



## **CITY OF KIRKLAND**

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### **MEMORANDUM**

**To:** Kurt Triplett, City Manager

**From:** Dorian Collins, AICP, Senior Planner  
Paul Stewart, AICP, Deputy Director  
Eric Shields AICP, Director

**Date:** June 1, 2011

**Subject:** CITY COUNCIL STUDY SESSION ON PROPOSED AMENDMENTS FOR THE SOUTH KIRKLAND PARK AND RIDE (FILE ZON10-00014)

### **RECOMMENDATIONS**

- ❖ Receive an overview of the recommendations from staff, the Planning Commission and the Houghton Community Council regarding map and code amendments related to the South Kirkland Park and Ride (see Exhibit 1). Attachments 1-4 to Exhibit 1 contain the proposed amendments.
- ❖ Direct staff to provide any necessary revisions to the amendments to be brought back for Council consideration. If no changes are needed, Council may take action during its regular meeting on June 7<sup>th</sup>. If changes are required, Council action may occur on June 21<sup>st</sup>.

### **PLANNING COMMISSION (PC) AND HOUGHTON COMMUNITY COUNCIL (HCC) RECOMMENDATIONS AND KEY ISSUES**

#### **PC and HCC Recommendation:**

Following a series of public workshops, study sessions and a joint public hearing, the Planning Commission on April 14 and the Houghton Community Council on April 25<sup>th</sup> both unanimously recommended approval of the amendments related to the South Kirkland Park and Ride property. However, the HCC added a few recommendations that the PC did not have a chance to review. Therefore, the different recommendations do not necessarily indicate a conflict in the recommendations from the two bodies. The PC and HCC recommendations are discussed further on page 8 of this memo as well as in Exhibit 1. The recommended amendments include:

- A change to the Zoning Map to rezone the South Kirkland Park and Ride property (Exhibit 1, Attachment 1)
- Revision to Table of Contents to add Chapter 56 (Exhibit 1, Attachment 2.a)

- Text changes to the Zoning Code, including a new Chapter 56 for the Yarrow Bay Business District, which will include the use zone chart for YBD 1 proposed with these amendments. *Minor changes have been made following the public hearing to provide clarification and consistency with other sections of the Zoning Code* (Exhibit 1, Attachment 2.b)
- Minor changes to Chapter 105 and Chapter 110 related to Design Districts (Exhibit 1, Attachments 2.c-2.d)
- An amendment to Chapter 180 to add a new Plate, number 34L, to establish pedestrian circulation in the YBD 1 zone (Exhibit 1, Attachment 2.e)
- An amendment to the Municipal Code to add reference to new design guidelines (Exhibit 1, Attachment 3).
- New design guidelines for Yarrow Bay Business District 1 (Exhibit 1, Attachment 4)

### **Key Issues:**

*Zoning Change:* The PC and HCC agreed that the property should be rezoned to a new YBD 1 zone which allows for residential use.

*Affordable Housing:* Both the PC and HCC agreed to the minimum affordable housing requirement of at least 20% of the units be available to low and moderate income households.

*Height:* The PC and HCC agreed to a height of 65 feet provided specific design elements to address building mass and useable public space are included in the regulations. The HCC was concerned about the gateway and supported a height limit of 55 feet within a limited specific portion of that corner (a 50' radius from the property line at the intersection).

*Gateway Area:* The HCC in particular was concerned with the gateway area at NE 38<sup>th</sup> and 108<sup>th</sup> Avenue and recommended additional upper story step backs within that defined area. The HCC also recommended a pedestrian accessible and welcoming public space be included as part of the gateway.

*Retail Use:* The HCC and PC agreed that retail use should be allowed without limitations on the size of individual retail square footage.

*Pedestrian Connections:* Both the HCC and PC emphasized the need for good pedestrian connections through the site and to the surrounding area and the Eastside Rail Corridor.

*Public Open Space:* Both the HCC and PC agreed that a significant public space was important to create the desired site environment, and established standards and design guidelines for public open space containing at least 2,500 square feet.

*Design:* The HCC and PC were in concurrence regarding design standards and guidelines. Both groups want to see a very high quality design for the buildings.

*Review Process:* The HCC and PC agreed that proposed buildings should be reviewed by the Design Review Board to ensure that the design guidelines would be met.

### **BACKGROUND**

This section of the memo contains a recap of the history of this effort along with a summary of the public process and recommendations as well as a description of the key issues and

proposed amendments. For quick reference, the topics are noted below along with page numbers for each item:

<b>Section</b>	<b>Topic</b>	<b>Page #</b>
1.	Site Description	3
2.	History	3
2A.	King County Concept	4
2B.	Amendment Process	5
3.	Public Workshops and Meetings	5
4.	Planning Commission Recommendation	8
5.	HCC Recommendation	8
6.	Discussion Issues	9
7.	Description of Proposed Amendments	10
8.	City Council Options and Next Steps	13

## **1. Site Description**

The seven-acre South Kirkland Park and Ride property is owned by King County, and lies within the cities of both Kirkland and Bellevue. The site is located at the southernmost end of the city of Kirkland, at the intersection of NE 38<sup>th</sup> Place and 108<sup>th</sup> Avenue NE (see Attachment 1). The site is used for a 603 space park and ride facility and transit center. Currently, the parking use is over capacity. The Kirkland portion of the property is zoned PO (Professional Office) which does not allow residential use and has a height limit of 30 feet.

For several years, King County has identified the South Kirkland Park & Ride property as a potential site for transit-oriented-development. Transit-oriented development (TOD) is development that maximizes the use of transit and reduces the use of single occupancy vehicles, by increasing the opportunities to walk, bicycle, carpool or take transit. The center of a TOD area has a bus or rail station, generally surrounded by higher-density development. Consolidating housing at major transit facilities is an effective strategy to increase transit ridership and reduce the harmful effects of congestion and greenhouse gas emissions. King County has developed several TOD projects and to date, and all of the completed projects have included a component of affordable housing.

## **2. History**

At its 2007 retreat, the City Council became aware that the King County Department of Transportation had ranked the South Kirkland Park & Ride as its top TOD priority in the County. The site rose in priority for the County after it applied for and received \$6.25 million in federal funding to improve the transit facility and expand the park and ride capacity as part of mixed-use transit-oriented development at the site. The funds were part of a larger Urban Partnership Award associated with improvements to SR 520, and are intended to address an anticipated increase in demand for park and ride spaces with the application of tolling on the SR 520 Bridge. Following the retreat, the City Council established the creation of affordable housing at the Park & Ride as its highest-priority housing strategy, and scheduled the task of studying amendments to the Comprehensive Plan in support of TOD.

In 2008, the City began to work on amendments to the Lakeview Neighborhood Chapter of the Comprehensive Plan to provide support and specific objectives for TOD at the South Kirkland Park and Ride site. The Kirkland City Council adopted the amendments in December of 2008,

and in January 2009, the Houghton Community Council approved the amendments. The amendments establish the following principles for TOD at the site, which provide the framework for subsequent changes to regulations to implement the new objectives:

- Provide for affordable housing
- Ensure high quality site and building design
- Maximize effectiveness of transit-oriented development
- Coordination with the City of Bellevue

The current project to develop amendments to the Zoning Code and Municipal Code has been on the City's Planning Work Program since the Comprehensive Plan amendments were adopted. The task did not move forward immediately, however, since the City of Kirkland and King County were awaiting a decision from the City of Bellevue as to whether Bellevue would also pursue TOD amendments to its Comprehensive Plan in for the Bellevue portion of the park and ride. Eventually, Bellevue elected not to study Comprehensive Plan amendments for the site due to other citywide priorities. Following that decision, King County revised the concept for a project limited to the Kirkland portion of the site.

#### A. King County Preliminary Development Concept

King County's concept for the South Kirkland Park and Ride includes a mixed-use TOD with expanded park and ride capacity. The expansion to park and ride capacity is only feasible if it is developed with mixed-use transit-oriented development (TOD), since the grant funding alone does not provide sufficient resources to construct the parking. Future development would likely involve partnerships between King County Metro and private developers. ARCH (A Regional Coalition for Housing), nonprofit housing providers and/or the King County Housing Authority could also be involved. This approach for mixed use TOD would provide the ability to share parking to keep costs affordable.

King County's preliminary development concept is included as Attachment 2. Building massing studies prepared in response to direction from the Planning Commission and Houghton Community Council at the public hearing are contained in Attachment 3. Under the King County concept, development might include:

- Expansion to park and ride capacity (250 additional park and ride stalls)
- A transit facility that encourages alternative modes of transportation and provides electric vehicle charging stations
- Approximately 200-250 mixed income housing units with associated parking for residents
- A mix of commercial spaces for site residents, transit riders, nearby residents and employees

In mid-2010, representatives from King County advised Kirkland staff that they believed it was possible that the grant could be redirected if code amendments were not adopted by the end of 2010, or early in 2011, allowing a project to move forward in 2011. Without this funding, the expansion of the Park and Ride and associated transit-oriented- development could not occur at this time.

#### B. Process to Amend the Zoning Map, Zoning Code and Municipal Code

In order to preserve the opportunity for King County to retain the grant funding, the Kirkland City Council directed staff to initiate the preparation of the regulations for TOD at the South Kirkland Park and Ride, and confirmed that the existing Comprehensive Plan policy direction was appropriate to guide that process. This action took place at the Council meeting on September 21, 2010.

Since Bellevue chose not to consider changing its Comprehensive Plan on the Bellevue portion of the site, coordination with the City of Bellevue has been a key issue and the City's policy calls for that coordination to occur. This issue was also raised by the Lakeview Neighborhood Advisory Group during its discussions related to the update of the Lakeview Neighborhood plan.

Staff from Kirkland, Bellevue and King County developed a set of "*Principles of Agreement*". These principles were intended to indicate consensus among Kirkland, Bellevue and King County on broad issues where the interests of the three jurisdictions overlap, and to provide assurance that the entities were working together and aware of future plans. The principles outline the mutual objectives for the proposal as it pertains to public outreach, zoning, parking capacity, site development, permitting, timing, impact mitigation and feasibility. The principles have been approved by the Kirkland and Bellevue city councils. The King County Council is expected to take action on the agreement by June, 2011 (see Attachment 4). Attachment 5 is a diagram that illustrates the concurrent decision and implementation processes underway within the jurisdictions of Kirkland, Bellevue and King County.

Since early 2010, the City of Kirkland has had a parallel study underway, involving the update of the remainder of the Lakeview Neighborhood Plan. A Lakeview Advisory Group was formed to provide comments and recommendations on the neighborhood plan update. The City has moved forward concurrently with the development of policies and regulations for the Lakeview Neighborhood Plan, as well as with preparing regulations to implement the policies under study.

The draft plan and amendments contemplate transitioning the Yarrow Bay business district from low rise office and surface parking to a pedestrian-friendly urban village with increased building heights up to 5 stories and a mix of office, commercial uses, housing and services. A concept illustration is shown in Attachment 6.

The plan also calls for improving pedestrian connections between properties and businesses, the Park & Ride facility, and to the Eastside Rail Corridor. Design guidelines are proposed for major new development. Since the South Kirkland Park and Ride lies within the Yarrow Bay Business District (YBD), the development of regulations for the Park and Ride is being coordinated with the Lakeview Neighborhood Plan update underway for the YBD. Key areas of coordination include the design guidelines as well as the standards related to building height, pedestrian connections, and street improvements.

### **3. Public Workshops and Meeting**

On December 13 2010, staff presented the plan for public outreach and schedule for the South Kirkland Park and Ride code amendments at a joint meeting of the Planning Commission and Houghton Community Council. The schedule laid out a series of public workshops, study sessions and a public hearing before the PC and HCC. At the meeting, staff also outlined the approach to the zoning and design standards that would be brought to the HCC and PC following the public workshops. That packet can be accessed here [HERE](#).

January 2011 Public Workshops

Public workshops were held on January 20<sup>th</sup> at Northwest University and on January 25<sup>th</sup> at City Hall. Both workshops had the same format. Approximately 25 people signed in attending the first workshop and 36 people signed in for the second workshop. Several City Council, Houghton Community Council and Planning Commission members attended one or both workshops to observe. Marilynne Beard, Assistant City Manager, facilitated the workshops. Staff representatives from the City, King County and ARCH described the project background and concept. Staff from the City of Bellevue was also in attendance and responded to questions. Following the presentation, the participants (excluding Council and Commission members) met in small groups at tables to discuss a central question regarding the park and ride:

*"How can zoning regulations, design guidelines and project requirements assure that a TOD is as well-integrated as possible with the surrounding neighborhoods?"*

The discussions were facilitated by staff and volunteers from Kirkland and Bellevue, with comments and questions recorded on flip charts. The comments were then transferred to "sticky notes" and pasted up on the wall under four "buckets" or topics. The "buckets" were a way to organize the comments under various processes. Although a key objective of the workshops was to solicit input for the development of zoning regulations and design guidelines for the TOD, the workshops also provided an opportunity for questions and comments that would be more appropriately addressed through other processes, such as King County's Request for Proposals (RFP), the permitting process, or other planning processes underway or planned for the future.

The objective was to gather as much community input as possible and to assign it to one or more of the processes where it could be addressed. The comments were organized by the following processes:

- Zoning Regulations/Design Guidelines
- Request for Proposals (RFP)
- Project Review/Permitting
- Project Management

Another category consisted of comments that didn't easily fit into the four processes noted above. At the end of the exercise, the facilitator for each table reported out to the larger group. Attachment 7 is a summary of the comments from both workshops. Attachment 8 is the "raw" data (individual comments from participants).

A number of common issues emerged from the workshop comments:

- Having an attractive project that incorporates high quality design standards
- Neighborhood compatibility
- Good screening, landscaping and buffering
- Adequate parking for residential use
- Affordable housing –Both support and concerns about affordability, as well as interest in ensuring development also included market rate housing
- Commercial services were generally supported (dry cleaner, grocery, coffee shop)
- Traffic impacts, circulation, access and bike and pedestrian connections
- Project management during and after construction (e.g. manage parking during construction, address safety and security, and ensure the management has a good track record).

The comments were provided to the Houghton Community Council and Planning Commission for consideration at their first study session following the workshops, held on February 10, 2010. The entire packet for that meeting can be viewed [HERE](#).

### February Study Sessions

Two joint study sessions were held by the Planning Commission and Houghton Community Council following the public workshops in January. At the first meeting in February, the PC and HCC received a presentation of the preliminary traffic and parking study completed by the Transpo Group (see [Traffic and Parking Assessment](#)), as well as a presentation on the draft proposed amendments to the Zoning Code, Zoning Map and design guidelines that had been developed based on their direction and the input provided to date. The materials prepared for these sessions can be viewed [HERE](#) and [HERE](#).

### March Public Meeting

One additional public meeting was held in early March to present the draft amendments prior to the public hearing. At the meeting, staff described how comments from the workshops held in January had been addressed by the regulations and guidelines. The workshop facilitator sought to identify areas where those in attendance felt the amendments had adequately addressed the issues expressed at earlier workshops, as well as areas where the amendments might not have fully addressed the issue or where concerns remained. Four citizens who are not members of the City's commissions, King County staff, consultants or reporters attended the meeting.

On many issues, citizens in attendance indicated that the proposed amendments appropriately addressed the input and concerns from earlier meetings. Generally speaking, the attendees indicated the City was on the right track with the amendments. Other issues remained as concerns for some and are listed here:

- ♦ Parking ratios (parking ratios for non-residential use and guests were not included in the draft regulations at the time of the workshop)
- ♦ "Hide and ride"- concerns about parking in the neighborhoods by transit riders
- ♦ Setbacks for parking on grade/parking structure design
- ♦ Parking encroachment from neighboring businesses (use of park and ride spaces by off-site office tenants and others)
- ♦ Building height
  - Whether additional height beyond the 53' included in the draft regulations should be allowed to provide more flexibility for roof treatments
  - Whether additional height might enable smaller building footprints
- ♦ General increase in traffic in the immediate area

The table included in the discussion on page 9 of this memo describes the approaches taken in the regulations and guidelines to address the public comments received.

### March Public Hearing

The Planning Commission and the Houghton Community Council held a joint public hearing on the proposed amendments for the South Kirkland Park and Ride on March 24, 2011. The materials prepared for the public hearing can be viewed [HERE](#). At that time, public testimony was taken. Twelve people spoke at the hearing with approximately ten speaking in favor of the proposed amendments. E-mail comments and letters were included in the hearing packet. The PC and HCC closed the public hearing to further oral testimony, but allowed the hearing to remain open for additional written comments until April 15, 2011. Written comments received

prior to April 15<sup>th</sup> are included as Attachment 9. Comments received after the comment period can be found in Attachment 10.

Following the public hearing, the PC and HCC provided direction to staff regarding additional information they wanted to have available for their separate study sessions in April. The key direction provided to staff was in response to a request from King County for additional height beyond the 53'-55' under consideration for the draft regulations. The two bodies requested that staff provide additional graphic information to be used to understand the request, as well as options for additional design elements that could be included in the regulations as requirements if additional "bonus" height were to be considered.

#### April Study Sessions

During their April study sessions, the Houghton Community Council and Planning Commission developed recommendations on the proposed amendments. The materials prepared for the meetings can be viewed [HERE](#) and [HERE](#).

#### **4. Planning Commission Recommendation**

On April 14<sup>th</sup>, the Planning Commission met to discuss and deliberate on the proposed amendments following the public hearing. The materials for that meeting can be viewed [HERE](#). Following its deliberation, the PC voted unanimously to recommend approval of the amendments to the City Council, with several changes directed at the meeting. The PC recommendation on the proposed amendments is combined with the recommendation from the HCC, included as Exhibit 1 to this memorandum.

#### **5. Houghton Community Council Recommendation**

The Houghton Community Council met on April 25<sup>th</sup> to discuss and deliberate on the proposed amendments following the public hearing. The HCC also reviewed the recommendation of approval from the Planning Commission. The materials for that meeting can be viewed [HERE](#). Exhibit 1 to this memorandum contains the recommendation on the proposed amendments from the HCC and PC. The five Houghton Community Council members in attendance also voted unanimously to recommend approval of the proposed amendments with several changes although there were differences of opinion on the issue of building height. The discussion of the HCC in Exhibit 1 provides more information on this topic.

#### **6. Discussion Issues**

The Houghton Community Council and the Planning Commission have focused on similar issues in their discussions throughout this process. The recommendations from the two bodies are very similar, containing only two relatively minor differences, which are discussed in Exhibit 1. As noted in the recommendation from the PC and HCC, since the PC recommendation occurred on April 14, prior to the April 25<sup>th</sup> HCC meeting, it is not known whether the PC might also have supported the changes suggested by the HCC. So, the absence of these changes in the PC recommendation is not necessarily evidence of a lack of support for the changes.

As the Planning Commission and Houghton Community Council developed the new zoning and design standards for the South Kirkland Park and Ride, they considered many sources of input, including:

- Adopted policies in place for the site in the Lakeview Neighborhood Chapter of the Comprehensive Plan
- Input from the Lakeview and Central Houghton advisory groups through their plan update processes and the emerging vision for the Yarrow Bay Business District
- Comments received from the general public throughout the study process
- The Mutual Objectives and Principles of Agreement approved by both Kirkland and Bellevue (action expected by King County by June)
- Input collected at the two public workshops in January, public meeting in March, and written and oral comments from the public at study meetings and public hearing

The zoning and design standards strive to seek balance between the guiding objectives of the Comprehensive Plan which support the development of the site with TOD, while incorporating standards to address issues and direction provided through the other sources of input. The table below identifies measures included in the proposed regulations and guidelines to address comments received:

<b><i>Issue/Concern</i></b>	<b><i>Regulations/Guidelines</i></b>
<b>Support for affordable housing</b>	Minimum of 20% of units required to be affordable (at least 10% of these at 50% of median income)
<b>Ensure development includes market rate units</b>	Mutual Objectives and Principles agreement states majority will be market rate. City Council to consider amendments to the Multifamily Housing Property Tax Exemption regulations in KMC 5.88 that would allow an eight year tax exemption for the project as long as at least 50% of the units are market rate. This item will be introduced as new business on June 7 <sup>th</sup> and a public hearing will be scheduled for June 21 <sup>st</sup> . The Request for Proposals (RFP) to be issued by King County provides another opportunity to set forth additional parameters that cannot be addressed through zoning, such as the maximum percentage of affordable units provided within the development.
<b>Density</b>	Height and setbacks to control. Estimated density to be 55-68 units/acre (comparable to Plaza on State or Juanita Village)
<b>Affordable units to be "good, livable units"</b>	Regulations ensure affordable units will be comparable to market rate units.
<b>Adequate parking for all uses</b>	Regulations include standards for all uses, and state that TOD required parking must be in addition to those provided as part of the P&R expansion. Parking study may determine final requirement based on proposed mix of uses
<b>Attractive design for parking structure</b>	Minimum 10' setback. Design standards for landscaping and screening, location (underground or behind intervening uses unless not feasible). Architecturally compatible design, high quality materials, avoid appearance of parking structure, special attention to gateway.
<b>Flexible requirements for uses and services (avoid vacancies) &amp; provide ground floor services</b>	Broad range of restaurant, retail, school, cultural and recreational uses allowed, no minimum or maximum size. At least 50% of linear frontage on NE 38 <sup>th</sup> Place must contain these uses.
<b>Supporting services and amenities needed</b>	Non-residential uses required in TOD. Public open space, pedestrian pathways required.
<b>Building height – lower profile, compatible, also flexibility</b>	Regulations allow up to 65'. Design guidelines address building scale and massing – upper story step backs, ped-oriented ground floor elements to define "bottom" and minimize height, vertical and horizontal modulation to reduce perceived mass, varied roof form, separation of taller building elements. Reduced height in gateway ( <i>HCC Rec.</i> ).

<b>Site design</b>	Design guidelines at ground level, buildings planned in context with site improvements, gateway, plazas and open space.
<b>Community compatibility – orientation, character, landscaping</b>	Regulations and design guidelines require building orientation to NE 38 <sup>th</sup> Place, ped connections within site and link to adjacent properties, human scale elements along NE 38 <sup>th</sup> Place, building modulation, incentive for pitched roofs, landscape buffering along driving and parking areas. Additional trees on through-block pathway ( <i>PC Rec.</i> )
<b>Comfortable and safe ped and bike connections. Connections to recreation &amp; open space</b>	Guidelines for well-defined and safe pathways, bike connections – lighting, separation from cars, future connection to Eastside Rail Corridor.
<b>Traffic circulation to/from P&amp;R, congestion at intersections</b>	Traffic study identified potential mitigation measures to be required with project permits– may include turn lanes, signals and other steps. Improved sidewalks on NE 38 <sup>th</sup> Place.*
<b>Safe &amp; secure</b>	RFP criteria to stress project management experience of developer.
<b>Parking management of TOD stalls, overflow parking in neighborhoods</b>	King County RFP selection committee includes local representative. RFP criteria to include parking standards and management of use of stalls as well as criteria for parking study to address shared parking. Overflow parking addressed through expanded capacity, option for Residential Parking Zone.
<b>Parking management during construction, construction &amp; noise mitigation</b>	RFP criteria to address construction phasing, plan for parking during construction. King County to identify temporary P&R location for transit riders during construction.

\*(Note: King County had a detailed traffic analysis conducted by Transpo. The study noted that traffic impacts on Lake Washington Boulevard and 108<sup>th</sup> Ave NE would be minimal. The analysis stated that there would be no significant or noticeable change to traffic on surrounding streets. Daily variation in total traffic would be less than the normal daily variance of +/- 5%. The study did note that site access would need to be carefully addressed at the time of project review. Left turn restrictions, an additional traffic signal at 108<sup>th</sup> Ave NE/NE 38<sup>th</sup> Place and safe pedestrian crossings should be evaluated.)

## 7. Description of Proposed Zoning Code and Municipal Code Amendments

The proposed amendments are included in Attachments 1-4 to this Exhibit 1. *The amendments contain all of the changes recommended by both the Houghton Community Council and the Planning Commission.* Where the recommendations from the PC and the HCC differ, a notation is provided to this effect. The proposed amendments are:

- ♦ Zoning Map: The Zoning Map would be revised to reflect the rezone of the portion of the South Kirkland Park and Ride that lies within the City of Kirkland from PO (Professional Office) to a new YBD 1 (Yarrow Bay Business District, subarea 1) zone. The land use color used on the map would be purple, in keeping with the color used in the Lakeview Neighborhood Chapter, Figure LU-1, of the Comprehensive Plan for this area. The purple color is unique to land zoned for TOD (see Exhibit 1, Attachment 1).
- ♦ Zoning Code Chapter 56 – Section 56.010 – Standards for the Attached or Stacked Dwelling Units listing in the YBD 1 Zone: A new Zoning Code chapter would be created to contain the use zone charts for the Yarrow Bay Business District. The regulations for the YBD 1 subarea would be contained in Section 56.10.

Within this subarea, Section 56.10.010 would contain the regulations for the

"Attached or Stacked Dwelling Units" use, which would be the primary use within the transit-oriented development on the property (see Exhibit 1, Attachment 2.b). Key elements of the proposed regulations for this use are summarized below:

- Project review by the Design Review Board
- Provisions for mixed-use development, containing residential use above one floor of non-residential uses (two floors of commercial allowed along NE 38<sup>th</sup> Place).
- Ground floor uses may include retail, restaurants or taverns, banking, schools (including day-care), government facilities, community facilities, and entertainment, cultural and/or recreational activities.
- Requirement that at least 50% of the linear frontage along NE 38<sup>th</sup> Place contain one or more of the ground floor uses noted above
- Development standards:
  - Front setback for building: 0-5'
  - Front setback for above ground parking structure: 10'
  - Parking standards for all uses, including additional parking for guests
  - Maximum building height of 65' above average building elevation (see HCC recommendation for proposed reduced building height in gateway area)
  - Required upper story setback above second floor
  - Required separation between portions of buildings over two stories and more than 200 feet in length
  - Required pedestrian connection between NE 38<sup>th</sup> Place and the transit center (see PC recommendation for additional criteria for the "Through-block pathway")
  - Required public open space (2,500 square feet)
  - Requirements that buildings and parking structure meet green building standards
  - Residential density to be controlled by building height and setbacks
- Requirement that at least 20% of residential units be affordable to low-to moderate income households
- Requirement that parking stalls to serve the TOD are in addition to those provided as part of the expansion of capacity for the Park and Ride facility.

For uses other than "attached or stacked dwelling units," the YBD 1 zone incorporates permitted uses and regulations from the existing PO zone. Attachment 2.b to Exhibit 1 shows that all existing uses from the PO zone would remain unchanged, except for the following two items which are shown as additions and deletions: (1) addition of the review process of development proposals which would be by the Design Review Board; and (2) the removal of language related to "Height of Structure" where adjoining a low density zone, since this language is not applicable to YBD 1 (no portion of this zone adjoins low density zones).

- ◆ Zoning Code Chapter 180 - Plate 34L: The graphic in Exhibit 1, Attachment 2.e would be added to the plates provided in Chapter 180 of the Zoning Code that establishes the requirements for pedestrian circulation in the City's design districts.

Plate 34L establishes the section of NE 38<sup>th</sup> Place abutting the YBD 1 zone as a Major Pedestrian Sidewalk. This classification would require that an 8-foot wide sidewalk be provided in this area to contribute to the streetscape and pedestrian environment

for the TOD. However, under the Lakeview Neighborhood Plan and code amendment study currently underway, the draft direction of the Planning Commission would require 10 foot wide sidewalks along NE 38<sup>th</sup> Place. If that amendment is later adopted, it would supersede the 8 foot sidewalk requirement.

The graphic also notes the approximate location for a pedestrian pathway to connect NE 38<sup>th</sup> Place through the development and park and ride, and its eventual connection to the Eastside Rail Corridor. Since the eastern segment of this connection is within the city of Bellevue, this objective is indicated with a light dashed line.

- ◆ Section 105.58: This section of the Zoning Code regulates where parking areas may be located on a property located within a design district. Since design review is proposed for the YBD 1 zone, a reference to this zone must be added to Section 105.58 (see Exhibit 1, Attachment 2.c). This section would prohibit surface parking areas within the street and the building unless no other feasible alternative exists on the subject property.
- ◆ Section 110.52: This section provides standards for sidewalks and other public improvements in design districts. The proposed amendment to this code section would add a reference to the Yarrow Bay Business District. Since Plate 34L (discussed above) designates the portion of NE 38<sup>th</sup> Place along the subject property as a Major Pedestrian Sidewalk, the reference in Chapter 110 is necessary to establish the required sidewalk improvements (see Exhibit 1, Attachment 2.d).
- ◆ Municipal Code Section 3.30.040: An amendment to the Design Review Board chapter of the Municipal Code is necessary to add the design guidelines for the Yarrow Bay Business District 1 zone to those used by the DRB to review development permits (see Exhibit 1, Attachment 3).

Attachment 11 to this memorandum contains a Design Guideline Matrix, which provides the proposed design guidelines to address the objectives set forth for TOD at the site in the adopted Comprehensive Plan. The matrix also notes the proposed and existing zoning regulations that address the design objectives. Key elements of the proposed design guidelines regulations for this use are summarized below:

- Ensure high quality building and design
- Address building scale and massing
  - Viewpoints and vantages of site considered in building design
  - Varied and attractive roof forms; flat roofs discouraged
- Ensure pedestrian features and amenities
  - Through-block pathway (*PC recommendation*)
  - Public open space
  - Gateway (*recommended by HCC after PC review*)
- Address the streetscape along NE 38<sup>th</sup> Place
  - Street trees
  - Windows, awnings, multiple storefronts, building orientation
  - Upper story step backs
- Address the gateway at NE 38<sup>th</sup> Place/108<sup>th</sup> Avenue NE
  - Design elements

- Reduced mass (prescribed upper story setbacks and building height *(recommended by HCC after PC review)*)
- Minimize the visual impacts of parking areas and facilities from NE 38<sup>th</sup> Place
  - Screening, intervening uses, design treatments
- Foster the creation of a vibrant and desirable living environment through high quality design, public amenities and open space

Attachment 4 to Exhibit 1 contains only the proposed design guidelines, without the columns that provide additional information about existing regulations that are included in the matrix. Staff recommends that this outline be approved as the design guidelines for YBD 1. Once the Comprehensive Plan and Zoning Code update for the Lakeview Neighborhood is completed, these guidelines will be integrated into the standard format for design guidelines in a comprehensive document that will provide design guidelines for the entire Yarrow Bay Business District.

## **8. Options for the City Council and Next Steps**

### Options

At its meeting on June 7<sup>th</sup>, the City Council has two options to move forward with the proposed code amendments and regulations:

- Approve the ordinances amending the Zoning Map, Zoning Code and Municipal Code during the regular Council meeting that evening, or
- Direct staff to revise the amendments for Council action at its meeting on June 21<sup>st</sup>, if additional information is needed or changes to the proposed regulations and guidelines are desired that require additional time to prepare.

Following action by the City Council on the ordinances at either meeting, the amendments will be presented to the Houghton Community Council for final action on June 27, 2011.

### Next Steps - King County and ARCH

Once zoning regulations are in place for the site, King County will issue a Request for Proposals (RFP) to develop the TOD. King County, as property owner, will select a developer. King County expects to issue the RFP promptly following the adoption of the new regulations, in order to ensure the retention of the Urban Partnership Award funding. King County estimates that completion of the project by 2014 will require project permitting in 2012, with the contract award to occur during summer, 2011.

ARCH (A Regional Coalition for Housing) also hopes to move forward quickly, as applications for typical funding sources will be due earlier than in previous years (late July/early August rather than September). Due to expectations that the State Trust Fund will be funded at a significantly lower amount than in previous years, the State has announced it will have only one funding cycle, during which Trust Fund dollars made available for the biennium budget will be allocated. As a result, if an award is not received this year, the applicant would be required to wait two years before submitting another application. In addition, ARCH expects that State funding will be far more competitive this year due to the limited resources, and that emphasis will be placed on "ready-to-go" projects. To make the application to the State more

competitive, it is important for the zoning to be in place prior to submitting an application, to indicate the ability to proceed with construction in a timely manner.

To ensure the best chances that an application in support of affordable housing at the South Kirkland Park and Ride will be eligible for funding, ARCH is proceeding with a unique approach. ARCH is in the process of seeking State and County funding by applying for project funding rather than waiting for a developer to be selected through the County's RFP process.

## **9. Environmental Review**

An EIS Addendum for the draft code amendments and design guidelines was issued on March 23, 2011 (see [HERE](#)). The Addendum was to the 2004 Draft and Final Comprehensive Plan EIS. No additional significant adverse impacts are anticipated as a result of the proposed amendments, beyond those identified in the EIS for the Comprehensive Plan.

### Exhibits

1. Recommendation from the Planning Commission and Houghton Community Council

### Attachments

1. Site Map
2. King County TOD Concept
3. Massing Study prepared by Mithun Architects, March 2011
4. Mutual Objectives and Principles of Agreement
5. SKPR Draft Decision and Implementation Process Timeline
6. Yarrow Bay Business District Concept Illustration
7. Summary of Public Workshop Comments
8. Workshop Comments – "Raw" Data
9. Public Comments – prior to close of comment period (4/15/11)
10. Public Comments – after close of comment period
11. Design Guideline Matrix

cc: File ZON10--00014  
Planning Commission  
Houghton Community Council  
Arthur Sullivan, ARCH  
Gary Prince, King County Department of Transportation, 201 S. Jackson Street, M/S  
KSC-TR-0815, Seattle, WA 98104-3856



## CITY OF KIRKLAND

Planning and Community Development Department  
 123 Fifth Avenue, Kirkland, WA 98033 425.587-3225  
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### MEMORANDUM

**To:** City Council

**From:** Houghton Community Council  
 Rick Whitney, Chair

Planning Commission  
 Jay Arnold, Chair

**Date:** May 26, 2011

**Subject:** **HOUGHTON COMMUNITY COUNCIL AND PLANNING COMMISSION  
 RECOMMENDATIONS ON PROPOSED AMENDMENTS FOR THE SOUTH  
 KIRKLAND PARK AND RIDE (FILE ZON10-00014)**

### INTRODUCTION

We are pleased to submit, on behalf of the Planning Commission and Houghton Community Council, our recommendation of approval of the proposed amendments to the Zoning Map, Zoning Code, and Municipal Code for the South Kirkland Park and Ride for the consideration of the City Council. We believe the proposed amendments would successfully implement the objectives of the Lakeview Neighborhood Plan.

Our study of these amendments included an extensive public process, where we gathered input from interested citizens at a series of public workshops, study sessions and a public hearing, held on March 24<sup>th</sup>, 2011. Over the past several months, the Planning Commission and Houghton Community Council conducted a series of joint meetings. This enabled us to share ideas and comments and to work through a number of complex issues in a productive and thoughtful manner. We gave careful consideration to all of the input in formulating our recommendations to the City Council. Please see the transmittal memo from staff on this topic for a complete history of the public process and a discussion of key issues we evaluated in the development of the proposed amendments.

### **RECOMMENDATION**

The Planning Commission and Houghton Community Council recommend that the following amendments be approved (see Attachments 1-4):

- A. Kirkland Zoning Map: Rezone of the Kirkland portion of the South Kirkland Park & Ride site from PO (Professional Office) to YBD 1 (Yarrow Bay Business District 1).
- B. Kirkland Zoning Code: Text changes to the Kirkland Zoning Code, establishing the standards for Transit-Oriented Development (TOD) within the new YBD 1 zone. Changes include:
  - a. Table of Contents - Addition of Chapter 56 to contain YBD 1 chart. As subsequent charts are adopted for other districts in the YBD zone, they would be located in this Chapter.

- b. Chapter 56 – New Use Zone Chart for the YBD 1 zone. The chart would contain the new Attached or Stacked Dwelling Units Use, as well as all other uses currently allowed within the existing PO zone.
  - c. Chapter 105: Minor changes related to location of parking areas in YBD 1.
  - d. Chapter 110: Minor changes related to sidewalks and public improvements in YBD.
  - e. Chapter 180: Addition of a new plate, number 34L, to establish pedestrian circulation in YBD 1.
- C. Kirkland Municipal Code – Section 3.30.040: Amendments to add a reference to the design guidelines for the Yarrow Bay Business District 1 zone.
  - D. Design Guidelines for YBD 1 zone: A separate document containing design guidelines unique to the South Kirkland Park and Ride site. The guidelines would be combined with those adopted this summer for the remainder of the Yarrow Bay Business District when that effort is completed.

While there are two differences in our recommendations, as noted in the next section, the Planning Commission (PC) and the Houghton Community Council (HCC) are in agreement on the vast majority of our recommendation on the amendments. Furthermore, two factors should be noted with regard to our recommendations. First, the PC recommendation occurred on April 14, prior to the April 25<sup>th</sup> HCC meeting, so it is not known whether the PC might also have supported the changes related to the gateway area included in the HCC recommendation. Second, only five members of the HCC attended the study session where the recommendation was made. Consequently, when the amendments return to the HCC for final action on June 27, the other two members of the HCC may have opinions that differ from those expressed at the HCC meeting in April. However, both the PC and HCC were unanimous in recommending approval of the proposed amendments.

## **BACKGROUND**

Throughout the study process, both the Planning Commission (PC) and the Houghton Community Council (HCC) sought to understand the areas of concern to citizens, and to develop approaches to address these issues while meeting the objectives set forth for the South Kirkland Park and Ride site in the Comprehensive Plan.

Neighborhood concerns included traffic impacts, lack of nearby retail goods and services, impacts of low income and high density housing, insufficient transit service and connectivity, inadequate parking for the new housing units, limiting future parking capacity at the site by erecting housing structures, negative impacts during the construction phase of the project, lack of a coordinated comprehensive plan for the entire Yarrow Bay Business District - including the Bellevue portion of the Park and Ride site, and how to assure that the project's appearance would be a positive addition to the community - particularly as a gateway to Kirkland. All concerns were given serious consideration. Some were addressed by codes and regulations, some by design standards and guidelines, and in some instances there were legitimate concerns that had to be weighed against the positive benefits and potential of the project.

The PC and HCC held joint study sessions and a joint public hearing on the proposed amendments. The joint meetings provided an efficient process and convenient opportunities for citizens to provide testimony before both bodies. Following the public hearing on the amendments, the PC and HCC held separate study sessions for the purpose of developing recommendations to the City Council.

## Planning Commission and Houghton Community Council Recommendation – Common Elements

As is noted in the transmittal memo from staff on this topic, the PC and the HCC voted unanimously to recommend approval of the amendments to the City Council at our meetings on April 14<sup>th</sup> and April 25<sup>th</sup>. After considerable discussion focusing on the topics of building height, regulations and guidelines to address building massing, public amenities and limitations on the size of retail uses, both bodies elected to recommend that changes be made to the draft amendments. The proposed amendments contained in Attachments 1-4 contain all recommendations from the PC and the HCC.

Key areas of discussion among the HCC and PC during the development of their recommendations included:

- Building Height: *Support for an allowed height limit of 65' above average building elevation.* The PC and HCC concluded that the additional development standards and design guidelines related to building massing that had been suggested by staff following the public hearing would produce a far superior design, and should be required of all transit-oriented development. A number of public amenities would also be required of all development.

Since the Planning Commission (PC) met before the Houghton Community Council (HCC), the Commission did not know how the HCC would respond to the request from King County for additional building height. The PC members discussed that they would also support an alternative approach, if preferred by the HCC, which would provide a base height of 55', with a bonus of 10 additional feet (to 65') available if the additional design elements related to building massing, public open space, etc. were included in development.

Initially in their discussion on this issue, three members of the HCC were in support of establishing a maximum building height of 65' in the regulations, while two members supported the 55' limit contained in the draft regulations. Several members of the HCC stated that they believed that good design was more important than restricting the building height. After considerable discussion on this issue, the HCC supported the 65' building height, along with mandating the package of design elements suggested by staff. However, the HCC had additional concerns related to building mass in the gateway area, and suggested a reduced height and other design guidelines for this area (see discussion below).

- Upper Story Setback: *Requirement for a specific setback above the second story of buildings.* The regulation would require that all floors above the second story, within 40' of the property line along NE 38<sup>th</sup> Place, be set back an average of 15'. While the design guidelines call for a step back above the second story, the PC and HCC agreed that a specific regulation, similar to that used in the downtown, would provide predictability regarding this massing technique.
- Building Separation: *Regulation to limit the length of taller elements of buildings in TOD.* The HCC and PC concluded that due to the length of the property along NE 38<sup>th</sup> Place, a limit to the length of buildings parallel to this street was important to prevent an overly massive structure. The regulation would limit portions of structures over two stories in height to no more than 200' in length, and require that they be separated by at least 30' from other similar structures.
- Pedestrian Connection: *Requirement for a "Through-Block Pathway" to provide pedestrian access between NE 38<sup>th</sup> Place and the transit center.* The regulation would establish standards for this connection, and design guidelines would provide specific text for the Design Review Board to use in evaluating the quality of the connection for use by the public.

- Public Open Space: *Requirement that at least 2,500 square feet of public space be provided, in one continuous piece.* The PC and HCC both discussed the importance of a significant public space on site, to offset the impact of additional building mass, and to contribute to the desired site environment. Design guidelines to support this space state that the area should be located in close proximity to commercial and retail uses that are required along NE 38<sup>th</sup> Place. The guidelines also support the placement of additional public open space in a location convenient to the site's transit users.
- Retail Size Restriction: *No limit on the size of retail establishments.* The PC and HCC had discussed the concept of limiting the size of retail uses at the site in earlier study sessions. Both bodies, the HCC in particular, had been concerned about the implications for parking usage on site with a business that might be viewed as a regional destination. However, following receipt of a letter suggesting that the site might be desirable for a large grocery store which is considering locating within Kirkland, the PC and HCC re-evaluated the use of a restriction. Both groups concluded that the benefits of convenient retail use, particularly a grocery store, would be an asset to the site and the surrounding business district. The bodies also noted that the parking standards and an eventual parking study would ensure that adequate parking would be provided for any commercial uses to be included in development.
- Sustainability: *Requirement for a combination of Evergreen and LEED standards.* In response to requests by the affordable housing community, the HCC and PC agreed that regulations related to green building standards should allow use of standards that minimize costs to applicants and provide some flexibility, while still mandating accountability.
- Location of Retail Space: *Provisions to allow two-story commercial space along NE 38<sup>th</sup> Place.* The HCC and PC concluded that greater flexibility for retail space along NE 38<sup>th</sup> Place was appropriate to encourage successful retail uses and allow for retail space that faces the Park and Ride, given the topography of the site.

#### Planning Commission Recommendation – Unique Elements

Through-Block Pathway: The Planning Commission recommendation has one change that is different from the recommendation from the Houghton Community Council, related to the Through-Block Pathway. While this additional regulation (and associated design guidelines) were included in materials provided to the HCC for its study session on April 25<sup>th</sup>, the HCC did not discuss the regulation, and therefore did not say whether or not it also supported the additional standards.

The PC discussed the importance of a strong visual and functional connection through the parking area to the transit facility, and to the eventual connection to the Eastside Rail Corridor. The PC directed staff to add guidelines to call for a raised pedestrian walkway, raised planter beds, and increased landscaping, and to consult the City's Urban Forester regarding techniques to provide guidance for a denser tree canopy along this pathway. The suggested standards have been incorporated into the design guidelines and are noted below:

*"If located within a parking lot, the following guidelines should be incorporated into the design of the through-block pathway.*

- *Increased landscaped island size adjoining the pathway. This helps to narrow the driveway width where appropriate to help slow parking lot traffic.*
- *Raised landscape beds*
- *Raised pathway with pavement material, texture, and color different from traffic lanes*

- *Selection of tree species that provide the broadest canopy possible to produce a dense landscaped environment."*

### Houghton Community Council Recommendation – Unique Elements

Gateway. The HCC discussed the importance of site and building design within the gateway area at length during their study session. The gateway area is the portion of the site located near the intersection of NE 38<sup>th</sup> Place and 108<sup>th</sup> Avenue NE.

In its discussions, the HCC emphasized the key role it believes this area has in the Yarrow Bay Business District, and in providing a welcoming entry to Kirkland. The HCC expressed interest in ensuring that controls would be in place to minimize the mass of buildings within the gateway area, and that an element of public access is provided. To that end, the recommendation from the HCC includes the regulations and guidelines described below. These elements have been incorporated into the regulations and guidelines contained in Attachments 2-4. As noted above, these recommendations were made after the Planning Commission review was completed.

- A regulation in the Zoning Code to provide for a reduced building height within the gateway area:

*"Building height of a structure located within the gateway area, defined as the area located within a 50-foot radius from the point where the property line along NE 38<sup>th</sup> Place intersects with the Kirkland/Bellevue city boundary, shall not exceed 55' above average building elevation."*

- A regulation in the Zoning Code to require additional upper story setbacks within the gateway area:

*"Any portion of a structure located within the gateway area, defined as the area located within a 50-foot radius from the point where the property line along NE 38<sup>th</sup> Place intersects with the Kirkland/Bellevue city boundary, shall provide, at a minimum, upper story setbacks of at least five feet above the second story and ten feet at the fourth story. The final building façade design shall be based on the applicable design guidelines and determined through the Design Review process."*

- A regulation requiring that an area, accessible to the public, be provided within the gateway area. This space would be in addition to the public space required to be located in the vicinity of NE 38<sup>th</sup> Place, and would not be subject to the 2,500 square foot minimum requirement.

*"A visible and welcoming pedestrian-oriented space must be located between the sidewalk and the building in the gateway area."*

- A related design guideline would be added to the guidelines for the gateway:

*... "Contain a highly visible and welcoming public space between the sidewalk and the building which is easily accessible, comfortable, safe, and includes pedestrian amenities".*

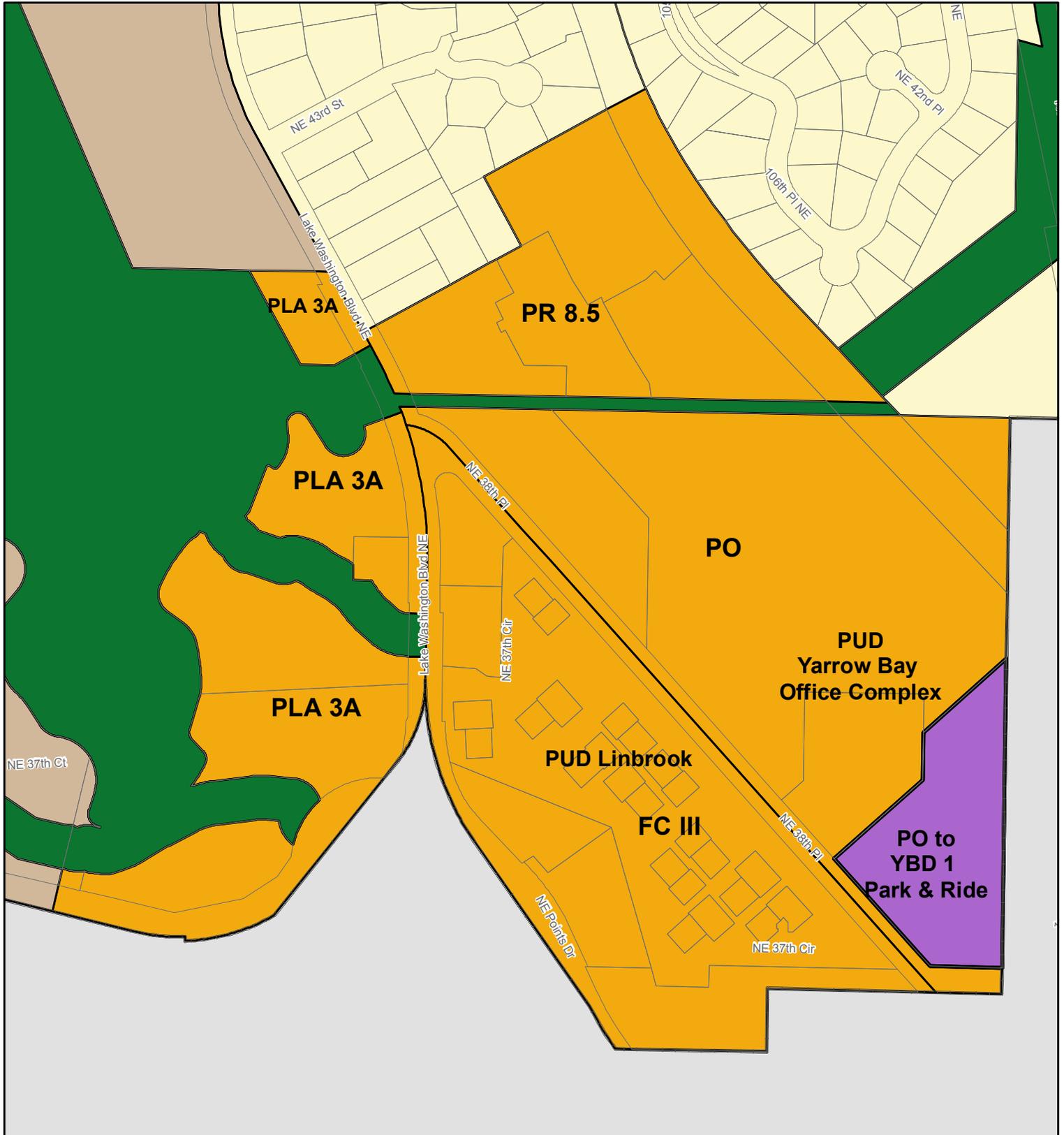
### Attachments

1. Proposed Amendments to Zoning Map
2. Proposed Amendments to Zoning Code

- a. Table of Contents
  - b. Chapter 56 – Use Zone Chart for YBD 1
  - c. Chapter 105 – Location of Parking Areas
  - d. Chapter 110 – Sidewalks and Public Improvements
  - e. Chapter 180- Plate 34L – Pedestrian Circulation
3. Proposed Amendment to Municipal Code
  4. Proposed Design Guidelines for YBD 1

cc: Houghton Community Council  
Planning Commission

# Proposed PO to YBD 1 Rezone



 Kirkland City Limits
  Tax Parcels
  PO to YBD 1

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# ZONING

## TABLE OF CONTENTS

This code contains zoning regulations for the Finn Hill, North Juanita, and Kingsgate annexation areas as adopted by the Kirkland City Council through Ordinance 4196. The effective date of the annexation and Ordinance 4196 zoning regulations is June 1, 2011.

[Click here](#) to view adopted ordinances that have not yet been inserted into the Zoning Code as well as pending regulations under consideration.

### [Zoning Code Interpretations](#)

[Chapter 1](#) – User Guide

[Chapter 5](#) – Definitions

[Chapter 10](#) – Legal Effect/Applicability

[Chapter 15](#) – Single-Family Residential (RS) Zones

[Chapter 17](#) – Single-Family Residential X (RSX) Zones

[Chapter 18](#) – Single-Family Residential A (RSA) Zones

[Chapter 20](#) – Multifamily Residential (RM and RMA) Zones

[Chapter 25](#) – Professional Office Residential (PR) and Professional Office Residential A (PRA) Zones

[Chapter 27](#) – Professional Office (PO) Zones

[Chapter 30](#) – Waterfront District (WD) Zones

[WDI Zone](#)

[WDII Zone](#)

[WDIII Zone](#)

[Chapter 35](#) – Freeway Commercial (FC) Zones

[FCIII Zone](#)

[Chapter 40](#) – Neighborhood Business (BN) Zones and Neighborhood Business A (BNA) Zones

[Chapter 45](#) – Community Business (BC, BC 1 and BC 2) Zones

[Chapter 47](#) – Community Business X (BCX) Zones

[Chapter 48](#) – Light Industrial Technology (LIT) Zones

[Chapter 49](#) – Park/Public Use (P) Zones

[Chapter 50](#) – Central Business District (CBD) Zones

[CBD-1A & 1B](#)

[CBD-2](#)

[CBD-3](#)

[CBD-4](#)

[CBD-5](#)

[CBD-5A](#)

[CBD-6](#)

[CBD-7](#)

[CBD-8](#)

[50.60](#) Special Parking Provisions in the CBD 1, 2, and 8 Zones

[50.62](#) Building Height Provisions in the CBD

[Chapter 51](#) – Market Street Corridor (MSC) Zones

[MSC 1, 4](#)

[MSC 2](#)

[MSC 3](#)

[Chapter 52](#) – Juanita Business District (JBD) Zones

[JBD-1](#)

[JBD-2](#)

[JBD-3](#)

[JBD-4](#)

[JBD-5](#)

[JBD-6](#)

[Chapter 53 – Rose Hill Business District \(RHBD\) Zones](#)

[RH 1A](#)

[RH 1B](#)

[RH 2A, 2B, 2C](#)

[RH 3](#)

[RH 4](#)

[RH 5A, 5B](#)

[RH 5C](#)

[RH 7](#)

[RH 8](#)

[Chapter 54 – North Rose Hill Business District \(NRHBD\) Zones](#)

[NRH-1A](#)

[NRH-1B](#)

[NRH-2](#)

[NRH-3](#)

[NRH-4](#)

[NRH-5](#)

[NRH-6](#)

[Chapter 55 – Totem Lake \(TL\) Zones](#)

[TL 1A](#)

[TL 1B](#)

[TL 2](#)

[TL 3A, 3B, 3C, 3D](#)

[TL 4A, 4B, 4C](#)

[TL 5](#)

[TL 6A, 6B](#)

[TL 7](#)

[TL 8](#)

[TL 9A](#)

[TL 9B](#)

[TL 10A](#)

[TL 10B](#)

[TL 10C](#)

[TL 10D](#)

[TL 10E](#)

[TL 11](#)

[Chapter 56 – Yarrow Bay Business District \(YBD\) Zones](#)

[YBD 1](#)

[Chapter 60 – Planned Areas \(PLA\)](#)

[PLA1](#)

[PLA2](#)

[PLA3](#)

[PLA5](#)

[PLA6](#)

[PLA7](#)

[PLA9](#)

[PLA14](#)

[PLA15](#)

[PLA16](#)

[PLA17](#)

[Chapter 70 – Holmes Point Overlay Zone](#)

[Chapter 72 – Adult Activities Overlay Zone](#)

[Chapter 75 – Historic Landmark Overlay Zone](#)

[Chapter 78 – Secure Community Transition Facility Overlay Zone](#)

[Chapter 80 – Equestrian Overlay Zone](#)

- [Chapter 83](#) – Shoreline Management
- [Chapter 85](#) – Geologically Hazardous Areas
- [Chapter 90](#) – Drainage Basins
- [Chapter 92](#) – Design Regulations
- [Chapter 95](#) – Tree Management and Required Landscaping
- [Chapter 100](#) – Signs
- [Chapter 105](#) – Parking Areas, Vehicle and Pedestrian Access, and  
Related Improvements
- [Chapter 110](#) – Required Public Improvements
- [Chapter 112](#) – Affordable Housing Incentives – Multifamily
- [Chapter 113](#) – Cottage, Carriage and Two/Three-Unit Homes
- [Chapter 115](#) – Miscellaneous Use Development and Performance Standards
- [Chapter 117](#) – Personal Wireless Service Facilities
- [Chapter 120](#) – Variances
- [Chapter 125](#) – Planned Unit Development
- [Chapter 127](#) – Temporary Use
- [Chapter 130](#) – Rezone
- [Chapter 135](#) – Amendments to the Text of the Zoning Code
- [Chapter 140](#) – Amendments to the Comprehensive Plan
- [Chapter 141](#) – Shoreline Administration
- [Chapter 142](#) – Design Review
- [Chapter 145](#) – Process I
- [Chapter 150](#) – Process IIA
- [Chapter 152](#) – Process IIB
- [Chapter 160](#) – Process IV
- [Chapter 161](#) – Process IVA
- [Chapter 162](#) – Nonconformance
- [Chapter 165](#) – Authority
- [Chapter 170](#) – Code Administration
- [Chapter 175](#) – Bonds
- [Chapter 180](#) – Plates

[Table of Revised Pages](#)  
[Ordinance History Table](#)  
[Ordinance Table](#)

## **Regulations for Residential (Mixed Use) Development Yarrow Bay Business District 1 (YBD 1)**

**56.05 User Guide.** The charts in KZC 56.10 contain the basic zoning regulations that apply in the YBD 1 zone of the City. Use these charts by reading down the left hand column entitled Use. Once you locate the use in which you are interested, read across to find the regulations that apply to that use.

