreation amenity and alternative transportation option.

Pedestrian and bicycle pathways are part of the transportation system but also provide recreational opportunities. Pathways and trails should be provided to activity nodes such as the Houghton/Everest Neighborhood Center, parks and transit facilities, and the Lakeview Neighborhood. Directional signs indicating path locations should also be provided.

Policy CH-12.2: Support future development of the Eastside Rail Corridor as a multipurpose trail for pedestrian and bicycles with access points along the corridor.

The unused BNSF railroad right-of-way, known as the Eastside Rail Corridor, provides an opportunity for a bicycle, pedestrian and rail transportation corridor. Pedestrian and bicycle transportation is a high priority, but regardless of the function of the Corridor it should be designed so that it will:

- Serve as a gateway to the City.
- Provide neighborhood pedestrian and bicycle connections, with the highest priority access points at NE 52nd, NE 60th and NE 68th Streets.

- Be compatible with adjacent neighborhoods.
- Ensure a high degree of safety.
- Show environmental stewardship.

7. Open Space and Parks

There are currently three publicly owned parks and two public school-based recreation sites within the Central Houghton Neighborhood. The neighborhood has been fortunate to have a high degree of community involvement in the development and maintenance of its park facilities.

The City has a level of service (LOS) goal of locating a neighborhood park within a quarter-mile radius of each household in Kirkland. This desired LOS standard is being met for the Central Houghton Neighborhood. LOS within the neighborhood is also enhanced by the proximity of parks just outside the defined neighborhood boundaries, including Terrace Park, Houghton Beach Park, Marsh Park, and Everest Park.

Watershed Park is an undeveloped 73-acre park which takes up a large amount of the southeastern portion of the neighborhood. It is heavily wooded with varying terrain including steep slopes, and



features soft-surface walking trails. This property has been identified as a high priority for removal of invasive plants and for revegetation activities for its urban reforestation program.

Any future development of the park should be undertaken following a community-based master planning process. Considerations for a park master plan should include protection and enhancement of natural resources and minimizing potential impacts to surrounding residential areas.

Phyllis A. Needy Houghton

Neighborhood Park is a small 0.50

acre neighborhood park adjacent to 108th Avenue N.E. It includes a small playground, a basketball hoop, and picnic tables. No further development of this park is anticipated.

Carillon Woods is an 8.7 acre neighborhood park that features soft-surface and asphalt trails, interpretive signage, native plantings, and a children's playground. Carillon Woods was historically the water supply for Yarrow Bay and was designated Water District #1. It was later purchased from the Water District by the City through a park bond and its creation and use were determined through several public workshops. Approximately 2 acres of the property are fenced off to protect several deactivated artesian wells, steep slopes, wetlands, and emerging springs which serve as the headwaters for Carillon Creek. As with Watershed Park, reforestation efforts are a high priority for this property. Although no further development is anticipated for this park, the removal of existing wells, pumping systems, and other facilities related to former use of the site by a local water district should occur in the future.

B.E.S.T. High School is on a 10 acre site and is part of the Lake Washington School District (LWSD). The City has constructed and maintains a multi-purpose playfield at B.E.S.T. High School through an interlocal agreement with LWSD. The playfield is available for both organized and informal sports activities such as baseball/softball, soccer, and football. A small gymnasium at the school is also available on a limited basis for community recreation programming, with scheduling and use dictated by LWSD.

International Community School (ICS) is located at the north end of the neighborhood. This approximately 11- acre site provides both indoor and outdoor recreation space for the neighborhood. All facilities on the property are maintained by LWSD.

Goal CH-13: Ensure adequate park and recreation facilities in the Central Houghton Neighborhood.

Policy CH-13.1: Pursue acquisition of property and partnerships with schools and other institution in Central Houghton.

The City should seek opportunities to acquire land to expand parks as properties adjacent to existing parks become available. It is also important to provide and maintain a diversity of park recreation types for the neighborhood. The City should pursue cooperative agreements for joint use of the facilities at schools and other institutions. In addition, street ends should be developed and expanded into park and open space areas for public enjoyment.

8. Public Services and Facilities

Water, sewer, and drainage services and facilities are adequate for existing and foreseeable future developments in the Central Houghton Neighborhood. The goals and policies contained in the Utilities, Capital Facilities and Public Services Chapters of the Comprehensive Plan provide the general framework for these services and facilities.

Goal CH-14: Provide public and private utility services for the Central Houghton Neighborhood.

Policy CH-14.1: Undergrounding of overhead utilities should be actively encouraged.

In order to contribute to a more attractive and safe living environment, to improve views and enhance a sense of community identity, the undergrounding of utilities should be actively encouraged.

9. Urban Design

Central Houghton's unique urban design assets are identified in Figure CH-5 and play an important role in the visual image of the Central Houghton Neighborhood.

Views

Goal CH-15: Preserve public view corridors and natural features that contribute to the visual identity of the Central Houghton neighborhood.



Policy CH-15.1: *Preserve public scenic views and view corridors of Lake Washington, Seattle and the Olympic Mountains from public rights-of-ways and parks.*

Public view corridors are important assets and should continue to be enhanced as new development occurs. Wide, expansive views of Lake Washington looking west from public rights-of-ways should be maintained. Street trees along rights-of-ways that offer local and territorial views should be of a variety that will not block views as trees mature.

Gateways

Goal CH-16: *Enhance gateways to the neighborhood to strengthen neighborhood identity.*

Policy CH 16.1: *Use public and private efforts to establish gateway features at the locations identified in Figure CH-5.*

Gateways welcome residents, employees and visitors into the City and help define neighborhood identity. Gateways can be in the form of natural features, such as landscaping or structures, such as signs or buildings. The northern and southern gateways to the Central Houghton neighborhood both occur along 108th Avenue NE. The City should pursue opportunities to work with private property owners to install neighborhood gateway features as part of future development. Improvements such as signs, public art, structures, lighting and landscaping can be included.

Design Standards for 108th Avenue NE and Pedestrian Pathways

Goal CH-17: *Provide public improvements that contribute to a sense of neighborhood identity and enhanced visual quality.*

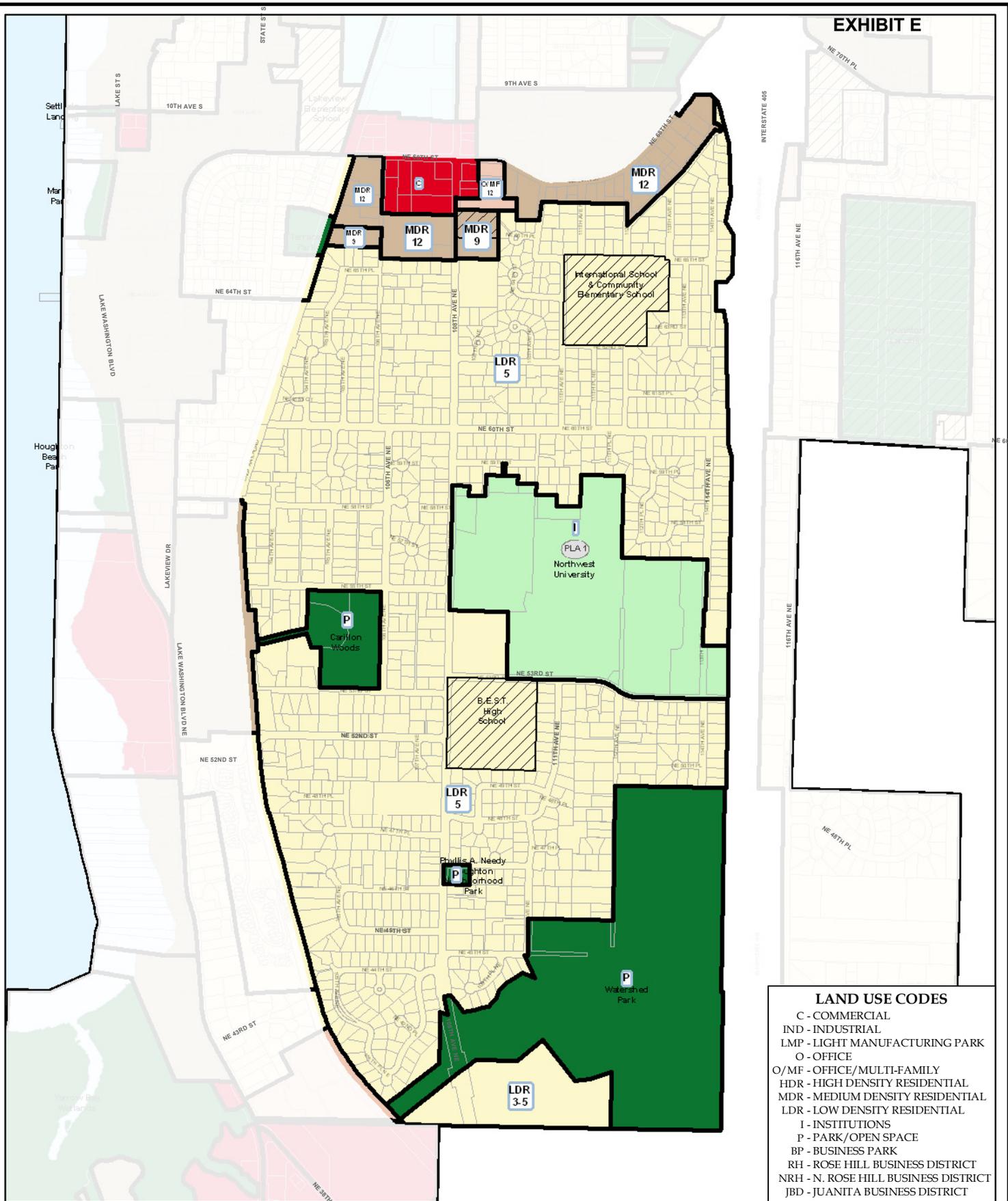
Policy CH-17.1: *Identify design standards for 108th Avenue right-of-way:*

These standards should include:

- Adequate sidewalk widths on both sides of the street.
- Street trees that are of a type that will not block views from the public rights-of-way as the trees mature.
- Public amenities such as benches, pedestrian lighting, public art, beautification of traffic medians and directional signs pointing to public facilities and points of interest.

Central Houghton Neighborhood Plan Figures:

- CH-1 - Land Use*
- CH-2 - Sensitive Areas*
- CH-3 - Geological hazardous areas*
- CH-4 - Transportation street network*
- CH-5 - Urban design assets*



LAND USE CODES

- C - COMMERCIAL
- IND - INDUSTRIAL
- LMP - LIGHT MANUFACTURING PARK
- O - OFFICE
- O/MF - OFFICE/MULTI-FAMILY
- HDR - HIGH DENSITY RESIDENTIAL
- MDR - MEDIUM DENSITY RESIDENTIAL
- LDR - LOW DENSITY RESIDENTIAL
- 1 - INSTITUTIONS
- P - PARK/OPEN SPACE
- BP - BUSINESS PARK
- RH - ROSE HILL BUSINESS DISTRICT
- NRH - N. ROSE HILL BUSINESS DISTRICT
- JBD - JUANITA BUSINESS DISTRICT

Central Houghton Neighborhood Land Use Map

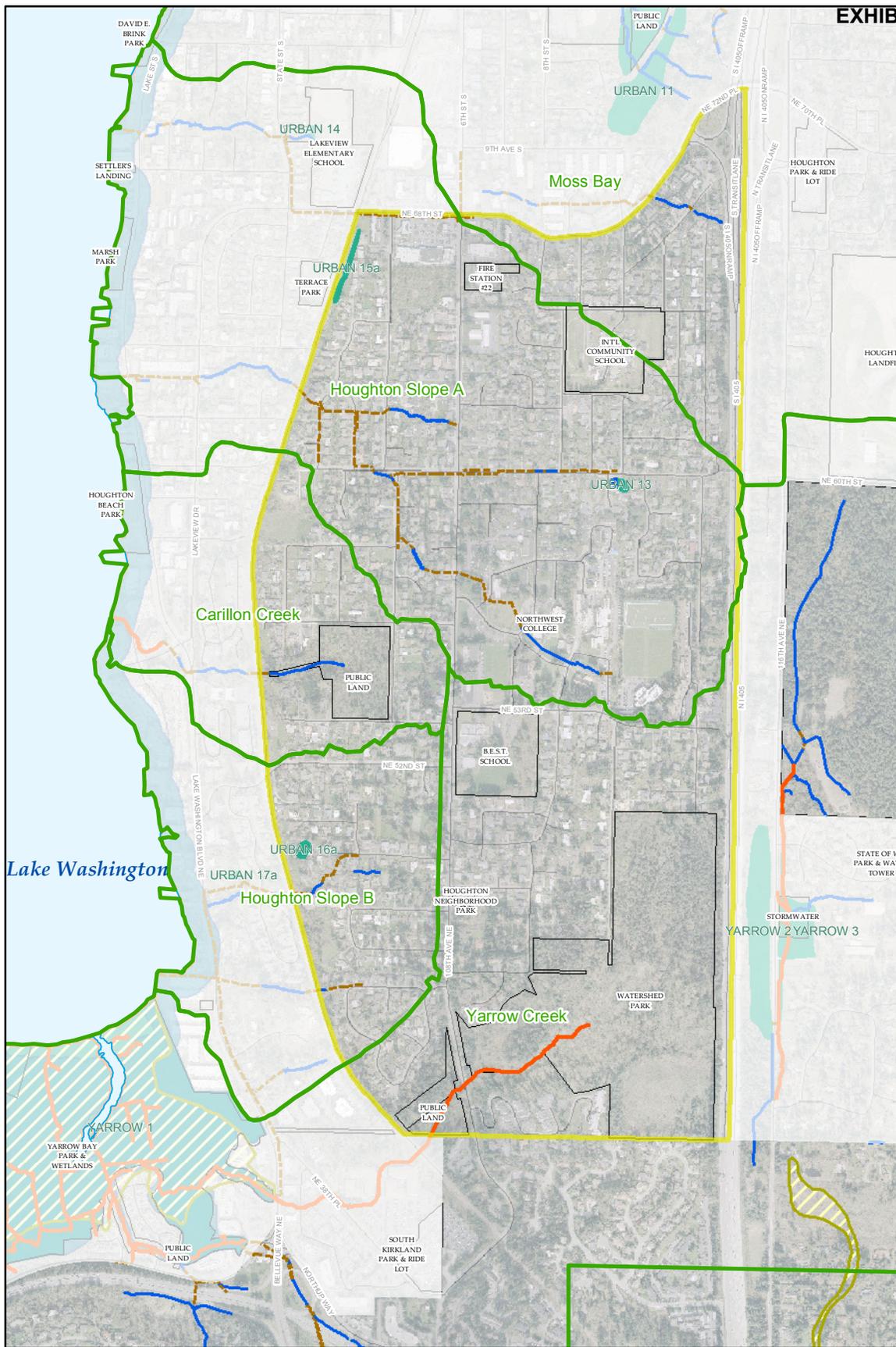
ORDINANCE NO. 3974
ADOPTED by the Kirkland City Council
December 14, 2004

LAND USE BOUNDARIES	PARCEL BOUNDARIES
SUBAREA BOUNDARY	PLANNED AREA NUMBER
TOTEM CENTER	LAND USE CODE
PUBLIC FACILITIES	DENSITY (UNITS/ACRE)

NOTE: WHERE NOT SHOWN, NO DENSITY SPECIFIED
* INDICATES CLUSTERED LOW DENSITY

Maps produced September 6, 2011.
Produced by the City of Kirkland. © 2011, the City of Kirkland, all rights reserved.
No warranties of any sort, including but not limited to accuracy, fitness or merchantability, accompany this product.





- Legend**
- Known Salmonid Locations
 - Streams in Pipes
 - Open Streams
 - 100-Year Floodplain
 - Wetlands
 - Shoreline of Statewide Significance
 - Drainage Basin Boundaries
 - Selected Public Properties
 - Lakes
 - Central Houghton Neighborhood Boundary

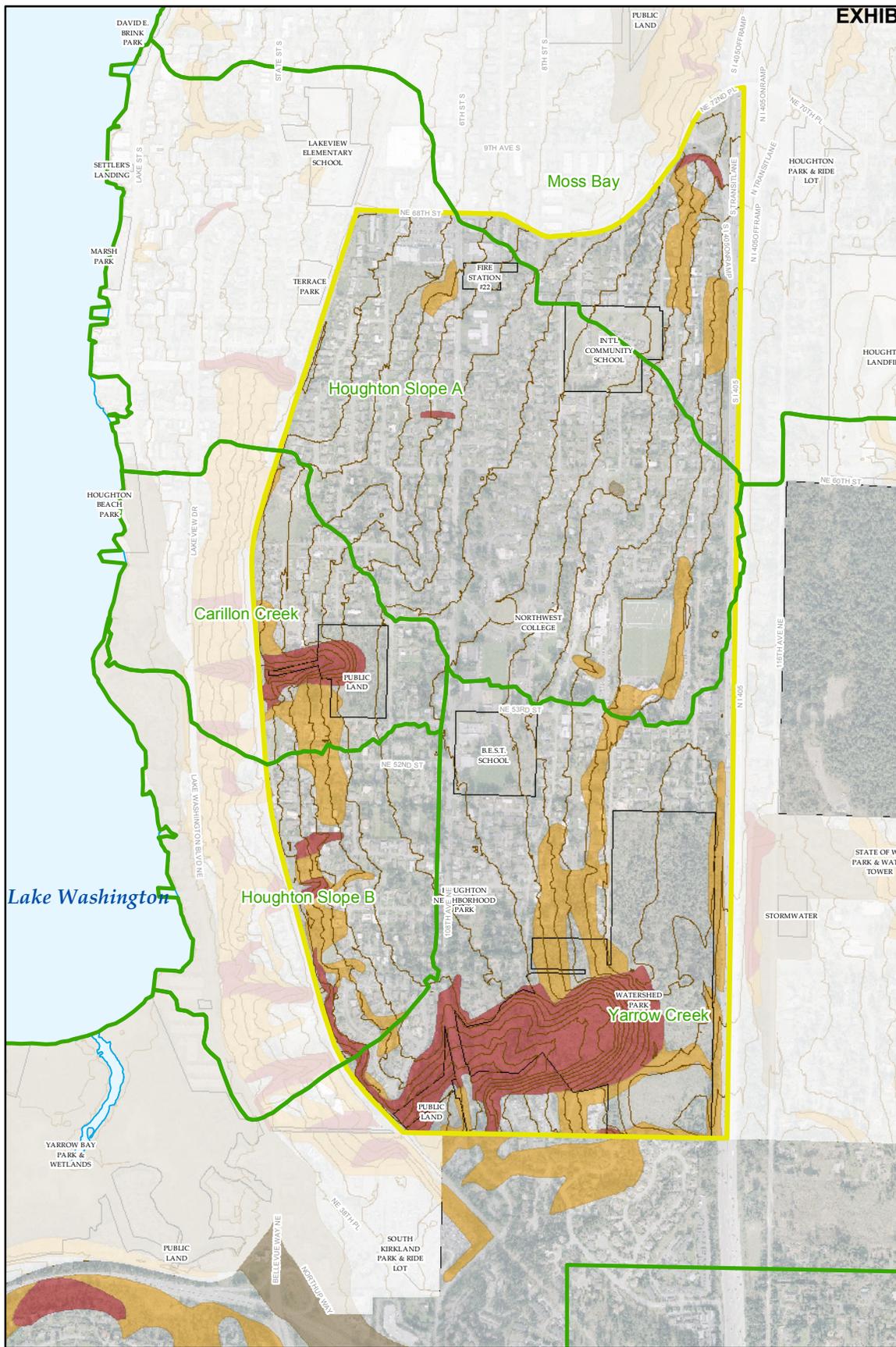
N

0 600 1,200
Feet

Scale: 1" = 1,200'

Produced by the City of Kirkland.
© 2011, the City of Kirkland, all rights reserved.
No warranties of any sort, including but not limited to accuracy, fitness or merchantability, accompany this product.

Figure CH-2: Central Houghton Sensitive Areas



- Legend**
- Medium Landslide Hazard
 - High Landslide Hazard
 - Seismic Hazard Area
 - Twenty-Foot Contours
 - Drainage Basin Boundaries
 - Selected Public Properties
 - Lakes
 - Central Houghton Neighborhood Boundary

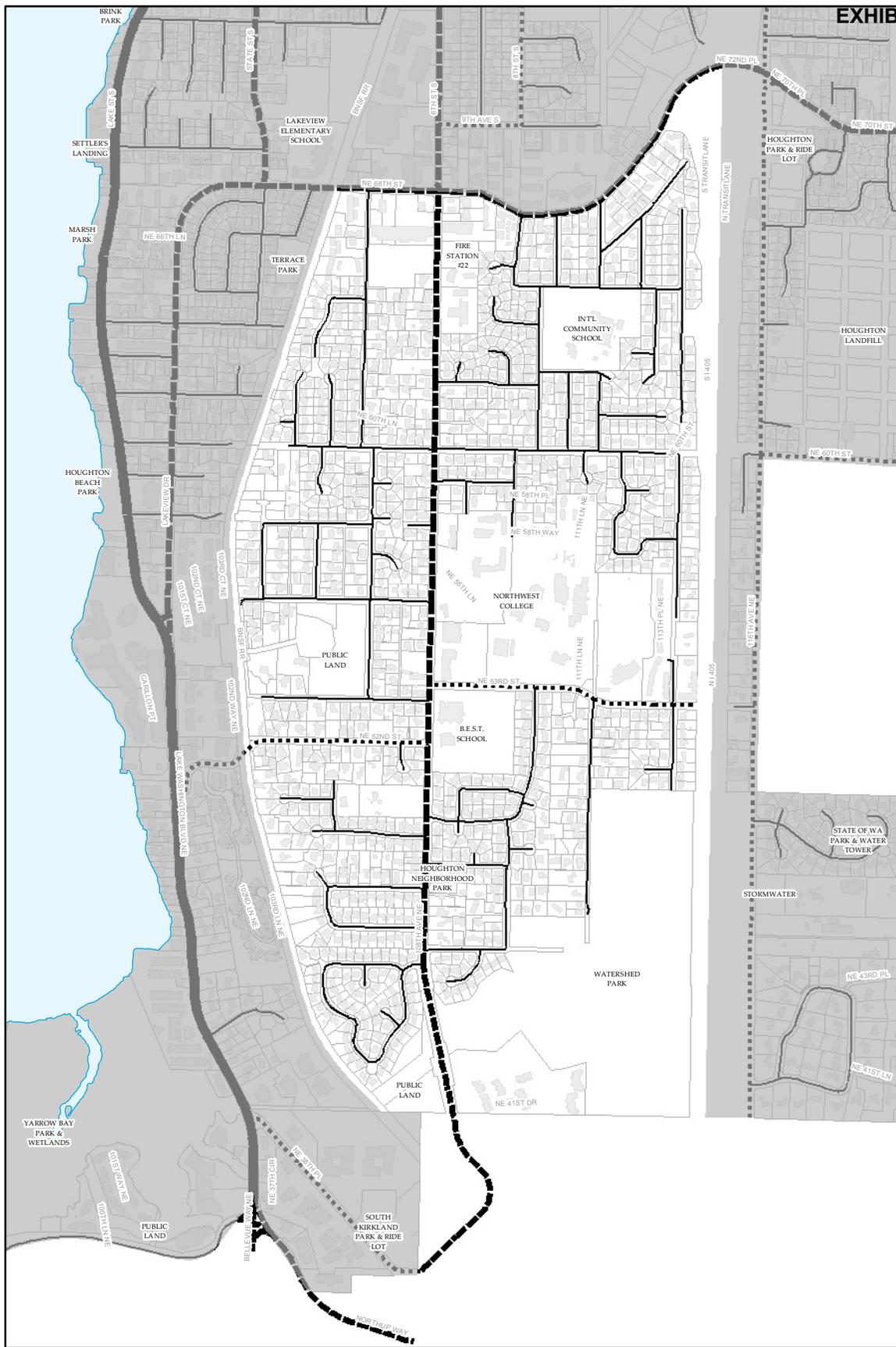
N

0 600 1,200
Feet

Scale: 1" = 1,200'

Produced by the City of Kirkland.
© 2011, the City of Kirkland, all rights reserved.
No warranties of any sort, including but not limited
to accuracy, fitness or merchantability, accompany this product.

Figure CH-3: Central Houghton Landslide and Seismic Hazard Areas



- Legend**
- Neighborhood Access
 - ▬ Building Footprints
 - ⋯ Collector
 - Tax Parcel Boundaries
 - - - Minor Arterial
 - ▬ Principal Arterial

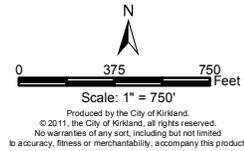


Figure CH-4: Central Houghton Street Classifications

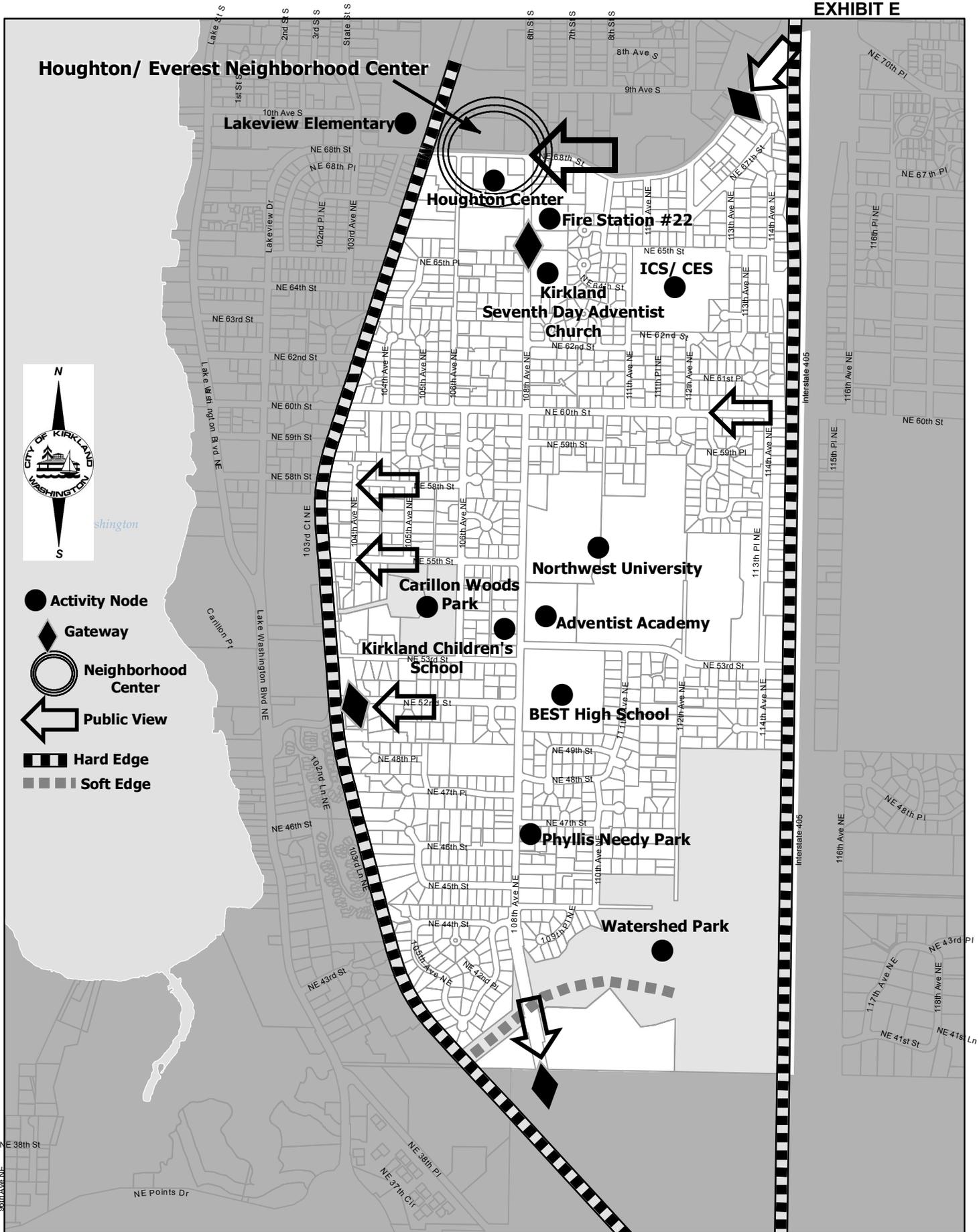


Figure CH-5: Central Houghton Neighborhood Assets Urban Design

PUBLICATION SUMMARY
OF ORDINANCE NO. O-4329

AN ORDINANCE OF THE CITY OF KIRKLAND RELATING TO COMPREHENSIVE PLANNING AND LAND USE AND AMENDING THE COMPREHENSIVE PLAN, ORDINANCE 3481 AS AMENDED, TO IMPLEMENT THE CENTRAL HOUGHTON NEIGHBORHOOD PLAN UPDATE; AMEND THE LAND USE ELEMENT AND IMPLEMENTATION STRATEGIES CHAPTERS; ADJUST THE NEIGHBORHOOD BOUNDARY LINE BETWEEN CENTRAL HOUGHTON AND LAKEVIEW; AND APPROVE A SUMMARY FOR PUBLICATION, FILE NO ZON09-00016 .

SECTION 1. Amends the following specific portions of the Kirkland Comprehensive Plan:

- A. Amends City of Kirkland Neighborhoods Map in the Introduction;
- B. Amends Figure LU-2 Commercial Areas in the Land Use Element;
- C. Amends text in Policy LU-5.8 in the Land Use Element;
- D. Adds text to Table IS-1: Implementation Tasks in the Implementation Strategies Chapter
- E. Repeals existing Central Houghton Neighborhood Plan and replaces it with a new Central Houghton Neighborhood Plan;

SECTION 2. Provides a severability clause for the ordinance.

SECTION 3. Authorizes publication of the ordinance by summary, which summary is approved by the City Council pursuant to Kirkland Municipal Code 1.08.017 and establishes the effective date as January 1, 2012, which will be more than five days after publication of summary.

SECTION 4. Establishes certification by City Clerk and notification of King County Department of Assessments.

The full text of this Ordinance will be mailed without charge to any person upon request made to the City Clerk for the City of Kirkland. The Ordinance was passed by the Kirkland City Council at its meeting on the ____ day of _____, 2011

I certify that the foregoing is a summary of Ordinance _____ approved by the Kirkland City Council for summary publication.

City Clerk

ORDINANCE NO. O-4330

AN ORDINANCE OF THE CITY OF KIRKLAND RELATING TO PLANNING AND LAND USE AND AMENDING KIRKLAND MUNICIPAL CODE SECTION 22.28.042 – LOTS - SMALL LOT SINGLE FAMILY TO INCLUDE THE CENTRAL HOUGHTON NEIGHBORHOOD AND APPROVING A SUMMARY FOR PUBLICATION, FILE NO. ZON09-00016.

WHEREAS, the City Council has received a recommendation from the Kirkland Planning Commission and the Houghton Community Council to amend the Municipal Code, as set forth in that certain report and recommendation of the Planning Commission and Houghton Community Council for the Central Houghton Neighborhood dated September 7, 2011 and bearing Kirkland Department of Planning and Community Development File No. ZON09-00016; and

WHEREAS, prior to making said recommendation the Planning Commission and Houghton Community Council, following notice thereof as required by RCW 35A.63.070 and RCW 36.70A.035, held a public hearing on June 23, 2011 on the amendment proposal and considered the comments received at said hearing; and

WHEREAS, pursuant to the State Environmental Policy Act (SEPA), there has accompanied the legislative proposal and recommendation through the entire consideration process, a SEPA Addendum to Existing Environmental Documents issued on June 15, 2011 by the responsible official pursuant to WAC 197-11-625; and

WHEREAS, in regular public meeting the City Council considered the environmental documents received from the responsible official, together with the report and recommendation of the Planning Commission and Houghton Community Council;

NOW, THEREFORE, the City Council of the City of Kirkland do ordain as follows:

Section 1. The following specific portions of the Municipal Code are hereby amended to read as follows:

Section 22.28.042 – Lots – Small lot single-family. as set forth in Exhibit A attached to this ordinance and incorporated by reference.

Section 2. If any section, subsection, sentence, clause, phrase, part or portion of this ordinance, including those parts adopted by reference, is for any reason held to be invalid or unconstitutional by any court of competent jurisdiction, such

decision shall not affect the validity of the remaining portions of this ordinance.

Section 3. This ordinance shall be in full force and effect January 1, 2012, which will be more than five days from and after its passage by the City Council and publication pursuant to Kirkland Municipal Code 1.08.017, in the summary form attached to the original of this ordinance and by this reference approved by the City Council as required by law.

Section 4. A complete copy of this ordinance shall be certified by the City Clerk, who shall then forward the certified copy to the King County Department of Assessments.

Passed by majority vote of the Kirkland City Council in open meeting this ____ day of _____, 2011.

Signed in authentication thereof this ____ day of _____, 2011.

Mayor

Attest:

City Clerk

Approved as to Form:

City Attorney

22.28.042 Lots—Small lot single-family.

In the **Central Houghton**, Market and Norkirk neighborhoods, as defined in the comprehensive plan, for those subdivisions not subject to the lot size flexibility provisions of Sections 22.28.030 and 22.28.040 and historic preservation provisions of Section 22.28.048, the minimum lot area shall be deemed to be met if at least one-half of the lots created contain no less than the minimum lot size required in the zoning district in which the property is located. The remaining lots may contain less than the minimum required lot size; provided, that such lots meet the following standards:

- (a) Within the RS 6.3 and RS 7.2 zones, the lots shall be at least five thousand square feet.
- (b) Within the RS 8.5 zone, the lots shall be at least six thousand square feet.
- (c) The portion of any flag lot that is less than thirty feet wide, and used for driveway access to the buildable portion of the lot may not be counted in the lot area.
- (d) The floor area ratio (FAR) shall not exceed thirty percent of lot size; provided, that FAR may be increased up to thirty-five percent of the lot size if the following criteria are met:
 - (1) The primary roof form of all structures on the site is peaked, with a minimum pitch of four feet vertical to twelve feet horizontal; and
 - (2) All structures are set back from side property lines by at least seven and one-half feet.
- (e) The FAR restriction shall be recorded on the face of the plat.
- (f) Accessory dwelling units are prohibited. This restriction shall be recorded on the face of the plat. (Ord. 4102 § 1(A), 2007)

PUBLICATION SUMMARY
OF ORDINANCE NO. O-4330

AN ORDINANCE OF THE CITY OF KIRKLAND RELATING TO PLANNING AND LAND USE AND AMENDING KIRKLAND MUNICIPAL CODE SECTION 22.28.042 – LOTS - SMALL LOT SINGLE FAMILY TO INCLUDE THE CENTRAL HOUGHTON NEIGHBORHOOD AND APPROVING A SUMMARY FOR PUBLICATION, FILE NO. ZON09-00016.

Section 1. Amends the following specific portions of the Municipal Code:

- A. Section 22.28.042 – Lots – Small lot single-family to include the Central Houghton Neighborhood.

Section 2. Provides a severability clause for the ordinance.

Section 3. Authorizes publication of the ordinance by summary, which summary is approved by the City Council pursuant to Kirkland Municipal Code 1.08.017 and establishes the effective date as January 1, 2012, which will be more than five days after publication of summary.

Section 4. Establishes certification by City Clerk and notification of King County Department of Assessments.

The full text of this Ordinance will be mailed without charge to any person upon request made to the City Clerk for the City of Kirkland. The Ordinance was passed by the Kirkland City Council at its meeting on the ____ day of _____, 2011.

I certify that the foregoing is a summary of Ordinance _____ approved by the Kirkland City Council for summary publication.

City Clerk

ORDINANCE NO. O-4331

AN ORDINANCE OF THE CITY OF KIRKLAND RELATING TO COMPREHENSIVE PLANNING AND LAND USE AND AMENDING THE COMPREHENSIVE PLAN, ORDINANCE 3481 AS AMENDED, TO IMPLEMENT THE LAKEVIEW NEIGHBORHOOD PLAN UPDATE; AMEND THE COMPREHENSIVE LAND USE MAP AND IMPLEMENTATION STRATEGIES CHAPTER; AND APPROVE A SUMMARY FOR PUBLICATION, FILE NO ZON07-00032.

WHEREAS, the City Council has received a recommendation from the Kirkland Planning Commission and the Houghton Community Council to amend certain portions of the Comprehensive Plan for the City, Ordinance 3481 as amended, all as set forth in that certain report and recommendation of the Planning Commission and Houghton Community Council for the Lakeview Neighborhood dated September 7, 2011 and bearing Kirkland Department of Planning and Community Development File No. ZON07-00032; and

WHEREAS, prior to making said recommendation the Planning Commission and Houghton Community Council, following notice thereof as required by RCW 35A.63.070 and RCW 36.70A.035, held public hearings on June 23, 2011 and July 14, 2011 on the amendment proposals and considered the comments received at said hearing; and

WHEREAS, pursuant to the State Environmental Policy Act (SEPA), there has accompanied the legislative proposal and recommendation through the entire consideration process, a SEPA Addendum to Existing Environmental Documents, issued on July 15, 2011 by the responsible official pursuant to WAC 197-11-625; and

WHEREAS, in regular public meeting the City Council considered the environmental documents received from the responsible official, together with the report and recommendation of the Planning Commission and Houghton Community Council; and

NOW, THEREFORE, the City Council of the City of Kirkland do ordain as follows:

Section 1. Comprehensive Plan Text and Graphics amended: The following specific portions of the text of the Comprehensive Plan, Ordinance 3481 as amended, be and they hereby are amended to read as follows:

- A. Section VI. Land Use Element:
Map amendment to Figure LU-1: Comprehensive Land Use Map as set forth in Exhibit A attached to this ordinance and incorporated by reference.

- B. Section XIV. Implementation Strategies:
Amendment to Table IS-1: Implementation Tasks as set forth in Exhibit B attached to this ordinance and incorporated by reference.
- C. Section XV.A Lakeview Neighborhood Plan:
Updated Lakeview Neighborhood Plan as set forth in Exhibit C attached to this ordinance and incorporated by reference.

Section 2. If any section, subsection, sentence, clause, phrase, part or portion of this ordinance, including those parts adopted by reference, is for any reason held to be invalid or unconstitutional by any court of competent jurisdiction, such decision shall not affect the validity of the remaining portions of this ordinance.