### **Section 56.08 - GENERAL REGULATIONS**

The following regulations apply to all uses in this zone unless otherwise noted:

1. Refer to Chapter 1 KZC to determine what other provisions of this code may apply to the subject property.
2. In addition to the height exceptions established by KZC [115.60](#), the following exceptions to height regulations in the YBD 1 zone are established:
  - a. Decorative parapets may exceed the height limit by a maximum of four feet; provided that the average height of the parapet around the perimeter of the structure shall not exceed two feet.
  - b. For structures with a peaked roof, the peak may extend eight feet above the height limit if the slope of the roof is equal to or greater than four feet vertical to 12 feet horizontal.

### **USE ZONE CHART**

#### **Section 56.010**

- 1) Use: Attached or Stacked Dwelling Units:

See Special Regulations.

Required Review Process: DR, Chapter 142 KZC.

#### Minimums:

Lot Size: None

Required Yards:

Front: 5' (see Special Regulation 2)

Side: 0'

Rear: 0'

Maximums:

Lot Coverage: 100%.

Height of Structures: 65' above average building elevation. *(HCC RECOMMENDATION – Additional text: See Special Regulation 11).*

Landscape Category: C

Sign Category: E. See Special Regulation 15.

Required Parking (See KZC 105.103):

- Residential use: 1.1 per unit. In addition, guest parking shall be provided at a rate of 0.05 stalls per unit.
- Restaurant/tavern: 1 per 125 square feet of gross floor area
- Retail: 1 per 350 square feet of gross floor area
- Office: 1 per 350 square feet of gross floor area
- Entertainment, Cultural, Recreational: Chapter 105.25

**Special Regulations:**

1. The required minimum front yard for any portion of the structure containing parking facilities shall be 10'.
2. The front setback may be reduced to 0' where retail uses or other ground floor space is designed to provide direct pedestrian access to the street and located adjacent to a pedestrian oriented street, major pedestrian pathway or adjacent to a transit facility.
3. May include one or more of the other uses allowed in this zone.
4. The following uses are prohibited:
  - a. Drive-through facilities.
  - b. The outdoor storage, sale, service and/or rental of motor vehicles, sailboats, motor boats, and recreational trailers.

5. At least 50% of the linear frontage of the ground floor along NE 38<sup>th</sup> Place must include one or more of the following uses: Retail uses selling goods or providing services, including restaurants or taverns; Banking and Related Financial Services; School, Day-Care or Mini School or Mini Day-Care Center; Government Facility; Community Facility; and retail establishments providing entertainment, cultural and/or recreational activities. The required uses shall have a minimum depth of 20 feet and an average depth of at least 30 feet (as measured from the face of the building on the abutting right-of-way). The Design Review Board (or Planning Director if not subject to D.R.) may approve a minor reduction in the depth requirements if the applicant demonstrates that the requirement is not feasible given the configuration of existing or proposed improvements and that the design of the retail frontage will maximize visual interest. Lobbies for residential are allowed within this space subject to applicable design guidelines. The minimum ground floor story height for these uses shall be 13 feet.
6. Commercial uses along NE 38<sup>th</sup> Place may occupy the first two floors of a structure. Otherwise, gross floor area constructed above the ground floor must be dedicated to residential use.
7. Any portion of a structure exceeding two stories in height above NE 38<sup>th</sup> Place may not exceed 200' in length as measured parallel to NE 38<sup>th</sup> Place, and shall be separated by at least 30 feet from any other portion of a structure exceeding two stories above NE 38<sup>th</sup> Place on the subject property.
8. At least 2,500 square feet of public open space shall be provided in conjunction with new development. The space shall be in one continuous piece, and designed to be consistent with the design guidelines for public open space on site.
9. ***(HCC RECOMMENDATION) A visible and welcoming pedestrian-oriented space must be located between the sidewalk and the building in the gateway area.***
10. The upper story setback for all floors above the second story within 40' of the property line abutting NE 38<sup>th</sup> Place shall average 15'. For the purpose of this regulation, the term "setback" shall refer to the horizontal distance between the property line and any exterior wall abutting the street prior to any potential right-of-way dedication. The required upper story setbacks for all floors above the second story shall be calculated as Total Upper Story Setback Area, as shown on Plate 35.

*(HCC RECOMMENDATION – Additional text): Any portion of a structure located within the gateway area, defined as the area located within a 50-foot radius from the point where the property line along NE 38<sup>th</sup> Place intersects with the Kirkland/Bellevue city boundary, shall provide, at a minimum, upper story setbacks of at least five feet above the second story and ten feet at the fourth story. The final building façade design shall be based on the applicable design guidelines and determined through the Design Review process.*

11. *(HCC RECOMMENDATION – Additional text): Building height of a structure located within the gateway area, defined as the area located within a 50-foot radius from the point where the property line along NE 38<sup>th</sup> Place intersects with the Kirkland/Bellevue city boundary, shall not exceed 55' above average building elevation.*
12. A Through-Block Pathway, developed according to the standards in Section 105.19.3, must be installed to provide pedestrian access between NE 38<sup>th</sup> Place and the transit center. *(PC Recommendation – Additional text): The final design of the pathway shall be based on the applicable design guidelines and determined through the Design Review Process.*
13. Development of residential uses within the zoning district shall result in a minimum of 20 percent of total residential units being affordable with affordability levels as follows:
  - a. For renter-occupied housing:
    - A minimum of 20 percent of the total residential units shall be affordable. A minimum of 10 percent of total residential units shall be affordable at 50% of median income. The remaining affordable units shall be affordable at no greater than 70% of median income Affordable rent levels will be determined using the same methodology used in the definition of Affordable Housing Unit in Chapter 5 KZC.
  - b. For owner-occupied housing:
    - A minimum of 20 percent of total residential units shall be affordable housing units as defined in KZC Section 5.10.023.1.a.
14. The following additional regulations apply to affordable housing units included in development:
  - a. Alternative Affordability Levels – Subject to Planning Director approval, an applicant for owner-occupied housing may propose affordability levels different from those defined in this Chapter. In approving any different affordability levels, the Director shall use ratios similar to those in Chapter KZC 112.20.3.b.
  - b. Affordable housing provided pursuant to this section shall also comply with the following sections of Chapter 112KZC: 112.15.4 (Rounding); 112.35.2 (Affordability Agreement)

- c. The following provisions of Chapter 112 KZC do not apply to this zoning district: 112.20 (Basic Affordable Housing Incentives); 112.25 (Additional Affordable Housing Incentives); 112.30 (Alternative Compliance).
  - d. Other provisions for the affordable housing units include:
    - o The type of ownership of the affordable housing units shall be the same as the type of ownership for the rest of the housing units in the development.
    - o The affordable housing units shall consist of a range in number of bedrooms that are comparable to units in the overall development.
    - o The size of the affordable housing units, if smaller than the other units with the same number of bedrooms in the development, must be approved by the Planning Director. In no case shall the affordable housing units be more than 10 percent smaller than the comparable dwelling units in the development, based on number of bedrooms, or less than 500 square feet for a one-bedroom unit, 700 square feet for a two-bedroom unit, or 900 square feet for a three-bedroom unit, whichever is less.
    - o The affordable housing units shall be available for occupancy in a time frame comparable to the availability of the rest of the dwelling units in the development, unless the Planning Director approves a phasing plan pursuant to KMC 5.88.090.
    - o The exterior design of the affordable housing units must be compatible and comparable with the rest of the dwelling units in the development.
    - o The interior finish and quality of construction of the affordable housing units shall at a minimum be comparable to entry level rental or ownership housing in the City of Kirkland.
  - e. Applicants providing affordable housing units may request an exemption from payment of road impact fees for the affordable housing units as established by KMC 27.04.050.
  - f. Applicants providing affordable housing units may request an exemption from payment of park impact fees for the affordable housing units as established by KMC 27.06.050.
  - g. Applicants providing affordable housing units are eligible for exemption from various planning, building, plumbing, mechanical and electrical permit fees for the affordable housing and moderate income units as established in KMC 5.74.070 and KMC Title 21.
  - h. Property Tax Exemption – A property providing affordable housing units may be eligible for a property tax exemption as established in Chapter 5.88 KMC
15. Signs for a development approved under this provision must be proposed within a Master Sign Plan application (KZC 100.80) for all signs within the project.

16. Development should be designed, built and certified to achieve or exceed the following green building standards:
  - a. Evergreen Standard or Built Green 4 star certified for all housing units.
  - b. For the parking garage and non-residential uses, either a LEED Silver CS (Core and Shell) certified or LEED CS checklist with a third party independent verification and inspection to meet the LEED CS Silver Standard.
17. This use must be part of a development that includes an increase in the number of parking stalls available exclusively to users of the Park and Ride facility.
18. Parking stalls to serve the use must be in addition to those provided as part of the expansion of capacity for the Park and Ride facility.

105.58 Location of Parking Areas Specific to Design Districts

If the subject property is located in a Design District, the applicant shall locate parking areas on the subject property according to the following requirements:

1. Location of Parking Areas in the CBD, TC (TL 1, TL 2, TL 3) Zones
  - a. Parking areas shall not be located between a pedestrian-oriented street and a building unless specified in a Conceptual Master Plan in TL 2. (See Plate 34 in Chapter 180 KZC and Chapters 92 and 110 KZC for additional requirements regarding pedestrian-oriented streets).
  - b. On all other streets, parking lots shall not be located between the street and the building on the subject property unless no other feasible alternative exists.
2. Location of Parking Areas in the JBD 2, ~~and the NRHBD~~ and YBD 1 Zones – Parking areas shall not be located between the street and the building unless no other feasible alternative exists on the subject property.
3. Location of Parking Areas in the MSC Zones – Parking areas in the MSC zones shall not be located between the street and the building unless the Planning Official determines that the proposed landscape design provides superior visual screening of the parking area.
4. Location of Parking Areas in Certain TLN and RHBD Zones – Parking areas and vehicular access may not occupy more than 50 percent of the street frontage in the following zones (see Figure 105.58.A):
  - a. TL 4, only properties fronting on 120th Avenue NE;
  - b. TL 5;
  - c. TL 6A, only properties fronting on 124th Avenue NE. Auto dealers in this zone are exempt from this requirement;
  - d. TL 6B, only properties fronting on NE 124th Street;
  - e. TL 10E.

Alternative configurations may be considered through the Design Review process, if the project meets the objectives of the KMC Design Guidelines for the Totem Lake Neighborhood.

- f. In the Regional Center (RH 1A, RH 2A, RH 3 and RH 5A zones west of 124th Avenue). For parcels over two acres in size, parking lots and vehicular access areas may not occupy more than 50 percent of the NE 85th Street property frontage (see Figure 105.58.A). Alternative configurations will be considered through the Design Review process, if the project meets the intent of the KMC Design Guidelines for the Rose Hill Business District.



Parking lots and spaces  
reside occupy no more  
than 50% of the total  
street frontage

FIGURE 105.58.A

## 110.52 Sidewalks and Other Public Improvements in Design Districts

1. This section contains regulations that require various sidewalks, pedestrian circulation and pedestrian-oriented improvements on or adjacent to properties located in Design Districts subject to Design Review pursuant to Chapter 142 KZC such as CBD, JBD, TLN, TC, RHBD, ~~and~~ NRHBD ~~and~~ YBD zones.

The applicant must comply with the following development standards in accordance with the location and designation of the abutting right-of-way as a pedestrian-oriented street or major pedestrian sidewalk shown in Plate 34 of Chapter 180 KZC. See also Public Works Pre-Approved Plans manual for public improvements for each Design District. If the required sidewalk improvements cannot be accommodated within the existing right-of-way, the difference may be made up with a public easement over private property; provided, that a minimum of five feet from the curb shall be retained as public right-of-way and may not be in an easement. Buildings may cantilever over such easement areas, flush with the property line in accordance with the International Building Code as adopted in KMC Title 21. (See Figure 110.52.A and Plate 34).

2. Pedestrian-Oriented Street Standards – Unless a different standard is specified in the applicable use zone chart, the applicant shall install a 10-foot-wide sidewalk along the entire frontage of the subject property abutting each pedestrian-oriented street. (See Figure 110.52.A).

### Required Sidewalk on Pedestrian-Oriented Streets and Major Pedestrian Sidewalks

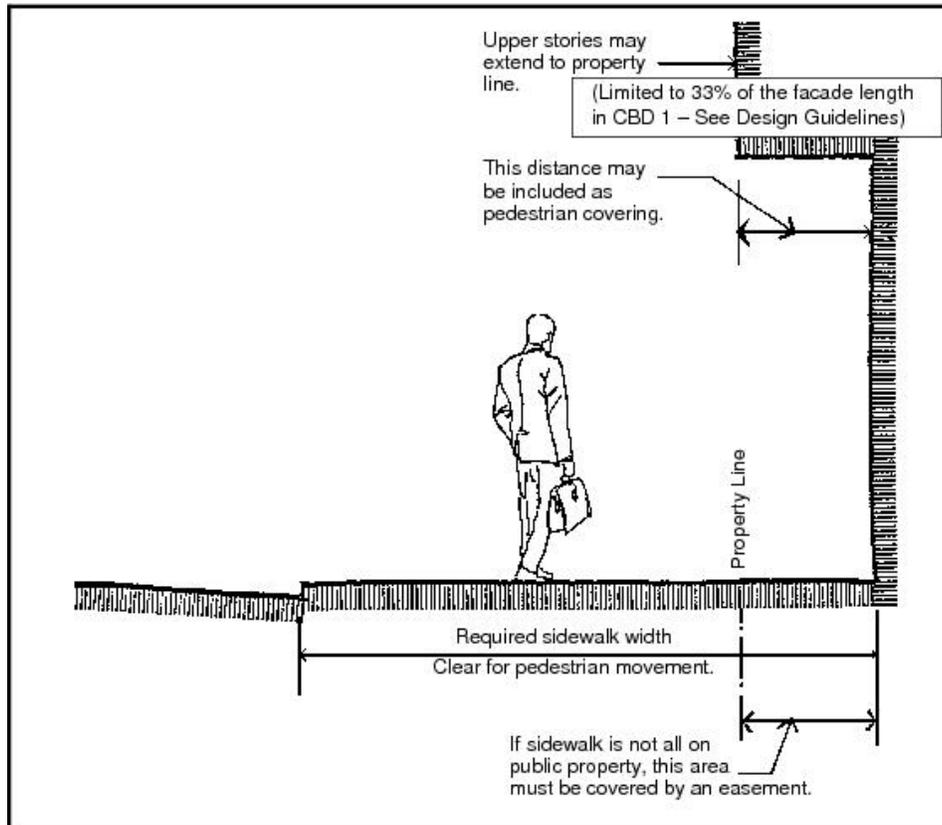
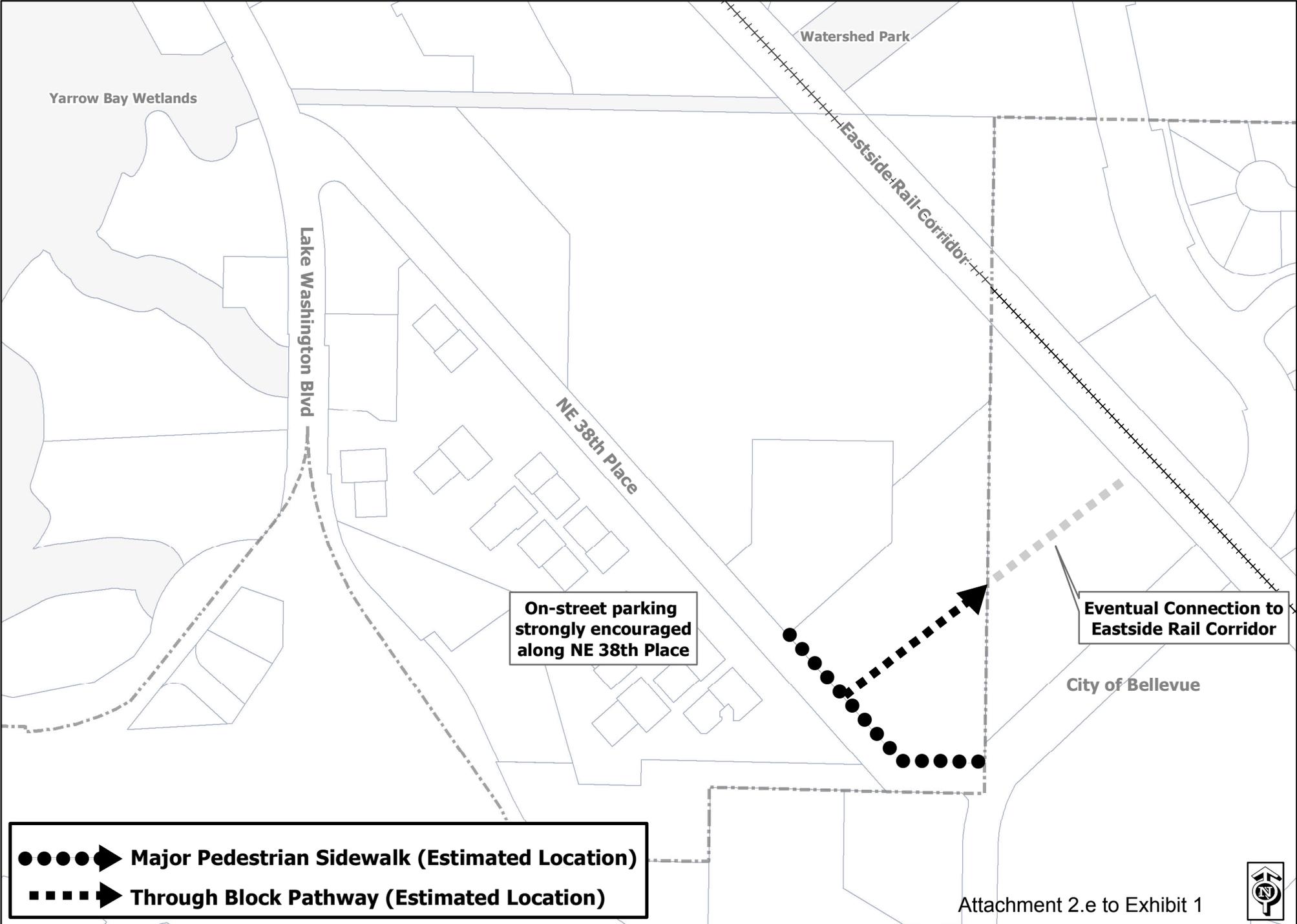


FIGURE 110.52.A

3. Major Pedestrian Sidewalk Standards – If the subject property abuts a street designated to contain a major pedestrian sidewalk in Plate 34, Chapter [180](#) KZC, the applicant shall install that sidewalk on and/or adjacent to the subject property consistent with the following standards:
  - a. Install in the approximate location and make the connections shown in Plate 34;
  - b. A sidewalk width of at least eight feet, unless otherwise noted in Plate 34;
  - c. Have adequate lighting with increased illumination around building entrances and transit stops; and
  - d. If parcels are developed in aggregate, then alternative solutions may be proposed.
4. Streets in the Totem Lake Neighborhood – Streets in the Totem Lake Neighborhood designated as major pedestrian sidewalks in Plate 34.E that are also shown to be within the landscaped boulevard alignment or “Circulator” in Plate 34.D in Chapter [180](#) KZC may have varied or additional requirements, such as wider sidewalks, widened and meandering planting areas, continuous and clustered tree plantings, special lighting, directional signs, benches, varying pavement textures and public art, as determined by the Director of Public Works.
5. NE 85th Street Sidewalk Standards – If the subject property abuts NE 85th Street, the applicant shall install a minimum 6.5-foot-wide landscape strip planted with street trees located adjacent to the curb and a minimum seven-foot-wide sidewalk along the property frontage. Where the public right-of-way lacks adequate width to meet the previous standard, a 10-foot-wide sidewalk with street trees in tree grates may be permitted or in an easement established over private property.

# Plate 34L: PEDESTRIAN CIRCULATION IN YBD



## Chapter 3.30

### DESIGN REVIEW BOARD

#### Sections:

- 3.30.010 Membership—Appointment—Compensation—Removal.**
- 3.30.020 Qualifications.**
- 3.30.030 Powers and duties.**
- 3.30.040 Design guidelines adopted by reference.**
- 3.30.050 Conflict of interest.**

#### **3.30.010 Membership—Appointment—Compensation—Removal.**

The design review board shall be composed of seven appointed members. In addition, the director of planning and community development shall sit on the design review board (“DRB”) as a nonvoting member for purposes of advising the board on regulatory and urban design issues. Members shall be appointed by a majority vote of the city council, without regard to political affiliation. The members of the DRB shall serve without compensation. Each member shall be appointed to a four-year term; provided, that as to the two positions added in 2003, one new member’s initial term shall expire March 31, 2005, and the other new member’s initial term shall expire March 31, 2007. Any vacancy shall be filled for the remainder of the unexpired term of the vacant position. When a member misses three or more consecutive meetings not excused by a majority vote of the DRB, the DRB will consider recommending removal of that member. The board shall recommend removal if the absences have negatively affected the board’s abilities to perform its duties. The recommendation will be forwarded to city council. Members finding themselves unable to attend regular meetings are expected to tender their resignations. A member may be removed by a majority vote of the city council. (Ord. 3901 § 1, 2003; Ord. 3683A § 1 (part), 1999)

#### **3.30.020 Qualifications.**

Members of the design review board shall include design professionals and building/construction experts, and residents of Kirkland capable of reading and understanding architectural plans and knowledgeable in matters of building and design. The board shall at all times have a majority composition of professionals from architecture, landscape architecture, urban design/planning, or similar disciplines. In selecting members, professionals who are residents and/or whose place of business is within Kirkland will be preferred. (Ord. 3683A § 1 (part), 1999)

#### **3.30.030 Powers and duties.**

The design review board shall have the responsibilities designated in the Zoning Code. In addition, the design review board shall perform such advisory functions related to design issues as designated by the city council. (Ord. 3683A § 1 (part), 1999)

#### **3.30.040 Design guidelines adopted by reference.**

The design review board in combination with the authority set forth in Chapter 142 of the Zoning Code shall use the following design guidelines documents to review development permits:

- (1) The document entitled “Design Guidelines for Pedestrian Oriented Business Districts” bearing the signature of the mayor and the director of the department of planning and community development dated August 3, 2004, is adopted by reference as though fully set forth herein. The city council shall consult with the planning commission prior to amending this document.
- (2) The document entitled “Design Guidelines for the Rose Hill Business District” bearing the signature of the mayor and the director of the department of planning and community development dated January 3, 2006, is adopted by reference as though fully set forth herein. The city council shall consult with the planning commission prior to amending this document.

(3) The document entitled “Design Guidelines for the Totem Lake Neighborhood” bearing the signature of the mayor and the director of the department of planning and community development dated June 6, 2006, is adopted by reference as though fully set forth herein. The city council shall consult with the planning commission prior to amending this document.

(4) The document entitled “Kirkland Parkplace Mixed Use Development Master Plan and Design Guidelines” bearing the signature of the mayor and the director of the department of planning and community development, dated December 16, 2008, is adopted by reference as though fully set forth herein. The city council shall consult with the planning commission prior to amending this document.

(5) The document entitled “Design Guidelines for the Yarrow Bay Business District” bearing the signature of the mayor and the director of the department of planning and community development, dated June 7, 2011, is adopted by reference as though fully set forth herein. The city council shall consult with the planning commission and the Houghton community council prior to amending this document.

~~(5)~~ Text Amended. The following specific portions of the text of the design guidelines are amended as set forth in Attachment A attached to Ordinance 4106 and incorporated by reference. (Ord. 4172 § 1, 2008: Ord. 4106 § 1, 2007; Ord. 4052 § 1, 2006: Ord. 4038 § 1, 2006: Ord. 4031 § 1, 2006)

### **3.30.050 Conflict of interest.**

If a member of the design review board is an applicant or a paid or unpaid advocate, agent, or representative for an applicant on a design review application, the member shall not participate in a decision on that design review application. (Ord. 3683A § 1 (part), 1999)

## Design Guidelines for the Yarrow Bay Business District 1 Zone

<p>1. <i>Ensure high quality building and design</i></p>	<ul style="list-style-type: none"><li>• Building materials should exhibit permanence.</li><li>• Building materials and color should be selected to integrate with each other and complement architectural design.</li><li>• Ornament and applied art should be integrated with the structures and the site environment and not haphazardly applied.</li><li>• Emphasis should be placed on highlighting building features such as doors, windows, and eaves, and on the use of materials such as wood siding and ornamental masonry. Ornament may take the form of traditional or contemporary elements</li><li>• Original artwork or hand-crafted details should be considered in special areas.</li></ul>
<p>2. <i>Ensure that regulations support appropriate building scale and massing throughout the site, produce buildings that exhibit high quality design and incorporate pedestrian features and amenities that contribute to a livable urban village character for the TOD.</i></p>	<p><b><i>Building Scale &amp; Massing</i></b></p> <ul style="list-style-type: none"><li>• Large window areas should be avoided. Instead smaller window units should be used to achieve human scale.</li><li>• Facing the street, buildings above the 2<sup>nd</sup> story should use upper story step backs to create receding building forms as building height increases to maintain human scale. A rigid stair step or “wedding cake” approach to upper story step backs is not appropriate. <u><i>(HCC Recommendation – additional text): Prescribed upper story step backs in the gateway area at the intersection of NE 38<sup>th</sup> Place and 108<sup>th</sup> Avenue NE are appropriate to prevent the building from overpowering the gateway design.</i></u></li><li>• Decks and/or balconies should be designed so that they do not significantly increase the apparent mass of the building.</li><li>• The location of the subject property makes any new multi-story building highly visible from the surrounding streets and properties. Building design should be based on viewpoints or vantages to be identified through the Design Review process. The final arrangement of building mass should therefore address the key vantage points and respond to the context of existing and/or planned improvements, gateway features, and location of plazas and open space.</li><li>• All building facades should be designed carefully, i.e. there should be no “backside” of a building.</li><li>• Building facades should be well modulated to avoid blank walls and provide architectural interest.</li><li>• Landscaping should be used to provide visual interest and help soften building form at appropriate locations, including upper level terraces.</li><li>• To help moderate the vertical scale of buildings, buildings should incorporate design techniques which clearly define</li></ul>

the building's top, middle, and bottom.

Examples include using a sloped roof and strong eave lines to help define the top; using windows, balconies, and material changes to define a building's middle; and pedestrian-oriented storefronts, awnings, and use of 'earth' materials such as concrete and stone to help define the building's bottom.

- Roof forms should be varied and attractive. Where appropriate, roof forms should also help reinforce the modulation or articulation interval of the building façade.
- Roof forms should be designed to screen rooftop mechanical units
- A predominantly flat roof design is discouraged. For portions of the building where a flat roof design is used, architectural details such as eaves, cornices, or other articulation elements should be used to provide interest at the ground level.
- Vertical building modulation should be used to add variety by avoiding monotonous design. A technique that may be used is to make large buildings appear to be an aggregation of smaller buildings. Different colors and/or materials may be used to help differentiate between façade planes.
- Horizontal building modulation should be used to reduce the perceived mass of a building and to provide continuity at the ground level of large building complexes. Building design should incorporate strong pedestrian-oriented elements at the ground level and distinctive roof treatments. Different colors and/or materials maybe used to help differentiate between façade planes.

#### ***High Quality Design***

See Policy #1

#### ***Pedestrian Features & Amenities***

- Pedestrian walkways should be placed throughout the site to allow for efficient access between the residential, commercial, transit center uses, and adjacent streets. The walkways should be situated to minimize walking distance from the public sidewalk and transit facilities to building entrances.
- Pedestrian and bicycle pathways and/or connections should be well-defined and safe.
- Pedestrian connections should be provided to adjacent properties to allow for efficient access to the transit facilities and commercial uses.
- Landscaping should be used to help define and provide visual interest along pedestrian walkways.

- Convenient and safe pedestrian areas should be designed in centralized locations to accommodate transit users.
- Lighting should be provided to walkways and sidewalks through building mounted light and canopy or awning mounted lights.
- Low level lighting in the form of bollards or similar style of lighting should be encouraged along pedestrian pathways not adjacent to buildings.
- Through-block pathways should be designed so that it is clear that access by the general public is allowed. The following guidelines also apply:
  - Because the subject property is steep along NE 38<sup>th</sup> Place, stairways may be used in the design of the through-block pathway where connecting to the street. If located along NE 38<sup>th</sup> Place, the stairway should function as a focal entry/exit point and contain design elements that make it a welcoming, safe, and attractive entry.
  - *(PC Recommendation – additional text): If located within a parking lot, the following guidelines should be incorporated into the design of the through-block pathway.*
    - *Increased landscaped island size adjoining the pathway. This helps to narrow the driveway width where appropriate to help slow parking lot traffic.*
    - *Raised landscape beds*
    - *Raised pathway with pavement material, texture, and color different from traffic lanes*
    - *Selection of tree species that provide the broadest canopy possible to produce a dense landscaped environment.*
  - If the through-block pathway is located between buildings, appropriate plants and trees should be selected based on solar access and the location of proposed improvements.

3. *Provide guidance for the streetscapes along NE 38th Place and 108th Avenue NE to ensure buildings do not turn their backs on the streets and development provides a welcoming and attractive presence at this gateway to Kirkland.*

**Streetscape**

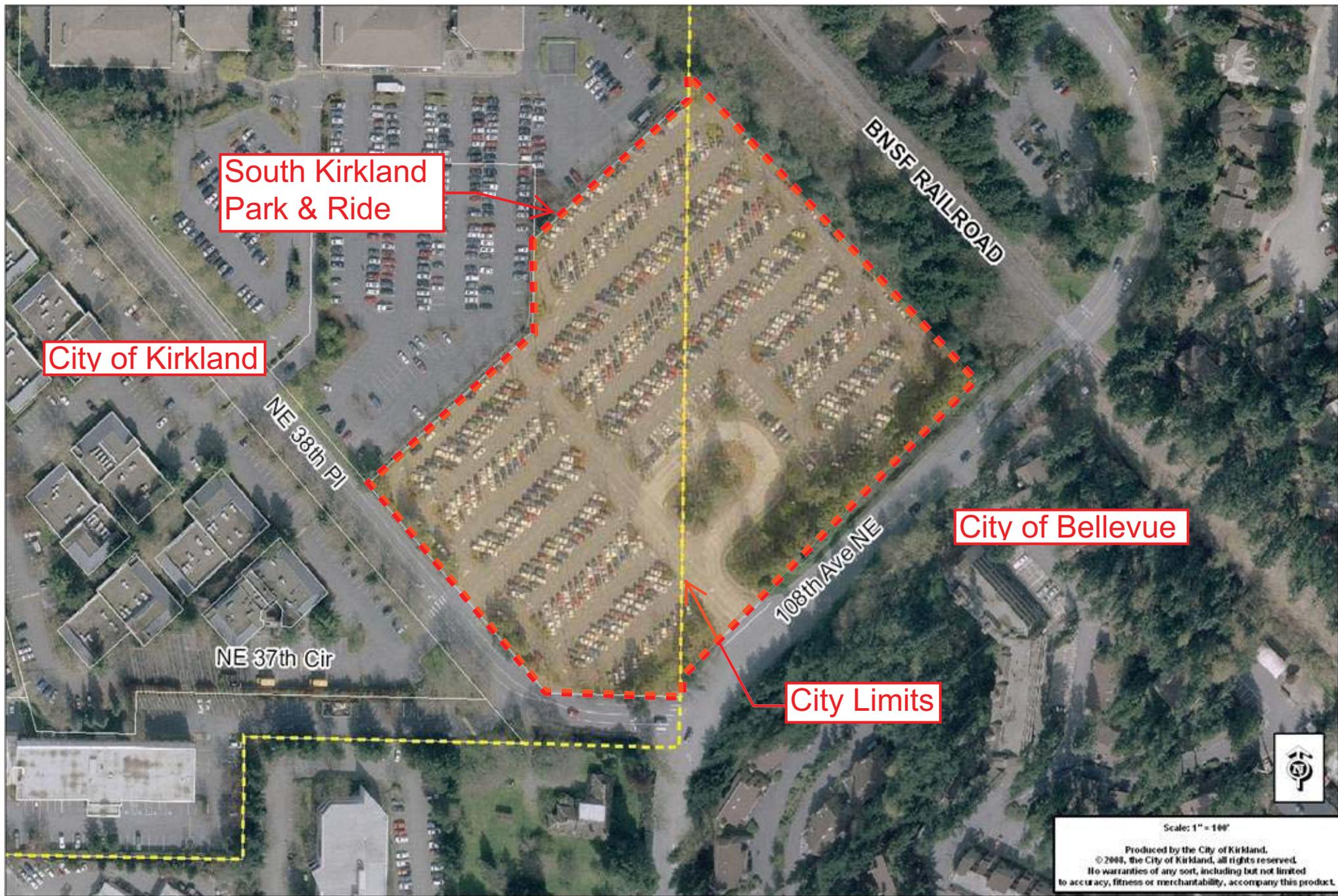
- Street trees species should be selected and spaced to allow for visual continuity along NE 38th Place, buffer pedestrians from the street, and provide visibility of ground floor retail uses.
- Buildings should be oriented towards the street when located along NE 38<sup>th</sup> Place.
- Design elements such as multiple storefronts, pedestrian-oriented signs, exterior light fixtures, glazing, landscaping, and awnings should be utilized to add human scale and interest at the street level.
- Ground floor spaces along NE 38<sup>th</sup> Place should be transparent with windows of clear vision glass beginning no higher than 2’ above grade to at least 10’ above grade. Windows should extend across, at a minimum, 75% of the façade length. Continuous window walls should be avoided by providing architectural building treatments, mullions, building modulation, entry doors, and/or columns at appropriate intervals.
- Varied window treatments should be encouraged. Architectural detailing at window jambs, sills, and heads should be emphasized. Use of ribbon windows should be avoided.
- A street wall is a wall or portion of a wall of a building facing a street. Continuous street walls should incorporate vertical and horizontal modulations into the building form.
- Along pedestrian oriented streets, upper story building facades should be stepped back to provide enough space for decks, balconies, and other activities overlooking the street.
- Awnings or canopies should be required on facades adjoining sidewalks. Blank walls should be avoided near sidewalks, open spaces, and pedestrian areas.
- Blank walls should not be visible from the street or sidewalk. Where blank walls are unavoidable, they should be treated with landscaping, art, or other architectural treatments.

**Gateway**

- A gateway is an urban design feature that signifies a sense of place and arrival into a city or neighborhood. A gateway should be designed in the location shown in the Comprehensive Plan.
- The design elements of the gateway should include a combination of landscaping, architectural features, and artwork which:
  - *(HCC Recommendation – additional text): Contain a highly visible and welcoming public space between the sidewalk and the building which is easily accessible, comfortable, safe, and includes pedestrian amenities;*
  - Establish a landmark that reflects the TOD elements of the site;
  - Reinforce NE 38th Place and 108th Avenue NE as a focal point;

	<ul style="list-style-type: none"><li>○ Transition between Kirkland and Bellevue and the Yarrow Bay Business District to the west; and</li><li>○ Are integrated with the TOD building design</li></ul>
<p>4. <i>Minimize the visual impacts of parking facilities from adjacent rights-of-ways.</i></p>	<ul style="list-style-type: none"><li>• Parking areas should not be located between NE 38<sup>th</sup> Place and buildings.</li><li>• Access driveways to parking areas should be minimized.</li><li>• Parking lots should be designed to provide for clear vehicular and pedestrian circulation and be well organized.</li><li>• Screening and landscaping should be used to reduce the visual impact of parking lots and/or parking structures to the surrounding neighborhood.</li><li>• Parking structures shall be designed and located to obscure the view of parked cars from adjacent properties. Parking structures should be located to the back of buildings or underground with intervening uses.,</li><li>• Portions of parking structures visible from the street that cannot be placed behind an intervening use due to site topography, should be constructed with high quality materials and be architecturally compatible with the character of surrounding buildings.</li></ul> <p>In addition, architectural treatment, artwork, building setbacks, and/or dense landscaping should be used to further reduce the visual impact of parking structures along the street.</p> <p>If adjacent to the required gateway, the exterior of parking structure should reflect the design elements of the gateway. Design should avoid the appearance of a parking structure.</p>

<p>5. Foster the creation of vibrant and desirable living environment through the use of high quality design, public amenities, and open space.</p>	<p><b><i>High Quality Design</i></b></p> <p>See Policy #1</p> <p><b><i>Public amenities and Open Space</i></b></p> <ul style="list-style-type: none"><li>• Public open space should be provided on the subject property which can be used by the general public, residents, and transit users.</li><li>• Public open space should be open to the sky except where overhead weather protection is provided (e.g. canopies and awnings). The space should appear and function as public space rather than private space.</li><li>• Public open space should be located in close proximity to commercial and retail uses that are required along NE 38<sup>th</sup> Place. The public open space should be well defined and contain amenities such as outdoor dining, seating areas, art, water features, and/or landscaping. Adequate room for pedestrian movement through the space should be maintained. Additional public open space in a location convenient to the site's transit users may also be appropriate.</li><li>• Careful attention should be paid to the transition between transit operations and the building to create a well defined pedestrian space such as a small plaza with landscaping features.</li><li>• A combination of lighting, access to sunlight, paving, landscaping, and seating should be used to enhance the pedestrian experience with the public open space.</li></ul>
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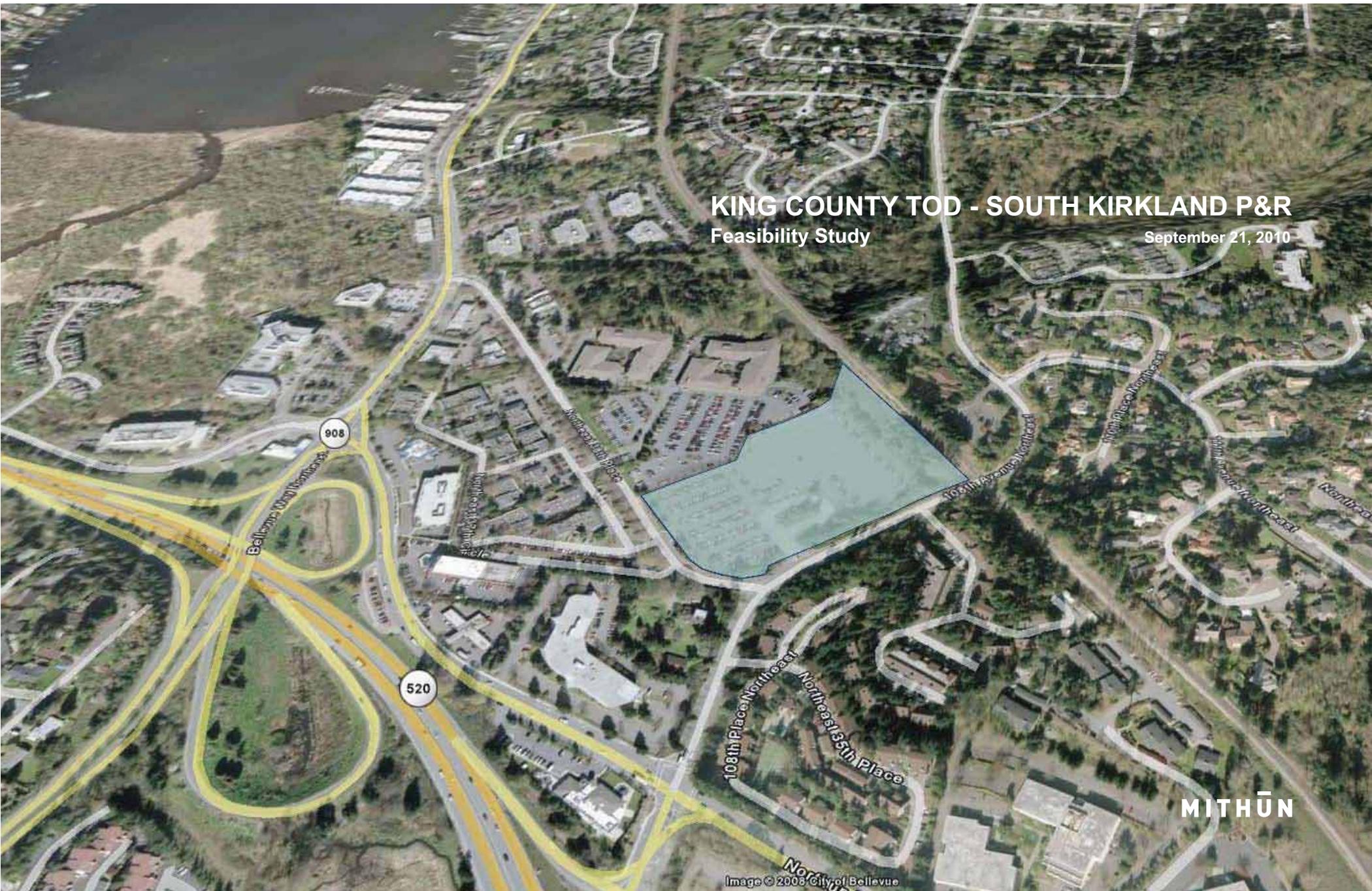
South Kirkland  
Park & Ride

City of Kirkland

City of Bellevue

City Limits

Scale: 1" = 100'  
Produced by the City of Kirkland.  
©2008, the City of Kirkland, all rights reserved.  
No warranties of any sort, including but not limited  
to accuracy, fitness or merchantability, accompany this product.



**KING COUNTY TOD - SOUTH KIRKLAND P&R**  
Feasibility Study

September 21, 2010

**MITHUN**

Image © 2008 City of Bellevue

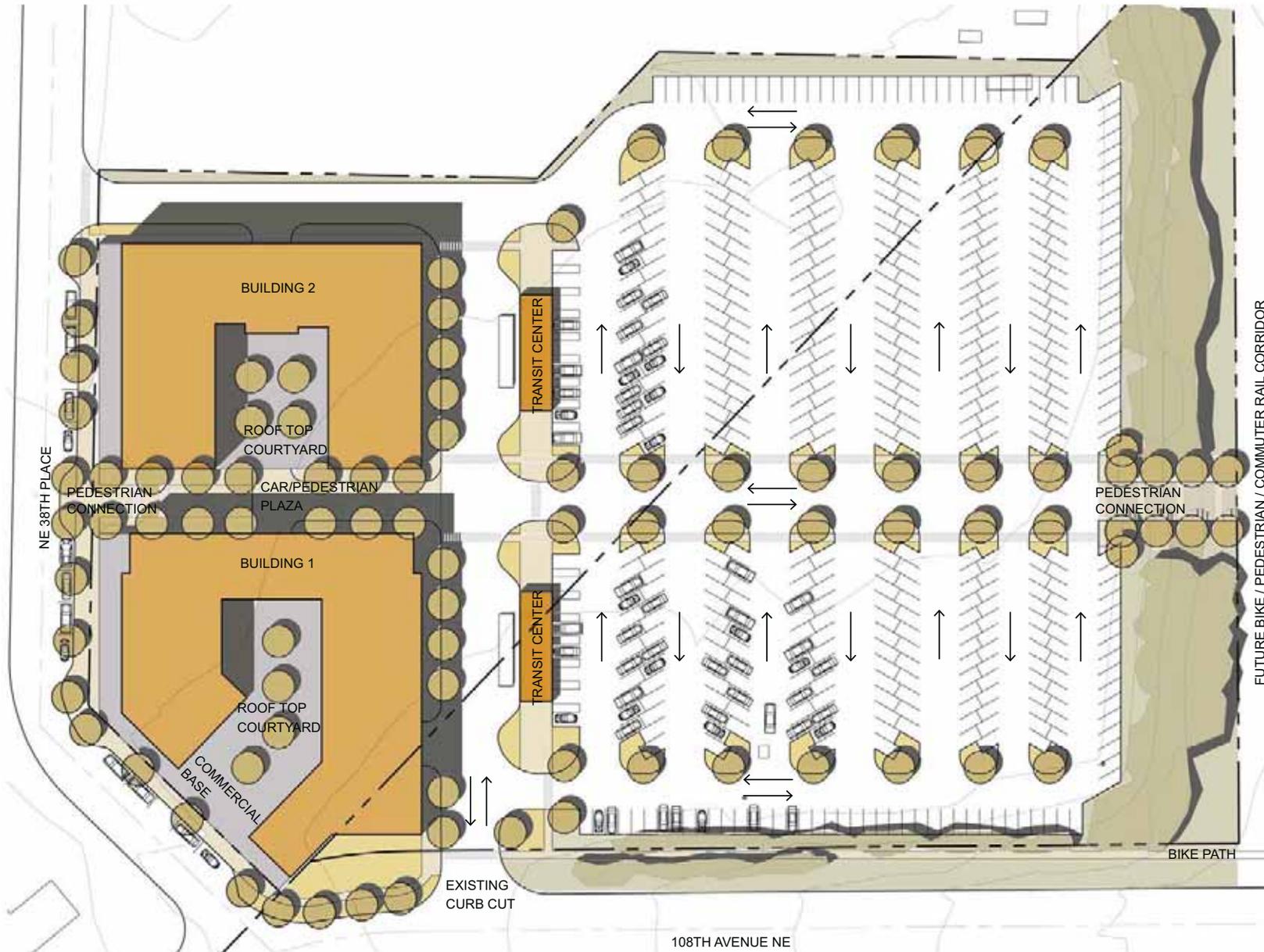


**King County Objectives at South Kirkland:**

- Increase ridership
- Provide housing opportunities with an emphasis on affordability
- Increase transportation options, including additional parking, better passenger facilities
- Future interconnection with BNSF
- Provide vehicle charging stations to serve park and ride users and residents.

**Funding:**

- Limited funding of \$6.25 million is available for 250 additional parking stalls from federal transit administration as part of SR520 bridge tolling project.

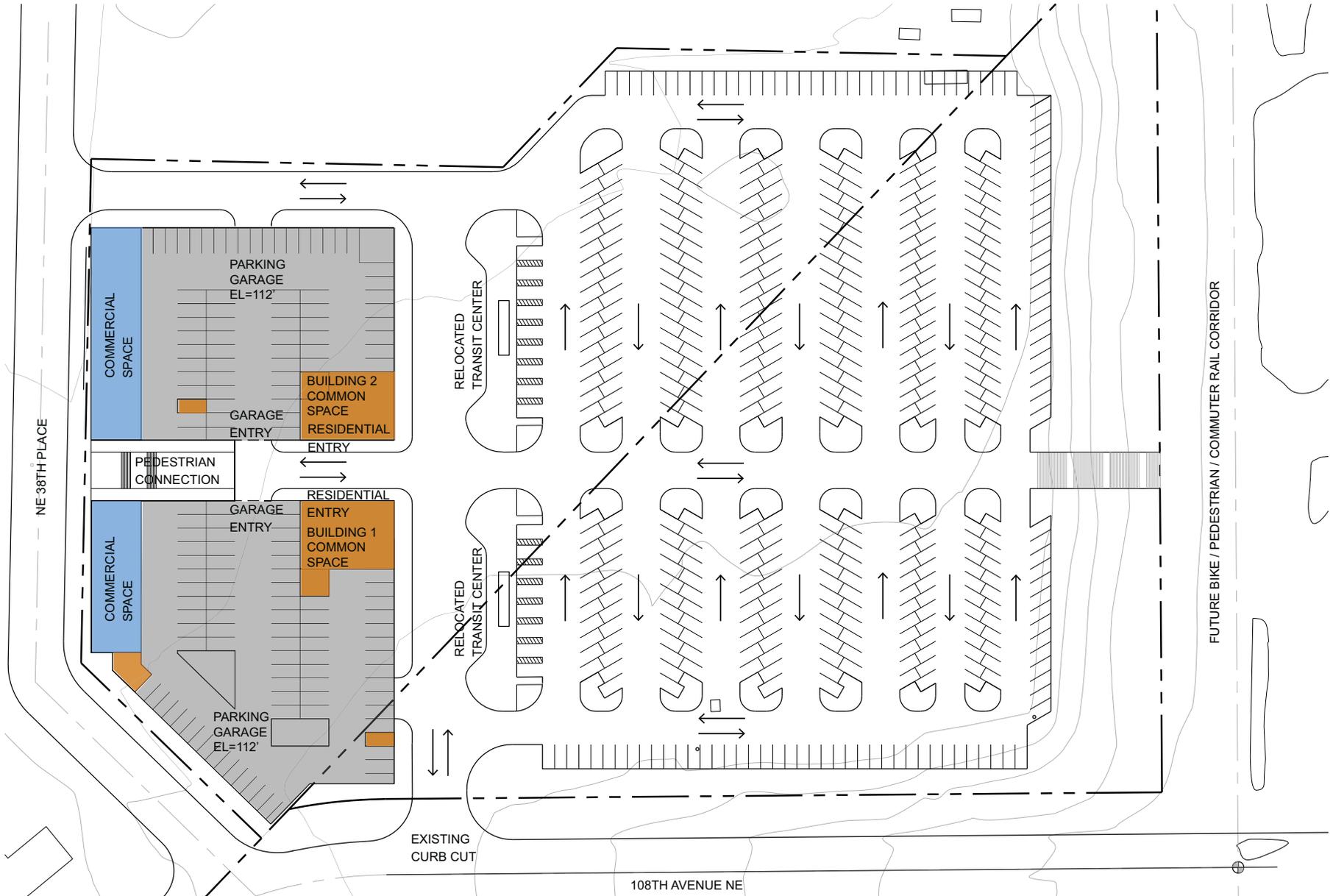


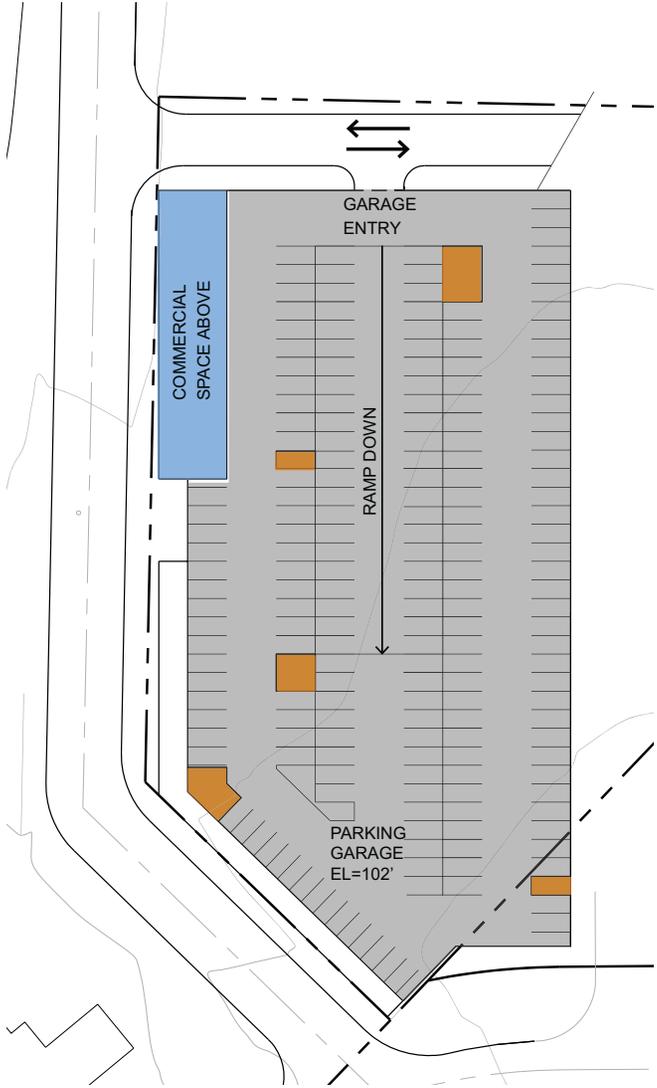
**Study:**

Transit oriented development providing additional parking for South Kirkland Park and Ride to support the increased demand resulting from tolling on the SR520 bridge. The study considers the 3.65 acre Kirkland side of the 6.97 acre site.