Section 3. This ordinance shall be in full force and effect January 1, 2012, which will be more than five days from and after its passage by the City Council and publication pursuant to Kirkland Municipal Code 1.08.017, in the summary form attached to the original of this ordinance and by this reference approved by the City Council as required by law.

Section 4. A complete copy of this ordinance shall be certified by the City Clerk, who shall then forward the certified copy to the King County Department of Assessments.

Passed by majority vote of the Kirkland City Council in open meeting this _____ day of _____, 2011.

Signed in authentication thereof this _____ day of _____, 2011.

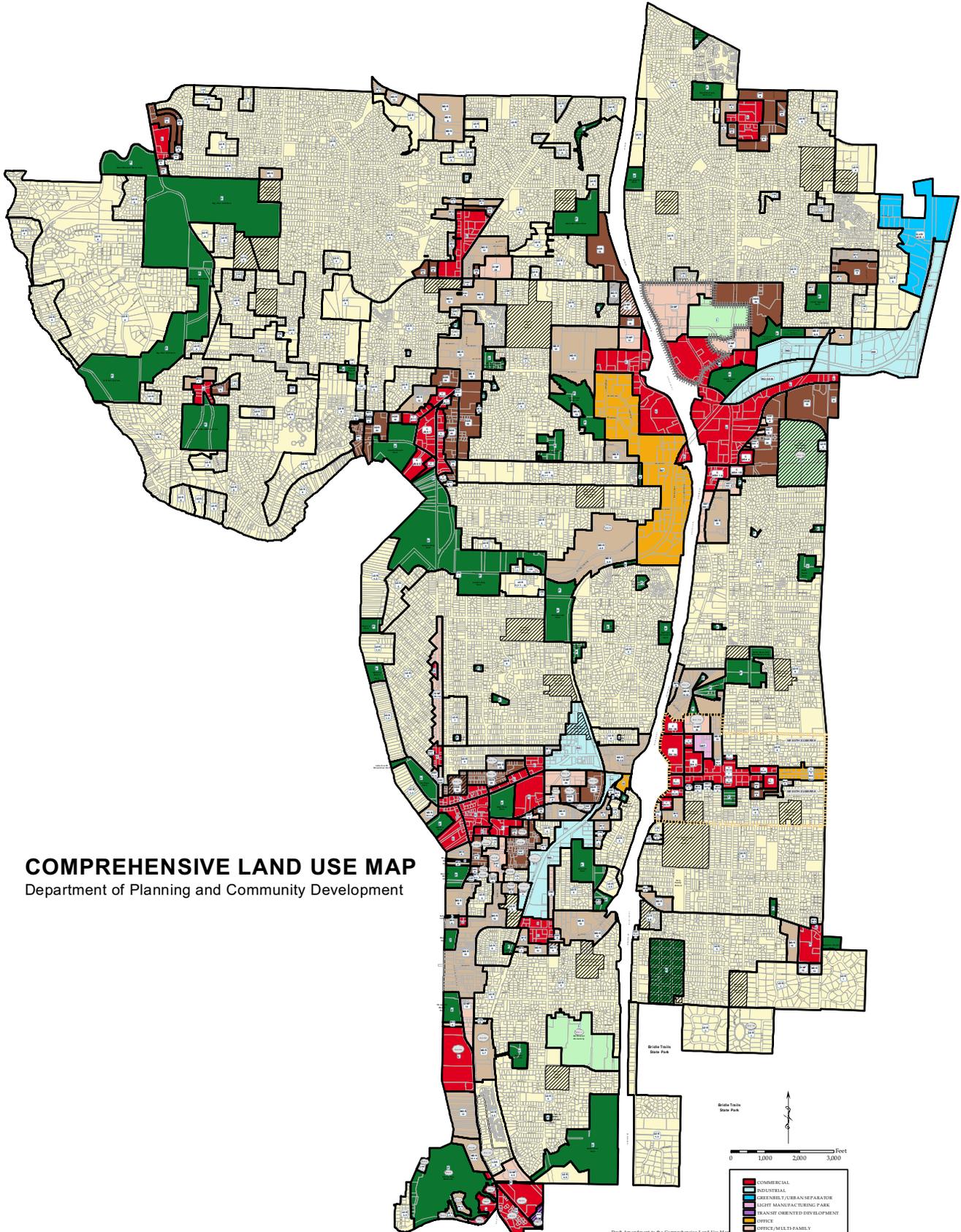
Mayor

Attest:

City Clerk

Approved as to Form:

City Attorney



COMPREHENSIVE LAND USE MAP
 Department of Planning and Community Development

Draft Amendment to the Comprehensive Land Use Map
 Prepared August 2, 2011

LAND USE CODES	
C-COMMERCIAL	IND-INDUSTRIAL
LMP-LIGHT MANUFACTURING PARK	TOD-TRANSIT ORIENTED DEVELOPMENT
O/OFFICE	O/MF-OFFICE/MULTIFAMILY
HR-HIGH DENSITY RESIDENTIAL	MR-MEDIUM DENSITY RESIDENTIAL
LR-LOW DENSITY RESIDENTIAL	I-INSTITUTIONS
P-PARK/OPEN SPACE	BP-BUSINESS PARK
BR1-ROSE HILL BUSINESS DISTRICT	BR2-ROSE HILL BUSINESS DISTRICT
BR3-JUANITA BUSINESS DISTRICT	CR-CORNER/URBAN SEPARATE

INDUSTRIAL	URBAN SEPARATE
RIGHT MANUFACTURING PARK	TRANSIT ORIENTED DEVELOPMENT
OFFICE	OFFICE/MULTIFAMILY
HIGH DENSITY RESIDENTIAL	MEDIUM DENSITY RESIDENTIAL
LOW DENSITY RESIDENTIAL	INSTITUTIONS
PARK/OPEN SPACE	LAND USE BOUNDARIES
URBAN BOUNDARY	SYSTEM CENTER
PUBLIC FACILITIES	PARCEL BOUNDARIES
PLANNED AREA NUMBER	LAND USE CODE
DENSITY (UNITS/ACRE)	

Produced by the City of Fort Lauderdale. © 2011. All rights reserved.
 No warranty of any sort, including but not limited to accuracy, shall be made by the City of Fort Lauderdale.

LU-1 Comprehensive Land Use Map

XIV. IMPLEMENTATION STRATEGIES

Table IS-1
Implementation Tasks (Continued)

TASK	PRIORITY
LAND USE ELEMENT	
<u>Projects</u>	
LU.1. Prepare zoning regulations consistent with the revised NE 85th Street Subarea Plan.	**
LU.2. Prepare zoning regulations consistent with the revised Totem Lake Neighborhood Plan.	**
LU.3. Review existing development regulations for consistency with State law on the process for essential public facilities.	
LU.4. As part of the Bridle Trails Neighborhood Plan Update project, review land use densities and zoning for consistency with the GMA.	
LU.5. Refine open space network maps, identify missing links, and develop preservation techniques.	
LU.6. Amend the Zoning Code as appropriate to establish standards for residential markets.	*
<u>Ongoing</u>	
LU.7.8 When neighborhood plans are updated, consider design principles and standards for the local commercial center(s).	
LU.8.9 Monitor and update information concerning: <ul style="list-style-type: none"> ◆ Development capacity; ◆ Development trends; and ◆ Demographics. 	
LU.7. Research KZC 162 allowance for maintenance & redevelopment of existing multi-family developments that do not conform to current density standards. Consider the ability of these units to retain housing capacity and affordability.	
HOUSING ELEMENT	
<u>Projects</u>	
H.1. Consider regulations that allow innovative housing, including compact development and cottage housing.	**
H.2. Adopt regulations for market incentives to encourage low- and medium-income housing.	**
<u>Ongoing</u>	
H.3. Adopt a housing strategy plan and work program at least every five years that outlines housing strategies to be considered in order to address the City's housing needs and goals.	
H.4. Monitor and update information concerning: <ul style="list-style-type: none"> ◆ Construction and demolition of affordable housing; ◆ Creation of accessory units and associated rent levels. 	
H.5. Continue to work with ARCH to fund low-income and special needs housing projects.	

XIV. IMPLEMENTATION STRATEGIES

Table IS-1
Implementation Tasks (Continued)

TASK	PRIORITY
<p>HUMAN SERVICES ELEMENT</p> <p><u>Ongoing</u></p> <p>HS.1. Review the Senior Council’s Strategic Plan every five years and revise as appropriate.</p> <p>HS.2. Review the Youth Council’s Strategic Plan every five years and revise as appropriate.</p> <p>HS.3. Continue regional collaboration of the Human Service’s grant program to increase efficiencies.</p> <p>HS.4. Identify potential funding sources and submit grant applications for Senior, Youth and Human Services programs.</p>	
<p>CAPITAL FACILITIES ELEMENT</p> <p><u>Projects</u></p> <p>CF.1. Consider new revenue sources for capital facilities and implement as appropriate, including voter-approved bond issues.</p> <p><u>Ongoing</u></p> <p>CF.2. Annually update the Capital Facilities Element to reflect capacity of facilities, land use changes, level of service standards, and financing capability.</p> <p>CF.3. Annually update the Capital Facilities Element consistent with the Capital Improvement Program.</p> <p>CF.4. Periodically update impact fees to reflect increases in road and park construction costs.</p>	*
<p>NEIGHBORHOOD PLANS</p> <p><u>Ongoing</u></p> <p>NP.1. Regularly review neighborhood plans and amend as appropriate.</p> <p>NP.2. Incorporate the following capital project elements into the CIP and CFP processes and/or the neighborhood connection and neighborhood grant program:</p> <p>Highlands Neighborhood</p> <ul style="list-style-type: none"> Emergency Access Bridge to Forbes Creek Drive Highlands Park facility improvement Nonmotorized street enhancements to 116th Avenue NE and NE 87th Street Cedar View Park play structure <p>Market Neighborhood</p> <ul style="list-style-type: none"> Neighborhood park development in north sector View stations at 4th and 5th Street West Improved Market Street access <p>Market Street Corridor</p> <ul style="list-style-type: none"> Pedestrian and bicycle facilities 	*

Lakeview Neighborhood

1. Work with the Public Works Department to have historic street names added to street signs as they are replaced.
2. Identify streetscape standards for Lake Washington Boulevard.

Lakeview Neighborhood Plan

1. Overview

The Lakeview Neighborhood is bounded by Lake Washington on the west and the Burlington Northern Santa Fe Railroad (BNSFR) right of way and the Central Houghton Neighborhood to the east (See Figure L- 1, Land Use Map). Lake Washington Boulevard and Lakeview Drive provide north-south vehicular, bicycle and pedestrian connections from the SR 520 interchange to Downtown Kirkland and adjacent neighborhoods. The Yarrow Bay Business District serves as a southern gateway to the City.

The Marsh and Houghton Beach waterfront parks are recreational hubs for neighborhood residents and visitors, while Terrace Park serves as a neighborhood park. Yarrow Bay wetlands with its lush tree canopy functions as a pristine wetland and stream system and provides critical wildlife habitat while serving as a sanctuary from surrounding urban development.

Land uses within the neighborhood consist of low to medium residential densities, offices and neighborhood oriented businesses. Carillon Point is an important employment center and regional tourism draw with its mix of offices, retail, hotel, restaurants, housing and marina on the shores of Lake Washington. The Yarrow Bay Business District contains large office parks with limited services for businesses and freeway travelers.

The policy direction for the waterfront is established in the Shoreline Area Chapter of the Comprehensive Plan. The thrust of those shoreline policies is to maintain residential uses, permit water-dependent commercial uses where commercial uses presently exist, and place a high priority on public access to the water either through park acquisition or pedestrian easements.

2. Vision Statement

The following vision statement is intended to describe the desired state of the neighborhood 20 years in the future.

Located along the eastern shores of Lake Washington the Lakeview Neighborhood has a special waterfront town charm. Lakeview residents value the visual and physical connection to Lake Washington. Wide, expansive views of the Lake and the Olympic mountains have been sustained because of careful selection and placement of trees and vegetation, to avoid view obstruction of the Lake from public streets and properties to the east. Over time the neighborhood has maintained its unique waterfront neighborhood character.

The neighborhood is a mix of single family and multifamily residential areas, offices, neighborhood oriented businesses and two commercial centers - Carillon Point and the Yarrow Bay Business District. Adequate parking is available on streets for easy access to neighborhood oriented businesses in the center of the neighborhood.

Infill development on the Houghton and Yarrow Bay slopes continues while maintaining the visual character of the hillsides and retaining trees to the maximum extent. Overall, the neighborhood has resisted development pressure to allow a large amount of density increases.

The Yarrow Bay Business District is a vibrant pedestrian urban village with a mix of commercial uses, housing, hotels, and services for businesses, residents, transit users and freeway travelers. The Business District has evolved over time to incorporate pedestrian oriented improvements such as landscaped green spaces and plazas for people to gather, public art, and improved street design with decorative pedestrian lighting.

The South Kirkland Park and Ride lot has transformed from a surface parking lot and transit center to a transit oriented development with additional parking stalls to serve transit riders, a mix of housing for a range of incomes, commercial services, improved vehicular and pedestrian access to the site and is a well designed architectural gateway to the City.

The street network in Lakeview is well established. A master plan for Lake Washington Boulevard has resulted in creating a streetscape design that includes wide sidewalks, landscaping, pedestrian decorative lighting, benches, and art. Improvements to both Lake Washington Boulevard and Lakeview Drive have increased pedestrian and bicycle safety and reduced traffic congestion.

Pedestrian and bicycle trails provide increased connections between the Yarrow Bay wetlands, Lake Washington Boulevard, Watershed Park, Carillon Woods Park and the future Eastside Rail Corridor along the old BNSF railroad right of way.

Lakeview's parks are clean, well maintained, and enjoyable for residents and visitors. Our waterfront parks are a model for how shoreline areas can provide a soft, natural shoreline to improve habitat with the planting of native vegetation. Access to a majority of the water's edge has been maintained for residents to enjoy our lake.

Our streams and wetlands are protected through management of development, maintaining existing vegetation and restoration projects. At the Yarrow Bay wetlands, people may observe the scenic beauty of the wetlands and wildlife habitat from viewpoints.

3. Historical Context

The Lakeview neighborhood is part of what was once the city of Houghton until 1968 when Houghton merged with Kirkland. As a result of the merger, the Houghton Community Council retained jurisdiction over land use decisions within the neighborhood.

Notable Houghton settlers were the Samuel French, the Jay O'Conner, the Curtis, Fish and Lute Marsh families. What is now known as the Orton House (Sutthoff House) at 4120 Lake Washington Blvd. was originally built in 1903 by realtor Charles Parrish for the Morris Orton family. The Orton house was then rented to a Dr. George Hudson Davis around 1910 and used as the area's first hospital and dental office. The Herman Schuster house (grandfather of Louis Marsh) was built just north of the Orton home. The French house was moved from its original location at 10126 NE 63rd Street to its present location at 4130 Lake Washington Blvd. in 1978.

In 1929, Louis Marsh built the Marsh Mansion on the property (6610 Lake Washington Blvd.) his parents purchased in 1905. Marsh Park, donated by Mr. Marsh, is on the land he acquired when Lake Washington was lowered in 1916. Harry French and other Houghton residents commuted to Seattle to work in Yesler's Mill either by rowing boats, by horse or daily ferry service. Harry French built a frame cabin for his family which later became Pleasant Bay's (original name for Houghton) first classroom and its first Sunday school.

The French House was moved from 10129 NE 63rd ST to 4130 Lake Washington Blvd. in 1978.

On Lake Washington Boulevard between NE 59th – 60th Streets, two older buildings exist that have been used as an antique store and offices. One was built in the 1900's and was the early site of the Houghton Post Office.

Where Carillon Point is today was the original location of the Lake Washington Shipyard, started in 1905 by two brothers-in-law - Bartsch and Tompkins. The shipyard was an employment hub, building wood ships during the First World War, then steel ships during the Second World War.

Near this location, NE 52nd Street (Curtis Road) was the first street in Houghton connecting Lake Washington Boulevard to 108th Avenue NE (Cort Road). After the shipyards closed in the late 1940's, the site was used for many years as a practice facility for the Seattle Seahawks football team.

Curtis Landing dock and the Houghton Post Office location.

The Lakeview Terrace neighborhood south of NE 68th Street and Lakeview Drive was built in 1942 to serve as housing for the Lake Washington Shipyard workers during the Second World War and many of the existing homes remain today. Terrace Park was originally the site for a community center for the Lakeview neighborhood during the war. Around 1955, the buildings were converted to house the Houghton City Hall, library, fire station and police station.

Lake Washington Shipyards during WWI and WWII.

For more detail on the history of Houghton see the Central Houghton Neighborhood Plan and the Community Character Chapter for goals and policies regarding the preservation and designation of historic buildings, structures, sites and objects of historical significance.

Goal L-1: Encourage preservation of structures, sites and objects of historical significance in the Lakeview Neighborhood.

Policy L-1.1: Encourage property owners to preserve buildings, structures, sites and objects of historical significance.

The Community Character Element list of Historic Buildings, Structures, Sites and Objects includes the structures and sites in the Lakeview Neighborhood: the Marsh Mansion at 6610 Lake Washington Blvd., the French House at 4130 Lake Washington Blvd., and the Orton House at 4120 Lake Washington Blvd,



the Shumway site at 510-528 Lake Street S. (structure was moved to Juanita), Lake Washington Shipyards site at Carillon Point and the Lake House site at 10127 NE 59th ST. The Marsh Mansion is recognized on the National and State Registers of Historic Places and contains a Historic Landmark zoning designation.

Notwithstanding the language regarding historic structures in the Goals and Policies Section of this Comprehensive Plan, it is the intent of the Houghton Community Council and the Kirkland City Council that only residential use should be permitted in either the Orton or French houses at their present site. The Marsh Mansion is the only historic structure which should be considered as possibly appropriate for non-residential use.

Policy L-1.2: Provide directional signs, markers and interpretive information at structures, buildings, sites or objects of historical significance.

Individual historic properties are encouraged to add historic plaques and interpretive signs. Additional directional signs and interpretive centers at or near structures, buildings, sites or objects of historical significance around the neighborhood would help bridge the Houghton's rich history with future generations. Most of the original historic street names have been changed over the years. As street signs are replaced, the original street names could be added to recognize the neighborhood's history. The Community Character Element of this Comprehensive Plan lists other techniques to preserve the neighborhood's history.

4. Natural Environment

Goal L-2: Protect and enhance the natural environment in the Lakeview Neighborhood.

Natural Water Systems

Policy L-2.1: Protect and improve water quality and promote fish passage by undertaking measures to protect Lake Washington, and the wetlands and streams in the Carillon Creek, Yarrow Creek and Houghton Slope basins.

Four drainage basins and associated creeks flow through Lakeview toward Lake Washington: Yarrow Creek, Houghton Slope A, Houghton Slope B, Carillon Creek and Yarrow Bay wetlands (See Figure L- 2, Sensitive Areas Map). These drainage systems provide important ecological functions such as flood and storm water conveyance, water quality, fish habitat, wildlife and riparian corridors, and open space benefits. Cutthroat Trout inhabit Yarrow Creek. Cochran Springs Creek is considered a tributary to Yarrow Creek and also contains Cutthroat Trout, juvenile Coho salmon and Lamprey.

Where feasible, barriers within stream corridors should be removed to allow fish passage (such as through the SR 520 interchange, along Northup Way, and at the railroad crossing). Use of pesticides and fertilizer near stream and wetland areas is discouraged.

Policy L-2.2: Develop viewpoints and interpretive information around streams and wetlands if protection of the natural features and private property can be reasonably ensured.

Yarrow Bay wetlands function as a pristine natural wildlife reserve and water quality system filtering contaminants prior to discharge into Lake Washington. With improved access, the wetlands would also

provide passive recreation and educational opportunities. Installation of viewpoints would improve visual access to the wetlands and Lake Washington if they could be constructed to protect the natural system and rights of private property owners.

Soils and Geology

The Houghton and Yarrow Slopes contain soils susceptible to moderate to high landslide hazards particularly when wet or sliding as a result of earthquake activity (See Figure L-3 Geologically Hazardous Areas Map).

Policy L-2.3: Manage development to protect potentially hazardous areas, such as landslide, erosion, and seismic areas.

Houghton Slope

The most sensitive portions of the Houghton Slope are generally south of NE 58th Street. The soil types there are prone to sliding and erosion; and the slopes are steep, averaging 15 percent with portions greater than 40 percent. There are several steep ravines which have a particularly high hazard of sliding because of the large amounts of groundwater in the slope causing artesian pressure and many small streams. The trees and other vegetation on the slope help to provide slope stability. They also provide significant aesthetic value because of the wooded slopes, particularly for those who enter the City from the south on Lake Washington Boulevard.

Houghton Slope north of NE 58th Street although less sensitive than the slopes further south also bears careful scrutiny. This area is mostly developed with low- and medium-density residential. Construction on or adjacent to these slopes may cause or be subject to land sliding, excessive erosion, and drainage or other problems associated with development on a slope.

Yarrow Slope

The Yarrow Slope, west and south of the Yarrow Bay Wetlands has also been identified as having soils susceptible to moderate landslide hazards. Some landslides occurred in the early 1960s southward along the present location of SR520. Nearby landslides, steep slopes, high water content, and peat deposits warrant additional geotechnical analysis to ensure slope stability. Locating structures on the site to minimize disruption to natural systems such as steep slopes, hillside streams and wetlands is preferred. Development on these slopes should consider the same development standards listed under the Houghton Slope land use section below.

Some properties surrounding the Yarrow Bay Wetlands contain seismic hazard areas because the soil type is subject to risk of earthquake damage as a result of seismically induced settlement or soil liquefaction. Regulations governing development on geologically hazardous areas are located in the Kirkland Zoning Code.

Policy L-2.4: Protect wildlife throughout the neighborhood and encourage the creation of backyard sanctuaries for wildlife habitat.

The National Wildlife Federation has designated the City of Kirkland as a certified Community Wildlife Habitat. The Community Wildlife Habitat Program for the City began in the Central Houghton Neighborhood. Lakeview contains many wildlife corridors connecting parks in the Central Houghton

neighborhood and along stream channels to Lake Washington and Yarrow Bay Wetlands. Residents are encouraged to improve wildlife habitat on their private property by planting native vegetation, providing food, water, shelter and space for wildlife.

5. Land Use

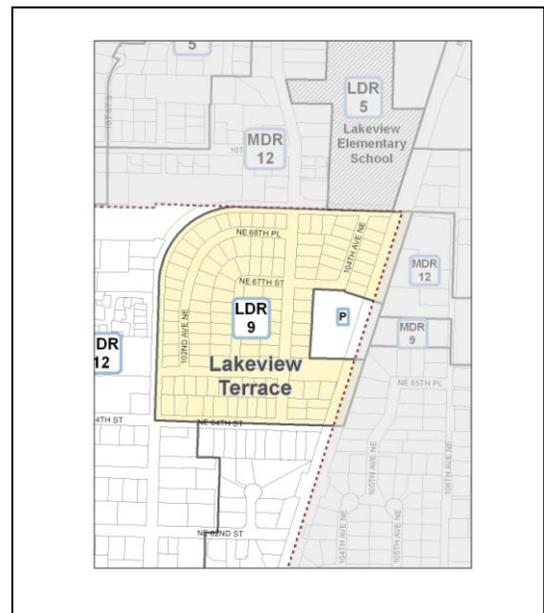
Figure L-1 describes the land use designations throughout the Lakeview Neighborhood.

Residential

Goal L-3: Retain the residential character of the neighborhood while accommodating compatible infill development.

Policy L-3.1: Maintain Lakeview Terrace as a single family residential area up to nine dwelling units per acre.

The single-family residential area of Lakeview Terrace, encircled by Lakeview Drive, NE 64th Street, and the railroad tracks, contains housing with some older structures. This area should be maintained as single-family at up to nine dwelling units per acre reflecting the existing small lots. The area should be protected from encroachment and adverse impacts of neighboring commercial and multifamily uses.



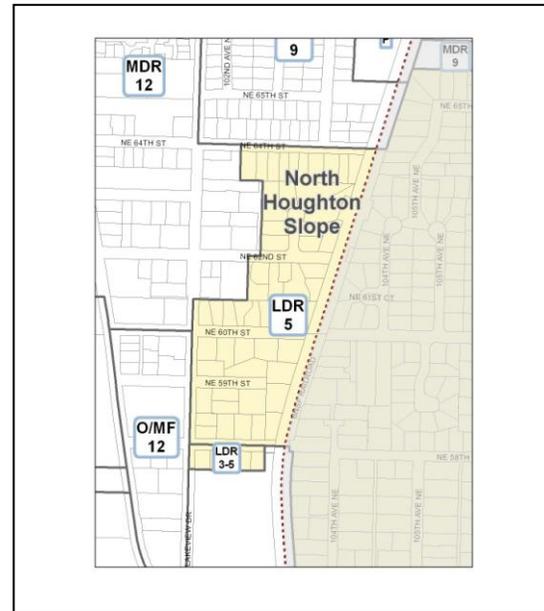
Houghton and Yarrow Slopes

There are geologic constraints, and aesthetic attributes to consider for development on the Houghton and Yarrow Slopes.

Policy L-3.2: Along the Houghton and Yarrow Slopes, establish development standards to protect property from landslides, seismic events and surface water runoff while allowing redevelopment compatible with existing development.

Policy L-3.3: Along the north portion of the Houghton Slope between NE 58th Street and NE 64th Street retain the existing single family residential development at 3-5 dwelling units per acre.

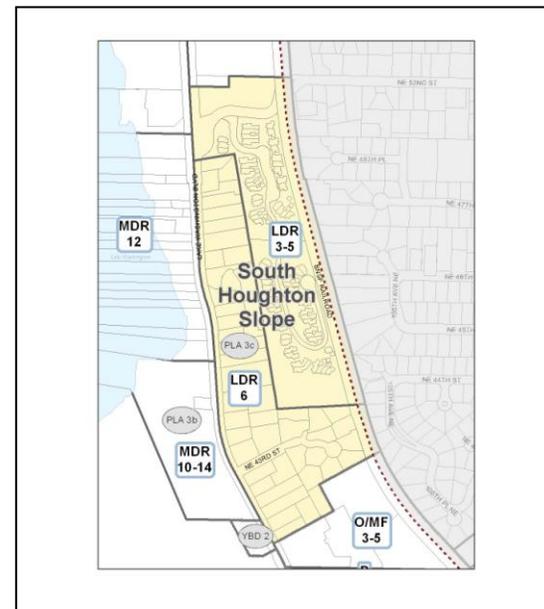
The area bounded by Lakeview Drive, NE 64th Street, the railroad right of way, and approximately NE 58th Street falls within a Moderate Landslide Hazard slope area (see the Natural Environment section). All developments should be preceded by adequate slope stability investigations. The presence of an open stream, limited access, and existing small lot sizes impose limits on the feasible residential densities.



South Houghton Slope

The entire residential area south of NE 58th Street lies on the part of the Houghton Slope identified as containing High Landslide Hazard soils (see the Natural Environment Section). Several underground springs, watercourses and forested ravines located along the hillside may contribute to slope instability.

The east portion of the slope is developed with the Yarrow Hill housing development. The majority of the lots in Planned Area 3C are under single ownership, are long, narrow, and have steep sloped driveways making vehicular and emergency access to Lake Washington Boulevard challenging. In many instances, the line of sight distances for automobiles entering and leaving the flow are generally too short to be safe. For these reasons consolidating driveways and limiting vehicular access points along Lake Washington Blvd should be a priority in the design of new development.



Policy L-3.4: Residential development on the south Houghton slope should be limited. The Yarrow Hill Development should remain at three – four dwelling units per acre. The PLA 3C is appropriate for six dwelling units per acre with a minimum lot size of 5,000 sq. ft. and subject to the development standards listed below.

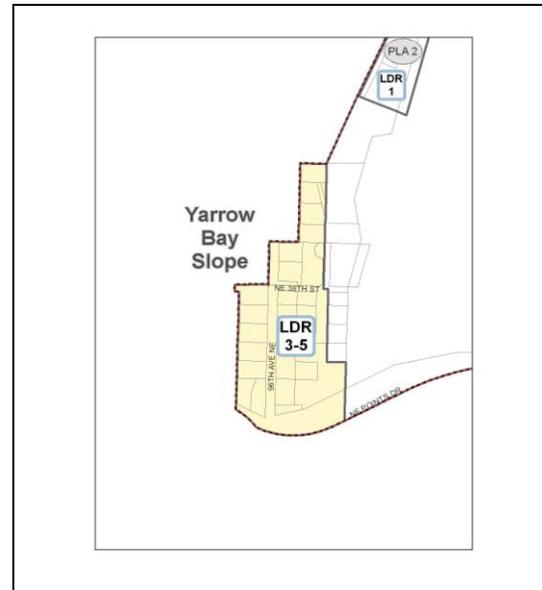
Given the unique physical constraints of the west portion of the slope, the area should be treated as a planned area to allow for flexibility in site design, the location of structures and lot layout to protect steep slopes, existing water courses, and the retention of vegetation. Such techniques as aggregation of lots, smaller lots or clustering of units away from steep slopes should be encouraged.

Development should be subject to a public review process to ensure new development is consistent with the development standards described in Policy L-3.6 and compatible with surrounding existing residential uses. The size of the homes on the smaller lots should be limited by a reduced floor area ratio or other zoning requirements.

Policy L-3.5: Along the Yarrow slope allow residential density of three to five dwelling units per acre.

Along the slope west of the Yarrow Bay wetlands, because of the presence of geological, wetland and stream constraints found in the area, residential densities of three to five dwelling units per acre are appropriate. New development along the slope should also follow the development standards listed below for the Houghton and Yarrow slopes.

Policy L-3.6: Regulate development on Houghton and Yarrow slopes to avoid damage to life and property.

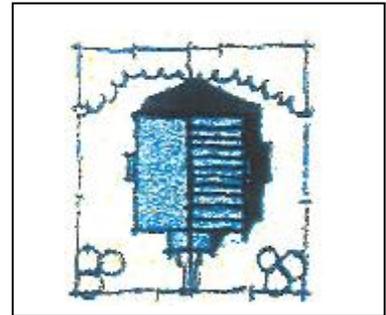


Development Standards for Houghton Slope and Yarrow Bay Slope

The Houghton and Yarrow slopes contain areas identified as potential landslide and erosion hazards. New development on these slopes should use the best management geotechnical practices specific to the site and design of project to minimize any potential hazards. New development should be subject to the following conditions:

1. A slope stability analysis should be prepared which evaluates the site and surrounding area to minimize damage to life and property. Specific structural designs and construction techniques to ensure long term stability should be considered as part of the analysis. Within the PLA 3C area, as part of a development permit, the applicant's geotechnical report should include a hazard assessment. The analysis and recommendations should be reviewed by a qualified geotechnical engineer selected and retained by the City at the applicant's expense.
2. Hillsides with the steepest slopes and or ravines may be required to be undisturbed in a natural condition and retained as permanent natural open space through the creation of a greenbelt easement or dedication.
3. A covenant which indemnifies and holds harmless the City for any damages resulting from slope instability should be required to be recorded on the property.
4. Lot coverage should be minimized to retain vegetation and watercourses.
5. Surface water runoff should be controlled at predevelopment levels.

6. Watercourses and wetlands should be retained in a natural state.
7. Vegetative cover should be retained to the maximum extent possible.
8. Flexibility in lot size and layout should be allowed through clustering of structures away from steep slopes and drainage courses and to preserve significant grouping of trees. Minimum lot size should be no less than 5,000 sq. ft. *(does not apply to Yarrow Bay slope)*
9. For sites containing wetlands, the maximum density allowed with sensitive areas is prescribed in KZC Chapter 90.
10. In the PLA 3C area to provide flexibility in site design, one required side or rear yard may be 0 feet (zero lot line) for the internal lot of a short plat or subdivision to allow for a two unit attached homes provided that:
 - a. individual dwelling units are on separate lots, and
 - b. no more than two units may be in one building, and
 - c. Two- unit homes are designed to look like a detached single family house using design techniques such as limiting the points of entry on each facade pitched roofs and covered porches.



Goal L-4: Allow alternative residential development options that are compatible with surrounding development.

Policy L-4.1: Allow a variety of development styles that provide housing choice in low density areas.

Providing housing options for a wide spectrum of households is an important objective to support and encourage. Alternative housing provides more housing choice to meet changing housing demographics, such as smaller households and an aging population. Allowing design innovations can help lower land development costs and improve affordability. Compatibility with the predominant detached single family housing style in the neighborhood will determine the acceptance of housing alternatives. Alternative housing styles such as cottage, compact single family, and common wall (attached) homes, accessory dwelling units, and clustered dwellings are appropriate options to serve a diverse population and changing household size and composition.

Policy L-4.2: Encourage diversity in the size of dwelling units by preserving and/or promoting small homes on small lots.

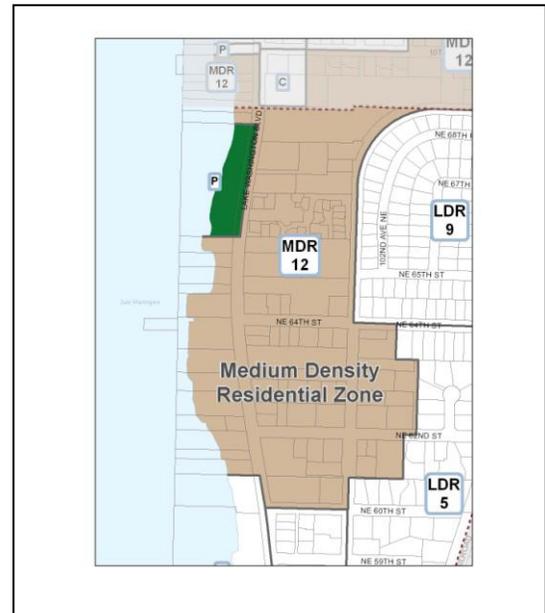
Diversity can be achieved by allowing properties to subdivide into lots that are smaller than the minimum lot size allowed in the zone if at least one of the lots contains a small home. This incentive encourages diversity, maintains neighborhood character, and provides more housing choice. Allowing smaller lots can also be an option for property containing environmentally sensitive areas.

Up to 50 percent of the single family lots within a subdivision should be allowed to be smaller than the zoning normally allows if a small home is retained or built on the small lots. The lots containing the small homes should be no less than 5,000 square feet in the RS 7.2 zones and no less than 6,000 square feet in the RS 8.5 zones.

Medium Density Residential

Policy L-4.3: In the north portion of the neighborhood west of Lakeview Drive, allow multifamily use at medium density 12 dwelling units per acre.

In the northern portion of the neighborhood west of Lakeview Drive, medium density residential is appropriate. Some parcels have multi-family development that was constructed under previous higher density development which is non-conforming under the current zoning. The Zoning Code contains the regulations governing nonconforming density.



Yarrow Bay Wetlands and Shoreline Areas

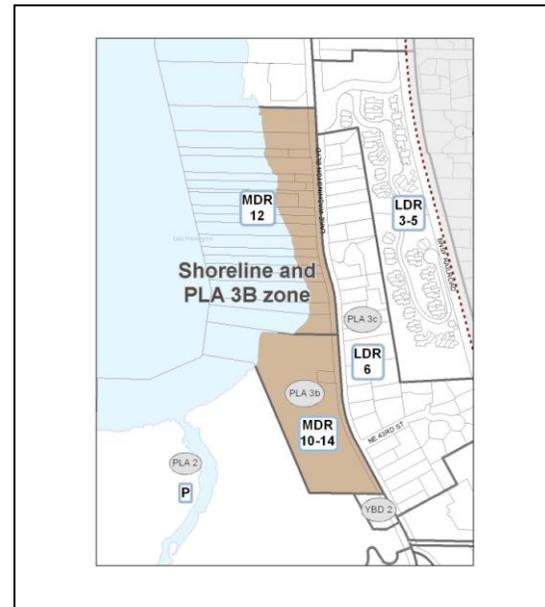
Policy L-4.4: In the upland area of Planned Area 2 adjacent to Points Drive allow multifamily development at a density of 10-12 dwelling units per acre. In the wetland portions of Planned Area 2 limit residential development.

Planned Area 2 is located adjacent to the Yarrow Bay wetlands. Any development in this area should maintain the functional integrity of the wetlands and the biologic functions of storage and cleansing of runoff waters (see Shoreline Area Chapter and Natural Environment section). In 1987, the majority of the Yarrow Bay wetlands were dedicated to the City of Kirkland to ensure protection. The wetlands have also been identified as an area subject to uneven settlement problems. For the land west of the Yarrow Bay wetlands and along the shoreline, densities should be extremely limited. Upland portions of PLA 2, outside the shoreline boundary and adjacent to or with direct access to Points Drive, have been developed as medium-density multifamily development (up to 12 dwelling units per acre).

Policy L-4.5: Allow multifamily, hotel/motel, and limited marina use within Planned Area 3B.

Planned Area 3B is fully developed with multifamily residential. Because of its adjacency to existing single-family and multifamily uses on the east and north, the development of office or other similar nonresidential uses in Subarea B would not be desirable. Use of existing multifamily units for overnight lodging, however, would be acceptable provided that the site development maintains its residential character and that accessory restaurants, retail, or similar uses are not allowed.

North of Yarrow Bay, existing development on the shoreline is primarily residential. As discussed in the Shoreline Area Chapter of this Comprehensive Plan, residential uses should continue to be permitted along the shoreline.



Policy L-4.6: Prohibit commercial uses along the shoreline south of Planned Area 15.

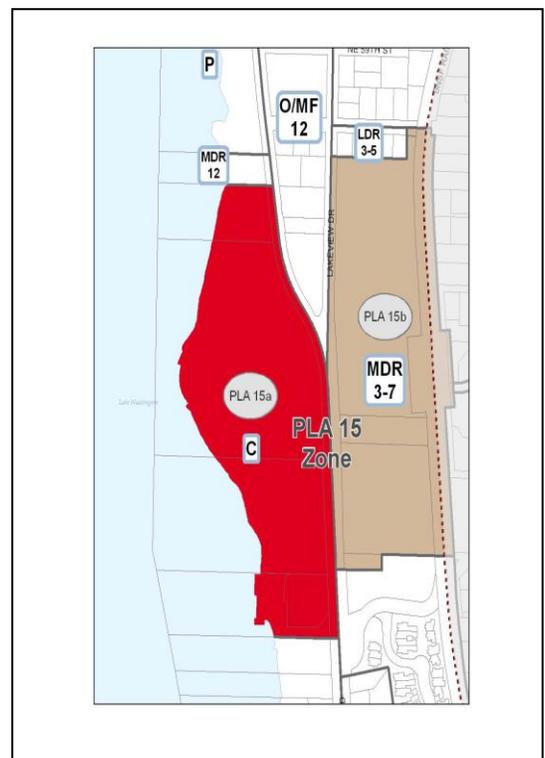
Commercial uses should not be permitted along the shoreline south of Planned Area 15 due to the residential character of the area as well as access and visibility limitations. North of Planned Area 15, commercial activities should be permitted if public access to and use of the shoreline is enhanced. Other standards for shoreline activities are specified in the Shoreline Master Program.