**Study highlights:**

- High density residential housing with affordable units
- Market supported mix of commercial spaces
- Underground shared parking to support new TOD development and increased park and ride capacity
- Transit center that encourages alternative modes of transportation utilizing pedestrian and bike friendly design
- Pedestrian friendly connections to future BNSF bike, pedestrian and rail corridor
- High quality design
- Sustainable green building strategies
- Utilize the sites existing topography
- Retain and enhance vegetation buffers
- Maximize public streetscape development potential

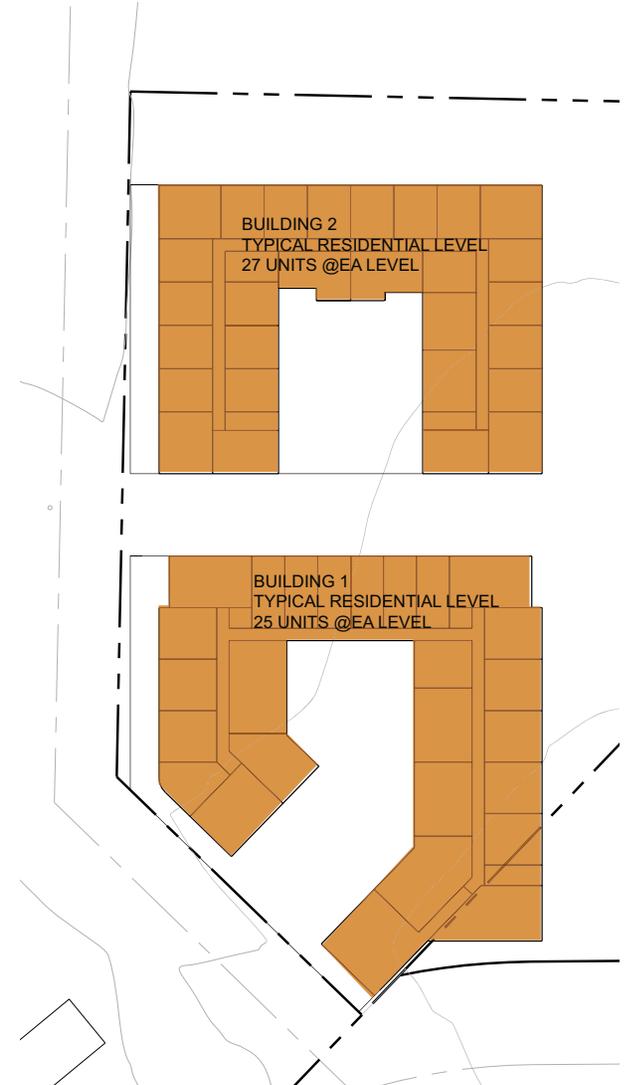




FLOOR PLAN LEVEL P2 EL=102'

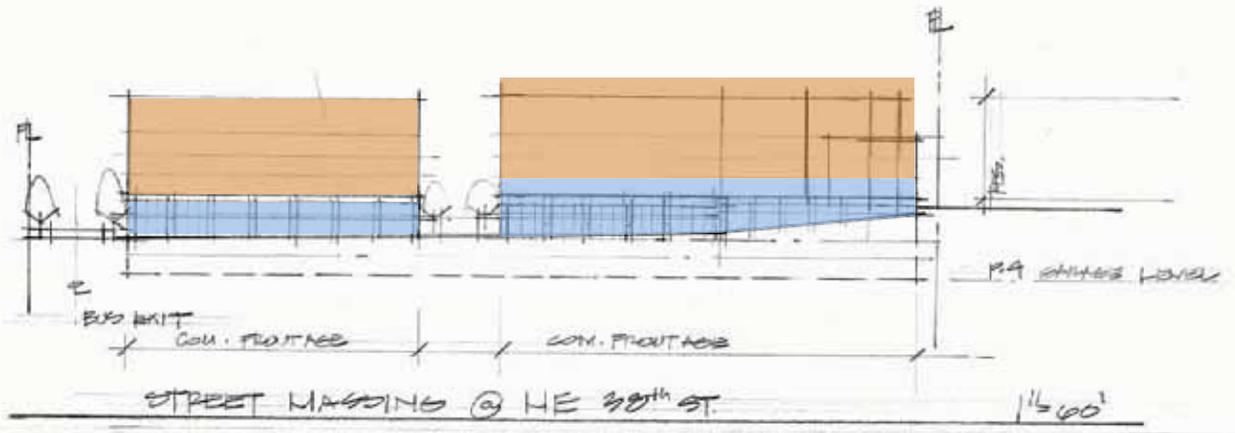
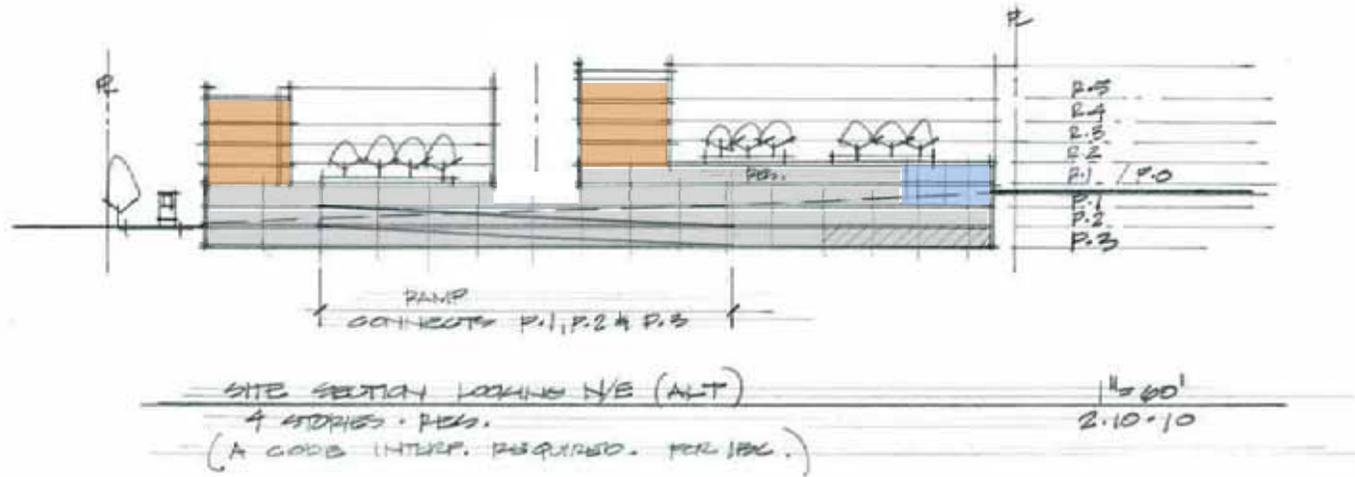


FLOOR PLAN LEVEL P3 EL=92'



FLOOR PLAN TYPICAL RESIDENTIAL LEVELS



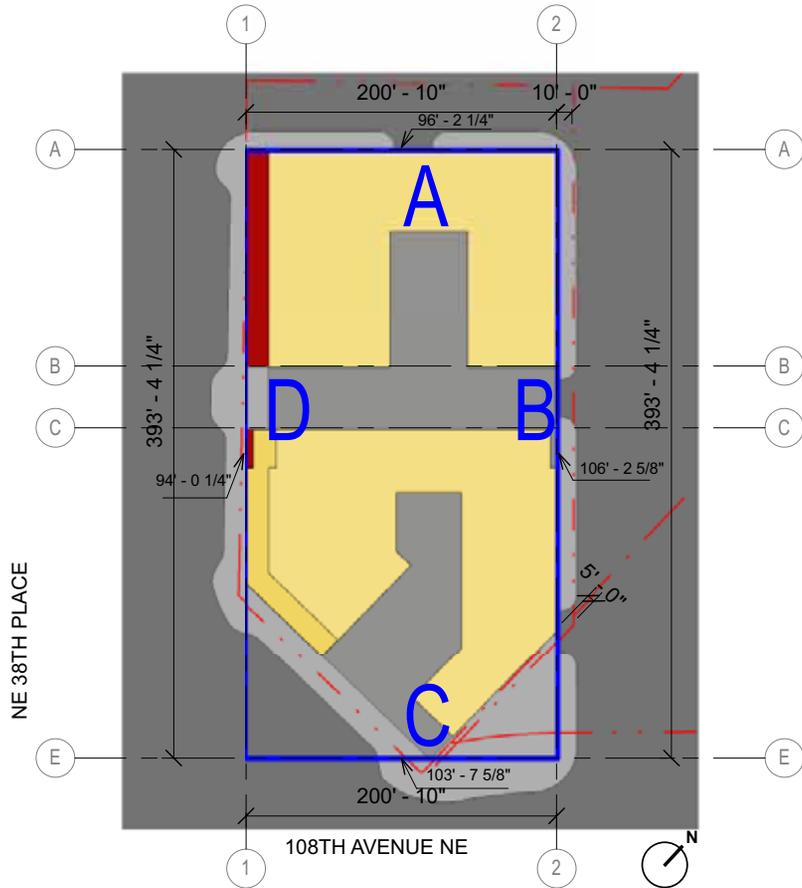




- View impact to existing residences is from existing tree canopy
- No Impact to existing residences views from 70' high building

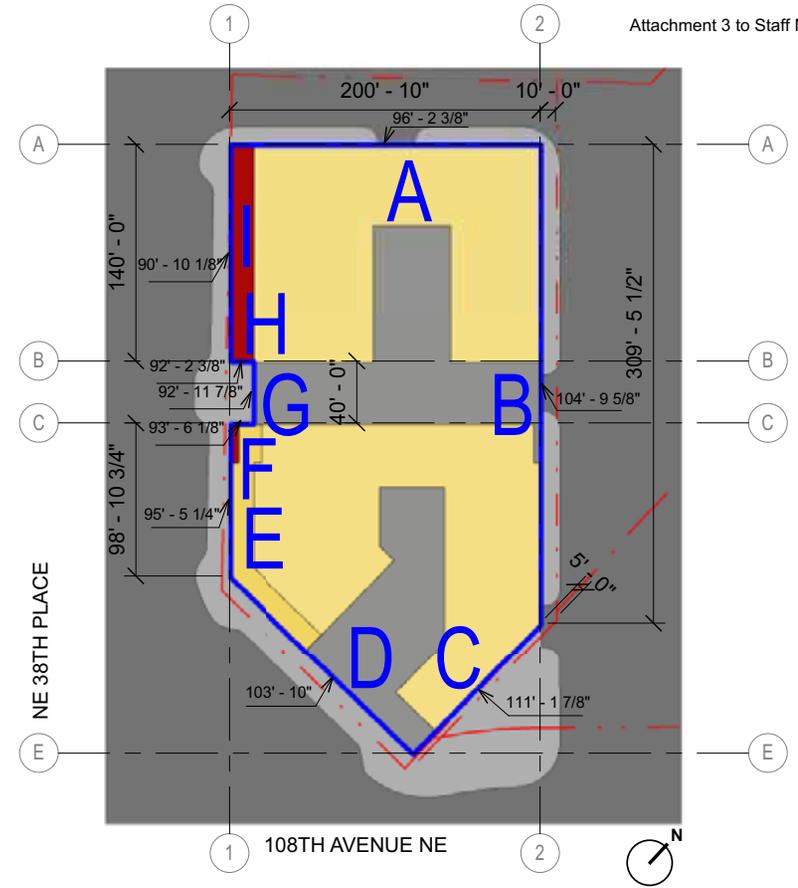
TOD Elements





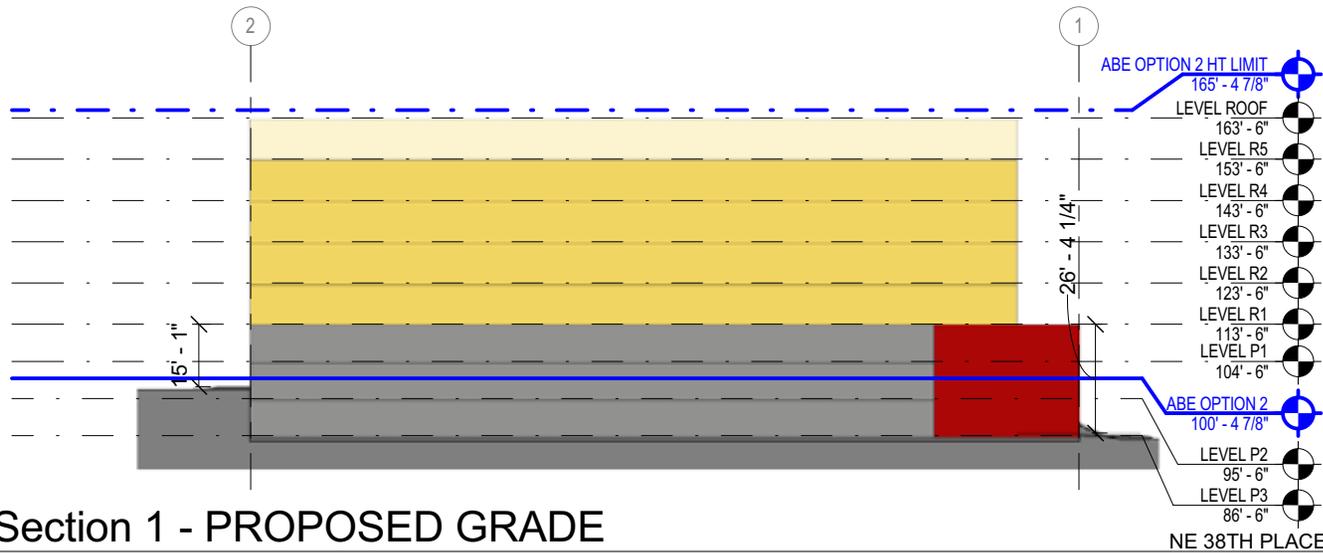
**1 ABE - OPTION 1 EXISTING GRADE**  
1" = 80'-0"

BLDG ELEVATION	MIDPOINT ELEVATION	SEGMENT LENGTH	product	sum	
A	96.188	200.830	19317.336	abe	118895.558
B	106.219	393.354	41781.669	abe	100.049
C	103.635	200.830	20813.017	max ht elevation	100.049'
D	94.021	393.354	36983.536		165.049'

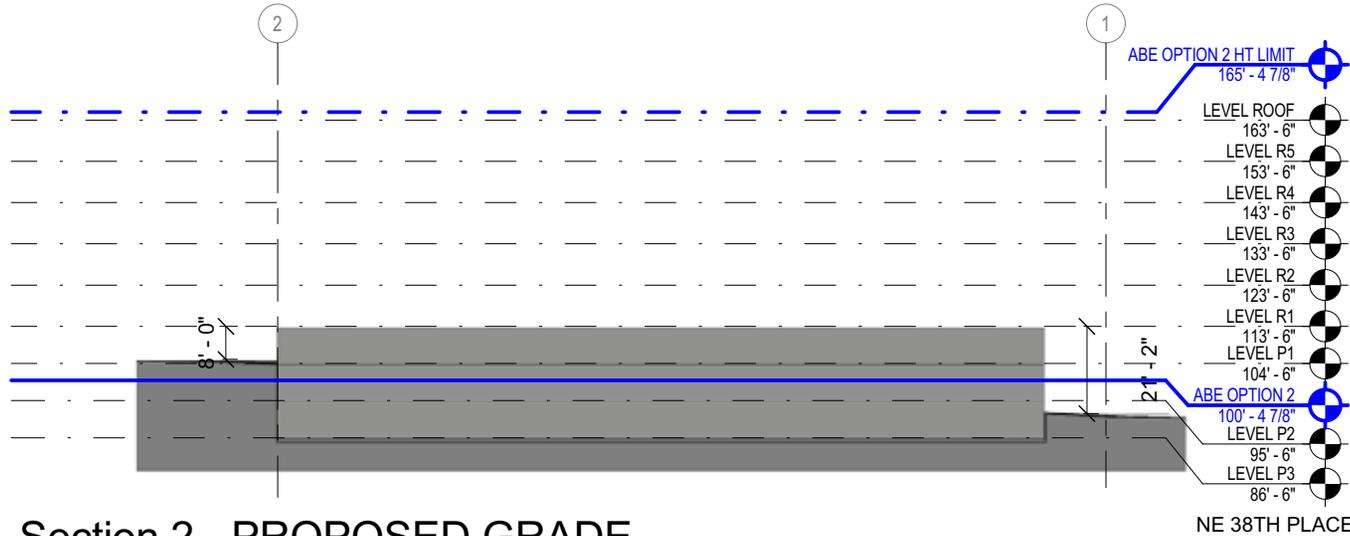


**2 ABE - OPTION 2 EXISTING GRADE**  
1" = 80'-0"

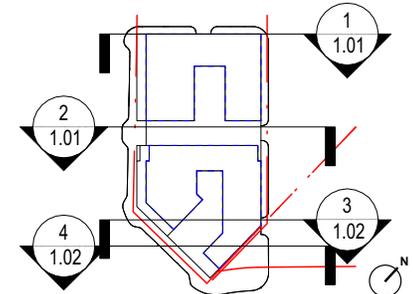
BLDG ELEVATION	MIDPOINT ELEVATION	SEGMENT LENGTH	product	sum	
A	96.198	200.830	19319.444	abe	110636.436
B	104.802	309.458	32431.817	abe	100.408
C	111.156	117.958	13111.739	max ht elevation	100.408'
D	103.833	165.395	17173.459		165.408'
E	95.438	98.896	9438.436		
F	93.510	14.667	1371.511		
G	92.990	40.000	3719.600		
H	92.198	14.667	1352.268		
I	90.844	140.000	12718.160		

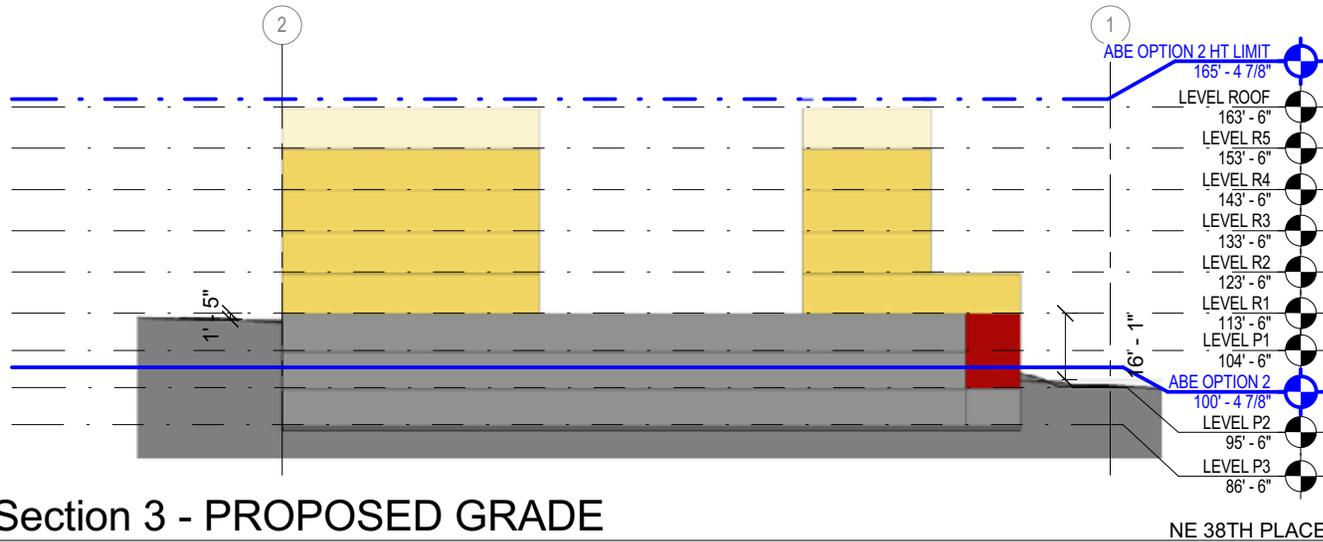


**1** Section 1 - PROPOSED GRADE  
1" = 30'-0"

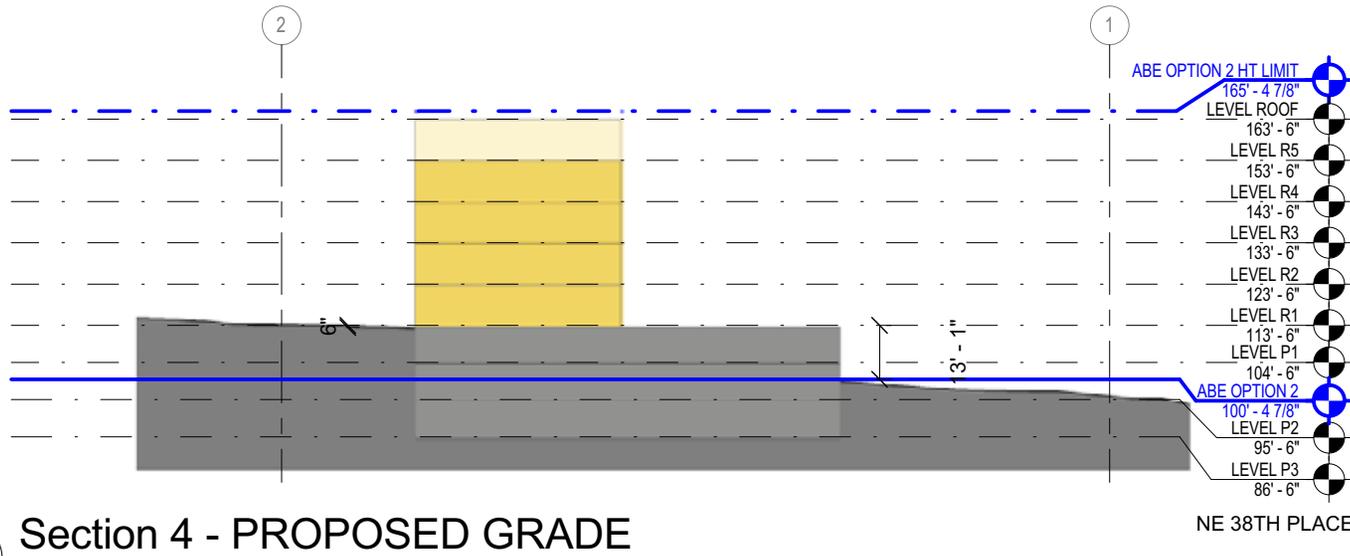


**2** Section 2 - PROPOSED GRADE  
1" = 30'-0"

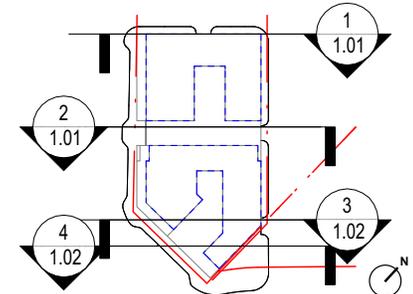


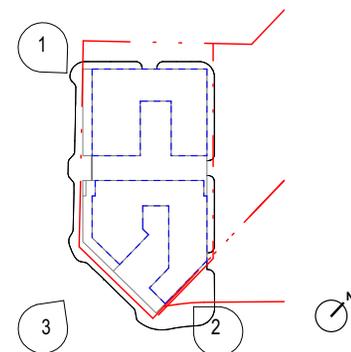
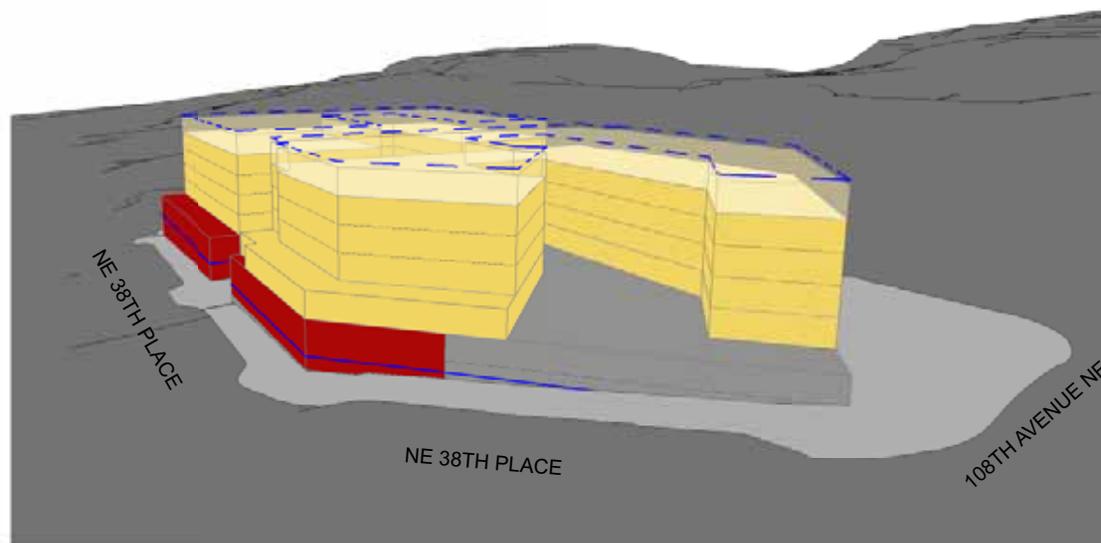
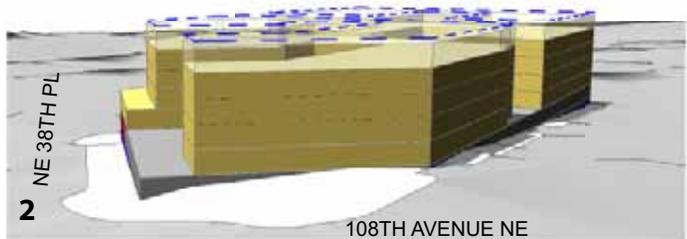
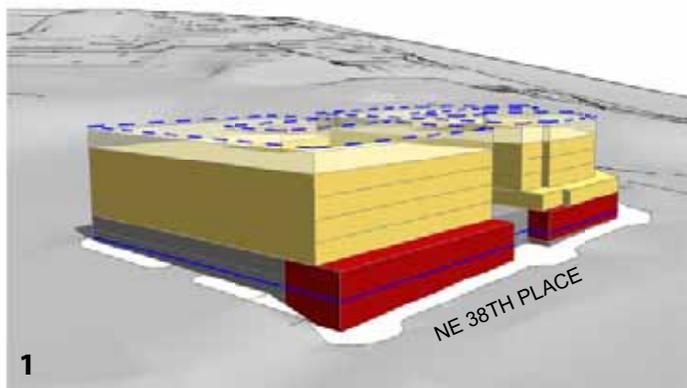


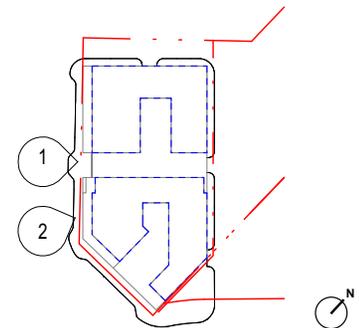
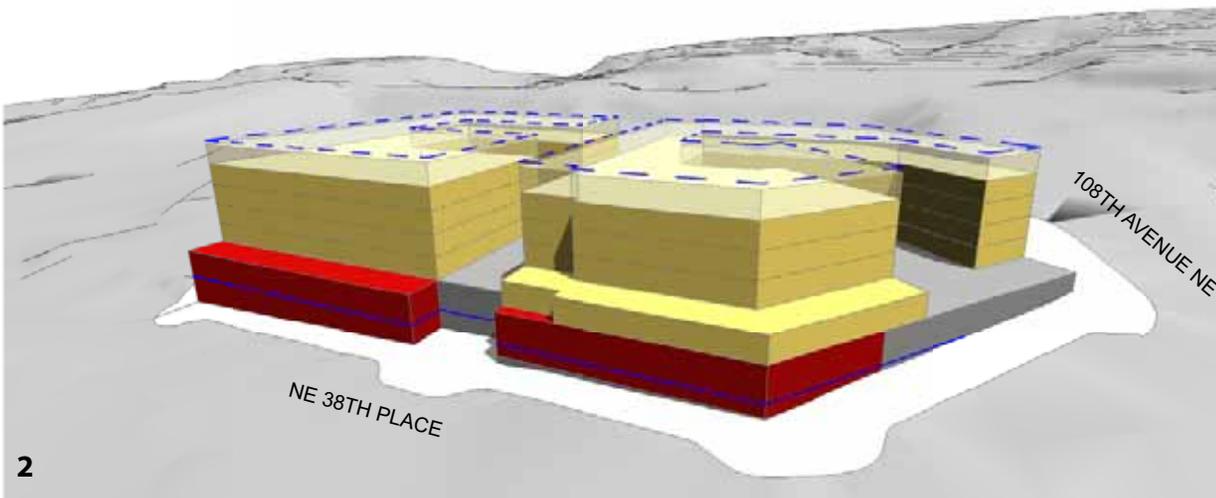
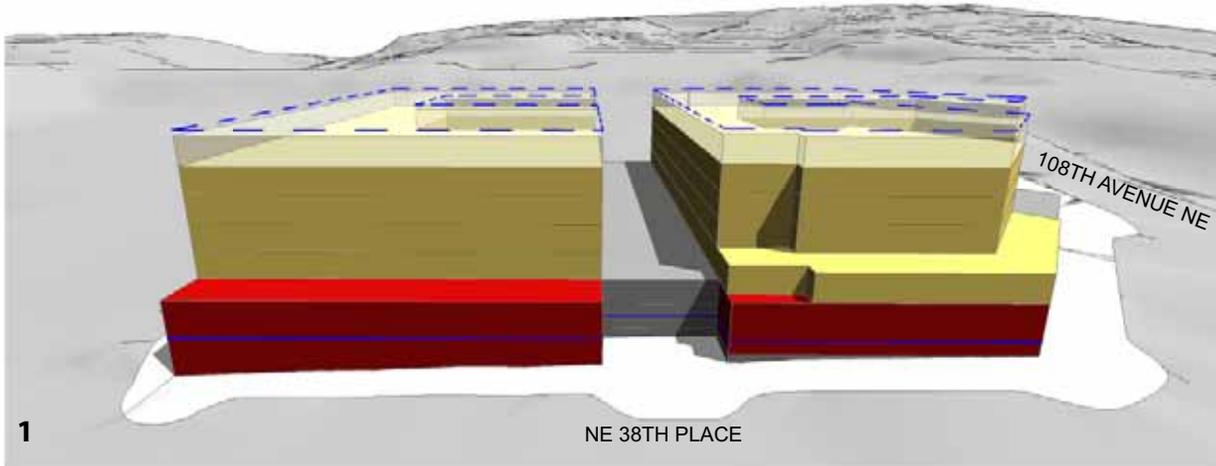
**3** Section 3 - PROPOSED GRADE  
1" = 30'-0"



**4** Section 4 - PROPOSED GRADE  
1" = 30'-0"







King County

City of Bellevue

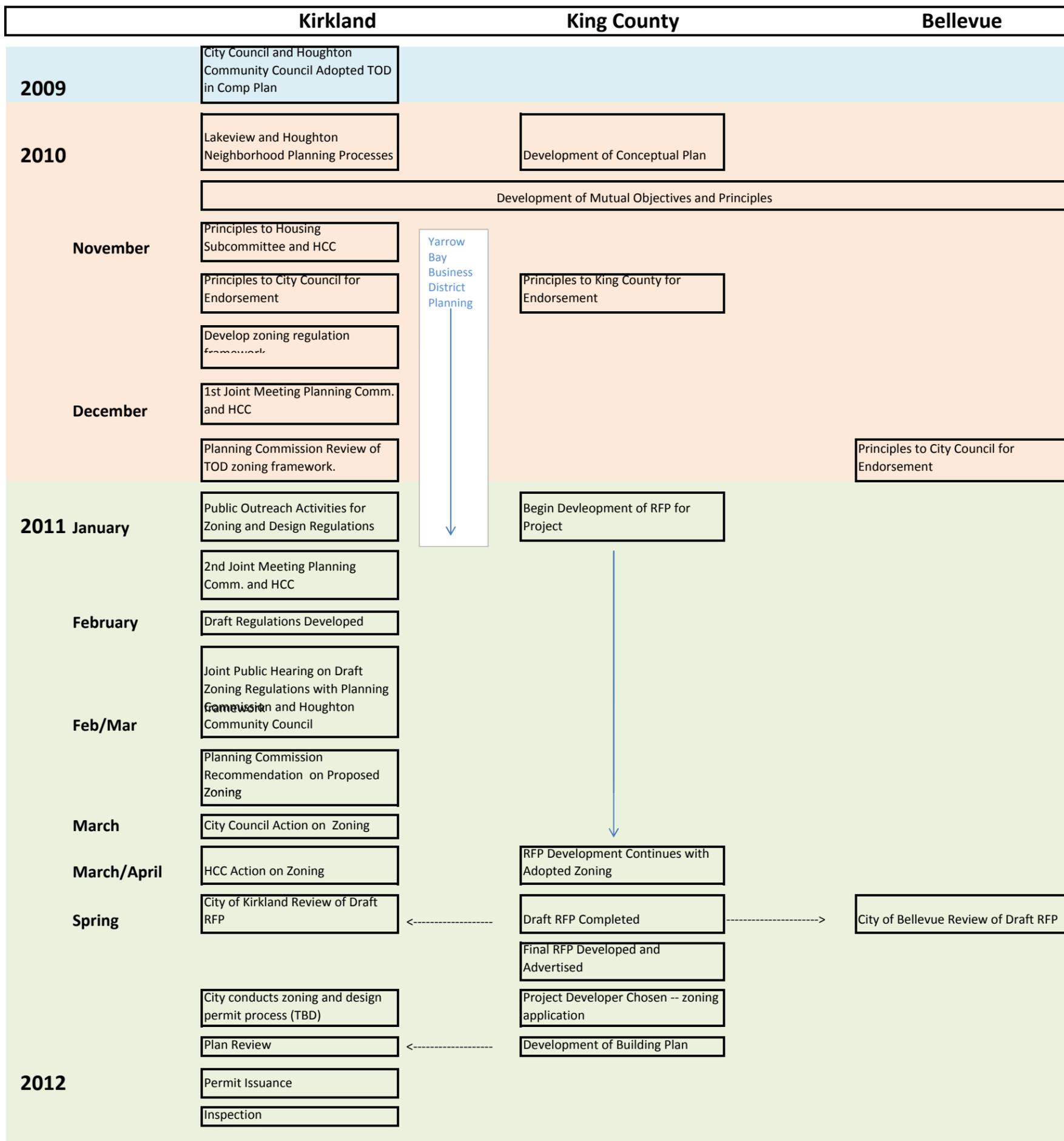
City of Kirkland

**Mutual Objectives and Principles of Agreement**  
**for the South Kirkland Park and Ride Transit Oriented Development Project**

- Expand park and ride capacity. Add a significant number of parking spaces for transit riders at the South Kirkland Park and Ride, to better serve Bellevue and Kirkland residents and encourage higher transit ridership. Promote shared use parking between residents and Park and Ride users. Improve transit facility and provide vehicle charging stations as funding is available. Preserve the park and ride as a long term use of the property for transit riders.
- Local services. Incorporate ground floor commercial space into the housing project design to provide opportunities for businesses that support transit riders, residents and surrounding activities. Add TOD supportive services in the adjacent area through neighborhood planning.
- Timing. Proceed with the project in a timeframe that protects the existing FTA funding available for the park and ride expansion.
- Feasibility. Allow for a financially feasible project.
- Coordination. Coordinate among Bellevue, Kirkland, and King County Metro Transit to develop an appropriate permit review and inspection process that is efficient and avoids conflict and redundancy to the extent practical and consistent with the goals of the project.
- Attractive and compatible site development. Incorporate high quality design standards. Develop an attractive site and building complex that is compatible with the surrounding area and provides a welcoming gateway to both cities in this location. As appropriate and feasible, preserve areas of existing landscaped buffers and use green building techniques. Provide a safe and secure facility.
- Range of housing affordability. Ensure that housing on the site includes a range of affordability, including market rate housing. It is expected that a majority of the housing will be market rate, while a significant share will be affordable at moderate and/or lower income levels with some units that are accessible to those with disabilities.
- Impact mitigation. Minimize and mitigate traffic and other impacts of the development, including impacts of the SR-520 project. Encourage alternative modes of transportation, including transit, bicycling and walking.

- Construction impacts. Minimize construction impacts on park and ride users and the surrounding area. Coordinate project construction with SR520 construction, to the extent possible.
- Connections to BNSF Corridor. Design to accommodate a future connection to the BNSF corridor.
- Public Involvement. Engage the surrounding community and interested parties in both cities in the planning and review of the proposal. City staff in both cities will collaborate to support outreach efforts.

**City of Kirkland  
South Kirkland Park and Ride  
Draft Decision and Implementation Process**



**Decision:** What zoning regulations should apply to the South Kirkland TOD

**Decision Makers:** City Council and HCC

**Stakeholders:**  
Lakeview residents  
Lakeview businesses  
Houghton residents  
Houghton Businesses  
City of Bellevue  
Cascade Land Conservancy  
King County  
Developers  
ARCH  
Bellevue businesses  
All Kirkland Residents  
Affordable housing advocates

**Decision:** What elements and criteria should be included in the RFP?

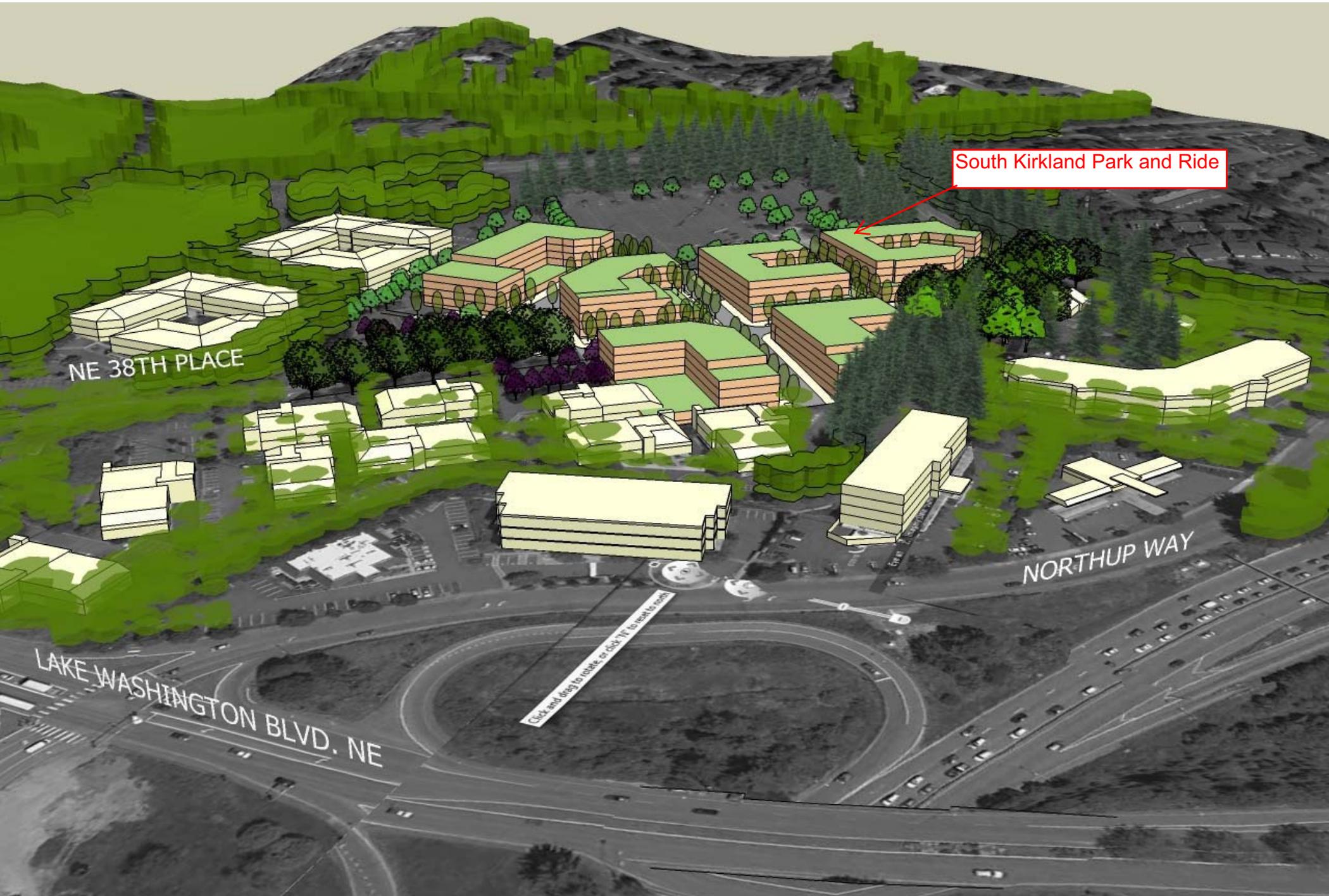
**Decision Maker:** King County

**Stakeholders:**  
Lakeview residents  
Lakeview businesses  
Houghton residents  
Houghton Businesses  
City of Bellevue  
Cascade Land Conservancy  
King County  
Developers  
ARCH  
Bellevue businesses  
City of Kirkland  
Houghton Comm. Council  
Affordable housing advocates

**Decision:** Does the City of Bellevue agree with the mutual objectives?

**Decision Maker:** Bellevue City Council

**Stakeholders:**  
King County  
ARCH  
Bellevue residents  
Bellevue businesses  
City of Kirkland  
Houghton Comm. Council





## **South Kirkland Park and Ride Summary of January 20<sup>th</sup> and January 25<sup>th</sup> Workshop Comments**

### **Design**

- Attractive project and living space
- Have good design qualities (brick, wood siding, offsets, setbacks, incidental breaks, good lighting, pitched roofs, color scheme, building configuration)
- Height (3 stories, lower profile)
- Site is appropriate for high density housing
- Orient towards community; fit into community; respect neighborhood character
- Residential feel
- Not big block (building)
- Address views; non-intrusive; Preserve neighborhood feel (trees, landscaping, green spaces)
- Active and integrate green features (solar, water treatment, lighting, LID, playgrounds, pea patch, roof gardens, recreation.
- Integrate landscape design of Park and Ride with housing
- Have good lighting
- Use full site
- Mitigate Noise (buffers, screening)
- Keep site well buffered
- Mix affordable and market housing in appearance
- Have design review board look at project
- ADA accessibility

### **Housing**

- Minimum affordable housing requirements
- Specify amount; develop minimum number
- Use FAR as density control
- Support affordable housing
- Who are the residents?
- Good livable units
- Could affordable housing be better located elsewhere?

### **Uses**

- Be flexible
- Set realistic minimum amount (but allow market flexibility)
- Ground floor services (dry clean, food service, grocery, drugs, coffee, daycare)
- Starbucks yes, bars no.
- Have supporting services in area & amenities (parks, retail, schools)

### **Parking**

- Adequate parking for all uses (residents, transit users, business)
- Minimize offsite and overflow parking in surrounding neighborhoods
- Keep Park & Ride parking separate from residential
- Attractive design for parking structure (human scale, safe, lighting)
- Address parking during construction
- Include parking for bikes (stalls and lockers) and plug-ins; be realistic about compact spaces
- Address drainage; treat on site
- Include landscaping and trees

### **Traffic and Circulation**

- Ensure ease of access to homes on 108<sup>th</sup>
- Provide a comfortable and safe pedestrian environment; pedestrian safety on arterials (crosswalks, lights, advanced warning)
- Traffic lights/traffic control at P&R entrance
- Address/minimize traffic impacts on Lake Washington Blvd and 108<sup>th</sup>
- Protect neighborhood access
- Have good bike and pedestrian connections
- Good, safe and controlled access to park and ride for busses and vehicles
- Connect to recreation and open space.

### **Management**

- Ensure managers have good track record
- Minimize noise impacts during construction
- Address overflow parking
- Address safety, policing, security, increase patrol
- Have priority for Kirkland employees
- Assure funding
- Charge for parking (free for residents)
- Ensure proper management of residents

### **Other**

- Spread affordable housing throughout city
- Want full transit center
- Master plan site – Phase I in Kirkland & Phase II for Bellevue
- Ensure Bellevue side is attractive and well maintained
- Plan for future Bellevue site; coordinate with Bellevue
- Any changes for bus service?
- Expand parking by purchasing other sites (e.g. WSDOT property)

- Improve transit into Kirkland
- Improve transit connectivity and service (especially with snow)
- Local schools can continue to operate

	Zoning/Design Guidelines	RFP	Project Review/Permitting	Project Management
Parking, Traffic and Circulation	<p>Parking/Traffic – 520 access change Traffic access via on and off ramps.</p> <p>Minimal impact on neighborhood parking. Parking Impacts – Look at neighborhood (Hide &amp; Riders). Parking for Businesses. Need to provide enough parking: Park &amp; Ride, apartments. Parking: adequate Adequate parking for all uses on the site: housing, Park &amp; Ride, retail. Keep Park &amp; Ride and residential parking separate. Over height and handicapped parking.</p> <p>Traffic: neighbors need ease of access to/from homes onto 108<sup>th</sup>. Protect Neighborhood access, pedestrian and vehicle. Pedestrian safety within the site. Comfortable pedestrian environment. Safety: pedestrian walkways and traffic lights.</p> <p>Bike paths - access.</p>	<p>Traffic: Minimum impact on neighborhood parking. Minimize parking in the neighborhood (good circulation). Address overflow parking. Resident parking underneath housing units. Parking: Adequate for transit users and housing residents. Be realistic about the number of compact parking spaces. If less residential parking, then increase transit.</p> <p>Parking: safety with lighting. Parking structure, human scale and comfortable</p> <p>Timing of construction, impact on commuters Parking during construction.</p> <p>Pedestrian safety on arterial streets: crosswalk, lights, advanced warning. Create, connection for pedestrians and bikes that is pleasant and safe (ERC) within/outside site</p> <p>Bicycle storage. Improved bicycle paths and storage.</p>	<p>Minimize traffic impacts (108th &amp; Lake Washington Boulevard). Traffic control at the Park &amp; Ride entrance. Improve access in and out of Park &amp; Ride: 38<sup>th</sup>, 37<sup>th</sup> and Lake Washington Boulevard Bus routes, traffic light (on 108<sup>th</sup>/38<sup>th</sup>). Concern about traffic on 108<sup>th</sup>.</p> <p>Parking: Construction parking where? Parking: adequate. Parking – not enough now</p> <p>School impact – buses.</p> <p>Parking – neighborhood permits and during construction.</p> <p>Parking: electric plug in, bike stalls and lockers. Parking: drainage treated on site.</p> <p>Proper, clearer, transportation study.</p>	<p>Security in the parking structure. Good lighting, look, feel and be safe.</p> <p>Parking: Charge for parking (free for residents).</p>
Design/Character	<p>Make project attractive and inviting for users and residents. Visual – Brick. Visual: offset/setback of buildings like Bank of America. Orient toward existing community. Area: Holistic design to Lake Washington Boulevard How are the buildings configured? (Design) Have the look of the buildings in the neighborhood Integration: To fit in with the neighborhood Respect Neighborhood character. Should not look like a big block: Include playgrounds, village ambiance, roof garden, pea patch. Intermixture of affordable and market units, at least in appearance. Integrate landscape design of Park &amp; Ride with housing.</p>	<p>Visual: emphasize residential not commercial: wood siding, pitched roof, trees. Lower profile buildings. Integrate trees into site. Connect to recreation and open space. Attractive living space. Visual: Community garden, pea patch.</p> <p>Design: Green features – solar &amp; rapid ride, water treatment, lighting (high efficiency). Green development.</p> <p>Protect children around windows – pyramid structure.</p>	<p>ADA accessibility. Low impact development techniques (reduce impacts). Consideration of conservation principles (runoff, green roofs etc.)</p>	

	<p>Preserve look/feel of Neighborhood: Trees, landscaping, storm water management, water quality.          Urban forestry/landscaping.          Active and integrated green spaces.</p> <p>View: Site surrounded by trees (businesses to be seen).          View – Hillside = Non-obstructive          View Impacts: Keep site well buffered, site is appropriate for high density building.</p> <p>Zoning: Building Height – incidental breaks, color scheme, less than three stories.          Height of buildings and views, look like single family housing.          Building height.</p> <p>Use FAR as density control (more flexible).</p> <p>Design guidelines – Prefers Design Review Board to look at the project</p> <p>Parking structure: human scale, safety, and aesthetics.          Good lighting (safe environment).          Parking lot has landscaping.          Mitigate noise – buffers/screening, landscaping.</p> <p>Design options – Use full site including Bellevue.</p>			
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	Zoning/Design Guidelines	RFP	Project Review/Permitting	Project Management
Affordable Housing	Minimum affordable requirements. Develop threshold of minimum number, from approval standpoint.	Minimum affordable housing. Specify the amount of affordable housing for a viable project Support affordable housing. Who are the affordable housing residents (senior, young)? Affordable housing units have good core structure (thick enough walls for sound, etc.). .		Priority to workers within Kirkland for housing.
Uses/Services	Allow flexibility so private sector can be creative and package market demands. Set a realistic minimum amount of retail but allow flexibility in location and market demands. Consider job/housing balance. Ground Floor Services. Supporting services (parks, retail, school)	Create on-site recreation for apartments: Passive – Sitting, Active – Pool. Help make this a vibrant/interesting place to live: Retail-housing mix, look for good examples; plazas, meeting space, art, 24 hour character. Improved retail uses for both neighbors and commuters, i.e. Starbucks yes, bars no Public restrooms. Businesses need to be visible (Parking on P&R side). Provide services. Central Services: dry cleaner, food service, grocery store, drug/notions, coffee, daycare. What amenities (services) fit in with the neighborhood Local availability of services (daycare).  Possible live/work units.	Safe school access – protect perimeters. Local schools can continue operating	
Ongoing Implementation		Assurance project can be funded – financing  Coordination between projects to maximize transit access, 520 and the Park & Ride	Research – who is going to manage site, make sure they have good track record and related to housing type.  Noise impacts during construction.	Address overflow parking (in management).  Safety: Community oriented policing. Make the project an asset to the neighborhood: Crime prevention. Safety: Increase patrol, Kirkland & Bellevue Police. Parking Safety: Security.  How to ensure proper management of residents?
<b>Parking Lot:</b>	Spread Affordable Housing through the City Could affordable housing be better located elsewhere?			

Would like a full Transit Center.

Area: Master Plan – Phase I: Kirkland, Phase II: Bellevue.

Ensure that Bellevue side is attractive well maintained and managed.

Planning for future Bellevue site.

Area: Coordinate with Bellevue, important for success (at the table now).

520 project design.

Bus service: Any changes? Flyer?

Expanded parking by purchasing/building more (WSDOT property).

Improved transit into Kirkland.

Snow route bus service improvements so Park & Ride will remain usable on snow days (buses stop there).

Improved connectivity between transit (especially with snow).

Public Comments  
South Kirkland Park and Ride TOD  
File ZON10-00014

*Keith Maehlum  
10836 NE 108<sup>th</sup> Street  
Kirkland, WA 98033*

February 3, 2011

CITY OF KIRKLAND  
PLANNING DEPARTMENT

RE: LAKEVIEW NEIGHBORHOOD TOD PROPOSAL

**I strongly support the TOD proposal being considered by the City.**

I have been a fourteen (14) year resident and business owner in Kirkland, having also lived and worked in downtown Kirkland for many years.

I am also an original member of the Downtown Action Team for the Kirkland Downtown Strategic Plan and have been involved in almost all of the City's major land use discussions for the past 22 years.

The City has undertaken an extensive community outreach program and has incorporated many elements resulting from that outreach. They have been responsible and responsive.

The project not only is consistent with the vision of the smart growth but exceeds the expectations we had for this property to make this area pedestrian friendly, economically vibrant and market responsive. For that they should be commended.

The Lakeview neighborhood continues to struggle and suffer from the lack of critical mass and market significance. This proposal will help to address those current shortcomings.

More importantly, the TOD redevelopment is forward thinking. Urban Land Institute's new book "Growing Cooler" documents what will happen with our climate if we don't redevelop smart. If we follow a low density redevelopment approach CO2 emissions will continue to grow excessively.

With dense mixed-use compact development ULI's book shows that vehicle-miles-traveled ("VMT") moderate. The denser we develop, the lower the VMT. The lower the VMT, the lower the CO2 emissions.

Please do the right thing for the environment and approve the proposed TOD project.

Thanks You – Keith Maehlum

Dear Editor:

The South Kirkland Park & Ride needs to be expanded, and it is a perfect place for Transit Oriented Development (TOD).

Many may not realize how important buses have become. Metro now has over one hundred million boardings a year, that's an average of over 50 for every man, woman and child in King County! Buses carry fifty percent of rush hour commuters into downtown Seattle: there would be total gridlock without them! The corresponding number for Bellevue now exceeds 20%. As our population grows, access to an expanding transit system will be an even more essential part of a sustainable future. For this, suburban cities such as ours will continue to need park and ride lots.

The TOD proposed for the South Kirkland Park & Ride will expand access to transit both by providing housing on site, and by significantly increasing the number of existing stalls beyond that needed for the housing. It will provide housing choices, including units that are affordable to lower income people such as retail clerks, teachers, and perhaps some of our own children, who are forced to commute long distances now. Expansion of the existing stalls will relieve overflow parking in the neighborhoods, and loss of access for many potential riders.

With excellent freeway and rail access, and a topography that will support increased density and height, it is difficult to imagine a better location for this type of development.

The City is working through a process to address neighborhood concerns. I hope the focus will not be on minimizing inconvenience and preserving the status quo. It should be about building a future with viable transportation choices for all.

Dave Russell

## Related Articles:

- [Kirkland Hosts Second Community Meeting about Transit-Oriented Development at the South Kirkland Park & Ride](#)
- [City Council advances Transit Oriented Development despite objections from neighborhoods](#)
- [Letter | Market Neighborhood Meeting of January 19, 2011](#)
- [Kirkland Hosts Community Meetings about Zoning Regulations for South Kirkland Park & Ride](#)
- [Letter | Questions regarding the South Kirkland Park & Ride plans](#)

**Dorian Collins**

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**From:** georgine foster [georginef@msn.com]  
**Sent:** Friday, January 28, 2011 2:32 PM  
**To:** Janice Coogan  
**Cc:** Dorian Collins; Paul Stewart  
**Subject:** seattletimes.com: Click to share a ride and toll on the 520

Janice, would you please Forward this Seattle Times article to the Houghton Community Council, City Council, and Planning Commission.....I think it illustrates that King County and the City of Kirkland are moving much TOO FAST regarding the South Kirkland Park & Ride.....There is going to be such HIGH demand for parking once tolling starts that there must be PLANNING for MORE parking (than just 250 additional stalls).....and SOON.

Thank you.

Click to share a ride and toll on the 520

Just before tolls return to the old Highway 520 bridge this spring, the state government and a tech firm are trying to create a new form of ride sharing in the crowded corridor.

[http://seattletimes.nwsources.com/html/localnews/2014060417\\_eslugging28m.html](http://seattletimes.nwsources.com/html/localnews/2014060417_eslugging28m.html)

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[www.seattletimes.com](http://www.seattletimes.com)

## The Seattle Times

Winner of a 2010 Pulitzer Prize

Originally published January 27, 2011 at 9:33 PM | Page modified January 28, 2011 at 12:00 PM

### Click to share a ride and toll on the 520

Just before tolls return to the old Highway 520 bridge this spring, the state government and a tech firm are trying to create a new form of ride sharing in the crowded corridor.

By Mike Lindblom

Seattle Times transportation reporter

Just before tolls return to the old Highway 520 bridge this spring, the state government and a tech firm are trying to create a new form of ride sharing in the crowded corridor.

Using a smartphone, drivers can match up with riders at busy hubs such as Husky Stadium or Eastside park-and-ride lots. That way, they can travel in the high-occupancy-vehicle lanes, as well as share toll or gasoline costs.

The concept is similar to slugging — the custom in Washington, D.C. and the San Francisco Bay Area of motorists who pick up strangers en route to work, in hopes of driving quickly in the HOV lanes.

But while riders in those cities essentially hitchhike from park-and-ride lots or bus stops, local riders will send out an electronic beacon on their smartphones.

You might call the Seattle experiment "e-slugging."

Avego, the company providing the software, prefers the phrase "real-time ride sharing." Its go520 program enrolls a finite community of users, whose driving and criminal records are screened beforehand.

When a rider presses "Get a Ride" on the phone display, nearby drivers see or hear that request, then press an icon to claim the passenger. The passenger sees an image of the driver's car type, such as a silver Volvo, and the driver's rating of one to five stars, based on overall impressions by past riders.

When the rendezvous occurs, the driver logs a personal identification number that confirms the trip.

Prime locations include Seattle Children's hospital and Husky Stadium, as well as the Houghton, South Kirkland and Bear Creek park-and-ride lots; Capitol Hill and the Microsoft campus in Redmond are coming soon, said James Donovan, Avego's local project manager.

State lawmakers in 2009 authorized a test project to boost carpooling, so the Department of Transportation (DOT) is spending \$400,000 to subsidize this year's test run, designed for up to 250 drivers and 750 riders.

Even at that level, instant ride sharing would barely affect the bridge's 115,000 daily car trips — while tolling itself would cause about one-fifth of drivers to choose other routes, take transit or not travel, the state's own studies predict.

The state DOT plans to launch tolls that vary by time of day, peaking at \$3.50, in April. (The tolls still require legislative approval, due to the recent passage of Tim Eyman's Initiative 1053.)

Participants in the ride-sharing test are paid up to \$30 a month. Before the official launch Thursday, there were only a small group of closely watched drivers signed up.

They've been picking up virtual "ghost riders" since December, as Avego fine-tunes the system, Donovan said.

Josh Kavanagh, transportation director at UW, is helping with recruitment, saying it's compatible with UW's culture of innovation.

The 520 corridor presents certain obstacles to e-slugging.

One is the difficulty of losing commute minutes trying to re-enter the mainline after grabbing a passenger.

Donovan replies the driver and rider often will begin a trip from the same spot, such as Husky Stadium. Perhaps they just got off work in the University of Washington Medical Center, across the street, at the same time.

Another is the requirement of three people to use the HOV lanes near the east shoreline. For that reason, the new technology is being marketed to existing carpools and van pools, Donovan said.

Thirdly, frequent and increasing bus services, including the private Microsoft Connector, serves the Highway 520 corridor. Would instant ride sharing really be easier?

"We're not bound by time and we're not bound by schedules. We're bound by availability," Donovan said.

Some people would use both modes, he acknowledges. A phone-wielding transit rider might learn from the One Bus Away app that his bus is running late, then click over to the go520 app. Donovan said any profit Avego makes won't come off the six-month test, but through future phases or ventures.

"Our hope is that a thousand people, they tell another thousand. It's a viral thing, that's what we're hoping."

*Mike Lindblom: 206-515-5631 or mlindblom@seattletimes.com*

**Dorian Collins**

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**From:** Janet Jonson  
**Sent:** Thursday, January 27, 2011 3:34 PM  
**To:** City Council  
**Cc:** Kurt Triplett; Marilynne Beard; Eric Shields; Paul Stewart; Janice Coogan; Dorian Collins; Ray Steiger; David Godfrey; Kari Page; Ellen Miller-Wolfe  
**Subject:** FW: resident of MNA who e-mailed me regarding the SKP&R TOD

Janet Jonson  
City Manager's Office  
City of Kirkland  
123 5th Avenue  
Kirkland, WA 98033  
425 587 3007  
425 587 3010 fax  
jjonsong@ci.kirkland.wa.us

**From:** Michelle Sailor [mailto:msailor@comcast.net]  
**Sent:** Thursday, January 27, 2011 3:10 PM  
**To:** Janet Jonson  
**Subject:** Re: resident of MNA who e-mailed me regarding the SKP&R TOD

Thanks. Another resident said the more he thought about it, the better he thought the project was as long as Kirkland doesn't have to put money towards it. He is very knowledgeable about real estate development so I value his opinion too.

Michelle

Sent from my iPhone please excuse the brevity.

**From:** Michelle Sailor [mailto:msailor@comcast.net]  
**Posted At:** Tuesday, January 25, 2011 7:05 PM  
**Posted To:** Kirkland Council  
**Conversation:** resident of MNA who e-mailed me regarding the SKP&R TOD  
**Subject:** resident of MNA who e-mailed me regarding the SKP&R TOD

Hello,

I wanted to share the e-mails that were sent to me on Sunday, Feb. 23rd from a resident who I normally don't hear from and when I do it is quite thoughtful. I have copied and pasted them without altering except to remove his name (asked if I could share comments but didn't specify using his name so I have removed it). He is going to try to go to the meeting tonight but in case he doesn't I wanted to make you aware of his questions. I apologize for not getting it to you sooner but for those who have smart phones maybe it is not too late to see if some of the questions and concerns can be addressed. His comment from 3<sup>rd</sup> e-mail - *I don't fancy myself a conspiracy theorist but it makes me wonder whether the goal is additional parking or whether*

*the goal is real estate development and additional parking is just a convenient excuse to justify variations to the building codes, etc.?* - is a recurring theme that I have been hearing from others. I hope you will consider these points when developing your strategy.

Thanks,

Michelle Sailor

MNA Chair

**1<sup>st</sup> email**

Hi Michelle.

Many thanks for your notes on the meeting. I read the points with great interest, in part because I've been commuting by bus from Kirkland to downtown Seattle for 6 years.

I have a few thoughts and observations on the discussion but, so far, no particular opinion on the correct conclusion to draw with regard to the proposal for adding parking spaces and, possibly, adjacent housing to South Kirkland Park & Ride (SKP&R). I'd like to discuss these points with you before you post them to the wider group, if possible.

For my thinking I like to make a distinction between

- the objective of additional parking spaces at SKP&R, and
- the development of adjacent property as one possible implementation.

The first question I have is how much DOES it cost to add 15% more parking spots to SKP&R? There seems to be no discussion of this in the materials and links as far as I can tell.

Developing housing adjacent to SKP&R is only necessary if

(a) \$6.25 million funding from the Department of Transportation's Urban Partnership Agreement cannot cover the cost of adding the parking spaces (and other transit improvements as yet unspecified) to create a "Sustainable Transportation Hub", and

(b) The only other solution is to provide incentives to a real estate developer to help cover the cost. The incentives seem to amount to changing existing building codes to open up development and make development sufficiently profitable. The expense to Kirkland in this scenario is supporting this development (as Mr. Style points out in his email below).

Are there any additional options for funding the necessary work? If the cost of the project could be estimated then additional options could be contemplated.

For example, it could very possibly cost the city of Kirkland less to fund the additional costs of adding parking spaces to SKP&R (i.e., above the \$6.25 million grant) than it would to support the additional infrastructure of 200 new housing units for the next ten years. If we can quantify the cost to support 200 new housing units (for some reasonable period of time) then an informed cost/benefit analysis could be made.

The most significant and immediate beneficiary of developing the land adjacent to SKP&R are real estate developers. Expect them to advocate emphatically for developing the land as the only viable alternative.

It was asserted Kirkland residents would not benefit from the additional parking at SKP&R and that Bellevue residents would. Is there any data to quantify who is currently using the SKP&R?

Regards,

**2<sup>nd</sup> email**

Side-issues/perspectives:

As you'll gather from my email I'm wondering if there may be some energy on the parking spaces topic being put into side-issues without addressing the most important core questions. It would seem only reasonable for the City of Kirkland to have asked and have answers to the cost trade-off questions I'm posing. We should expect answers at the ready for these cost estimates (i.e., estimated cost to construct 250 additional parking spaces and estimated cost to support 200 new housing units for x years). If not, it would seem almost negligent. Perhaps we should pose

these questions to the City of Kirkland right away and see what we learn? These contacts (below) for Kirkland and King County appear on the Kirkland web site for the SKP&R project ([http://www.ci.kirkland.wa.us/depart/Planning/Code\\_Updates/TOD.htm](http://www.ci.kirkland.wa.us/depart/Planning/Code_Updates/TOD.htm)). Perhaps you know of more appropriate contacts?

**3<sup>rd</sup> e-mail**

Please forgive all the emails today. Another discussion point...

What is the objective number of additional parking spaces?

I was under the impression it was 250 but this passage from the Kirkland web page describes including some parking for the 200 additional housing units as well.

Approximately 250 additional parking stalls (some to be shared between the site's residents and transit riders).

(from [http://www.ci.kirkland.wa.us/depart/Planning/Code\\_Updates/TOD.htm](http://www.ci.kirkland.wa.us/depart/Planning/Code_Updates/TOD.htm))

I don't fancy myself a conspiracy theorist but it makes me wonder whether the goal is additional parking or whether the goal is real estate development and additional parking is just a convenient excuse to justify variations to the building codes, etc.?

**Dorian Collins**

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**From:** Janet Jonson  
**Sent:** Thursday, January 27, 2011 3:34 PM  
**To:** City Council  
**Cc:** Kurt Triplett; Marilynne Beard; Eric Shields; Paul Stewart; Janice Coogan; Dorian Collins; Ray Steiger; David Godfrey; Kari Page; Ellen Miller-Wolfe  
**Subject:** FW: resident of MNA who e-mailed me regarding the SKP&R TOD

Janet Jonson  
City Manager's Office  
City of Kirkland  
123 5th Avenue  
Kirkland, WA 98033  
425 587 3007  
425 587 3019 fax  
jjonson@ci.kirkland.wa.us

**From:** Michelle Sailor [mailto:msailor@comcast.net]  
**Sent:** Thursday, January 27, 2011 3:10 PM  
**To:** Janet Jonson  
**Subject:** Re: resident of MNA who e-mailed me regarding the SKP&R TOD

Thanks. Another resident said the more he thought about it, the better he thought the project was as long as Kirkland doesn't have to put money towards it. He is very knowledgeable about real estate development so I value his opinion too.

Michelle

Sent from my iPhone please excuse the brevity.

**From:** Michelle Sailor [mailto:msailor@comcast.net]  
**Posted At:** Tuesday, January 25, 2011 7:05 PM  
**Posted To:** Kirkland Council  
**Conversation:** resident of MNA who e-mailed me regarding the SKP&R TOD  
**Subject:** resident of MNA who e-mailed me regarding the SKP&R TOD

Hello,

I wanted to share the e-mails that were sent to me on Sunday, Feb. 23rd from a resident who I normally don't hear from and when I do it is quite thoughtful. I have copied and pasted them without altering except to remove his name (asked if I could share comments but didn't specify using his name so I have removed it). He is going to try to go to the meeting tonight but in case he doesn't I wanted to make you aware of his questions. I apologize for not getting it to you sooner but for those who have smart phones maybe it is not too late to see if some of the questions and concerns can be addressed. His comment from 3<sup>rd</sup> e-mail - *I don't fancy myself a conspiracy theorist but it makes me wonder whether the goal is additional parking or whether*

**Dorian Collins**

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**From:** Janet Jonson  
**Sent:** Thursday, January 27, 2011 2:08 PM  
**To:** City Council  
**Cc:** Kurt Triplett; Marilynne Beard; Eric Shields; Paul Stewart; Janice Coogan; Dorian Collins; Ray Steiger  
**Subject:** FW: My response to the Market neighborhood also relate to Houghton's neighborhood

Janet Jonson  
City Manager's Office  
City of Kirkland  
123 5th Avenue  
Kirkland, WA 98033  
425 587 3007  
425 587 3010 fax  
jjonson@ci.kirkland.wa.us

**From:** RLSTYLE@aol.com [mailto:RLSTYLE@aol.com]  
**Posted At:** Tuesday, January 25, 2011 10:27 AM  
**Posted To:** Kirkland Council  
**Conversation:** My response to the Market neighborhood also relate to Houghton's neighborhood  
**Subject:** My response to the Market neighborhood also relate to Houghton's neighborhood

Paul: Please forward this to the Houghton Community Council, the Lakeview Advisory group, and the Planning Commission.

Bob

Answers for the Market Neighborhood

\* Would Kirkland have to put any funds toward it? The short answer is NO. There would be no payment of money from Kirkland going to help pay for the TOD unless you consider the \$59,768 we are paying into ARCH to subsidize affordable housing. It's not near enough to cover the market cost of \$150,000 per housing unit in Kirkland. Someone has to pay for capital facilities and the mitigation measures necessary to meet the Concurrency requirement of the Growth Management Act. It's in the millions. In one article from the County, it said it would go it alone. The County is already in the hole and can't afford to go it alone. That means they would have to be reimbursed for mitigating a TOD. Money is needed for increased road capacity, intersection improvements, bus turnouts and the acceleration and deceleration lanes, sewer, surface water management, and the business use requirements of what it takes to meet to meet the objectives of a TOD. If proper mitigation is not done, it will seriously degrade our quality of life.

\* How would it impact public school system and was the public school system involved in planning for the increase in children that would come from this development? The school district uses its own methods of determining impacts from development. Many times the results of their demographics is far different than the city's and is biased to show need when there is none. I don't know if they have taken the TOD into account.

^ Does the city have the resources to accommodate this project (police, fire, public works, etc.)? It does if we are willing to accept a lower level of service. The cost to provide services increased greatly because of the annexation and subsequently pre-empted the additional needs for a TOD. Which comes first? Providing city adopted level of service levels equally throughout the entire city would require a cost increase needed fill the deficits created by Council.

^ Much discussion on affordable housing and what that really means. The discussion should continue before any more affordable housing is built in Kirkland. We already have about 25% of low income housing units in the inventory. The more we have, the greater our fees and taxes.

\* Could we proceed without TOD and just use the funding for additional parking for the Park and Ride as intended by King County? NO. The cost for parking will not cover the cost. It will be interesting to see what Seattle's increase to \$4.00/hr does for their economics.

\* Who will subsidize the affordable housing percentage of development? Taxpayers at every level be it federal HUD, state, county, or city, and new home buyers that have to pay more in order for others to pay less.

\* Is this really a need in Kirkland? My answer is no. If approved, our quality of life will decrease. We are primarily a residential community servicing regional needs. There is a need to plan for TOD's, but not in Kirkland.

\* The Market Neighborhood believes many of our residents do not believe that this is a high priority for the city. (They are probably right. If the TOD is approved, they will only notice it when the traffic jams get worse. Residents haven't made the connection between downtown jams with what caused it, the existing 600 parking spots that found it way into our quality of life.

\* Economic vitality in downtown Kirkland and Totem Lake are areas that seem to be a high priority for many of the residents. (Not high enough for Council to do something about it.)

Some citizens have a lot more trust in our staff and Council than I do. Given their past performance, they can't be trusted to serve the citizens of Kirkland, only themselves. When it comes to supporting our neighborhoods, they've taken a turn for the worse.

Bob Style

Mr. Russell said TOD's are a good thing because building or adding road capacity is too expensive and will not ease the traffic jams that create gridlock. He partly right, mostly wrong. TOD's create gridlock where they are located. Kirkland is a residential area serving the region. There's no reason to change its roll in regional planning. It's too bad Mr. Russell doesn't believe that. The Mayor and Council are trying to make Kirkland a major metropolitan area by ignoring neighborhood concerns in the process.

The mass transit he promoted when he served on the transportation committees and a Kirkland Councilmember became his mantra when he supported adopting the RTA which became METRO. Boasting it will help prevent traffic jams, he wanted a Yes vote on the RTA ballot. He misled the public. Reducing traffic jams did not happen and increased as evidenced by the traffic jams it now generates. Now, METRO wants to add 250 more to the 600 parking spots already there. The South Park & Ride will create even more congestion than it does now.

The facts are traffic jams are worse. Routes are being changed to add ridership most of which benefits Seattle, Bellevue, and Redmond, not Kirkland. But it didn't get worse for Mr. Russell who works at the University of Washington. He somehow got METRO to redirect bus routes from Lake Washington Blvd to 108<sup>th</sup> Ave NE, a route that allowed Mr. Russell to walk to his bus stop and go to work. He also was instrumental in getting a route (540) to run on 108<sup>th</sup> Ave that went directly to the University where he works. He benefited. We did not.

The Council's support of neighborhood has changed. They use to care about neighborhoods. Now, they are refusing to honor the request of Lakeview and Central Houghton neighborhoods while at the same time honoring the request of Norkirk and Highlands. Inconsistency prevails as evidenced by whoever has the most political influence. They don't treat all the neighborhoods with the same respect. Whose going to run Kirkland, METRO or us?

Something to consider. Kirkland already has 25% low income housing units. The 2010 median price for a condo in this region is now \$244,000. How much of that will be subsidized by who? New home buyers will have to pay more so that others can pay less. Existing homeowners can expect higher fees and taxes to pay for services.

The South Kirkland Park and Ride area should not be rezoned to allow TOD's.

Sincerely,

Bob Style

**Dorian Collins**

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**From:** Janet Jonson  
**Sent:** Thursday, January 27, 2011 2:07 PM  
**To:** City Council  
**Cc:** Kurt Triplett; Marilynne Beard; Eric Shields; Paul Stewart; Janice Coogan; Dorian Collins; Ray Steiger; David Godfrey  
**Subject:** FW: resident of MNA who e-mailed me regarding the SKP&R TOD

Janet Jonson  
City Manager's Office  
City of Kirkland  
173 5th Avenue  
Kirkland, WA 98033  
425-587-3007  
425-587-3019 fax  
jjonson@ci.kirkland.wa.us

**From:** Michelle Sailor [mailto:msailor@comcast.net]  
**Posted At:** Tuesday, January 25, 2011 7:05 PM  
**Posted To:** Kirkland Council  
**Conversation:** resident of MNA who e-mailed me regarding the SKP&R TOD  
**Subject:** resident of MNA who e-mailed me regarding the SKP&R TOD

Hello,

I wanted to share the e-mails that were sent to me on Sunday, Feb. 23rd from a resident who I normally don't hear from and when I do it is quite thoughtful. I have copied and pasted them without altering except to remove his name (asked if I could share comments but didn't specify using his name so I have removed it). He is going to try to go to the meeting tonight but in case he doesn't I wanted to make you aware of his questions. I apologize for not getting it to you sooner but for those who have smart phones maybe it is not too late to see if some of the questions and concerns can be addressed. His comment from 3<sup>rd</sup> e-mail - *I don't fancy myself a conspiracy theorist but it makes me wonder whether the goal is additional parking or whether the goal is real estate development and additional parking is just a convenient excuse to justify variations to the building codes, etc.?* - is a recurring theme that I have been hearing from others. I hope you will consider these points when developing your strategy.

Thanks,  
Michelle Sailor  
MNA Chair

**1<sup>st</sup> email**  
Hi Michelle.

Many thanks for your notes on the meeting. I read the points with great interest, in part because I've been commuting by bus from Kirkland to downtown Seattle for 6 years.

I have a few thoughts and observations on the discussion but, so far, no particular opinion on the correct conclusion to draw with regard to the proposal for adding parking spaces and, possibly, adjacent housing to

South Kirkland Park & Ride (SKP&R). I'd like to discuss these points with you before you post them to the wider group, if possible.

For my thinking I like to make a distinction between

- the objective of additional parking spaces at SKP&R, and
- the development of adjacent property as one possible implementation.

The first question I have is how much DOES it cost to add 15% more parking spots to SKP&R? There seems to be no discussion of this in the materials and links as far as I can tell.

Developing housing adjacent to SKP&R is only necessary if

(a) \$6.25 million funding from the Department of Transportation's Urban Partnership Agreement cannot cover the cost of adding the parking spaces (and other transit improvements as yet unspecified) to create a "Sustainable Transportation Hub", and

(b) The only other solution is to provide incentives to a real estate developer to help cover the cost. The incentives seem to amount to changing existing building codes to open up development and make development sufficiently profitable. The expense to Kirkland in this scenario is supporting this development (as Mr. Style points out in his email below).

Are there any additional options for funding the necessary work? If the cost of the project could be estimated then additional options could be contemplated.

For example, it could very possibly cost the city of Kirkland less to fund the additional costs of adding parking spaces to SKP&R (i.e., above the \$6.25 million grant) then it would to support the additional infrastructure of 200 new housing units for the next ten years. If we can quantify the cost to support 200 new housing units (for some reasonable period of time) then an informed cost/benefit analysis could be made.

The most significant an immediate beneficiary of developing the land adjacent to SKP&R are real estate developers. Expect them to advocate emphatically for developing the land as the only viable alternative.

It was asserted Kirkland residents would not benefit from the additional parking at SKP&R and that Bellevue residents would. Is there any data to quantify who is currently using the SKP&R?

Regards,

**2<sup>nd</sup> email**

Side-issues/perspectives:

As you'll gather from my email I'm wondering if there may be some energy on the parking spaces topic being put into side-issues without addressing the most important core questions. It would seem only reasonable for the City of Kirkland to have asked and have answers to the cost trade-off questions I'm posing. We should expect answers at the ready for these cost estimates (i.e., estimated cost to construct 250 additional parking spaces and estimated cost to support 200 new housing units for x years). If not, it would seem almost negligent. Perhaps we should pose these questions to the City of Kirkland right away and see what we learn? These contacts (below) for Kirkland and King County appear on the Kirkland web site for the SKP&R project ([http://www.ci.kirkland.wa.us/depart/Planning/Code\\_Updates/TOD.htm](http://www.ci.kirkland.wa.us/depart/Planning/Code_Updates/TOD.htm)). Perhaps you know of more appropriate contacts?

3<sup>rd</sup> e-mail

Please forgive all the emails today. Another discussion point...

What is the objective number of additional parking spaces?

I was under the impression it was 250 but this passage from the Kirkland web page describes including some parking for the 200 additional housing units as well.

Approximately 250 additional parking stalls (some to be shared between the site's residents and transit riders).

(from [http://www.ci.kirkland.wa.us/depart/Planning/Code\\_Updates/TOD.htm](http://www.ci.kirkland.wa.us/depart/Planning/Code_Updates/TOD.htm))

I don't fancy myself a conspiracy theorist but it makes me wonder whether the goal is additional parking or whether the goal is real estate development and additional parking is just a convenient excuse to justify variations to the building codes, etc.?

**Dorian Collins**

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**From:** Marie Stake  
**Sent:** Wednesday, January 26, 2011 9:47 AM  
**To:** Kurt Triplett; Marilynne Beard; Eric Shields; Paul Stewart; Dorian Collins  
**Subject:** K. Views Editorial by B. Style RE: SKPR TOD Comm Mtgs

All,  
FYI

[http://www.kirklandviews.com/archives/23963?utm\\_source=feedburner&utm\\_medium=email&utm\\_campaign=Feed%3A+KirklandViews+%28Kirkland+Views%29#](http://www.kirklandviews.com/archives/23963?utm_source=feedburner&utm_medium=email&utm_campaign=Feed%3A+KirklandViews+%28Kirkland+Views%29#)

Marie

## **Letter | South Kirkland Park & Ride Meeting Makes Mockery of Citizen Input Process**

Dear Editor:

Council's workshop last night and the previous meeting at Northwest College were nothing more than an attempt to gain support for what the Mayor and Council had previously decided. They did not want to hear that the TOD would create greater gridlock downtown for the people getting off the buses. They didn't want to hear that their arguments for affordable were bogus.

The meetings were designed to cover their backside by saying they were interested in hearing from the public what it would take to get their support. The Council had already made up its mind so the meeting was only to manipulate the public into thinking they had some influence in the decision making process. Nothing could be further from the truth.

To make sure nothing against the TOD would disrupt the process, a moderator was chosen to control the meeting. A city employee, the Assistant City Manager Marilynne Beard, whose very existence depends on her ability to cow tail to what the Mayor wants, did her job by not allowing information that would discredit the justifications that were presented by staff.

Almost all of Kirkland and those living north of downtown feel the impact of congestion downtown with traffic from the existing 600 space park & ride. Adding 250 more cars to the existing jams will add the gridlock the citizens do

not want. But, that doesn't matter to the Council. They do not want anything getting in the way of what they have already decided.

If the City is going to have a public meeting, everyone should be heard. The meetings are nothing more than a disingenuous attempt to give the public the false impression they had some role in determining the outcome. The meetings were designed to defeat the opposition. We should not let that happen unless you want more congestion, higher fees and taxes, and a lower quality of life.

Bob Style

**Dorian Collins**

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**From:** Paul Stewart  
**Sent:** Tuesday, January 25, 2011 9:52 AM  
**To:** 'Carolyn Hitter'  
**Cc:** Dorian Collins; Eric Shields  
**Subject:** RE: We back the development at South Kirkland Park and Ride

**Follow Up Flag:** Follow up  
**Flag Status:** Flagged

Thanks Carolyn and Jim – we will forward your comments to the Planning Commission and Houghton Community Council.  
Paul

**From:** Carolyn Hitter [<mailto:cjhitter@earthlink.net>]  
**Sent:** Tuesday, January 25, 2011 9:37 AM  
**To:** Paul Stewart  
**Subject:** We back the development at South Kirkland Park and Ride

Houghton Community Council, Kirkland City Council and Kirkland Planning Commission,

Carolyn and Jim Hitter both strongly back the development of housing at the South Kirkland Park and Ride. As the State of Washington slowly slides into an era of mediocrity and social meanness, the City of Kirkland can stand as a beacon of common sense and good planning. The proposed project has many positive factors going for it.

Just where should we expect our teachers and store clerks to live? In Duvall or Monroe? Be realistic; living at a key transit node makes the most sense for our local community, and more importantly for our greater Community!

Sincerely,  
Jim and Carolyn Hitter

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Jim and Carolyn Hitter  
119 8th Lane  
Kirkland, WA 98033  
425 803 0590

**Dorian Collins**

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**From:** Paul Stewart  
**Sent:** Tuesday, January 25, 2011 8:54 AM  
**To:** Dorian Collins  
**Subject:** FW: Public Input for the So.Kirkland P&R and TOD

**Follow Up Flag:** Follow up  
**Flag Status:** Flagged

Here is a comment to the HCC and PC on the South Kirkland Park and Ride .

-----Original Message-----

**From:** Naomi Lombard <[naomi.lombard@gmail.com](mailto:naomi.lombard@gmail.com)>  
**To:** [pstewart@ci.kirkland.us.wa](mailto:pstewart@ci.kirkland.us.wa)  
**Sent:** Mon, Jan 24, 2011 9:45 pm  
**Subject:** Public Input for the So.Kirkland P&R and TOD

Houghton Community Council  
Paul Stewart  
Deputy Director of Planning  
City of Kirkland

RE: South Kirkland Park and Ride Transit Oriented Development

Dear Mr. Stewart,

We are writing in support of what we consider to be a very important development in Kirkland: affordable housing, retail and additional parking proposed by the TOD at the South Kirkland P & R. We live in Houghton and are frequent bus riders. As Kirkland residents for 27 years, we have seen Kirkland grow from a 'small affordable town' to the upscale city that it is now.

We love Kirkland and consider it our permanent home. While we have seen the value of our home rise over the years, my husband and I have often wondered how our own children would ever be able to buy or rent in our fair city. Kirkland simply lacks the affordability that this TOD will bring to our community.

We can not think of a better location for this additional housing; within walking distance to public transportation for work, Kirkland retail, services, schools, etc. The proximity of these units to the two major freeways, will not add significant congestion to Kirkland's city streets. Furthermore, we applaud the additional parking that this development will bring in helping to alleviate current crowded conditions at the P&R.

We do not believe, as we have heard say, that providing homes to lower income individuals and families in our community will have adverse impact on the resale value of our home. A viable city needs to accommodate a healthy range of property pricing in order to attract a diverse population of young people, families and seniors.

We wholeheartedly support plans to join with Bellevue in approving this development. Thank you for the opportunity to voice our opinion.

Sincerely,  
Naomi and Henry Lombard  
10917 NE 66th Place  
Kirkland, WA 98033  
425-828-468

## **Dorian Collins**

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**From:** Janet Jonson  
**Sent:** Monday, January 24, 2011 8:25 AM  
**To:** City Council  
**Cc:** Kurt Triplett; Marilynne Beard; Eric Shields; Paul Stewart; Janice Coogan; Angela Ruggeri; Jeremy McMahan; Kari Page; Dorian Collins  
**Subject:** FW: Market Neighborhood meeting

**Follow Up Flag:** Follow up  
**Flag Status:** Flagged

Janet Jonson  
City Manager's Office  
City of Kirkland  
123 5th Avenue  
Kirkland, WA 98033  
425-587-3007  
425-587-3019 fax  
jjonson@ci.kirkland.wa.us

**From:** Bhaj [mailto:bhaj@nwlink.com]  
**Posted At:** Friday, January 21, 2011 5:26 PM  
**Posted To:** Kirkland Council  
**Conversation:** Market Neighborhood meeting  
**Subject:** Re: Market Neighborhood meeting

Michelle

As I am and was out of town for business meetings, I appreciate hearing your summary. The notes on the South Kirkland Park & Ride Affordable Housing Project was particularly interesting in the seeming lack of financial accountability and responsibility of the city. I think it is important for us to know their plans and projections for a project they are looking to support or even partner on.

Before I close, I want you to know that I appreciate your communication style on your leadership role with the WOM neighborhood.

With warm regards  
Bhaj

Sent from my Verizon Wireless BlackBerry

**From:** "Michelle Sailor" <msailor@comcast.net>  
**Date:** Thu, 20 Jan 2011 15:48:19 -0800  
**To:** <RLSTYLE@aol.com>; <kirklandviews@gmail.com>; <editor@eastidesun.com>; <greg.johnston@patch.com>  
**Cc:** <citycouncil@ci.kirkland.wa.us>; Dorian Collins<DCollins@ci.kirkland.wa.us>; Michelle Sailor<msailor@comcast.net>  
**Subject:** Market Neighborhood meeting

Market Neighborhood Meeting (1/19/11)

I would like to clarify what our neighborhood concerns were about the South Kirkland Park & Ride. The main questions raised were:

- Would Kirkland have to put any funds toward it? Not sure of answer as obviously resources from the city are required for this project but appeared that no actual money would go towards it from the city.
- How would it impact public school system and was the public school system involved in planning for the increase in children that would come from this development? The project did not seem to have a strategy for this aspect of the development.
- Does the city have the resources to accommodate this project (police, fire, public works, etc.)?
- Much discussion on affordable housing and what that really means.
- Could we proceed without TOD and just use the funding for additional parking for the Park and Ride as intended by King County?
- Who will subsidize the affordable housing percentage of development?
- Is this really a need in Kirkland?

Overall, there was an interesting discussion on the South Kirkland Park & Ride with plenty of time for questions from the residents. I would not say that the majority of our residents were for or against the project as I believe they are still trying to understand the project. The affordable housing part of the project needs to be explained better as to how that is a need in Kirkland. Affordable housing vs. affordable rent should be discussed as well (ownership vs. renting). I believe that many of our residents do not believe that this is a high priority for the city. Economic vitality in downtown Kirkland and Totem Lake are areas that seem to be a high priority for many of the residents. We appreciated Dorian Collin, AICP and Paul Stewart, Deputy Director of Planning and Community Development City of Kirkland, for taking the time to highlight details of the project and answer our questions about it. We also appreciated Dave Russell and Robert Style for expressing their views and concerns. We look forward to hearing back from the city with regards to the questions and concerns outlined in this e-mail.

Sincerely,  
Michelle M. Sailor  
Market Neighborhood Chair

*Bcc to MNA Neighborhood distribution list*

**From:** RLSTYLE@aol.com [mailto:RLSTYLE@aol.com]  
**Sent:** Thursday, January 20, 2011 2:32 PM  
**To:** kirklandviews@gmail.com; editor@eastidesun.com; greg.johnston@patch.com  
**Cc:** citycouncil@ci.kirkland.wa.us; msailor@comcast.net  
**Subject:** Market Neighborhood meeting

Market Neighborhood Meeting (1/19/11)

Proponents and opponents of the TOD at the South Kirkland Park & Ride were invited to speak. Mr. Bob Style spoke against. Mr. Dave Russell (ex Kirkland Councilmember and Mayor) spoke for it.

Mr. Style pointed out that when those using the additional 250 parking spots get off the bus, where do they go and what do they do? They get into their cars and try to go home. Most of those using the bus go north and have to get thru Kirkland. They add to the traffic that jams on 108<sup>th</sup> Ave. N.E. and Lake Washington Blvd, NE. The Council refuses address the problem on how to get thru or around Kirkland. It brings up the question of whom does the TOD benefit and who doesn't.

The answer is clear. The benefits will go to Bellevue, points south, and METRO. There are no benefits to the City of Kirkland particularly to the Lake View, Central Houghton, and Market neighborhoods. It will come as our expense and force us to accept a lower quality of life.

Mr. Russell said it was a good thing because building or adding road capacity is too expensive and will not ease the traffic jams that create gridlock. He partly right, mostly wrong. Kirkland is a residential area serving the region. There's no reason to change its roll in regional planning. It's too bad Mr. Russell doesn't believe that.

The mass transit he promoted when he served on the transportation committees and a Kirkland Councilmember became his mantra when he supported adopting the RTA which became METRO. Boasting it will help prevent traffic jams, he wanted a Yes vote on the RTA ballot. He misled the public. Reducing traffic jams did not happen and increased as evidenced by the traffic jams it now generates. Now, METRO wants to add 250 more to the 600 parking spots already there. The South Park & Ride will create even more congestion than it does now.

The facts are traffic jams are worse. Routes are being changed to add ridership most of which benefits Seattle, Bellevue, and Redmond, not Kirkland. But it didn't get worse for Mr. Russell who works at the University of Washington. He somehow got METRO to redirect bus routes from Lake Washington Blvd to 108<sup>th</sup> Ave NE, a route that allowed Mr. Russell to walk to his bus stop and go to work. He also was instrumental in getting a route (254) that went directly to the University where he works. He benefited. We did not.

The Council's support of neighborhood has changed. They use to be supportive of neighborhoods. Now, they are refusing to honor the request of Lakeview and Central Houghton neighborhoods while at the same time honoring the request of Norkirk and Highlands. Inconsistency prevails as evidenced by whoever has the most political influence. They don't treat all the neighborhoods with the same respect.

The issue of affordable housing came up at the meeting. The cost and purpose were very controversial. Most of the arguments were against it. No one except staff spoke for subsidized housing spoke.

Bob Style

**Dorian Collins**

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**From:** Marilynne Beard  
**Sent:** Tuesday, January 11, 2011 2:02 PM  
**To:** Margaret Bull  
**Cc:** Dorian Collins; Paul Stewart  
**Subject:** RE: TOD neighborhood workshop comments

Thank you so much for your comments. We will put them into the mix.

**From:** Margaret Bull [mailto:wisteriouswoman@gmail.com]  
**Sent:** Tuesday, January 11, 2011 1:02 PM  
**To:** Marilynne Beard  
**Subject:** TOD neighborhood workshop comments

*January 11, 2011*

*Hi Marilyn,*

*I will be out of town during the TOD workshops. Therefore, I am writing to contribute my thoughts.*

*My main concerns have to do with sidewalks, crosswalks and commuters.*

*I would like to see a continuous sidewalk along Northup between the transit center and Lowe's Hardware store. It would also be wise to have a continuous sidewalk between the Park and Ride and Kirkland along the west side of 108<sup>th</sup> Ave NE/6<sup>th</sup> Street. 108<sup>th</sup> is often extremely hazardous to cross. For a variety of reasons, crosswalks are not always effective along this stretch of road and pedestrian lives are in danger when they are required to cross from one side of the road to the other. The area around the park and ride lot is not pedestrian friendly! I would also like to see better bus service (everyday and late evening) going east along Northup. These are some of the reasons that I question the wisdom of developing housing in this area. I also believe the lack of everyday services in this area will force people living in the housing development to use their cars for daily needs as well as commuting to work. I question where the money will come from that will enable the city of Kirkland and Bellevue to work together to make this development come to fruition and provide the infrastructure, including improved roads and sidewalks, that will be necessary.*

*Most importantly, I would like to see Rapid Bus service implemented between South Kirkland Park and Ride and East Bellevue, Redmond, North Kirkland, Seattle, Everett and Renton. Rapid Bus is under-developed on the Eastside. It makes sense to use it at South Kirkland Park and Ride Lot so that people living in the TOD can easily get to job locations in other cities. Many workers living in the various cities mentioned can take buses to South Kirkland Park and Ride and transfer to buses going across Lake Washington into Seattle for jobs, etc. Or*

*conversely, with the bridge construction and tolling becoming an immense problem for many, commuters from Seattle will also benefit from a Rapid Bus system that enables them to transfer to various Rapid Buses going to jobs in Eastside cities. This type of bus system will limit much of the need for residents of this development to drive their cars to work. South Kirkland Park and Ride is under-utilized as a transfer point. It is in a very important strategic location because it is between Bellevue and Kirkland, as well as between Seattle and Redmond. Also, now that Boeing has shifted all its engineers to the Everett site there is more need than ever to connect Boeing employees living in Renton, Kirkland, Bellevue, and Seattle with Everett with a commuter bus system.*

*I feel it is foolish to include plans to develop light rail along the rail corridor in the TOD proposal. The rail corridor should be designed for pedestrians and bicycles only. In my opinion, it is a waste of city money planning for light rail at this location due to the fact that the rail corridor does not connect to major employment locations nor to the majority of neighborhoods on the Eastside. Available park and ride lots are not situated along this corridor in strategic locations to make it a via transportation option for most people. Rapid Bus is a better option than light rail due to the fact that the routes can be altered when employment opportunities and housing density shifts in the various cities on the Eastside. Even though the vision of Kirklanders working in Kirkland has been emphasized during the Park Place development meetings, the reality may turn out to be much different.*

*In many ways South Kirkland Park and Ride is a better location for connecting Eastside cities by public transportation than the Kirkland Transit Center is. As it is, downtown Kirkland is not well situated for current Sound Transit buses to be routed through. I can't see that situation improving any time soon. Kirkland's downtown growth will produce an increase in traffic congestion due to ongoing construction projects over the next 8 years and thus cause delays in transit bus service through the downtown area.*

*As a citizen of Houghton it is in my self-interest to see growth at the Park and Ride Lot limited. I don't want an increase in traffic along 108<sup>th</sup> Ave NE causing noise and congestion. Even so, I believe there will be greater congestion along 108<sup>th</sup> whether or not the TOD goes in due to all the development that is planned in downtown Kirkland as well as the bridge reconstruction and tolling that will soon be underway. It is much easier for people to drive along 108<sup>th</sup> Ave NE and park on the side streets in my neighborhood in order to catch a bus to Seattle than park in downtown Kirkland and get on a bus at the transit center. People living in many parts of Kirkland including the annexation area have poor bus service and can't take a bus from their home to the Kirkland transit center in order to take public transit to their places of employment. There are two reasons many commuters choose to park in the Houghton neighborhood: the difficulty finding a parking place at the lot and the fact that 255 buses often have standing room only by the time all the people board that are waiting at the South Kirkland Park and Ride. An increase in parking stalls at all park and ride lots in*

*the greater Kirkland area will be greatly needed in the future. I'd like to see a more comprehensive plan that adds parking at other existing park and ride locations before the city goes forward with plans that focus on the South Kirkland Park and Ride alone. This may take pressure off using Houghton neighborhood streets as park and ride 'overflow lots'.*

*When I step back and look at the bigger picture I can see myself as a citizen of the greater Seattle area and not just as a Houghtonite. My family members commute to jobs at Boeing and Microsoft on a daily basis, but many of their colleagues do not have this option. I can see that a better public transportation system is necessary that can quickly transport workers from one city to the next. I believe that South Kirkland Park and Ride is an ideal location for this type of system to be developed. Its proximity to both 405 and 520 could be capitalized upon especially if freeway access is improved.*

*Sincerely,*

*Margaret Bull  
6225 108<sup>th</sup> Place NE  
Kirkland WA 98033  
425 822 2925*

*Please do not distribute my e-mail address*

**Janice Coogan**

---

**From:** georgine foster [georginef@msn.com]  
**Sent:** Wednesday, January 05, 2011 9:39 AM  
**To:** Janice Coogan  
**Subject:** Fw: South Kirkland P&R

Janice, could you forward this to the HCC and Planning Commissioners.....I don't know if only the City Council may have received it as I addressed the email to 'citycouncil@ci.kirkland.wa.us'.

Thank you.....and I hope you had a great holiday.

georgine foster

----- Original Message -----

**From:** [georgine foster](mailto:georgine.foster@ci.kirkland.wa.us)  
**To:** [citycouncil@ci.kirkland.wa.us](mailto:citycouncil@ci.kirkland.wa.us)  
**Sent:** Tuesday, September 21, 2010 8:57 AM  
**Subject:** South Kirkland P&R

Dear City Council members, Houghton Community Council members, City Manager,

I am a member of the Lakeview Neighborhood Plan Update Advisory Group, but I would like to express some personal views about the "Process" and the possible "Fast Tracking" of the Zoning Code Amendments for the South Kirkland Park& Ride. (You will remember that the Comp Plan Amendments for the P&R were "fast tracked" the end of 2008.)

My concern is that the County is asking for expediting the Zoning Code revisions without DUE PROCESS, possibly circumventing the Lakeview Neighborhood Plan Update process, and WITHOUT Bellevue's "cooperation", as is called for in the Comp Plan.....I don't understand how a few phone calls, or meetings that do not produce at least an MOU between Kirkland, Bellevue and King County, is adequate. Un-intended consequences could be devastating to the area, with congestion at the top of the list...changes to the 520 will surely have their effect on traffic in the area, too.

The amount of Affordable housing, as is stated in your packet supplied by Dorian Collins, suggests that 100% of the project could be "affordable". Redmond's Town Center TOD, the TOD in Renton, and the TOD in Northgate are all 20% Affordable and 80% Market rate.....why is Kirkland seeking higher percentages for South Kirkland when obviously neighboring cities have chosen differently? I realize the TOD at Redmond's Overlake area is 100% affordable, but are the demographics of Lakeview and Central Houghton comparable to Overlake? If South Kirkland is to have Affordable Housing as part of its mixed use development, why not at a Rate more in keeping with what has been developed in other neighboring jurisdictions?

Note the April 16th email (below) from Gary Prince of Metro (who also authored the Application for the Grant to the Federal government)...."grant funding...is not related to the affordability issue but rather to increasing the number of parking places and mixed use development". So there is no "must have" percentage, or number, of Affordable units.

In the Affordable Housing Regulations recently approved by the City, it is noted that INITIAL "affordable housing projects" will not be required to provide the entire "mandatory" 10% affordable units for projects (as they are viewed as almost experimental....that is my summation), YET the South Kirkland Park & Ride could have 100%?

Our Lakeview Neighborhood Plan Update process had many of us spending MANY hours in meetings because we felt we had something of value to add to the process, our neighborhood, and Kirkland. Please consider how you might feel if you "participated", only to find out that it didn't really matter.

Thank you.

georgine foster  
Lakeview Neighborhood Resident

**From:** georgine foster [mailto:georginef@msn.com]  
**Sent:** Friday, April 16, 2010 10:59 AM  
**To:** Prince, Gary  
**Cc:** Paul Stewart  
**Subject:** Re: South Kirkland P&R

Gary,  
Thanks for the information!

Who might I contact to get the # and type of affordable units at the Redmond Downtown TOD, Northgate, and Overlake projects.....just for comparison sake. I'm meeting with Paul Stewart and Dorian Collins next Wednesday and if I had these comparison figures it would be helpful for me to get "the big picture".

Again, thank you.

~georgine

----- Original Message -----

**From:** Prince, Gary  
**To:** georgine foster  
**Cc:** Paul Stewart  
**Sent:** Friday, April 16, 2010 10:33 AM  
**Subject:** RE: South Kirkland P&R

Georgine:

The county has worked with local jurisdictions, ARCH, and private developers to determine the number and type of affordable units. The County does not have a "vision" for the number or type of affordable units for this particular site. The grant funding which Metro Transit has available is not related to the affordability issue but rather to increasing the number of parking spaces and the mixed use development

We do not have an appraisal on the parcel so I cannot speak to the price for the underlying land.

Gary Prince  
Senior Project Manager  
Transit Oriented Development  
King County Department of Transportation  
206.263.6039

**From:** georgine foster [mailto:georginef@msn.com]  
**Sent:** Wednesday, April 14, 2010 6:09 PM  
**To:** Prince, Gary  
**Cc:** Paul Stewart  
**Subject:** Re: South Kirkland P&R

Gary, thanks for the info....I wasn't thinking that Mithun was an Architectural/Design firm, I thought they developed the Northgate project. Has the County any "vision" for % to Median income, or the # of Units that will be 'affordable'.....and do these numbers effect how much grant money could be available for the project?

(And what might the asking price be for the "underlying land"?)

~georgine foster

--- Original Message -----

**From:** Paul Stewart

**To:** georgine foster ; Janice Soloff

**Cc:** johnk ; Dorian Collins ; Prince, Gary

**Sent:** Wednesday, April 14, 2010 10:26 AM

**Subject:** RE: South Kirkland P&R

Georgine,

It is my understanding that King County would request proposals from developers. Mithun is an architectural and design firm that is advising King County and is not a developer. You should contact Gary Prince for more information on this.

I would suggest that instead of these back and forth e-mails, why don't we have a meeting and we can explain the project in detail and respond to your questions.

Paul

## EDITORIAL

**New development means more affordable housing**

The City of Kirkland supports transit-oriented development (TOD) at the South Kirkland Park and Ride.

The site is ideal for combining higher residential and employment densities with frequent transit service. It's a major transit hub, with service to Totem Lake, downtown Seattle, the University of Washington and other employment and residential areas.

The park and ride lot, owned by King County Metro, is located in the Lakeview neighborhood, near the intersection of Lake Washington Boulevard and 108th Ave. N.E. The site is about seven acres with equal portions lying within the cities of Kirkland and Bellevue.

The Kirkland City Council had good reason for recently voting to approve the TOD when they did.

At stake was \$6.25 million in funding that King County will receive from the U.S. Department of Transportation that would add 250 much needed parking stalls. The park and ride is currently at capacity with 603 stalls.

King County could have lost the federal grant if the city did not take action by early 2011.

Even more significant is the TOD ranks among the city's top affordable housing strategies.

No doubt, there is a dire need for affordable housing in Kirkland. Despite the city's long-standing commitment to support housing issues that face the community, the city continues to fall short of meeting its annual affordable housing targets.

In fact, the Eastside has the smallest stock of affordable rental housing in the county for people at 50 percent of the area median income, according to a King County Benchmark Report. For a four-person family, that's an annual salary of \$42,150.

Creating more affordable housing eliminates long commutes for those who have to travel from places they can afford to the places they work. It would allow lower wage workers to stay in the community they work in so they could develop a sense of community and get more involved. It

would strengthen families.

And the most common cause of homelessness on the Eastside is a lack of affordable housing. Why not prevent this widespread problem and create more affordable housing?

The TOD project at the South Kirkland Park and Ride calls for 200 multi-family units, of which 20 percent would be affordable to low or moderate income households, in two five-story buildings. An additional 20 percent of units could be affordable to median income households through a city policy that urges this type of affordability.

The affordable housing issue at the TOD site has drawn the greatest controversy to many residents in the area. A majority of the neighborhood advisory group members have expressed outright opposition to affordable housing.

Among the "NIMBY" (Not In My Backyard) sentiments expressed is that affordable housing would lower property values and degrade the neighborhood.

Not so. Numerous King County studies show

that existing affordable housing – including on the Eastside – has not lowered property values.

Whether the project causes significant traffic impacts remains to be seen. We agree the city should mitigate these impacts as part of the project. This includes relieving congestion on Lake Washington Boulevard and nearby streets.

Assessing the adequacy of parking at the park and ride and future development on Bellevue's portion of the property are other issues the city should address going forward.

Some feel the council ignored the neighborhood advisory groups by moving ahead with the TOD.

But council's approval of the project solidifies some of the crucial factors established for the South Kirkland Park and Ride – including the housing element.

And not all avenues for change are lost. Residents still have the opportunity to address other issues through zoning code regulations, such as those related to parking and design.



**Dorian Collins**

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**From:** Marie Stake  
**Sent:** Friday, December 17, 2010 8:49 AM  
**To:** Eric Shields; Dorian Collins; Marilynne Beard  
**Subject:** K . Views Editorial: SKP&R TOD

All,  
Not sure if you saw this  
[http://www.pnwlocalnews.com/east\\_king/kir/opinion/letters/111955909.html](http://www.pnwlocalnews.com/east_king/kir/opinion/letters/111955909.html)  
Marie

## **Kirkland's tragedy of transit and Transit Oriented Development**

Dec 15 2010, 2:09 PM

If we are to believe the importance of neighborhoods in Kirkland as stated by the council on their Web page under Community Neighborhood Resources, the council will not approve the TOD (Transit Oriented Development) at the South Kirkland Park & Ride. If they do, it will violate their policy of supporting neighborhoods. What are we to believe, words or actions?

Mitigating Kirkland's residential traffic impacts has not and will not be done as long as the council refuses to add capacity to arterials, collectors, and re-designate access streets to support growth and business. Residential streets will suffer.

The council's actions speak louder than their propaganda. In spite of what they say, their decisions have not and do not support the TOD. TOD's do not relieve traffic. It's the fatal flaw in their thinking. TOD's create traffic jams. Expansion of the Park & Ride means more people will be getting on and off the bus who have to go somewhere in their cars.

The council has not found a way to get traffic through or around downtown. Thru-traffic should not be using residential streets. With the expansion and additional use of the Park & Ride, traffic jams will get worse on Lake Washington Blvd. and 108th Ave. N.E. Bus users will have to use residential streets to get to and from the TOD. The traffic jam downtown is reason enough by itself to disapprove the TOD.

An update of the comprehensive plans for the Lakeview and Central Houghton neighborhoods is required. Advisory committees consisting of the citizens who live there were formed and came up with their recommendations, which are now being ignored. If elected officials approve the TOD, they will be insulting the citizens of those who live there. It will probably become an issue at election time in Houghton and the city. The citizens have determined the impacts of the TOD, the elected officials have not. That's the tragedy of transit and the TOD.

*Bob Style*

*(Comments from Nona Ganz)*

According to Vision 2040, which is the PSRC regional plan, the 4-county area is supposed to get 1.7 million more people and 1.2 million more jobs by 2040. The majority of these people and jobs will be in the urban growth areas.

Kirkland is to absorb or has a housing target of 7200 new units by 2031 (around 15,000 people) and a job target of 20,200 new jobs. This does not include the annexation area.

So where does it make most sense to accommodate the new growth? Besides in the Totem Lake area, which we have discussed for years, I believe the S K P&R lot is an ideal location for transit-oriented development for it a major transit hub, next to 520, close to 405 and close to retail in Kirkland and Bellevue. There is excellent bus service to Seattle and to employment centers in all directions. Consolidating housing at major transit facilities is an effective strategy to increase transit ridership and to reduce the harmful effects of congestion and greenhouse gas emission. This is not a new concept....it is being done all over the world.

I was pleased to hear that affordable housing would be a significant component of the multifamily development for, as we all know there is a tremendous need for such housing in Kirkland and in the area. People who wish not to own a car or who are unable to have a car would have all transit options at their door.

The existing P & R site is not well utilized land.....it's just parking stalls, and not enough of them. The demand for the P&R will certainly increase when changes occur in the 520 corridor. A TOD would provide about 250 additional parking stalls and it would provide housing units toward our housing target.

Like it or not, growth will occur.....we have an opportunity here to direct it to where it makes most sense.

My last comment - good design is critically important for a successful development.

Y\\

**Dorian Collins**

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**From:** Paul Stewart  
**Sent:** Friday, December 17, 2010 4:29 PM  
**To:** Dorian Collins  
**Subject:** FW: South Kirkland Park and Ride  
**Attachments:** LOGO-FOR-EMAIL-SIG; ATT492256.htm; McGladrey\_Email\_Power Signature\_300x75.gif; ATT492257.htm; Oct 2008 comments.docx; ATT492258.htm

I came across this in one of my e-mail folders regarding Yarrowood Condos.

**From:** Joan McBride  
**Sent:** Tuesday, November 16, 2010 3:32 PM  
**To:** Paul Stewart; Eric Shields  
**Cc:** Doreen Marchione; Jessica Greenway  
**Subject:** Fwd: South Kirkland Park and Ride

Hi just spoke with Jan (see below) and let her know she had some wrong or outdated info. Told her we could have some one come to one of their meetings and give a presentation. She was happy about that. To get on their schedule call Steve Taylor T 206 935 7951. I would love to go too

Joan McBride  
Mayor  
City of Kirkland  
425.698.7556

Sent from iPhone

Begin forwarded message:

**From:** "Brannan, Liz" <[Liz.Brannan@mcgladrey.com](mailto:Liz.Brannan@mcgladrey.com)>  
**Date:** November 16, 2010 2:22:18 PM PST  
**To:** <[AWalen@ci.kirkland.wa.us](mailto:AWalen@ci.kirkland.wa.us)>, <[BSternoff@ci.kirkland.wa.us](mailto:BSternoff@ci.kirkland.wa.us)>, <[DAsher@ci.kirkland.wa.us](mailto:DAsher@ci.kirkland.wa.us)>, <[DMarchione@ci.kirkland.wa.us](mailto:DMarchione@ci.kirkland.wa.us)>, <[JGreenway@ci.kirkland.wa.us](mailto:JGreenway@ci.kirkland.wa.us)>, <[JMcBride@ci.kirkland.wa.us](mailto:JMcBride@ci.kirkland.wa.us)>, <[PSwcct@ci.kirkland.wa.us](mailto:PSwcct@ci.kirkland.wa.us)>  
**Cc:** <[theborde@aol.com](mailto:theborde@aol.com)>, <[303@midinet.net](mailto:303@midinet.net)>, <[jren@msn.com](mailto:jren@msn.com)>, <[lisa.muth@comcast.net](mailto:lisa.muth@comcast.net)>, <[lisab29@gmail.com](mailto:lisab29@gmail.com)>, <[marjferrin@comcast.net](mailto:marjferrin@comcast.net)>, <[McCaulley2@aol.com](mailto:McCaulley2@aol.com)>  
**Subject:** South Kirkland Park and Ride

I represent the Board of Directors of Yarrowood Condominiums, a 155 unit residential community located at 108<sup>th</sup> NE and Northup Way.

We are concerned that the Kirkland City Council is continuing to explore transforming the South Kirkland Park and Ride into a TOD, with as many as 500 residential units, light retail and a multi-level parking garage for Metro park and ride patrons.

I spoke at two meetings in 2008 ( a Houghton Community meeting and a City Council meeting) and send information to a Planning Commission meeting. At the meeting I attended, it appeared that the overwhelming sentiment towards to proposed project was negative, especially when given by residents of the affected community. I have attached a copy of an e-mail sent to Dorian Collins of the Planning Commission staff. It outlines what were then my concerns, but are now the concerns of our Board and of the community of Yarrowood.

In short, we believe that the proposed development is contrary to the general feel of the immediate neighborhood; that the increased traffic will put an undue strain on limited infrastructure (At times, it is nearly impossible to make a left hand turn onto 108<sup>th</sup> from our driveways); and that the existing services in the neighborhood will not support a high density residential development. A 500 unit apartment complex will have a drastic effect on the appearance of this community as a building accommodating 500 units will be of several stories. So far this has been a low density residential area and this proposal will change that environment.

Liz Brannan

Director, Tax Services

RSM McGladrey, Inc.