Planned Area 15

Planned Area 15 is comprised of Subarea A located west of Lake Washington Boulevard and Subarea B east of Lake Washington Boulevard and Lakeview Drive.

For many years, most of Subarea 15A was the site of the Lake Washington Shipyards, which ceased production in the late 1940s. The site was used as the Seattle Seahawks training facility until the late 1980s. The site is now developed as Carillon Point, a mixed-use commercial center containing office, retail, hotel, restaurant, marina and residential uses.

South of Carillon Point is the Yarrow Bay Marina containing over-water covered moorage facilities, dry dock boat storage, boat launch, boat sales and service, a pump-out facility and accessory office building. The marina has been in existence since the 1950's. In 2008, it was remodeled and added a shoreline public use area and public walkway connection to Carillon Point



and the condominiums to the south. An office building exists on the parcel fronting Lake Washington Boulevard.

The majority of Subarea 15B is developed with medium to high density residential developed in conjunction with the Carillon Point Development to the west. Slopes in Subarea B are designated as containing moderate to high landslide hazard areas. Carillon Creek flows from Carillon Woods down the hillside through Carillon Point on its way to Lake Washington. With the development of Carillon Point the stream was enhanced with native plantings to improve fish habitat and serves as a natural amenity along the shoreline pedestrian walkway in Subarea A.

Goal L-5: Ensure development in PLA 15 continues to provide water oriented uses, visual and direct access to the lake, and maintains the natural characteristics and amenities of the stream and Houghton Slope.

Policy L-5.1: Within PLA 15 A , provide a mix of uses with priority to water dependent, water related and water enjoyment uses located along the shoreline. Allow residential development at a density of 12 dwelling units per acre.

Subarea 15A, west of Lake Washington Boulevard is developed with a mixture of uses. The City's Shoreline regulations KZC Chapter 83, governs the types of uses and activities allowed in PLA 15A. Shoreline regulations designate the area as an Urban Mixed shoreline environment. Like the shoreline areas lying immediately to the north and south, residential development in Subarea A is allowed at a density of 12 dwelling units per acre.

Policy L-5.2: Retain water dependent uses and the view corridor south of Carillon Point.

The marina development south of Carillon Point provides water-dependent uses, recreational activities and services. It incorporates a waterfront public use area and public shoreline pedestrian walkway connection to Carillon Point to the north and residential property to the south. A view corridor from Lake Washington Boulevard to the water should be maintained across the southern portion of both sites including maintaining the height of vegetation to not obscure the view of Lake Washington.

Goal L-6: Recognize and enhance Carillon Point as a mixed use employment center and tourism destination.

In the hierarchy of commercial areas in Kirkland, the Land Use Element designates Carillon Point as a business district with its mix of office, retail, restaurants, housing, hotel, service businesses and marina. Carillon Point serves not only as a regional employment center but visitors and local communities frequent the area as a waterfront tourism destination.

Policy L-6.1: Govern development and uses at Carillon Point by an approved Master Plan.

Carillon Point was developed under a master plan with an extensive public review and City approval process. Any future major change to the development should be reviewed to ensure Master Plan compliance.

The Master Plan and Zoning Code regulations for PLA 15A ensure that development will minimize impacts to existing uses in the vicinity including view obstruction, traffic volume and movement, noise and glare from uses of higher intensity, and compatibility of building scale. The Master Plan includes specific design guidelines for the site plan, circulation plan, and architectural design for the buildings.

The following is a summary of the key principles of the Master Plan to guide uses and development of the area (see KZC PLA 15 A and B for more detail):

- Within the shoreline area water dependent, water related, and water oriented commercial uses should be included such as marinas, fueling and sewage pump out facilities, and possibly tour boat operations, float plane service, passenger only ferry or water taxi facility, and public amenities access to piers for fishing, strolling or other pedestrian activities.
- Public access to and along the water's edge and waterfront public use areas should be maintained including public access signs.
- Public improvements adjacent to Lake Washington Boulevard are also desirable, such as wide sidewalks.
- Visual access to Lake Washington from Lake Washington Boulevard should be maintained. To achieve greater visual access, building height, setback and view corridor requirements may be varied. Views from existing developments to the east should be protected.
- Manage parking on site to avoid impact to adjacent properties.
- Traffic impacts to Lake Washington Boulevard should be minimized including limiting vehicular access points.
- Subarea B has been fully developed as part of a master plan, including an allowed transfer of density from the PLA 15A Subarea.

Neighborhood Oriented Commercial, Professional Office and Multi Family

Goal L-7: Accommodate a mix of uses south of NE 60th Street between Lakeview Drive and Lake Washington Boulevard consistent with the development pattern in the neighborhood.

Policy L-7.1: South of NE 60th Street between Lakeview Drive and Lake Washington Boulevard NE, allow professional offices and medium-density residential use at twelve dwelling units per acre.

Allow small neighborhood oriented retail businesses provided that:

- a. Front facades of buildings are not facing or oriented to Lakeview Drive.**
- b. Vehicular access is not directly from Lakeview Drive.**
- c. Internally lit signs are not located along Lakeview Drive and NE 60th St.**

Medium-density residential uses, at a density of 12 dwelling units per acre, and professional offices should be considered the primary uses. Small, neighborhood oriented retail, convenience stores, coffee shops or similar uses that serve primarily the surrounding neighborhood are appropriate except where building front facades would face Lakeview Drive or direct vehicular access is provided along Lakeview Dr. because of potential impacts to low density residential uses across the street. Internally lit sign faces should also not face Lakeview Drive. Appropriate uses are those that focus on local pedestrian traffic and will not result in spillover parking on neighborhood streets. Vehicle sales, service, and drive-through facilities should not be permitted in the PR zone.

Policy L-7.2: Provide a historic interpretive sign on the site of the old Houghton Post Office.

On the eastside of Lake Washington Blvd between NE 60th - 59th Streets, two older single family house style buildings and a fast food restaurant exist. One of the older buildings was constructed in the early 1900s and was the early site of the Houghton Post Office. Both of the older buildings clearly do not meet zoning standards for building setbacks parking, and other zoning non-conformances are likely. The other structure was built in 1940's. The restaurant meets most or all of the current zoning standards for such uses. All three buildings are of a scale and design which are compatible with neighboring residential uses.

These parcels are appropriate for multifamily residential, office, and small, limited in size, and neighborhood oriented commercial uses discussed in L.7.1. Continuation of existing office and



commercial uses within the existing nonconforming structures should be allowed and reviewed administratively.

Because of the non-conforming conditions of both properties discussed above, some flexibility in applying normal zoning standards should be allowed provided certain development standards are met. If a change of use is proposed that requires more parking than the current use, the proposal should be evaluated for consistency with the following standards:

- a. The use should provide a strong pedestrian orientation.
- b. The number of required additional stalls for the new use should be determined based on the actual parking demand. New on-street parking on NE 60th St. may be counted toward a portion of the required parking with necessary improvements to the right of way provided at the developer's expense.
- c. New parking areas should be placed, screened, and buffered to mitigate impacts to nearby residential uses.
- d. A historic interpretive sign should be erected on the site of the old Houghton Post Office.
- e. Redevelopment of the properties should comply with all applicable zoning standards.

Policy L-7.3: Along neighborhood streets, parking associated with commercial development and waterfront parks should be monitored to avoid parking congestion.

Over time the area south of NE 60th St has transitioned from single family and industrial uses to primarily office and multifamily uses. Increased parking congestion along streets in the neighborhood from summer use of Houghton Beach Park and nearby businesses can also be a problem limiting access to surrounding businesses or the park. Therefore, parking on surrounding streets should be monitored to ensure access to parks and businesses.

Policy L-7.4: Limit commercial activities north of NE 64th Street east of Lake Washington Boulevard.

A convenience commercial grocery store located on Lake Washington Boulevard and NE 64th Street serves a localized need by providing limited grocery service to the surrounding residential neighborhood. Limited neighborhood commercial uses should be allowed to remain at this site and improvements should be encouraged to enhance its compatibility with surrounding residential uses and the scenic character of Lake Washington Boulevard. No further development of retail commercial facilities in this residential area should be permitted.

Yarrow Bay Business District

The Yarrow Bay Business District serves as an employment center containing corporate headquarters, large office complexes, restaurants, a motel, schools, and convenience services for local office workers and freeway travelers along SR 520. The Business District is divided into subareas primarily because of differences in topography and maximum building height.

Goal L-8: Promote the vitality of the Yarrow Bay Business District as a coordinated, mixed use district.

The policies in this section are intended to support and strengthen the business district to evolve into a greater mix of retail, office, services, and housing to provide a more vibrant commercial district with greater pedestrian orientation and connections to transit facilities. Focus will be on integration of businesses and residents with a potential redevelopment of the area into a mixed use transit oriented district.

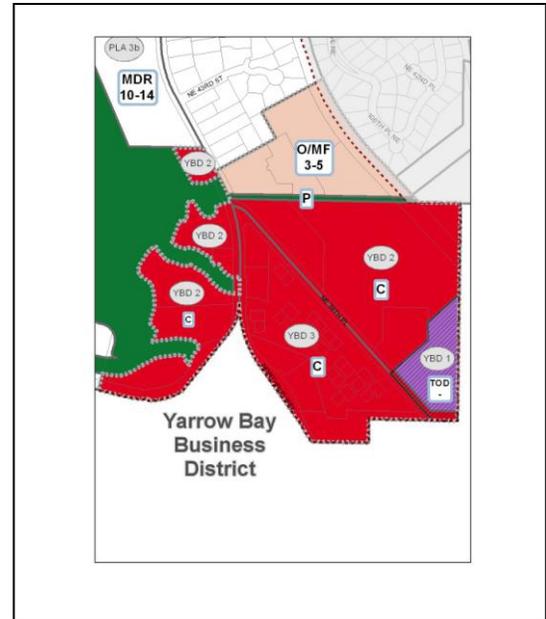
Due to the availability of adequate public services, easy access to major arterials, the freeway, and the overall compatibility with adjacent land uses, the predominate use should be devoted to commercial activities. Retail uses may be included as part of office structures but not as stand-alone large structures. Incorporating residential uses with commercial development would strengthen the area into a twenty four hour active community. All developments should include landscaping and other elements to enhance this interchange as a gateway to the City.

See also the Urban Design section regarding design policies for the Yarrow Bay Business District.

Yarrow Bay Business District 1- YBD 1

The property containing the South Kirkland Park and Ride is about seven acres in size, with approximately equal portions of the site lying within the cities of Kirkland and Bellevue. The site is owned by King County, and currently developed as a Park and Ride with approximately 600 parking stalls and a transit facility. The site is generally level, but has a steep slope along the eastern and southeastern boundaries within the city of Bellevue section of the site. Tall trees and heavy vegetation are present within the hillside areas.

King County has identified the South Kirkland Park and Ride as a potential site for transit-oriented development (TOD) for several years. Affordable housing is generally included in King County TOD projects, and is anticipated to be a significant component of future residential development at the South Kirkland site. The City of Kirkland has identified transit-oriented development at the South Kirkland Park and Ride as a key affordable housing strategy. The City supports multifamily residential as the predominant use of the site in a transit-oriented-development project, with a variety of other uses to be allowed as well.



The South Kirkland Park and Ride property may continue as a transit facility with the potential for office use. Alternatively, if the site is redeveloped with TOD, the principles discussed below should be used to guide development at the Park and Ride.

Policy L-8.1: Provide for affordable housing.

Ensure that transit-oriented development provides for mixed-income housing, including a minimum of 20 percent of total units to be affordable to low and/or moderate income households.

- Development should strive to achieve greater affordability for at least 20 percent of its units, with an additional 25 percent to be affordable to median income households, through the use of as many funding sources as are necessary.

Policy L-8.2: Ensure high quality site and building design.

Develop implementing regulations for coordinated development of the entire site.

- Establish standards for building height and mass that acknowledge site topography and existing vegetation as factors for consideration.

Implement design standards for YBD 1.

- Ensure that regulations support appropriate building scale and massing throughout the site, produce buildings that exhibit high quality design and incorporate pedestrian features and amenities that contribute to a livable urban village character for the TOD.
- Provide guidance for the streetscapes along NE 38th Place and 108th Avenue NE to ensure buildings do not turn their backs on the streets and development provides a welcoming and attractive presence at this gateway to Kirkland.
- Protect the vegetative buffers and significant trees along the site's eastern and southeastern borders through development standards.
- Minimize the visual impacts of parking facilities from adjacent rights-of-way.



Foster the creation of a vibrant and desirable living environment through the use of high quality design, public amenities and open space.

Promote sustainable development through support of green building practices at the Park and Ride.

Policy L-8.3: Maximize effectiveness of transit-oriented development (TOD).

- Create the opportunity for Transit-Oriented Development (TOD) at the site through the development of standards and regulations that support necessary densities.
- Expand opportunities for retail development, incidental office development, and childcare facilities at the site to serve users of the Park and Ride, site residents and others.
- Provide opportunities for all types of users of the site to access the BNSF corridor; however it is developed, along the eastern boundary of the Park and Ride site.
- Reduce the need for parking at the site through regulations that promote shared parking between uses, and incentives to support alternatives such as shared car services and electric cars.
- Mitigate traffic, visual, noise and other impacts from more intensive development of the Park and Ride to the surrounding street network and residential areas.



Policy L-8.4: Coordination with the City of Bellevue.

- Coordinate an approach for the review and approval of development proposals for the site with the City of Bellevue.
- Manage emergency services to the site through agreements with the City of Bellevue.

Yarrow Bay Business District 1 and 2- YBD 2 and YBD 3

Policy L-8.5: In YBD 2 and YBD 3 encourage a mix of office, retail, hotels, restaurants, housing, and services and limit the size of freestanding retail establishments.

Development in YBD 2 and YBD 3 is appropriate for a mix of uses such as offices, specialty retail banks, hotel, motel, restaurants, schools or day care facilities, residential and grocery stores to serve offices and other employment nearby, or the freeway traveler. Individual freestanding retail establishments should be limited in size to less than 15,000 sq. ft. of gross floor area unless they are part of a mixed use project to avoid large scale, stand alone retail uses more appropriate for other business districts. Drive through facilities should not be permitted in the Yarrow Bay Business District because they discourage pedestrian oriented development.

The clustering of development away from wetlands and streams is encouraged. Cochran Springs Creek requires protection. This area is the entrance to the City and, hence, the character of development is important. Because of the prominent location of the development as a southern gateway to the City, a

gateway feature, art, superior landscaping, and pedestrian amenities should be provided along Lake Washington Blvd (see *Urban Design Section*).

Policy L-8.6: Limit maximum building height to 5 stories in YBD 2 and YBD 3. Reduce building mass generally above the second floor with upper story setbacks, and vertical and horizontal modulation evaluated through the Design Review process.

Building height should be slightly lower in YBD 3 than YBD 2 to accentuate the rise in the topography of the district from west to east.

Policy L-8.7: At the southern end of the Houghton Slope professional offices or multifamily uses are allowed.

An existing office development is located at the south portion of the Houghton slope. The office land use designation should not extend further northward into the residential area on the southern end of the Houghton Slope. The offices provide a desirable transition to the residential area to the north and east. Accessory commercial uses are only permitted to serve the offices.

Policy L-8.8: Establish urban design standards for commercial and mixed use residential development in the Yarrow Bay Business District

Design Guidelines should be created to encourage attractive development in this gateway to the City. The design standards should encourage greater pedestrian orientation and pedestrian connections to other businesses, to the South Kirkland Park and Ride and other transportation facilities. Along the perimeter of the district, buildings should be stepped back vertically from the street and designed to be compatible with adjacent residential development.

Goal L-9: Provide transitions between residential uses and commercial uses.

Policy L-9.1: Minimize impacts of commercial development on residential areas and protect neighborhood character.

When locating more intensive commercial uses along the perimeter of commercial activity nodes, techniques should be used to minimize impacts on adjacent residential areas such as ensuring there is adequate parking on neighborhood streets for residents and businesses, minimizing noise in evening hours, and minimizing glare from commercial lighting. Regulating building height, building mass, building placement, and vehicular access and providing landscape buffers are effective transition techniques to reduce impacts of commercial uses on surrounding residential uses.

6. Transportation

The circulation patterns in the Lakeview Neighborhood are well established and permit through traffic to flow north and south on both Lakeview Drive and Lake Washington Boulevard. Northup Way, NE 52nd Street and NE 68th Street provide the east-west connections to the Central Houghton neighborhood.

Goal L-10: Improve vehicle, pedestrian and bicycle mobility along Lake Washington Boulevard NE.

Lake Washington Boulevard is designated as a principal arterial and provides the major north-south route through Kirkland south of the Central Business District and west of I-405 (See Figure L- 4). The Boulevard also provides local access for a substantial number of residential developments and businesses. A significant proportion of existing traffic, however, is probably attracted to the Boulevard as much because of the scenic vistas of Lake Washington and ease of convenience or necessity. The scenic qualities of the Boulevard also contribute to making it a major pedestrian and bicycle corridor, serving waterfront park users, joggers, strollers, and Downtown shoppers.

Traffic on Lake Washington Boulevard has greatly increased, particularly during morning and evening commute periods. This congestion restricts local access to and from the Boulevard and has created noise, safety problems, and conflicts for pedestrians, bicyclists, and adjacent residents.

Policy L-10.1: Enhance Lake Washington Boulevard NE as a scenic, recreational, open space and transportation corridor.

Improvements to the Boulevard could help accommodate its broader amenity function in such a manner that the safety of all the Boulevard's diverse users is enhanced, while significant amounts of through traffic are not diverted to other arterials. Accordingly, a master plan or set standards for Lake Washington Boulevard should be established through a public process that considers the following objectives:

1. Strategies to relieve congestion during commute times to improve traffic flow and provide gaps in traffic to improve access from adjacent properties.
2. Widen sidewalks to improve pedestrian circulation on both sides of the street with the widest sidewalks on the west side.
3. Improve pedestrian crossings at intersections and adjacent to waterfront parks where safety considerations allow such installation. One option that could be studied is the concept of providing a pedestrian bridge across Lake Washington Blvd in the Yarrow Bay Business District to facilitate pedestrian crossing and provide a gateway feature to the City.
4. Use of landscaped median islands to separate traffic and provide pedestrian safety where center left-turn lanes or on-street parking are not needed.
5. Widening bicycle lanes.
6. Installation of on-street parking in areas of high parking demand, provided that traffic safety will not be impaired.
7. Installation of streetscape amenities such as public art, pedestrian lighting, street furniture, and low level landscaping that will not obscure views of the Lake and will enhance the pedestrian experience along the street.

Policy L-10.2: Implementation of the above street improvements should be considered through the City's Capital Improvement Program process and site specific with private redevelopment.

The means for implementing these improvements should be both on a comprehensive area wide basis and to the extent possible, on an incremental basis by encouraging or requiring them to be incorporated into private development.

Policy L-10.3: Support regional transportation solutions that will reduce commuter or pass through traffic through the neighborhood and along Lake Washington Blvd. NE.

Also important to the successful achievement of a greater amenity and mobility functions for Lake Washington Boulevard will be traffic improvements that are regional in scope. Accordingly, the City should support and encourage the following regional solutions:

1. Alternatives to the single-occupancy vehicle for commuting purposes, such as increased use of Metro Transit, commuter pool, High-Occupancy Vehicles (HOV), and the investigation of future modes, such as light rail.
2. Improvements to the I-405/SR 520 corridors.

Policy L-10.4: Maintain Lakeview Drive as a minor arterial and alternative route to Lake Washington Blvd. NE through the neighborhood.

Lakeview Drive is designated as a minor arterial and fully developed with two through lanes, bicycle lanes, sidewalks, and street trees. From its intersection with Lake Washington Boulevard, Lakeview Drive provides the primary route to the Houghton Business District and to State Street, which in turn provides access to the Central Business District. Lakeview Drive/ State Street provide an alternative north-south vehicular route from Lake Washington Blvd during peak commute times. Future traffic levels should be monitored and necessary measures undertaken to mitigate impacts.

Policy L-10.5: Improve pedestrian and bicycle circulation systems as both recreation amenities and as non-motorized transportation connections to neighborhood as well as city and regional destinations.

The path/trail system shown in Figures L-5 and L-6 indicates the major elements of the pedestrian and bicycle circulation network in the neighborhood. Pedestrian and bicycle pathways provide a recreation as well as transportation function. The following pedestrian and bicycle connections should be priorities within the neighborhood:

1. From Lake Washington Blvd east to the future Eastside Rail Corridor on the railroad right of way and the Central Houghton Neighborhood.
2. Between properties in the Yarrow Bay Business District and to the South Kirkland Park and Ride and future Transit Oriented Development.

3. Along the Lake Washington shoreline with connections to Lake Washington Boulevard as required by the shoreline regulations. Existing signs marking the location of public shoreline pedestrian walkways should be maintained by private development.
4. From Yarrow Bay Wetlands to Watershed Park.
5. Along NE 60th Street trail from Houghton Beach Park east through the City to connect to the regional trail at Marymoor Park in Redmond.
6. From SR 520, and Bellevue to the South.

These trails will cross a combination of City parklands, City rights-of-way, and public access easements. The trails should be part of the City's Active Transportation Plan and implemented through the Capital Improvement Program or -private development. The trails will improve neighborhood access and enhance the unique areas they traverse.

Policy L-10.6: Support development of a future Eastside Rail Corridor as multipurpose trail.

Development of the old BNSF railroad right of way as a multipurpose corridor for bikes, pedestrians and potentially for rail transit should be designed to:

- Result in a public benefit to the citizens of Kirkland.
- Serve as a gateway to the City.
- Provide neighborhood connections.
- Be compatible in scale with adjacent neighborhoods.
- Ensure a high degree of safety.
- Show environmental stewardship.

7. Open Space and Parks

Goal L-11: Ensure adequate park and recreation facilities in the Lakeview Neighborhood.

Current park needs for the Lakeview Neighborhood are being met by existing facilities. Terrace Park is a neighborhood park. Marsh and Houghton Beach Park are waterfront parks, and Yarrow Bay Wetlands is a passive natural area (see Figure L-1).

Policy L-11.1: The City should continue to acquire property in Lakeview for recreation purposes wherever possible.

As properties adjacent to existing parks become available, the City should seek opportunities to acquire land for expansion. In addition, shoreline street ends should be accessible and enhanced for public enjoyment.

Policy L-11.2: Restore the shoreline within waterfront parks. Replace hard shoreline armoring with native plants and soft armoring techniques while ensuring erosion protection and public access to Lake Washington.

A goal in the Shoreline Area Chapter is to replace hard armoring such as bulkheads and rockeries with softer, natural shorelines planted with native plants to improve shoreline habitat including along waterfront parks. Park restoration can be used as a model for how private property owners can restore their shoreline.

Policy L-11.3: Maintain wide, expansive views of Lake Washington through waterfront parks. Prevent view obstruction by vegetation or placement of structures.

A high priority for the neighborhood is to maintain the wide expansive views of Lake Washington and beyond, especially at waterfront parks. Ongoing maintenance of existing vegetation at parks to retain views of the Lake from Lake Washington Boulevard and properties to the east is a priority. Shoreline regulations also encourage planting of shoreline vegetation and trees. A balance must be achieved between shoreline restoration with the planting of vegetation with retaining views.

As new trees or vegetation are planted, the placement and variety should carefully be chosen to avoid view obstruction. Neighbors to the east who may be impacted by new vegetation should be involved in providing input on the placement and variety. In addition to the normal notification techniques, the Parks and Community Services Department should notify surrounding residents and the neighborhood association prior to placement of new trees or vegetation that have the potential for impeding views.

Planning Commission recommends the following alternative text for Policy L-11.3:

Policy L-11.3: Maintain public views of wide, expansive views of Lake Washington through waterfront parks. Prevent view obstruction by vegetation or placement of structures.

~~A high priority for the neighborhood is to maintain the wide expansive views of Lake Washington and beyond, especially at waterfront parks.~~ Ongoing maintenance of existing vegetation at parks to retain views of the Lake Washington and beyond from Lake Washington Boulevard ~~and properties to the east~~ is a priority. Shoreline regulations also encourage planting of shoreline vegetation and trees. A balance must be achieved between shoreline restoration using vegetation with the planting of vegetation with retaining while maintaining public views.

As new trees or vegetation are planted, the placement and variety should carefully be chosen to avoid view obstruction. Neighbors to the east who may be impacted by new vegetation should be involved in providing input on the placement and variety. In addition to the normal notification techniques, the Parks and Community Services Department should notify surrounding residents and the neighborhood association prior to placement of new trees or vegetation that have the potential for impeding views.

Policy L-11.4: Seek opportunities to improve wildlife habitat, increase pedestrian and non motorized boat access, if ecological functions can be enhanced at Yarrow Bay wetland.

Yarrow Bay wetlands are one of the largest remaining wetlands on Lake Washington and serve as valuable wildlife habitat, water quality functions as well as aesthetic open space for the community. Public access is available by existing public rights of way but is limited. Improving access for viewing

wildlife and environmental education through constructing a series of boardwalks should be evaluated provided ecological functions are protected. Wildlife habitat may be improved by removing upland and underwater invasive plants in and near the wetlands. Any future development of the park should be undertaken following a community based master planning process. Considerations for a future park master plan should include protection and enhancement of natural resources while providing appropriate public access. Opportunities for further acquisition of adjacent land in order to preserve and protect the wetlands and associated wetland buffers should also be pursued.

8. Public Services and Facilities

Goal L-12: Provide public and private utility services for the Lakeview Neighborhood.

Water, sewer, and drainage facilities are adequate for planned development in the Lakeview Neighborhood. The goals and policies contained in the Utilities, Capital Facilities and Public Services Chapters of the Comprehensive Plan provide the general framework for these services and facilities.

Policy L-12.1: Encourage undergrounding of overhead utilities.

In order to contribute to a more amenable and safe living environment and to enhance views and a sense of community identity, the undergrounding of utilities should be actively encouraged.



9. Urban Design Policies

Lakeview's unique urban design assets are identified on Figure L-7 and described below.

Lakeview's north-south orientation and west facing Houghton slope allow for a majority of residents to take advantage of the views of Lake Washington, Seattle and the Olympic Mountains. Lake Washington and the Yarrow Bay Wetlands are two visual landmarks that provide a sense of openness and natural beauty. Preserving public views of Lake Washington and beyond from Lake Washington Boulevard is a high priority. Other landmarks in this neighborhood include the waterfront parks and the historic places. The Lakeview Neighborhood serves as the southwestern gateway to the City at SR 520 and Lake Washington Blvd. and 108th intersections.

Goal L-13: Preserve public view corridors and natural features that contribute to Lakeview's visual identity.

Policy L-13.1: Preserve public scenic views and view corridors of Lake Washington, Seattle and the Olympic Mountains from public rights of ways and waterfront parks.

Public and private view corridors along Lake Washington's shoreline are important assets and should continue to be enhanced as new development occurs. Wide, expansive views of Lake Washington looking west from public rights of ways and waterfront parks should be maintained. Street trees along rights of ways and trees in public parks that offer local and territorial views should be of a variety that will not block views as trees mature.

Policy L-13.2: Preserve natural landforms, vegetation and scenic areas such as the Houghton and Yarrow Bay Slopes and Yarrow Bay Wetlands.

Lakeview's natural landforms, such as steep slopes and ravines, contain significant woodlands, streams, open space and wildlife that help define neighborhood character. These natural landforms should be preserved, restored and incorporated into the design of new development.

Goal L-14: Enhance neighborhood gateways to strengthen neighborhood identity.

Policy L-14.1: Establish gateway features at the locations identified in Figure L.7 through public and private efforts.

Gateways welcome residents, employees and visitors into the City and help define neighborhood identity. Gateways can be in the form of natural feature such as landscaping or structures, such as signs or buildings. The northern gateway to the neighborhood is at NE 68th Street where views of Lake Washington are prominent. At the "Y" intersection at Lake Washington Boulevard and Lakeview Drive, the triangular median with the sculpture serves as a gateway to the Carillon Point development, neighborhood businesses and shoreline parks.

The intersections at SR 520 and Lake Washington Boulevard and at 108th Avenue NE provide two southern gateways to the City. These intersections provide opportunities to enhance the gateways with future private development or through community efforts. For example, the existing gateway sign located on the east side of Lake Washington Blvd at NE 38th Pl by Cochrane Springs Creek, could be enhanced by relocating the sign to a more prominent location, removing the clutter of street signs and utility poles, screening the adjacent utility box or highlighting the stream crossing and coordinated with a similar gateway treatment on the west side of the boulevard.

The City should pursue opportunities to work with private property owners to install gateway features as part of future development. Improvements such as signs, public art, structures, lighting, and landscaping could be included.

Goal L-15: Provide public improvements that contribute to a sense of neighborhood identity and enhanced visual quality.

Policy L-15.1: Identify design standards for Lake Washington Boulevard, NE 38th Pl, and Northup Way right of ways that include:

- ***Adequate sidewalk widths (preferably 10' in width) on both sides of the street to encourage greater pedestrian circulation.***
- ***Street trees that are of a variety that will not obstruct views of Lake Washington from public rights of way, properties to the east or businesses***
- ***Public amenities such as benches, pedestrian lighting, view platforms, public art and directional signs pointing to public facilities and points of interest.***

Lake Washington Boulevard is a major pedestrian shoreline promenade connecting Downtown points south and north. Within the Yarrow Bay Business District, design standards for NE 38th PL, Lake Washington Blvd and Northrup Way should be developed to ensure a consistent design including wider sidewalks, landscape strips, decorative street lighting and street furniture. As redevelopment occurs, sections of the sidewalk on both sides of the street should be improved to meet these standards. Opportunities to install public art and street furniture along arterials throughout the neighborhood should also be pursued.

Yarrow Bay Business District

The urban design vision for the Yarrow Bay Business District is to transform the suburban style office park development into a more integrated, mixed use commercial and residential district. Implementing the following strategies will help achieve this vision such as allowing a broader range of commercial uses with residential above the ground floor, improving pedestrian connections between properties, businesses, the South Kirkland Park and Ride transit facility and SR 520 freeway. Providing public plazas, green spaces and pedestrian amenities in new development will help create a sense of place for employees and residents. New design standards and design review for new development will ensure quality architecture, site design and identity for the commercial district.

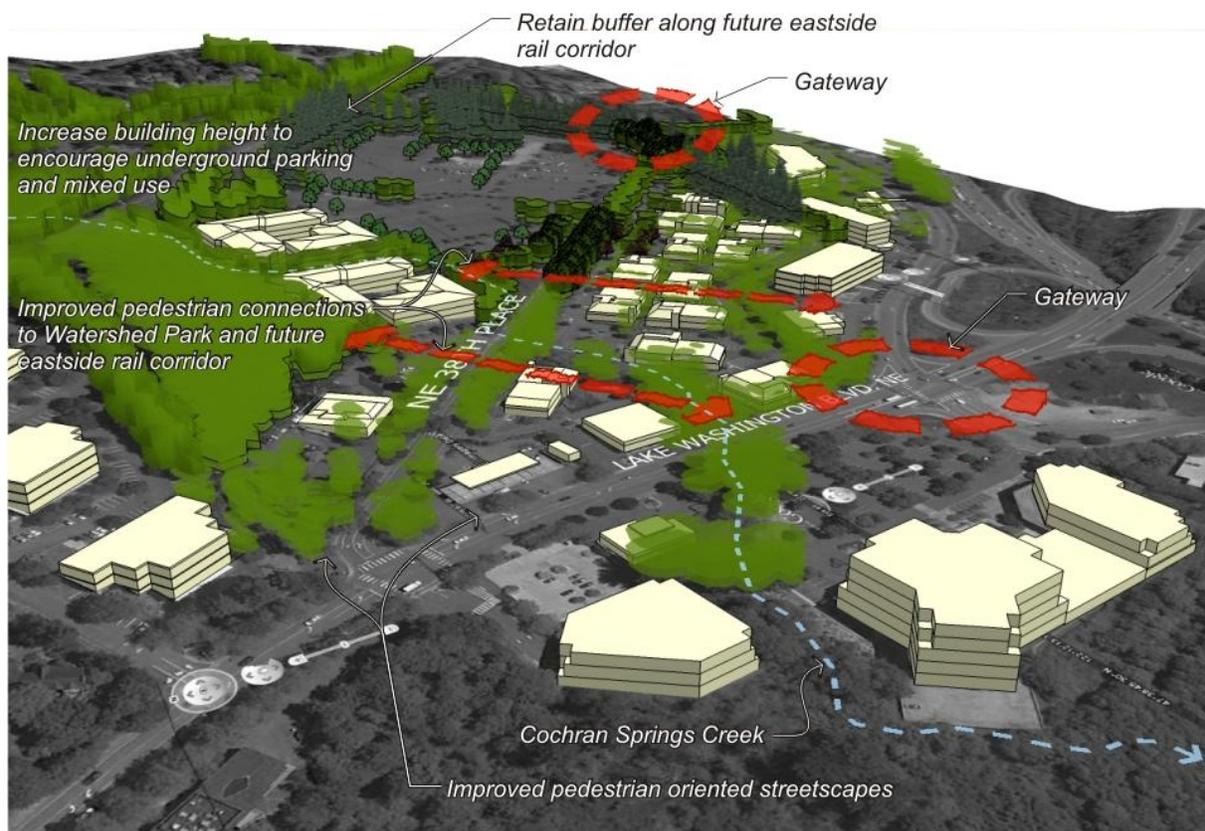


Illustration by Makers Architects shows the future urban design concept for the Yarrow Bay Business District.

Goal L-16: Promote high quality design in the Yarrow Bay Business District.

Policy L-16.1: Establish design guidelines and regulations that apply to all new, expanded or remodeled commercial, multifamily or mixed use buildings in the Yarrow Bay Business District. Ensure that guidelines should address the following design principles:

- ***Promote pedestrian oriented design techniques such as minimizing blank walls, providing generous window treatments, awnings, superior building materials, open space plazas, and pedestrian amenities especially around retail uses.***
- ***Encourage pedestrian links between uses on site, to adjacent properties and to the transit facility at the South Kirkland Park and Ride property.***
- ***Enhance streetscapes along Lake Washington Blvd, NE 38th Pl and Northup Way improved with wide sidewalks, street trees, pedestrian lighting, benches, and street furniture distinctive to the District.***
- ***Orient buildings to sidewalks or other pedestrian routes.***
- ***Moderate the scale of large buildings through vertical and horizontal modulation. Incorporate upper story step backs along all street frontages and perimeter of district.***
- ***Incorporate gateway features at locations shown in Figure L-7 incorporating signs, sculpture, lighting, and landscaping.***



This illustration by Makers Architects shows conceptual redevelopment of parcels at and adjacent to the South Kirkland Park and Ride along with improved pedestrian connections and streetscape improvements to NE 38th Pl.

Policy L-16.2: Encourage buildings and public infrastructure to include high quality materials, art, and bicycle and pedestrian amenities.

Policy L-16.3: Utilize design review to administer building and site design standards in the Yarrow Bay Business District.

Site and architectural design standards should address the principles above and be used in the design review process to evaluate new public and private development. These will help create an attractive image for the Yarrow Bay Business District and create a desirable place to work and live.