600 University Street, Suite 1100

Seattle, WA 98101-3119

Phone: 206-281-4444 Fax: 206-749-7136

[liz.brannan@mcgladrey.com](mailto:liz.brannan@mcgladrey.com)

[www.mcgladrey.com](http://www.mcgladrey.com)

Dorian: I am a resident and homeowner at Yarrowood, a condominium community of 155 units located on 108<sup>th</sup> Ave NE, across the street from the South Kirkland Park and Ride.

I attended the first two meetings relating to the proposed changes to the Park and Ride facility. I was able to speak at the Houghton Community Council meeting and express some concerns as a direct neighbor to the facility. I attended the Planning Council meeting, but arrived too late due to my commute for the public comment section of the meeting.

I am concerned about the residential density proposed for this Park and Ride facility. It appears that to meet the affordable housing guidelines that are a goal set by the Houghton Community Council and the city of Kirkland, as many as 500 residential units would be needed to make the development economically viable. It is my understanding that the Park and Ride property totals 7 acres. Yarrowood is an 11 acre property and has only 155 units. Yarrowood is certainly a different design than what has been discussed in the two meetings. We have a mix of building styles, 2 unit townhouses, 4 unit (two above, two below) buildings, and two large buildings, three stories each, with one level "garden style" apartments. There is a lot of green space in the development, much of which is left to natural woods and ground cover because of the slope of the property.

A phrase that I have heard discussed in the meetings as a design concept for the Park and Ride is "urban village". When I hear that phrase, I think of the mixed use development in the Juanita area. This is a large development and I would think it would not translate well to a piece of property that still must have as a primary function, providing parking for commuters who use Metro and Sound Transit busses.

There are three main concerns I have about the size of development proposed:

**Traffic-** the addition of 500 families to the permanent population. These families will bring a certain number of cars, and although the ideal would be that they would be candidates for public transportation for the daily commute, there are many transportation needs that are not solved by the use of public transportation in our area. Access to shopping, errands, children's activities etc would almost certainly involve the use of personal autos. I cannot see a parent, with small children in tow, doing the weekly grocery shopping by bus. In addition, not all commuting needs are met by public transit. There are times when I find it hard to make a left turn out of Yarrowood onto 108<sup>th</sup> because of traffic both north and southbound on 108<sup>th</sup>. How would the effect of 500 new residents and their cars be mitigated?

**Security-** we have experienced car prowls and other property damage by "visitors" to Yarrowood. The Park and Ride seems to provide a point of late night access. The Burlington Northern tracks also contribute to non-conventional access to Yarrowood. We are concerned about the potential for increased access into our community by folks who are not residents or invited guests. It is not a given that 500 new residential units across the street will have an adverse effect on security as relates to Yarrowood, but I would like to see that issue discussed in the planning process, with consideration for what must be done to accommodate increased police protection/patrols considering that both Bellevue and Kirkland police would be involved.

**Esthetic/ Environmental:** The appearance of the area as one drives 108<sup>th</sup> Ave, is pleasant because of the trees that buffer the street. Yarrowood has trees along its property bounded by 108<sup>th</sup>, as does the Park and Ride and a large piece of property, currently a single family residence across the street. To the casual observer, the existing residents of the immediate area are not easily discernable and may be overlooked when considering the impact of the proposed development on the existing community. The current office parks are set back from the neighboring streets. These trees and set backs preserve the appearance of a less densely used portion of the community and provide the benefit of green spaces as opposed to large paved areas. Near this area are several wetlands that have been encroached by development. Especially with the plans to expand 520, it is important that these green spaces and fragile areas be preserved.

Because of the change in the date of the next Planning Commission meeting, I will not be able to attend. Please submit these comments to the Commission. Please feel free to contact me if you have questions about my comments.

**Dorian Collins**

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**From:** Paul Stewart  
**Sent:** Wednesday, December 15, 2010 4:20 PM  
**To:** Dorian Collins; Janice Coogan; 'Prince, Gary'  
**Subject:** FW: The tragedy of transit and TOD traffic

FYI

**From:** Janet Jonson  
**Sent:** Wednesday, December 15, 2010 9:16 AM  
**To:** City Council  
**Cc:** Kurt Triplett; Marilynne Beard; Eric Shields; Paul Stewart; Ellen Miller-Wolfe; Ray Steiger; David Godfrey; Kari Page  
**Subject:** FW: The tragedy of transit and TOD traffic

Janet Jonson  
City Manager's Office  
City of Kirkland  
123 5th Avenue  
Kirkland, WA 98033  
425-587-3007  
425-587-3019 fax  
[jjonson@ci.kirkland.wa.us](mailto:jjonson@ci.kirkland.wa.us)

**From:** [RLSTYLE@aol.com](mailto:RLSTYLE@aol.com) [mailto:[RLSTYLE@aol.com](mailto:RLSTYLE@aol.com)]  
**Posted At:** Tuesday, December 14, 2010 4:02 AM  
**Posted To:** Kirkland Council  
**Conversation:** The tragedy of transit and TOD traffic  
**Subject:** The tragedy of transit and TOD traffic

Kirkland's tragedy of transit and the TOD

If we are to believe the importance of neighborhoods in Kirkland as stated by the Council on their Web page under Community Neighborhood Resources, the Council will not approve the TOD (Transit Oriented Development) at the South Kirkland Park & Ride. If they do, it will violate their policy of supporting neighborhoods. What are we to believe, words or actions?

Mitigating Kirkland's residential traffic impacts **has not and will not be done** as long as the Council refuses to add capacity to arterials, collectors, and re-designate access streets to support growth and business. Residential streets will suffer.

The Council actions speak louder than their propaganda. In spite of what they say, their decisions have not and do not support the TOD. TOD's do not relieve traffic. It's the fatal flaw in their thinking. TOD's create traffic jams. Expansion of the park & ride means more people will be getting on and off the bus who have to go somewhere in their cars.

The Council **has not** found a way to get traffic thru or around downtown. Thru traffic should not be using residential streets. With the expansion and additional use of the park & ride, traffic jams will get worse on Lake Washington Blvd and 108<sup>th</sup> Ave NE. Bus users will have to use residential streets

to get to and from the TOD. **The traffic jam downtown is reason enough by itself to disapprove the TOD.**

An update of the comprehensive plans for the Lakeview and Central Houghton neighborhoods is required. Advisory committees consisting of the citizens who live there were formed and came up with their recommendations which are now being ignored. If elected officials approve the TOD, they will be insulting the citizens of those who live there. It will probably become an issue at election time in Houghton and the city. **The citizens have determined the impacts of the TOD, the elected officials have not.** That's the tragedy of transit and the TOD.

Bob Style

**Dorian Collins**

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**Subject:** Public & HCC still has "YES" "NO" input on TOD per Comp Plan Update  
**Attachments:** Document from Karen Levenson - 121610.pdf

**From:** Uwkkg@aol.com [mailto:Uwkkg@aol.com]  
**Sent:** Thursday, December 16, 2010 8:33 AM  
**To:** Janice Coogan  
**Cc:** Uwkkg@aol.com  
**Subject:** Public & HCC still has "YES" "NO" input on TOD per Comp Plan Update

Janice:  
Please distribute this to all members of HCC, Kirkland City Council, Planning Commission, Staff and staff and the City Manager and Assistant Manager.

HCC: Could you please confirm that you have received this.

=====

Date: December 15, 2010  
To: All members of HCC, KCC, Planning Commission, Staff and City Manager/Assistant City Manager:  
Attached: Revised Comp Plan Document XV.A-8 (see bottom of 1st column)  
Subject: HCC and Citizens still have input on Yes or No for TOD

At Monday night's meeting it was emphasized that this new "moderated" process needed to start with accurate, unbiased information.

In an effort to help achieve accuracy, I've attached highlighted update to Comp Plan and it is also attached below.

Please note that per the Revised Comp Plan, the TOD is NOT a done deal. It is specifically stated as just an option (see actual Comp plan verbiage below "continue as a transit facility" or "ALTERNATIVELY be redeveloped as a TOD"). Remove the bias and you'll see that HCC and citizens, neighbors and businesses DEFINITELY STILL HAVE the opportunity to give input on whether the TOD should happen or not!!!

With that in mind, it is important to correct the record from Monday's meeting. The correction would be to the statements (repeated several times quite forcefully by city) that the Comp Plan change meant that the TOD was definitely going to happen... Well...This is just NOT TRUE.

It was particularly bothersome to see that staff and an "impartial" moderator who is also our Assistant City Manager as they dismissed comments by two or three Houghton Council members who attempted to be clear that the TOD is not a foregone conclusion.

Please review the recording of the 1.26.09 HCC meeting (minutes 25-52) you will hear specific comments and concerns from most of the HCC members. There was talk of what it would mean if they did not "disapprove" and whether they should disapprove. ... After much discussion, it was resolved that the wording of 4155 was such that by allowing approval they were signaling that they were open to "CONSIDERATION" and that as the project gained more definition they would be "considering" the project and whether it was something they could support or deny. (50 min) At the end of the meeting City Staff was asked if they they could convey the "Consideration" but also "that the project would have to meet some very high expectations regarding not too big, not too bulky, traffic mitigation, sufficient parking." Staff said that these "conditions" could be conveyed to City Council and the 4155 therefore was not disapproved.

The actual comp plan verbiage from the 1.26.09 approval is below and also attached. The verbiage provides for EITHER continuation of parking (with possible office) or ALTERNATIVELY the site may be redeveloped with a TOD ..... (see last paragraph).

**New text to be added to the Lakeview Neighborhood Chapter, page XV.A-8, following discussion of PLA 3, and preceding discussion of PLA 15:**

***Planned Area 4: South Kirkland Park & Ride***

The property containing the South Kirkland Park and Ride is about seven acres in size, with approximately equal portions of the site lying within the cities of Kirkland and Bellevue. The site is owned by King County, and currently developed as a Park and Ride with approximately 600 parking stalls and a transit facility. The site is generally level, but has a steep slope along the eastern and southeastern boundaries within the city of Bellevue section of the site. Tall trees and heavy vegetation are present within the hillside areas.

King County has identified the South Kirkland Park and Ride as a potential site for transit-oriented-development (TOD) for several years. Affordable housing is generally included in King County TOD projects, and is anticipated to be a significant component of future residential development at the South Kirkland site. The City of Kirkland has identified transit oriented development at the South Kirkland Park & Ride as a key affordable housing strategy. The City supports multifamily residential as the predominant use of the site in a transit-oriented-development project, with a variety of other uses to be allowed as well.

The South Kirkland Park and Ride property may continue as a transit facility with the potential for office use. Alternatively, if the site is redeveloped with TOD, the principles discussed below should be used to guide development at the Park and Ride.

Karen Levenson  
President  
The Park, A Condominium  
6620 Lake Washington Blvd NE, Kirkland

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## XV.A. LAKEVIEW NEIGHBORHOOD

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existing multifamily units for overnight lodging, however, would be acceptable provided that the site development maintains its residential character and that accessory restaurants, retail, or similar uses are not allowed.

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### *Subarea B should include public use areas.*

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Because of its adjacency to Lake Washington and Yarrow Bay wetlands, development in Subarea B should also include a public trail along its entire perimeter as well as other areas suitable for passive public use.

### **PLANNED AREA 4: SOUTH KIRKLAND PARK & RIDE**

The property containing the South Kirkland Park and Ride is about seven acres in size, with approximately equal portions of the site lying within the cities of Kirkland and Bellevue. The site is owned by King County, and currently developed as a Park and Ride with approximately 600 parking stalls and a transit facility. The site is generally level, but has a steep slope along the eastern and southeastern boundaries within the city of Bellevue section of the site. Tall trees and heavy vegetation are present within the hillside areas.

King County has identified the South Kirkland Park and Ride as a potential site for transit-oriented development (TOD) for several years. Affordable housing is generally included in King County TOD projects, and is anticipated to be a significant component of future residential development at the South Kirkland site. The City of Kirkland has identified transit-oriented development at the South Kirkland Park and Ride as a key affordable housing strategy. The City supports multifamily residential as the predominant use of the site in a transit-oriented-development project, with a variety of other uses to be allowed as well.

The South Kirkland Park and Ride property may continue as a transit facility with the potential for office use. (Alternatively, if the site is redeveloped with

TOD, the principles discussed below should be used to guide development at the Park and Ride.

---

### *Provide for affordable housing.*

---

- ◆ Ensure that transit-oriented development provides for mixed-income housing, including a minimum of 20 percent of total units to be affordable to low and/or moderate income households.
  - Development should strive to achieve greater affordability for at least 20 percent of its units, with an additional 25 percent to be affordable to median income households, through the use of as many funding sources as are necessary.

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### *Ensure high quality site and building design.*

---

- ◆ Develop implementing regulations for coordinated development of the entire site.
  - Establish standards for building height and mass that acknowledge site topography and existing vegetation as factors for consideration.
- ◆ Implement design standards for Planned Area 4.
  - Ensure that regulations support appropriate building scale and massing throughout the site, produce buildings that exhibit high quality design and incorporate pedestrian features and amenities that contribute to a livable urban village character for the TOD.
  - Provide guidance for the streetscapes along NE 38th Place and 108th Avenue NE to ensure buildings do not turn their backs on the streets and development provides a welcoming and attractive presence at this gateway to Kirkland.
  - Protect the vegetative buffers and significant trees along the site's eastern and

To the Houghton Community Council and the Planning Commission:

The Lakeview Policies that were adopted by the Council were predicated on a 2008 document that was revised in May of 2009. **Now, staff and the Council are using outdated documents to support a TOD while at the same time ignoring the results of the committees.**

As stated in the City's fact sheet, "*King County has focused on the feasibility of transit-oriented-development on the Kirkland portion of the Park and Ride site alone.*" They said "**alone**". If the County wants to do it alone, let the pay for all the mitigation measures. Apparently, a deal was cut without our input. We did not elect our Council to represent someone else. The advisory groups who do represent us were assigned the task of updating the existing policies. Their recommendations are being ignored.

Staff says, *the City of Kirkland's Comprehensive Plan supports the development of **mixed use** at the South Kirkland Park and Ride.* (This is old information.) With new information currently being considered, the neighborhoods do not support this TOD. This is now, not then.)

The neighborhood advisory committees decided that the scope of the TOD went beyond their desires. Later in the city's fact sheet, this is what is said. "*Develop standards that support necessary densities, expand opportunities for complementary uses, provide opportunities for all users to access the BNSF corridor, promote shared parking and transportation alternatives and mitigate traffic, visual and noise impacts to surrounding streets and residential areas.*" (**It has not and will not be done** as long as the Council refuses to add capacity to arterials, collectors, and re-designate access streets to support growth and business. The Council actions do not support the TOD. The Council has not found a way to get traffic thru or around downtown. The traffic jams will get worse on Lake Washington Blvd and 108<sup>th</sup> Ave NE. Bus users will have to use residential streets to get to and from the TOD. **The traffic jam downtown is reason enough by itself to disapprove the TOD.**

Bus turnouts must be developed with acceleration and deceleration lanes to prevent traffic backups when loading and unloading the buses, parking spaces need to be developed adjacent to the bus stops for feeder routes, and our roads redesigned to accommodate wider turn radiuses.

Page 2: Our quality of life if the TOD is approved.

In addition, as noted on the previous page, the US Department of Transportation has agreed to provide \$6.25 million for additional parking as part of a mixed use development at the South Kirkland Park and Ride. (The \$6.25 million was for parking only, not for affordable housing. None of the \$6.25 should be reduced and used for something other than parking.)

*The preliminary concept for the future of the area, as envisioned in the update of the Lakeview Chapter of the Comprehensive Plan that is underway and expected to be completed by spring, 2011, is for a more pedestrian-oriented mixed-use district, with increased retail and office uses. (That did not include the proposed TOD as requested. The mixed uses are not their now and therefore do not qualify for a site specific TOD.)*

In 2007, King County ranked the South Kirkland Park and Ride as its top TOD priority in the region. Grant funds in support of additional parking stalls and TOD in this location, in light of future tolling on SR 520, were sought and received. (The funding was for the concept of TOD's, not for a particular site. Of all the sites chosen in King County, this site was among others. The urgency of spending \$6.25 million was created by the County with polices that did not consider the Kirkland Park and Ride very high on the list until now. "**Now**" is important because **now** is also the time for the neighborhoods to update their comprehensive plans base on current information, not past information that didn't exist until the County decided to spend the money. The new information regarding the need for a much expanded TOD only came into being with the improvements to 520 along with the proposed tolling, the need for more ridership to make up for a failed METRO system, and the need for better access through Kirkland to the park and ride.)

*The Lakeview Neighborhood Chapter of the Kirkland Comprehensive Plan is supportive of TOD at the South Kirkland Park and Ride. (Not now). The Plan provides the following principles to guide future development: (What is the date of the document the City quoting from? Where are the results of the Lakeview Advisory Committee?)*

Sincerely,

Robert L. Style  
6735 Lake Washington Blvd, NE  
Kirkland, WA 98033  
425-827-0216

**Dorian Collins**

---

**From:** Eric Shields  
**Sent:** Friday, November 19, 2010 4:01 PM  
**To:** Dorian Collins; Janice Soloff  
**Subject:** FW: Say goodbye to Kirkland as you know it

FYI

Eric Shields

**From:** Janet Jonson  
**Sent:** Friday, November 19, 2010 3:15 PM  
**To:** City Council  
**Cc:** Kurt Triplett; Marilynne Beard; Eric Shields; Paul Stewart  
**Subject:** FW: Say goodbye to Kirkland as you know it

Janet Jonson  
City Manager's Office  
City of Kirkland  
123 5th Avenue  
Kirkland, WA 98033  
425-587-3007  
425-587-3019 fax  
[jjonson@ci.kirkland.wa.us](mailto:jjonson@ci.kirkland.wa.us)

**From:** [RLSTYLE@aol.com](mailto:RLSTYLE@aol.com) [<mailto:RLSTYLE@aol.com>]  
**Posted At:** Friday, November 19, 2010 1:41 PM  
**Posted To:** Kirkland Council  
**Conversation:** Say goodbye to Kirkland as you know it  
**Subject:** Say goodbye to Kirkland as you know it

Paul: Make sure this forwarded to the Houghton Community Council.

Say goodbye to Kirkland as you know it.

At the last Council meeting, the Council violated the trust of neighborhoods, neighborhoods that trusted the Council to protect their interest as they did for almost all neighborhoods, especially Norkirk and Highlands regarding traffic. That all went out the window at the Council meeting Tuesday night.

The Council decided to ram the proposed TOD (Transit Oriented Development) down the throats of Central Houghton and Lakeview neighborhoods even leaving the neighborhoods who don't want it off the list of those whose agreement is necessary for the "Mutual Objectives and Principles of Agreement for the South Kirkland Park and Ride Transit Oriented Development (TOD) Project.

Specifically, the Lakeview Neighborhood has veto power of land uses, a legal agreement that was required when Kirkland wanted Lakeview as part of Kirkland in 1968. Call their omission stupidity. To leave them off the list was inexcusable. I call it arrogance and a breach of trust.

If the TOD is approved, 108<sup>th</sup> Ave. NE and Lake Washington Blvd, NE will jammed, congested for hours, in order to get those who get off or on the bus thru Kirkland to their homes. The bus routes to and from the site will become crowded. Where are those riders going to park to catch the interconnected routes (230,234, 254, and 255) to the site? If it's going to be in your neighborhood, it will degrade your quality of life. You will pay the price.

There may be mitigating measures to protect neighborhoods but I don't think so. In order to prevent the traffic backup when busses are loaded and unloaded, turnouts with acceleration and decelerating lanes are needed. More parking in neighborhoods is needed. Roads need to be wider. Road capacity thru Kirkland is essential; however the Council doesn't want it. And yet, they are willing to sacrifice where you live.

If it does, say goodbye to Kirkland as you know it.

Bob Style

**Dorian Collins**

---

**From:** Janice Soloff  
**Sent:** Tuesday, November 16, 2010 9:46 AM  
**Cc:** Dorian Collins  
**Subject:** Email from Robert Style

Houghton Community Council,

At the request of Bob Style I am forwarding you an email he sent to City Council related to the principles of agreement for the transit oriented development at the South Kirkland Park and Ride on the Council agenda for tonight.

Janice Coogan (Soloff)  
Planning and Community Development  
425-587-3257  
[jsoloff@ci.kirkland.wa.us](mailto:jsoloff@ci.kirkland.wa.us)  
[www.ci.kirkland.wa.us](http://www.ci.kirkland.wa.us)

**From:** [RLSTYLE@aol.com](mailto:RLSTYLE@aol.com) [<mailto:RLSTYLE@aol.com>]  
**Sent:** Tuesday, November 16, 2010 5:47 AM  
**To:** Paul Stewart; Janice Soloff  
**Subject:** Fwd: Tomorrows (11/16/10) agenda

Please forward my letter to the Council to the Houghton Community Council.

Take note of my TOD comments in regard to how the Council has interacted with the Highlands and the Norkirk neighborhoods.

Bob Style

From: [RLSTYLE@aol.com](mailto:RLSTYLE@aol.com)  
To: [citycouncil@ci.kirkland.wa.us](mailto:citycouncil@ci.kirkland.wa.us)  
CC: [kirklandviews@gmail.com](mailto:kirklandviews@gmail.com), [editor@kirklandreporter.com](mailto:editor@kirklandreporter.com), [editor@eastidesun.com](mailto:editor@eastidesun.com)  
Sent: 11/15/2010 12:28:27 P.M. Pacific Standard Time  
Subj: Tomorrows (11/16/10) agenda

Honorable Councilmembers:

One good budget decision not to include funds from the states liquor business does not excuse poor planning. The city is still facing a shortfall of millions as reflected in their request for a \$35 to \$45 million to pay for annexation costs. It's not for maintaining the service levels for the current citizens of Kirkland. It is an additional debt in the form of a Councilmatic bond without the approval of the people. Not asking for public approval has become a habit. We end up paying for it. There's over \$4 million in the budget that should be used to reduce the budget deficit. Instead, it is being spent on unnecessary and on non-essential projects (NM0058 and NM0041 in the CIP) at a time when we have a budget shortfall.

Also on the agenda is an agreement for a TOD at the South Kirkland Park and Ride. Whereas the Council previously honored a request from the Norkirk Neighborhood to not open up 111<sup>th</sup> Ave. NE to

traffic and also honor the request of the Highlands Neighborhood not to increase the traffic on 124<sup>th</sup> Ave NE, the Council is now ignoring the request of the Lakeview and Central Houghton neighborhoods to prevent more traffic jams on the streets serving their neighborhoods. . The Council should honor the request of the Lakeview and Central Houghton neighborhoods just like they did for Norkirk and Highlands. The Council is playing favorites by being consistently inconsistent.

One good deed is not excuse for bad planning. Consider the following.

Opening up 111<sup>th</sup> Ave NE was on the agenda years ago for a budget of less than a million when Doris Cooper was still on the Council . It caused the Council to spend more than \$2 million on the 100<sup>th</sup> Street overpass of 405. Now, what was going to be a road for better traffic circulation for \$700,000 is limited to the fire department, not the public, at a cost for another \$2 million dollars making the total over \$4 million for what they could have had for \$700,000 that included a better transportation system.

Bad planning, yes. Expensive, yes.

Also on the agenda is the Countywide planning process. Kirkland's regional role has never included the necessary traffic circulation that focused on growth centers and high density locations, something the Council says they want. That's because the Council has refused to add the capacity that is needed to support what they want. The Council needs to either support regional transportation planning or allow Kirkland to protect its neighborhoods. If they protect our neighborhoods, the TOD at the South Kirkland Park and Ride will not be approved unless some very expensive mitigation measures are taken to protect our neighborhoods. Knowing what's happened before regarding Norkirk, Highlands, Lakeview, and Central Houghton neighborhoods, the Council cannot be relied on keeping their promise. Their treatment of each neighborhood is not consistent. Kirkland's role in King County Countywide Planning should reflect the Council's desire to protect our neighborhoods, either that or increase our road capacity to reflect support for growth centers and high density locations. **What's it going to be?**

Robert L. Style  
6735 Lake Washington Blvd, NE  
Kirkland, WA 98033  
425 827 0216

**Dorian Collins**

---

**From:** Dorian Collins  
**Sent:** Wednesday, February 02, 2011 11:27 AM  
**To:** Dorian Collins  
**Subject:** FW: K. Views Blog: So. Kirk. P&R/TOD

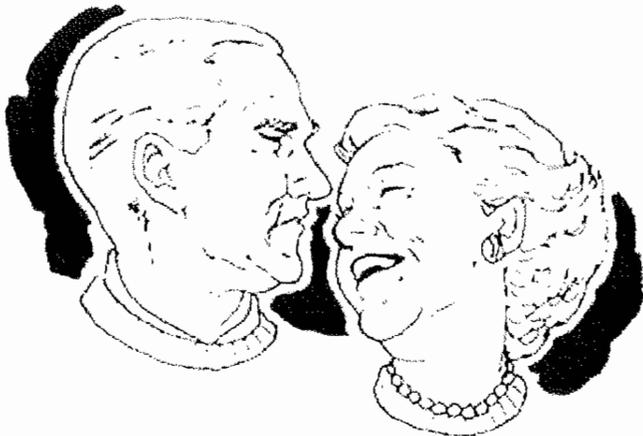
**From:** Marie Stake  
**Sent:** Tuesday, November 16, 2010 10:01 AM  
**To:** Kurt Triplett; Marilynne Beard; Eric Shields; Paul Stewart; Dorian Collins; Janice Soloff  
**Subject:** K. Views Blog: So. Kirk. P&R/TOD

[http://www.kirklandviews.com/archives/22200?utm\\_source=feedburner&utm\\_medium=email&utm\\_campaign=Feed%3A+KirklandViews+%28Kirkland+Views%29](http://www.kirklandviews.com/archives/22200?utm_source=feedburner&utm_medium=email&utm_campaign=Feed%3A+KirklandViews+%28Kirkland+Views%29)

Marie

## Is Transit Oriented Development right for Kirkland?

by Ralph and Gladys on November 16, 2010 in Opinion



(un)common sense  
with Ralph & Gladys  
fake names, real opinions

Definition: Transit Oriented Development

Development that maximizes the use of transit and reduces the use of single occupancy vehicles, by increasing the opportunities to walk, bicycle, carpool or take transit. The center of a TOD neighborhood has a bus or rail station, generally surrounded by higher-density development.

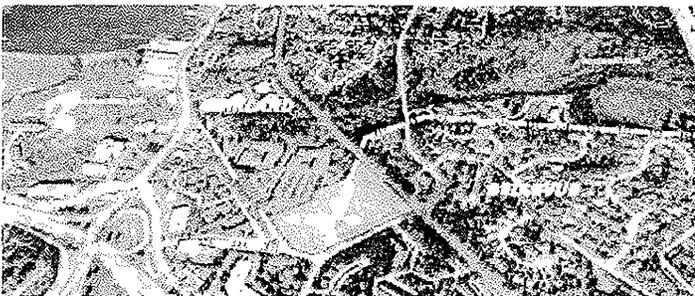


Have you seen what they are planning to do at the **South Kirkland Park & Ride**? They want to build a huge "affordable housing" Transit Oriented Development complex where there is now a park and ride.

*Affordable housing in that location? you ask.*

Yes, someone in city hall thinks it is a good idea to spread the wealth so to speak, and put affordable housing in every neighborhood. What kind of sense does that make? Next thing we know there will be a lovely tenement propped up along the waterfront. Be damned with the cost of land as a consideration as to where affordable housing should be placed. Our bull-headed do-gooders know what's best for us.

For those who are itching to call me an elitist or a NIMBY, hold your horses for two seconds. The logic of my argument is as sound as the sky is blue. By putting affordable housing in every neighborhood, we ignore the fact that some areas have higher land values than other areas. Why waste good money on high land costs when that same money could be spent on MORE AFFORDABLE HOUSING if only you put it where it makes economic sense.



Click to enlarge.

And speaking of economic sense, what is the logic behind putting 200 units (up to 100 of them are affordable) in a location where there is no retail, hardly any services and the only thing to eat is Burgermaster, The Keg and the convenience store at the gas station on Lake Washington Blvd.?!? All of the people who will live in this Transit Oriented Development will have to jump on the bus or get in a car to get services. Not very eco-friendly in my mind. Wouldn't a Transit Oriented Development be better sited WHERE THERE ARE SERVICES, like Totem Lake?

Now I don't know what "affordable" means these days but I can tell you this: as soon as someone who buys an affordable unit wants to sell it because he can't stand the noise and the smell of diesel bus fumes wafting in his windows, he will sell it AT MARKET RATES!!! Goodbye affordable housing, and hello boondoggle!!!

Bellevue owns half the land in question and wants nothing to do with Kirkland's cockamamie plans. I wonder why no one else is onboard if it is such a good idea?

Both the **Lake View neighborhood** and the **Houghton neighborhood** are staunchly against this plan, but those in power have ignored them. If the neighbors are so much against the plan, how can it be such a good idea?

I smell a rat. The South Kirkland Transit Oriented Development is being pushed by some at city hall when it doesn't make sense because they have a different agenda. They want to build as much affordable housing as they can despite what everyone else thinks about it because they think they know what's best for us.

Well I am tired of people thinking they are smarter than everyone else in the room. The neighbors know what's best, not some politicians or bureaucrats in city hall.

Ralph

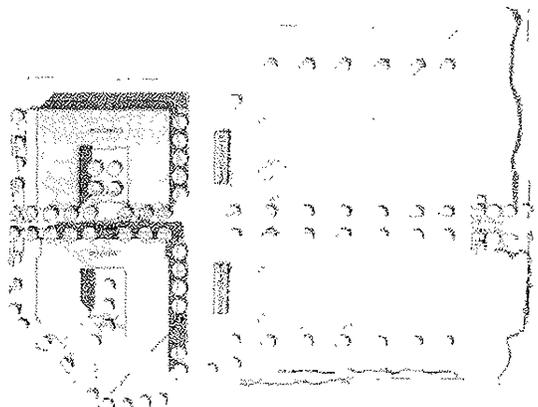


Ralph,

I don't know how much longer I can put up with your neanderthal reasoning! You would have been a hero to John Rockefeller and Andrew Mellon in the 1800's when they were pillaging the American economy and it's workers. Marie Antoinette, with her "let them eat cake" mentality would have loved you. The lower classes be damned as far as you are concerned.

Putting affordable housing next to public transportation is not only sensible, it is in consonance with the **Growth Management** policies which this state enacted years ago. Where have you been for the last 20 years!?

Sprawl has not worked for anyone and Transit Oriented Development is a smart tactic to try to make it easier for people to get to and from work using public transportation.



Planned TOD development (Click to enlarge)

By the way, providing affordable housing has been a core value of the state, county and city for years. You would have us build more tent cities?

Of course the Houghton neighborhoods are against this project. They define the term, *Not in my back yard, NIMBY*. Thanks to them we lost the **Lake Washington School District Headquarters** and it's good paying jobs. They are against anything and everything progressive in their territory.

You talk about Bellevue not wanting any part of the project. Since when is Bellevue a good example of anything forward looking?!? Their city council squabbling is an embarrassment.

Kudos to our city council for trying to do the right thing.

Kindest regards,

Gladys

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(un)common sense is a column featuring personal views on issues from around town as seen through the eyes of these long-time Kirkland residents.

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**Dorian Collins**

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**Subject:** FW: S. Kirkland Park and Ride - Comparative TODs are < 1/2 as dense and near svcs  
**Attachments:** Comparative details TOD projects.xls

**From:** Janet Jonson  
**Sent:** Tuesday, November 16, 2010 3:39 PM  
**To:** City Council  
**Cc:** Kurt Triplett; Marilynne Beard; Eric Shields; Paul Stewart; Ray Steiger; David Godfrey; Kathi Anderson; Cheri Aldred  
**Subject:** FW: S. Kirkland Park and Ride - Comparative TODs are < 1/2 as dense and near svcs

Council: For tonight's Council meeting. JJ

Janet Jonson  
City Manager's Office  
City of Kirkland  
123 5th Avenue  
Kirkland, WA 98033  
425 587 3007  
425 587 3019 fax  
[jjonson@ci.kirkland.wa.us](mailto:jjonson@ci.kirkland.wa.us)

**From:** [Uwkkq@aol.com](mailto:Uwkkq@aol.com) [<mailto:Uwkkq@aol.com>]  
**Posted At:** Tuesday, November 16, 2010 3:14 PM  
**Posted To:** Kirkland Council  
**Conversation:** S. Kirkland Park and Ride - Comparative TODs are < 1/2 as dense and near svcs  
**Subject:** S. Kirkland Park and Ride - Comparative TODs are < 1/2 as dense and near svcs

Hi and thank you for taking time to review the TOD at S. Kirkland P&R.

As you all know TOD has for many, many years been considered for Totem Lake. It has been in the comprehensive plan for years and is appropriately zoned in anticipation of this event.

S. Kirkland P&R is needed for parking, lots of parking.... It is the last entry to transit before the bridge. Building a housing TOD here will put a permanent cap on parking and will only raise the overall # of spaces by approximately 50 spaces after you factor in the additional parking that will be needed by residents.

Built to the size and scale as proposed, the S Kirkland P&R would have be approximately 2 Portsmouths worth of housing and bulk on approximately the same amount of acreage per unit. When Bellevue comes along it will be approximately 3.5-4 Portsmouths. Quite a HUGE amount of housing, 400-450 new residents as Kirkland alone and nearly 1000 residents when Bellevue joins. Then on top of the residents, add in their guests and all the transit users... You have got one heck of a lot of people in an area where there are not a lot of jobs, not a lot of healthcare, not a lot of retail, groceries or other.

The HCC requested information on comparative TODs in mid-2008. The the Lakeview Advisory group asked for comparative information (if they'd prepared for HCC that would be acceptable). To date there has been no comparative details given.

The Seattle area TODs do not easily list information like units/acre, etc, but fortunately staff has referenced California projects.

Attached you will see California projects. Please note that each is only 1/2 as many units per acre ... even for the most crowded ones. All have services right near by and thus fewer residents require cars and yet even more parking than proposed for S. Kirkland.

Many of the TODs have started with initial project of as little as 41 units. Renton is 90 units. And yet we are told that we must have at least 200 to start with and then another 200-300 when Bellevue decides to join in...

Why?

Also as far as the housing being part of the grant requirements, if you go to the website for the grant, you will see that 4 cities were chosen and there DOES NOT appear to be any requirement for housing.

- Miami
- Minneapolis/St. Paul
- San Francisco Bay-Area
- Seattle (Lake Washington)

If you look at the requirements, there are 4 T's and no H (or housing)

T- Tolling

T- Transit

T- Telecommuting

T- Technology

<http://www.upa.dot.gov/agreements/docs/termsheetseattle.htm>

This TOD may be very good in an area that is not locked up with traffic and deplete of necessary services that are provided at all other TODs. It is too dense for this location and it robs us of the potential for large increase in parking when needed in the future.

Thank you,

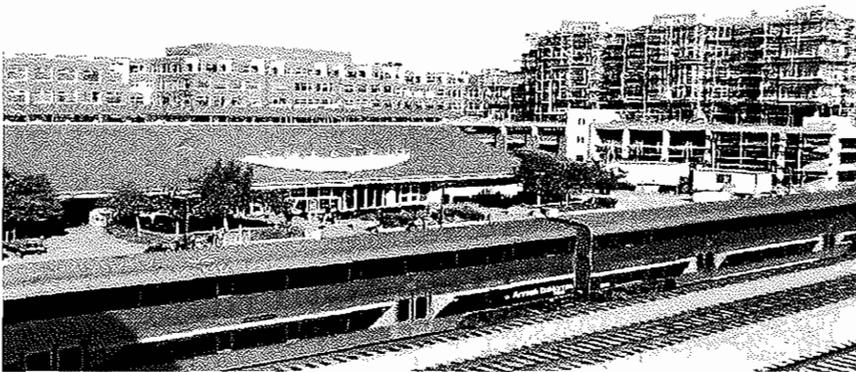
Karen Levenson

President

The Park, A Condominium

6620 Lake Washington Blvd NE, Kirkland 98033

		# units	Acres	Units/acre	Low Mid Income Units
Emeryville	Emery Station	101	20	5	N
Mountain View	Whisman Station	503	65	8	N
Mountain View	The Crossings	359	18	20	N
Hayward Bart	Atherton Place	83	3.5	24	N
San Jose	Ohlone-Chynoweth	194	7.3	27	Y
Pleasant Hill	Millenium Partners	500	18	28	N
Richmond	(ownership housing)	230	16	14	
Hercules	Transit Village	450	20	22	
S. Kirkland		200	3.5	57	Y
Renton	Metropolitan Place	90			Y
Auburn (proposed TOD)					
Redmond			4.8		
Overlake	Village @ O Station	308	5	62	Y
Kent Station			22		



Emery Station - Emeryville, CA



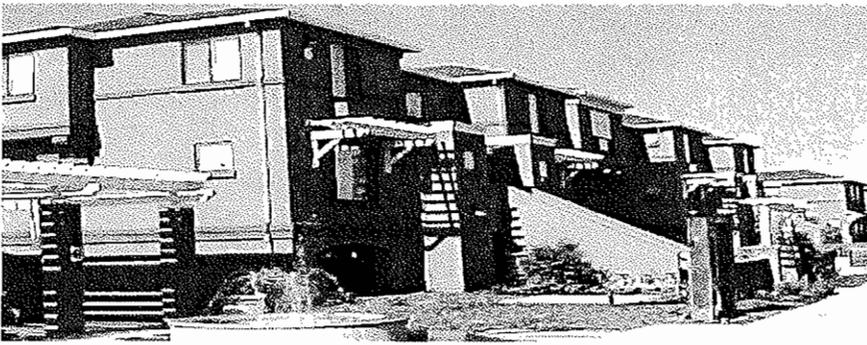
Whisman Station, Mount



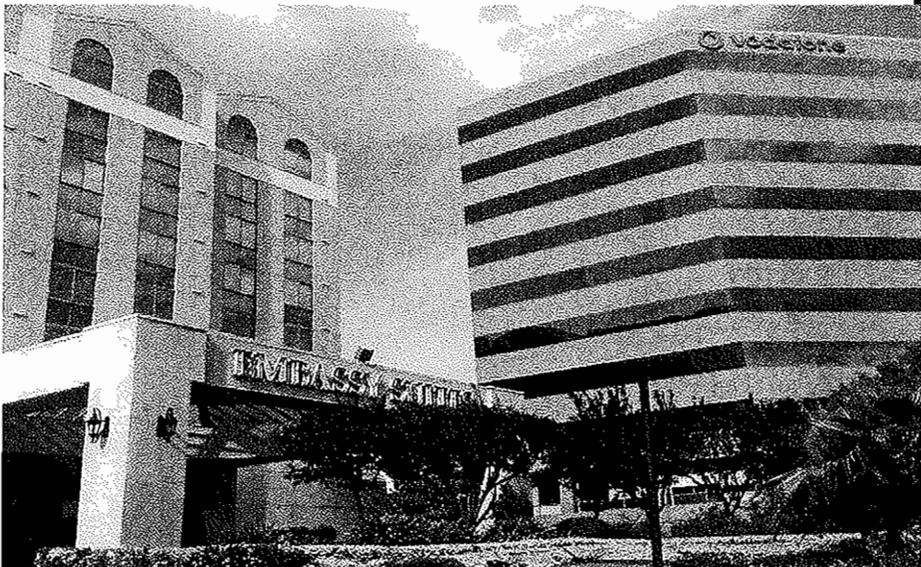
Atherton Place, Hayward, CA



Richmond Village, Richm



Ohlone-Chynoweth, San Jose, CA



Millenium Partners, Avalon, Walnut Creek/Pleasant Hill, California  
9 story business complex and Hotel across the street  
Other street corners have similar tall business complexes  
Cafes, high end gym, etc on the neighboring street corners

Pkg/unit	Add'l Pkg Structure	Max Height (ft)	Notes:
1.2		80	3 buildings (2 mid-rise office buildings) Retail
2.5		40	3 Twnhouse dev & open space, limited svcs in
2		60	Grocery Store
1		55	
1.7		90	
1.4	Freestanding Pkg Garage	150	150 ft Office bldgs around
800 spaces not specified	Freestanding Pkg Garage	3 stories	
1		70	
1 + .3 shared	Y	5-6 stories	64 blocks of Renton Regional Growth Ctr, Shops, Grocery
	Now 600 pkg garage proposed		
		6 stories	
1.7		5-6 stories	In heart of commercial area, grocery, restaura



Mountain View, CA



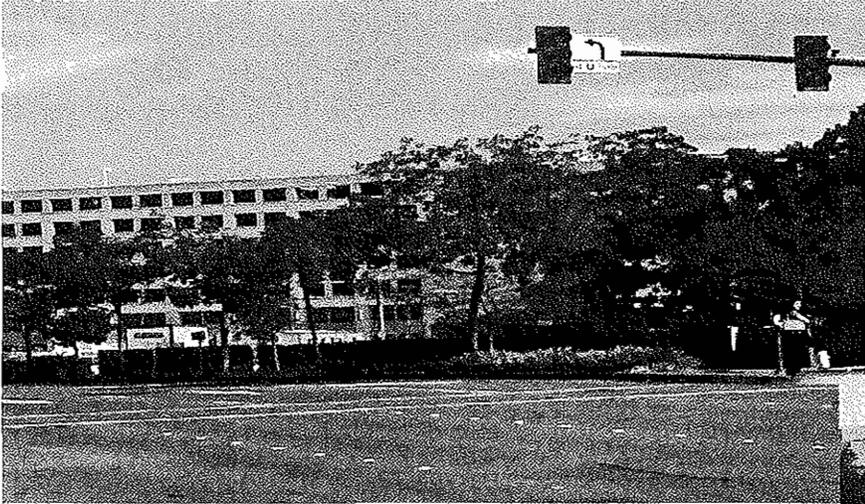
The Crossings, Mountain View, CA



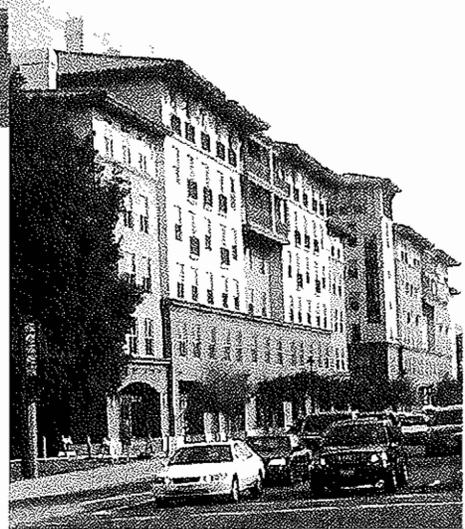
Mountain View, CA



Hercules Transit Village, Hercules, CA



6 story Freestanding parking garage  
4 lanes of traffic southbound  
4 lanes of traffic northbound



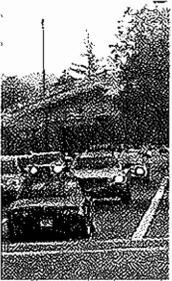
4 of 8 lanes of traffic west and east  
Retail and parking main level, 6 st

area therefore lighter than anticipated use of transit

int, major retailers, personal svcs, daycare



CA



tbound  
ories up to 150 ft tall

**Dorian Collins**

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**From:** Paul Stewart  
**Sent:** Tuesday, November 16, 2010 2:43 PM  
**To:** Dorian Collins; Janice Soloff  
**Subject:** FW: TOD at South Kirkland Park and Ride

FYI

-----Original Message-----

*From:* Janet Jonson  
*Sent:* Tuesday, November 16, 2010 2:42 PM  
*To:* City Council  
*Cc:* Kurt Triplett; Marilynne Beard; Eric Shields; Paul Stewart; Ray Steiger; David Godfrey; Kathi Anderson; Cheri Aldred  
*Subject:* FW: TOD at South Kirkland Park and Ride

*Council:* For tonight's Council meeting. JJ

Janet Jonson  
City Manager's Office  
City of Kirkland  
123 5th Avenue  
Kirkland, WA 98033  
425-587-3007  
425-587-3019 fax  
[jjonson@ci.kirkland.wa.us](mailto:jjonson@ci.kirkland.wa.us)

-----Original Message-----

*From:* Chuck Pilcher [<mailto:chuck@bourlandweb.com>] *Posted At:* Tuesday, November 16, 2010 2:36 PM *Posted To:* Kirkland Council  
*Conversation:* TOD at South Kirkland Park and Ride  
*Subject:* TOD at South Kirkland Park and Ride

*Dear Mayor and Council:*

*I am opposed to the plans for the Transit Oriented Development at the South Kirkland Park and Ride for the following reasons:*

- 1. It seems to me that a faction of the Council/Staff support this idea simply for fear of losing a \$4million + contribution from either King County or the federal government, I believe it's the latter. It seems imprudent to spend additional money just because someone else is putting forward a small ante. We need to make sure the project makes good sense for us, not just respond because some OTHER agency thinks it makes good sense for us.*
- 2. I agree with adding parking to the SKPR, but we don't need to add housing. See below.*
- 3. I agree we could stand to improve our housing with a TOD, but the SKPR is the wrong location. The Lakeview and Houghton Neighborhood Advisory Groups are both on record as opposing it, especially with the addition of "affordable housing" in an otherwise upscale neighborhood.*
- 4. The Totem Lake area surrounding Evergreen Hospital could benefit much more from increased density and affordable housing than S. Kirkland. Totem Lake badly needs an infusion of SOMETHING, and the hospital staff could use some affordable housing. Putting a TOD near the new Transit Center (developed jointly with COK) would actually IMPROVE the housing mix in that area, and help TL become even more of an economic engine. We would also not have to add new retail to the extent that a SKPR location would require.*
- 5. Finally, doing this without the cooperation of the City of Bellevue, which "owns" adjacent property which should be a part of such a SKPR TOD is non-sensical. If our major neighbor doesn't think this deserves their participation, perhaps we should pay attention.*

*Please consider the above in your deliberations. I am sure that those providing funds on the table for the SKPR TOD could be persuaded to transfer those funds to a better, more community-acceptable, location like Totem Lake.*

*Chuck Pilcher*  
*[chuck@bourlandweb.com](mailto:chuck@bourlandweb.com)*  
*206-915-8593*

**Dorian Collins**

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**From:** Janet Jonson  
**Sent:** Monday, December 06, 2010 8:43 AM  
**To:** City Council  
**Cc:** Kurt Triplett; Marilynne Beard; Eric Shields; Paul Stewart; Dawn Nelson; Dorian Collins; Janice Soloff; Ellen Miller-Wolfe  
**Subject:** FW: TOD comments

Council: The Houghton Community Council and Planning Commission will be discussing this project at their joint meeting on December 13<sup>th</sup>. JJ

Janet Jonson  
City Manager's Office  
City of Kirkland  
123 5th Avenue  
Kirkland, WA 98033  
425 587 3007  
425 587 3019 fax  
[jjonson@ci.kirkland.wa.us](mailto:jjonson@ci.kirkland.wa.us)

**From:** [RLSTYLE@aol.com](mailto:RLSTYLE@aol.com) [<mailto:RLSTYLE@aol.com>]  
**Posted At:** Saturday, December 04, 2010 4:41 AM  
**Posted To:** Kirkland Council  
**Conversation:** TOD comments  
**Subject:** TOD comments

I you live in Kirkland or north of Kirkland, you're putting up with traffic jams. If the TOD is approved, it will get unacceptably worse.

The Council has not taken actions to relieve congestion. Instead of adding capacity on its arterials, instead of adding capacity on their collector streets, they have endorsed congestion that forces traffic to use residential streets. Now they want to exacerbate the problem, not solve the problem. They're created more congestion in order to get us out of our cars.

The Council has created congestion in hopes of forcing commuters to use busses instead of allowing us the freedom to use using our vehicles. The last time I checked, Kirkland was still a suburban city. We are a residential community. People like it here because it's not a Seattle. Yet, the Council is insidiously trying to change it.

The TOD will result in more and longer periods of traffic jams on 108<sup>th</sup> Ave NE, Lake Washington Blvd all of which lead to downtown where the real jam up is the most notable. The ridership has show that most of the people getting on or off the busses need to go through Kirkland to get catch a bus or to get home.

The first time downtown jams can be avoided is an exit at NE 70<sup>th</sup> Street to I-405 which also is jammed: so much for good planning.

The argument can be made that the Council has not complied with the Concurrency requirement of the Growth Management Act. They've bastardized the definition of the LOS (Level of Service) to foster traffic jams instead of promoting and protecting what was once Kirkland.

While having more affordable housing may be good for some, it also comes with its bad features that override the marginal benefits of affordable housing. Kirkland already has 25% of its housing stock set aside for low income housing. The more we have, the less income the City has to pay for services.

Look at the decisions that must be made if the TOD is approved. Residential areas served by bus routes will need more parking lots adjacent to bus stops. Turnouts with acceleration and deceleration lanes will be needed to prevent backups when the busses are loading. Lake Washington Blvd, NE and 108<sup>th</sup> Ave NE will have to become 4 lanes going nowhere because the Council has not found a way to get traffic thru or around downtown. It's a bottleneck of momentous proportion. Traffic signs reading **Local Access Only** in residential areas will have to be removed and replaced with signs saying **To be Used if Congest is encountered**.

I don't think we'll get the 4 lanes. What then?

With so many jurisdictions putting tolls on roads, why doesn't the city put tolls on its roads? Rebates could be sent to the residents of Kirkland? It would help redirect thru traffic around Kirkland. Why not declare Lake Washington Blvd NE a scenic route with pedestrian and bicycle paths? It too should have a toll significant enough to discourage thru traffic.

The Council is hell bent on making Kirkland like Seattle. They say affordable housing can go anywhere in the city. They wrong on both occasions. The number of mitigations required to protect Kirkland residential areas are incredible. There are too many and too costly. Wouldn't it be better if the TOD were not approved. At least we will be able to keep Kirkland a city that people move to because it is a residential community.

Bob Style

Dorian Collins

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**From:** Paul Stewart  
**Sent:** Wednesday, September 22, 2010 1:29 PM  
**To:** Dorian Collins  
**Subject:** FW: The TOD

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**From:** Janet Jonson  
**Sent:** Wednesday, September 22, 2010 1:28:36 PM  
**To:** City Council  
**Cc:** Kurt Triplett; Marilynne Beard; Eric Shields; Paul Stewart  
**Subject:** FW: The TOD  
**Auto forwarded by a Rule**

Janet Jonson  
City Manager's Office  
City of Kirkland  
123 5th Avenue  
Kirkland, WA 98033  
425 587 3007  
425 587 3019 fax  
[jjonson@ci.kirkland.wa.us](mailto:jjonson@ci.kirkland.wa.us)

**From:** [RLSTYLE@aol.com](mailto:RLSTYLE@aol.com) [<mailto:RLSTYLE@aol.com>]  
**Posted At:** Wednesday, September 22, 2010 10:55 AM  
**Posted To:** Kirkland Council  
**Conversation:** The TOD  
**Subject:** The TOD

Honorable Councilmembers:

Many very enlightening disappointments were revealed in your vote to proceed with the TOD.

Your decision to exclude the neighborhood from influencing what goes on in their neighborhood. You limited their ability to determine the outcome. Why am I not shocked? You've done it before. Why don't you just quit having neighborhood meetings if you're not going to listen to them? Once the development is started even in the planning stage, the mitigation measures will not solve or even ease the problem.

No Councilmember even mentioned or discussed traffic. Lake Washington Blvd and 108th Ave NE are already congested for more than 3 hours a day. Yes, you could require impact fees from METRO to add capacity to our roads but the Council has refused to add capacity. I don't think you will. You could require the County and State to add capacity to their roads so as to allow traffic to get from the park & ride to their homes. I don't think you will. You could make 108th Ave. NE and Lake Washington Blvd toll roads. Make them scenic routes and through a barcode system, charge those who do not

live in Kirkland. I don't think you will. I think you do what you want without regard to the neighborhood concerns and our quality of life.

You arrogantly and autocratically decided to proceed independently. Who's running the city, you or the County?

Much of the discussion was about affordable housing. It's interesting that the \$6.25 million grant was for park & ride improvements, not affordable housing. What's there now doesn't even qualify using TOD criteria. It must be developed. Affordable housing was an earmark added by special interest especially at this location.

To qualify for the units, it will be expensive. Even in the moderate affordable housing category of 80%, it will not even come close to the market value of the unit. What's 80% of a half million dollar condo? Affordable housing should be at a location that's affordable. The application only ask how much you make and doesn't care about what the applicant spends their money.

I've written much about affordable housing. Many people have been subject to unforeseen circumstances. However, most have made bad financial decisions.

The affordable housing subsidy allows families to spend what they would like instead going toward their housing cost. They don't want to sacrifice like others have in order to have the housing they want. They prefer to spend it on cable or satellite TV, Internet, an expensive car rather than an inexpensive one and maybe more cars than what they need. The options for spending money on something other than housing are numerous.

So I say to you, get back to representing the citizens of Kirkland. Protect their properties, protect and enhance their freedoms, and do so treating everyone equally and with respect.

Robert L. Style  
6735 Lake Washington Blvd, NE  
Kirkland, WA 98033  
425-827-0216

**Dorian Collins**

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**From:** Joanie Dolsen [joanie4@comcast.net]  
**Sent:** Tuesday, September 21, 2010 4:58 PM  
**To:** Joan McBride; Penny Sweet; Dave Asher; Jessica Greenway; Doreen Marchione; Amy Walen; Bob Sternoff  
**Cc:** Paul Stewart; David Godfrey; Eric Shields; Dorian Collins; Angela Mason  
**Subject:** TOD in the South Kirkland Park & Ride

**Please do not create a TOD in South Kirkland P&R!**

**Dorian Collins**

---

**From:** Dale Sunitsch [dales5@comcast.net]  
**Sent:** Thursday, September 23, 2010 10:15 AM  
**To:** Joan McBride; Penny Sweet; Dave Asher; Jessica Greenway; Doreen Marchione; Amy Walen; Bob Sternoff  
**Cc:** Paul Stewart; David Godfrey; Eric Shields; Dorian Collins; Angela Mason

**Please do not create a TOD in South Kirkland P&R!!!!**

**Dorian Collins**

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**From:** Annemarie Riese [amriese@msn.com]  
**Sent:** Tuesday, September 21, 2010 10:33 AM  
**To:** Joan McBride; Penny Sweet; Dave Asher; Jessica Greenway; Doreen Marchione; Amy Walen; Bob Sternoff; Paul Stewart; David Godfrey; Eric Shields; Dorian Collins; Angela Mason  
**Subject:** South Kirkland Park & Ride TOD

Please do not create a TOD in South Kirkland P&R.

The impact will be too much for our infrastructure. The 108th corridor between 520 and Houghton is becoming overwhelmed with traffic congestion and it makes it difficult to move about in our own neighborhood. The intersection of 108th and Northrup is continuously backed up during rush hours and this project would only add to the volume. The other concern would be parking overflow which again impacts our neighborhood...just this past weekend cars lined our neighborhood streets which makes it unsafe for the children to play in their own yard. Also, once construction begins on the "520 Project" this too will have a negative impact on our neighborhood....we can't take much more!

A concerned citizen.

**Dorian Collins**

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**From:** Paul Stewart  
**Sent:** Wednesday, September 22, 2010 1:29 PM  
**To:** Dorian Collins  
**Subject:** FW: The TOD

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**From:** Janet Jonson  
**Sent:** Wednesday, September 22, 2010 1:28:36 PM  
**To:** City Council  
**Cc:** Kurt Triplett; Marilynne Beard; Eric Shields; Paul Stewart  
**Subject:** FW: The TOD  
**Auto forwarded by a Rule**

Janet Jonson  
City Manager's Office  
City of Kirkland  
123 5th Avenue  
Kirkland, WA 98033  
425-587-3007  
425-587-3019 fax  
[jjonson@ci.kirkland.wa.us](mailto:jjonson@ci.kirkland.wa.us)

**From:** RLSTYLE@aol.com [mailto:RLSTYLE@aol.com]  
**Posted At:** Wednesday, September 22, 2010 10:55 AM  
**Posted To:** Kirkland Council  
**Conversation:** The TOD  
**Subject:** The TOD

Honorable Councilmembers:

Many very enlightening disappointments were revealed in your vote to proceed with the TOD.

Your decision to exclude the neighborhood from influencing what goes on in their neighborhood. You limited their ability to determine the outcome. Why am I not shocked? You've done it before. Why don't you just quit having neighborhood meetings if you're not going to listen to them? Once the development is started even in the planning stage, the mitigation measures will not solve or even ease the problem.

No Councilmember even mentioned or discussed traffic. Lake Washington Blvd and 108th Ave NE are already congested for more than 3 hours a day. Yes, you could require impact fees from METRO to add capacity to our roads but the Council has refused to add capacity. I don't think you will. You could require the County and State to add capacity to their roads so as to allow traffic to get from the park & ride to their homes. I don't think you will. You could make 108th Ave. NE and Lake Washington Blvd toll roads. Make them scenic routes and through a barcode system, charge those who do not

live in Kirkland. I don't think you will. I think you do what you want without regard to the neighborhood concerns and our quality of life.

You arrogantly and autocratically decided to proceed independently. Who's running the city, you or the County?

Much of the discussion was about affordable housing. It's interesting that the \$6.25 million grant was for park & ride improvements, not affordable housing. What's there now doesn't even qualify using TOD criteria. It must be developed. Affordable housing was an earmark added by special interest especially at this location.

To qualify for the units, it will be expensive. Even in the moderate affordable housing category of 80%, it will not even come close to the market value of the unit. What's 80% of a half million dollar condo? Affordable housing should be at a location that's affordable. The application only ask how much you make and doesn't care about what the applicant spends their money.

I've written much about affordable housing. Many people have been subject to unforeseen circumstances. However, most have made bad financial decisions.

The affordable housing subsidy allows families to spend what they would like instead going toward their housing cost. They don't want to sacrifice like others have in order to have the housing they want. They prefer to spend it on cable or satellite TV, Internet, an expensive car rather than an inexpensive one and maybe more cars than what they need. The options for spending money on something other than housing are numerous.

So I say to you, get back to representing the citizens of Kirkland. Protect their properties, protect and enhance their freedoms, and do so treating everyone equally and with respect.

Robert L. Style  
6735 Lake Washington Blvd, NE  
Kirkland, WA 98033  
425-827-0216

**Dorian Collins**

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**From:** Janice Soloff  
**Sent:** Tuesday, September 21, 2010 10:29 AM  
**To:** Dorian Collins  
**Subject:** FW: Park and Ride - Clarification on proposal

Janice Soloff  
Planning and Community Development  
425-587-3257  
[jsoloff@ci.kirkland.wa.us](mailto:jsoloff@ci.kirkland.wa.us)  
[www.ci.kirkland.wa.us](http://www.ci.kirkland.wa.us)

**From:** [Uwkg@aol.com](mailto:Uwkg@aol.com) [mailto:[Uwkg@aol.com](mailto:Uwkg@aol.com)]  
**Sent:** Tuesday, September 21, 2010 9:55 AM  
**To:** undisclosed-recipients  
**Subject:** Park and Ride - Clarification on proposal

Paul:

Problems with accepting the comments that "housing is included" ...there seem to be pretty LARGE inconsistencies and no documentation from Kirkland or Metro is given to back up the claim of housing.

A) The timing doesn't appear to support the claim of "housing included" It appears that the grant was applied for and processed beginning at least a couple years BEFORE Metro approached Kirkland/Bellevue suggesting housing.

B) Actual federal governments DO NOT SHOW HOUSING REQUIREMENT

The actual grant documents discovered to date are very thorough review of the grants.  
They ask for 4 requirements

- 1) Tolling (congestion-pricing) which turned out to be pivotal in their decision
- 2) Transit
- 3) Technology
- 4) Telecommuting

No housing element is stated

Transit is specifically defined "Transit projects included expanded bus service (and sometimes ferry service), including providing additional buses and bus stops, express bus routes, and park-and-ride facilities." Other items are also defined and don't include housing.

C) We keep being "told" that the grant includes housing but even when information is relayed to Gary Prince we NEVER get any official documents that actually show this. In contrast all of the official documents that residents have located are VOID OF HOUSING requirements

Please.... If in our thorough search we are missing something, please send the documentation that shows housing included.

Otherwise it seems that the failure to produce this "proof" indicates that the official documents that we are finding are correct and there is no housing requirement....

A final note... If Metro and City of Kirkland/Bellevue wanted to apply for a grant to include housing, perhaps they should have first investigated whether housing would be acceptable and if so, what scale of housing. It seems that there may have been a lot of wasted time on something that the neighborhood advisory groups (hopefully a reflection of a cross-section) are opposed to.... Maybe the advisory group should have come before the application?

Going after a grant before you know if your community will accept one is kind of like investing in a puppy as a gift to someone without first checking if the person is willing to take on the added costs and other responsibilities that come along with the "gift" .... often the gift can have a very high cost that surpasses the fact that the item was "free."Karen Levenson (Paul's comments below)

===== Paul's email

Thanks for the thoughtful comments and we will forward to the HCC as well as the Planning Commission.

I wanted to clarify a couple of misunderstandings with this that seem to be floating around. The \$6.25 is simply not enough to pay for the transit and parking improvements alone. This grant is only part of the funding for the 250 addition parking stalls. Without the development (or the "D" as you note) there will be no additional park and ride spaces or transit improvements.

Secondly, there seems to be a misimpression that housing is not part of the grant proposal. This is not accurate. The main focus of the grant proposal is to create a Sustainable Transportation Hub that will integrate housing and transportation in a mixed use and mixed income project (that's why the grant is under the Livability Initiative Grant Program). The grant proposal including housing was written this way. Gary Prince with King County can provide more information on this but housing is an integral part of the grant proposal  
[Gary.Prince@kingcounty.gov](mailto:Gary.Prince@kingcounty.gov).

I hope this clarifies a couple of points. Thanks.

Paul Stewart

**Dorian Collins**

---

**From:** Paul Stewart  
**Sent:** Tuesday, September 21, 2010 4:44 PM  
**To:** Uwkgg@aol.com  
**Cc:** Dorian Collins; Janice Soloff  
**Subject:** FW: South Kirkland Park and Ride TOD  
**Attachments:** Lake Washington Urban Partnership - FTA Final App 5 22 07.doc

Hi Karen,  
I got your e-mail and it looks like you saw Lisa's e-mail as well. I responded back to Lisa with the following.

Hi Lisa,  
I have sent your questions on to Gary and he should be able to respond in more detail. However, I want to correct something in my response. It is the Urban Partnership grant that is funding the parking (not the Livability Initiative). I was looking at the wrong grant application. Sorry for adding to the confusion.

However, it still needs the housing to make it work financially. This has been on our Planning Work Program prior to commencing the neighborhood plans and is also a follow up to the Lakeview Plan policies previously adopted a couple of years ago which call for providing for housing at the site.

Gary Prince sent the following to Lisa and I thought you would be interested in seeing this too. I hope this answers a few of the questions anyway. I'm out of town for a couple of weeks so contact Dorian Collins or Janice Soloff or even Gary if you have any more questions. Thanks. Take care.  
Paul

**From:** Prince, Gary [mailto:Gary.Prince@kingcounty.gov]  
**Sent:** Tuesday, September 21, 2010 3:40 PM  
**To:** Paul Stewart; Lisa A. McConnell  
**Cc:** Dorian Collins; Janice Soloff; Eric Shields; Posthuma, Ron  
**Subject:** RE: South Kirkland Park and Ride TOD

Attached is the Urban Partnership application submitted by King County. South Kirkland P & R is discussed on page 13 of 16 (in the printed version. It appears differently on the screen)

The relevant language is:

Metro requests \$8.4 million to assist construct multi-level parking structure with 853 stalls, 250 more than the existing surface lot, as part of a Transit Oriented development which will combine parking and housing, office, and other mixed uses at the South Kirkland Park and Ride location and for the development of a structured garage at the Redmond Transit Oriented Development (TOD) project, as well provide additional capacity at other existing facilities.

South Kirkland Park and Ride is adjacent to SR 520 near Lake Washington Blvd. current operates at capacity (603 stalls) on a daily basis. It is anticipated that once tolling begins on the SR 520 corridor there will be a strong increase in demand for parking at this facility.

Gary Prince  
Senior Project Manager  
Transit Oriented Development

**From:** Paul Stewart [mailto:PStewart@ci.kirkland.wa.us]  
**Sent:** Tuesday, September 21, 2010 2:46 PM  
**To:** Lisa A. McConnell  
**Cc:** Dorian Collins; Janice Soloff; Prince, Gary; Eric Shields  
**Subject:** RE: South Kirkland Park and Ride TOD

Hi Lisa,

I have sent your questions on to Gary and he should be able to respond in more detail. However, I want to correct something in my response to you. It is the Urban Partnership grant that is funding the parking (not the Livability Initiative). I was looking at the wrong grant application. Sorry for adding to the confusion.

However, it still needs the housing to make it work financially. This was anticipated and has been on our Planning Work Program prior to commencing the neighborhood plans. It is also a follow-up to the Lakeview Plan policies previously adopted a couple of years ago which call for providing for housing at the site.

Paul

**From:** Lisa A. McConnell [mailto:kirby994@frontier.com]  
**Sent:** Monday, September 20, 2010 8:55 PM  
**To:** Paul Stewart  
**Subject:** FW: South Kirkland Park and Ride TOD

Paul,

I did find the Livability Initiative grant you mentioned on the FTA site. Thanks for the heads up and direction (Question 1 below)

Lisa

**From:** Lisa A. McConnell [mailto:kirby994@frontier.com]  
**Sent:** Monday, September 20, 2010 8:43 PM  
**To:** 'Paul Stewart'  
**Subject:** RE: South Kirkland Park and Ride TOD

Paul,

Thank you for taking the time to actually read my opinions. I am unclear on a few things.

1. I cannot find on either the King County Metro or the WSDOT site the specific details of the Urban Partnership Agreement or the details of the grant. Would Gary Prince be the one to contact or do you have that link? None of the references to the Urban Partnership Agreement I have found mention anything about housing or mixed use. They do however mention, repeatedly, transit and parking improvements.

2. Does the term mixed use necessarily mean housing?

3. What is the status of the 2 other grants (King County EECBG and the FTA traffic signal/ped improvements)?

4. I think that including the HB2912 \$8.4M as part of funding is misleading. It would only be a portion of that amount and only if a qualified renter actually applied for it and received it. It is in no way part of the development or construction of this project and only applies AFTER the project is complete. (My search of HB 2912 showed it to be lodging tax revenue for bond repayment. I'm sure the affordable housing recipient portion is in some amendment part of the bill)

5. If the \$6.25M is not enough to cover the cost of the additional parking stalls, does that mean that the development is expected to cover the balance? And where does that leave the balance of costs for transit improvements?

Thanks again for taking the time.  
Lisa

**From:** Paul Stewart [mailto:PStewart@ci.kirkland.wa.us]  
**Sent:** Monday, September 20, 2010 3:05 PM  
**To:** Lisa A. McConnell  
**Cc:** Prince, Gary; Dorian Collins; Eric Shields; Janice Soloff; Arthur Sullivan; Betsyp@beckermayer.com; go2marine06@yahoo.com; John Kappler; Kathleen McMonigal; Lora Hein; rwhit5009@aol.com  
**Subject:** RE: South Kirkland Park and Ride TOD

Hi Lisa,

Thanks for the thoughtful comments and we will forward to the HCC as well as the Planning Commission. I wanted to clarify a couple of misunderstandings with this that seem to be floating around. The \$6.25 is simply not enough to pay for the transit and parking improvements alone. This grant is only part of the funding for the 250 addition parking stalls. Without the development (or the "D" as you note) there will be no additional park and ride spaces or transit improvements.

Secondly, there seems to be a misimpression that housing is not part of the grant proposal. This is not accurate. The main focus of the grant proposal is to create a Sustainable Transportation Hub that will integrate housing and transportation in a mixed use and mixed income project (that's why the grant is under the *Livability Initiative Grant Program*). The grant proposal including housing was written this way. Gary Prince with King County can provide more information on this but housing is an integral part of the grant proposal [Gary.Prince@kingcounty.gov](mailto:Gary.Prince@kingcounty.gov).

I hope this clarifies a couple of points. Thanks.

Paul Stewart

**From:** Lisa A. McConnell [mailto:kirby994@frontier.com]  
**Sent:** Monday, September 20, 2010 2:23 PM  
**To:** Eric Shields; Dorian Collins; Paul Stewart  
**Subject:** South Kirkland Park and Ride TOD

Please forward to Houghton Community Council (no contact email given on website)

### RE: South Kirkland Park and Ride TOD

The \$6.25 Million could easily be spent on transit and parking improvements alone. Let's make sure that we create a truly world class Transportation Hub for Kirkland and the Eastside, and indeed all regional commuters. Then, with its proven success, housing and further development at that site will be an issue that we can address. Let's make sure the T works before adding the OD.

**The US DOT grant has no component requiring TOD or housing** at this site to be a necessity for King County Metro to receive the funding. Quite to the contrary it calls out parking expansion and transit improvements. From the Fact Sheet provided to Council in the meeting packet: *As part of the Urban Partnership Agreement, which includes tolling on SR 520, the US Department of Transportation has agreed to provide \$6.25 million to King County Metro to create a Sustainable Transportation Hub at the South Kirkland Park and Ride. The funding will primarily be used for additional parking and other transit improvements.*

The stated goal of the funding is for parking and improving transit. Although \$6.25 Million seems like a vast amount, it can and should be spent entirely on these two goals alone. Creating a Sustainable Transportation Hub means designing the most successful transportation system at this Park and Ride. This can be done the following ways:

**Building a premium parking facility, one that is pedestrian focused, not car focused, would be truly innovative and much in line with vision of pedestrian and human scaled facility.** I'm sure you've all tried to navigate some of these parking "structures". The focus is obviously on how many cars you can squeeze into as small a space as possible, without regard to the fact that human beings, with differing abilities (Ex: mobility, vision, English as a second language, children) will be needing to get out of these cars and safely find their way to the transit hub. Ingress and egress of cars from the site also needs to be improved and not shared with transit.

**Make the Transportation Hub world class, not just regionally functional.** Improving the transit users experience will also improve and increase transit use and ridership. Again, the focus needs to be on the human using the facility. There needs, at minimum, to be a covered waiting area that considers ALL the seasons. We need to improve ticketing access, either through ATM-style ORCA stations or an actual staffed booth. Better information access to incoming bus routes as well as delays or rider alerts (for those of us who do not have the latest phone app), and routes that are available (aka route maps and information kiosk with route pamphlets, ORCA information, etc) This might be a fantastic place for digital media boards for local city governments to post local events and meetings, maybe even stream local government TV, as a way to reach citizens waiting for buses. It also could be a place for local art presentation.

**Transit itself needs to be improved.**

**1. We need to improve access to the transit hub from 108<sup>th</sup> Avenue,** if there is indeed going to be an expected increase in ridership. Flow in and out for buses needs to be safer and faster for the transit driver as well as for cars using that road. I think there needs to be a transit only entrance and exit so as to not conflict with cars. Additionally that could be the gateway structure.

**2. Inside the transit hub, bus loading and unloading zones need to be extended and expanded.** Currently it allows only one bus to safely load and unload. This will allow for faster loading and unloading.

**3. Increase improvements/frequency for the 230 and 234 Routes.** Although the 255 is indeed one of the most popular routes, the 230 and the 234 are likely to be increasing. They are our two major routes north/south through our city and they allow the most access to Kirkland City Center and to major parts of Bellevue. The 230 goes from the Bellevue Square area and Lake Washington Boulevard out to our Totem Lake Transit Center. The 230 is poised to be the route to help us most with traffic congestion on Lake Washington Boulevard. The 234 goes by Google, Kirkland downtown, and out to the new annexation area. It will help to serve all our citizens. It will also be East Link rider's most direct access to Kirkland.

## **Improve Transportation Options**

I highly approve of the addition of **electric car charging stations** being included in the Transportation Hub. Furthering the goal of increased alternative transportation use, I would also like to include a **Bike Sharing station** at the South Kirkland Park and Ride site. This site is uniquely poised to serve the two major cities of the Eastside (Kirkland and Bellevue). Bike Sharing is on the list of projects of the PSRC and King County has already received Transportation Enhancement funds to develop a business model (see Seattle Transit Blog for information

<http://seattletransitblog.com/2010/09/14/king-county-seeks-grant-for-bike-sharing/> ) **A Bike Station**, such as the one in downtown Seattle, would also be a welcome addition to the arsenal of the Transportation Hub. (see <http://www.bikestation.org/seattle/index.asp> for a description of Bike Stations). I also encourage the **pedestrian access to the BNSF corridor be included in the**

design of the Transportation Hub. The Corridor provides safe and pleasant (aka flat) pedestrian access to the Houghton Center, Carillon Point area, and for the more intrepid, downtown Kirkland and Totem Lake. It also goes southward, connecting to the future Bel-Red corridor, Whole Foods and the Overlake Hospital /Lake Bellevue area.

Given that the stated goal is to improve transit at the location, I believe that \$6.25 Million must be spent to improve and insure that we do indeed have a Sustainable Transportation Hub, before adding in the burden of housing issues that come with TOD.

Sincerely,

Lisa McConnell

Houghton resident and Central Houghton Neighborhood Advisory group member

**Dorian Collins**

---

**From:** Paul Stewart  
**Sent:** Tuesday, September 21, 2010 2:04 PM  
**To:** Dorian Collins; Janice Soloff  
**Subject:** FW: South Kirkland P&R

FYI

**From:** Janet Jonson  
**Sent:** Tuesday, September 21, 2010 12:55 PM  
**To:** City Council  
**Cc:** Kurt Triplett; Marilynne Beard; Eric Shields; Paul Stewart  
**Subject:** FW: South Kirkland P&R

Council: 10.c. on tonight's Council meeting agenda. JJ

Janet Jonson  
City Manager's Office  
City of Kirkland  
123 5th Avenue  
Kirkland, WA 98033  
425 587 3007  
425 587 3019 fax  
jjonson@ci.kirkland.wa.us

**From:** georgine foster [mailto:georginef@msn.com]  
**Posted At:** Tuesday, September 21, 2010 9:58 AM  
**Posted To:** Kirkland Council  
**Conversation:** South Kirkland P&R  
**Subject:** South Kirkland P&R

Dear City Council members, Houghton Community Council members, City Manager,

I am a member of the Lakeview Neighborhood Plan Update Advisory Group, but I would like to express some personal views about the "Process" and the possible "Fast Tracking" of the Zoning Code Amendments for the South Kirkland Park& Ride. (You will remember that the Comp Plan Amendments for the P&R were "fast tracked" the end of 2008.)

My concern is that the County is asking for expediting the Zoning Code revisions without DUE PROCESS, possibly circumventing the Lakeview Neighborhood Plan Update process, and WITHOUT Bellevue's "cooperation", as is called for in the Comp Plan.....I don't understand how a few phone calls, or meetings that do not produce at least an MOU between Kirkland, Bellevue and King County, is adequate. Un-intended consequences could be devastating to the area, with congestion at the top of the list...changes to the 520 will surely have their effect on traffic in the area, too.

The amount of Affordable housing, as is stated in your packet supplied by Dorian Collins, suggests that 100% of the project could be "affordable". Redmond's Town Center TOD, the TOD in Renton, and the TOD in Northgate are all 20% Affordable and 80% Market rate.....why is Kirkland seeking higher percentages for South Kirkland when obviously neighboring cities have chosen differently? I realize the TOD at Redmond's Overlake area is 100% affordable, but are the demographics of Lakeview and Central Houghton comparable to Overlake? If South Kirkland is to have Affordable Housing as part of its mixed use development, why not at a Rate more in keeping with what has been developed in other neighboring jurisdictions?

Note the April 16th email (below) from Gary Prince of Metro (who also authored the Application for the Grant to the Federal government)...."grant funding...is not related to the affordability issue but rather to increasing the number of parking places and mixed use development". So there is no "must have" percentage, or number, of Affordable units.

In the Affordable Housing Regulations recently approved by the City, it is noted that INITIAL "affordable housing projects" will not be required to provide the entire "mandatory" 10% affordable units for projects (as they are viewed as almost experimental....that is my summation), YET the South Kirkland Park & Ride could have 100%?

Our Lakeview Neighborhood Plan Update process had many of us spending MANY hours in meetings because we felt we had something of value to add to the process, our neighborhood, and Kirkland. Please consider how you might feel if you "participated", only to find out that it didn't really matter.

Thank you.

georgine foster  
Lakeview Neighborhood Resident

**From:** georgine foster [mailto:georginef@msn.com]  
**Sent:** Friday, April 16, 2010 10:59 AM  
**To:** Prince, Gary  
**Cc:** Paul Stewart  
**Subject:** Re: South Kirkland P&R

Gary,  
Thanks for the information!

Who might I contact to get the # and type of affordable units at the Redmond Downtown TOD, Northgate, and Overlake projects.....just for comparison sake. I'm meeting with Paul Stewart and Dorian Collins next Wednesday and if I had these comparison figures it would be helpful for me to get "the big picture".

Again, thank you.

~georgine

----- Original Message -----

**From:** [Prince, Gary](#)  
**To:** [georgine foster](#)  
**Cc:** [Paul Stewart](#)  
**Sent:** Friday, April 16, 2010 10:33 AM  
**Subject:** RE: South Kirkland P&R

Georgine:

The county has worked with local jurisdictions, ARCH, and private developers to determine the number and type of affordable units. The County does not have a "vision" for the number or type of affordable units for this particular site. The grant funding which Metro Transit has available is not related to the affordability issue but rather to increasing the number of parking spaces and the mixed use development

We do not have an appraisal on the parcel so I cannot speak to the price for the underlying land.

Gary Prince  
Senior Project Manager  
Transit Oriented Development

King County Department of Transportation  
206.263.6039

**From:** georgine foster [mailto:georginef@msn.com]  
**Sent:** Wednesday, April 14, 2010 6:09 PM  
**To:** Prince, Gary  
**Cc:** Paul Stewart  
**Subject:** Re: South Kirkland P&R

Gary, thanks for the info....I wasn't thinking that Mithun was an Architectural/Design firm, I thought they developed the Northgate project. Has the County any "vision" for % to Median income, or the # of Units that will be 'affordable'.....and do these numbers effect how much grant money could be available for the project?

(And what might the asking price be for the "underlying land"?)

~georgine foster

--- Original Message -----

**From:** Paul Stewart  
**To:** georgine foster ; Janice Soloff  
**Cc:** johnk ; Dorian Collins ; Prince, Gary  
**Sent:** Wednesday, April 14, 2010 10:26 AM  
**Subject:** RE: South Kirkland P&R

Georgine,

It is my understanding that King County would request proposals from developers. Mithun is an architectural and design firm that is advising King County and is not a developer. You should contact Gary Prince for more information on this.

I would suggest that instead of these back and forth e-mails, why don't we have a meeting and we can explain the project in detail and respond to your questions.

Paul

**Dorian Collins**

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**From:** Terry Kisner [terrencek@gmail.com]  
**Sent:** Monday, September 20, 2010 11:40 AM  
**To:** Joan McBride; Penny Sweet; Dave Asher; Jessica Greenway; Doreen Marchione; Amy Walen; Bob Sternoff  
**Cc:** Paul Stewart; David Godfrey; Eric Shields; Dorian Collins; Angela Mason  
**Subject:** Transit Oriented Development center in Kirkland

City Council & Planning Department,

As a local resident within 1 mile of the current P&R, please **DO NOT** create a Transit Oriented Development (TOD) center in the South Kirkland Park & Ride. As with all elected officials, you are elected to serve all people within the community and listen to their instructions for building a better and brighter future. If you polled the population of your constituents, the overwhelming vote would be **NOT** to build the TOD Center. Thank you for your time and dedication to building a better Kirkland.

Cheers,

Terry Kisner

**Dorian Collins**

---

**From:** Paul Stewart  
**Sent:** Tuesday, September 21, 2010 11:54 AM  
**To:** Dorian Collins; Eric Shields  
**Subject:** FW: Kirkland City council meeting and the TOD site staff memorandum to council

**From:** John Kappler [mailto:JohnK@KapplerHomePlans.com]  
**Sent:** Tuesday, September 21, 2010 11:06 AM  
**To:** Nancy Cox  
**Cc:** Paul Stewart; Janice Soloff  
**Subject:** Kirkland City council meeting and the TOD site staff memorandum to council

Nancy,

Please forward this to all HCC members, Lakeview Neighborhood Advisory group members (and possibly Central Houghton Advisory group members as FYI).

Thanks, John

To all who have participated in the neighborhood planning efforts for Lakeview,

When we all began this process, I mentioned that the neighborhood advisory group was but one piece of the process and would not yield a conclusion, rather a beginning to the ongoing neighborhood planning process. It is important to follow this process through to completion. As we all know, this process is arduous at best and frustrating at times. I know this myself, as I went through a process like this years ago and decide to become involved further. This is why I continue to serve the community on the Houghton Community Council. My desire is that my efforts make a difference.

As a result, I am sending this email to you all today to remind you all that the Kirkland City Council is continuing the discussion on one area of the planning areas we have addressed in our neighborhood plan. This area is the Park and Ride site also known as the Transit Oriented Development Site (TOD). The link below is the memo and agenda to the Council.

<http://www.ci.kirkland.wa.us/depart/council/Agendas/agenda092110.htm>

Please review as the outcome will affect the neighborhood plan. If you can attend, please do so. You can also listen through the city web site.

I will in the future I be more diligent in alerting you with more notice to activities regarding your neighborhood plan.

**John Kappler, President**  
***Architectural Innovations P.S.***

**Dorian Collins**

---

**From:** Paul Stewart  
**Sent:** Monday, September 20, 2010 10:35 AM  
**To:** Prince, Gary; Dorian Collins; Janice Soloff; 'Arthur Sullivan'  
**Subject:** FW: So. Kirkland P&R TOD Proposal

FYI

**From:** S. Etchevers [mailto:setchev@comcast.net]  
**Sent:** Sunday, September 19, 2010 9:46 PM  
**To:** Joan McBride; Penny Sweet; Dave Asher; Jessica Greenway; Doreen Marchione; Amy Walen; Bob Sternoff  
**Cc:** Paul Stewart; David Godfrey; Eric Shields  
**Subject:** So. Kirkland P&R TOD Proposal

Dear Mmc. Mayor and Council Members,

I am very disappointed, on various levels, about the way the South Kirkland P&R TOD proposal has been developed. Below is a brief outline of my concerns about the proposal.

**PROCEDURALLY**

The work has been done, for all practical purposes, essentially 'in the shadows' of the communities most affected: Central Houghton and Lakeview. Posting signs in places hard to see within a busy community, in which people are focused on raising a family and making a living, does not equate to proper outreach to the community. Yet, even 3 years ago when I became aware of the project, I remember that strong reservations were voiced by local residents attending a few, already-advanced, planning meetings. Their voices were drowned out and ignored by the political interests of the Seattle people involved in the project. Now, compare Kirkland's community 'outreach' for this proposed idea to what Redmond is doing for its Central Connector project!

Then, just a few months ago, when the rezoning issue was discussed within the context of updating the Lakeview and Central Houghton Urban Plans with an Advisory Group of local residents, there was again strong opposition to the proposal supported by reasons and logic of which you should be aware.

It is also a bit curious that the City's representatives, who coordinated the above-referenced Lakeview and Central Houghton Advisory Groups, did not bother to send the information about next Tuesday's meeting to those of us who participated in that effort.

**URBANISTICALLY**

The South Kirkland P&R TOD proposal seems like another poorly thought out, piecemeal approach to development. The South Kirkland Park & Ride area had been zoned one way by the City a long time ago. Lakeview and Central Houghton developed over the years based on that zoning, and people chose to settle in southwest Kirkland based on that reality. Changing the urban zoning should not be taken lightly, both out of respect for the local residents and respect for the overall long-term plan of the city, especially in established, low-density, residential areas. In fact, if there is a well thought out, long-term development plan for the city, it should be adhered to and enhanced to make the community better, esthetically more attractive, more inviting, and more livable. Buildings should not be raised randomly where there appears to be an open space. Zoning codes should not be capriciously and surreptitiously ignored or modified. Changes should be done very publicly in a way that makes sense from the point of view of a well thought out Urban Architectural design and the best possible quality of life for the residents. Nice cities and urban spaces are created by careful, long-term planning, not haphazardly or by chance.

High-density areas within a community should follow one or just a few normal ('bell') distribution curves. They should not follow a random distribution like a flat pancake dotted with scattered blueberries. The latter has a long-term negative effect on urban efficiency, transportation needs, urban space choices, and the quality of life of its residents. The proposed TOD in the South Kirkland P&R falls into this last category.

**QUALITY OF LIFE**

All successful and appealing high-density residential areas are located around a well-integrated mix of the following: commercial areas offering services needed for daily life, recreational areas, parks, cafes, security, lighting, pedestrian spaces, and with few dark empty spaces between business buildings. That is true for residential areas for affluent as well as for less affluent people. Creating dense, isolated islands of less affluent people in the middle of a low-density area may be expedient, but it is not a wise choice, nor a step leading to an improved quality of life for anybody in those areas. The TOD development in downtown Redmond meets the above-mentioned criteria for improving the quality of life of its residents. The proposed South Kirkland P&R TOD does not by a long shot.

Similarly, citizens who choose to live in established, low-density (a normal and necessary component of a healthy city), neighborhood communities should be entitled to do so without the city changing their community – especially after they specifically indicated, through proper and established communication channels, that they did not want the proposed city changes.

#### ALTERNATIVE CHOICES FOR T.O.D.

##### A. Areas already zoned for higher buildings include:

- Downtown Kirkland: In the area behind the Wells Fargo Bank, somewhere in the future Park Place development, and/or over or near the new bus transit terminal.
- Totem Lake (East of I-405)
- Kingsgate
- Western area of NE 85<sup>th</sup>
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- Juanita
- Over City Hall and the Police Department

##### B. Other P&R options: Totem Lake P&R, Houghton P&R.

Both of these options are *almost* as bad as the proposed So. Kirkland location, and are just as inherently discriminatory against people with lower incomes. But, at least they are closer to necessary urban amenities, and one or both of them is located in an *area already zoned* for taller buildings and higher density than the South Kirkland P&R.

#### FINALLY

If you are still really interested in considering a resident's opinion and choose to ignore the input already provided by the most affected neighborhood associations and Advisory Groups, please consider conducting a mail poll in Lakeview and Central Houghton, briefly outlining the pros and cons of your proposal, and then truly factor those results into your plans.

Sincerely,

Shawn Etchevers  
Central Houghton

**Dorian Collins**

---

**From:** Lisa A. McConnell [kirby994@frontier.com]  
**Sent:** Monday, September 20, 2010 2:23 PM  
**To:** Eric Shields; Dorian Collins; Paul Stewart  
**Subject:** South Kirkland Park and Ride TOD

Please forward to Houghton Community Council (no contact email given on website)

**RE: South Kirkland Park and Ride TOD**

The \$6.25 Million could easily be spent on transit and parking improvements alone. Let's make sure that we create a truly world class Transportation Hub for Kirkland and the Eastside, and indeed all regional commuters. Then, with its proven success, housing and further development at that site will be an issue that we can address. Let's make sure the T works before adding the OD.

**The US DOT grant has no component requiring TOD or housing** at this site to be a necessity for King County Metro to receive the funding. Quite to the contrary it calls out parking expansion and transit improvements. From the Fact Sheet provided to Council in the meeting packet: *As part of the Urban Partnership Agreement, which includes tolling on SR 520, the US Department of Transportation has agreed to provide \$6.25 million to King County Metro to create a Sustainable Transportation Hub at the South Kirkland Park and Ride. The funding will primarily be used for additional parking and other transit improvements.*

The stated goal of the funding is for parking and improving transit. Although \$6.25 Million seems like a vast amount, it can and should be spent entirely on these two goals alone. Creating a Sustainable Transportation Hub means designing the most successful transportation system at this Park and Ride. This can be done the following ways:

**Building a premium parking facility, one that is pedestrian focused, not car focused, would be truly innovative and much in line with vision of pedestrian and human scaled facility.** I'm sure you've all tried to navigate some of these parking "structures". The focus is obviously on how many cars you can squeeze into as small a space as possible, without regard to the fact that human beings, with differing abilities (Ex: mobility, vision, English as a second language, children) will be needing to get out of these cars and safely find their way to the transit hub. Ingress and egress of cars from the site also needs to be improved and not shared with transit.

**Make the Transportation Hub world class, not just regionally functional.**

Improving the transit users experience will also improve and increase transit use and ridership. Again, the focus needs to be on the human using the facility. There needs, at minimum, to be a covered waiting area that considers ALL the seasons. We need to improve ticketing access, either through ATM-style ORCA stations or an actual staffed booth. Better information access to incoming bus routes as well as delays or rider alerts (for those of us who do not have the latest phone app), and routes that are available (aka route maps and information kiosk with route pamphlets, ORCA information, etc) This might be a fantastic place for digital media boards for local city governments to post local events and meetings, maybe even stream local government TV, as a way to reach citizens waiting for buses. It also could be a place for local art presentation.

**Transit itself needs to be improved.**

**1. We need to improve access to the transit hub from 108<sup>th</sup> Avenue**, if there is indeed going to be an expected increase in ridership. Flow in and out for buses needs to be safer and faster for the

transit driver as well as for cars using that road. I think there needs to be a transit only entrance and exit so as to not conflict with cars. Additionally that could be the gateway structure.

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**3. Increase improvements/frequency for the 230 and 234 Routes.** Although the 255 is indeed one of the most popular routes, the 230 and the 234 are likely to be increasing. They are our two major routes north/south through our city and they allow the most access to Kirkland City Center and to major parts of Bellevue. The 230 goes from the Bellevue Square area and Lake Washington Boulevard out to our Totem Lake Transit Center. The 230 is poised to be the route to help us most with traffic congestion on Lake Washington Boulevard. The 234 goes by Google, Kirkland downtown, and out to the new annexation area. It will help to serve all our citizens. It will also be East Link rider's most direct access to Kirkland.

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Sincerely,

Lisa McConnell

Houghton resident and Central Houghton Neighborhood Advisory group member

**Dorian Collins**

---

**From:** Terry Kisner [terrencelk@gmail.com]  
**Sent:** Monday, September 20, 2010 11:40 AM  
**To:** Joan McBride; Penny Sweet; Dave Asher; Jessica Greenway; Doreen Marchione; Amy Walen; Bob Sternoff  
**Cc:** Paul Stewart; David Godfrey; Eric Shields; Dorian Collins; Angela Mason  
**Subject:** Transit Oriented Development center in Kirkland

City Council & Planning Department,

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Terry Kisner

## Dorian Collins

---

**From:** Paul Stewart  
**Sent:** Monday, September 20, 2010 10:35 AM  
**To:** Prince, Gary; Dorian Collins; Janice Soloff; 'Arthur Sullivan'  
**Subject:** FW: So. Kirkland P&R TOD Proposal

FYI

**From:** S. Etchevers [mailto:setchev@comcast.net]  
**Sent:** Sunday, September 19, 2010 9:46 PM  
**To:** Joan McBride; Penny Sweet; Dave Asher; Jessica Greenway; Doreen Marchione; Amy Walen; Bob Sternoff  
**Cc:** Paul Stewart; David Godfrey; Eric Shields  
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Sincerely,

Shawn Etchevers  
Central Houghton

## Dorian Collins

---

**From:** Janice Soloff  
**Sent:** Monday, September 20, 2010 8:52 AM  
**To:** Paul Stewart; Eric Shields; Dorian Collins  
**Subject:** FW: Affordable Housing & Seniors

FYI

Janice Soloff  
Planning and Community Development  
425-587-3257  
[jsoloff@ci.kirkland.wa.us](mailto:jsoloff@ci.kirkland.wa.us)  
[www.ci.kirkland.wa.us](http://www.ci.kirkland.wa.us)

**From:** Uwkkg@aol.com [mailto:Uwkkg@aol.com]  
**Sent:** Friday, September 17, 2010 6:31 PM  
**To:** undisclosed-recipients  
**Subject:** Affordable Housing & Seniors

One additional point of LVN that may be misleading.

The super tiny units that would seem to result from high density seemed like something that only seniors who tend to have less belongings, no car, etc might be well suited to inhabit. Many of the problematic issues of dense housing (arguments over noise, too little parking, etc) seemed to not be so problematic with seniors who might not have car and could benefit from transit... who might not throw wild late night parties in a tight living environment, etc.

The other thought was around affordability. While LVN acknowledges that providing affordable housing is important, it seemed that we did not want to take on more than our share. (e.g. if there were 10 equal sized "neighborhoods" we felt it would be reasonable to house 1/10th of the affordable units). Many people enter Kirkland from our gateway neighborhoods of Lakeview and Central Houghton.

You will recall the famous quote "You are now entering the twilight zone" ... We don't visitors to Kirkland to enter at LVN or Houghton and hear "You are now entering the Density Zone."

Karen Levenson  
Lakeview Neighborhood Advisory Committee  
6620 Lake Washington Blvd NE, Kirkland 98033

**Dorian Collins**

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**From:** Uwkkkg@aol.com  
**Sent:** Friday, September 17, 2010 6:15 PM  
**To:** undisclosed-recipients  
**Subject:** City Council Mtg - Corrections for Council and HCC

Hi all:  
 Please include my letter in the meeting packet.

After all the long hours and commitment of Lakeview Neighbors, it is very upsetting to see that the summary of Lakeview Neighborhood comments is GROSSLY incorrect. We previously asked that these incorrect statements be corrected so that our actual concerns and opinions would reach you. Somehow the incorrect version is still finding its way to you today...

In the taped HCC meeting, the LVN Advisory opinions were well presented to HCC and Planning commission by John Kappler, but every opinion of LVN seemed to be immediately discounted by someone I do not know... I believe he repeatedly referred to himself as "snide Andy." With every item he seemed to discount the long hours and deep research that LVN committee did over 9 months, as if this was a rogue group of bandits. Even if the LVN was unanimous following tremendous research, our opinions seemed immediately discredited and thrown out.

To be sure, the Lakeview Neighborhood Advisory participants spent a VERY long time researching and discussing the Park and Ride. We spent a very large amount of time outside of our meeting reading through the comp plan, digging out research on TOD projects, reading and watching past city meetings and study sessions, etc....We were even able to get some answers that never got answered by the City or Metro... Then we discussed for hours... and eventually had some well founded, and consensus opinions.

As a very active participant in the LVN Advisory group, I'll try and present corrections and my belief on where we eventually landed ...(many/most were unanimous or near unanimous). Please include this information with your materials for the Sept 21st meeting.

The concerns of the neighborhood were:

- A) LVN already shoulders more than our share of density - No More High Density in LVN
- B) LVN already has more than our share of big multi-unit buildings - Big bldgs threaten neighborhood "feel"
- C) The proposed project blatantly disregards dozens of aspects of the Comp Plan(why have plan if we ignore)
- D) We already have more than our share of traffic. Even current traffic not mitigated... NO MORE TRAFFIC
- E) There is no shopping, nor much employment nearby. Other TODs are built where these already exist.  
 Our Comp Plan has for years identified Totem Lake as it is already zoned for this and has all the appropriate amenities to support this type of urban village (see years and years of comp plan)...

LVN opinions:

- 1) The neighborhood advisory groups voted at last mtg. UNANIMOUSLY opposed residential use @ TOD
- 2) The neighborhood advisory group is asking HCC to deny zoning change
- 3) The group DID NOT state that we should merely "have agreements with Bellevue" before going forward.  
 HERE'S THE STATEMENT... VERY CLEARLY...  
"WE SHOULD NOT TAKE ANY STEP W/O Bellevue taking the same step at same time".  
 We simply don't move forward without Bellevue acting on this with us (more on this later)
- 4) No more than 200 units TOTAL (including Bellevue and Kirkland ... approx 100 each) if this goes forward.
- 5) There appears to be repeated comment that the grant necessitates housing. This seems incorrect. We did not get any official document that shows housing necessary from the city or from Metro. We have found numerous documents about the grant and all seem to require increased parking but to date we have not found any document that requires housing. This has felt misleading. If a document exists that shows housing required for the grant, we assume our prior requests would have had this information provided to LVN
- 6) Renton TOD is 90 units. It is not understandable why we are told developers will only do 200+ units.  
 (What could possibly be different about Renton... We have asked yet received no answer).
- 7) Requests for acreage or units/acre of other TODs were sent to City, then City sent to Metro ... and two months later we've still not received any answers. This is concerning. We were able to find out much of this information on our own, so why does the city and metro not have this info to send to LVN?

- If this gets built, it appears that it will be the BIGGEST TOD PROJECT (inclusive of Bvue & KLand).
- This appears to be one of the HIGHEST # units/acre and the units will be TINY out of necessity.
- The idea that some units could be this small and be above market rate is unthinkable
- The idea that families could move into units this small is unthinkable.
- The California TODs that were mentioned for comparison are not in comparable neighborhoods (Here's a few... Downtown Hayward, Oakland, Richmond, San Pablo)  
Also in big cities like San Jose & San Francisco where big city amenities surround.
- Consider how many people in a 6.9 acres if there are 500 units... 1500? ... maybe 215 people per acre??  
Add to that 215 people/acre the additional riders, etc ... an enormous concentration of folks in one spot

HCC PLEASE SUPPORT THE STRONG FEELINGS OF YOUR NEIGHBORHOODS ...DO NOT MOVE FORWARD ...  
Your neighborhoods depend on your ability to protect Houghton's special characteristics.  
Houghton joined Kirkland w/the provision that we could veto things that will have this type of negative impact

IF YOU MOVE FORWARD, MOVE ONLY AT THE SAME PACE AS BELLEVUE

- Building w/Bvue is how project was designed & proposed. Going solo seems desperate & not rational.
- Building w/Bvue is the only way to place various components in the area where topography suits
- If developers will only participate with a minimum of 200 units it is very important to wait for Bvue so that the project doesn't have to be so GIGANTIC. We can then build just 200 for the whole project vs 400-500 if done in two phases.

Thank you,  
Karen Levenson  
Lakeview Neighborhood Advisory Group Member  
6620 Lake Washington Blvd NE, Kirkland, WA 98033

In a message dated 9/17/2010 4:01:30 P.M. Pacific Daylight Time, AMason@ci.kirkland.wa.us writes:

You are receiving this email at the request of Senior Planner Dorian Collins

The South Kirkland Park and Ride City Council meeting packet has now been posted to the City webpage. You may review the full meeting packet by clicking on the link below:

[http://www.ci.kirkland.wa.us/Assets/City+Council/Council+Packets/092110/10c\\_UnfinishedBusiness1.pdf](http://www.ci.kirkland.wa.us/Assets/City+Council/Council+Packets/092110/10c_UnfinishedBusiness1.pdf)

If you have any questions please contact Dorian Collins at [dcollins@ci.kirkland.wa.us](mailto:dcollins@ci.kirkland.wa.us) or 425-587-3249.

*Angela Mason*

City of Kirkland Planning Department

Office Technician

425-587-3237

amason@ci.kirkland.wa.us

Mon.- Fri. 8:00-5:00

**Dorian Collins**

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**From:** Janet Jonson  
**Sent:** Monday, September 20, 2010 8:46 AM  
**To:** City Council  
**Cc:** Kurt Triplett; Marilynne Beard; Eric Shields; Paul Stewart; Dorian Collins  
**Subject:** FW: One of the subjects for Tuesday's Council meeting

Janet Jonson  
City Manager's Office  
City of Kirkland  
123 5th Avenue  
Kirkland, WA 98033  
425-587-3007  
425-587-3019 fax  
jjonson@ci.kirkland.wa.us

**From:** RLSTYLE@aol.com [mailto:RLSTYLE@aol.com]  
**Posted At:** Monday, September 20, 2010 7:40 AM  
**Posted To:** Kirkland Council  
**Conversation:** One of the subjects for Tuesday's Council meeting  
**Subject:** One of the subjects for Tuesday's Council meeting

South Kirkland Park & Ride

If ever there were proof of how staff gerrymanders facts to overcome the will of the people in their respective neighborhoods, it's the staff report on the South Kirkland Park & Ride. The concerns of the Lakeview Neighborhood that evolved from the 9 months of Advisory meetings and so well expressed in letters written by Mr. Chuck Pilcher and Ms. Karen Levenson were determined to be insignificant and deemed lesser in value than King County's request that would overwhelm our already congested streets and proposing increase housing densities that would not enhance the image of Kirkland. Staff neglected the importance of the neighborhood input saying it wasn't enough to deny what the County and our Council had proposed.

Council has a record of ignoring neighborhoods if it's not in the Council interest. We didn't get to vote on annexation, staff failed to include facts that disproved the "best available science" when updating the Shoreline Management Act, and now they are gerrymandering facts again ignoring the citizens concerns in their staff report on the South Kirkland Park & Ride.

There are two major fatal flaws in what the Council wants: traffic and housing density. We already have congestion on Lake Washington Blvd and 108<sup>th</sup> Ave. NE. The County wants 250 parking spaces for METRO and parking for 200 more multi-family units that according to Kirkland development requirements requires 2 spaces for each unit unless they make an exception to the rule.

Almost all of the congestion on the two major arterials comes from the inability for traffic to get thru Kirkland. There's a bottleneck downtown and 405 is already full. So where do the people go when they get off the bus at the park and ride. A scatter-gram was taken of the parking lot to determine where the cars were going after the bus let the passengers off. Using license plate information, it was determined that almost all of the traffic went north of the city. The scatter-gram was not included in the staff report. The Preliminary Trip Distribution/Assignment Estimate chart is wrong and needs to be redone.

**The Council has refused to add capacity to our transportation system.** The TOD will use up capacity that is needed for single family homes leaving us with nothing in the future.

Kirkland's Transportation Engineer Mr. Thang T. Nguyen was responsible for the Preliminary Traffic Impact Assessment for the TOD. He use information from resources that could be interpreted in many ways, some better than others. For instance, he used the old lettering system for determining road capacity: "A" was great. You're the only one on the road. "F" was failing. He determined that Lake Washington Blvd and 108 Ave. NE at the park & ride was level "E". He failed to point out the **road conditions downtown** which are at the "F" level during peak hour. In the evening, now the peak hour starts at 4:00 PM and last till 7:00 PM. One hour has become three and getting longer.

You don't have to be a traffic engineer with a lettering system to know the road is congested. All you have to do is look out on the street from your house or even worse, your car. The road is congested.

However, the city has used vehicle to capacity system called the V/C ratio. Why haven't they done so for this report?

On page 2 of his report Mr. Nguyen wrote, "This project [the TOD] will contribute to the future poor level of service." What an understatement that is. He also wrote, "...it is most likely that the impact from the development will not trigger off-site improvements." He's wrong. They should. The City should require them. METRO should be required to develop turnouts, and acceleration and deceleration lanes at all of their bus stops. They should not be in the business of stopping traffic while loading and unloading passengers.

Another traffic report should be generated using V/C ratios for streets and intersections especially for downtown where Lake Washington Blvd and 108<sup>th</sup> Ave NE lead to. Traffic at intersections should not be averaged out directionally or in time. Only the lanes of traffic being congested should be considered during the 3 to 4 hours they're impacted and not averaged out with times before and after.

I would invite Councilmembers to get in your cars at the park & ride at 5:PM and try to get through Kirkland going north where most of the traffic goes when you get off the bus at the Park & Ride and tell me the TOD will improve our quality of life in Kirkland. Who's representing Kirkland citizens, our Council or METRO?

Robert L. Style  
6735 Lake Washington Blvd, NE  
Kirkland, WA 98033  
425-827-0216

**Dorian Collins**

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**From:** Eric Shields  
**Sent:** Monday, September 20, 2010 8:29 AM  
**To:** Paul Stewart; Dorian Collins; Janice Soloff  
**Subject:** FW: TOD  
**Attachments:** FMT - Flags.JPG; ATT3142617.txt

*Eric Shields*

*-----Original Message-----*

*From: Frank [mailto:[fmt97@comcast.net](mailto:fmt97@comcast.net)]  
Sent: Sunday, September 19, 2010 4:36 PM  
To: Eric Shields  
Subject: TOD*

*Please do not create a TOD in S. Kirkland P&R.  
Frank M. Tyllia  
[fmt97@comcast.net](mailto:fmt97@comcast.net)*

**Dorian Collins**

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**From:** Brian Staples [brian@brianandemily.com]  
**Sent:** Friday, September 17, 2010 11:24 PM  
**To:** Angela Mason; Dorian Collins  
**Cc:** Paul Stewart; Eric Shields; 'Lisa A. McConnell'; betsyp@beckermayer.com; 'S. Etchevers'; 'georgine foster'  
**Subject:** RE: Kirkland City Council Meeting September 21 - South Kirkland Park and Ride

Hello Angela and Dorian – thanks for forwarding me this information packet. On page 3 of the packet under “Considerations” it states:

- Lakeview Neighborhood Advisory Group: The concept of transit-oriented development (TOD) at the South Kirkland Park and Ride was controversial and of concern to many members of the Lakeview Neighborhood Advisory Group. A majority of the group is opposed to residential use at this site, particularly affordable housing...

I would think it would certainly be appropriate, given that the Central Houghton Neighborhood Advisory Group spent almost an entire meeting on the subject of the South Kirkland Park and Ride Development in a somewhat contentious manner, to add a bullet for our advisory group as well.

- Central Houghton Neighborhood Advisory Group: The concept of transit-oriented development (TOD) at the South Kirkland Park and Ride was controversial and of concern to many members of the Lakeview Neighborhood Advisory Group. The main points of concern were affordable housing, lack of amenities and supporting retail services, increased traffic volumes on 108th Ave NE, appropriate building mass for a gateway to the neighborhood, net loss of parking spaces after accounting for added residences, and walkability. There were also concerns about moving forward with this project without Bellevue’s partnership or a robust process in place to ensure high quality architecture and site design.

I’ve cc’ed Shawn Etchevers, Lisa McConnell, and Betsy Pringle on my thoughts because I have their email addresses and they were part of the advisory group. If you’d like to forward my email to others of the Central Houghton Neighborhood Advisory Group, that would be great.

Thanks for all your work.

-brian

**From:** Angela Mason [mailto:AMason@ci.kirkland.wa.us]  
**Sent:** Friday, September 17, 2010 4:01 PM  
**To:** Dorian Collins  
**Cc:** Paul Stewart; Eric Shields  
**Subject:** Kirkland City Council Meeting September 21 - South Kirkland Park and Ride

You are receiving this email at the request of Senior Planner Dorian Collins

The South Kirkland Park and Ride City Council meeting packet has now been posted to the City webpage. You may review the full meeting packet by clicking on the link below:

[http://www.ci.kirkland.wa.us/Assets/City+Council/Council+Packets/092110/10c\\_UnfinishedBusiness1.pdf](http://www.ci.kirkland.wa.us/Assets/City+Council/Council+Packets/092110/10c_UnfinishedBusiness1.pdf)

If you have any questions please contact Dorian Collins at [dcollins@ci.kirkland.wa.us](mailto:dcollins@ci.kirkland.wa.us) or 425-587-3249.

## Dorian Collins

---

**From:** Uwkkkg@aol.com  
**Sent:** Monday, March 14, 2011 3:20 PM  
**To:** Janice Coogan; Uwkkkg@aol.com; Paul Stewart; Dorian Collins  
**Subject:** HCC Mtg # 3 Letter re: Trees, YBD, TOD & S. Houghton Slope

Janice:

Here's one last one with other topics for tonight...Thanks for distributing these to the emails of the HCC members...

Other items for HCC discussion

1) Trees and vegetation in parks. The reason that the neighbors wanted to make sure and be included when new trees and vegetation is introduced is that previously there was a horrific example where a row of Kentucky Coffee trees was planted. They looked small and not harmful until a local resident discovered their foliage starts at 6-8 feet off ground and grows 100 feet tall. It's branches reach 45 feet in either direction and these trees had been planted at 90 feet apart. This would have been a hedge that no one would see through (even those in properties elevated by topography).

2) Yarrow Bay Business District, TOD and S. Houghton Slope.

The Neighborhood group began to feel that the city was unbridled in their desire for housing, lots of housing, dense housing, etc. We want to maintain Kirkland as somewhere that has a neighborhood feel and a walkable feel and we want to maintain a mix of housing which includes Single Family (and some of us really like the fact that there are larger lots.... and know folks wanting to buy these larger lot parcels.

We stated that we did not want folks to drive into our gateways and feel that "YOU ARE NOW ENTERING THE DENSITY ZONE" ... Think twilight zone....

A couple last comments on each of these three areas.

2a) Houghton Slope...

I am disappointed that many from the group requesting higher density have mis-represented things.

- Last mtg several spoke and stated that they originally requested 3.6 so they were compromising
- This is not true.
- *The original request came from Sally Mackle and was a request to reduce from 12 to 8.5 That was approved unanimously and I personally asked if 8.5 would give enough for there to be two times as many units there as now. Based on this discussion, the city asked for a geotechnical report on 8.5 or 7.2.*
- *The votes by LVNC have been consistently only in favor unanimously of 8.5 with one person stating maybe 7.2 and one (Sally) wanting 3.5. Sometimes the neighbor vote was skewed by city staff voting We later asked to have votes be just of neighbors so you'd know what the neighbors thought.*
- *I changed my vote from 7.2 back to 8.5 when I began to see numerous mis-representations. (Folks represented "they just wanted to move back" but county records show rezones & developments) (Folks represented that they couldn't sell their house, but records show they listed 50% above mkt value) (signatures gathered were of "30 people" but only 21 unique properties v. 17 properties opposing or silent)*

2b) TOD ...