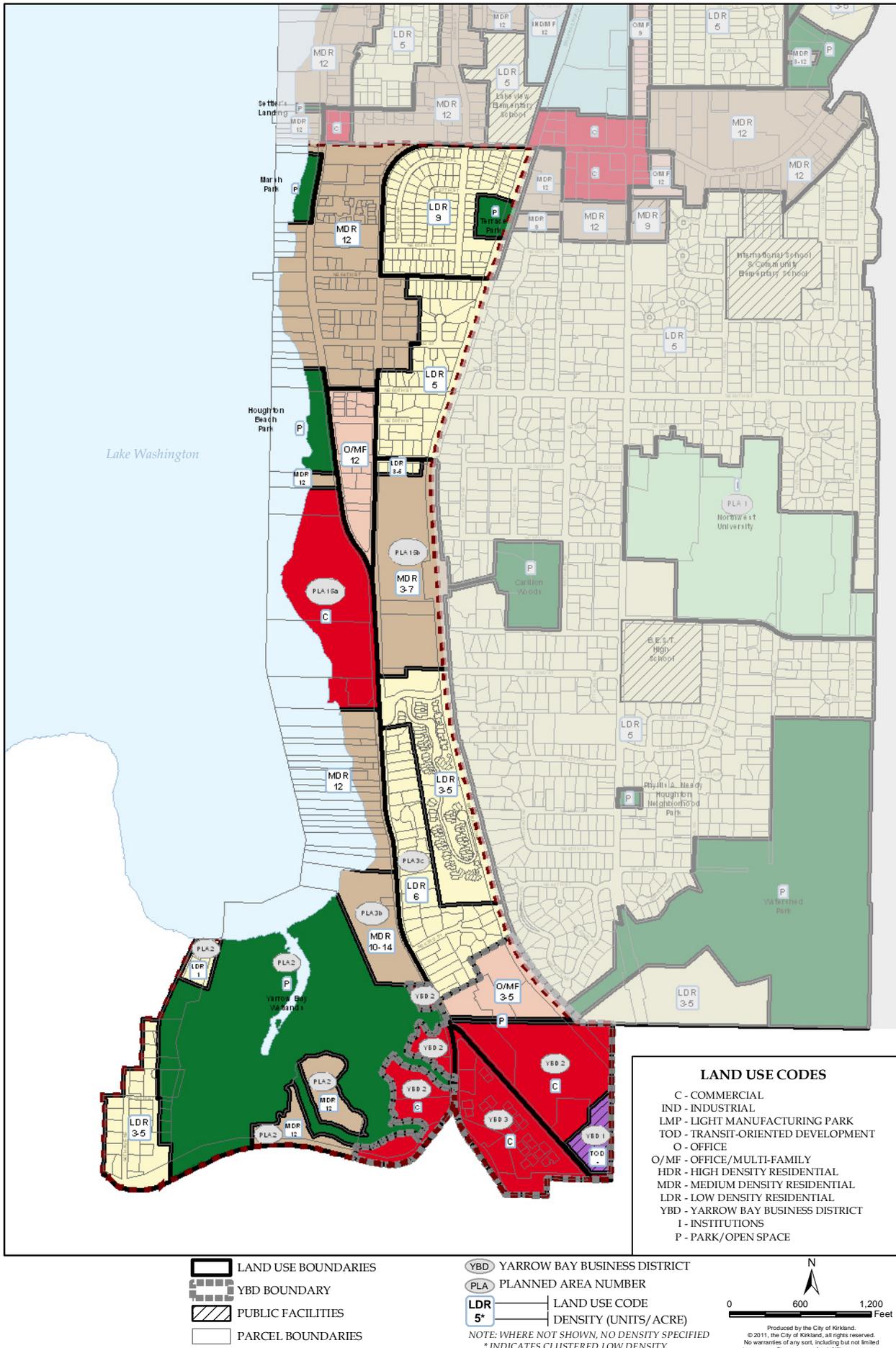
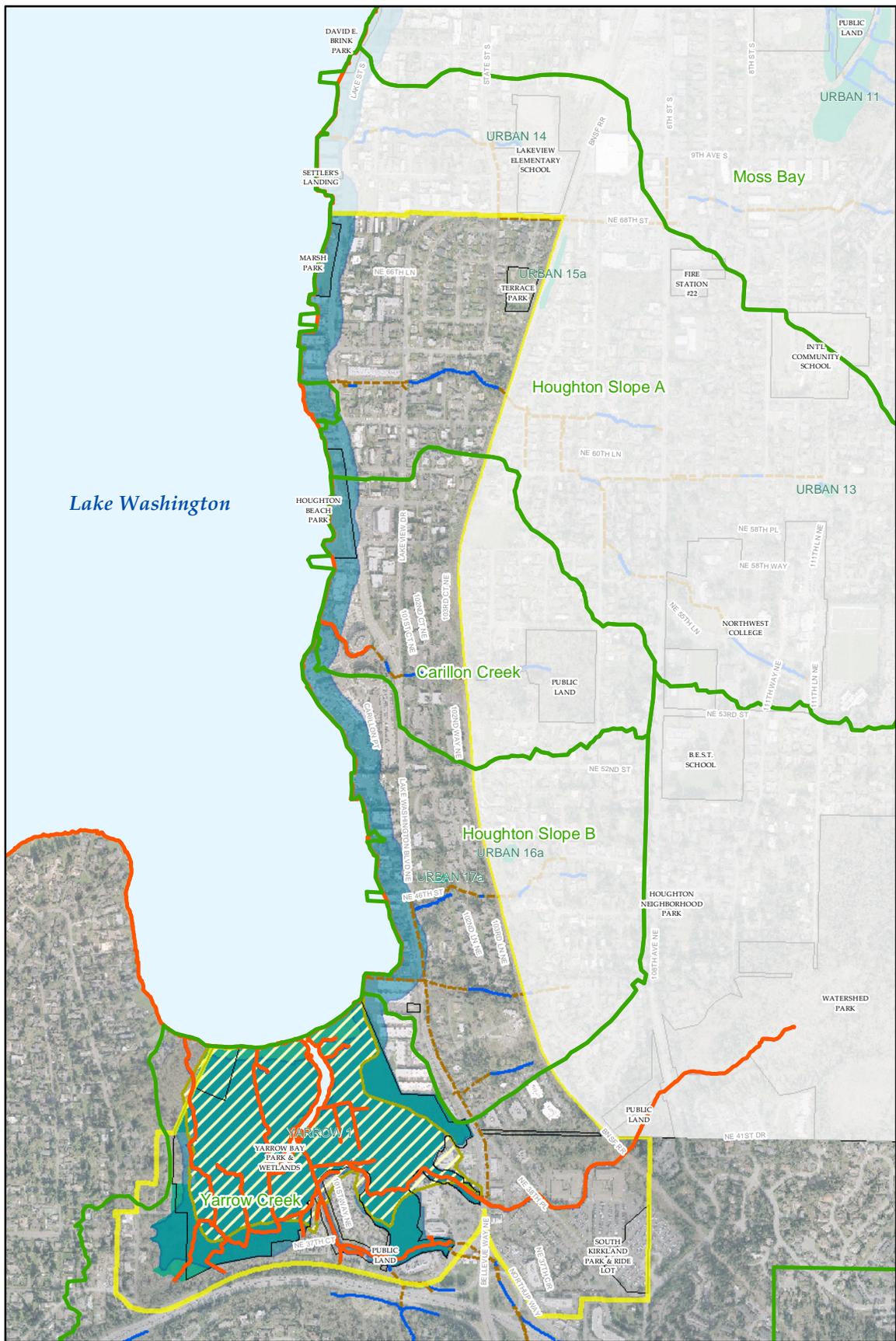


Figure L-1: Lakeview Comprehensive Plan Land Use



Legend

- Known Salmonid Locations
- Shoreline of Statewide Significance
- Streams in Pipes
- Drainage Basin Boundaries
- Open Streams
- Selected Public Properties
- 100-Year Floodplain
- Lakes
- Wetlands
- Lakeview Neighborhood Boundary

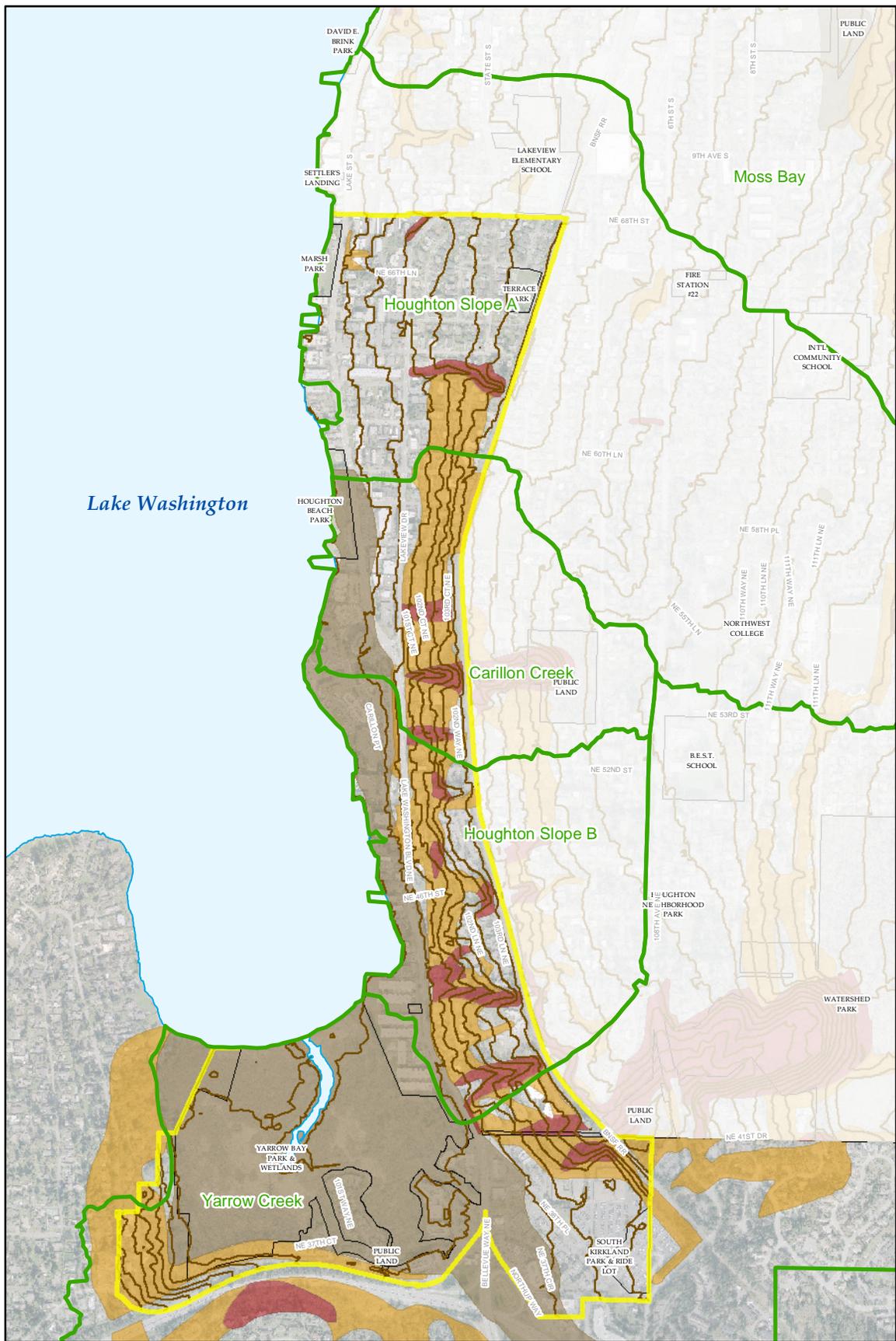
N

0 600 1,200
Feet

Scale: 1" = 1,200'

Produced by the City of Kirkland.
© 2011, the City of Kirkland, all rights reserved.
No warranties of any sort, including but not limited to accuracy, fitness or merchantability, accompany this product.

Figure L-2: Lakeview Sensitive Areas



Legend

- Seismic Hazard Area
- Drainage Basin Boundaries
- Medium Landslide Hazard
- Twenty-Foot Contours
- Selected Public Properties
- High Landslide Hazard
- Lakes

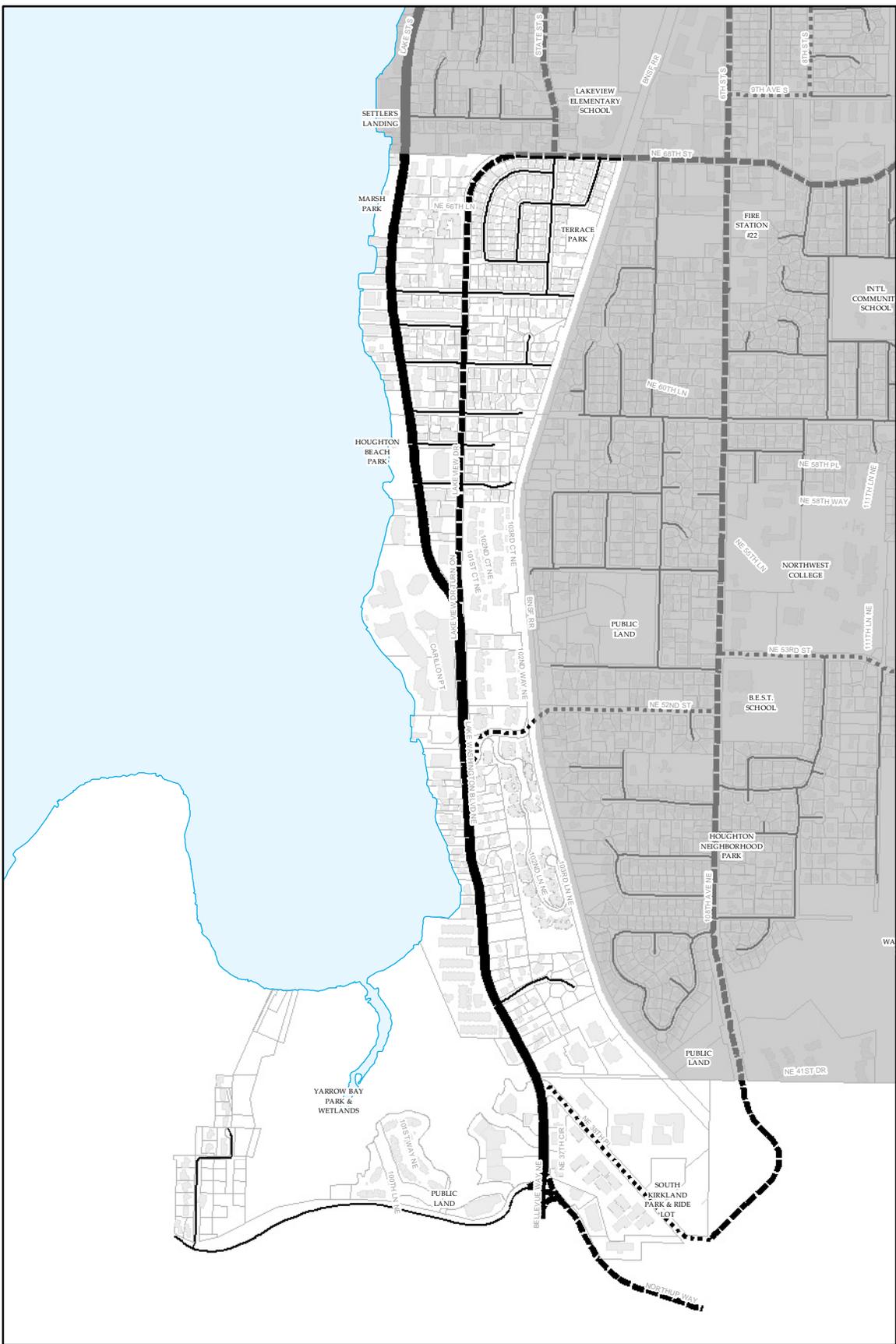
N

0 600 1,200
Feet

Scale: 1" = 1,200'

Produced by the City of Kirkland.
© 2011, the City of Kirkland, all rights reserved.
No warranties of any sort, including but not limited to accuracy, fitness or merchantability, accompany this product.

Figure L-3: Lakeview Landslide and Seismic Hazard Areas



Legend

- Neighborhood Access
- Collector
- ▬ Minor Arterial
- ▬ Principal Arterial
- ▭ Building Footprints
- ▭ Tax Parcel Boundaries

N

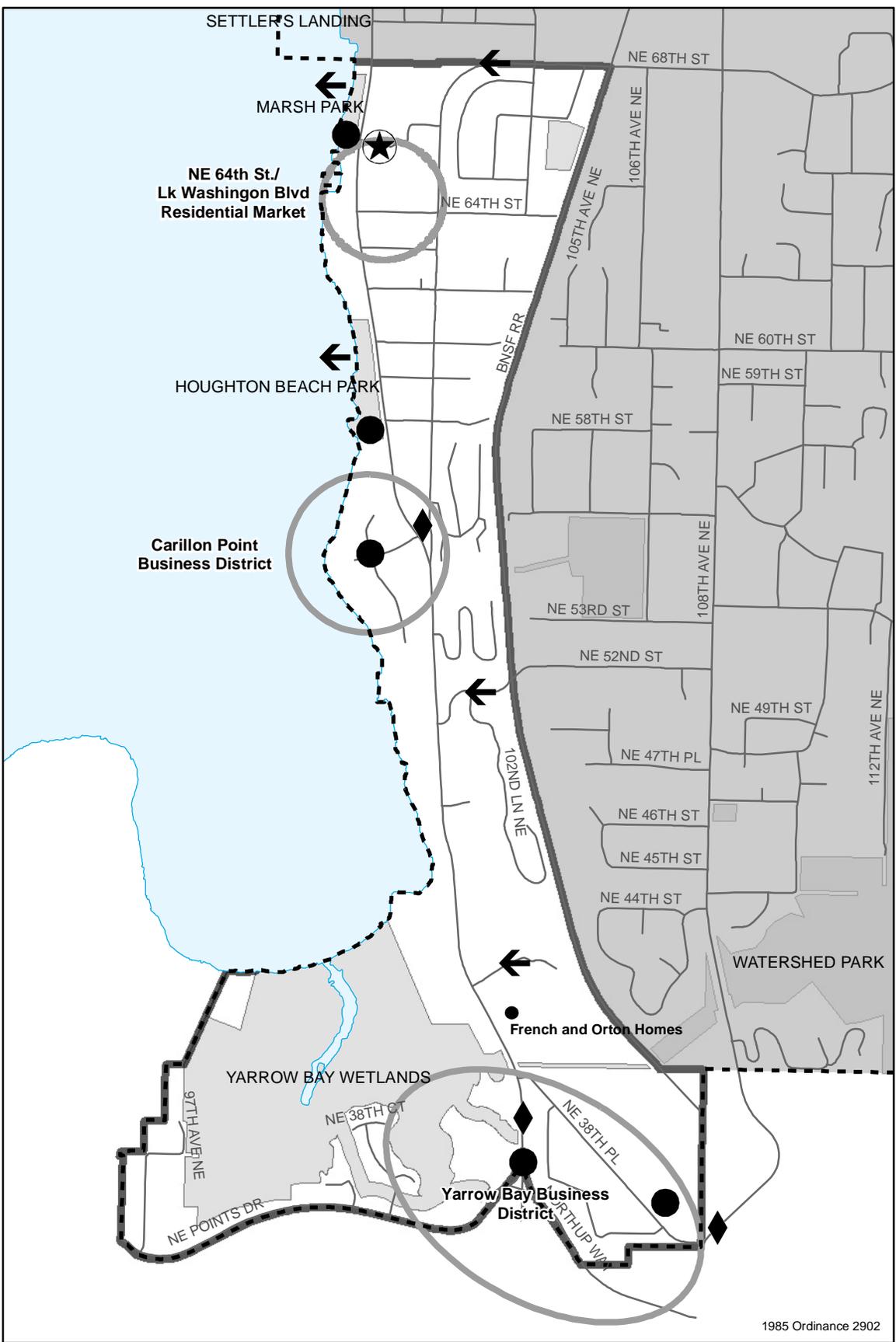
0 600 1,200

Feet

Scale: 1" = 1,200'

Produced by the City of Kirkland.
 © 2011, the City of Kirkland, all rights reserved.
 No warranties of any sort, including but not limited to accuracy, fitness or merchantability, accompany this product.

Figure L-4: Lakeview Street Classifications



1985 Ordinance 2902

- Legend**
- Activity Node
 - Historic Structures
 - ◀ Public View
 - ◆ Gateway
 - ★ Landmark
 - Business District

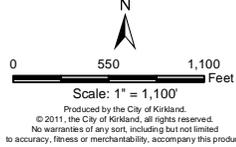


Figure L-7: Lakeview Neighborhood Character/Urban Design

PUBLICATION SUMMARY
OF ORDINANCE NO. O-4331

AN ORDINANCE OF THE CITY OF KIRKLAND RELATING TO COMPREHENSIVE PLANNING AND LAND USE AND AMENDING THE COMPREHENSIVE PLAN, ORDINANCE 3481 AS AMENDED, TO IMPLEMENT THE LAKEVIEW NEIGHBORHOOD PLAN UPDATE; AMEND THE COMPREHENSIVE LAND USE MAP AND IMPLEMENTATION STRATEGIES CHAPTER; AND APPROVE A SUMMARY FOR PUBLICATION, FILE NO ZON07-00032.

SECTION 1. Amends the following specific portions of the Kirkland Comprehensive Plan:

- A. Amends Figure LU-1 Comprehensive Land Use Map in the Land Use Element;
- B. Adds text to Table IS-1: Implementation Tasks in the Implementation Strategies Chapter
- C. Repeals existing Lakeview Neighborhood Plan and replaces it with a new Lakeview Neighborhood Plan;

SECTION 2. Provides a severability clause for the ordinance.

SECTION 3. Authorizes publication of the ordinance by summary, which summary is approved by the City Council pursuant to Kirkland Municipal Code 1.08.017 and establishes the effective date as January 1, 2012, which will be more than five days after publication of summary.

SECTION 4. Establishes certification by City Clerk and notification of King County Department of Assessments.

The full text of this Ordinance will be mailed without charge to any person upon request made to the City Clerk for the City of Kirkland. The Ordinance was passed by the Kirkland City Council at its meeting on the ____ day of _____, 2011

I certify that the foregoing is a summary of Ordinance _____ approved by the Kirkland City Council for summary publication.

City Clerk

ORDINANCE NO. O-4332

AN ORDINANCE OF THE CITY OF KIRKLAND RELATING TO PLANNING AND LAND USE AND AMENDING PORTIONS OF KIRKLAND MUNICIPAL CODE CHAPTER 22.28 RELATING TO SUBDIVISIONS AND CHAPTER 3.30 RELATING TO YARROW BAY BUSINESS DISTRICT DESIGN GUIDELINES AND APPROVING A SUMMARY FOR PUBLICATION, FILE NO. ZON07-00032.

WHEREAS, the City Council has received a recommendation from the Kirkland Planning Commission and the Houghton Community Council to amend the Municipal Code, as set forth in that certain report and recommendation of the Planning Commission and Houghton Community Council for the Lakeview Neighborhood dated September 7, 2011 and bearing Kirkland Department of Planning and Community Development File No. ZON07-00032; and

WHEREAS, prior to making said recommendation the Planning Commission and Houghton Community Council, following notice thereof as required by RCW 35A.63.070 and RCW 36.70A.035, held public hearings on June 23, 2011 and July 14, 2011 on the amendment proposals and considered the comments received at said hearings; and

WHEREAS, pursuant to the State Environmental Policy Act (SEPA), there has accompanied the legislative proposal and recommendation through the entire consideration process, a SEPA Addendum to Existing Environmental Documents issued on July 15, 2011 by the responsible official pursuant to WAC 197-11-625; and

WHEREAS, in regular public meeting the City Council considered the environmental documents received from the responsible official, together with the report and recommendation of the Planning Commission and Houghton Community Council; and

NOW, THEREFORE, the City Council of the City of Kirkland do ordain as follows:

Section 1. The following specific portions of the Municipal Code are hereby amended to read as follows:

- A. Section 3.30.040 – Design guidelines adopted by reference: Addition of “Design Guidelines for the Yarrow Bay Business District” as set forth in Exhibit A attached to this ordinance and incorporated by reference.
- B. Section 22.28.040, Lots – Lot averaging. as set forth in Exhibit B attached to this ordinance and incorporated by reference.
- C. Section 22.28.042 – Lots – Small lot single-family. as set forth in Exhibit C attached to this ordinance and incorporated by reference.

Section 2. If any section, subsection, sentence, clause, phrase, part or portion of this ordinance, including those parts adopted by reference, is for any reason held to be invalid or unconstitutional by any court of competent jurisdiction, such decision shall not affect the validity of the remaining portions of this ordinance.

Section 3. This ordinance shall be in full force and effect January 1, 2012, which will be more than five days from and after its passage by the City Council and publication pursuant to Kirkland Municipal Code 1.08.017, in the summary form attached to the original of this ordinance and by this reference approved by the City Council as required by law.

Section 4. A complete copy of this ordinance shall be certified by the City Clerk, who shall then forward the certified copy to the King County Department of Assessments.

Passed by majority vote of the Kirkland City Council in open meeting this ____ day of _____, 2011.

Signed in authentication thereof this ____ day of _____, 2011.

Mayor

Attest:

City Clerk

Approved as to Form:

City Attorney

3.30.040 Design guidelines adopted by reference.

The design review board in combination with the authority set forth in Chapter 142 of the Zoning Code shall use the following design guidelines documents to review development permits:

(1) The document entitled “Design Guidelines for Pedestrian Oriented Business Districts” bearing the signature of the mayor and the director of the department of planning and community development, dated August 3, 2004, is adopted by reference as though fully set forth herein. The city council shall consult with the planning commission prior to amending this document.

(2) The document entitled “Design Guidelines for the Rose Hill Business District” bearing the signature of the mayor and the director of the department of planning and community development, dated January 3, 2006, is adopted by reference as though fully set forth herein. The city council shall consult with the planning commission prior to amending this document.

(3) The document entitled “Design Guidelines for the Totem Lake Neighborhood” bearing the signature of the mayor and the director of the department of planning and community development, dated June 6, 2006, is adopted by reference as though fully set forth herein. The city council shall consult with the planning commission prior to amending this document.

(4) The document entitled “Kirkland Parkplace Mixed Use Development Master Plan and Design Guidelines” bearing the signature of the mayor and the director of the department of planning and community development, dated December 16, 2008, is adopted by reference as though fully set forth herein. The city council shall consult with the planning commission prior to amending this document.

(5) The document entitled “Design Guidelines for the Yarrow Bay Business District” bearing the signature of the mayor and the director of the department of planning and community development, dated ~~June 7, 2011~~ **November 15, 2011**, is adopted by reference as though fully set forth herein. The city council shall consult with the planning commission and the Houghton community council prior to amending this document.

(6) Text Amended. The following specific portions of the text of the design guidelines are amended as set forth in Attachment A attached to Ordinance 4106 and incorporated by reference. (Ord. 4308 § 1, 2011; Ord. 4172 § 1, 2008; Ord. 4106 § 1, 2007; Ord. 4052 § 1, 2006; Ord. 4038 § 1, 2006; Ord. 4031 § 1, 2006)

Design Guidelines

For Yarrow Bay Business Districts



Adopted by the City Council pursuant to
Kirkland Municipal Code Section 3.30.040.

Joan McBride, Mayor

Eric Shields, Director
Planning & Community
Development

Design Guidelines for Yarrow Bay Business District

Table of Contents

Introduction	1
Vision for Yarrow Bay Business District	2
 Design Guidelines for YBD 1	
1. High Quality Building and Design.....	2
2. Building Scale and Massing	4
3. Pedestrian Features and Amenities	5
4. Streetscape	6
5. Gateways	7
6. Parking Facilities.....	8
7. Public Amenities and Open Space.....	8
 Design Guidelines for YBD 2 and 3	
1. Building Location and Orientation.....	9
2. Parking Lots and Vehicular Circulation	10
3. Parking Structures	11
4. Architectural Scale	12
5. Human Scale.....	14
6. Pedestrian Connections	15
7. Natural Features.....	15
8. Blank Walls	16
9. Pedestrian-Friendly Building Fronts	17
10. Pedestrian Plazas.....	17
11. Residential Open Space.....	18
12. Pedestrian Amenities	19
13. Pedestrian Coverings	21
14. Building Details and Materials	22
15. Entry Gateway Features.....	23
16. Sidewalk and Pathway Widths	24
17. Street Trees	25
18. Landscaping.....	26

19. Service Areas 27
20. Lighting 28
21. Signs 30

Introduction

This document sets forth a series of Design Guidelines, adopted by Section 3.30 of the Kirkland Municipal Code that will be used by the City in the design review process for development in the Yarrow Bay Business District located in the Lakeview Neighborhood. The Yarrow Bay Business District includes the YBD 1, YBD 2, YBD 3 and PR 8.5 zones. Other documents that should be referred to during design review are the YBD (Chapter 56) and PR (Chapter 25) Use Zone Charts found in the Kirkland Zoning Code.

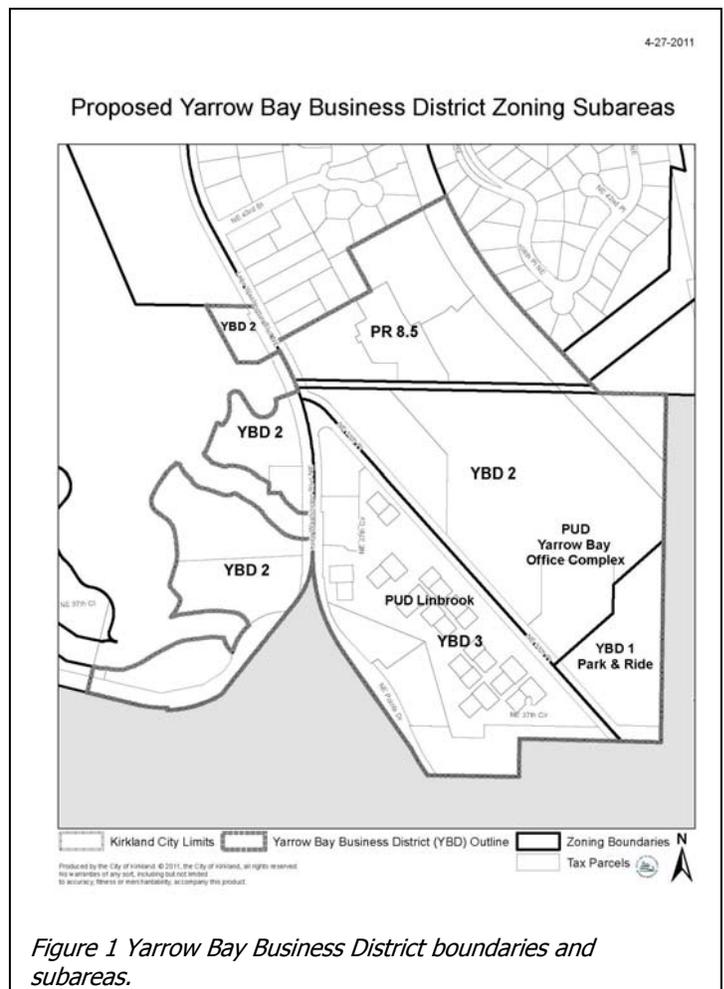
Purpose of the Design Guidelines

The Design Review Board will use these guidelines to evaluate development proposals during the design review process. The Design Guidelines are intended to establish a greater sense of quality, unity, and conformance with Kirkland's physical assets and civic identity. These guidelines are not intended to slow or restrict development, but rather to add consistency and predictability to the permit review process.

Urban Design Goals and Objectives

The key design objectives promoted in the Lakeview Neighborhood Plan for the Yarrow Bay Business District include:

- *Promote quality architectural and site design.*
- *Encourage architectural vertical and horizontal modulation along all street frontages and perimeter of district.*
- *Preserve public scenic views and natural features that contribute to Lakeview's visual identity.*
- *Provide interconnected street and pedestrian improvements throughout the district that tie the district together, contribute to a sense of identity and enhance visual quality. Included in this concept are pedestrian linkages on site, to adjacent properties, and to transit facilities. Provide directional signs that indicate path locations.*



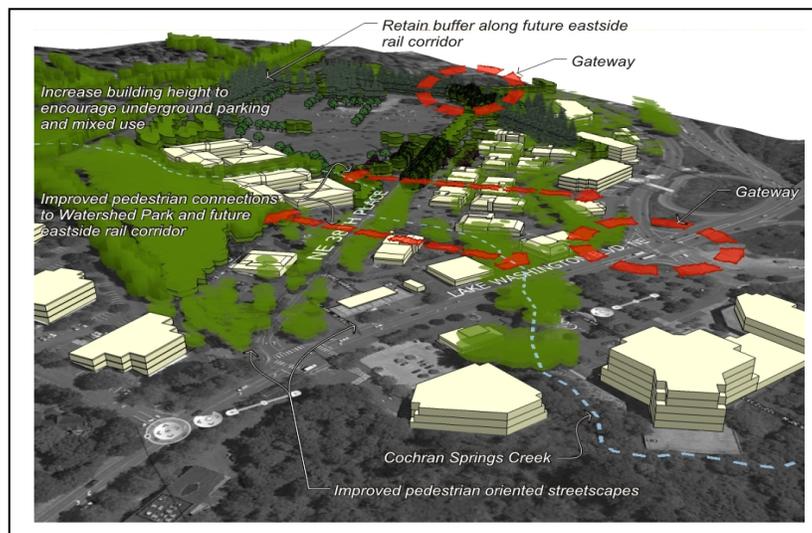
- *Enhance streetscapes distinctive to the neighborhood along Lake Washington Blvd., Northup Way and NE 38th PI with wide sidewalks, street trees, decorative pedestrian lighting, benches, or other street furniture.*
- *Incorporate gateway features to the neighborhood to strengthen neighborhood identity at the locations identified in the Lakeview Neighborhood Plan.*
- *Especially along retail uses and streets, promote using pedestrian oriented design techniques such as, generous ground floor window treatments, awnings, superior building materials, open space plazas, and pedestrian amenities.*
- *Locate development away from streams and wetlands. Enhance stream corridors for both habitat and as a natural amenity.*

Vision for the Yarrow Bay Business District

The Lakeview Neighborhood Plan vision for the Yarrow Bay Business District is to transform the large suburban style office park development into a more integrated, mixed use residential and commercial district. Several strategies will help achieve this vision such as allowing a broader range of uses, and improving pedestrian connections between properties, businesses, the Transit Oriented Development at the South Kirkland Park and Ride facility and the Eastside Rail Corridor. Incorporating public plazas, green spaces and pedestrian amenities into new development will help create an inviting environment for employees, residents and visitors. New design standards and design review for development will ensure quality architecture, site design and identity for the district.

The Lakeview Neighborhood Plan envisions improvements to NE 38th PI, Lake Washington Blvd. NE and Northup Way to upgrade the streets with wider sidewalks, street trees and decorative pedestrian lighting, directional signs, benches and varying pavement textures. On-street parking is encouraged along NE 38th PI to support pedestrian-oriented uses or retail frontage.

Figure 2 illustrates the future urban design concept for the Yarrow Bay Business District



Vision for YBD 1

The YBD 1 zone contains the South Kirkland Park and Ride property, planned for transit-oriented development (TOD). In light of the district's role in fostering a vibrant and desirable community that includes mixed use and promotes the use of transit, design guidelines are established to create a supportive built environment.

The guidelines implement the vision for the area described in the Lakeview Neighborhood Plan. They emphasize coordinated development of the site through high quality building design and pedestrian amenities and connections within and beyond the site. The importance of building orientation and connections to NE 38th Place, and the creation of a welcoming and attractive gateway at the entrance to the district at NE 38th Place and 108th Avenue NE are also highlighted in the guidelines.

Vision for YBD 2 and 3

Larger sites within the Yarrow Bay Business District provide opportunities for coordinated development. Mixed-use developments combining retail, office and residential uses with an attractive face along the major traffic corridors or provide interior vehicular and pedestrian pathways and open space as focal points for pedestrians. Focal points may include plazas surrounded by shops, offices, services or wide sidewalk areas along an interior access street.

Buildings should front along NE 38th Pl. and orient toward Lake Washington Blvd. Storefronts may be clustered around major entry points to the development to provide a welcoming entry. Use of a variety of materials and colors and modulated walls and rooflines is encouraged to reduce architectural scale.

Residential buildings should feature prominent building entries and individual balconies. Orienting residential buildings around a courtyards, plazas, or natural features also is encouraged.

The following design guidelines for the Yarrow Bay Business District (YBD) are intended to help guide the future development toward the vision described in the Lakeview Neighborhood Plan and in this document.

Design Guidelines for YBD 1

1. High Quality Building and Design

- a. Building materials should exhibit permanence.

- b. Building materials and color should be selected to integrate with each other and complement architectural design.
- c. Ornament and applied art should be integrated with the structures and the site environment and not haphazardly applied.
- d. Emphasis should be placed on highlighting building features such as doors, windows, and eaves, and on the use of materials such as wood siding and ornamental masonry. Ornament may take the form of traditional or contemporary elements
- e. Original artwork or hand-crafted details should be considered in special areas.

2. Building Scale and Massing

- a. Large window areas should be avoided. Instead smaller window units should be used to achieve human scale.
- b. Facing the street, buildings above the 2nd story should use upper story step backs to create receding building forms as building height increases to maintain human scale. A rigid stair step or “wedding cake” approach to upper story step backs is not appropriate. Prescribed upper story step backs in the gateway area at the intersection of NE 38th Place and 108th Avenue NE are appropriate to prevent the building from overpowering the gateway design.
- c. Decks and/or balconies should be designed so that they do not significantly increase the apparent mass of the building.
- d. The location of the subject property makes any new multi-story building highly visible from the surrounding streets and properties. Building design should be based on viewpoints or vantages to be identified through the Design Review process. The final arrangement of building mass should therefore address the key vantage points and respond to the context of existing and/or planned improvements, gateway features, and location of plazas and open space.
- e. All building facades should be designed carefully, i.e. there should be no “backside” of a building.
- f. Building facades should be well modulated to avoid blank walls and provide architectural interest.
- g. Landscaping should be used to provide visual interest and help soften building form at appropriate locations, including upper level terraces.

- h. To help moderate the vertical scale of buildings, buildings should incorporate design techniques which clearly define the building's top, middle, and bottom.

Examples include using a sloped roof and strong eave lines to help define the top; using windows, balconies, and material changes to define a building's middle; and pedestrian-oriented storefronts, awnings, and use of 'earth' materials such as concrete and stone to help define the building's bottom.

- i. Roof forms should be varied and attractive. Where appropriate, roof forms should also help reinforce the modulation or articulation interval of the building façade.
- j. Roof forms should be designed to screen rooftop mechanical units.
- k. A predominantly flat roof design is discouraged. For portions of the building where a flat roof design is used, architectural details such as eaves, cornices, or other articulation elements should be used to provide interest at the ground level.
- l. Vertical building modulation should be used to add variety by avoiding monotonous design. A technique that may be used is to make large buildings appear to be an aggregation of smaller buildings. Different colors and/or materials may be used to help differentiate between façade planes.
- m. Horizontal building modulation should be used to reduce the perceived mass of a building and to provide continuity at the ground level of large building complexes. Building design should incorporate strong pedestrian-oriented elements at the ground level and distinctive roof treatments. Different colors and/or materials may be used to help differentiate between façade planes.

3. Pedestrian Features and Amenities

- a. Pedestrian walkways should be placed throughout the site to allow for efficient access between the residential, commercial, transit center uses, and adjacent streets. The walkways should be situated to minimize walking distance from the public sidewalk and transit facilities to building entrances.
- b. Pedestrian and bicycle pathways and/or connections should be well-defined and safe.
- c. Pedestrian connections should be provided to adjacent properties to allow for efficient access to the transit facilities and commercial uses.
- d. Landscaping should be used to help define and provide visual interest along pedestrian walkways.

- e. Convenient and safe pedestrian areas should be designed in centralized locations to accommodate transit users.
- f. Lighting should be provided to walkways and sidewalks through building mounted light and canopy or awning mounted lights.
- g. Low level lighting in the form of bollards or similar style of lighting should be encouraged along pedestrian pathways not adjacent to buildings.
- h. Through-block pathways should be designed so that it is clear that access by the general public is allowed. The following guidelines also apply:
 - i. Because the subject property is steep along NE 38th Place, stairways may be used in the design of the through-block pathway where connecting to the street. If located along NE 38th Place, the stairway should function as a focal entry/exit point and contain design elements that make it a welcoming, safe, and attractive entry.
 - ii. If located within a parking lot, the following guidelines should be incorporated into the design of the through-block pathway:
 - a) Increased landscaped island size adjoining the pathway. This helps to narrow the driveway width where appropriate to help slow parking lot traffic.
 - b) Raised landscape beds.
 - c) Raised pathway with pavement material, texture, and color different from traffic lanes.
 - d) Selection of tree species that provide the broadest canopy possible to produce a dense landscaped environment.
 - iii. If the through-block pathway is located between buildings, appropriate plants and trees should be selected based on solar access and the location of proposed improvements.