You've received my other emails on this. I have watched HCC ask for comparisons and I've asked for comparisons and when they still haven't arrived after 2 years I did my own research and I even toured the San Francisco area properties that were mentioned. Generally the proposed project is twice as dense as other projects and would supply about half the dedicated parking of other projects. The research articles on TOD don't support the small amount of parking and state that turning Park and Ride into TOD is problematic.

I am against having any folks in Kirkland live in sardine cans even if those sardine cans are "affordable" and I really disliked the descriptions provided by Lora and Elsie when they toured and mentioned that things were immense, shocking, etc... and NO WINDOWS in bedrooms. Low income rentals also tend to get junked up with things like bikes on balconies, etc.

Mostly we need the Park and Ride property for a Park and Ride and cannot give it away for housing. We are told that there is no money for garages but one has just broken ground in another King County city. I also believe that the \$6.25 Million is available whether there is housing or not. I have repeatedly asked to see evidence of the comment that housing must be included. When I research the Urban planning site and those cities that were awarded, there are specifics of what the award will be based upon. Housing was not one of the criterion.

2c) YBD...If TOD doesn't go forward, does housing at YBD make sense? Again, LVNC did not want folks to arrive into our city and have big buildings staring at them. We want to be clearly different than Bellevue. We want to be more like a cousin to our neighbors in Yarrow Bay, Clyde Hill, etc. We felt that adding residential here just makes the project bigger. We also felt that it seemed reckless that the city wants to jam residential (and affordable) into every corner they can squish it.

Karen Levenson  
President  
The Park, A Condominium  
6620 Lake Washington Blvd NE #101  
Kirkland, WA 98033

**Dorian Collins**

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**From:** Robert & Phyllis [racpar@w-link.net]  
**Sent:** Monday, March 14, 2011 11:27 AM  
**To:** Dorian Collins  
**Subject:** Support for Park & Ride TOD Project

*Dorian,*

*As a Kirkland resident of Central Houghton, I just want to register my strong support for the proposed South Kirkland Park & Ride TOD Project. I regularly use the present Park & Ride and often find it difficult to locate an empty parking spot mid-day.*

*I also am strongly in favor of providing more affordable housing for families.*

*Robert*

*Robert Carlson & Phyllis Ray  
11119 NE 68th  
Kirkland, WA 98033  
425.827.6125-home phone*

**Dorian Collins**

---

**From:** Margaret Bull [wisteriouswoman@gmail.com]  
**Sent:** Friday, March 11, 2011 12:30 PM  
**To:** Dorian Collins  
**Subject:** TOD

**Follow Up Flag:** Follow up  
**Flag Status:** Flagged

*Hi Dorian,*

*The one thing I am still concerned about is the parking situation for the housing units at the TOD.*

*I believe that there should be NO LESS than one space for each unit. Often people who don't drive for one reason or another need other people to visit them to take care of their various needs or give them rides or whatever. I feel that there needs to be a designated spot for each unit for this reason. I have a friend who uses a cane, or a walker, or a wheelchair. It makes a big difference that there is a space near her apartment for me when I pick her up or when her cleaning person comes or her grandchildren spend the night or bring her groceries. Some people might not use their spot all the time but then they can share it with someone else who needs two spots. If affordable housing is to be used by seniors, than a parking spot for each unit is necessary. My friend also uses her van as a second 'room'. She can sit comfortably in it and get around to restaurants, the library and the park with very little walking. People who have mobility issues may choose to have a car over taking the bus because they can't walk from the bus stop to the places they are going to. That is why there are handicapped parking spots close to public places. There are not bus stops close to many businesses. Affordable housing isn't the same as low income housing.*

*I just don't believe that you can compare the data from other TOD sites with the situation at South Kirkland Park and Ride. The location next to a freeway and away from services is unique. The assumption that the Park and Ride will be used predominately by people avoiding the tolls going into Seattle by taking the bus is perhaps false. As parking becomes more difficult in Kirkland and Bellevue and companies offer bus passes many individuals may choose to use the park and ride lot as 'off site' parking and take the bus from there. Even with the additional spaces planned it is possible that in a few years time the lot will be full every day. If that happens, there won't be many spaces left for people living in the TOD housing units.*

*Every time we have a meeting with the planning commission I want to ask how many people there actually took the bus or walked to city hall. I find it is hard to even arrange a carpool. I feel like I'm begging and really putting people out.*

*Three out of four members of my family use a bus pass even so we would have a hard time going without a car. I think it would be useful to survey the tenants of all the apartments and condos around the South Kirkland Park and Ride and find out how many units are 'car-less'.*

*Cheers,*

*Margaret Bull  
6225 108<sup>th</sup> Place NE  
Houghton*

**Dorian Collins**

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**From:** Marie Stake  
**Sent:** Monday, February 28, 2011 10:36 AM  
**To:** Eric Shields; Paul Stewart; Dorian Collins  
**Cc:** Marilynne Beard  
**Subject:** Kirkland Views Blog Posting: SK P&R

All,  
FYI

[http://www.kirklandviews.com/archives/24926?utm\\_source=feedburner&utm\\_medium=email&utm\\_campaign=Feed%3A+KirklandViews+%28Kirkland+Views%29#](http://www.kirklandviews.com/archives/24926?utm_source=feedburner&utm_medium=email&utm_campaign=Feed%3A+KirklandViews+%28Kirkland+Views%29#)

Marie

## **[Letter | South Kirkland Park & Ride TOD will not benefit us as promised](#)**

by [Karen Levenson](#) on February 27, 2011 in [Opinion](#)

Dear Editor:

After much more than a year of researching the proposed TOD, digging into major research documents on TODs, and evaluating comparisons with other transit oriented development, the Lakeview Neighborhood Advisory Council re-confirmed it's prior position regarding a change of zoning for TOD at our most recent meeting.

Most of us live too far from the proposed TOD to be even remotely considered NIMBY opponents. So, here's where the trouble lies. We are not willing to give up one of the most needed and oversubscribed Park and Rides in order to make it a site for extremely dense housing. The two uses do not appear compatible. Parking spaces planned for the TOD are well below similar TOD projects and will rob necessary Park and Ride spaces or further exacerbate the parking that occurs on neighborhood streets.

Since we were having a hard time getting some information from the City of Kirkland or Metro, we did our own. The idea that housing is a requirement of the Urban Partnership Grant didn't seem to align with what we uncovered. There were 4 T's required: Tolling and Telecommuting (congestion pricing on 520), Technology (real time information signs), and Transit improvements at S. Kirkland and Overlake P&R which were to improve passenger shelters and lighting. We could find no housing requirement built into the approval criterion of the grant, but continue to be open to seeing this information if someone would supply us with documentation.

Now with respect to parking. The S Kirkland Park and Ride has been at 107% of capacity for years and in 2005 was earmarked for a parking garage with 250 additional spaces dedicated to P&R users alone. Now a Transit oriented development is proposed with a projected 1.08 parking spaces per unit. This provided parking is well below CalTrans / University of California Berkeley research that indicates that only a 20% reduction in parking spaces may be achieved with TOD. It is well below the 1.41 spaces per unit on average as detailed in research by California Polytechnic University. So where will the residents park a second car, or a guest? Likely they will pirate P&R spaces. The history of S. Kirkland Park and Ride would also indicate that the overflow will fill neighboring streets. The TOD proposal seems destined to provide even more overflow parking. Smartgrowthplanning.org comments on conversion of station area parking to TOD "Elimination or reduction of parking at these stations best suited to park-and-ride activity can generate unacceptable impacts."

The city has proposed a "shared" parking example that doesn't seem recognize that TODs aim to have residents who use transit to get to their work locations during the day (thus leaving their car at home). In "A Transit Cooperative Research Program," sponsored by the Federal Transit Administration there is the citation that "Shared parking between transit agencies and adjoining development is often seen as one way to shrink the footprint of TOD parking. However, this does not always work in practice." John Gosling, a designer of mixed-use TODs, says "shared-parking reductions in mixed-use settings are not what they are cracked up to be."

"Further complicating the mixed use challenge is the lack of comparables. The "comps" that do exist do not always have distinguished track records. Mixed-use TODs, such as Palm Court...in Long Beach, California, fell into arrears, forcing banks to take it over. Often it has been the ground-floor retail component of TODs that have suffered the most." Retail must be market and destination driven, and not transit driven.

At this time there appears insufficient evidence that this project will blend with the neighborhoods, provide adequate parking, be economically sustainable or be an asset to Kirkland. It is concerning that this could turn into something similar to a project cited by the Transportation Research Board "after six years, areas around stations remain ... forsaken and decaying – denying planners dreams of transit villages."

We are planning for a parcel that welcomes folks into Kirkland and gives them the first impression of our community. Let's not greet visitors with a project that is overbuilt, double density and perhaps an economic failure.

Karen Levenson

**Dorian Collins**

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**From:** Chuck Pilcher [chuck@bourlandweb.com]  
**Sent:** Friday, February 25, 2011 4:59 PM  
**To:** Dorian Collins  
**Subject:** SKPR TOD

*I'm very aware of the meetings, but have said my piece in several venues already and feel like this is already a done deal from the City's perspective, so don't feel like wasting any more of my time commenting. I'm not adamantly opposed, but it really needs more work. This city needs only one thing: Redevelopment of Totem Lake.*

*The rest is all rearranging the deck chairs on the Titanic.*

*Chuck Pilcher  
[chuck@bourlandweb.com](mailto:chuck@bourlandweb.com)  
206-915-8593*

**Dorian Collins**

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**From:** Janice Coogan  
**Sent:** Wednesday, February 16, 2011 12:50 PM  
**To:** Dorian Collins  
**Subject:** FW: TOD Parking for Residential, Office, Retail, Commercial: Research & Evalu...

Janice Coogan  
 Planning and Community Development  
 425-587-3257  
[jcoogan@ci.kirkland.wa.us](mailto:jcoogan@ci.kirkland.wa.us)  
[www.ci.kirkland.wa.us](http://www.ci.kirkland.wa.us)

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**From:** [Uwkkq@aol.com](mailto:Uwkkq@aol.com) [<mailto:Uwkkq@aol.com>]  
**Sent:** Wednesday, February 16, 2011 12:43 PM  
**To:** [Uwkkq@aol.com](mailto:Uwkkq@aol.com); Janice Coogan; Paul Stewart; [johnk@kapplerhomeplans.com](mailto:johnk@kapplerhomeplans.com); [jay@jayarnold.org](mailto:jay@jayarnold.org); [jayarn@msn.com](mailto:jayarn@msn.com); [bkatsuyama@mrsc.org](mailto:bkatsuyama@mrsc.org); Kurt Triplett; Marilynne Beard; [william.goggins@gmail.com](mailto:william.goggins@gmail.com); [betsyp@hotmail.com](mailto:betsyp@hotmail.com); [kathleen.a.mcmonigal@boeing.com](mailto:kathleen.a.mcmonigal@boeing.com); [Tica2345@gmail.com](mailto:Tica2345@gmail.com); [lhein@washingtonea.org](mailto:lhein@washingtonea.org); [RWhit5009@aol.com](mailto:RWhit5009@aol.com); [Rwhit@aol.com](mailto:Rwhit@aol.com); [Rallshouse@ci.shoreline.wa.us](mailto:Rallshouse@ci.shoreline.wa.us); [TennysonKK@aol.com](mailto:TennysonKK@aol.com); Joan McBride; Penny Sweet; Amy Walen; Dave Asher; Jessica Greenway; Doreen Marchione; Bob Sternoff; [georginef@msn.com](mailto:georginef@msn.com); [brian@brianandemily.com](mailto:brian@brianandemily.com); [chuck@bourlandweb.com](mailto:chuck@bourlandweb.com); [RLSTYLE@aol.com](mailto:RLSTYLE@aol.com); [shthornes@comcast.net](mailto:shthornes@comcast.net); [NinaKpete@aol.com](mailto:NinaKpete@aol.com)  
**Subject:** Re: TOD Parking for Residential, Office, Retail, Commercial: Research & Evalu...

The CalTrans University of California Berkeley Study stated that if a TOD is developed with routine services (grocery, gym etc) in easy walking distance then number of parking stalls needed is reduced by about 20%. Based on our numbers this would mean we should be able to get by with 1.4 to 1.5 parking spaces per unit before we begin to cut into the spaces for Park and Ride users or further fill the neighboring streets with additional cars.

The UC Berkeley study also provides a worksheet and one of the first requirements for TOD is that there is currently excess parking available and currently street parking is well under control.

Karen Levenson

In a message dated 2/16/2011 12:38:36 P.M. Pacific Standard Time, [Uwkkq@aol.com](mailto:Uwkkq@aol.com) writes:

Hi all:

The data on what amount of parking is necessary in a TOD is definitely out there. Below are two studies that are fairly representative and indicate how the project was perceived years later.

Fehr & Peers Transportation Consultants  
 (Experience in San Francisco Bay Area, Washington DC, Washington State, other cities throughout US, Canada, Brazil and Venezuela).

**"Conversion of station-area parking to TOD:** When developed at sufficient densities, TOD can produce higher transit utilization and lower auto use than transit station parking. Elimination or reduction of parking at those stations best suited to park-and-ride activity can generate unacceptable impacts."

===

A Study of parking and TOD projects by CalTrans and University of California Berkeley

Transit frequency |

1	Pacific Court, Long Beach, CA	Every 5 -10 min
2	Uptown District, San Diego, CA	Every 15-30 min
3	Rio Vista West, San Diego, CA	Every 15 min
4	Ohlone-Chynoeth, San Jose, CA	Every 10-20 min
5	Emeryville, Emeryville, CA	10-15 min
6	Pleasant Hill, PH, CA	5-10 min
7	Dadeland South, Miami, FL	5-10 min
8	Dadeland North, Miami, FL	5-10 min
9	Lindbergh City Ctr, Atlanta, GA	4-8 min
10	Union Station, Portland, OR	5-10 min
11	Mockingbird Station, Dallas, TX	5-10 min

=== Parking spaces per residential unit (Notes at end state success or failure)

	<u>Residential</u>	<u>TOD Residential</u>
1	1/studio, 2/1BDR Guest 3/10 units	1/sudio, 2/1BDR, Guest 0
2	2.25 spaces/unit	2.25 spaces/unit
3	1.25-2.25/unit, 1/Senior Housing	1-2/unit, 1/Senior Housing
4	1.7 spaces/unit	Not changed for TOD
5	1.25 sp/1BDR, 2.25 sp/2BDR (incl guest)	Varies
6	1.75/unit	1.35/unit
7	1.5/1BDR, 1.75/2BDR	1/unit
8	1/2BDR	1/unit
9	1/1BDR, 2/2BDR	1.85 per condos, 1.5 per apartmt
10	-	.75/unit
11	1.16/unit	Dallas doesn't allow reduction

Commercial/Office/Retail spaces (see notes at end re: success/failure)

	<u>Retail/Comm</u>	<u>TOD Retail/Comm</u>
1	Retail 5 per 1000 sq ft	Retail 2 per 1000 sq ft
2	Comm 1 per 250 sq ft	1 per 280 sq ft
3	N/A	N/A
4	R/C 1 per 231 sq ft	R/C not changed for TOD
5	R/Office/C 3 per 1000 sq ft	Varies
6	Office/Ret 5 per 1000 sq ft	Off 3.3/1000sf, Ret 4/1000sf
7	Office/Ret 1/250 sq ft	Office/Ret 1/400 sq ft
8	Office/Ret 1/250 sq ft	Office/Ret 1/400 sq ft
9	Off 3.3/1000sf, Ret 5/1000sf	Off 2.5/1000sf, Ret 5/1000sf
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Comments

- 1 Pkg for res/retail appears sufficient but not excessive
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- 10 parking
- 11 Experience - The developer had to build more
- 12 parking
- 13 Flexibility exists as future development is expected in
- 14 area
- 15 Pkg is \$75 per month, Street pkg avail at \$40/month
- 16 Too early to see if adequate
- 17 ~ Karen Levenson

**Dorian Collins**

**From:** Janice Coogan  
**Sent:** Wednesday, February 16, 2011 12:50 PM  
**To:** Dorian Collins  
**Subject:** FW: TOD Parking for Residential, Office, Retail, Commercial: Research & Evaluation

Janice Coogan  
 Planning and Community Development  
 425-587-3257  
[jcoogan@ci.kirkland.wa.us](mailto:jcoogan@ci.kirkland.wa.us)  
[www.ci.kirkland.wa.us](http://www.ci.kirkland.wa.us)

---

**From:** [Uwkkq@aol.com](mailto:Uwkkq@aol.com) [<mailto:Uwkkq@aol.com>]  
**Sent:** Wednesday, February 16, 2011 12:38 PM  
**To:** [Uwkkq@aol.com](mailto:Uwkkq@aol.com); Janice Coogan; Paul Stewart; [johnk@kapplerhomeplans.com](mailto:johnk@kapplerhomeplans.com); [jay@jayarnold.org](mailto:jay@jayarnold.org); [jayarn@msn.com](mailto:jayarn@msn.com); [bkatsuyama@mrsc.org](mailto:bkatsuyama@mrsc.org); Kurt Triplett; Marilynne Beard; [william.goggins@gmail.com](mailto:william.goggins@gmail.com); [betsyp@hotmail.com](mailto:betsyp@hotmail.com); [kathleen.a.mcmonigal@boeing.com](mailto:kathleen.a.mcmonigal@boeing.com); [Tica2345@gmail.com](mailto:Tica2345@gmail.com); [lhein@washingtonea.org](mailto:lhein@washingtonea.org); [RWhit5009@aol.com](mailto:RWhit5009@aol.com); [Rwhit@aol.com](mailto:Rwhit@aol.com); [Rallshouse@ci.shoreline.wa.us](mailto:Rallshouse@ci.shoreline.wa.us); [TennysonKK@aol.com](mailto:TennysonKK@aol.com); Joan McBride; Penny Sweet; Amy Walen; Dave Asher; Jessica Greenway; Doreen Marchione; Bob Sternoff; [georginef@msn.com](mailto:georginef@msn.com); [brian@brianandemily.com](mailto:brian@brianandemily.com); [chuck@bourlandweb.com](mailto:chuck@bourlandweb.com); [RLSTYLE@aol.com](mailto:RLSTYLE@aol.com); [shthornes@comcast.net](mailto:shthornes@comcast.net); [NinaKpete@aol.com](mailto:NinaKpete@aol.com)  
**Subject:** TOD Parking for Residential, Office, Retail, Commercial: Research & Evaluation

Hi all:

The data on what amount of parking is necessary in a TOD is definitely out there. Below are two studies that are fairly representative and indicate how the project was perceived years later.

Fehr & Peers Transportation Consultants  
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9	Off 3.3/1000sf, Ret 5/1000sf	Off 2.5/1000sf, Ret 5/1000sf
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#### Comments

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- 9 Flexibility exists as future development is expected in area

10 Pkg is \$75 per month, Street pkg avail at \$40/month

11 Too early to see if adequate  
~ Karen Levenson

## Dorian Collins

---

**From:** Janice Coogan  
**Sent:** Wednesday, February 16, 2011 10:12 AM  
**To:** Dorian Collins; Paul Stewart  
**Subject:** FW: S Kirkland input so far doesn't explain how this will work

**Follow Up Flag:** Follow up  
**Flag Status:** Flagged

Can one of you answer her questions for tonight?

Janice Coogan  
Planning and Community Development  
425-587-3257  
[jcoogan@ci.kirkland.wa.us](mailto:jcoogan@ci.kirkland.wa.us)  
[www.ci.kirkland.wa.us](http://www.ci.kirkland.wa.us)

---

**From:** [Uwkkq@aol.com](mailto:Uwkkq@aol.com) [<mailto:Uwkkq@aol.com>]  
**Sent:** Tuesday, February 15, 2011 7:24 PM  
**To:** [Uwkkq@aol.com](mailto:Uwkkq@aol.com)  
**Subject:** S Kirkland input so far doesn't explain how this will work

Hi all:

I have gotten some response on this but so far no explanation on how this will provide any new parking stalls without taking away stalls. If anyone has any perspective on the stalls we will be gaining, please email me prior to tomorrow's meeting.

Additionally, I got one email stating that the 5 stories were to be two below ground parking, but then realized that the two parking floors had been disallowed below ground due to too expensive to put there. At this point it seems we have floors 1-2 parking, floor 3 retail, floor 4-5 100-125 residential units on each floor. Any other perspectives are welcomed prior to tomorrow's meeting.

Thanks  
Karen

---

**From:** [Uwkkq@aol.com](mailto:Uwkkq@aol.com)  
**To:** [Uwkkq@aol.com](mailto:Uwkkq@aol.com)  
**CC:** [kathleen.a.mcmonigal@boeing.com](mailto:kathleen.a.mcmonigal@boeing.com)  
**Sent:** 2/15/2011 4:13:57 P.M. Pacific Standard Time  
**Subj:** Reaching out for your perspective & TOD information clarification

Hi all:

I am preparing for my input to Lakeview Neighborhood Advisory Committee tomorrow night. I am reaching out to see if anyone can provide me with understanding of some things I don't understand. They could impact my thoughts and my pro/con comments on the TOD project.

- 1) Can anyone explain to me the 5 stories as proposed for the project. Here's what it sounds like.
  - a) There are some locations where the slope of the terrain adds or subtracts a story
  - b) Generally the first two floors will be parking (currently 300 but will be 550)
  - c) The 3rd floor is retail
  - d) The 4th and 5th floors are residential (approximately 100-125 units per floor to equal 200-250 units)  
.....Please explain what is consistent with the plan and what I've mis-understood. It seems like it would be pretty tight to have 125 units per floor so I think I may be missing something.
- 2) We have been told that we are doing this to receive a grant and the project must net the P&R more stalls

Here's what I'm seeing and cannot figure out where the "extra" stalls will come from

- a) 2005 Metro was showing that S. Kirkland P&R was most over parked at 107%
- b) Grant funds were to improve transit facility and provide more parking
- c) 250 new stalls will be built
- d) 200-250 new residential units

Based on the fact that the retail units will need spaces, units need 1-2 spaces each, guests need spaces, etc.... 250 stalls doesn't even seem to provide for the new residents. It seems that we will net a loss of spaces as residents or their guests are forced to park in P&R spaces.

.....Does someone have an explanation that I'm not grasping??????

Thanks all... I am really interested in making sure that I convey information to LVNAdvisory that is as factual and non-emotional as possible. Your insights... even if different than mine... will be helpful for me to achieve this.

Karen Levenson  
President  
The Park, A Condominium

**Dorian Collins**

---

**From:** georgine foster [georginef@msn.com]  
**Sent:** Tuesday, February 15, 2011 9:27 AM  
**To:** Dorian Collins; johnk  
**Subject:** When Transit oriented development disappoints  
**Attachments:** Wake Up, Washtenaw!: When Transit Oriented Development Disappoints

Dorian and John,

I got some help with my computer problem.....the article is now an Attachment that can be easily accessed. If you would please Distribute to the HCC and PC, as they had asked for information on both successful and not-so-successful TOD projects.

Thank you.  
georgine foster

**Dorian Collins**

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**From:** Saved by Windows Internet Explorer 8  
**Sent:** Tuesday, February 15, 2011 9:08 AM  
**Subject:** Wake Up, Washtenaw!: When Transit Oriented Development Disappoints  
**Attachments:** ATT555673.dat; ATT555674.dat; ATT555675.gif; ATT555676.gif; ATT555677.css; ATT555678.dat; ATT555679.dat; ATT555680.dat; ATT555681.dat

## Wake Up, Washtenaw!

A citizen organization with a vision and a plan for sustainable, transit-oriented development in Washtenaw County, Michigan. [Wake Up Washtenaw Home Page](#)

**Thursday, November 4, 2010**

### When Transit Oriented Development Disappoints

[Rail~Volution 2010](#) was great. I'm very thankful to have been able to go to Portland this year. We expect great transit in large cities like Boston (which hosted Rail~Volution last year), but it's more refreshing to see a mid-size metro area that has invested in transit for that last 25 years.

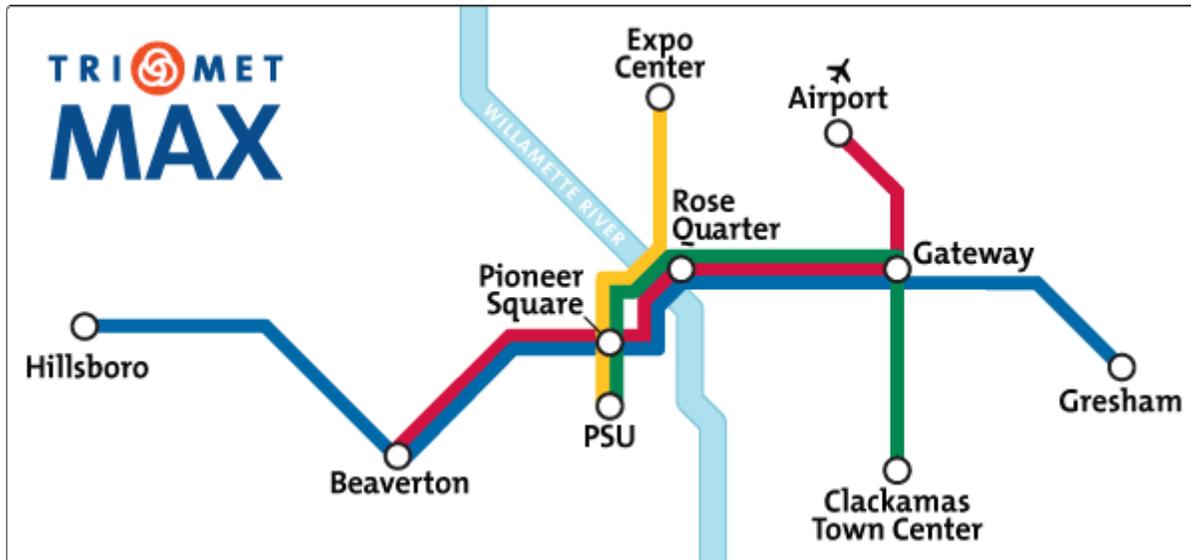
There are so many great lessons to be learned when a thousand transit-oriented development folks gather in one place. What I'd like to focus on is when TOD *doesn't* work, or when it looks different from what we might expect or prefer.

The highlights of the week for me were the two field trips to TOD sites. The first was east to Gresham, the second west to Beaverton and Hillsboro.



## East Side

In 1986, Gresham was a working-class town that became the eastern terminus of Portland's first light rail service, the [Blue Line](#). It's still a working-class town with very little "gentrification" evident. Transit-oriented development appears confined to a few new buildings near stations, and a shopping center.



The TOD does follow the new urbanist principles of mixed use, residential over commercial. In the shopping center, the parking will eventually be located underground or beneath commercial-residential buildings. Otherwise, there's little to distinguish it from other recent developments. Well, perhaps the fact that it's recent development is a distinguishing feature in itself, since very little development has taken place recently.

Most of the eastern Blue Line runs in the middle of a wide boulevard. It wasn't a boulevard until the light rail was built, though - and lots of engineering work had to be done to mitigate the widening and leveling of the original two-lane roadway. The cost of building the line included re-doing hundreds of driveways, lawns, fences, and retaining walls. And though the neighborhoods don't look well-to-do, they certainly don't look run-down or impoverished except in one or two old warehouse districts where the loft/office refurbishing trend hasn't quite taken off.

A couple of things were disappointing. One was the new county office building whose front is turned away from the Gresham Central Transit Center, as if to ignore its presence. Another is the new station at Civic Drive being built within 30 yards of a new mixed-use development, with no architectural connection to it. In many parts of the world, developers and mall owners are eager to attach their space as closely as possible to rail stations. In most Japanese cities and towns, the railway station is the central shopping center, crowded not only with travelers and commuters, but with shoppers. There seems no physical reason why electric rail vehicles can't run through

the middle of a shopping center here in the US, too. [Federal transit legislation](#) for years has encouraged public-private partnership in the construction of transit stations. So why aren't we doing it? In this case, the answer was, "Hmmm, well, we had this Federal grant to build the station, so we just did it." It has a (very nice) outdoor platform covered with a (very nice) glass roof, but is basically an outdoor space. What a shame the concept of integrating transportation and shopping just isn't part of most planners' thinking here in the US.

So on Portland's east side, light rail appears to provide a foundation to keep property values steady without overly inflating them. TOD has been modestly successful, and there is likely to be more as the economy recovers.

## West Side

Beaverton and Hillsboro are in the opposite direction from Gresham on the Blue Line, both geographically and socially. Where Gresham is mainly working class, the west side is predominantly middle class and high-tech. In fact, Beaverton and Hillsboro were small towns up through the 1960s. Change came in the late 1960s when the Sunset Highway became a freeway, built to speed access to downtown Portland across the Tualatin Mountains from the west. Light rail didn't arrive until 1998, because it required a 3.1-mile tunnel bored through the mountains. But both towns have been sites of high-tech industry since the 1960s, starting with instrument-maker Tectronix in Beaverton, followed by Intel, which now operates four large campuses for research, development, and chip fabrication in the Hillsboro area. If there is a place where TOD *should* work, this is it.

Has it? Yes, and no.



The [Beaverton Round](#) is such an exciting project, and such a disappointing failure. Conceived by visionary developers Selwyn Bingham and Sylvia Cleaver, it was to include condos, shopping, and office space built, literally, around the light rail station and a park. What could have gone wrong?



For one thing, construction problems. The site is apparently over a disused sewage treatment plant, with all the remediation and uncertainty such a place can bring. Then there was use of shoddy materials and construction techniques, resulting in multiple law suits and requiring expensive replacement of windows and facing. (The sad details are [here](#) if you really want to know.) Continually plagued with [financing problems](#), only part of the planned development has been built, and even less occupied.



[Orenco Station](#)'s development fared much better. Longer in the planning, it involved more community input and balanced practicality with idealism. No compromise was made in new

urbanist principles. The big disappointment to me is the location of the town's center: at a busy intersection, not at the rail station. In fact, it's a good quarter mile through (currently) empty space from station to center. Traffic in the center is heavy, and our group was warned that when the walk-light came on, we must hurry across the road because it was a short light. I hate to see a town center where pedestrians are chivvied and it's too noisy to hold a conversation. But as the developer pointed out, they couldn't have made it work without the traffic passing by to lure customers to the town's businesses.



And centering the town on the highway has worked financially. Businesses have leased most of the available space, and residents have bought most of the available housing. Once away from the highway, the open spaces and built environment are very inviting and peaceful.



This is a [well-studied project](#), and one finding from the developer's study was what most attracted people to this TOD. No, it wasn't the transit (shucks!). Transit was seen as a good amenity, but what really drew people was the architecture and design.



[Pac Trust](#), the developer, went in with little if any experience building residential communities - they had specialized in commercial and industrial. Not knowing what buyers would prefer, they had a number of styles illustrated by photos and drawings, and showed them to people. What folks kept saying "Wow" about was a design based on Boston's classic townhouses, so that's what they built for the town center. Free-standing houses are built in an English cottage style, with lots of quaint detailing. And it sold beautifully (though of course it doesn't hurt to be surrounded by large Intel facilities). What about shopping? Yes, there is a Starbucks on the main corner ;- ) and one of the best Indian restaurants in the Portland area (Intel influence again). More important, they attracted New Seasons, Oregon's biggest regional natural foods grocery chain, to anchor the shopping district, along with a number of small, trendy shops. There's still a lot of undeveloped land in the TOD, but what there is has been developed very densely: 9.9 houses per acre, and far more units per acre in the townhouse and rental spaces. Washington County had no zoning that would allow that much density, so planned unit development standards were crafted and implemented for the new urbanist model.

The [article cited](#) has lots of discussion on whether there's decreased auto use due to proximity of work and shopping options, and that's clearly a win-win for any development. There has also been support in [Todd Litman's](#) blog of the notion that the biggest advantage of TOD is that people walk more. Looks like this is a case in point.



So what to take away? First and foremost, TOD has to be done well, and the financing has to be as solid as any other type of development - even if financing TOD has to be creative. But also, we may have to sacrifice some of our ideals to make it work, at least while the automobile is still in the ascendant. We can't expect TOD to create instant transit riders out of everyone, but if we can get them to walk and ride their bikes for the short trips, we've really done our job.

Posted by Faramir at [6:14 PM](#)

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Advocating Sustainable, Transit-Oriented Development for Washtenaw County, Michigan

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    - [Getting Back On Track](#)
  - ▼ November (3)
    - [It's About Diversity](#)
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    - [When Transit Oriented Development Disappoints](#)
  - ▶ October (1)
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  - ▶ December (2)
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    - [WALLY! Jackson!](#)
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- ▶ 2007 (3)
  - ▶ December (2)
  - ▶ November (1)

## About Me



Larry Krieg

[View my complete profile](#)

**Dorian Collins**

---

**From:** Paul Stewart  
**Sent:** Thursday, February 10, 2011 11:47 AM  
**To:** Dorian Collins  
**Subject:** FW: South Kirkland Park and Ride Transit Orientated Development

**Follow Up Flag:** Follow up  
**Flag Status:** Completed

[Another letter.](#)

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**From:** [wallycostello@comcast.net](mailto:wallycostello@comcast.net) [<mailto:wallycostello@comcast.net>]  
**Sent:** Wednesday, February 09, 2011 11:56 PM  
**To:** Paul Stewart  
**Subject:** South Kirkland Park and Ride Transit Orientated Development

Dear City of Kirkland,

I am writing in support of the South Kirkland Park and Ride Transit Orientated Development. In my opinion public agencies/jurisdictions should always be looking for ways to better utilize public assets. I could go as far as to say public agencies/jurisdictions have an obligation to look for ways to better utilize public assets. In these challenging economic times it is even more important to find ways to better utilize public assets especially under utilized public assets.

Although the South Kirkland Park and Ride is currently maximizing all of the existing parking stalls it is still under utilized as just a surface parking lot. Combining Transit Orientated Development (TOD) with expanding the capacity of the Park and Ride lot is an ideal example of better utilizing a public asset. Expanding the capacity of the park and ride lot is obviously needed but with funding sources difficult, combining the expansion of the park and ride facility with TOD creates the avenue for the expansion to happen.

During the past few decades people in this region have had to drive further and further to locations where they can afford to live. We all know the impact on our roads and environment from these long commutes. When I was growing up in Kirkland during the 50s and 60s housing was affordable. Today this is not the case. It seems to me we should be looking for ways to provide affordable housing in Kirkland. Combining Transit Orientated Development that has a component of affordable housing with the expansion of the park and ride lot seems like an ideal combination of promoting public transit with the increased capacity of the park and ride lot with TOD that has some component of affordable housing.

The location seems ideal for redevelopment with separation with the residential neighborhoods either topographically or by very mature trees and the majority of adjacent uses commercial or office in nature which the proposed Transit Center Development would be very compatible with.

The Cascade Land Conservancy supports vibrant urban communities as one of the ways of helping conserve and protect undeveloped land in this region and help reduce traffic on our highways. As a Kirkland resident and a member of the Cascade Land Conservancy I support the proposed Transit Orientated Development combined with the increased capacity of the South Kirkland Park and Ride Lot.

Thank you,  
Wally Costello

Dear Council Members and Planning Commissioners,

2/7/11

I believe the South Kirkland Park & Ride needs to be expanded, and that it and the surrounding area is a perfect place for Transit Oriented Development (TOD).

Transit has become a vital part of our transportation network. Metro now has over one hundred million boardings a year, an average of over 50 for every man, woman and child in King County. Transit carries fifty percent of rush hour commuters into downtown Seattle, providing a welcome alternative to being stuck in traffic. (The corresponding number for Bellevue now exceeds 20%). As our population grows, access to an expanding transit system will be an even more essential part of a sustainable future. For this, suburban cities such as ours will continue to need park and ride lots.

The TOD concept under discussion for the South Kirkland Park & Ride would expand access to transit both by providing housing on site, and by significantly increasing the number of existing stalls beyond that needed for the housing. It would provide housing choices, including units that are affordable to lower income people such as retail clerks, teachers, and perhaps some of our own children, who are forced to commute long distances now. Expansion of the existing stalls would relieve overflow parking in the neighborhoods, and loss of access for many potential riders.

With excellent freeway and rail access, and a topography that will support increased density and height, it is hard to imagine a better location for this type of development. I fully support the draft regulations prepared by city staff to make the concept work. Design will be important. Some limits on finish color might help, as would designing for a mix of income levels and ages.

I see the South Kirkland Park and Ride TOD as an unusual opportunity to get something started that doesn't conflict with long range TOD development for the area, and urge your support.

Thank you for your service to Kirkland!

Dave Russell

February 5, 2011

Mayor Joan McBride  
City Council Members  
City of Kirkland  
123 5<sup>th</sup> Avenue  
Kirkland, Washington 98033-6189

Re: South Kirkland Park and Ride TOD

Dear Mayor McBride and City Council Members:

I am writing in support of the transit oriented development proposed for the South Kirkland P&R. As principal of a small Kirkland consulting firm, I use the Metro buses for meetings in Seattle as much as possible, with the majority of my trips originating from or travelling through the South Kirkland P&R on Metro Route 255. There are instances when I take the bus into Seattle twice in the same day for meetings. The buses arrive in the downtown tunnel about 20 minutes later, I avoid the hassle/cost of parking, and one additional single occupancy car is taken off the over crowded SR 520 highway. The fast approaching toll road fees will make taking the bus even more favorable.

I believe that the South Kirkland P&R is a logical, pragmatic location for a mixed use housing development for the following reasons:

1. The adjacent bus transit facility will provide easy access to public transportation for the users and residents of the development.
2. The location provides easy, direct access to SR 520 and I-405 without having to traverse residential neighborhoods.
3. Given the location of the site and the topography, there will be minimal visual or physical impact on the existing residential neighborhoods to the north, even with the multi-story structures over the underground parking..
4. The mixed use building is very compatible with the adjacent office, commercial, and educational uses, and will provide needed housing near jobs.
5. The mixed use housing development will feature both market rate and affordable housing, providing a broad range of housing choices for the region and the City of Kirkland.
6. The multi-level underground parking structure will provide needed additional spaces for users of the park and ride, since the current lot is over capacity at times during the day. This current shortage of spaces will only worsen once the system for collecting toll fees is put in place later this Spring.
7. The mixed use development will make better and more intense use of properties that are already under public ownership, rather than having to compete for other privately owned parcels in the area.

One of the major arguments made by the opponents to the South Kirkland TOD is the traffic impact on neighborhoods, especially to the north through the Lakeview and

**RECEIVED**

**FEB 08 2011**

CITY OF KIRKLAND  
CITY MANAGER'S OFFICE

Houghton neighborhoods. If there is no residential development at the park and ride, Metro will certainly need to construct structured parking to meet the demand for more spaces. This will result in even more transit users having to **drive** to the park and ride. If the housing is provided in conjunction with the additional parking, the residents can simply use the transit without getting in their cars. I am a development consultant, not a traffic engineer, but I believe the bulk of the auto trips made by the future residents of the proposed development will travel to the west, south, and east due to having better access to needed services and highways.

As principal of a firm providing community land planning services, I am an active participant with the Urban Land Institute-Seattle and the Master Builders Association of King and Snohomish Counties, as well as a Community Trustee with the Cascade Land Conservancy. Each of these groups recognizes the importance of transit oriented mixed use development, housing affordability, and the provision of higher density housing in appropriate locations within the existing urban footprint. Through the implementation of smart growth principles, the land can be developed more efficiently with easy access to public transportation, parks, community services, and shopping. The Cascade Land Conservancy is an advocate for compact, complete, and connected communities to create livable neighborhoods in our urban areas, thereby reducing the development pressures on outlying resource and open space lands. The South Kirkland Transit Oriented Development is one important step toward this vision and I urge your support of the proposed mixed use housing development. Thank you.

Sincerely,



Craig J. Krueger  
Community Land Planning  
733 - 7<sup>th</sup> Avenue, Suite 100  
Kirkland, WA. 98033  
425-285-2393

**Dorian Collins**

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**From:** Paul Stewart  
**Sent:** Thursday, February 10, 2011 5:10 PM  
**To:** Dorian Collins  
**Subject:** FW: TOD 2 yrs of discussion and still critical questions never answered

Another comment.

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**From:** [Uwkkkg@aol.com](mailto:Uwkkkg@aol.com) [<mailto:Uwkkkg@aol.com>]  
**Sent:** Thursday, February 10, 2011 5:03 PM  
**To:** Janice Coogan; Paul Stewart; [Uwkkkg@aol.com](mailto:Uwkkkg@aol.com)  
**Subject:** TOD 2 yrs of discussion and still critical questions never answered

Janice: Please forward to all HCC Members... HCC, could someone let me know this was received?? Thx  
=====

Good Afternoon HCC members:

Thanks, as always, for your tireless energies on the subject of TOD.

You have been at this over two years. You (and our neighborhood groups) have been asking PC for critical information that we will need if we are to review and potentially accept this project. Still none of the questions ever gets answered. I am asking you to consider the lack of response as the city's willingness to have you disapprove the TOD.

You have stated, as we have, that we would like to support their proposal but have significant concerns that they need to put to rest if we are to have this in our community.

To date... No outreach with any answers to the questions asked. NONE.

**This failure to provide answers sends one message loud and clear. The planning commission is willing to have the TOD disapproved for the S. Kirkland P&R site.**

Rick Whitney, recently closed a HCC meeting telling staff that he couldn't stress more that it was of utmost importance that they reach out to the communities with the information that would help the communities accept the TOD... The answers to their questions.

In spite of the very direct comment by Mr. Whitney, we still have not gotten a single answer. To name just a few of the requests... there has been request to provide comparison to other TODs that are successful even when routine services are not in walking distance, there has been request to provide comparison to other TODs built to such immense density (and how we can build so dense without having windowless bedrooms), as most TODs provide for between 1.6-1.8 vehicles per unit we've asked to be provided with comparative TODs that have worked with the tiny amount of parking that has been proposed. No answers. No comparison projects. .... No approval.

The most troubling issue is parking. We are building on a parcel that currently provides 300 spaces. A two story garage provides 600. This park and ride has been awaiting an expansion of 250 spaces that was earmarked as dedicated P&R spaces since 2005. The importance of these extra dedicated spaces has been noted since S. Kirkland P&R is the most overcrowded in the system at 107% of capacity. If you consider that this project will allow us to meet that target then there will only be 50 spaces for approximately 400 new residents. When questioned about this, a vague "sharing" concept was described as follows..."when someone leaves their residence for work, a Park and Ride car could use that space".... BUT LORDY... isn't the whole idea that the TOD resident will take the bus to work... ??? They will need a car to run to the grocery store at 6pm and might free up their space for an hour or two during the hours when P&R spaces are not needed.

Are we going to have a residential project or are we going to have a park and ride. Without supporting information to the contrary, it appears these are incompatible. We must choose one or other. I say transit and Park and Ride. This is one of our most important places for folks to leave their car and take transit. Hopefully we'll encourage more and more ridership over the years and we will thus need more and more parking spaces. Let's not give away our P&R.

In closing, if the planning commission has information that can make us like or love this project, please share that information. If it doesn't arrive, I am asking us all to assume that they are OK with a disapproval from HCC.

Karen Levenson  
President  
The Park, A Condominium  
6620 Lake Washington Blvd NE, Kirkland 98033

April 3<sup>rd</sup>, 2011

Dear Planning Commission members,

Just for your information here are the planned bus route changes. The 234 bus will no longer go down 108<sup>th</sup> Ave NE if the changes take place in the fall.

These are the only bus routes from South Kirkland Park and Ride that run on Sundays. The frequency is listed starting with peak times during the weekdays, then midday times, then evening and weekend times. Please notice that the buses run only every 30 to 60 minutes after 6:00 p.m.

**Trips**

**Added 255**

IDS - Brickyard or Totem Lake TC via SR-520, South Kirkland, Kirkland TC, Juanita, and NE 124th Street

5:30 a.m. to  
midnight 10-15 15 30-60

**Revised**

**230\***

**WEST**

**(235)**

Kingsgate - Bellevue via Kirkland, South Kirkland, 116th Ave NE

5:15 a.m. to  
12:30 a.m. 30 30 60

**Revised 234** Bellevue TC - Kenmore via South Kirkland,

KTC, Juanita, and Finn Hill

5:45 a.m. to  
9:45 p.m. 30 30 60

I checked the current Sunday schedules from South Kirkland Park and Ride. It would only take an hour if I wanted to go to a Redmond church for a 9:00 a.m. service via bus from South Kirkland Park and Ride. I would have to get on the 230 at 7:57 in order to get to Redmond Transit Center by 8:48 or the 255 at 7:29 to get there by 8:30. This would only work if my church was a few minutes walk from the transit center and there wasn't any problems transferring buses in Kirkland. When Metro talks about improving routes they don't usually expect to improve Sunday routes. Often they don't improve mid-day routes either which is when many seniors will be taking the bus to get to doctor's appointments and other day-time activities. The improvement of routes is dependent on public funds in addition to fares. Because of limited resources improvements will usually be allocated to routes and times with the greatest ridership. This is great for commuters but not for those who need public transportation the most because they don't drive: seniors, youths and the disabled. Even though I appreciate the concept of TOD, I wonder how many people have actually tried to take a bus for all their weekend activities especially with children in tow. Fares for adults run from \$ 2.25 to \$3.00 at this time. Two adults with two children taking the bus to church would be \$6.00. It isn't that much money if you double up on your errands too. It may be true that many commuters will not own cars if the gas prices continue to go up. Unfortunately, despite the high gas prices now, some of the future workers in King County are filling up the Lake Washington High School parking lot with their cars. (Perhaps we should change the law so young people have to wait until 18 to get their licenses. Teenagers would then get more practice riding public transportation.) I believe that Lake Washington School District

does provide Orca cards to its students. Just because someone has access to a bus pass doesn't mean that they don't ever use a car.

I am not making these comments to discourage planning for TODs in the future. In theory having transit, services, and housing in the same location makes sense. It is something the Europeans have always done. I question whether or not the South Kirkland Park and Ride lot is the best place for this concept to be successful. Hopefully the Planning Commission will come up with a plan that will work.

I get tired of people making assumptions using a theoretical concept rather than concrete knowledge from their own personal experience. We talk about 'those people who need affordable housing and won't own a car because they can't afford the gas'. I have one of 'those people' in my family but I can tell you that she frequently has to get a ride from someone else in order to visit friends and relatives at night even though she lives next to a park and ride transit center. I'm sure that people living in the market rate housing units planned for this development will still own cars even if they take transit to work. High gas prices don't necessarily keep people from owning cars...just driving less or owning a hybrid or other alternative vehicle. When a car owner reduces his vehicle trips, his car is taking up a parking space most of the time, either at his place of residence or on the street. In this instant it may be a space at the park and ride lot.

On the King County Transportation web page it states :

To reduce external trips, TOD projects should be located in higher-density, mixed-use, urban pedestrian districts with high-quality transit service. External single-occupancy vehicle trips can be reduced as much or more by people walking within a mixed-use urban district as they can by using transit within and between urban centers.

The thing I think is missing at the South Kirkland Park and Ride is the 'higher-density, mixed-use, urban pedestrian district'. I think that part of the urban pedestrian district idea is that you can SAFELY walk to the services that you need.

I have to correct some of my previous comments. I did not realize that the North Towne QFC in Bellevue is open 24 hours a day and only 1 mile from South Kirkland Park and Ride. It takes 3 minutes to get there on the 230 bus (this may change when it becomes 235) which runs every half an hour. They even have a pharmacy that is open 9 to 9 on most days. If you want to save the \$ 2.50 bus fare you can take your life in your hands and walk there in 25 minutes. And you can walk back even quicker since it is all downhill. So I realize that one of my objections to the TOD at South Kirkland Park and Ride is not valid—there is a grocery store that includes a pharmacy within walking distance.

Many of you laugh at the idea of a multilevel garage at a TOD but it is a possibility in the future. The Redmond TOD already has a multistory garage, and the TOD projects in Auburn, Kent, Renton, and Shoreline are being planned near existing transit oriented parking garages or where new ones may be developed in the future. I wish the Kirkland Planning Commission would actually study the plans for other proposed TODs before making any final decisions on what needs to be incorporated into the South Kirkland Park and Ride design. The fact that there is only one Sound Transit bus route at South Kirkland Park and Ride doesn't mean there won't be more in the future and the need for parking will be much greater than it is now. It is my belief that the City of Kirkland and the City of Bellevue need a fuller vision for the whole area surrounding the South Kirkland Park and Ride and how it should be developed over the next 15 years, not just the Lakeview Neighborhood piece of the park and ride lot. It would be interesting to know how these other cities have incorporated the idea of a TOD into their greater city development plans.

Sincerely,

Margaret Bull  
6225 108<sup>th</sup> Place NE  
Kirkland WA 98033



April 5, 2011

Houghton Community Council  
Kirkland Planning Commission  
c/o Mr. Eric Shields, Planning Director  
City of Kirkland  
123 Fifth Avenue  
Kirkland, WA 98033

Mr. Shields:

I am writing to you regarding the TOD project proposed for the South Kirkland Park and Ride site.

As you know, Fuller/Sears is currently involved in several residential and retail mixed-use projects located throughout the greater Puget Sound area.

The purpose of this letter is to explore whether or not the size of retail component of the project has any flexibility, as you are clearly at an advanced stage in writing the development criteria.

A grocery store with whom we work is very interested in the site, but the 7,500 sf size restriction would effectively exclude them from considering locating in the project. They are not prepared to identify themselves just yet, but have authorized me to contact you on their behalf.

You may not be aware, but there are few sites with the capacity to accommodate a full-service grocery store to serve the South Kirkland neighborhoods for which there is currently a void in the marketplace.

In reviewing the public comments, we understand the community concerns regarding "big box" retail. Having said that, there did seem to be some support for locating a TOD nearer to a grocery store, thus negating the need for a drive or bus ride to acquire daily necessities.

Would it be possible to amend the zoning language to keep the size of individual retail stores to a maximum of 7,500 sf, but provide an exemption for a grocery store use?

With 200 to 250 residential units, the inclusion of a true neighborhood grocery store could introduce significant vitality to the project and reduce the need for TOD residents to travel by vehicle to a grocery store.

Additionally, as retail advisors, we are concerned that the current requirement for small shop tenants will likely result in slow lease up and tenants that are ultimately not "first choice".

If you, the Houghton Community Council or the Kirkland Planning thinks this proposal might be viewed favorably, we would be happy to discuss in further detail.

A handwritten signature in black ink, appearing to read "William A. Fuller". The signature is fluid and cursive, with a long horizontal stroke at the end.

William A. Fuller, AIA, NCARB, LEED-AP  
Principal

**Dorian Collins**

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**From:** Paul Stewart  
**Sent:** Tuesday, March 29, 2011 8:53 AM  
**To:** 'Peter Wilson'  
**Cc:** Joan McBride; Dorian Collins  
**Subject:** RE: Support for South Kirkland Park & Ride TOD

Thank you for your comments Peter. We will transmit this to both the Houghton Community Council and Planning Commission.  
Paul Stewart

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**From:** Peter Wilson [<mailto:peterwilson@gmail.com>]  
**Sent:** Monday, March 28, 2011 5:44 PM  
**To:** Paul Stewart  
**Cc:** Joan McBride  
**Subject:** Support for South Kirkland Park & Ride TOD

Hi Paul,

I am a resident of Lakeview, represented by both the Kirkland City Council and the Houghton Community Council. My address is:

10127 NE 66th Lane,  
Kirkland, WA 98033

(Note: none of the HCC members have their email addresses on the web - please will you forward this email to Rick Whitney.)

I am writing to you today to express my strong support for the South Kirkland Park & Ride TOD project. This is the sort of innovative project we should be developing in Kirkland that both supports and enables our growing city's community and protects rural lands by building density. It is great that Kirkland is planning this project.

I had the opportunity to attend Thursday's joint meeting of the Kirkland Planning Commission and the Houghton Community Council and I was pleased to see the high-level of support from those in attendance. I hope both groups can move forward with this project, while avoiding the temptation to also do the job of the Design Review Board.

Please let me know if you have any questions.

Thanks,

-- Pete

--

Peter Wilson  
c: 425.985.0194

**Dorian Collins**

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**From:** Paul Stewart  
**Sent:** Monday, March 28, 2011 8:54 AM  
**To:** 'Iggycdog@aol.com'; Janet Jonson  
**Cc:** Dorian Collins  
**Subject:** RE: low income housing in Kirkland

Gerri,

Thank you for your comment. We will pass it on to the Planning Commission and Houghton Community Council.

Paul Stewart

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**From:** [Iggycdog@aol.com](mailto:Iggycdog@aol.com) [<mailto:Iggycdog@aol.com>]

**Sent:** Friday, March 25, 2011 10:35 AM

**To:** Paul Stewart; Janet Jonson

**Subject:** low income housing in Kirkland

Hello - Just wanting to voice my support for the addition of mixed-use housing in Kirkland. If we could find a spot in north Kirkland, that would also be great - thanks, Gerri Haynes, 104 - 7th Ave, 98033

**Dorian Collins**

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**From:** Paul Stewart  
**Sent:** Friday, April 15, 2011 3:48 PM  
**To:** 'Dan Krehbiel'  
**Cc:** Dorian Collins  
**Subject:** RE: South Kirkland Park and Ride Transit Oriented Development

Dan,  
Thank you for your comments. We will pass these on.  
Paul Stewart  
425-587-3227

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**From:** Dan Krehbiel [<mailto:dan.krehbiel@gmail.com>]  
**Sent:** Friday, April 15, 2011 3:41 PM  
**To:** Paul Stewart  
**Subject:** South Kirkland Park and Ride Transit Oriented Development

Dear Mr. Stewart,

As a Kirkland resident **I support the South Kirkland Park and Ride Transit Oriented project**. I believe that there will be host of positive outcomes if/when the TOD is approved. For me those include superlative design for the whole project; 200 units of housing (affordable to market rate) with their own parking; 200 to 250 MORE parking stalls for park and ride users (so important once the 520 tolling begins since the Park and Ride is already appears at capacity); 1% for the arts; first floor retail; traffic mitigation; superior landscaping; and a charging station for electric cars and perhaps a zip car.

Thank you for your consideration of this important project that combines many of Kirkland's core issues like economic development, workforce housing, green house emissions, affordable housing and housing choice, and transit supportive development.

May I also please request that you pass all these comments to both the Kirkland Planning Commission and the Houghton Community Council.

Sincerely,

Dan Krehbiel  
206.349.7622

**Dorian Collins**

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**From:** Paul Stewart  
**Sent:** Thursday, April 14, 2011 4:06 PM  
**To:** 'Waluconis, Carl J.'  
**Cc:** Dorian Collins; Andrew Held; Byron Katsuyama; C. Ray Allshouse - Home; C. Ray Allshouse - Work; George Pressley; Glenn Peterson; Jay Arnold; Jon Pascal; Jon Pascal; Karen Tennyson; Mike Miller; Betsy Pringle; Bill Goggins ; Elsie Weber; John Kappler; Kathleen McMonigal; Lora Hein; Rick Whitney  
**Subject:** RE: TOD zoning regulations

Hi Carl,

Thank you for your comment on the proposed TOD. We will provide this to the Planning Commission and City Council.  
Paul Stewart

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**From:** Waluconis, Carl J. [<mailto:cwaluc@sccd.ctc.edu>]  
**Sent:** Thursday, April 14, 2011 4:00 PM  
**To:** Paul Stewart  
**Subject:** TOD zoning regulations

To the Houghton Community Council and Kirkland Planning Commission,

Please support the zoning regulations for the TOD in Kirkland. I also support extra height in exchange for public amenities such as additional open space. Also to ensure that the design is a splendid addition for Kirkland, I would love to see the project have review through the city's design review board.