4. Streetscape

- a. Street trees species should be selected and spaced to allow for visual continuity along NE 38th Place, buffer pedestrians from the street, and provide visibility of ground floor retail uses.
- b. Buildings should be oriented towards the street when located along NE 38th Place.

- c. Design elements such as multiple storefronts, pedestrian-oriented signs, exterior light fixtures, glazing, landscaping, and awnings should be utilized to add human scale and interest at the street level.
- d. Ground floor spaces along NE 38th Place should be transparent with windows of clear vision glass beginning no higher than 2' above grade to at least 10' above grade. Windows should extend across, at a minimum, 75% of the façade length. Continuous window walls should be avoided by providing architectural building treatments, mullions, building modulation, entry doors, and/or columns at appropriate intervals.
- e. Varied window treatments should be encouraged. Architectural detailing at window jambs, sills, and heads should be emphasized. Use of ribbon windows should be avoided.
- f. A street wall is a wall or portion of a wall of a building facing a street. Continuous street walls should incorporate vertical and horizontal modulations into the building form.
- g. Along pedestrian oriented streets, upper story building facades should be stepped back to provide enough space for decks, balconies, and other activities overlooking the street.
- h. Awnings or canopies should be required on facades adjoining sidewalks. Blank walls should be avoided near sidewalks, open spaces, and pedestrian areas.
- i. Blank walls should not be visible from the street or sidewalk. Where blank walls are unavoidable, they should be treated with landscaping, art, or other architectural treatments.

5. **Gateways**

- a. A gateway is an urban design feature that signifies a sense of place and arrival into a city or neighborhood. A gateway should be designed in the location shown in the Comprehensive Plan. The design elements of the gateway should include a combination of landscaping, architectural features, and artwork which:
 - i. Contain a highly visible and welcoming public space between the sidewalk and the building which is easily accessible, comfortable, safe, and includes pedestrian amenities;

- ii. Establish a landmark that reflects the TOD elements of the site;
- iii. Reinforce NE 38th Place and 108th Avenue NE as a focal point;
- iv. Transition between Kirkland and Bellevue and the Yarrow Bay Business District to the west; and
- v. Are integrated with the TOD building design

6. Parking Facilities

- a. Parking areas should not be located between NE 38th Place and buildings.
- b. Access driveways to parking areas should be minimized.
- c. Parking lots should be designed to provide for clear vehicular and pedestrian circulation and be well organized.
- d. Screening and landscaping should be used to reduce the visual impact of parking lots and/or parking structures to the surrounding neighborhood.
- e. Parking structures shall be designed and located to obscure the view of parked cars from adjacent properties. Parking structures should be located to the back of buildings or underground with intervening uses.
- f. Portions of parking structures visible from the street that cannot be placed behind an intervening use due to site topography, should be constructed with high quality materials and be architecturally compatible with the character of surrounding buildings.
- g. Architectural treatment, artwork, building setbacks, and/or dense landscaping should be used to further reduce the visual impact of parking structures along the street.
- h. If adjacent to the required gateway, the exterior of a parking structure should reflect the design elements of the gateway. Design should avoid the appearance of a parking structure.

7. Public Amenities and Open Space

- a. Public open space should be provided on the subject property which can be used by the general public, residents, and transit users.

- b. Public open space should be open to the sky except where overhead weather protection is provided (e.g. canopies and awnings). The space should appear and function as public space rather than private space.
- c. Public open space should be located in close proximity to commercial and retail uses that are required along NE 38th Place. The public open space should be well defined and contain amenities such as outdoor dining, seating areas, art, water features, and/or landscaping. Adequate room for pedestrian movement through the space should be maintained. Additional public open space in a location convenient to the site's transit users may also be appropriate.
- d. Careful attention should be paid to the transition between transit operations and the building to create a well defined pedestrian space such as a small plaza with landscaping features.
- e. A combination of lighting, access to sunlight, paving, landscaping, and seating should be used to enhance the pedestrian experience with the public open space.

Design Guidelines for YBD 2 and 3

1. Building Location and Orientation

Objectives

- To enhance the character and identity of the Yarrow Bay Business District.
- To upgrade the appearance of streets in the Yarrow Bay Business District.
- To enhance pedestrian circulation.
- To create focal points, particularly on large sites.

Guidelines

- a. Locate and orient buildings toward sidewalks along streets.
- b. Within interior portions of sites orient buildings to plazas, common open spaces or major internal pedestrian pathways.
- c. Where buildings are located at the sidewalk with direct pedestrian access, provide pedestrian oriented building façade treatments described in the Pedestrian Friendly Facades Section 9.



- d. Provide landscaping, plazas or building façade treatments to enhance the pedestrian experience. In general, buildings that have less pedestrian orientation will merit more landscaping and façade treatments to prevent blank walls.
- e. Locating parking to the side and/or rear of buildings is preferred.
- f. Configure development to provide focal points and opportunities for coordinated pedestrian and vehicular access. Where there are no current opportunities for coordinated access provide the opportunity for future coordination should adjacent site redevelop in the future.

2. Parking Lots and Vehicular Circulation

Objectives

- To minimize the impact of parking facilities on the fronting street, pedestrian environment, and neighboring properties.
- To enhance pedestrian and vehicular safety.
- To maintain traffic flow on streets.
- To promote shared parking.
- To provide attractive and connected vehicular circulation routes.

Discussion

Parking lots can detract from the pedestrian and visual character of a commercial area. The adverse impacts of parking lots can be mitigated through sensitive design, location, and configuration. Large parking lots can be confusing unless vehicle and pedestrian circulation patterns are well organized and marked.

Guidelines

Driveways

- a. Minimize the number of driveways into a development along Lake Washington Blvd, NE 38th Pl. and Northup Way. To the extent possible, adjacent developments should share driveways.

Parking Lot Location and Design



Figure 4 A good example of incorporating trees between parking lot and along pedestrian paths in front of uses.



Figure 5 An example of interior parking lot landscaping.

- a. Surface parking lots are discouraged. Where they are provided, locate parking to the side or rear of buildings so it is not between a building and the sidewalk.
- b. Avoid parking layouts that visually dominate a development. Design parking lots to be attractive to pedestrian's walking by and to break up large parking lots into smaller ones.
- c. Provide a clear and well organized parking lot design. Space should be provided for pedestrians to walk safely in all parking lots.

Parking Lot Landscaping and Screening

- a. Integrate landscaping into parking lots to reduce their visual impact. Provide planting beds with a variety of trees, shrubs, and ground cover to provide visual relief, summer shade, and seasonal interest.
- b. Provide low level perimeter landscaping where parking is adjacent to sidewalks in order to maintain a visual screen and reduce clutter. Use screening methods that maintain visibility at eye level between the street and parking area.
- c. Provide extensive screening and landscaping between parking lots, residential uses, and open spaces. A combination of a screen wall with a landscape buffer is preferred.

3. Parking Structures

Objective

- To mitigate the visual impacts of parking structures in the urban environment.

Guidelines

- a. Structured parking garages are preferred over surface parking lots provided they can be designed to mitigate the intrusive qualities of parking garages along streets, pedestrian pathways and in pedestrian areas using the following design techniques:
 - Locate parking structures, service areas, and storage away from the street edge and so they are not visible from the street or sidewalks.
 - Incorporate ground-level commercial space, oriented to the adjacent street, into parking structures.
 - Use landscaping to screen the parking garage façade.
 - Design and locate parking garage entries to complement, not subordinate the pedestrian entry. Where possible, locate the parking entry away from the primary street, to either the side or rear of the building.



Figure 6. This parking garage includes street front retail space and landscaped trellises to mitigate visual impacts

- Use architectural forms, materials, and/or details to integrate parking structure with the design of other buildings on the property.
- Locate and design parking structures to obscure the view of parked cars from adjacent properties.

4. Architectural Scale

Objectives

- To encourage an architectural scale of development desired for each zone within the Yarrow Bay Business District.
- To add visual interest to buildings.

Discussion

“Architectural scale” means the size of a building relative to the buildings or elements around it. When the buildings in a neighborhood are about the same size and proportion, we say they are “in scale.” The vision and development regulations for the Yarrow Bay Business District provide for larger buildings than currently exist. Care must be taken to design buildings so they appropriately respond to the evolution of the District from the current low rise condition to its vibrant mixed use future. For example, a new project need not step down to a one story edge condition to acknowledge an existing one story building on an adjoining site, but it can incorporate horizontal and vertical modulation that allow it to “fit” with the existing context and provide cues for future development of the adjoining site.

Guidelines

A combination of techniques to reduce the architectural scale of buildings is important. In general the following techniques should be included at intervals of 70 feet for office uses and 30 feet for residential uses. Alternatives will be considered if they meet the objectives.

- Incorporate fenestration techniques proportionate in size and pattern for the scale of the building. This is particularly important on upper floors, where windows should be divided into individual units with each window unit separated by a visible mullion or other element. “Ribbon windows” (continuous horizontal bands of glass) or “window walls” (glass over the entire surface) do little to indicate the scale of the building and are thus discouraged, except in special circumstances where they serve as an accent element.
- Encourage vertical modulation on multi-story buildings to add variety. Vertical modulation may be particularly effective for tall buildings adjacent to a street, plaza, or



Figure 7. Fenestration and vertical modulation techniques help to reduce the architectural scale of this office building

residential area to provide compatible architectural scale and to minimize shade and shadow impacts.

- c. Incorporate horizontal building modulation techniques to reduce the architectural scale of the building and add visual interest. Horizontal building modulation is the horizontal articulation or division of an imposing building façade through upper story setbacks, awnings, balconies, roof decks, eaves, and banding of contrasting materials. Elevations that are modulated with horizontal elements appear less massive than those with sheer, flat surfaces.

Recommended horizontal building modulation techniques include:

- Roofline modulation and a change in building materials.
 - Step back building facades, generally above the second floor.
 - For residential uses, provide horizontal building modulation based on individual unit size, use roofline modulation, and changes in color and/or building materials. The depth and width of the modulation should be sufficient to meet the objectives of the guidelines. Avoid repetitive modulation techniques, since they may not be effective when viewed from a distance. Larger residential buildings will require greater horizontal modulation techniques to provide appropriate architectural scale.
- d. Break up long continuous walls with a combination of horizontal building modulation, change in fenestration, and/or change in building materials. This is especially important for office buildings.
- e. Encourage a variety of roofline modulation techniques such as hipped or gabled rooflines and modulated flat rooflines. As a general rule, the larger the building or unbroken roofline, the bigger the modulation should be. In determining the appropriate roof type and amount of modulation, consider the distance from which the building can be viewed. For example, a large commercial building adjacent to a parking lot is capable of being viewed from a relatively large distance and will consequently necessitate greater roofline modulation.

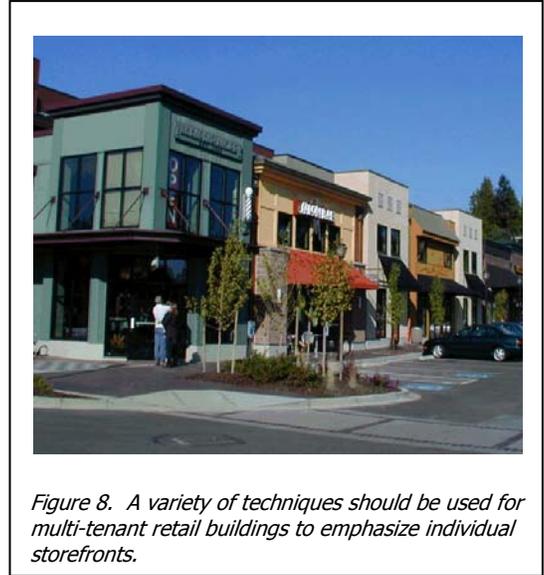


Figure 8. A variety of techniques should be used for multi-tenant retail buildings to emphasize individual storefronts.

5. Human Scale

Objectives

- To encourage the use of building components that relate to pedestrian activity.
- To add visual interest to buildings.

Discussion

The term “human scale” is generally used to indicate a building’s size in proportion to pedestrians. The actual size of a building is often not as important as its perceived size. A variety of design techniques may be used to make a building less imposing and to make people feel comfortable using and approaching it.

How the pedestrian interacts with the building at street level, along store fronts and portions of the building that are within view and reach of the pedestrian are most important factors. Upper story setbacks can also prevent taller structures from overwhelming the pedestrian scale at the street level. The use of materials, detailing, and transparency of windows along a building façade are important techniques. A bay window suggests housing, while an arcade suggests a public walkway with retail frontage. Each element must be designed for an appropriate urban setting and for public or private use. A building should incorporate special features that enhance its character and surroundings. Such features give a building a better defined “human scale.”

Guidelines

- a. Encourage a combination of architectural elements that give buildings a human scale. Examples include arcades, balconies, bay windows, roof decks, trellises, landscaping, awnings, cornices, friezes, art concepts, street front courtyards and plazas outside of retail spaces. Window fenestration techniques described in Section 4 can also be effective. Consider the distances from which buildings can be viewed (from the sidewalk, street, parking lot, open space, etc.).

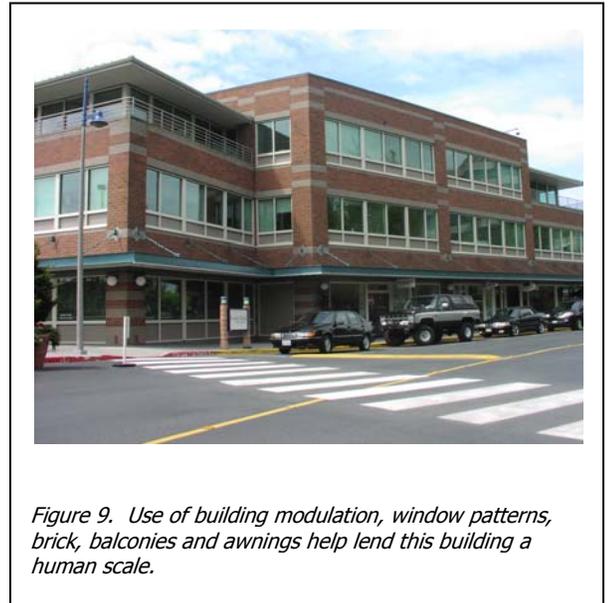


Figure 9. Use of building modulation, window patterns, brick, balconies and awnings help lend this building a human scale.

6. Pedestrian Connections

Objectives

- To provide convenient pedestrian access.
- To reduce vehicle trips.
- To encourage pedestrian activity.

Guidelines

- a. Provide convenient pedestrian access between the street, bus stops, buildings, parking areas, and open spaces.
- b. Provide direct pedestrian access from buildings to abutting public sidewalks and major internal pathways.
- c. Provide paved walkways through large parking lots. Separate walkways from vehicular parking and travel lanes by use of contrasting paving material which may be raised above the vehicular pavement and by landscaping.
- d. Provide safe and convenient pedestrian connections east to west through the business district consistent with Plate 34 of the Zoning Code.
- e. Consider installing a public trail along the stream corridor as a pedestrian connection and natural amenity.

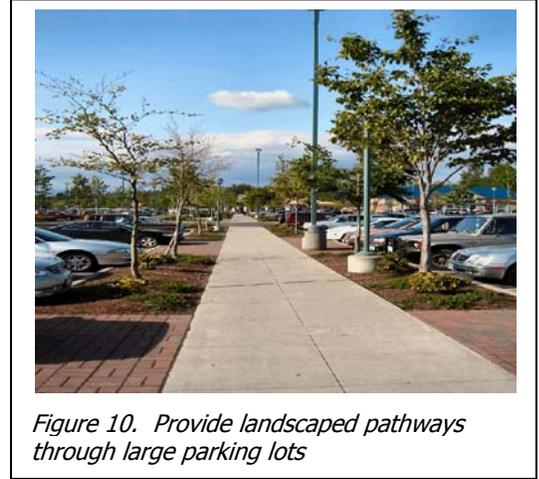


Figure 10. Provide landscaped pathways through large parking lots

7. Natural Features

Objectives

- Establish a “greenway” corridor extending in an east/west direction across the business district from the Yarrow Bay wetlands and along stream corridors to the Houghton slope.

Guidelines

- a. Configure buildings and site features to preserve and enhance stream corridors. Consider these natural features as open space amenities.
- b. Use wooded slopes as a natural site amenity and buffer by using and retaining native vegetation.
- c. Encourage buildings and rooflines to step down or be tucked against hillsides to roughly follow the slope of the existing terrain.



Figure 11. Seek opportunities to expand the existing public pedestrian pathway.

8. Blank Walls

Objectives

- To minimize visible blank walls.
- To enhance public safety along sidewalks and pathways.
- To encourage design elements that enhance the character of buildings at all perceived distances.

Discussion

Blank walls deaden the pedestrian environment and break the continuity of ground floor activity along a street or pathway. Blank walls can also create a safety problem, particularly where adjacent to pedestrian areas, as they don't allow for natural surveillance of those areas.

Guidelines

- a. Avoid blank walls near sidewalks, major internal walkways, parks, and pedestrian areas. Use the following treatments to mitigate the negative effects of blank walls (in order of preference):
 - Configure buildings and uses to avoid blank walls exposed to public view.
 - Provide a planting bed with plant material to screen most of the wall.
 - Install trellises with climbing vines or plant materials to cover the surface of the wall. For long walls, use trellises to avoid monotony.
 - Provide artwork on the wall surface.
 - Provide architectural techniques that add visual interest at a pedestrian scale, such as a combination of horizontal building modulation, change in building materials and/or color, and use of decorative building materials.
 - Provide decorative lighting fixtures.



Figure 12. An example of treatment for blank wall or parking structure.



Figure 13. This building was a combination of alternating building materials, details, and landscaping elements to add visual interest at a close range.

9. Pedestrian-Friendly Building Fronts

Objectives

- To enhance the pedestrian environment.
- To create safe and active sidewalks and pathways.

Guidelines

- a. Incorporate transparent windows, pedestrian entrances, and weather protection along facades adjacent to a sidewalk or internal pathway. Weather protection features could include awnings, canopies, marquees, or other similar treatments.
- b. Where buildings are not located at the sidewalk, incorporate landscaping, a pedestrian plaza or open space between the building and the sidewalk or provide building façade treatment.



Figure 14. An example of pedestrian friendly building façade.

10. Pedestrian Plazas

Objectives

- To provide a variety of pedestrian-oriented areas to attract shoppers and employees to commercial areas and enrich the pedestrian environment.
- To create gathering spaces for the community.
- To configure buildings to encourage pedestrian activity and pedestrian focal points.

Discussion

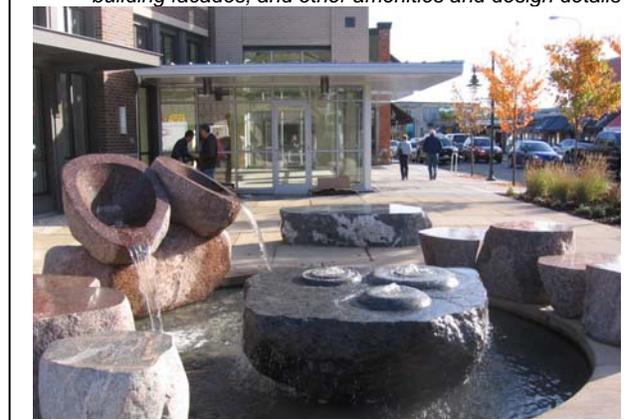
Pedestrian plazas serve as open space and places for people to gather.

Guidelines

- a. Provide pedestrian plazas in conjunction with building and site spaces that are accessible to the general public, residents and transit users.
- b. Position plazas in locations adjacent to and visible from major streets, such as along NE



Figure 15. Good examples of pedestrian plazas. Notice the decorative pavements, landscaping components, adjacent building facades, and other amenities and design details



38th Pl, major internal circulation routes, or where there are strong pedestrian flows on neighboring sidewalks. For large sites, development should be configured to create one or more focal plazas. To enhance visibility and accessibility, plazas usually should be no more than 3' above or below the adjacent sidewalk or internal pathway.

- c. Locate building entrances that open on to plazas.
- d. Provide landscaping elements that add color and seasonal interest. This can include trees, planting beds, potted plants, trellises, and hanging plants.
- e. Incorporate pedestrian amenities, as described in Section 12.
- f. Locate plazas in sunny locations.
- g. Provide transitional zones along building edges to allow for outdoor seating areas and a planted buffer.

11. Residential Open Space

Objectives

- To create useable space that is suitable for leisure activities for residents.
- To create open space that contributes to the residential setting.

Guidelines

- a. Incorporate common open space for use by residents. Guidelines for common open space include:
 - Design space as a focal point of the development.
 - Space may be provided in one large area or in multiple smaller spaces, provided that each space is large enough to provide functional leisure activity. For example, long narrow spaces rarely function as usable common space.
 - Provide space for a range of activities and age groups. Children's play areas should be visible from dwelling units and positioned near pedestrian activity.
 - Separate common space from ground floor windows, streets, service areas, and parking lots with landscaping and/or low-level fencing. However, care should be used to maintain visibility from dwelling units towards open space for safety.
- c. Provide private open space for individual residential units. For townhouses and other ground-based

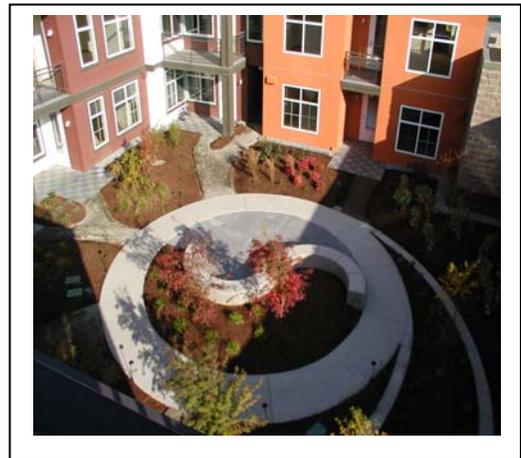


Figure 16. Good examples of common open space, including internal courtyard (above), a children's play area (below).

housing units, provide patios, decks, and/or landscaped front or rear yards adjacent to the units. For all other units, provide balconies large enough for usable space for sitting, outdoor cooking and eating etc.

12. Pedestrian Amenities

Objectives

- To provide amenities that enrich the pedestrian environment.
- To increase pedestrian activity.

Discussion

Site features and pedestrian amenities, such as lighting, benches, paving, waste receptacles, and other site elements, are an important aspect of a business district's character. These elements reduce apparent walking lengths and unify the district's visual character.

Guidelines

Provide pedestrian amenities along all sidewalks, interior pathways and within plazas and other open spaces. Examples include:

- Pedestrian-scaled lighting less than 15' above the ground.
- Seating space such as benches, steps, railings and planting ledges. Ideal heights are between 12" to 18". An appropriate seat depth ranges from 6" to 24".
- Pedestrian furniture such as trash receptacles, consolidated newspaper racks, and drinking fountains.
- Planting beds and/or potted plants.
- Unit paving such as stones, bricks, or tiles.
- Decorative pavement patterns and tree grates.
- Water features.
- Informational kiosks.
- Transit shelters.
- Decorative clocks.
- Artwork.



Figure 17. Consolidated newspaper racks



Figure 18. Bicycle racks



Figure 19. Potted plants

- Bicycle racks.



Figure 21. Decorative pavement patterns (top), benches and pedestrian-scale lighting (middle), and informational kiosk



Figure 20. This example combines a sculptural water feature with landscaping

13. Pedestrian Coverings

Objectives

- To provide shelter for pedestrians.
- To provide spatial enclosure and add design interest to a retail or office streetscapes.

Discussion

The design and width of pedestrian coverings should be determined by their function, the building's use and the type of street.

As a general rule, the more traffic an entry is expected to accommodate, the larger the covered area at the entry should be.

The width of the sidewalk should also be considered when sizing the pedestrian covering (wider sidewalks can accommodate wider pedestrian coverings). Canopies and awnings should be appropriately dimensioned to allow for tree growth, where applicable. The architecture of the building and the spacing of individual storefronts should help determine the appropriate placement and style of the canopy or awning. Continuous, uniform awnings or canopies, particularly for multi-tenant retail buildings, can create a monotonous visual environment and are discouraged.

Guidelines

- a. Provide weather protection along the primary exterior entrance of all businesses, residential units, and other buildings.
- b. Design weather protection features to provide adequate width and depth at building entries.
- c. Pedestrian covering treatments may include: covered porches, overhangs, awnings, canopies, marquees, recessed entries or other similar features. A variety of styles and colors should be considered and be compatible with the architectural style of the building and the ground floor use.
- d. Back lit, plastic awnings are not appropriate.



Figure 22. Wider pedestrian coverings allow for outdoor dining



Figure 23. Note how these awnings have been integrated into the building's storefront spaces

14. Building Details and Materials

Objectives

- To use building and site design details that add visual interest to buildings/sites at a pedestrian scale.
- To use a variety of quality building materials such as brick, stone, glass, timber, and metal appropriate to the Pacific Northwest climate.

Guidelines

- a. Encourage the integration of ornament and applied art with structures and site environments. For example, significant architectural features should not be hidden, nor should the urban context be overshadowed.



Figure 24. Consider changes in building materials with modulation techniques

Emphasis should be placed on highlighting building features such as doors, windows, eaves, and ornamental masonry. Ornament may take the form of traditional or contemporary elements. Original artwork or hand-crafted details should be considered in special areas. Ornament may consist of raised surfaces, painted surfaces, ornamental or textured banding, changing of materials, or lighting.

- b. Use a variety of quality building materials such as brick, stone, timber, and metal to add visual interest to the buildings and reduce their perceived scale. Use masonry or other durable materials - especially near the ground level.
- c. Avoid use of concrete block and large expansive tilt up concrete facades.



Figure 25. A combination of materials is preferred

15. Entry Gateway Features

Objectives

- To enhance the character and identity of the Lakeview Neighborhood.
- To provide a welcoming statement for visitors entering the City.

Discussion

The Lakeview Neighborhood Plan calls for gateway features at two key entry points into neighborhood and the Yarrow Bay Business District:

- Intersection of SR 520 and Lake Washington Blvd. NE
- Intersection of 108th Avenue NE and NE 38th PL at the Transit Oriented Development.

Guideline

Incorporate entry gateway features in new development in the vicinity of gateways/nodal intersections identified in the Lakeview Neighborhood Plan. Locate and provide a new design for a gateway sign on Lake Washington Blvd. Gateway features may include some or all of the following:

- a. Distinctive landscaping.
- b. Artwork (e.g. vertical sculpture incorporating historical information about the Lakeview Neighborhood).
- c. Decorative lighting elements.
- d. Distinctive architectural features that are unique to the neighborhood or provide open space.
- e. Incorporation of the Cochrane Springs Creek crossing into a gateway feature as a soft, green entrance to the City.



Figure 26 Existing gateway city entrance sign

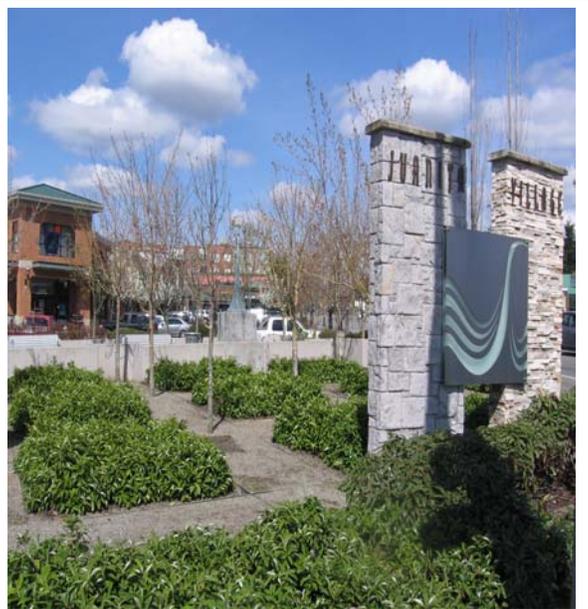


Figure 27 an example of a gateway feature in a business district.

16. Sidewalk and Pathway Widths

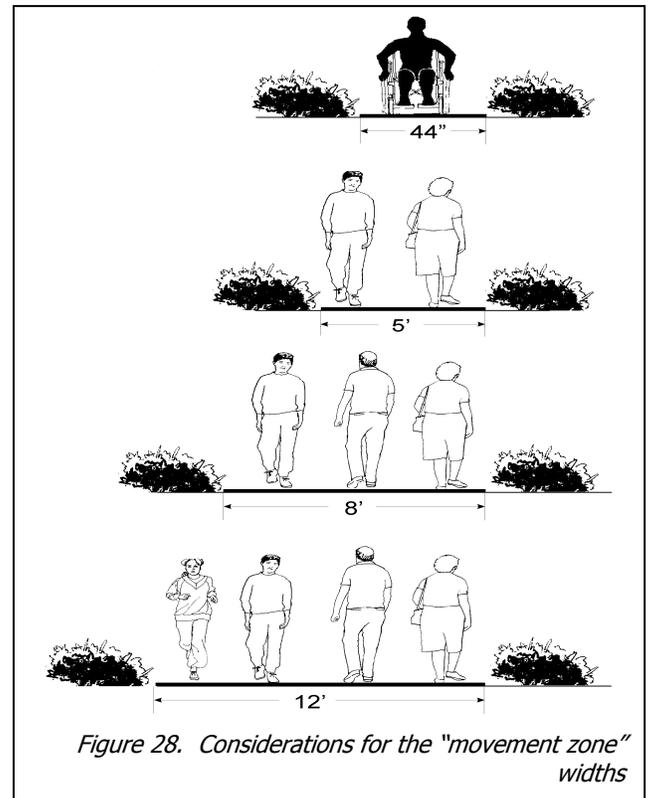
Objectives

- To provide wide sidewalks and pathways that accommodates pedestrian movement and provides a pleasing pedestrian experience.

Discussion

Sidewalks have three overlapping parts with different functions: the curb zone, the movement zone, and the storefront or activity zone. A well-sized and uncluttered movement zone allows pedestrians to move at a comfortable pace.

Sidewalks or pathways adjacent to moving vehicular traffic need generous buffers to make them safer and more inviting. Landscaping elements are particularly important physical and visual buffers between walkways and streets or other vehicle access areas. As a general rule, the higher the travel speed, the greater the buffer should be between moving cars and pedestrians.



Guidelines

- Integrate a "curb zone" into the sidewalk or pathway width to separate the pedestrian from the street. This space should include street trees in a landscape strip or tree grates. Subtle changes in paving patterns between the curb zone and the movement zone can be effective and should be considered.
- Design sidewalks and pathways to support a variety and concentration of activities and provide a separation for the pedestrian from the busy street. Provide decorative pedestrian lighting and amenities described in the pedestrian amenities section below.
- For the movement or storefront activity zone design sidewalks to be wide enough to allow for pedestrians to pass those window shopping or seated at sidewalk cafes.



17. Street Trees

Objectives

- To utilize street trees to upgrade the character and identity of the Yarrow Bay Business District.
- To enhance the pedestrian environment in the Business District.
- To use trees that provide seasonal interest.
- To use trees that will not obscure views of businesses from the street.

Discussion

The repetition of trees bordering streets, internal roadways, and pathways can unify the District. Trees can add color, texture, and form to the urban environment and provide a respite from the weather.

Guidelines

- a. Incorporate street trees along all streets, internal access roads, and pathways.
- b. Encourage street trees to be used as a unifying features for the District.
- c. Select and maintain tree species that will accommodate pedestrian and vehicular traffic, and maintain visibility into and through sites for safety purposes.



Figure 30. Provide street trees along all streets and internal access roads

18. Landscaping

Objective

- To enhance the visual quality of the urban environment.
- To incorporate greenery into the urban environment.

Discussion

Landscaping can soften the hard edges and improve the visual quality of the urban environment. Landscaping treatment in the urban environment should focus on the automobile, pedestrian, and building landscapes.



Along high speed and high volume traffic areas, raised planting strips can be used to protect pedestrians from traffic. The pedestrian landscape should offer variety at the ground level through the use of shrubs, ground cover, and trees. Pedestrian circulation, complete with entry and resting points, should be emphasized. Landscaping around buildings particularly along blank walls can reduce scale and add diversity through pattern, color, and form.

Examples of how landscaping is used to soften and enhance the visual quality of the urban environment include:

- Screening of parking lots;
- Tall cylindrical trees to mark an entry;
- Continuous street tree plantings to protect pedestrians;
- Clusters of dense trees along long building facades;
- Cluster plantings at focal points;
- Parking lots with trees and shrubs planted internally as well as on the perimeter.

Guidelines

- a. Design landscaping for the purpose and context in which it will be located. The auto oriented landscaping requires strong plantings of a structural nature to act as buffers or screens for pedestrians. The pedestrian landscape should emphasize the subtle characteristics of the plant materials. The building landscape should use landscaping that complements the building's qualities and screens service areas or blank walls while not blocking views of the business or signage.
- b. Encourage a colorful mix of drought tolerant and low maintenance trees, shrubs and perennials. Except in special circumstances, ivy should be avoided.
- c. Consider the on-site topography to hide parking and enhance views.
- d. Use wooded slopes and streams as a natural site amenity and to screen unwanted views, where applicable.

19. Service Areas

Objectives

- To mitigate adverse impacts of service areas.
- To locate and design site service and storage areas to promote ease of use, safety, and visual cohesion.

Guidelines

- a. Locate and design service and storage areas (such as refuse, recycling, loading or mechanical equipment areas) to minimize visibility from public pedestrian spaces and adjacent properties.
- b. Locate service elements where they are accessible to service vehicles and convenient for tenant use.
- c. Design service enclosures to be compatible with the design of adjacent buildings. This may be accomplished by the use of similar building materials, details, and architectural styles. Such enclosures should be made of masonry, ornamental metal, heavy wood timber, or other durable materials.
- d. Locate roof-mounted mechanical equipment so as not to be visible from the street, public open space, parking areas, or from the ground level of adjacent properties. Equipment screening should blend with the architectural character of the building.
- e. Consider the location and screening of mechanical equipment and service areas early in building and site design.

20. Lighting

Objectives

- To enhance safety by providing light levels sufficient to adequately illuminate pedestrian areas and building facades.
- To create inviting pedestrian areas using a variety of illumination techniques.
- To provide adequate lighting without creating excessive glare or light levels.

Discussion

Overpowering and uniform illumination in commercial areas creates glare and destroys the quality of night light especially adjacent to residential areas. Well placed light fixtures provide sufficient lighting levels for security and safety as well as create a positive ambience. A blend of lighting directed downward on walking surfaces and up onto trees can be used to define these spaces. Care should be taken to avoid spilling excessive glare into adjacent properties and to avoid sky directed light pollution. For this reason utilizing fixtures that shield the light source as much as possible is prudent. In some instances highlighting light fixtures by allowing the light source to be seen is appropriate; however, care should be taken to diffuse the light source to not cause excessive glare. Overall, it is desirable to have different intensities and balances of light to create well defined and comfortable outdoor places.

Guidelines

- a. Provide adequate lighting levels in all areas used by pedestrians and automobiles, including building entries, walkways, parking areas, circulation areas, and open spaces.

Recommended minimum light levels:

- Building entries: 4 foot candles. This can be a combination of up lighting and down lighting and building mounted lighting. Care should be taken to emphasize the importance of the building entrance.
- Primary pedestrian walkway: 2 foot candles lighting can be ambient light from canopies and building mounted lighting. Emphasis should take place on creating pools of lighting on the pedestrian surface. Lighting sources should not be seen except when lighting is designed as part of a theme for the overall building such as in some sort of lamp lighting.

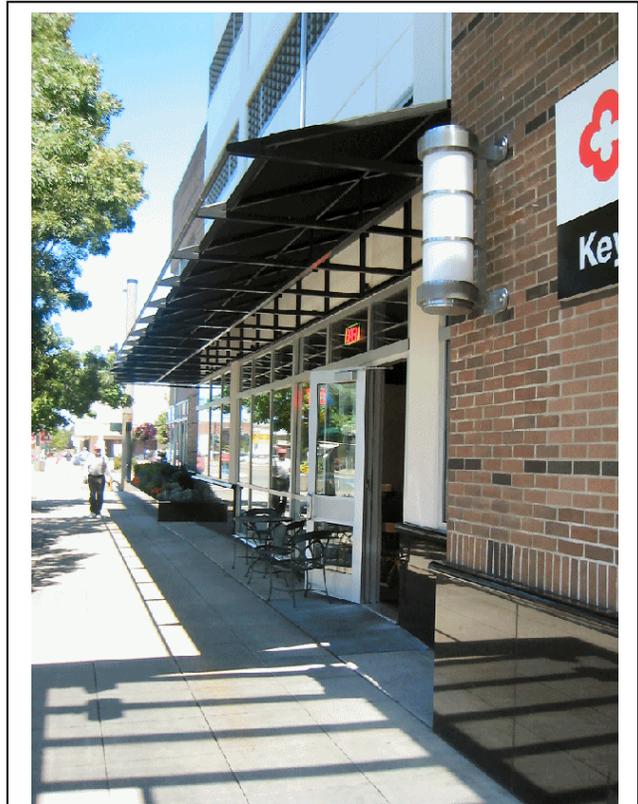


Figure 31. Building-mounted lighting is encouraged to enhance the pedestrian environment

- Secondary pedestrian walkway: 1-2 foot candles. Focus should be on pooling lighting on the walking surface and hiding the light source. Different levels of lighting should occur at focus or gathering points to provide destinations along pedestrian walkways.
 - Parking lot: .60 -1 foot candle
 - Enclosed parking garages for common use: 3 foot candles
- b. Provide lighting for walkways and sidewalks through building mounted lights, canopy or awning mounted lights, and display windows. Building-mounted light fixtures are encouraged to give visual variety and provide interest. It is acceptable to use the building mounted light as a piece of visual artwork unto itself. Care should be taken to prevent as much direct glare as possible from the light source. Canopies or awning-mounted lights are not to illuminate a glowing canopy. Canopies and awning lights should be directed onto walking surfaces, on the building façades or directed up under the canopy. Window display ambient light can spill onto the walkways and sidewalk.
- c. Provide parking lot light fixtures that are non-glare. Lower level lighting fixtures in a design that is coordinated with the architecture of the building are preferred. Lights up to 20' in height may be used for safety and security when needed. However, the light source shall not be seen beyond an approximate 20-degree angle from the light fixture itself.
- d. Prohibit flood illumination of building facades. Some directed façade lighting maybe appropriate when coordinated with the design theme of the building.

21. Signs

Objectives

- To encourage the use of creative, well-crafted signs that will contribute to the character of the district while providing adequate identification for buildings and tenants.

Discussion

Kirkland's Zoning Code regulates signs throughout the city in order to create a high-quality urban environment. The type and design of a sign will vary, depending on if it is geared toward the passing motorist, pedestrians or a commercial center. Signs should be an integral part of a building's façade or site design. The location, architectural style, and mounting of signs should conform to a building's architecture and not cover up or conflict with its prominent architectural features. A sign's design and mounting should be appropriate for the setting.

Guidelines

- Provide pedestrian oriented signs on all commercial facades where adjacent to a sidewalk or walkway. This includes signs located within 15' of the ground plane, such as "blade" signs which hang below canopies. Small signs located on canopies or awnings are also effective along building facades at the street. Sculpted signs and signs that incorporate artwork add interest.
- Prohibit internally lit cabinet signs. Neon signs are appropriate when integrated with the building's architecture.
- For ground mounted signs provide substantial sign bases in proportion to the sign face and install low level landscaping around the sign base.
- Use mounting supports for signs that reflect the materials and design character of the building or site elements or both. Too much variety, too much uniformity though unified by common design elements, signs can still express the individual character of businesses.
- Provide master sign plans for larger commercial centers to combine signage for the whole complex that describes the general location for signs, complements the architectural design of the center and signs oriented to automobile traffic.



Figure 32 good example of a sign geared to automobiles for a multi use development

22.28.040 Lots—Lot averaging.

In multiple lot subdivisions not located in an RSA **or PLA 3C** zone and not subject to Section 22.28.030, the minimum lot area shall be deemed to have been met if the average lot area is not less than the minimum lot area required of the zoning district in which the property is located as identified on the zoning map. Under this provision, either:

(a) Not more than twenty percent of the number of lots in a subdivision and one of the lots in a short plat may contain an area less than the prescribed minimum for this zoning district. In no case shall any lots be created which contain an area more than ten percent less than the prescribed minimum for this zoning district; or

(b) Up to seventy-five percent of the number of lots in a subdivision or short plat may contain an area less than the prescribed minimum for this zoning district if the lots which would be created contain an area no more than five percent less than prescribed.

These smaller lots shall be located so as to have the least impact on surrounding properties and public rights-of-way.

Using process IIA, Chapter 150 of Title 23 of this code, and the applicable sections of Chapter 22.12 or 22.20 of this title, additional lot averaging may be achieved. Through process IIA, not more than thirty percent of the number of lots in a subdivision, and two of the lots in a short plat, may contain an area less than the prescribed minimum for this zoning district as long as the average lot area is not less than the minimum lot area required for the zoning district in which the property is located as identified on the zoning map. In no case shall any lots be created through this process which contain an area more than fifteen percent less than the prescribed minimum for this zoning district. The smaller lots shall be located so as to have the least impact on surrounding properties and public rights-of-way. In addition, the plat or short plat must meet the following criteria:

(1) The averaging is necessary because of special circumstances regarding the size, shape, topography, or location of the subject property, or the location of a preexisting improvement on that subject property; and

(2) The averaging will not be materially detrimental to the property or improvements in the area of the subject property or to the city in part or as a whole; and

(3) Existing significant trees and vegetation will be preserved where feasible to buffer the adjacent properties from the smaller lots in the subject subdivision.

Additional lot averaging may only be addressed and obtained through the provisions of Chapter 125, Planned Unit Development, of Title 23 of this code and the applicable sections of Chapter 22.12 or 22.20 of this title. (Ord. 4196 § 2 (Exh. B) (part), 2010; Ord. 4011 § 2, 2005; Ord. 3705 § 2 (part), 1999)

22.28.042 Lots—Small lot single-family.

In the Market, and Norkirk and Lakeview (except for lots located in the PLA 3C zone and RS 12.5 zone) neighborhoods, as defined in the comprehensive plan, for those subdivisions not subject to the lot size flexibility provisions of Sections 22.28.030 and 22.28.040 and historic preservation provisions of Section 22.28.048, the minimum lot area shall be deemed to be met if at least one-half of the lots created contain no less than the minimum lot size required in the zoning district in which the property is located. The remaining lots may contain less than the minimum required lot size; provided, that such lots meet the following standards:

- (a) Within the RS 6.3 and RS 7.2 zones, the lots shall be at least five thousand square feet.
- (b) Within the RS 8.5 zone, the lots shall be at least six thousand square feet.
- (c) The portion of any flag lot that is less than thirty feet wide, and used for driveway access to the buildable portion of the lot may not be counted in the lot area.
- (d) The floor area ratio (FAR) shall not exceed thirty percent of lot size; provided, that FAR may be increased up to thirty-five percent of the lot size if the following criteria are met:
 - (1) The primary roof form of all structures on the site is peaked, with a minimum pitch of four feet vertical to twelve feet horizontal; and
 - (2) All structures are set back from side property lines by at least seven and one-half feet.
- (e) The FAR restriction shall be recorded on the face of the plat.
- (f) Accessory dwelling units are prohibited. This restriction shall be recorded on the face of the plat. (Ord. 4102 § 1(A), 2007)

PUBLICATION SUMMARY
OF ORDINANCE NO. O-4332

AN ORDINANCE OF THE CITY OF KIRKLAND RELATING TO PLANNING AND LAND USE AND AMENDING PORTIONS OF KIRKLAND MUNICIPAL CODE CHAPTER 22.28 RELATING TO SUBDIVISIONS AND CHAPTER 3.30 RELATING TO YARROW BAY BUSINESS DISTRICT DESIGN GUIDELINES AND APPROVING A SUMMARY FOR PUBLICATION, FILE NO. ZON07-00032.

Section 1. Amends the following specific portions of the Municipal Code:

Section 3.30.040 Design Guidelines adopted by reference.
Section 22.28.040, Lots – Lot averaging in reference to PLA 3C;
Section 22.28.042 – Lots – Small lot single-family to include the Lakeview Neighborhood except lots located in the PLA 3C and RS 12.5 zones;

Section 2. Provides a severability clause for the ordinance.

Section 3. Authorizes publication of the ordinance by summary, which summary is approved by the City Council pursuant to Kirkland Municipal Code 1.08.017 and establishes the effective date as January 1, 2012, which will be more than five days after publication of summary.

Section 4. Establishes certification by City Clerk and notification of King County Department of Assessments.

The full text of this Ordinance will be mailed without charge to any person upon request made to the City Clerk for the City of Kirkland. The Ordinance was passed by the Kirkland City Council at its meeting on the ____ day of _____, 2011.

I certify that the foregoing is a summary of Ordinance _____ approved by the Kirkland City Council for summary publication.

City Clerk

ORDINANCE NO. O-4333

AN ORDINANCE OF THE CITY OF KIRKLAND RELATING TO COMPREHENSIVE PLANNING AND LAND USE AND AMENDING THE KIRKLAND ZONING CODE (TITLE 23 OF THE KIRKLAND MUNICIPAL CODE), AND THE KIRKLAND ZONING MAP, ORDINANCE 3710 AS AMENDED TO IMPLEMENT THE LAKEVIEW NEIGHBORHOOD PLAN UPDATE AND APPROVING A SUMMARY FOR PUBLICATION, FILE NO ZON07-00032.

WHEREAS, the City Council has received a recommendation from the Kirkland Planning Commission and the Houghton Community Council to amend certain portions of the Kirkland Zoning Code (Title 23 of the Kirkland Municipal Code), all as set forth in that certain report and recommendation of the Planning Commission dated September 7, 2011 and bearing Kirkland Department of Planning and Community Development File No. ZON07-00032; and

WHEREAS, prior to making said recommendation the Planning Commission and the Houghton Community Council, following notice thereof as required by RCW 35A.63.070 and RCW 36.70A.035, held public hearings on June 23, 2011 and July 14, 2011, on the amendment proposals and considered the comments received at said hearings; and

WHEREAS, pursuant to the State Environmental Policy Act (SEPA), there has accompanied the legislative proposal and recommendation through the entire consideration process, a SEPA Addendum to Existing Environmental Documents, issued on July 15, 2011 by the responsible official pursuant to WAC 197-11-625; and

WHEREAS, in regular public meeting the City Council considered the environmental documents received from the responsible official, together with the report and recommendation of the Planning Commission and the Houghton Community Council;

NOW, THEREFORE, the City Council of the City of Kirkland do ordain as follows:

Section 1. Zoning Text amended: The following specified sections of the text of the Kirkland Zoning Code (Title 23 of the Kirkland Municipal Code) are amended as follows:

- A. Table of Contents:
Text amendments to Table of Contents as set forth in Exhibit A attached to this ordinance and incorporated by reference.
- B. Chapter 5. Definitions:
Text amendments to Sections 5.10.145, 5.10.490, 5.10.513, 5.10.595 and 5.10.785 as set forth in Exhibit B attached to this ordinance and incorporated by reference.

- C. Chapter 10. Legal Effect/Applicability:
Text amendments to Section 10.25 as set forth in Exhibit C attached to this ordinance and incorporated by reference.
- D. Chapter 25. Professional Office Residential (PR) and Professional Office Residential A (PRA) Zones:
Text amendments as set forth in Exhibit D attached to this ordinance and incorporated by reference.
- E. Chapter 30. Waterfront District (WD) Zones:
Text amendments to Sections 30.30 and 30.35.010 as set forth in Exhibit E attached to this ordinance and incorporated by reference.
- F. Repeal Chapter 35. Freeway Commercial (FC III) Zone
- G. Add to Chapter 56. Yarrow Bay Business District Subareas YBD 2 and 3 as set forth in Exhibit F attached to this ordinance and incorporated by reference.
- H. Chapter 60. Planned Areas:
Text amendments to PLA 2 Use Zone Chart as set forth in Exhibit G attached to this ordinance and incorporated by reference.

Delete PLA 3A use zone chart.

Add new Sections 60.19 – 60.22 PLA 3C Use Zone charts as set forth in Exhibit H attached to this ordinance and incorporated by reference.

Text amendments to 60.25 PLA 3B as set forth in Exhibit I attached to this ordinance and incorporated by reference.
- I. Add Yarrow Bay Business District (YBD) Design Regulations to Chapter 92 as set forth in Exhibit J attached to this ordinance and incorporated by reference.
- J. Chapter 100 Signs
Add Lakeview Drive and NE 60th Street to section 100.50 Designated Corridors and Yarrow Bay Business District to 100.52 Certain Signs Prohibited as set forth in Exhibit K attached to this ordinance and incorporated by reference.
- K. Chapter 105 Parking
Text amendment to Section 105.58.2 as set forth in Exhibit L attached to this ordinance and incorporated by reference.
- L. Chapter 115 Miscellaneous
Text amendment to Section 115.42 as set forth in Exhibit M attached to this ordinance and incorporated by reference.
- M. Chapter 142 Design Review

Text amendment to Section 142.25 and 142.37 as set forth in Exhibit N attached to this ordinance and incorporated by reference.

- N. Chapter 180 Plates
Revise Plate 34L and add Plate 34 M as set forth in Exhibit O attached to this ordinance and incorporated by reference.

Section 2. Zoning Map amended: The following specified zones of Ordinance 3710 as amended, the Kirkland Zoning Map, are amended as follows:

- A. Change zoning from RS 12.5 to PLA 3C in area set forth in Exhibit P attached to this ordinance and incorporated by reference.
- B. Change zoning from RM 3.6 to PR 3.6 and delete prefixes in areas set forth in Exhibit Q attached to this ordinance and incorporated by reference.
- C. Change zoning from PO and PLA 3A to YBD 2 and from FCIII to YBD 3 and designate Yarrow Bay Business District Boundary in areas set forth in Exhibit R attached to this ordinance and incorporated by reference.

Section 3. If any section, subsection, sentence, clause, phrase, part or portion of this ordinance, including those parts adopted by reference, is for any reason held to be invalid or unconstitutional by any court of competent jurisdiction, such decision shall not affect the validity of the remaining portions of this ordinance.

Section 4. This ordinance shall be in full force and effect January 1, 2012, which will be more than five days from and after its passage by the City Council and publication pursuant to Kirkland Municipal Code 1.08.017, in the summary form attached to the original of this ordinance and by this reference approved by the City Council as required by law.

Section 5. A complete copy of this ordinance shall be certified by the City Clerk, who shall then forward the certified copy to the King County Department of Assessments.

Passed by majority vote of the Kirkland City Council in open meeting this _____ day of _____, 2011.

Signed in authentication thereof this _____ day of _____, 2011.

MAYOR

Attest:

City Clerk

Approved as to Form:

City Attorney

Title 23 ZONING

TABLE OF CONTENTS

This code contains zoning regulations for the Finn Hill, North Juanita, and Kingsgate annexation areas as adopted by the Kirkland City Council through Ordinance 4196. The effective date of the annexation and Ordinance 4196 zoning regulations is June 1, 2011.

[Click here](#) to view adopted ordinances that have not yet been inserted into the Zoning Code as well as pending regulations under consideration.

[Zoning Code Interpretations](#)

[Chapter 1](#) – User Guide

[Chapter 5](#) – Definitions

[Chapter 10](#) – Legal Effect/Applicability

[Chapter 15](#) – Single-Family Residential (RS) Zones

[Chapter 17](#) – Single-Family Residential X (RSX) Zones

[Chapter 18](#) – Single-Family Residential A (RSA) Zones

[Chapter 20](#) – Multifamily Residential (RM and RMA) Zones

[Chapter 25](#) – Professional Office Residential (PR) and Professional Office Residential A (PRA) Zones

[Chapter 27](#) – Professional Office (PO) Zones

[Chapter 30](#) – Waterfront District (WD) Zones

[WDI Zone](#)

[WDII Zone](#)

[WDIII Zone](#)

~~[Chapter 35](#) – Freeway Commercial (FC) Zones [Delete Chapter 35 FCIII Zone](#)~~

~~[FCIII Zone](#)~~

[Chapter 40](#) – Neighborhood Business (BN) Zones and Neighborhood Business A (BNA) Zones

[Chapter 45](#) – Community Business (BC, BC 1 and BC 2) Zones

[Chapter 47](#) – Community Business X (BCX) Zones

[Chapter 48](#) – Light Industrial Technology (LIT) Zones

[Chapter 49](#) – Park/Public Use (P) Zones

[Chapter 50](#) – Central Business District (CBD) Zones

[CBD-1A & 1B](#)

[CBD-2](#)

[CBD-3](#)

[CBD-4](#)

[CBD-5](#)

[CBD-5A](#)

[CBD-6](#)

[CBD-7](#)

[CBD-8](#)

[50.60](#) Special Parking Provisions in the CBD 1, 2, and 8 Zones

[50.62](#) Building Height Provisions in the CBD

[Chapter 51](#) – Market Street Corridor (MSC) Zones

[MSC 1, 4](#)

[MSC 2](#)

[MSC 3](#)

[Chapter 52](#) – Juanita Business District (JBD) Zones

[JBD-1](#)

[JBD-2](#)

[JBD-3](#)

[JBD-4](#)

[JBD-5](#)

[JBD-6](#)

[Chapter 53 – Rose Hill Business District \(RHBD\) Zones](#)

[RH 1A](#)

[RH 1B](#)

[RH 2A, 2B, 2C](#)

[RH 3](#)

[RH 4](#)

[RH 5A, 5B](#)

[RH 5C](#)

[RH 7](#)

[RH 8](#)

[Chapter 54 – North Rose Hill Business District \(NRHBD\) Zones](#)

[NRH-1A](#)

[NRH-1B](#)

[NRH-2](#)

[NRH-3](#)

[NRH-4](#)

[NRH-5](#)

[NRH-6](#)

[Chapter 55 – Totem Lake \(TL\) Zones](#)

[TL 1A](#)

[TL 1B](#)

[TL 2](#)

[TL 3A, 3B, 3C, 3D](#)

[TL 4A, 4B, 4C](#)

[TL 5](#)

[TL 6A, 6B](#)

[TL 7](#)

[TL 8](#)

[TL 9A](#)

[TL 9B](#)

[TL 10A](#)

[TL 10B](#)

[TL 10C](#)

[TL 10D](#)

[TL 10E](#)

[TL 11](#)

[Insert Chapter 56-Yarrow Bay Business District YBD 2, YBD 3 Section 56.18](#)

[Chapter 60 – Planned Areas \(PLA\)](#)

[PLA1](#)

[PLA2](#)

[PLA3 –Delete PLA 3A Section 60.19 Use Zone](#)

[Insert new PLA 3C Use Zone Chart Section 60.19](#)

[PLA5](#)

[PLA6](#)

[PLA7](#)

[PLA9](#)

[PLA14](#)

[PLA15](#)

[PLA16](#)

[PLA17](#)

[Chapter 70 – Holmes Point Overlay Zone](#)

[Chapter 72 – Adult Activities Overlay Zone](#)

[Chapter 75 – Historic Landmark Overlay Zone](#)

- [Chapter 78](#) – Secure Community Transition Facility Overlay Zone
- [Chapter 80](#) – Equestrian Overlay Zone
- [Chapter 83](#) – Shoreline Management
- [Chapter 85](#) – Geologically Hazardous Areas
- [Chapter 90](#) – Drainage Basins
- [Chapter 92](#) – Design Regulations
- [Chapter 95](#) – Tree Management and Required Landscaping
- [Chapter 100](#) – Signs
- [Chapter 105](#) – Parking Areas, Vehicle and Pedestrian Access, and Related Improvements
- [Chapter 110](#) – Required Public Improvements
- [Chapter 112](#) – Affordable Housing Incentives – Multifamily
- [Chapter 113](#) – Cottage, Carriage and Two/Three-Unit Homes
- [Chapter 115](#) – Miscellaneous Use Development and Performance Standards
- [Chapter 117](#) – Personal Wireless Service Facilities
- [Chapter 120](#) – Variances
- [Chapter 125](#) – Planned Unit Development
- [Chapter 127](#) – Temporary Use
- [Chapter 130](#) – Rezone
- [Chapter 135](#) – Amendments to the Text of the Zoning Code
- [Chapter 140](#) – Amendments to the Comprehensive Plan
- [Chapter 141](#) – Shoreline Administration
- [Chapter 142](#) – Design Review
- [Chapter 145](#) – Process I
- [Chapter 150](#) – Process IIA
- [Chapter 152](#) – Process IIB
- [Chapter 160](#) – Process IV
- [Chapter 161](#) – Process IVA
- [Chapter 162](#) – Nonconformance
- [Chapter 165](#) – Authority
- [Chapter 170](#) – Code Administration
- [Chapter 175](#) – Bonds
- [Chapter 180](#) – Plates
- [Revise Plate 34 L Pedestrian Pathways in YBD](#)
- [Add new Plate 34 M Street Improvements for YBD](#)

[Table of Revised Pages](#)
[Ordinance History Table](#)
[Ordinance Table](#)

Chapter 5 – DEFINITIONS

5.10 Definitions

.145 Commercial Zones

– The following zones: BN; BNA; BC; BC 1; BC 2; BCX; CBD; JBD 1; JBD 2; JBD 4; JBD 5; JBD 6; MSC 2; MSC 3; NRH 1A; NRH 1B; NRH 4; RH 1A; RH 1B; RH 2A; RH 2B; RH 2C; RH 3; RH 5A; RH 5B; RH 5C; RH 7; TL 2; TL 4A; TL 4B; TL 4C; TL 5; TL 6A; TL 6B; and TL 8, ~~YBD 2, YBD 3.~~

.490 Low Density Zones

– The following zones: RS 35; RSX 35; RS 12.5; RSX 12.5; RS 8.5; RSX 8.5; RSA 8; RS 7.2; RSX 7.2; RS 6.3; RSA 6; RS 5.0; RSX 5.0; RSA 4; RSA 1; ~~PLA 3C~~, PLA 6C, 6E; PLA 16; WD II; and comparable zones in other adjoining jurisdictions, except properties with approved intent to rezone to zoning designations other than low density.

.513 Maximum Units per Acre

– Within RSA ~~and PLA 3C~~ zones, the maximum allowed number of dwelling units shall be computed by multiplying the gross area of the subject property by the applicable residential density number per acre shown on the Zoning Map. ~~In the RSA zone, f~~For the purpose of calculating the maximum units per acre, all road dedications and vehicular access easements and tracts shall be included in the calculation for density. The maximum development potential requirements of Chapter 90 KZC shall apply.

.595 Office Zones

– The following zones: PO; PR 8.5; PR 5.0; PR 3.6; PR 2.4; PR 1.8; PRA 1.8; JBD 3; ~~PLA 3A~~; PLA 5B, C; PLA 6B; PLA 15A; PLA 17A; ~~FC III~~; MSC 1; MSC 4; NRH 2; NRH 3; NRH 5; NRH 6; RH 4; RH 8; TL 1A; TL 10A, TL 10B, TL 10C, TL 10D and TL 10E.

.785 Residential Zone

– The following zones: RS 35; RSX 35; RS 12.5; RSX 12.5; RS 8.5; RSX 8.5; RSA 8; RS 7.2; RSX 7.2; RS 6.3; RSA 6; RS 5.0; RSX 5.0; RSA 4; RSA 1; RM 5.0; RMA 5.0; RM 3.6; RMA 3.6; RM 2.4; RMA 2.4; RM 1.8; RMA 1.8; WD I; WD II; WD III; TL 9B; PLA 2; PLA 3B; PLA 5A, D, E; PLA 6A, C, D, E, F, H, I, J, K; PLA 7A, B, C; PLA 9; PLA 15B; PLA 16; PLA 17; ~~and~~ TL 11; ~~PLA 3C~~.

Amendments to KZC 10.25 Zoning Categories Adopted-

The City is divided into the following zoning categories:

Zoning Category	Symbol
1. Single-Family Residential Zones	RS, RSA and RSX (followed by a designation indicating minimum lot size per dwelling unit or units per acre)
2. Multifamily Residential Zones	RM and RMA (followed by a designation indicating minimum lot size per dwelling unit)
3. Professional Office/Residential Zones	PR and PRA (followed by a designation indicating minimum lot size per dwelling unit)
4. Professional Office Zones	PO
5. Waterfront Districts	WD (followed by a designation indicating which Waterfront District)
6. Freeway Commercial Zones <u>Yarrow Bay Business District</u>	FC (followed by a designation indicating which Freeway Commercial Zone) <u>YBD (followed by a designation indicating which sub-zone within the Yarrow Bay Business District)</u>
7. Neighborhood Business	BN and BNA
8. Community Business	BC, BC 1, BC 2 and BCX
9. Central Business District	CBD (followed by a designation indicating which sub-zone within the Central Business District)
10. Juanita Business District	JBD (followed by a designation indicating which sub-zone within the Juanita Business District)
11. Market Street Corridor	MSC (followed by a designation indicating which sub-zone within the Market Street Corridor)
12. North Rose Hill Business District	NRH (followed by a designation indicating which sub-zone within the North Rose Hill Business District)
13. Rose Hill Business District	RH (followed by a designation indicating which sub-zone within the Rose Hill Business District)
14. Totem Center and Totem Lake Neighborhood	TL (followed by a designation indicating which sub-zone within Totem Center or the Totem Lake Neighborhood)
15. Light Industrial Zones	LIT, TL 7
16. Planned Areas	PLA (followed by a designation indicating which Planned Area, and in some cases, which sub-zone within a Planned Area)
17. Park/Public Use Zones	P

CHAPTER 25 – PROFESSIONAL OFFICE RESIDENTIAL (PR) AND PROFESSIONAL OFFICE RESIDENTIAL A (PRA) ZONES Proposed Amendments

25.05 User Guide.

The charts in KZC 25.10 contain the basic zoning regulations that apply in each PR 8.5, PR 5.0, PR 3.6, PR 2.4 and PR 1.8 and PRA 1.8 zone of the City. Use these charts by reading down the left hand column entitled Use. Once you locate the use in which you are interested, read across to find the regulations that apply to that use.

Section 25.08

Zone
PR, PRA

Section 25.08 – GENERAL REGULATIONS

The following regulations apply to all uses in this zone unless otherwise noted:

1. Refer to Chapter 1 KZC to determine what other provisions of this code may apply to the subject property.
2. Developments creating four or more new dwelling units shall provide at least 10 percent of the units as affordable housing units as defined in Chapter 5 KZC. Two additional units may be constructed for each affordable housing unit provided. In such cases, the minimum lot size listed in the Use Regulations shall be used to establish the base number of units allowed on the site, but shall not limit the size of individual lots. See Chapter 112 KZC for additional affordable housing incentives and requirements.
3. If any portion of a structure is adjoining a low density zone, then either:
 - a. The height of that portion of the structure shall not exceed 15 feet above average building elevation, or
 - b. The maximum horizontal facade shall not exceed 50 feet in width.See KZC 115.30, Distance Between Structures/Adjacency to Institutional Use, for further details.
- ~~4.~~ ~~The required yard of a structure abutting Lake Washington Boulevard or Lake St. S. must be increased two feet for each one foot that structure exceeds 25 feet above average building elevation (does not apply to Public Park uses).~~
- ~~5.~~4 If the property is located south of NE 85th Street between 124th Avenue and 120th Avenue, to the extent possible, the applicant shall save existing viable significant trees within the required landscape buffers separating nonresidential development from adjacent single-family homes.
- ~~6.~~5 Within the PRA zone, the maximum building height of a structure may be increased to 60 feet above average building elevation if:
 - a. All required yards are increased by one foot for every two feet of height above 35 feet;
 - b. Buildings may not exceed three stories; and
 - c. Rooftop appurtenances may not exceed the maximum height and are screened with sloped roof forms.
- ~~7.~~6 May also be regulated under the Shoreline Master Program; refer to Chapter 83 KZC.

Section 25.10	USE ↓ REGULATIONS ↓	Required Review Process	MINIMUMS			MAXIMUMS		Landscape Category (See Ch. 95)	Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)	Special Regulations (See also General Regulations)	
			Lot Size	REQUIRED YARDS (See Ch. 115)			Height of Structure					
				Front	Side	Rear						
.010	Detached Dwelling Units	None	8,500 sq. ft. if PR 8.5 zone, 5,000 sq. ft. if PR 5.0 zone, otherwise 3,600 sq. ft.	20'	5'	10'	70%	If adjoining a low density zone other than RSA or RSX, then 25' above average building elevation. See Spec. Reg. 6.	E	A	2.0 per dwelling unit.	<ol style="list-style-type: none"> For this use, only one dwelling unit may be on each lot regardless of lot size. Chapter 115 KZC contains regulations regarding home occupations and other accessory uses, facilities and activities associated with this use.
.020	Detached, Attached or Stacked Dwelling Units	Within the NE 85th Street Subarea and Yarrow Bay Business District , Chapter 142 KZC. Otherwise, none.	8,500 sq. ft. if PR 8.5 zone, 5,000 sq. ft. if PR 5.0 zone, otherwise 3,600 sq. ft. with a density as established on the Zoning Map. See Spec. Reg. 1.	For PR zones: 5' each for detached units and 5' but 2 side yards must equal at least 15' for attached and stacked units. For PRA zones: 5' each side. See Spec. Reg. 4.	10' See Spec. Reg. 5.	Otherwise, for PR zones, 30' above average building elevation and for PRA zones, 35' above average building elevation. See Gen. Reg. 6-5		D		1.7 per unit.	<ol style="list-style-type: none"> Minimum amount of lot area per dwelling unit is as follows: <ol style="list-style-type: none"> In PR 8.5 zones, the minimum lot area per unit is 8,500 sq. ft. In PR 5.0 zones, the minimum lot area per unit is 5,000 sq. ft. In PR 3.6 zones, the minimum lot area per unit is 3,600 sq. ft. In PR 2.4 zones, the minimum lot area per unit is 2,400 sq. ft. In PR 1.8 zones and PRA 1.8 zones, the minimum lot area per unit is 1,800 sq. ft. Chapter 115 KZC contains regulations regarding home occupations and other accessory uses, facilities and activities associated with this use. Chapter 115 KZC contains regulations regarding common recreational space requirements for this use. The side yard may be reduced to zero feet if the side of the dwelling unit is attached to a dwelling unit on an adjoining lot. If one side of a dwelling unit is so attached and the opposite side is not, the side that is not attached must provide a minimum side yard of five feet. The rear yard may be reduced to zero feet if the rear of the dwelling unit is attached to a dwelling unit on an adjoining lot. Where the 25-foot height limitation results solely from an adjoining low density zone occupied by a school that has been allowed to increase its height to at least 30 feet, then a structure height of 30 feet above average building elevation is allowed. 	

.030	Office Uses	Within the NE 85th Street Subarea <u>and Yarrow Bay Business District</u> , D.R., Chapter 142 KZC. Otherwise, none.	None	20'	For PR zones: 5' but 2 side yards must equal at least 15'. For PRA zones: 5' each in the PRA zones.	10'	70%	If adjoining a low density zone other than RSA or RSX, then 25' above average building elevation. Otherwise, for PR zones, 30' above average building elevation and for PRA zones, 35 feet above average building elevation. See Gen. Reg. <u>6-5</u>	C	D	If medical, dental or veterinary office, then one per each 200 sq. ft. of gross floor area. Otherwise one per each 300 sq. ft. of gross floor area.	<ol style="list-style-type: none"> 1. The following regulations apply to veterinary offices only: <ol style="list-style-type: none"> a. May only treat small animals on the subject property. b. Outside runs and other outside facilities for the animals are not permitted. c. Site must be designed so that noise from this use will not be audible off the subject property. A certification to this effect, signed by an acoustical engineer, must be submitted with the development permit application. 2. Ancillary assembly and manufacture of goods on the premises of this use are permitted only if: <ol style="list-style-type: none"> a. The ancillary assembled or manufactured goods are subordinate to and dependent on this use. b. The outward appearance and impacts of this use with ancillary assembly or manufacturing activities must be no different from other office uses.
.040	Development Containing Stacked or Attached Dwelling Units and Office Uses. See Spec. Reg. 1.	Within the NE 85th Street Subarea <u>and Yarrow Bay Business District</u> , D.R., Chapter 142 KZC. Otherwise, none.	3,600 sq. ft. with a residential density as established on the Zoning Map. See Spec. Reg. 2.	20'	For PR zones: 5' but 2 side yards must equal at least 15'. For PRA zones: 5' each in the PRA zones.	10'	70%	If adjoining a low density zone other than RSA or RSX, then 25' above average building elevation. See Spec. Reg. 5. Otherwise, for PR zones, 30' above average building elevation and for PRA zones, 35' above average building elevation. See Gen. Reg. <u>6-5</u>	C	D	See KZC 105.25.	<ol style="list-style-type: none"> 1. A veterinary office is not permitted in any development containing dwelling units. 2. Minimum amount of lot area per dwelling unit is as follows: <ol style="list-style-type: none"> a. In PR 8.5 zones, the minimum lot area per unit is 8,500 square feet. b. In PR 5.0 zones, the minimum lot area per unit is 5,000 square feet. c. In PR 3.6 zones, the minimum lot area per unit is 3,600 square feet. d. In PR 2.4 zones, the minimum lot area per unit is 2,400 square feet. e. In PR 1.8 and PRA 1.8 zones, the minimum lot area per unit is 1,800 square feet. 3. Chapter 115 KZC contains regulations regarding home occupations and other accessory uses, facilities and activities associated with this use. 4. Chapter 115 KZC contains regulations regarding common recreational space requirements for this use. 5. Ancillary assembly and manufacture of goods on the premises of this use are permitted only if: <ol style="list-style-type: none"> a. The ancillary assembled or manufactured goods are subordinate to and dependent on this use. b. The outward appearance and impacts of this use with ancillary assembly or manufacturing activities must be no different from other office uses. 6. Where the 25-foot height limitation results solely from an adjoining low density zone occupied by a school that has been allowed to increase its height to at least 30 feet, then a structure height of 30 feet above average building elevation is allowed.

.050	Restaurant or Tavern	Within the NE 85th Street Subarea, <u>and Yarrow Bay Business District</u> D.R., Chapter 142 KZC. Otherwise, Process I, Chapter 145 KZC.	8,500 sq. ft. if PR 8.5 zone, otherwise 7,200 sq. ft.	20'	10' on each side.	10'	70%	If adjoining a low density zone other than RSA or RSX, then 25' above average building elevation. Otherwise, for PR zones, 30' above average building elevation and for PRA zones, 35' above average building elevation. See Gen. Reg. <u>6-5</u>	B	E	1 per each 100 sq. ft. floor area.	<p>1. This use is not permitted in a PR 3.6 zone located in the NE 85th Street Subarea.</p> <p>2. <u>This use is allowed in the Lakeview Neighborhood if located south of NE 60th Street between Lakeview Dr. and Lake Washington Blvd NE provided that:</u></p> <p>a. <u>Both the front building façade and vehicular access are not located along Lakeview Dr.</u></p> <p>b. <u>Internal lit signs are not located along Lakeview Dr and NE 60th Street</u></p> <p>c. <u>Gross floor area shall not exceed 3,000 square feet</u></p> <p>d. <u>On lots 13 and 14 of Block 2 of Houghton Addition Volume 5 of Plats, Page 71 of King County Records and if a change of use is proposed within a structure that existed on November 15, 2011 and requires additional parking the following shall apply:</u></p> <p>1) <u>The number of required parking spaces shall be determined based on the actual parking demand pursuant to Section 105.25, KZC. The required additional parking for the new use may be provided by adding parking along the frontage of the subject property or across the street within the NE 60th ST right of way at the developer's expense.</u></p> <p>2) <u>On Lot 13 a historic interpretive sign shall be installed.</u></p> <p><u>32. Drive-in or drive-through facilities are prohibited.</u></p>
.060	Grocery Store, Drug Store, Laundromat, Dry Cleaners, Barber Shop, or Shoe Repair Shop										1 per each 300 sq. ft. floor area.	<p>1. This use is not permitted in a PR 3.6 zone located in the NE 85th Street Subarea.</p> <p>2. May not be located above the ground floor of a structure.</p> <p>3. Gross floor area shall not cannot exceed 3,000 square feet.</p> <p>4. <u>This use is allowed in the Lakeview Neighborhood if located south of NE 60th Street between Lakeview Dr. and Lake Washington Blvd NE provided that:</u></p> <p>a. <u>Both the front building façade and vehicular access are not located along Lakeview Dr.</u></p> <p>b. <u>Internal lit signs are not located along Lakeview Dr and NE 60th Street</u></p> <p>c. <u>Gross floor area shall not exceed 3,000 square feet</u></p> <p>d. <u>On Lots 13 and 14 of Block 2 of Houghton Addition Volume 5 of Plats, Page 71 of King County Records if a change of use is proposed within a structure that existed on November 15, 2011 and requires additional parking the following shall apply:</u></p> <p>1) <u>The number of required parking spaces shall be determined based on the actual parking demand pursuant to Section 105.25, KZC . The required additional parking for the new use may be provided by adding parking along the frontage of the subject property or across the street within the NE 60th ST right of way at the developer's expense.</u></p> <p>2) <u>On Lot 13 a historic interpretive sign shall be installed.</u></p>
.070	<u>Any Retail Establishment other than those specifically listed, limited, or prohibited in this zone, selling</u>				<u>10' on each side</u>	<u>10'</u>			<u>B</u>	<u>E</u>		<p><u>1. This use is only allowed in the Lakeview Neighborhood and if located south of NE 60th Street between Lakeview Dr. and Lake Washington Blvd NE provided that:</u></p> <p>a. <u>Both the front building façade and vehicular access are not located along Lakeview Dr.</u></p> <p>b. <u>Internal lit signs are not located along Lakeview Dr and NE 60th Street</u></p>

		<p>none.</p> <p>If this use is adjoining a low density zone, then Process I, Chapter 145 KZC.</p>		<p>If this use can accommodate 13 to 49 students or children, then:</p> <p>20' 20' on 20' each side</p>								<p>elevation.</p> <p>Otherwise, for PR zones, 30' above average building elevation and for PRA zones, 35' above average building elevation.</p> <p>See Gen. Reg. 6-5 and Spec. Reg. 7.</p>				<ol style="list-style-type: none"> 3. An on-site passenger loading area must be provided. The City shall determine the appropriate size of the loading area on a case-by-case basis, depending on the number of attendees and the extent of the abutting right-of-way improvements. Carpooling, staggered loading/unloading time, right-of-way improvements or other means may be required to reduce traffic impacts on nearby residential uses. 4. May include accessory living facilities for staff persons. 5. To reduce impacts on nearby residential uses, hours of operation of the use may be limited and parking and passenger loading areas relocated. 6. These uses are subject to the requirements established by the Department of Social and Health Services (WAC Title 388). 7. For school use, structure height may be increased, up to 35 feet in PR zones and 40 feet in PRA zones, if: <ol style="list-style-type: none"> a. The school can accommodate 200 or more students; and b. The required side and rear yards for the portions of the structure exceeding the basic maximum structure height are increased by one foot for each additional one foot of structure height; and c. The increased height is not specifically inconsistent with the applicable neighborhood plan provisions of the Comprehensive Plan; and d. The increased height will not result in a structure that is incompatible with surrounding uses or improvements. <i>This special regulation is not effective within the disapproval jurisdiction of the Houghton Community Council.</i> 8. For a Mini-School or Mini-Day-Care Center use, electrical signs shall not be permitted and the size of signs may be limited to be compatible with nearby residential uses. 9. These uses are subject to the requirements established by the Department of Social and Health Services (WAC Title 388).
.100, 110	Mini-School or Mini-Day-Care	<p>Within the NE 85th Street Subarea, D.R., Chapter 142 KZC. Otherwise, none.</p>	<p>8,500 sq. ft. if PR 8.5 zone, 7,200 sq. ft. if PR 7.2 zone, 5,000 sq. ft. if PR 5.0 zone, otherwise 3,600 sq. ft.</p>	<p>20'</p>	<p>For PR zones: 5' but 2 side yards must equal at least 15'. For PRA zones: 5' each in the PRA zones.</p>	10'	70%	<p>If adjoining a low density zone other than RSA or RSX, then 25' above average building elevation.</p> <p>Otherwise, for PR zones, 30' above average building elevation and for PRA zones,</p>	E	B	See KZC 105.25.	<ol style="list-style-type: none"> 1. A six-foot-high fence is required along the property lines adjacent to the outside play areas. 2. Structured play areas must be set back from all property lines by five feet. 3. An on-site passenger loading area may be required depending on the number of attendees and the extent of the abutting right-of-way improvements. 4. To reduce impacts on nearby residential uses, hours of operation of the use may be limited and parking and passenger loading areas relocated. 5. Electrical signs shall not be permitted. Size of signs may be limited to be compatible with nearby residential uses. 6. May include accessory living facilities for staff persons. 7. These uses are subject to the requirements established by the Department of Social and Health Services (WAC Title 388). 				

-140/120	Assisted Living Facility							35' above average building elevation. See Gen. Reg. 6-5	D	A	1.7 per independent unit. 1 per assisted living unit.	<ol style="list-style-type: none"> 1. A facility that provides both independent dwelling units and assisted living units shall be processed as an assisted living facility. 2. If a nursing home use is combined with an assisted living facility use in order to provide a continuum of care for residents, the required review process shall be the less intensive process between the two uses. 3. For density purposes, two assisted living units shall constitute one dwelling unit. Total dwelling units may not exceed the number of stacked dwelling units allowed on the subject property. Through Process IIB, Chapter 152 KZC, up to 1 1/2 times the number of stacked dwelling units allowed on the property may be approved if the following criteria are met: <ol style="list-style-type: none"> a. Project is of superior design, and b. Project will not create impacts that are substantially different than would be created by a permitted multifamily development. 4. The assisted living facility shall provide usable recreation space of at least 100 square feet per unit, in the aggregate, for both assisted living units and independent dwelling units, with a minimum of 50 square feet of usable recreation space per unit located outside. 5. Chapter 115 KZC contains regulations regarding home occupations and other accessory uses, facilities, and activities associated with this use.
-120/130	Convalescent Center or Nursing Home	Within the NE 85th Street Subarea, D.R., Chapter 142 KZC.	8,500 sq. ft. if PR 8.5 zone, otherwise 7,200 sq. ft.	20'	10' on each side	10'	70%	If adjoining a low density zone other than RSA and RSX, then 25' above average building elevation.	C	B	1 for each bed.	<ol style="list-style-type: none"> 1. If a nursing home use is combined with an assisted living facility use in order to provide a continuum of care for residents, the required review process shall be the less intensive process between the two uses.
-130/140	Public Utility	Otherwise, Process I, Chapter 145 KZC.	None		20' on each side	20'		Otherwise, for PR zones, 30' above average building elevation and for PRA zones, 35' above average building elevation. See Gen. Reg. 6-5	A		See KZC 105.25.	
-140/150	Government Facility Community Facility				10' each side	10'			C See Spec. Reg. 2.			<ol style="list-style-type: none"> 1. Site design must minimize adverse impacts on surrounding residential neighborhoods. 2. Landscape Category A or B may be required depending on the type of use on the subject property and the impacts associated with the use on the nearby uses.
-150/160	Public Park	Development standards will be determined on case-by-case basis. See Chapter 49 KZC for required review process.										

CHAPTER 56 – YBD 2 and YBD 3 – Draft Revised 8-1-2011

56.15 User Guide.

The charts in KZC 56.20 contain the basic zoning regulations that apply in each YBD 2 and YBD 3 zone of the City. Use these charts by reading down the left hand column entitled Use. Once you locate the use in which you are interested, read across to find the regulations that apply to that use.

**Section 56.18
YBD 2, YBD 3**

Section 56.18 – GENERAL REGULATIONS

The following regulations apply to all uses in this zone unless otherwise noted:

1. Refer to Chapter 1 KZC to determine what other provisions of this code may apply to the subject property.
2. In addition to the height exceptions established by KZC [115.60](#), the following exceptions to height regulations in the YBD 2 and YBD 3 zones are allowed:
 - a. Decorative parapets may exceed the height limit by a maximum of four feet; provided that the average height of the parapets around the perimeter of the structure shall not exceed two feet.
 - b. For structures with a peaked roof, the peak may extend eight feet above the height limit if the slope of the roof is equal to or greater than four feet vertical to 12 feet horizontal.
3. A City entry or gateway feature shall be designed and installed on the subject property adjacent Lake Washington Blvd. between the southern city limit line and NE 38th PI pursuant to the standards in KZC 110.60. The specific location and design of the gateway shall be evaluated with the Design Review Process.
4. Driveways onto Lake Washington Blvd., NE 38th PI. and Northup Way shall be limited to prevent arterial congestion and traffic safety hazards. Shared access points must be utilized where feasible (does not apply to Public Park uses). The Public Works Official shall approve the number, location and design of all driveways.
5. The minimum ground floor story height shall be 13' for retail establishments selling goods or services including banking and financial services, restaurant and tavern, or office.
6. The upper story setback for all floors above the second story within 40' of the property line abutting NE 38th Place shall average 15'. For the purpose of this regulation, the term "setback" shall refer to the horizontal distance between the property line and any exterior wall abutting the street prior to any potential right of way dedication. The required upper story setbacks for all floors above the second story shall be calculated as Total Upper Story Setback Area, as shown on Plate 35.
7. Developments in parts of this zone may be limited by chapter 83 or 90 KZC, regarding development near streams, lakes, and wetlands.

DIRECTIONS: FIRST, read down to find use...THEN, across for REGULATIONS												
Section 56.20	USE ↓ REGULATIONS ↓	Require d Review Process	MINIMUMS				MAXIMUMS		Landscape Category (See Ch. 95)	Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)	Special Regulations (See also General Regulations)
			Lot Size	REQUIRED YARDS (See Ch. 115)			Height of Structure					
				Front	Side	Rear						
.010	Vehicle Service Station	D.R., Chapter 142 KZC	22,500 sq. ft.	40'	15' on each side. See also Special Regulation 3.	15'	80%	In YBD 2 55' above average building elevation. In YBD 3 60' above average building elevation.	A	E	See KZC 105.25.	<ol style="list-style-type: none"> 1. The following uses and activities are prohibited: <ol style="list-style-type: none"> a. The outdoor storage, sale, service and/or rental of motor vehicles, sailboats, motor boats, and recreational trailers. 2. There may not be more than two vehicle service stations at any intersection. This use is only allowed if the subject property abuts Lake Washington Blvd. or Northup Way. 3. Gas pump islands may extend 20 feet into the front yard. Canopies or covers over gas pump islands may not be closer than 10 feet to any property line. Outdoor parking and service areas may not be closer than 10 feet to any property line. See KZC 115.105, Outdoor Use, Activity and Storage, for further regulations
.020	Restaurant or Tavern	D.R., Chapter 142 KZC	None	0' adjacent to NE 38 th Pl and Northup Way otherwise 20'	0'	0			B		1 per each 100 sq. ft. of gross floor area.	<ol style="list-style-type: none"> 1. The following uses and activities are prohibited: <ol style="list-style-type: none"> a. Drive in or drive through facilities. 2. The gross floor area of individual retail establishments may not exceed 15,000 sq. ft. except within a mixed use development in which the floor area of other uses exceeds the floor area of retail establishments.

<p>.030</p>	<p>Office Use</p>	<p>D.R. Chapter 14 2, KZC</p>	<p>None</p>	<p>0' adjacent to NE 38th Pl and Northup Way otherwise 20'</p>	<p>0'</p>	<p>0'</p>	<p>80%</p>	<p>In YBD 2 55' above average building elevation. In YBD 3 60' above average building elevation.</p>	<p>C</p>	<p>D</p>	<p>If Medical, Dental or Veterinary office, then one per each 200 sq. ft. of gross floor area. Otherwise, 1 per each 300 sq. ft. of gross floor area.</p>	<p>1. The following regulations apply to veterinary offices only: a. May only treat small animals on the subject property. b. Outside runs and other outside facilities for the animals are not permitted. c. Site must be designed so that noise from this use will not be audible off the subject property. A certification to this effect, signed by an Acoustical Engineer, must be submitted with the development permit application. 2. Ancillary assembly and manufacture of goods on the premises of this use are permitted only if: a. The ancillary assembled or manufactured goods are subordinate to and dependent on this use. b. The outward appearance and impacts of this use with ancillary assembly or manufacturing activities must be no different from other office uses.</p>
<p>.040 .050</p>	<p>Hotel or Motel A Retail Establishment other than those specifically listed, limited, or prohibited in the zone, selling goods, or providing services including banking and related financial services</p>	<p>D.R. Chapter 14 2, KZC</p>	<p>None</p>	<p>0' adjacent to NE 38th Pl and Northup Way otherwise 20'</p>	<p>0'</p>	<p>0'</p>	<p>80%</p>	<p>In YBD 2 55' above average building elevation. In YBD 3 60' above average building elevation.</p>	<p>B</p>	<p>E</p>	<p>1 per each room. See also Special Regulation 2. 1 per each 300 sq. ft. of gross floor area.</p>	<p>1. May include ancillary meeting and convention facilities. 2. Excludes parking requirements for ancillary meeting and convention facilities. Additional parking requirement for these ancillary uses shall be determined on a case by case basis. 1. The following uses and activities are prohibited: a. The outdoor storage, sale, service and/or rental of motor vehicles, sailboats, motor boats, and recreational trailers. b. Vehicle repair, c. Retail establishment providing storage services unless accessory to another permitted use. d. Storage and operation of heavy equipment, except delivery vehicles associated with retail uses. e. Storage of parts unless conducted entirely within an enclosed structure, f. Drive in or drive through facilities. 2. The gross floor area of individual retail establishments may not exceed 15,000 sq. ft. except within a mixed use development in which the floor area of other uses exceeds the floor area of retail establishments. 3. A delicatessen, bakery, or other similar use may include, as part of the use, accessory seating if: a. The seating and associated circulation area does not exceed</p>

more than 10 percent of the gross floor area of the use; and
 b. It can be demonstrated to the City that the floor plan is designed to preclude the seating area from being expanded.

.060	Stacked Dwelling Units	D.R. Chapter 142, KZC	None	0' adjacent to NE 38 th PI and Northup Way otherwise 20'	0'	0'	80%	In YBD 2 55' above average building elevation. In YBD 3 60' above average building elevation.	D	A	1.7 per unit	1. Chapter 115 KZC contains regulations regarding home occupations and other accessory uses, facilities and activities associated with this use.
.070	Assisted Living Facility, Convalescent Center or Nursing Home			0' adjacent to NE 38 th PI and Northup Way otherwise 20'	0"	0"			C		Independent unit: 1.7 per unit. Assisted living unit: 1 per unit. Convalescent Center or Nursing Home: 1 per each bed.	1. A facility that provides both independent dwelling units and assisted living units shall be processed as an assisted living facility. 2. 3. If a nursing home use is combined with an assisted living facility use in order to provide a continuum of care for residents the required review process shall be the least intensive process between the two uses.
.080	Private Lodge or Club	D.R. Chapter 142, KZC	None	0' adjacent to NE 38 th PI and Northup Way otherwise 20'	0'	0'	80%	In YBD 2 55' above average building elevation. In YBD 3 60' above average building elevation.	C	B	1 per each 300 sq. ft. of gross floor area	
.090	Hospital Facility	D.R. Chapter 142, KZC	None	0' adjacent to NE 38 th PI and Northup Way otherwise 20'	0'	0'	80%	In YBD 2 55' above average building elevation. In YBD 3 60' above average building elevation.	B	B	See KZC 105.25	

.100	Public Utility	D.R. Chapter 142, KZC	None	0' adjacent to NE 38 th Pl and Northup Way otherwise 20'	0'	0'	80 %	In YBD 2 55' above average building elevation. In YBD 3 60' above average building elevation.	A	B	See KZC 105.25	
.110	Church			0' adjacent to NE 38 th Pl and Northup Way otherwise 20'	0'	0'			C	B	1 for every 4 people based on maximum occupant load of any area of worship. See Special Reg 2	<ol style="list-style-type: none"> 1. May include accessory living facilities for staff persons. 2. No parking is required for day-care or school ancillary to the use.
.120	School or Day-Care Center	D.R. Chapter 142, KZC	None	0' adjacent to NE 38 th Pl and Northup Way otherwise 20'	0'	0'	80 %	In YBD 2 55' above average building elevation. In YBD 3 60' above average building elevation.	D	B	See KZC 105.25.	<ol style="list-style-type: none"> 1. A six-foot-high fence is required only along the property lines adjacent to the outside play areas. 2. Structured play areas must be setback from all property lines by at least five feet. 3. An on-site passenger loading area must be provided depending on the number of attendees and the extent of the abutting right-of-way improvements. 4. May include accessory living facilities for staff persons.
.130	Mini-School or Mini-Day-Care	D.R. Chapter 142, KZC	None	0' adjacent to NE 38 th Pl and Northup Way otherwise 20'	0>	0>	80 %	In YBD 2 55' above average building elevation. In YBD 3 60' above average building elevation.	E	B	See KZC 105.25.	<ol style="list-style-type: none"> 1. A six-foot-high fence is required along the property lines adjacent to the outside play areas. 2. Structured play areas must be setback from all property lines by at least five feet. 3. An on-site passenger loading area must be provided depending on the number of attendees and the extent of the abutting right-of-way improvements. 4. May include accessory living facilities for staff persons.
.140	Government Facility Community Facility								C See Spec. Reg. 1			<ol style="list-style-type: none"> 1. Landscape Category A or B may be required depending on the type of use on the subject property and the impacts associated with the use on the nearby uses.
.150	Public Park	Development standards will be determined on a case-by-case basis. See Chapter 49 KZC for required review process.										

60.14 User Guide. [Proposed Amendments](#)

The charts in KZC 60.17 contain the basic zoning regulations that apply in Planned Area 2, including sub-zones. Use these charts by reading down the left hand column entitled Use. Once you locate the use in which you are interested, read across to find the regulations that apply to that use.

**Section
60.15**

**Zone
PLA2**

Section 60.15 – GENERAL REGULATIONS

The following regulations apply to all uses in this zone unless otherwise noted:

1. Refer to Chapter 1 KZC to determine what other provisions of this code may apply to the subject property.
2. Developments creating four or more new dwelling units shall provide at least 10 percent of the units as affordable housing units as defined in Chapter 5 KZC. Two additional units may be constructed for each affordable housing unit provided. In such cases, the minimum lot size listed in the Use Regulations shall be used to establish the base number of units allowed on the site, but shall not limit the size of individual lots. See Chapter 112 KZC for additional affordable housing incentives and requirements.
3. Development in parts of this zone may be limited by Chapter 83 or 90 KZC, regarding development near streams, lakes and wetlands. In addition, the site must be designed to concentrate development away from, and to minimize impact on, the wetlands.
4. May not use lands waterward of the ordinary high water mark to determine lot size or to calculate allowable density.
5. May also be regulated under the Shoreline Master Program; refer to Chapter 83 KZC.

DIRECTIONS: FIRST, read down to find use...THEN, across for REGULATIONS

Section 60.17	USE ↓ REGULATIONS ↘	Required Review Process	MINIMUMS			MAXIMUMS		Landscape Category (See Ch. 95)	Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)	Special Regulations (See also General Regulations)	
			Lot Size	REQUIRED YARDS (See Ch. 115)			Height of Structure					
				Front	Side	Rear						
.010	Attached or Stacked Dwelling Unit	Process IIB, Chapter 152 KZC.	35,000 sq. ft. per unit	20'	5', but 2 side yards must equal at least 15'.	10'	60%	25' above average building elevation. See Spec. Reg. 3.	D	A	1.7 per unit.	<p>1. No structure may be waterward of the ordinary high water mark.</p> <p>2. If the development includes portions of Planned Area 3, the applicant may propose and the City may require that part or all of the density allowed in Planned Area 2 be developed in Planned Area 3.</p> <p>32.The height of a structure may be increased as long as neither of the following maximums is exceeded:</p> <p>a. The structure may not exceed 60 feet above average building elevation.</p> <p>b. The structure may not exceed a plane that starts 3.5 feet above the outside westbound lane of SR 520 and ends at the high waterline of Lake Washington in the zone, excluding the canal.</p> <p>43.Chapter 115 KZC contains regulations regarding home occupations and other accessory uses, facilities, and activities associated with this use.</p> <p>54.The side yard may be reduced to zero feet if the side of the dwelling unit is attached to a dwelling unit on an adjoining lot. If one side of a dwelling unit is so attached and the opposite side is not, the side that is not attached must provide a minimum side yard of five feet.</p> <p>65.The rear yard may be reduced to zero feet if the rear of the dwelling unit is attached to a dwelling unit on an adjoining lot.</p>

See Spec. Regs. ~~45~~ and ~~56~~.

.020	Mini-School or Mini-Day-Care	Process IIB, Chapter 152 KZC.	35,000 sq. ft.	20'	5', but 2 side yards must equal at least 15'.	10'	60%	25' above average building elevation. See Spec. Reg. 23.	D	B	See KZC 105.25.	<ol style="list-style-type: none"> 1. No structure may be waterward of the ordinary high water mark. 2. If the development includes portions of Planned Area 3, the applicant may propose and the City may require that part or all of the density allowed in Planned Area 2 be developed in Planned Area 3. 23. The height of a structure may be increased as long as neither of the following maximums is exceeded: <ol style="list-style-type: none"> a. The structure may not exceed 60 feet above average building elevation. b. The structure may not exceed a plane that starts 3.5 feet above the outside westbound lane of SR 520 and ends at the high waterline of Lake Washington in the zone, excluding the canal. 34. May locate on the subject property if: <ol style="list-style-type: none"> a. It will serve the immediate neighborhood in which it is located; or b. It will not be detrimental to the character of the neighborhood in which it is located. 45. A six-foot-high fence is required along the property lines adjacent to the outside play areas. 56. Hours of operation may be limited by the City to reduce impacts on nearby residential areas. 67. Structured play areas must be set back from all property lines by five feet. 78. An on-site passenger loading area may be required depending on the number of attendees and the extent of the abutting right-of-way improvements. 89. The location of parking and passenger loading areas shall be designed to reduce impacts on nearby residential areas. 940. May include accessory living facilities for staff persons. 1044. These uses are subject to the requirements established by the Department of Social and Health Services (WAC Title 388).
.030	Public Utility	Process IIA, Chapter 150 KZC.	None	20'	20' on each side	10'	70%	25' above average building elevation.	A	A	See KZC 105.25.	<ol style="list-style-type: none"> 1. Site design must minimize adverse impacts on surrounding residential neighborhoods. 2. Landscape Category A may be required depending on the type of use on the subject property and the impacts associated with the use on the nearby uses.
.040	Government Facility				10' on each side				B See Spec. Reg. 2.	B		
.050	Community Facility				Process IIB, Chapter 152 KZC.							
.060	Public Park	Development standards will be determined on a case-by-case basis. See Chapter 49 KZC for required review process.										<ol style="list-style-type: none"> 1. Portions of the park located within the wetlands must be devoted exclusively to passive recreation that is not consumptive of the natural environment.

30.29 User Guide. Proposed Amendments

The charts in KZC 30.35 contain the basic zoning regulations that apply in the WD III zones of the City. Use these charts by reading down the left hand column entitled Use. Once you locate the use in which you are interested, read across to find the regulations that apply to that use.

Section
30.30

Zone
WDIII

Section 30.30 – GENERAL REGULATIONS

The following regulations apply to all uses in this zone unless otherwise noted:

1. Refer to Chapter 1 KZC to determine what other provisions of this code may apply to the subject property.
2. Developments creating four or more new dwelling units shall provide at least 10 percent of the units as affordable housing units as defined in Chapter 5 KZC. Two additional units may be constructed for each affordable housing unit provided. In such cases, the minimum lot size listed in the Use Regulations shall be used to establish the base number of units allowed on the site, but shall not limit the size of individual lots. See Chapter 112 KZC for additional affordable housing incentives and requirements.
3. May not use lands waterward of the ordinary high water mark to determine lot size or to calculate allowable density.
4. The required 30-foot front yard may be reduced, subject to all of the following conditions:
 - a. The existing primary structure does not conform to the minimum shoreline setback standard;
 - b. The proposed complete replacement or replacement of portion of the existing primary structure comply with the minimum required shoreline setback established under the provisions of Chapter 83 KZC, or as otherwise approved under the shoreline setback reduction provisions established in KZC 83.380; and
 - c. The front yard for the complete replacement or the portion of replacement may be reduced one foot for each one foot of the shoreline setback that is increased in dimension from the setback of the existing nonconforming primary structure ~~;~~ provided, that subsection (4)(d) of this regulation is not; and
 - d. ~~Within the front yard, each portion of the replaced or portion of replaced primary structure is set back from the front property line by a distance greater than or equal to the maximum height of that portion above the front property line.~~

(Does not apply to Public Access Pier, Boardwalk, or Public Access Facility; Piers, Docks, Boat Lifts and Canopies Serving Detached Dwelling Unit; Piers, Docks, Boat Lifts and Canopies Serving Detached, Attached or Stacked Dwelling Units; Public Park; Public Utility Uses; Boat Launch; or Water Taxi).
5. ~~The required 30-foot front yard may be reduced, subject to the following conditions:~~
 - a. ~~The existing primary structure does not conform to the minimum shoreline setback standard;~~
 - b. ~~The front yard may be reduced one foot for each one foot of the shoreline setback that is increased in dimension;~~
 - c. ~~The new or remodeled primary structure must comply with the minimum required shoreline setback established under the provisions of Chapter 83 KZC, or as otherwise approved under the shoreline setback reduction provisions established in KZC 83.380; and~~
 - d. ~~Within 30 feet of the front property line, each portion of a structure is set back from the front property line by a distance greater than or equal to the height of that portion above the front property line.~~
5. The required 30-foot front yard may be reduced one foot for each one foot of this yard that is developed as a public use area if:
 - a. Substantially, the entire width of the yard (from north to south property line) is developed as a public use area; and,
 - b. The design of the public use area is specifically approved by the City.

(Does not apply to Public Access Pier; Boardwalk or Public Access Facility; Piers, Docks, Boatlifts and Canopies Serving Detached Dwelling Units; Piers, Docks Boat Lifts and Canopies Serving Detached, Attached or Stacked Dwelling Units, Public Park, Public Utility Uses, Boat Launch; or Water Taxi.)
6. May also be regulated under the Shoreline Master Program, Chapter 83 KZC.

DIRECTIONS: FIRST, read down to find use...THEN, across for REGULATIONS													
Section 30.35	USE REGULATIONS	Required Review Process	MINIMUMS				MAXIMUMS		Landscape Category (See Ch. 95)	Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)	Special Regulations (See also General Regulations)	
			Lot Size	REQUIRED YARDS (See Ch. 115)			Height of Structure						
				Front	Side Property Line	Shoreline Setback							
.010	Detached Dwelling Unit	None	3,600 sq. ft./unit, except 1,800 sq. ft./unit for up to 2 dwelling units if the public access provisions of KZC 83.420 are met.	30' See also Spec. Reg. 2.	5', but 2 side yards must equal at least 15'.	See Chapter 83 KZC.	80%	30' above average building elevation. This provision may not be varied.	E	A	2.0 per unit.	<ol style="list-style-type: none"> 1. No structures, other than moorage structures or public access piers, may be waterward of the ordinary high water mark. For the regulations regarding moorages and public access piers, see the specific listings in this zone and Chapter 83 KZC. 2. Chapter 83 KZC contains regulations regarding shoreline setbacks, view corridors, and public pedestrian walkways. 3. Chapter 115 KZC contains regulations regarding home occupations and other accessory uses, facilities and activities associated with this use. <li style="color: red;">4. The required yard of a structure abutting Lake Washington Blvd. must be increased two feet for each one foot that structure exceeds 25 feet above the adjacent centerline of Lake Washington Blvd. 	
.020	Attached or Stacked Dwelling Units	Process I, Chapter 145 KZC.	3,600 sq. ft. per unit	30'	5', but 2 side yards must equal at least 15'.	See Chapter 83 KZC.		30' above average building elevation. See also Spec. Reg. 3.	D			<ol style="list-style-type: none"> 1. No structures, other than moorage structures or public access piers, may be waterward of the ordinary high water mark. For the regulations regarding moorages and public access piers, see the specific listings in this zone and Chapter 83 KZC. 2. Chapter 83 KZC contains regulations regarding shoreline setbacks, view corridors, and public pedestrian walkways. 3. Structure height may be increased to 35 feet above average building elevation if the increase does not impair views of the lake from properties east of Lake Washington Boulevard; and <ol style="list-style-type: none"> a. The increase is offset by a view corridor that is superior to that required by Chapter 83 KZC. 4. Chapter 115 KZC contains regulations regarding home occupations and other accessory uses, facilities and activities associated with this use. 5. Any required yard, other than the front required yard or shoreline setback, may be reduced to zero feet if the side of the dwelling unit is attached to a dwelling unit on an adjoining lot. If one side of a dwelling unit is so attached and the opposite side is not, the side that is not attached shall provide the minimum required yard. 	

.030	Public Access Pier or Boardwalk or Public Access Facility	See Chapter 83 KZC.	None	See Chapter 83 KZC.	-	See Chapter 83 KZC.	See Chapter 83 KZC.	See Chapter 83 KZC.	See KZC 105.25.	1. Refer to Chapter 83 KZC for additional regulations.
------	---	---------------------	------	---------------------	---	---------------------	---------------------	---------------------	-----------------	--

Proposed New KZC Section 60.19 PLA 3C USE ZONE CHART -8-5-2011

User Guide. The charts in KZC 60.22 contain the basic zoning regulations that apply in the PLA 3A zone of the City. Use these charts by reading down the left hand column entitled Use. Once you locate the use in which you are interested, read across to find the regulations that apply to that use.

Section 60.20 - GENERAL REGULATIONS

The following regulations apply to all uses in this zone unless otherwise noted:

1. Refer to Chapter 1 KZC to determine what other provisions of this code may apply to the subject property.
2. If any portion of a structure is adjoining a detached or attached dwelling unit in a low density zone, then either:
 - a. The height of that portion of the structure shall not exceed 15 feet above average building elevation; or
 - b. The maximum horizontal facade shall not exceed 50 feet.
 - c. See KZC [115.30](#), Distance Between Structures/Adjacency to Institutional Use, for further details.
(Does not apply to Detached Dwelling Unit, Attached or Stacked Dwelling Unit and Mini-School or Mini-Day-Care Center uses).
3. Development shall be subject to the following development standards:
 - a. Structures must be clustered and located so that they will not significantly impact slope stability, drainage patterns, erosion or landslide hazards, and steep ravine areas on the subject property or adjacent property.
 - b. Vegetative cover shall be retained to the maximum extent possible to stabilize slopes.
 - c. Pursuant to the requirements of Chapter 85.15.1-4 KZC, the applicant shall submit a geotechnical report prepared by a qualified geotechnical engineer evaluating the potential geologic hazard areas of the subject and adjacent properties to minimize damage to life and property. Specific structural designs and construction techniques to ensure long term stability shall be considered as part of the analysis. The applicant's geotechnical report and recommendations shall be reviewed by a qualified geotechnical engineer selected and retained by the City at the applicant's expense. The applicant shall comply with the performance standards contained in 85.25 KZC and 85.45 KZC.
 - d. The City may require traffic control devices, shared access points, right of way realignment, or limit development if necessary to further reduce traffic impacts.
 - e. Development must ensure that the City has the ability to access and provide necessary emergency services.
4. May also be regulated under the Shoreline Master Program, refer to KZC Chapter 83.

USE ZONE CHARTS Section 60.22

Use: Detached Dwelling Unit

Required Review Process: None

Minimums:

Lot Size: 12,500 sq. ft. See Special Regulation 1, 2, and 3

Required Yards:

Front: 20' See Special Regulation 5

Side: Minimum 5' but 2 sides must equal at least 15'

Rear: 10'

Maximums:

Lot Coverage: 50%.

Height of Structures: 25' above average building elevation.

Landscape Category: E

Sign Category: A

Required Parking: 2.0 per dwelling unit.

Special Regulations:

1. Maximum dwelling units per acre is 6 dwelling units. Not more than one dwelling unit may be on each lot regardless of the size of the lot.
2. Within a subdivision or short plat the minimum lot size is 5,000 sq. ft.
3. Road dedication and vehicular access easements or tracts may not be included in the density calculation or in the minimum lot size per dwelling unit.
4. For lots containing less than 7,200 sq. ft., the Floor Area Ratio (FAR) requirements of KZC Section 115.42 shall apply. The maximum Floor Area Ratio is 50% of the lot size provided that F.A.R. may be increased to 60% if:
 - a. The primary roof form of all structures on the site is peaked with a minimum pitch of four feet vertical to 12 feet horizontal and
 - b. A setback of at least 7.5 feet is provided along each side yard.See KZC 115.42 Floor Area Ratio (FAR) calculation for Detached dwelling Units in Low Density Residential Zones for additional information.
5. On corner lots with two required front yards, one may be reduced to the average of the front yards for the two adjoining properties fronting the same street as the front yard to be reduced. The applicant may select which front yard will be reduced (see Plate 24).
6. Chapter 115 KZC contains regulations regarding home occupations and other accessory uses, facilities and activities associated with this use.

Use: Attached Dwelling Units

Required Review Process: Process I, KZC Chapter 145

Minimums:

Lot Size: See Special Regulation 1, 2 and 3

Required Yards:

Front: 20' See Special Regulation 6

Side: 10' See Special Regulation 7

Rear: 10' See Special Regulation 7

Maximums:

Lot Coverage: 50%.

Height of Structures: 25' above average building elevation.

Landscape Category: E

Sign Category: A

Required Parking: 2.0 per dwelling unit.

Special Regulations:

1. Maximum dwelling units per acre is 6 dwelling units. Not more than one dwelling unit may be on each lot regardless of the size of the lot.
2. Within a subdivision or short plat the minimum lot size is 5,000 sq. ft.
3. Road dedication and vehicular access easements or tracts may not be included in the density calculation or in the minimum lot size per dwelling unit.
4. No more than two units may be attached to each other.
5. For lots containing less than 7,200 sq. ft., the Floor Area Ratio (FAR) requirements of KZC Section 115.42 shall apply. The maximum Floor Area Ratio is 50% of the lot size provided that F.A.R. may be increased to 60% if the primary roof form of all structures on the site is peaked with a minimum pitch of four feet vertical to 12 feet horizontal.
See KZC 115.42 Floor Area Ratio (FAR) calculation for Detached dwelling Units in Low Density Residential Zones for additional information.
6. On corner lots with two required front yards, one may be reduced to the average of the front yards for the two adjoining properties fronting the same street as the front yard to be reduced. The applicant may select which front yard will be reduced (see Plate 24).
7. The side or rear yard may be reduced to zero feet if the side or rear of the dwelling unit is attached to a dwelling unit on an adjoining lot within the short plat or subdivision.
8. Attached dwelling units must be designed to look like a detached single family house using such techniques as limiting the points of entry on each façade, providing pitched roofs and covered porches.
9. Chapter 115 KZC contains regulations regarding home occupations and other accessory uses, facilities and activities associated with this use.

Section:

Use: Church

Required Review Process: Process IIA, Chapter 150 KZC

Minimums:

Lot Size: 12,500 sq. ft.

Required Yards:

Front: 20'

Side: 20' on each side

Rear: 20'

Maximums:

Lot Coverage: 50%.

Height of Structures: 25' above average building elevation. See General Regulations.

Landscape Category: C

Sign Category: B

Required Parking: 1 for every 4 people based on maximum occupancy load of any area of worship. See Special Reg. 1

Special Regulations:

1. No parking is required for day-care or school ancillary to the use.
2. The property must be served by a collector or arterial street.

Section:

Use: School or Daycare Center

Required Review Process: Process IIA, Chapter 150 KZC.

Minimums:

Lot Size: 12,500 sq. ft.

Required Yards:

If this use can accommodate 50 or more students or children, then: 50' front 50' on each side 50' rear

If this use can accommodate 13 to 49 students or children, then: 20' front 20' on each side 20' rear

Maximums:

Lot Coverage: 50%

Height of Structures: 25' above average building elevation. See General Regulations.

Landscape Category: D

Sign Category: B

Required Parking: See KZC 105.25

School Special Regulations:

1. May locate on the subject property only if:
 - a. It will not be materially detrimental to the character of the neighborhood in which it is located.
 - b. Site and building design minimizes adverse impacts on surrounding residential neighborhoods.
 - c. The property is served by a collector or arterial street.
2. A six-foot-high fence along the side and rear property lines is required only along the property lines adjacent to the outside play areas.
3. Hours of operation and maximum number of attendees at one time may be limited to reduce impacts on nearby residential uses.
4. Structured play areas must be setback from all property lines as follows:
 - a. 20 feet if this use can accommodate 50 or more students or children.
 - b. 10 feet if this use can accommodate 13 to 49 students or children.
5. An on-site passenger loading area must be provided. The City shall determine the appropriate size of the loading area on a case-by-case basis, depending on the number of attendees and the extent of the abutting right-of-way improvements. The location of parking and passenger loading areas shall be designed to reduce impacts on nearby residential uses.
6. Electrical signs shall not be permitted.
7. May include accessory living facilities for staff persons.

Section:

Use: Mini School or Mini Day Care Center

Required Review Process: Process I, Chapter 145 KZC.

Minimums:

Lot Size: 12,500 sq. ft.

Required Yards:

Front: 20'

Side: 5' but 2 side yards must equal at least 15'

Rear: 20'

Maximums:

Lot Coverage: 50%.

Height of Structures: 25' above average building elevation.

Landscape Category: E

Sign Category: B

Required Parking: See KZC 105.25

Special Regulations:

1. May locate on the subject property if:
 - a. It will not be materially detrimental to the character of the neighborhood in which it is located.
 - b. Site design must minimize adverse impacts on surrounding residential neighborhoods.
2. A six-foot-high fence is required along the property lines adjacent to the outside play areas.
3. Hours of operation and maximum number of attendees may be limited by the City to reduce impacts on nearby residential uses.
4. Structured play areas must be setback from all property lines by five feet.
5. An on-site passenger loading area may be required depending on the number of attendees and the extent of the abutting right-of-way improvements.
6. The location of parking and passenger loading areas shall be designated to reduce impacts on nearby residential uses.
7. Electrical signs shall not be permitted. Size of signs may be limited to be compatible with nearby residential uses.

8. May include accessory living facilities for staff persons.

Section:

Use: Public Utility

Required Review Process: Process IIA, Chapter 150 KZC

Minimums:

Lot Size: None

Required Yards:

Front: 20'

Side: 20' on each side

Rear: 20'

Maximums:

Lot Coverage: 50%.

Height of Structures: 25' above average building elevation. See General Regulations

Landscape Category: A See Special Regulation 2

Sign Category: B

Required Parking: See KZC 105.25

Special Regulations:

1. Site and building design minimizes adverse impacts on surrounding residential neighborhoods.
 2. Landscape Category A or B may be required depending on the type of use on the subject property and the impacts associated with the use on the nearby uses.
-

Section:

Use: Government Facility Community Facility

Required Review Process: Process IIA, Chapter 150, KZC

Minimums:

Lot Size: None.

Required Yards:

Front: 20'

Side: 10' on each side

Rear: 10'

Maximums:

Lot Coverage: 50%.

Height of Structures: 25' above average building elevation. See General Regulations

Landscape Category: C See special regulation 2

Sign Category: B

Required Parking: See KZC 105.25

Special Regulations:

1. Site and building design minimizes adverse impacts on surrounding residential neighborhoods.
2. Landscape Category A or B may be required depending on the type of use on the subject property and the impacts associated with the use on the nearby uses.

Section:

Use: Public Park

Development standards will be determined on a case-by-case basis. See chapter 49 KZC for required review process.

60.24 User Guide. Proposed Amendments

The charts in KZC 60.27 contain the basic zoning regulations that apply in Planned Area 3B, including sub-zones. Use these charts by reading down the left hand column entitled Use. Once you locate the use in which you are interested, read across to find the regulations that apply to that use.

Section 60.25

Zone
PLA3B

Section 60.25 – GENERAL REGULATIONS

The following regulations apply to all uses in this zone unless otherwise noted:

1. Refer to Chapter 1 KZC to determine what other provisions of this code may apply to the subject property.
2. Developments creating four or more new dwelling units shall provide at least 10 percent of the units as affordable housing units as defined in Chapter 5 KZC. Two additional units may be constructed for each affordable housing unit provided. In such cases, the minimum lot size listed in the Use Regulations shall be used to establish the base number of units allowed on the site, but shall not limit the size of individual lots. See Chapter 112 KZC for additional affordable housing incentives and requirements.
3. The required 30-foot front yard may be reduced, subject to all of the following conditions:
 - a. The existing primary structure does not conform to the minimum shoreline setback standard;
 - b. The proposed complete replacement or replacement of portion of the existing primary structure comply with the minimum required shoreline setback established under the provisions of Chapter 83 KZC, or as otherwise approved under the shoreline setback reduction provisions established in KZC 83.380; and
 - c. The front yard for the complete replacement or the portion of replacement may be reduced one foot for each one foot of the shoreline setback that is increased in dimension from the setback of the existing nonconforming primary structure; ~~provided, that subsection (3)(d) of this section is met; and~~
~~d.—Within the front yard, each portion of the primary structure that is replaced is set back from the front property line by a distance greater than or equal to the maximum height of that portion above the front property line.~~
4. The required 30-foot front yard may be reduced one foot for each foot of this yard that is developed as a public use area if:
 - ~~a.—Within 30 feet of the front property line, each portion of a structure is set back from the front property line by a distance greater than or equal to the height of that portion above the front property line; and~~
 - ~~ba.~~ Substantially, the entire width of this yard (from north to south property lines) is developed as a public use area; and
 - ~~eb.~~ The design of the public use area is specifically approved by the City.
(Does not apply to Public Access Pier, Boardwalk, or Public Access Facility; Boat Launch; Piers, Docks, Boat Lifts and Canopies Serving Detached Dwelling Unit; Piers, Decks, Boat Lifts and Canopies Serving Detached, Attached or Stacked Dwelling Units; Public Park; Public Utility uses; or Water Taxi).
5. A view corridor must be maintained across 30 percent of the average parcel width. Refer to Chapter 83 KZC for additional details.
6. May not use lands waterward of the high water mark to determine lot size or to calculate allowable density.
7. May also be regulated under the Shoreline Master Program. Refer to Chapter 83 KZC.

Chapter 92 – DESIGN REGULATIONS-Proposed Amendments

92.05 Introduction

1. General
2. Applicability
3. Design Review Procedures
4. Relationship to Other Regulations
5. Dedication
6. Design Districts in Rose Hill Business District
7. Design Districts in the Totem Lake Neighborhood

92.10 Site Design, Building Placement and Pedestrian-Oriented Facades

1. Building Placement in JBD
2. Pedestrian-Oriented Facades Defined for RHBD and TLN
3. Building Placement In RHBD, ~~and~~ TLN, and YBD
4. Multi-Story Buildings on Sites Adjacent to a Low Density Zone in RHBD and TLN
5. Multifamily Buildings Located in TLN
6. Building Location at Street Corners in the RHBD and TLN Zones
7. Building Location at Street Corners in CBD

92.15 Pedestrian-Oriented Improvements on or Adjacent to the Subject Property

1. All Zones – Pedestrian Oriented Space and Plazas in Parking Areas
2. Pedestrian-Oriented Space and Plazas in TC, CBD, NRHBD, RHBD and TLN Zones
3. Blank Wall Treatment
4. Parking Garages

92.30 Architectural and Human Scale

1. Techniques To Moderate Bulk and Mass in the CBD
2. Horizontal Definition in All Zones
3. Techniques To Moderate Bulk and Mass in the RHBD and TLN Zones
4. Techniques To Achieve Architectural Scale in All Zones
5. Techniques To Achieve Architectural Scale in the RHBD and the TLN Zones
6. Achieving Human Scale in All Zones

92.35 Building Material, Color and Detail

1. Required Elements
2. Prohibited Materials – All Zones
3. Metal Siding – All Zones
4. Concrete Block – All Zones
5. Awnings – All Zones
6. Covering of Existing Facades – All Zones
7. Building Cornerstone or Plaque – All Zones
8. Required On-Site Improvements – All Zones

92.05 Introduction

1. General – This chapter establishes the design regulations that apply to development in Design Districts including the Central Business District (CBD), Market Street Corridor (MSC), Juanita Business District (JBD), Rose Hill Business District (RHBD), Totem Lake Neighborhood (TLN), North Rose Hill Business District (NRHBD), Totem Center (TC), Yarrow Bay Business District (YBD) and in areas indicated on the use zone charts for PLA 5C.

Special provisions that apply to a particular Design District are noted in the section headings of the chapter.

92.10 Site Design, Building Placement and Pedestrian-Oriented Facades

This section contains regulations which establish the location of a building on the site in relationship to the adjacent sidewalk, pedestrian pathway or pedestrian-oriented elements on or adjacent to the subject property.

1. Building Placement in JBD – All buildings must front on a right-of-way or through-block pathway (see Plate 34).
2. Pedestrian-Oriented Facades Defined for RHBD and TLN – To meet the definition of a pedestrian-oriented facade (see Figure 92.10.A):
 - a. The building's primary entrance must be located on this facade and facing the street. For purposes of this chapter, "primary entrance" shall be defined as the primary or principle pedestrian entrance of all buildings along that street. The primary entrance is the entrance designed for access by pedestrians from the sidewalk. This is the principal architectural entrance even though customers or residents may use a secondary entrance associated with a garage, parking area, driveway or other vehicular use area more frequently.
 - b. Transparent windows and/or doors must occupy at least 75 percent of the facade area between two and seven feet above the sidewalk.
 - c. Weather protection feature(s) at least five feet wide must be provided over at least 75 percent of the facade. This could include awnings, canopies, marquees, or other permitted treatments that provide functional weather protection.
3. Building Placement In RHBD, ~~and~~ TLN and YBD
 - a. ~~-~~Building Location Featuring Pedestrian-Oriented Facades in RHBD, ~~and~~ TLN and YBD Zones – Buildings may be located adjacent to the sidewalk of any street (except west of 124th Avenue NE in the TLN) and in YBD (except for Lake Washington Blvd and Northup Way), if they contain a pedestrian-oriented facade along that street frontage pursuant to the standards in subsection (2) of this section. As part of the Design Review process, required yards, setbacks or other development standards may be modified along the street frontage. Buildings not featuring a pedestrian-oriented facade along a street must provide a building setback of at least 10 feet from any public street (except areas used for pedestrian or vehicular access) landscaped with a combination of trees, shrubs, and groundcover per the requirements of supplemental landscape standards of KZC 95.41(2).

92.30 Architectural and Human Scale

6. Achieving Human Scale in All Zones
 - a. General
 - 1) CBD – Except as provided in subsection (6)(a)(3) of this section, the applicant shall use at least two of the elements or techniques listed in subsection (6)(b) of this section in the design and construction of each facade of a building facing a street or public park.
 - 2) JBD, NRHBD, RHBD, MSC, TC, YBD and TLN – Except as provided in subsection (6)(a)(3) of this section, the applicant shall use at least one of the elements or techniques listed in subsection (6)(b) of this section in the design and construction of each facade of a one-story building facing a street or through-block pathway, and at least two of the elements or techniques for a two-story building facing a street or through-block pathway (see Plate 34 in Chapter 180 KZC).

Kirkland Zoning Code Amendments:

100.50 Designated Corridors

1. General – KZC [100.45](#) contains limitations on sign area along the following designated corridors:
 - a. Market Street between Central Way and N.E. 106th Street.
 - b. State Street, between N.E. 68th Street and 2nd Avenue South.
 - c. Lake Washington Boulevard and Lake Street South between N.E. 38th Street and 3rd Avenue South.
 - d. Lakeview Drive and NE 60th Street.
2. Electrical Signs Prohibited – Electrical signs shall not be located along designated corridors.

100.52 CBD and JBD – Certain Signs Prohibited

Cabinet signs shall be prohibited in all Central Business District (Chapter [50](#) KZC), ~~and~~ Juanita Business District zones (Chapter [52](#) KZC) and Yarrow Bay Business District (Chapter 56, KZC).

105.58 Location of Parking Areas Specific to Design Districts

If the subject property is located in a Design District, the applicant shall locate parking areas on the subject property according to the following requirements:

1. Location of Parking Areas in the CBD, TC (TL 1, TL 2, TL 3) Zones
 - a. Parking areas shall not be located between a pedestrian-oriented street and a building unless specified in a Conceptual Master Plan in TL 2. (See Plate 34 in Chapter 180 KZC and Chapters 92 and 110 KZC for additional requirements regarding pedestrian-oriented streets).
 - b. On all other streets, parking lots shall not be located between the street and the building on the subject property unless no other feasible alternative exists.
2. Location of Parking Areas in the JBD 2, and the NRHBD and YBD ~~X~~ Zones – Parking areas shall not be located between the street and the building unless no other feasible alternative exists on the subject property.
3. Location of Parking Areas in the MSC Zones – Parking areas in the MSC zones shall not be located between the street and the building unless the Planning Official determines that the proposed landscape design provides superior visual screening of the parking area.
4. Location of Parking Areas in Certain TLN and RHBD Zones – Parking areas and vehicular access may not occupy more than 50 percent of the street frontage in the following zones (see Figure 105.58.A):
 - a. TL 4, only properties fronting on 120th Avenue NE;
 - b. TL 5;
 - c. TL 6A, only properties fronting on 124th Avenue NE. Auto dealers in this zone are exempt from this requirement;
 - d. TL 6B, only properties fronting on NE 124th Street;
 - e. TL 10E.

Alternative configurations may be considered through the Design Review process, if the project meets the objectives of the KMC Design Guidelines for the Totem Lake Neighborhood.

 - f. In the Regional Center (RH 1A, RH 2A, RH 3 and RH 5A zones west of 124th Avenue). For parcels over two acres in size, parking lots and vehicular access areas may not occupy more than 50 percent of the NE 85th Street property frontage (see Figure 105.58.A). Alternative configurations will be considered through the Design Review process, if the project meets the intent of the KMC Design Guidelines for the Rose Hill Business District.

Insert: and attached dwelling unit in PLA 3C

115.42 Floor Area Ratio (F.A.R.) Calculation for Detached Dwelling Units in Low Density Residential Zones

Insert: and attached dwelling units in PLA 3C

1. Gross floor area for purposes of calculating F.A.R. and maximum floor area for detached dwelling units in low density residential zones does not include the following:
 - a. Attic area with less than five (5) feet of ceiling height, as measured between the finished floor and the supporting members for the roof.
 - b. Floor area with a ceiling height less than six (6) feet above finished grade. The ceiling height will be measured to the top of the structural members for the floor above. The finished grade will be measured along the outside perimeter of the building (see Plate 23).
 - c. On lots less than 8,500 square feet, the first 500 square feet of an accessory dwelling unit or garage contained in an accessory structure, when such accessory structure is located more than 20 feet from and behind the main structure (see KZC 115.30 for additional information on the required distance between structures); provided, that the entire area of an accessory structure, for which a building permit was issued prior to March 6, 2007, shall not be included in the gross floor area used to calculate F.A.R. For purposes of this section, "behind" means located behind an imaginary plane drawn at the back of the main structure at the farthest point from, and parallel to, the street or access easement serving the residence.
 - d. On lots greater than or equal to 8,500 square feet, the first 800 square feet of an accessory dwelling unit or garage contained in an accessory structure, when such accessory structure is located more than 20 feet from and behind the main structure (see KZC 115.30 for additional information on the required distance between structures); provided, that the entire area of an accessory structure, for which a building permit was issued prior to March 6, 2007, shall not be included in the gross floor area used to calculate F.A.R.
 - e. Uncovered and covered decks, porches, and walkways.
2. Floor area with a ceiling height greater than 16 feet shall be calculated as follows:
 - a. The first 100 square feet of such floor area, in aggregate, shall be calculated only once toward allowable F.A.R.;
 - b. Floor area in excess of the first 100 square feet shall be calculated at twice the actual floor area toward allowable F.A.R.
3. *This section is not effective within the disapproval jurisdiction of the Houghton Community Council.*

Insert: except for those lots in PLA 3C that are less than 7200 sq. ft. or lots that have less than the minimum lot size created through the small lot provisions of Subdivision 22.28.042.

Zoning Code Amendments to Chapter 142:

142.25 Administrative Design Review (A.D.R.) Process

1. Authority – The Planning Official shall conduct A.D.R in conjunction with a related development permit pursuant to this section.

The Planning Official shall review the A.D.R. application for compliance with the design regulations contained in Chapter [92](#) KZC. In addition, the following guidelines and policies shall be used to interpret how the regulations apply to the subject property:

- a. Design guidelines for pedestrian-oriented business districts, as adopted in KMC 3.30.040.
 - b. Design guidelines for the Rose Hill Business District (RHBD), ~~and~~ the Totem Lake Neighborhood (TLN) and Yarrow Bay Business District (YBD) as adopted in KMC 3.30.040.
 - c. For review of attached or stacked dwelling units within the NE 85th Street Subarea and the Market Street Corridor, Appendix C, Design Principles for Residential Development contained in the Comprehensive Plan.
2. Application – As part of any application for a development permit requiring A.D.R., the applicant shall show compliance with the design regulations in Chapter [92](#) KZC by submitting an A.D.R. application on a form provided by the Planning Department. The application shall include all documents and exhibits listed on the application form, as well as application materials required as a result of a pre-design conference.
 3. Pre-Design Conference – Before applying for A.D.R. approval, the applicant may schedule a pre-design meeting with the Planning Official. The meeting will be scheduled by the Planning Official upon written request by the applicant. The purpose of this meeting is to provide an opportunity for an applicant to discuss the project concept with the Planning Official and for the Planning Official to designate which design regulations apply to the proposed development based primarily on the location and nature of the proposed development.
 4. A.D.R. Approval
 - a. The Planning Official may grant, deny, or conditionally approve the A.D.R. application. The A.D.R. approval or conditional approval will become conditions of approval for any related development permit, and no development permit will be issued unless it is consistent with the A.D.R. approval or conditional approval.
 - b. Additions or Modifications to Existing Buildings
 - 1) Applications involving additions or modifications to existing buildings shall comply with the design regulations of Chapter [92](#) KZC to the extent feasible depending on the scope of the project. The Planning Official may waive compliance with a particular design regulation if the applicant demonstrates that it is not feasible given the existing development and scope of the project.
 - 2) The Planning Official may waive the A.D.R. process for applications involving additions or modifications to existing buildings if the design regulations are not applicable to the proposed development activity.

5. Lapse of Approval – The lapse of approval for the A.D.R. decision shall be tied to the development permit and all conditions of the A.D.R. approval shall be included in the conditions of approval granted for that development permit.
6. Design departure and minor variations may be requested pursuant to KZC [142.37](#).

142.37 Design Departure and Minor Variations

1. General – This section provides a mechanism for obtaining approval to depart from strict adherence to the design regulations or for requesting minor variations from requirements in the following zones:
 - a. In the CBD [and YBD](#): minimum required yards; and
 - b. In the Totem Center: minimum required yards, floor plate maximums and building separation requirements; and
 - c. In the RHBD and the TLN: minimum required yards, landscape buffer and horizontal facade requirements; and
 - d. In the MSC 1 and MSC 4 zones of the Market Street Corridor: minimum required front yards and horizontal facade requirements; and
 - e. In the MSC 2 zone of the Market Street Corridor: height (up to an additional five (5) feet), minimum required front yards and horizontal facade requirements; and
 - f. In the MSC 3 zone of the Market Street Corridor: horizontal facade requirements.

This section does not apply when a design regulation permits the applicant to propose an alternate method for complying with it or the use zone chart allows the applicant to request a reduced setback administratively.

2. Process – If a design departure or minor variation is requested, the D.R. decision, including the design departure or minor variation, will be reviewed and decided upon using the D.B.R. process.
3. Application Information – The applicant shall submit a complete application on the form provided by the Planning Department, along with all information listed on that form, including a written response to the criteria in subsection (4) of this section.
4. Criteria – The Design Review Board may grant a design departure or minor variation only if it finds that all of the following requirements are met:
 - a. The request results in superior design and fulfills the policy basis for the applicable design regulations and design guidelines;
 - b. The departure will not have any substantial detrimental effect on nearby properties and the City or the neighborhood.

Plate 34L: PEDESTRIAN CIRCULATION IN YBD

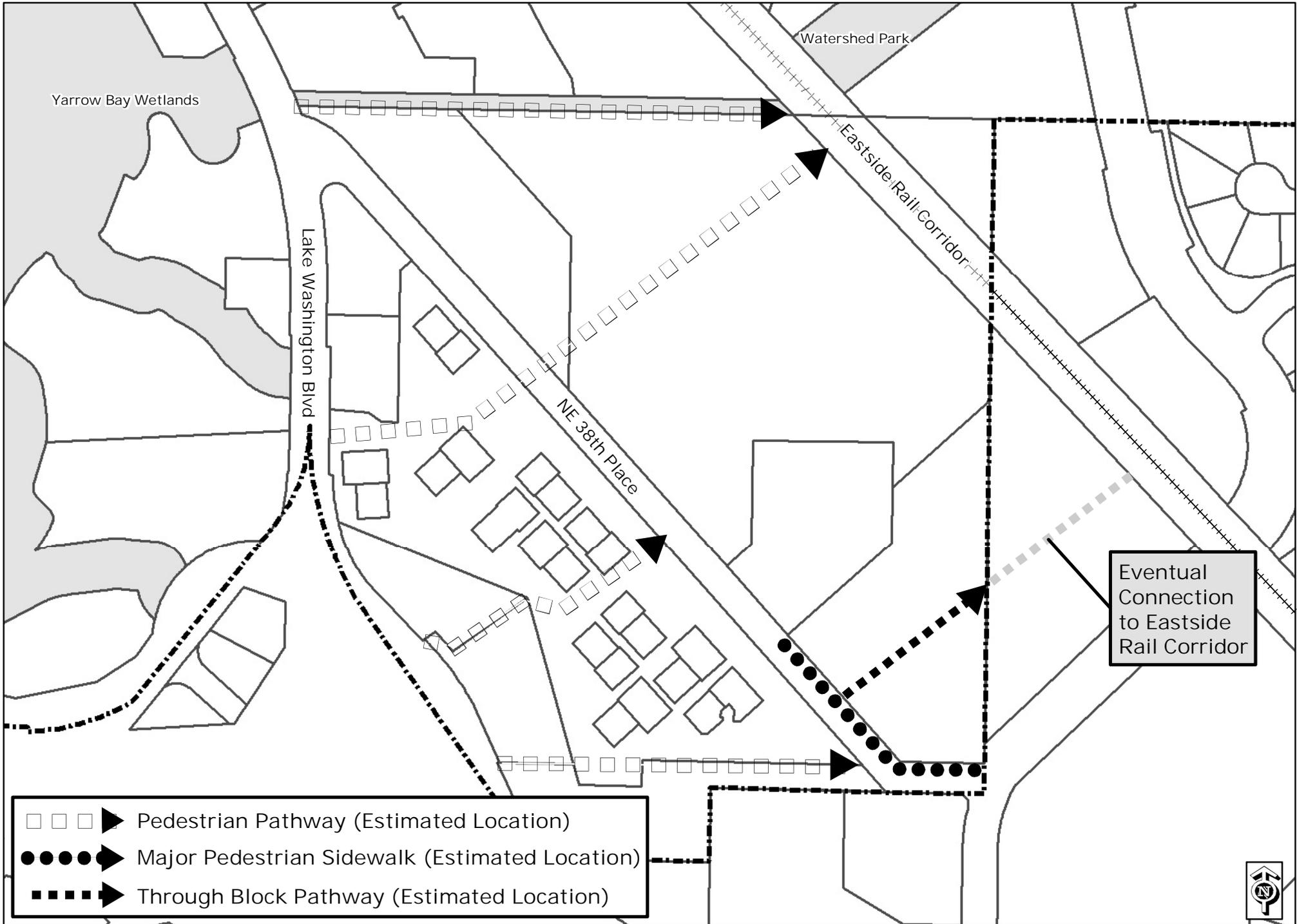
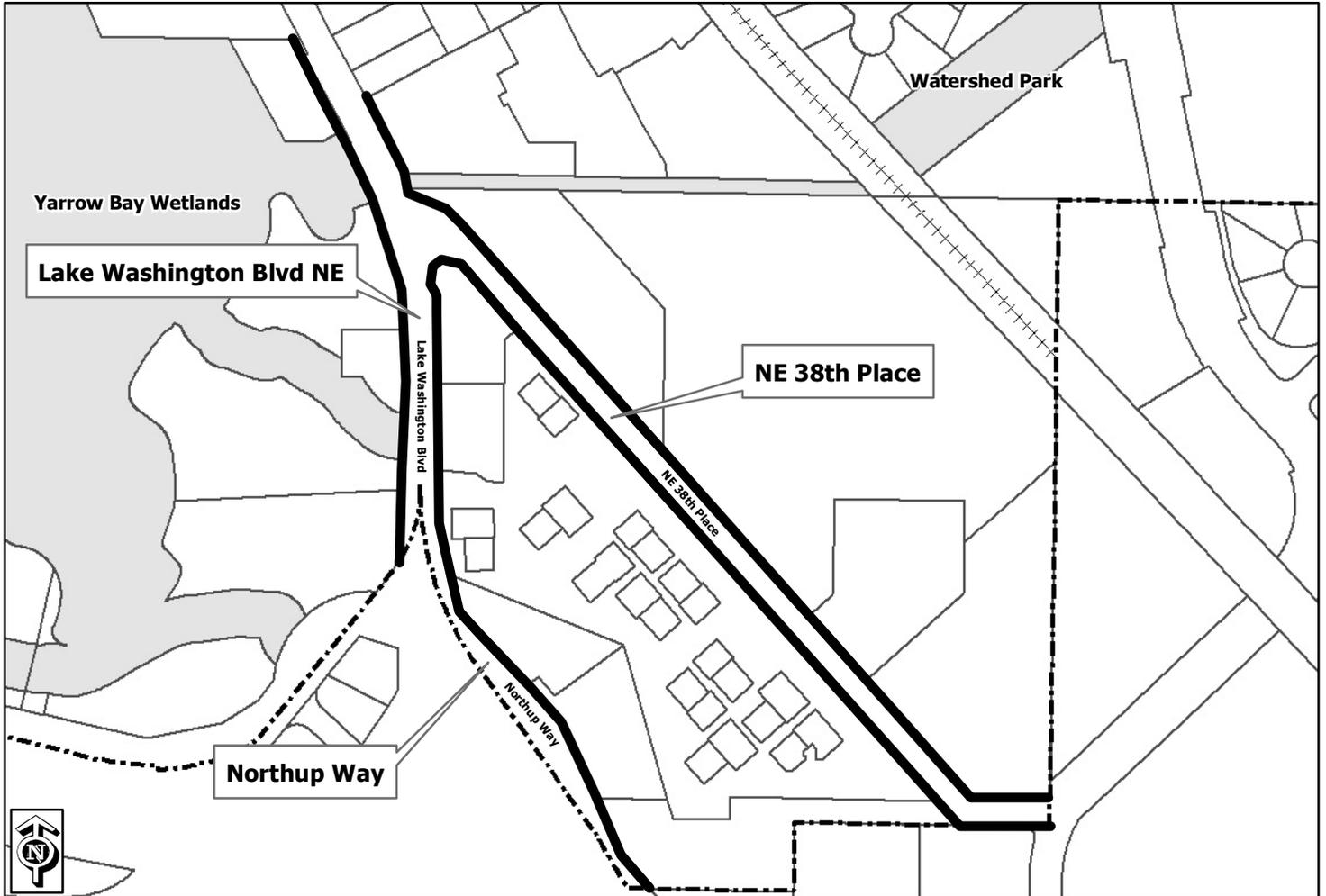


Plate 34M: Street Improvements in YBD



Required street improvements for NE 38th Pl, Lake Washington Blvd NE, and Northup Way in the Yarrow Bay Business District:

NE 38th Pl:

- 70-80 feet of public right of way**
- 10 foot wide sidewalks with street trees in landscape strip or tree grates on both sides of street and decorative street light fixtures**
- On-street parking strongly encouraged along street**

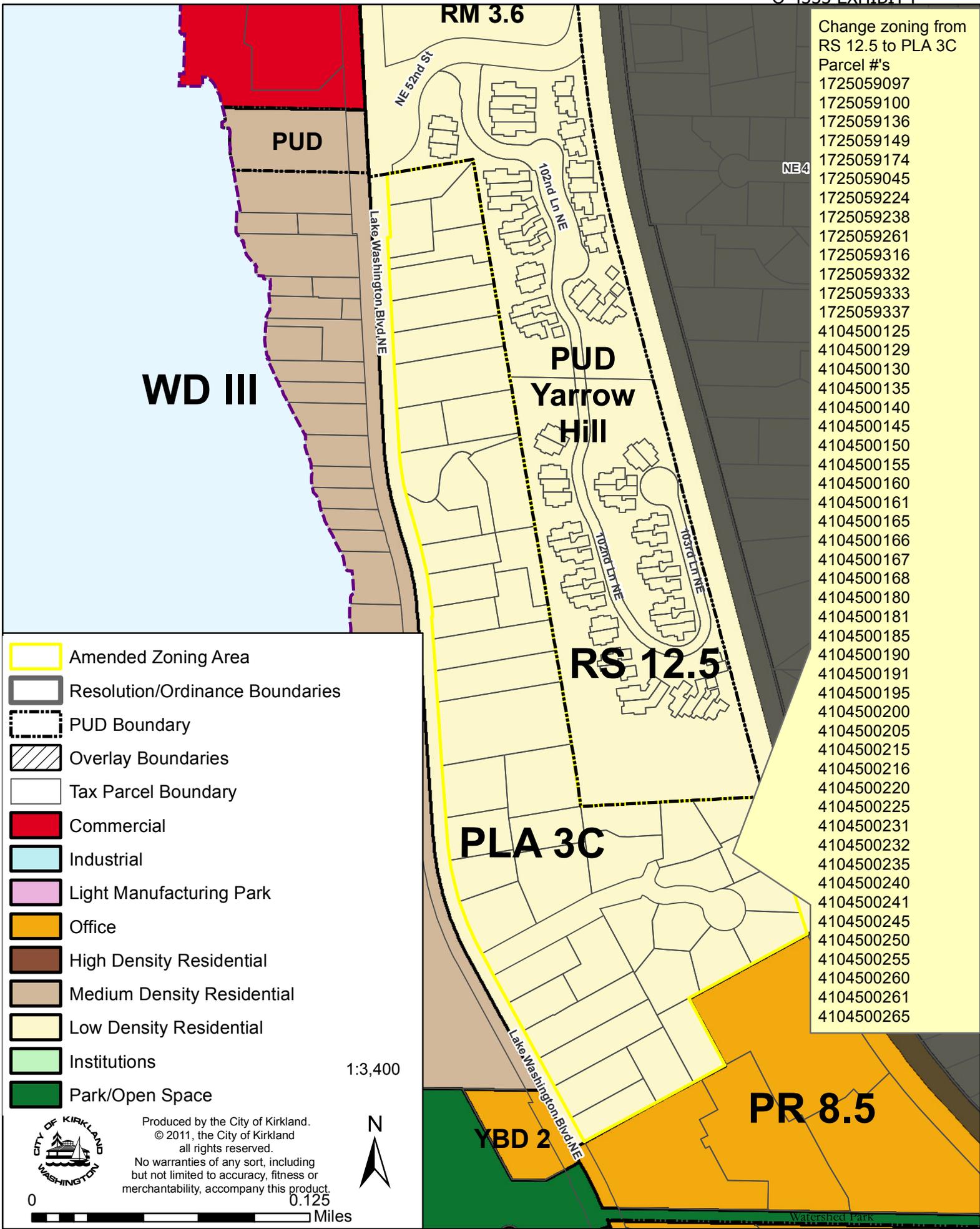
Lake Washington Blvd NE:

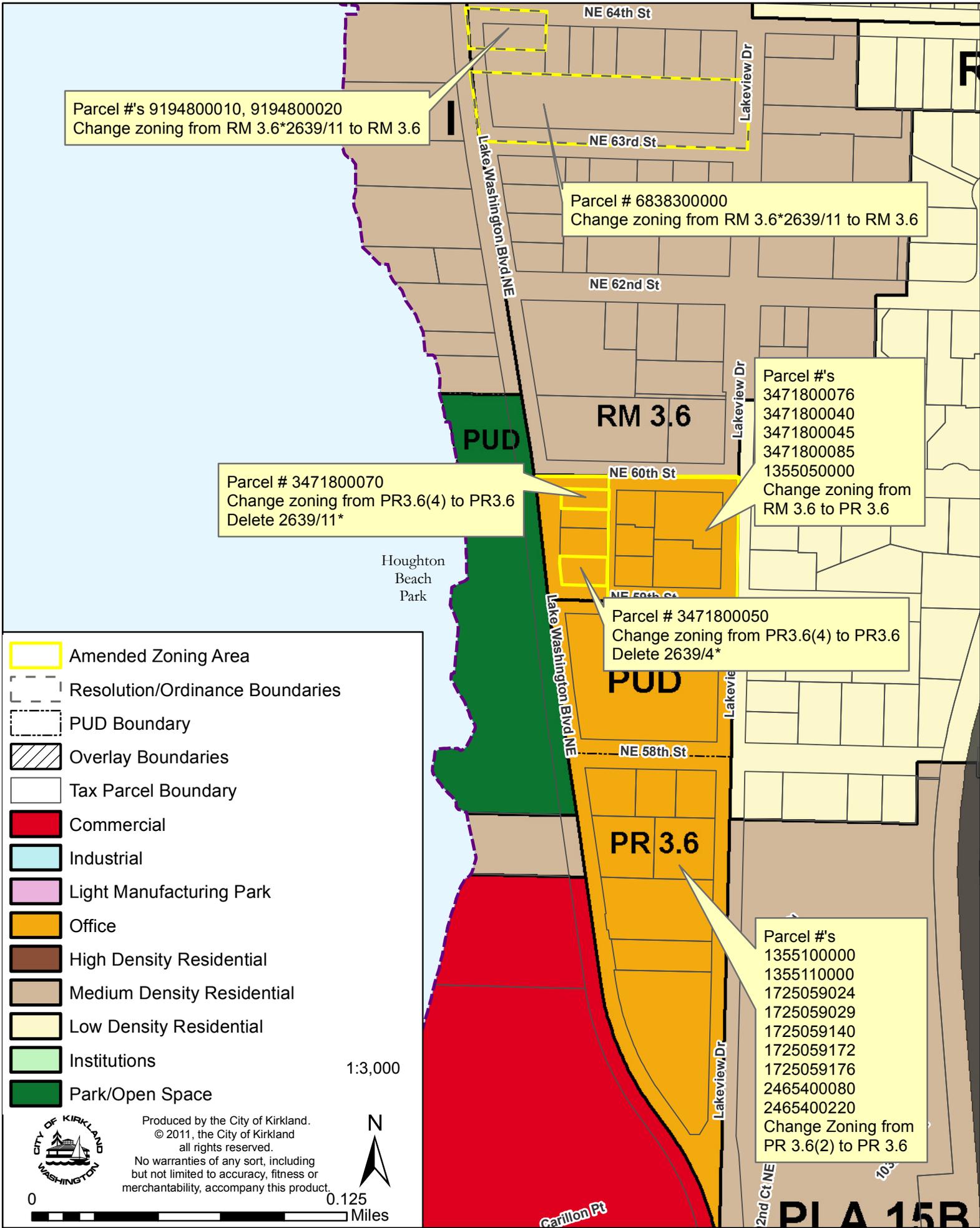
- 60 – 80 feet of public right of way**
- 10 foot wide sidewalks with street trees in landscape strip or tree grates on both sides of street and decorative street light fixtures**

Northup Way:

- 10 foot wide sidewalks with street trees in landscape strip or tree grates on both sides of street and decorative street light fixtures.**

Note: The precise right of way specifications may vary and shall be determined by the Public Works Director





Parcel #'s 9194800010, 9194800020
Change zoning from RM 3.6*2639/11 to RM 3.6

Parcel # 6838300000
Change zoning from RM 3.6*2639/11 to RM 3.6

Parcel # 3471800070
Change zoning from PR3.6(4) to PR3.6
Delete 2639/11*

Parcel #'s
3471800076
3471800040
3471800045
3471800085
1355050000
Change zoning from
RM 3.6 to PR 3.6

Parcel # 3471800050
Change zoning from PR3.6(4) to PR3.6
Delete 2639/4*

Parcel #'s
1355100000
1355110000
1725059024
1725059029
1725059140
1725059172
1725059176
2465400080
2465400220
Change Zoning from
PR 3.6(2) to PR 3.6

- Amended Zoning Area
- Resolution/Ordinance Boundaries
- PUD Boundary
- Overlay Boundaries
- Tax Parcel Boundary
- Commercial
- Industrial
- Light Manufacturing Park
- Office
- High Density Residential
- Medium Density Residential
- Low Density Residential
- Institutions
- Park/Open Space

1:3,000

Produced by the City of Kirkland.
© 2011, the City of Kirkland
all rights reserved.
No warranties of any sort, including
but not limited to accuracy, fitness or
merchantability, accompany this product.

0 0.125 Miles

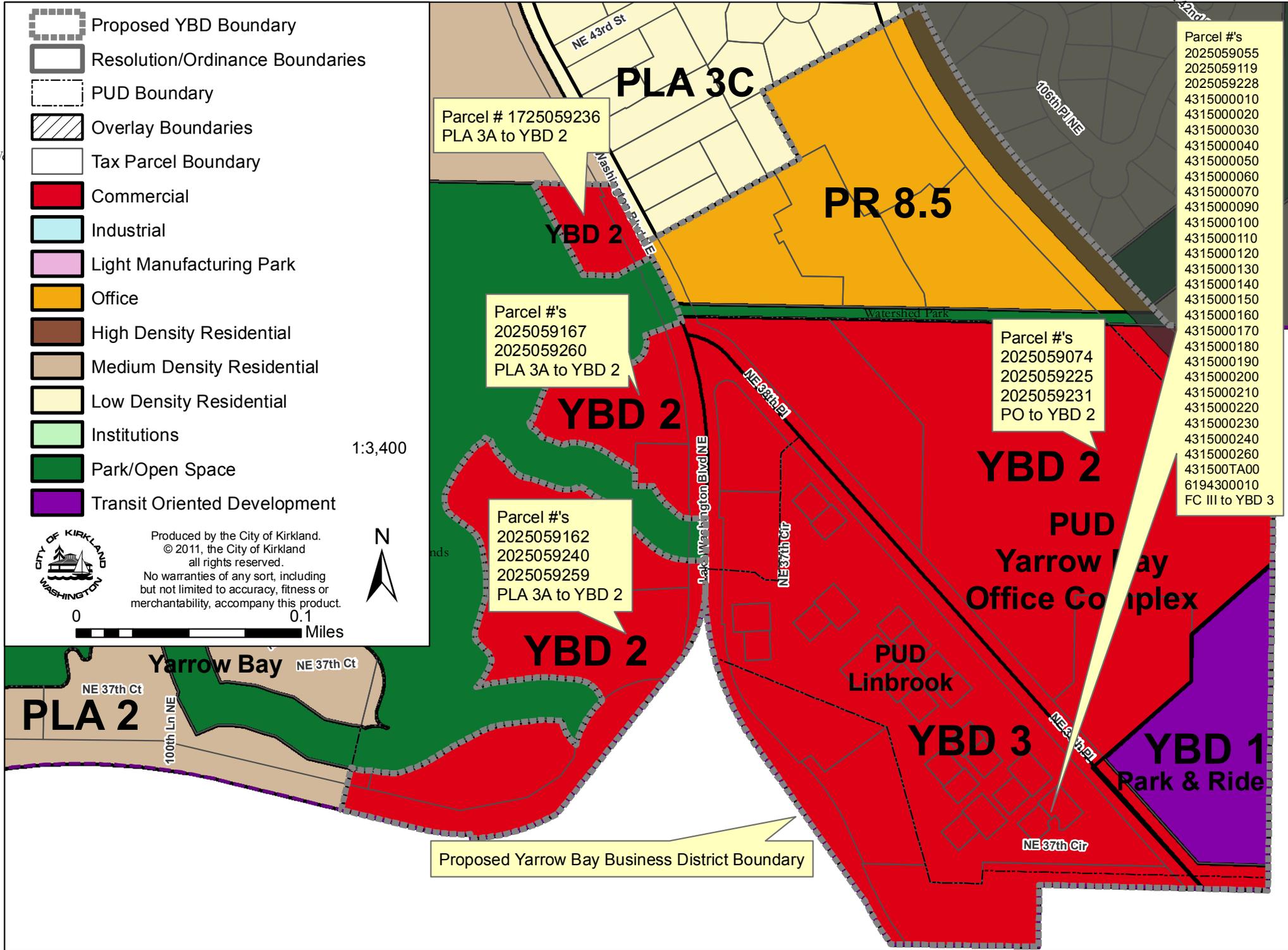
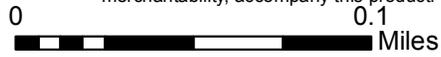
PLA 15B

-  Proposed YBD Boundary
-  Resolution/Ordinance Boundaries
-  PUD Boundary
-  Overlay Boundaries
-  Tax Parcel Boundary
-  Commercial
-  Industrial
-  Light Manufacturing Park
-  Office
-  High Density Residential
-  Medium Density Residential
-  Low Density Residential
-  Institutions
-  Park/Open Space
-  Transit Oriented Development

1:3,400



Produced by the City of Kirkland.
 © 2011, the City of Kirkland
 all rights reserved.
 No warranties of any sort, including
 but not limited to accuracy, fitness or
 merchantability, accompany this product.



Parcel # 1725059236
 PLA 3A to YBD 2

Parcel #'s
 2025059167
 2025059260
 PLA 3A to YBD 2

Parcel #'s
 2025059162
 2025059240
 2025059259
 PLA 3A to YBD 2

Parcel #'s
 2025059074
 2025059225
 2025059231
 PO to YBD 2

Parcel #'s
 2025059055
 2025059119
 2025059228
 4315000010
 4315000020
 4315000030
 4315000040
 4315000050
 4315000060
 4315000070
 4315000090
 4315000100
 4315000110
 4315000120
 4315000130
 4315000140
 4315000150
 4315000160
 4315000170
 4315000180
 4315000190
 4315000200
 4315000210
 4315000220
 4315000230
 4315000240
 4315000260
 431500TA00
 6194300010
 FC III to YBD 3

Proposed Yarrow Bay Business District Boundary

PUBLICATION SUMMARY
OF ORDINANCE NO. O-4333

AN ORDINANCE OF THE CITY OF KIRKLAND RELATING TO COMPREHENSIVE PLANNING AND LAND USE AND AMENDING THE KIRKLAND ZONING CODE (TITLE 23 OF THE KIRKLAND MUNICIPAL CODE), AND THE KIRKLAND ZONING MAP, ORDINANCE 3710 AS AMENDED TO IMPLEMENT THE LAKEVIEW NEIGHBORHOOD PLAN UPDATE AND APPROVING A SUMMARY FOR PUBLICATION, FILE NO ZON07-00032.

SECTION 1. Amends the following specific portions of the Kirkland Zoning Code:

- A. Amend Table of Contents;
- B. Amend Chapter 5. Definitions;
- C. Amend Chapter 10. Legal Effect/Applicability;
- D. Amend Chapter 25. Professional Office Residential (PR) and Professional Office Residential A (PRA) Zones;
- E. Amend Chapter 30. Waterfront District (WD) Zones;
- F. Repeal Chapter 35. Freeway Commercial (FCIII) Zone;
- G. Add new section to Chapter 56. Yarrow Bay Business District Subareas YBD 2 and YBD 3;
- H. Chapter 60. Planned Areas:
 - a. Amend PLA 2 Use Zone Chart;
 - b. Delete PLA 3A Use Zone Chart;
 - c. Add new Sections 60.19 – 60.22 PLA 3C Use Zone Chart;
 - d. Text amendments to 60.25 PLA 3B;
- I. Add Yarrow Bay Business District (YBD) Design Regulations to Chapter 92;
- J. Amend Chapter 100. Signs;
- K. Amend Chapter 105. Parking;
- L. Amend Chapter 115. Miscellaneous;
- M. Amend Chapter 142. Design Review;
- N. Amend Chapter 180 Plates.

SECTION 2. Amends the Kirkland Zoning Map as set forth in Exhibit P, Exhibit Q and Exhibit R.

SECTION 3. Provides a severability clause for the ordinance.

SECTION 4. Authorizes publication of the ordinance by summary, which summary is approved by the City Council pursuant to Kirkland Municipal Code 1.08.017 and establishes the effective date as January 1, 2012, which will be more than five days after publication of summary.

SECTION 5. Establishes certification by City Clerk and notification of King County Department of Assessments.

The full text of this Ordinance will be mailed without charge to any person upon request made to the City Clerk for the City of Kirkland. The Ordinance was passed by the Kirkland City Council at its meeting on the ____ day of _____, 2011.

I certify that the foregoing is a summary of Ordinance _____ approved by the Kirkland City Council for summary publication.

City Clerk