I have lived in Lakeview for nearly 28 years and prior to that I lived in central Houghton. I think this project is important not just to my neighborhood but to Kirkland as a whole.

Thank you,

Carl Waluconis  
6536 102<sup>nd</sup> Ave

C

**Dorian Collins**

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**From:** Paul Stewart  
**Sent:** Thursday, April 14, 2011 10:21 AM  
**To:** 'sjohnson119@comcast.net'  
**Cc:** Dorian Collins  
**Subject:** RE: Development of S Kirkland Park&Ride

Hi Sarah,  
Thank you for your comment. We will pass it on to the Planning Commission and Houghton Community Council.  
Paul

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**From:** [sjohnson119@comcast.net](mailto:sjohnson119@comcast.net) [<mailto:sjohnson119@comcast.net>]  
**Sent:** Thursday, April 14, 2011 10:10 AM  
**To:** Paul Stewart  
**Subject:** re:Development of S Kirkland Park&Ride

I would like the Kirkland Planning Commission and the Houghton Community Council to know that I support the present proposals for transit-oriented development at the South Kirkland park and Ride. I support the zoning regulations necessary for the project to go forward.

Most important, I am in favor of the inclusion of AFFORDABLE housing in the development.

Thank you.

Sarah Johnson  
703 4th Ave Apt 105  
Kirkland WA 98033

April 11, 2011

Dear Houghton Community Council Members,

I have cut and pasted information from the TOD website that describes the other TOD projects in the greater Seattle area. I would like you to make note of the access to Sound Transit that exists at many of the sites as well as the urban location with shops and entertainment close by. Please consider the location of the proposed TOD in the Lakeview neighborhood in comparison to these other TOD locations and their proximity to transit centers that include Sound Transit rail or bus lines. One of the keywords pay attention to is 'Downtown'.

"The Northgate Transit-Oriented Development concept ultimately selected for this location will preserve the site as a vital public transportation hub in north Seattle. The site will continue to be an important transfer point and could accommodate such additional services as light rail and monorail stations. Weekday bus trips through the Northgate Transit Center now total 785, with more than 7,000 passenger boardings each day. By 2020, Northgate light-rail ridership is forecast to be 10,000 boardings per day, with 75 percent of those riders arriving and departing by bus.

Integration of the bus transit center and light rail and monorail stations at Northgate with high-density, mixed-use urban development on the "super block" south of the mall will increase transit ridership and help the City of Seattle achieve its growth management targets.

The Overlake Park-and-Ride TOD

The development is in the heart of the Overlake commercial area of Redmond near 152nd Avenue NE and NE 24th Street. Overlake is a major employment center with about 600 firms, including Microsoft's main campus, and 22,600 employees. Grocery stores, restaurants, personal services and major retailers are within a short walking distance.

The City of Redmond's Downtown Transportation Master Plan for public transportation investments is designed to help facilitate full development of the downtown urban center. Key to this concept is a TOD design district that will provide regulatory guidelines and implementation strategies appropriate for land uses that support transit. The community's vision for downtown embraces a mix of residential, employment, retail, and recreational opportunities. The future of downtown Redmond is envisioned as an urban neighborhood where people can live and work, and where automobile use is an option, not a requirement.

## Metropolitan Place, Renton TOD

As part of the 30-year agreement to lease park-and-ride stalls to King County Metro Transit, the developer supplies one free Metro bus pass for every apartment unit. The agreement also stipulates that the units be affordable to a mix of incomes. Besides being located next to the transit center, Metropolitan Place is close to a new urban park, retail stores, theater, schools, and restaurants, all in downtown Renton. The building occupies the site of the old Good Chevrolet building.

### Kent Station private development

The 17-acre Kent Station property in the heart of downtown Kent is in full development mode and will soon be opening in phases, beginning with a major multi-screen cinema.

### Kent Municipal Parking Lot

Just across W Smith Street to the south of the Kent Station redevelopment lies the four-acre Kent Municipal Parking Lot. The city is actively interested in redevelopment of this surface lot as a link from the historic downtown area that lies to the south to the newly developing Kent Station to the north. In addition to preserving adequate parking for continuing uses, opportunities exist for mixed-use redevelopment of housing, retail, and office space to serve the area's needs, including those of the adjacent Regional Justice Center.

### Burien Transit Center project

Placing the Burien Transit Center's park-and-ride stalls in a multi-level garage will make the remaining half of the lot available for housing and commercial development. Future residents and employees at this location will enjoy easy access to many downtown Burien amenities and to regional transit connections via the adjacent transit center. This TOD or transit-oriented development project is designed to reduce auto usage, increase transit usage, and provide housing and employment density in areas designated for and encouraging growth, such as downtown Burien."

Here are also the bus changes that relate to the South Kirkland Park and Ride. The meeting about these changes is on Mercer Island on Tuesday. [http://metro.kingcounty.gov/up/projects/pdf/updated/a2011EN\\_235\\_226\\_234\\_249\\_255\\_256.pdf](http://metro.kingcounty.gov/up/projects/pdf/updated/a2011EN_235_226_234_249_255_256.pdf)

The Executive's proposal will be heard by the County Council at a public hearing on Tuesday, April 12 from 6:00 to 8:00 p.m. at the Mercer Island Community Center, 8236 SE 24th St., Mercer Island.

On the Eastside, the proposal calls for:

#### **New Metro routes:**

RapidRide B Line, 226, 235 and 241

#### **Routes with added service:**

212, 255, 271

**Routing changes:**

221, 234, 240, 245, 246, 249, 250, and 265

**Routes proposed for elimination since current service will be offered by other routes:**

222, 225, 229, 230, 233, 247, 253, 256, 261, 266, 272, and 926

Sincerely,

Margaret Bull

**April 11, 2011**

**Metro Community Relations,**

***One of my main concerns about the Metro routes throughout Kirkland is that the individuals that need bus service the most, those that don't drive due to age, disability, or financial limitations, are disadvantaged by the fact that the buses run less frequently during the evening and midday hours. Some of us also share a car with a family member and must frequently use the bus to get around. My family lives off of 108<sup>th</sup> Ave NE in Kirkland and we would be dependent on the 255 schedule to go anywhere (since you are discontinuing that portion of the 234 route) and then rely on convenient transfer times at South Kirkland Park and Ride, Houghton Park and Ride, or Downtown Kirkland Transit Center. If I am going to a medical appointment or shopping during the day, or want to visit relatives, go to a public meeting, or an entertainment venue in the evening, then the fact that some buses only run every 30 to 60 minutes could mean that I will have long waits to get to wherever I need to go. I know that many people do not feel comfortable standing alone at a bus stop at night for over 30 minutes at a transfer point. Even though the 255 will run more frequently, it doesn't help very much if you don't get to the transit center or park and ride in time to catch a bus that only runs once an hour.***

***This limitation in evening service is one of the main reason I believe that a TOD plan that encourages total dependence on bus transportation in order to curtail car ownership at the South Kirkland Park and Ride lot property will not be successful. At the TOD public meetings that I have attended several people have mentioned that residents will choose to not own cars because it will be too expensive to pay for gas. It doesn't seem to me that it will be possible to meet this goal because of the lack of urban amenities at this particular location. I do not believe the area meets the criteria laid out in the TOD guidelines.***

To reduce external trips, TOD projects should be located in higher-density, mixed-use, urban pedestrian districts with high-quality transit service. External single-occupancy vehicle trips can be reduced as much or more by people walking within a mixed-use urban district as they can by using transit within and between urban centers.

To be most effective, TOD should be "urban" even in a suburban setting. Pedestrian-scale design draws people to return repeatedly. Urban development supports transit; suburban development does not.

*There are several roadblocks to urban development of this area in the future: it is hemmed in by the freeway and an important wetland; surround by an area that is already developed with apartment complexes, light industry and office parks; sidewalks and bike lanes are nonexistent on some of the surrounding streets; the hilly nature of the area discourages pedestrian traffic, especially those using a wheelchair or a stroller; on nearby streets no on-street parking is allowed. There are no guarantees that there will ever be a bike/pedestrian trail or rail line in the next 15 years. The freeway exits and onramps make the area especially hazardous to those traveling south on foot or by bicycle toward Bellevue. I know this for a fact because one of my good friends was seriously injured crossing the street at a traffic light with a pedestrian crosswalk by a motorist turning left after coming off of the freeway ramp onto 108<sup>th</sup> Ave NE. With no bus service along 112<sup>th</sup>, a parent living at a TOD at South Kirkland Park and Ride would have to walk their preschooler past the 520 freeway entrances in order to take them to the Bellevue Montessori School. It would be worthwhile for those planning the TOD to walk into Bellevue from South Kirkland Park and Ride lot on 108<sup>th</sup> Ave NE/112<sup>th</sup> Ave NE and walk back along Bellevue Way/Lakeview Drive NE. It would be especially useful to try to walk the same route at night in the rain. The revised 249 Metro bus route only runs every 30 minutes and stops after 8:00 p.m. Even if a bus route is available, many people would prefer to walk a mile than spend the money on bus fare. The nearest grocery store with a pharmacy is south of the park and ride lot on the other side of the 520 freeway. There are very few services along Northrup, 112<sup>th</sup> Ave NE, 108<sup>th</sup> Ave NE, or Lakeview Drive that pedestrians have easy access to. I personally would find it very difficult to live there without access to a car.*

**Margaret Bull**  
**6225 108<sup>th</sup> Place NE**  
**Kirkland WA 98033**



**King County**

**Community Services Division**

Housing and Community Development

Department of Community and Human Services

401 5<sup>th</sup> Avenue, Suite 510

Seattle, WA 98104

**206-263-9033** FAX: 206-296-0229

TTY Relay: 711

April 7, 2010

Paul Stewart, Deputy Director  
Dorian Collins, Senior Planner  
City of Kirkland Planning Department

Dear Paul and Dorian,

At yesterday's informational meeting about the South Kirkland Park and Ride that we held at King County, the question of LEED Silver certification of the project came up. Several of the participants, who included non-profit affordable housing agencies as well as private developers, mentioned the high cost of going through the official LEED certification process. They appeared quite willing to use a LEED Silver checklist and/or to meet the Washington State Evergreen Standards, but objected to being required to pay the various fees and costs associated with the official certification, which they feel adds no real "green" value.

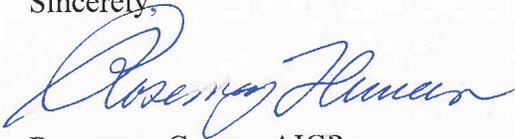
The King County Housing and Community Development Program has heard this concern a number of times before. I want to stress that we are highly committed to green and sustainable building projects. We require the Evergreen Standard as a minimum, and encourage projects to achieve a higher sustainability standard than that. However, if it is the case, as we have been repeatedly told, that getting the official certification for LEED is unduly burdensome, we would encourage Kirkland to write their sustainability guidelines in a way that allows for some flexibility.

I believe some cities (e.g. Bellingham) use the phrase "projects must meet a minimum of LEED Silver Certification OR EQUIVALENT". In other cases, jurisdictions use the Evergreen Standards as a minimum, and encourage higher levels of sustainability insofar as possible. It might be helpful to define the term "equivalent" in a way that requires the project to demonstrate, and *have independently verified*, that they have met the LEED Silver checklist requirements, without having to go through the formal application and certification process.

Paul Stewart  
April 7, 2011  
Page 2 of 2

King County Housing and Community Development strongly believes that this would be a preferable alternative to requiring official LEED silver certification for this project, or other projects in the City of Kirkland.

Sincerely,

A handwritten signature in blue ink, appearing to read "Rosemary Curran".

Rosemary Curran, AICP  
Affordable Housing Planning Coordinator

Cheryl Markham, Manager  
King County Housing and Community Development Program

cc: Gary Prince, King County Metro TOD  
David Blum, King County Metro Special Projects Manager  
Arthur Sullivan, ARCH

## Dorian Collins

---

**From:** S. Etchevers [setchev@comcast.net]  
**Sent:** Wednesday, May 04, 2011 11:40 AM  
**To:** Dorian Collins  
**Cc:** Angela Ruggeri  
**Subject:** Objections to TOD at the So. Kirkland P&R

Hi Dorian,

Could you please add to your packet related to the TOD project *this* comment, as well as an earlier written testimony (included below) that I left in the Council Chamber on April 25?

*Aside* from the well documented worsening traffic and parking problems in southern Houghton and Lakeview. My own personal overall urban concern about a So. Kirkland TOD is this. Adding density *outside* the already zoned areas for such purpose will dilute and spread density in Kirkland around the city. It will also slow down densification in already defined core areas. It will also take the focus of the City away from making those core areas really attractive for people to: 1. Live there, and 2. Visit those areas. Additionally, City efforts to carry out events that attract people from the region will continue to happen in the core areas that are less dense than they otherwise could/should be.

Imagine for a moment that Downtown Kirkland, Juanita, Rose Hill and Totem Lake were chosen by experienced Urban Architects as having great potential to be transformed into attractive dense urban areas. (I'm not sure about Totem Lk). Then, wouldn't the City want to encourage new dwelling construction in those areas - as much and as soon as possible within a context of a well thought-out plan - so that those areas became truly attractive?? In time, once the new development in those areas achieved critical mass, the city could start experimenting with van/small bus transportation services *between them and* downtown Kirkland. At that time, the City should also provide ample P&R facilities near the freeway. And, if the transportation service were good (comfortable, frequent and reliable), people would use it - especially if parking downtown were expensive or difficult to find. Then, imagine what Kirkland (Houghton and Lakeview) could do with the streets. Initially, close them to traffic (partially or completely), organize as many events as possible throughout the year, encouraging pedestrian and bike movement. I am not suggesting that this is easy to do, but there are experts who know how to do it. Looking into the future, something like this will HAVE to happen, or we'll be choked with traffic.

The area around the So. Kirkland P&R is an appropriate business park for Kirkland. It is at the entrance of the city, right off the freeway. The City should facilitate and encourage people to *drop* the car THERE, as well as in the Tot. Lk., Rose Hill and Houghton P&Rs, rather than making it easy for people to drive through the city and downtown.

All that would be in stark contrast to random development wherever there is an empty lot or a builder's project. The constant accumulation of those tiny little projects will only lead to a perpetuation of the current urban paradigm. One cannot play chess by moving pieces in a random fashion and expect to win. Nor can one do the same thing over and over and expect a different result. There are plenty of concrete examples of standard and successful urban development. The TOD at the So. Kirk. P&R may be good for Seattle and Bellevue, but it will do more bad than good for Kirkland.

Shawn Etchevers  
4119 107th PL NE  
Kirkland, WA 98033

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Written Testimony, April 25, 2011

Public testimony is a good thing. But, it can also be used as a fig leaf to hide a predetermined decision and manipulate public perception. One way of doing this is to value the same way the opinions of Local people as well as to individuals with personal and financial interests, and public employees involved in this project. Private interests can tilt a “perception game” in their favor. Especially, when written-and-private testimony given by local people is not televised. They can also be easily ignored and swept under the rug by those with a different point of view. .

As you know, BOTH the Houghton and Lakeview Community Councils strongly opposed the TOD project during the long process of updating their respective Neighborhood Plans that started more than a year ago. So far, you and the City Council have chosen to ignore that choice. The recent participation of the Deputy Mayor’s husband, Mr. Springer, in a stealthy effort to pass a law that would abolish the Houghton Community Council, further points in that direction.

During last month’s joint Kirkland Planning Commission and Houghton Community Council meeting, only about 1/3 of the people that gave testimony were from Houghton. 2/3 did not live in the area AND had a personal interest in this project. Among them were King County transportation employees, builders, businessmen, and people from the Cascade Land Conservancy. Not surprisingly, most of the LOCAL people were against the project, and all of the NON-local people were for the project. An experienced business individual, w/o personal interest in this project, gave a strong and substantive warning about the validity of the assumptions regarding the viability of any meaningful commerce within this project. Later in the testimony, the concerns of a person on the Bellevue side of the street were summarily dismissed as irrelevant to the interest of Kirkland’s TOD project. That was another clear indication that the priority of this project is NOT how the entire area around the TOD will work or whether it makes urban and strategic sense for Kirkland. The City Council made that decision long ago. The job now seems to be to grind through the bureaucratic process & get it done.

Unfortunately, building a great city requires good and experienced urban architects, strong community involvement, long-term planning, and careful execution over a long time-span. Densification needs to be encouraged in core areas already zoned for it. That requires extraordinary focus on how to make those core areas attractive, spacious, people friendly, as well as providing ample retail/business services. **The So. Kirk. P&R TOD cannot provide that, is not zoned for that purpose, and Bellevue - owner of half of the area - will not participate in the project. How can this project be a winner?**

One of the worst parts of this project, in my view, is that it will not meaningfully increase the number of usable parking spots for bus riders. At the same time, it will kill any hope that someday the P&R will be turned into a major location for visitors to park their car when entering Kirkland, and avail themselves of a good, frequent, and reliable transportation service to and from core areas in Houghton, downtown, and along Lk. WA Blvd. A good “small-bus” service around this loop would change the car-oriented paradigm in Kirkland. It would reduce traffic, noise, pollution, and encourage more non-motorized transportation, as well as more pedestrian movement in core commercial and recreational areas. - Bad moves now will make any effort to improve livability in Kirkland more costly and difficult. Please stop this project and focus on Kirkland’s long term livability factor!

Shawn Etchevers  
4119 107<sup>th</sup> PL NE  
Kirkland, WA 98033

**Dorian Collins**

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**From:** Paul Stewart  
**Sent:** Thursday, April 28, 2011 9:30 AM  
**To:** 'Naomi Lombard'  
**Cc:** Dorian Collins  
**Subject:** RE: Kirkland TOD

Naomi,

Thank you for your comment. The HCC recommended approval at their April 25<sup>th</sup> meeting. It now goes to the City Council for action and then back to the HCC. We will pass your letter on to the HCC and City Council.

Paul

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**From:** Naomi Lombard [<mailto:naomi.lombard@gmail.com>]  
**Sent:** Tuesday, April 26, 2011 5:31 PM  
**To:** Paul Stewart  
**Subject:** Kirkland TOD

To the Houghton City Council members,

I don't know if this letter is too late for consideration ( I had sent one earlier also), but if not I would like to build on my previous points in support of the South Kirkland TOD.

This is by far a solid investment for Houghton. Concentration of habitable space near to city cores, especially if close to mass transit in any and all forms is not only smart but important if we are to ever get out of our cars.

Proposed zoning regulations should be supported by the council. If that means increasing the height limitations, so that the structure(s) will accommodate both retail space and an ample amount of housing units to make the structure profitable (as well as house the maximum number of low and mid income units), then the decision should be obvious, with all due respect. Houghton/Kirkland benefits from a wider tax base and increased likelihood of new small businesses, added employment and diversity in its citizenship. The plan for the Kirkland TOD adjacent to the So. Kirkland Park and Ride is brilliantly conceived. I only hope it will be brilliantly executed.

I urge the Houghton City Council to utilize the Kirkland Design Review board with architects suited to this type of development. I note the gorgeous developments of this kind in Redmond and further north in our sister city, Vancouver, BC. A well designed apartment/condo complex should be visibly pleasing and will enliven that area, which is by and large commercial, improving its people friendliness. I have confidence that the KDR Board will ensure that this development will enhance Houghton and North Bellevue neighborhoods - assist in creating a space both street pleasing and functional in increasing the needed parking requirements. I urge the Houghton Council to make use of that resource.

Thank you for your consideration.

Naomi Lombard  
10917 NE 66th Place  
Kirkland - a Houghton resident for 27 years, and loving it.

**Dorian Collins**

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**From:** S. Etchevers [setchev@comcast.net]  
**Sent:** Monday, April 25, 2011 9:57 PM  
**To:** Dorian Collins  
**Subject:** TOD input

Hi Dorian,

Public testimony is a good thing. But, it can also be used as a fig leaf to hide a predetermined decision and manipulate public perception. One way of doing this is to value the same way the opinions of Local people as well as to individuals with personal and financial interests, and public employees involved in this project. Private interests can tilt a "perception game" in their favor. Especially, when written-and-private testimony given by local people is not televised. They can also be easily ignored and swept under the rug by those with a different point of view. .

As you know, BOTH the Houghton and Lakeview Community Councils strongly opposed the TOD project during the long process of updating their respective Neighborhood Plans that started more than a year ago. So far, you and the City Council have chosen to ignore that choice. The recent participation of the Deputy Mayor's husband, Mr. Springer, in a stealthy effort to pass a law that would abolish the Houghton Community Council, further points in that direction.

During last month's joint Kirkland Planning Commission and Houghton Community Council meeting, only about 1/3 of the people that gave testimony were from Houghton. 2/3 did not live in the area AND had a personal interest in this project. Among them were King County transportation employees, builders, businessmen, and people from the Cascade Land Conservancy. Not surprisingly, most of the LOCAL people were against the project, and all of the NON-local people were for the project. An experienced business individual, w/o personal interest in this project, gave a strong and substantive warning about the validity of the assumptions regarding the viability of any meaningful commerce within this project. Later in the testimony, the concerns of a person on the Bellevue side of the street were summarily dismissed as irrelevant to the interest of Kirkland's TOD project. That was another clear indication that the priority of this project is NOT how the entire area around the TOD will work or whether it makes urban and strategic sense for Kirkland. The City Council made that decision long ago. The job now seems to be to grind through the bureaucratic process & get it done.

Unfortunately, building a great city requires good and experienced urban architects, strong community involvement, long-term planning, and careful execution over a long time-span. Densification needs to be encouraged in core areas already zoned for it. That requires extraordinary focus on how to make those core areas attractive, spacious, people friendly, as well as providing ample retail/business services. **The So. Kirk. P&R TOD cannot provide that, is not zoned for that purpose, and Bellevue - owner of half of the area - will not participate in the project. How can this project be a winner?**

One of the worst parts of this project, in my view, is that it will not meaningfully increase the number of usable parking spots for bus riders. At the same time, it will kill any hope that some day the P&R will be turned into a major location for visitors to park their car when entering Kirkland, and avail themselves of a good, frequent, and reliable transportation service to and from core areas in Houghton, downtown, and along Lk WA Blvd. A good "small-bus" service around this loop would change the car-oriented paradigm in Kirkland. It would reduce traffic, noise, pollution, and encourage more non-motorized transportation, as well as more pedestrian movement in core commercial and

recreational areas. - Bad moves now will make any effort to improve livability in Kirkland more costly and difficult. Please stop this project and focus on Kirkland's long term livability factor!

Shawn Etchevers  
4119 107<sup>th</sup> PL NE  
Kirkland, WA 98033

**Dorian Collins**

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**From:** Lori Isch [lori.isch@usa.net]  
**Sent:** Friday, April 22, 2011 6:15 PM  
**To:** Dorian Collins  
**Cc:** lori.isch  
**Subject:** Re: South Kirkland Park & Ride - Transit Oriented Development Update

To: Dorian Collins, Paul Stewart, Houghton Community Council

I am writing about the TOD plans at the S Kirkland P&R. I appreciate the time and effort that is being spent to ensure this development will provide value to the community.

I am in favor of the TOD in general, however, I am concerned about two key areas:

- 1) sufficient parking
- 2) timing of the project (and whether sufficient alternate parking will be provided)

I depend on the 255 bus service each day to take me to my job in downtown Seattle. It is an easy drive to the S Kirkland P&R, but if I have to catch a connector bus or find parking further away, it could easily add 1/2 hour to my commute each way.

**for 1) sufficient parking**

I recently attended a community meeting on this topic and was disturbed that within the key goals for the TOD, it was NOT stated: retain and improve the parking service for current/future commuters. The current users seem to be an afterthought, and the consultant that did the traffic analysis was a joke.

The math is very simple:

- the current lot is 700-some units I believe
- the current lot is under-capacity by at least 200 units
- the new 520 tolls expect to add 20-30% additional riders, so another 200 units are needed
- the new TOD will have 200? units, if so, should have 300 parking units
- the new TOD will have retail, which need another 100 units or so

So, overall, you need to provide about **1500 parking spots** (not COMPACT spots, which are a waste).

**for 2) timing of the project (and whether sufficient alternate parking will be provided)**

using the same logic, there should be at least 1100 parking units available by June!!! Just to handle the current load, plus 520-tolling. Where are you going to get the additional parking? Then, where will you have temporary parking during construction?

Metro appears to have no plan to address the additional parking needs. If you can get agreement from neighboring businesses, that would be great. Sometimes I have to park in the lot to the West or the BCC parking lot, but both have signs that threaten towing.

In summary, I have seen many areas of Kirkland (residential and business), where the parking is significantly below what is needed. This affects the business, the customers, and the surrounding properties as people have to squeeze in wherever they can. This becomes a safety issue. Please do not continue to make this mistake! Vehicles are a fact of life - it is fine to encourage public transportation, but it is not your job to make vehicle travel & parking an intentional inconvenience.

Thank you for your consideration.

Lori Isch

10116 NE 64th St  
Kirkland, WA 98033  
425 444 7321

----- Original Message -----

**Received:** 03:18 PM PDT, 04/21/2011

**From:** City of Kirkland <kirkland@service.govdelivery.com>

**To:** lori.isch@usa.net

**Subject:** South Kirkland Park & Ride - Transit Oriented Development Update



You are subscribed to the South Kirkland Park & Ride – Transit Oriented Development Update List-Serv for City of Kirkland.

The Houghton Community Council meeting scheduled for Monday, April 25, 2011 at City Hall has an early start time of 6:00 p.m.

The meeting agenda and packet information can be viewed [here](#). The project website can be viewed [here](#).

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### South Kirkland Park & Ride – Transit Oriented Development Update

For more information contact:

**Dorian Collins - Senior Planner**

City of Kirkland

Phone: (425) 587-3249

email: [DCollins@kirklandwa.gov](mailto:DCollins@kirklandwa.gov)

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**Dorian Collins**

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**From:** Curran, Rosemary [Rosemary.Curran@kingcounty.gov]  
**Sent:** Friday, April 15, 2011 12:56 PM  
**To:** Dorian Collins; Paul Stewart  
**Cc:** 'ASullivan@bellevuewa.gov'; Prince, Gary  
**Subject:** FW: South Kirkand Park and Ride Sustainability Regulation

**Follow Up Flag:** Follow up  
**Flag Status:** Flagged

Another response from a developer regarding the proposed sustainability regulations.

Rose

*Rose Curran*

*Affordable Housing Planner, PPMIII  
King County Housing and Community Development  
401 Fifth Avenue, Suite 510  
Seattle, WA 98104  
[rosemary.curran@kingcounty.gov](mailto:rosemary.curran@kingcounty.gov)  
206-263-9268*

---

**From:** Scott Barkan [<mailto:scottb@beacondevgroup.com>]  
**Sent:** Friday, April 15, 2011 12:41 PM  
**To:** Curran, Rosemary  
**Subject:** RE: South Kirkand Park and Ride Sustainability Regulation - Quick Response needed!

Thanks very much for asking. I'm sorry I couldn't respond yesterday afternoon.

I think it's good that the City would not require the developer get the parking garage LEED Silver certified, but rather just meet the checklist standards. This will save many, although not all, of the costs of meeting this standard.

The affordable housing would likely be able to easily meet the Evergreen Standards. If a development moves forward with both a market rate building and an affordable building, the market rate developer may not be familiar with the Evergreen standards, but might be more familiar with the Built Smart program. I'm not familiar with that program. You might want to contact a market rate developer to confirm the additional costs of certifying a building under that program.

I hope that helps.

SB

Scott Barkan, Housing Developer  
Beacon Development Group  
1221 East Pike Street, Suite 300  
Seattle, WA 98122-3930  
(206) 860-2491 ext. 209 and (fax) 860-2094  
[www.beacondevgroup.com](http://www.beacondevgroup.com)

**Dorian Collins**

---

**From:** Curran, Rosemary [Rosemary.Curran@kingcounty.gov]  
**Sent:** Friday, April 15, 2011 8:39 AM  
**To:** Dorian Collins; Paul Stewart  
**Cc:** 'ASullivan@bellevuewa.gov'  
**Subject:** FW: South Kirkland Park and Ride Sustainability Regulation - Quick Response needed!

Another comment. Sounds like Mark already spoke with Paul about this, but I'm forwarding it as well. Not sure about the question he raises....something we should probably get an opinion on.

Thanks,  
Rose

*Rose Curran*  
*Affordable Housing Planner, PPMH*  
*King County Housing and Community Development*  
*401 Fifth Avenue, Suite 510*  
*Seattle, WA 98104*  
*rosemary.curran@kingcounty.gov*  
*206-263-9268*

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**From:** Mark E Thometz [mailto:metedwa@comcast.net]  
**Sent:** Friday, April 15, 2011 8:00 AM  
**To:** Len Brannen  
**Subject:** RE: South Kirkland Park and Ride Sustainability Regulation - Quick Response needed!

Morning all. I have spoken with Paul Stewart from your offices regarding LEED and other standards. I distributed to Paul the Evergreen Standard developed by the State of Washington. This standard will be required by any component of the project that utilizes State of Washington based financing and is verified by CTED and also an independent construction review by WCRA. The Evergreen Standard is easily applicable to non residential uses also (recycled materials, non toxic materials, minimizing construction waste, etc, etc). In the most recent project that SRI and I developed that had a significant commercial component; the commercial component did conform to the Evergreen Standard. As I discussed with Paul, the Evergreen Standard is quite robust in sustainability (exceeds Guild Green 4 and Green Communities), was developed for the specifics to Washington's environment and has been in place and applied to probably 70 to 100 projects since it's inception around 2007.

As for the costs associated with the Evergreen Standard, they are quite manageable and most prudent develops incorporate such practices (aside of the sustainable location related criteria) as a course of prudent development. I further wonder about the legal issues of utilizing LEED related products such as check lists without incurring the LEED agency related certification fees – to that I do not have an answer. Hope the above is helpful.

Len I did not forward this directly to Rosemary as the original e-mail was to you. You might want to forward the above to Rosemary. Thanks.

---

Mark T  
206 818-2398  
metedwa@comcast.net

**From:** Len Brannen [mailto:lenb@shelterresourcesinc.com]  
**Sent:** Thursday, April 14, 2011 7:39 PM  
**To:** Curran, Rosemary  
**Subject:** RE: South Kirkand Park and Ride Sustainability Regulation - Quick Response needed!

We would concur with the expressed sentiments regarding LEED Silver versus a verifiable high green standard as is typically accepted by public funding sources in King County and Washington State for affordable housing. The RFP program as depicted in our recent informational meeting and the distributed materials reveals a project that will face tight financial feasibility challenges. It need not be burdened with extra costs and restrictions that will make the challenge that much more difficult. Certainly sustainability is important and must be addressed but let's avoid blanket mandates and migrate to a practical standard that is sensitive to cost considerations.

Len

*Len Brannen, President*  
**Shelter Resources, Inc.**  
2223 112th Avenue NE, Suite 102  
Bellevue, WA 98004  
(425) 454-8205  
(425) 455-8546 - fax  
LenB@ShelterResourcesInc.com

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**From:** Curran, Rosemary [mailto:Rosemary.Curran@kingcounty.gov]  
**Sent:** Thursday, April 14, 2011 3:12 PM  
**To:** 'joeg@JBDG.com'; 'hbeaulieu@rafn.com'; 'randr@commonground.org'; Len Brannen; 'billhall@ccswco.org'; 'scottB@beacondevgroup.com'  
**Subject:** FW: South Kirkand Park and Ride Sustainability Regulation - Quick Response needed!

Dear Housing Professionals,

I'm writing to a small group of you who were at our informational meeting on the S. Kirkland Park and Ride project. Please see note at bottom from Kirkland, forwarded from ARCH. If you can give it a quick read and response before their meeting this evening that would be most helpful! Note that for 10.b, the requirement could be fulfilled with a LEED checklist with independent third-party verification – without the full formal certification process.

King County HCD had weighed in to ask that the full formal LEED Silver process not be required, as long as a verifiable high green standard could be met.

Thanks!

Rose

*Rose Curran*  
*Affordable Housing Planner, PPMIII*  
*King County Housing and Community Development*  
*401 Fifth Avenue, Suite 510*  
*Seattle, WA 98104*  
*rosemary.curran@kingcounty.gov*  
*206-263-9268*

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**From:** ASullivan@bellevuewa.gov [mailto:ASullivan@bellevuewa.gov]  
**Sent:** Thursday, April 14, 2011 12:56 PM  
**To:** Curran, Rosemary  
**Cc:** Prince, Gary  
**Subject:** FW: South Kirkand Park and Ride Sustainability Regulation  
**Importance:** High

Can you check with some of the developers to see if this works.

It would be great to hear back by the end of today one way or the other (they are meeting tonight), or at the absolute latest, tomorrow.

Thanx,  
Arthur

---

**From:** Dorian Collins [mailto:DCollins@kirklandwa.gov]  
**Sent:** Thursday, April 14, 2011 10:07 AM  
**To:** Prince, Gary; Sullivan, Arthur  
**Subject:** South Kirkland Park and Ride Sustainability Regulation

Hi Gary and Arthur. We are planning to propose some changes to the sustainability regulation, in response to the comments from the affordable housing community at that meeting last week. Here's the draft text at this point:

10. Development should be designed, built and certified to achieve or exceed the following green building standards:
- a. Evergreen Standard or Built Green 4 star certified for all housing units.
  - b. For the parking garage and non-residential uses, either a LEED Silver CS (Core and Shell) certified or LEED CS checklist with a third party independent verification and inspection to meet the LEED CS Silver Standard.

Please let me know if you have any comments or concerns. See you tonight –

Dorian

Note: My new email address is DCollins@kirklandwa.gov and you can now find the City of Kirkland online at [www.kirklandwa.gov](http://www.kirklandwa.gov).

## Dorian Collins

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**From:** Curran, Rosemary [Rosemary.Curran@kingcounty.gov]  
**Sent:** Friday, April 15, 2011 8:33 AM  
**To:** Dorian Collins; Paul Stewart  
**Cc:** 'ASullivan@bellevuewa.gov'  
**Subject:** FW: South Kirkland Park and Ride Sustainability Regulation - Quick Response needed!

Here's the second message of support for the standard you proposed.

Rose

### *Rose Curran*

*Affordable Housing Planner, PPMH  
King County Housing and Community Development  
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206-263-9268*

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**From:** Len Brannen [<mailto:lenb@shelterresourcesinc.com>]  
**Sent:** Thursday, April 14, 2011 7:39 PM  
**To:** Curran, Rosemary  
**Subject:** RE: South Kirkland Park and Ride Sustainability Regulation - Quick Response needed!

We would concur with the expressed sentiments regarding LEED Silver versus a verifiable high green standard as is typically accepted by public funding sources in King County and Washington State for affordable housing. The RFP program as depicted in our recent informational meeting and the distributed materials reveals a project that will face tight financial feasibility challenges. It need not be burdened with extra costs and restrictions that will make the challenge that much more difficult. Certainly sustainability is important and must be addressed but let's avoid blanket mandates and migrate to a practical standard that is sensitive to cost considerations.

Len

*Len Brannen, President  
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[LenB@ShelterResourcesInc.com](mailto:LenB@ShelterResourcesInc.com)*

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**From:** Curran, Rosemary [<mailto:Rosemary.Curran@kingcounty.gov>]  
**Sent:** Thursday, April 14, 2011 3:12 PM  
**To:** 'joeg@JBDG.com'; 'hbeaulieu@rafn.com'; 'randr@commonground.org'; Len Brannen; 'billhall@ccswco.org'; 'scottB@beacondevgroup.com'  
**Subject:** FW: South Kirkland Park and Ride Sustainability Regulation - Quick Response needed!

Dear Housing Professionals,

I'm writing to a small group of you who were at our informational meeting on the S. Kirkland Park and Ride project. Please see note at bottom from Kirkland, forwarded from ARCH. If you can give it a quick read and response before their meeting this evening that would be most helpful! Note that for 10.b, the requirement could be fulfilled with a LEED checklist with independent third-party verification – without the full formal certification process.

King County HCD had weighed in to ask that the full formal LEED Silver process not be required, as long as a verifiable high green standard could be met.

Thanks!

Rose

*Rose Curran*

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**From:** [ASullivan@bellevuewa.gov](mailto:ASullivan@bellevuewa.gov) [<mailto:ASullivan@bellevuewa.gov>]

**Sent:** Thursday, April 14, 2011 12:56 PM

**To:** Curran, Rosemary

**Cc:** Prince, Gary

**Subject:** FW: South Kirkland Park and Ride Sustainability Regulation

**Importance:** High

Can you check with some of the developers to see if this works.

It would be great to hear back by the end of today one way or the other (they are meeting tonight), or at the absolute latest, tomorrow.

Thanx,

Arthur

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**From:** Dorian Collins [<mailto:DCollins@kirklandwa.gov>]

**Sent:** Thursday, April 14, 2011 10:07 AM

**To:** Prince, Gary; Sullivan, Arthur

**Subject:** South Kirkland Park and Ride Sustainability Regulation

Hi Gary and Arthur. We are planning to propose some changes to the sustainability regulation, in response to the comments from the affordable housing community at that meeting last week. Here's the draft text at this point:

10. Development should be designed, built and certified to achieve or exceed the following green building standards:
- a. Evergreen Standard or Built Green 4 star certified for all housing units.
  - b. For the parking garage and non-residential uses, either a LEED Silver CS (Core and Shell) certified or LEED CS checklist with a third party independent verification and inspection to meet the LEED CS Silver Standard.

Please let me know if you have any comments or concerns. See you tonight –

Dorian

Note: My new email address is [DCollins@kirklandwa.gov](mailto:DCollins@kirklandwa.gov) and you can now find the City of Kirkland online at [www.kirklandwa.gov](http://www.kirklandwa.gov).



May 25, 2011

Dear Kirkland City Council,

For Kirkland supports Transit Oriented Development. It is Win-Win-Win! We are a Washington State non-profit organization dedicated to creating a healthy, sustainable city by promoting service to the community, active citizen engagement in city decision making, and a robust, honest dialogue regarding issues facing our community and region.

While other cities are looking at how to change their existing business areas to accommodate transit, Kirkland has an opportunity to plan for a transit-oriented community in an integrated way with business development in the proposed Yarrow Bay Business District. The first step is with the Transit Oriented Development at the South Kirkland Park & Ride.

After lengthy deliberation and much involvement within the community, the City Council will be looking at a proposal in June to change zoning to enable this TOD at the South Kirkland Park and Ride. The zoning changes would allow for housing, but also put in strong standards about how the site will develop.

For Kirkland supports the TOD and believes that these changes are a win-win-win for Kirkland.

The South Kirkland Park and Ride is over capacity on a daily basis. Commuters who have not arrived by 9:30 AM overflow into the neighborhoods and park their cars there. This will only get worse as tolling on the SR520 Bridge begins later this summer and when more Kirkland residents explore riding transit.

King County—which owns this Park & Ride—received a \$6.25 million federal grant to increase parking capacity at the Park & Ride and wants to build a transit-oriented development. This mixed use development would provide commercial space and housing, and will add 250 new spaces to the Park & Ride in addition to the needs of new residents and businesses.

The County will include affordable housing in addition to market rate housing in the TOD. In addition to expanding the affordable housing in Kirkland, convenient access to transit would allow the residents to be less dependent on cars. Recent service changes have increased bus frequency to about 10 minutes.



This TOD will be an anchor for the proposed Yarrow Bay Business District. Plans could enable redevelopment of the southern gateway of Kirkland pedestrian-friendly area with new businesses, residents, and neighborhood services.

After a lengthy review period and much participation within the community, recommendations by the Planning Commission and Houghton Community Council will address community concerns. The Design Review Board would approve the design of the TOD, provide for appropriate scale and modulation, and ensure safe, friendly pedestrian connections to the Park and Ride, and in the future to the BNSF corridor.

This kind of development—and more importantly—this kind of partnership and creativity is exactly what Kirkland needs.

Thank you for your consideration of our position.

For Kirkland

A handwritten signature in blue ink, appearing to read 'DKrehbiel', is written over a light blue horizontal line.

Dan Krehbiel, President

[www.forkirkland.com](http://www.forkirkland.com)

## South Kirkland Park & Ride TOD Design Guideline Matrix

Existing Comprehensive Plan Policies	Proposed Design Guidelines <sup>1</sup> - <i>Design Review Board Authority</i>	Proposed Zoning Regulations	Existing Zoning Regulations	Additional Guidelines Needed?	Specific Regulations Needed?
<p>1. <i>Ensure high quality building and design</i></p>	<ul style="list-style-type: none"> <li>• Building materials should exhibit permanence.</li> <li>• Building materials and color should be selected to integrate with each other and complement architectural design.</li> <li>• Ornament and applied art should be integrated with the structures and the site environment and not haphazardly applied.</li> <li>• Emphasis should be placed on highlighting building features such as doors, windows, and eaves, and on the use of materials such as wood siding and ornamental masonry. Ornament may take the form of traditional or contemporary elements</li> <li>• Original artwork or hand-crafted details should be considered in special areas.</li> </ul>	<ul style="list-style-type: none"> <li>• Require Design Review Board approval</li> <li>• A Master Sign Plan is required for signs on the subject property.</li> </ul>	<ul style="list-style-type: none"> <li>• Design Review Board provisions in KZC Chapter 142</li> <li>• Master Sign Plan provisions in KZC Chapter 100</li> </ul>		

<sup>1</sup> Proposed guidelines may address more than one policy.

Existing Comprehensive Plan Policies	Proposed Design Guidelines <sup>1</sup> - <i>Design Review Board Authority</i>	Proposed Zoning Regulations	Existing Zoning Regulations	Additional Guidelines Needed?	Specific Regulations Needed?
<p>2. Ensure that regulations support appropriate building scale and massing throughout the site, produce buildings that exhibit high quality design and incorporate pedestrian features and amenities that contribute to a livable urban village character for the TOD.</p>	<p><b>Building Scale &amp; Massing</b></p> <ul style="list-style-type: none"> <li>Large window areas should be avoided. Instead smaller window units should be used to achieve human scale.</li> <li>Facing the street, buildings above the 2<sup>nd</sup> story should use upper story step backs to create receding building forms as building height increases to maintain human scale. A rigid stair step or “wedding cake” approach to upper story step backs is not appropriate. <u>(HCC Recommendation – additional text): Prescribed upper story step backs in the gateway area at the intersection of NE 38<sup>th</sup> Place and 108<sup>th</sup> Avenue NE are appropriate to prevent the building from overpowering the gateway design.</u></li> <li>Decks and/or balconies should be designed so that they do not significantly increase the apparent mass of the building.</li> <li>The location of the subject property makes any new multi-story building highly visible from the surrounding streets and properties. Building design should be based on viewpoints or vantages to be identified through the Design Review process. The final arrangement of building mass should therefore address the key vantage points and respond to the context of existing and/or planned improvements, gateway features, and location of</li> </ul>	<ul style="list-style-type: none"> <li>Limit height to 65’ above average building elevation <u>(HCC Recommendation –limit height to 55’ in gateway area and add specific set back requirements of 5 feet above 2<sup>nd</sup> story and 10 feet above 4<sup>th</sup> story in gateway area).</u></li> <li>Require limited types of street level uses which include retail and restaurant uses</li> <li>Allow for decorative parapets and peaked roofs to extend above the height limit</li> <li>Create new Plate 34L which shows pedestrian connections in the YBD and future connection to Eastside Rail Corridor</li> </ul>	<ul style="list-style-type: none"> <li>Various provisions in KZC Section 105.18 – Pedestrian Access <ul style="list-style-type: none"> <li>Pedestrian access from buildings to sidewalks and transit facilities</li> <li>Pedestrian access between uses on subject property</li> <li>Pedestrian connections between properties</li> <li>Pedestrian access through parking areas</li> <li>Pedestrian access through parking garages</li> <li>Overhead weather protection</li> </ul> </li> <li>Various provisions in KZC 110.19 – Public Pedestrian Walkways</li> <li>KZC 105.32 – Bicycle Parking <ul style="list-style-type: none"> <li>Ratio of 1 bicycle space for each 12 required motor vehicle spaces. Planning official may modify this requirement based on development size and anticipated pedestrian and bicycle activity.</li> <li>Contains requirements for bike racks or enclosed storage container locations.</li> </ul> </li> <li>115.142 Transit Shelters and Centers, Public.</li> </ul>		

Existing Comprehensive Plan Policies	Proposed Design Guidelines <sup>1</sup> - <i>Design Review Board Authority</i>	Proposed Zoning Regulations	Existing Zoning Regulations	Additional Guidelines Needed?	Specific Regulations Needed?
	<p>plazas and open space.</p> <ul style="list-style-type: none"> <li>• All building facades should be designed carefully, i.e. there should be no “backside” of a building.</li> <li>• Building facades should be well modulated to avoid blank walls and provide architectural interest.</li> <li>• Landscaping should be used to provide visual interest and help soften building form at appropriate locations, including upper level terraces.</li> <li>• To help moderate the vertical scale of buildings, buildings should incorporate design techniques which clearly define the building’s top, middle, and bottom.</li> </ul> <p>Examples include using a sloped roof and strong eave lines to help define the top; using windows, balconies, and material changes to define a building’s middle; and pedestrian-oriented storefronts, awnings, and use of ‘earth’ materials such as concrete and stone to help define the building’s bottom.</p> <ul style="list-style-type: none"> <li>• Roof forms should be varied and attractive. Where appropriate, roof forms should also help reinforce the modulation or articulation interval of the building façade.</li> <li>• Roof forms should be designed to screen rooftop mechanical units</li> <li>• A predominantly flat roof design is</li> </ul>		<p>Public transit shelters and centers are allowed in all zones and shall not exceed 15 feet above average building elevation in low density zones. The public transit shelters and centers must not unreasonably impede pedestrian movement or create traffic safety problems. Transit route and information signs and markers may be installed. One hundred percent lot coverage is allowed. There are no specific requirements for review process, minimum lot size, minimum required yards, landscaping, or parking for this use.</p>		

Existing Comprehensive Plan Policies	Proposed Design Guidelines <sup>1</sup> - <i>Design Review Board Authority</i>	Proposed Zoning Regulations	Existing Zoning Regulations	Additional Guidelines Needed?	Specific Regulations Needed?
	<p>discouraged. For portions of the building where a flat roof design is used, architectural details such as eaves, cornices, or other articulation elements should be used to provide interest at the ground level.</p> <ul style="list-style-type: none"> <li>Vertical building modulation should be used to add variety by avoiding monotonous design. A technique that may be used is to make large buildings appear to be an aggregation of smaller buildings. Different colors and/or materials may be used to help differentiate between façade planes.</li> <li>Horizontal building modulation should be used to reduce the perceived mass of a building and to provide continuity at the ground level of large building complexes. Building design should incorporate strong pedestrian-oriented elements at the ground level and distinctive roof treatments. Different colors and/or materials maybe used to help differentiate between façade planes.</li> </ul> <p><b><i>High Quality Design</i></b></p> <p>See Policy #1</p> <p><b><i>Pedestrian Features &amp; Amenities</i></b></p> <ul style="list-style-type: none"> <li>Pedestrian walkways should be placed throughout the site to allow for efficient access between the residential, commercial, transit center uses, and adjacent streets. The walkways should be situated to minimize walking distance from the</li> </ul>				

Existing Comprehensive Plan Policies	Proposed Design Guidelines <sup>1</sup> - <i>Design Review Board Authority</i>	Proposed Zoning Regulations	Existing Zoning Regulations	Additional Guidelines Needed?	Specific Regulations Needed?
	<p>public sidewalk and transit facilities to building entrances.</p> <ul style="list-style-type: none"> <li>• Pedestrian and bicycle pathways and/or connections should be well-defined and safe.</li> <li>• Pedestrian connections should be provided to adjacent properties to allow for efficient access to the transit facilities and commercial uses.</li> <li>• Landscaping should be used to help define and provide visual interest along pedestrian walkways.</li> <li>• Convenient and safe pedestrian areas should be designed in centralized locations to accommodate transit users.</li> <li>• Lighting should be provided to walkways and sidewalks through building mounted light and canopy or awning mounted lights.</li> <li>• Low level lighting in the form of bollards or similar style of lighting should be encouraged along pedestrian pathways not adjacent to buildings.</li> <li>• Through-block pathways should be designed so that it is clear that access by the general public is allowed. The following guidelines also apply: <ul style="list-style-type: none"> <li>○ Because the subject property is steep along NE 38<sup>th</sup> Place, stairways may be used in the design of the through-block</li> </ul> </li> </ul>				

Existing Comprehensive Plan Policies	Proposed Design Guidelines <sup>1</sup> - Design Review Board Authority	Proposed Zoning Regulations	Existing Zoning Regulations	Additional Guidelines Needed?	Specific Regulations Needed?
	<p>pathway where connecting to the street. If located along NE 38<sup>th</sup> Place, the stairway should function as a focal entry/exit point and contain design elements that make it a welcoming, safe, and attractive entry.</p> <ul style="list-style-type: none"> <li>○ <u>(PC Recommendation – additional text): If located within a parking lot, the following guidelines should be incorporated into the design of the through-block pathway.</u> <ul style="list-style-type: none"> <li>▪ <u>Increased landscaped island size adjoining the pathway. This helps to narrow the driveway width where appropriate to help slow parking lot traffic.</u></li> <li>▪ <u>Raised landscape beds</u></li> <li>▪ <u>Raised pathway with pavement material, texture, and color different from traffic lanes</u></li> <li>▪ <u>Selection of tree species that provide the broadest canopy possible to produce a dense landscaped environment.</u></li> </ul> </li> <li>○ If the through-block pathway is located between buildings, appropriate plants and trees should be selected based on solar access and the location of proposed improvements.</li> </ul>				
<p>3. Provide guidance for the streetscapes along NE 38th Place and 108th Avenue NE to ensure buildings do not turn their backs on the streets and development</p>	<p><b>Streetscape</b></p> <ul style="list-style-type: none"> <li>• Street trees species should be selected and spaced to allow for visual continuity along NE 38th</li> </ul>	<ul style="list-style-type: none"> <li>• Identify NE 38<sup>th</sup> Place as a Major Pedestrian Sidewalk area</li> </ul>	<ul style="list-style-type: none"> <li>• 110.52 - Sidewalks and Other Public Improvements in Design Districts</li> </ul>		

Existing Comprehensive Plan Policies	Proposed Design Guidelines <sup>1</sup> - <i>Design Review Board Authority</i>	Proposed Zoning Regulations	Existing Zoning Regulations	Additional Guidelines Needed?	Specific Regulations Needed?
<p><i>provides a welcoming and attractive presence at this gateway to Kirkland.</i></p>	<p>Place, buffer pedestrians from the street, and provide visibility of ground floor retail uses.</p> <ul style="list-style-type: none"> <li>• Buildings should be oriented towards the street when located along NE 38<sup>th</sup> Place.</li> <li>• Design elements such as multiple storefronts, pedestrian-oriented signs, exterior light fixtures, glazing, landscaping, and awnings should be utilized to add human scale and interest at the street level.</li> <li>• Ground floor spaces along NE 38<sup>th</sup> Place should be transparent with windows of clear vision glass beginning no higher than 2' above grade to at least 10' above grade. Windows should extend across, at a minimum, 75% of the façade length. Continuous window walls should be avoided by providing architectural building treatments, mullions, building modulation, entry doors, and/or columns at appropriate intervals.</li> <li>• Varied window treatments should be encouraged. Architectural detailing at window jambs, sills, and heads should be emphasized. Use of ribbon windows should be avoided.</li> <li>• A street wall is a wall or portion of a wall of a building facing a street. Continuous street walls should incorporate vertical and horizontal modulations into the building form.</li> </ul>		<ul style="list-style-type: none"> <li>• KZC 110.60.11 - Entry or Gateway Features in Design Districts – In Design Districts, if the Comprehensive Plan or Design Guidelines designate the subject property for an entry or gateway feature, then the applicant shall design and install an entry feature area on the subject property. The size of the entry feature area shall be at least 100 square feet, and may include landscaping, art, signage or lighting. The design shall be reviewed by the City and decided upon as part of the Design Review for the proposed development. The applicant shall provide an easement or dedication of property surrounding the entry feature.</li> </ul>		

Existing Comprehensive Plan Policies	Proposed Design Guidelines <sup>1</sup> - <i>Design Review Board Authority</i>	Proposed Zoning Regulations	Existing Zoning Regulations	Additional Guidelines Needed?	Specific Regulations Needed?
	<ul style="list-style-type: none"> <li>• Along pedestrian oriented streets, upper story building facades should be stepped back to provide enough space for decks, balconies, and other activities overlooking the street.</li> <li>• Awnings or canopies should be required on facades adjoining sidewalks. Blank walls should be avoided near sidewalks, open spaces, and pedestrian areas.</li> <li>• Blank walls should not be visible from the street or sidewalk. Where blank walls are unavoidable, they should be treated with landscaping, art, or other architectural treatments.</li> </ul> <p><b>Gateway</b></p> <ul style="list-style-type: none"> <li>• A gateway is an urban design feature that signifies a sense of place and arrival into a city or neighborhood. A gateway should be designed in the location shown in the Comprehensive Plan.</li> <li>• The design elements of the gateway should include a combination of landscaping, architectural features, and artwork which:               <ul style="list-style-type: none"> <li>◦ <u><i>(HCC Recommendation – additional text): Contain a highly visible and welcoming public space between the sidewalk and the building</i></u></li> </ul> </li> </ul>	<ul style="list-style-type: none"> <li>• <u><i>HCC Recommendation – add additional special regulation requiring public space between sidewalk and building in gateway area.</i></u></li> </ul>			

Existing Comprehensive Plan Policies	Proposed Design Guidelines <sup>1</sup> - <i>Design Review Board Authority</i>	Proposed Zoning Regulations	Existing Zoning Regulations	Additional Guidelines Needed?	Specific Regulations Needed?
	<p><u>which is easily accessible, comfortable, safe, and includes pedestrian amenities;</u></p> <ul style="list-style-type: none"> <li>○ Establish a landmark that reflects the TOD elements of the site;</li> <li>○ Reinforce NE 38th Place and 108th Avenue NE as a focal point;</li> <li>○ Transition between Kirkland and Bellevue and the Yarrow Bay Business District to the west; and</li> <li>○ Are integrated with the TOD building design</li> </ul>				
4. <i>Protect the vegetative buffers and significant trees along the site's eastern and southeastern borders through development standards.</i>	None Proposed		<ul style="list-style-type: none"> <li>● Tree retention standards in KZC Section 95.30</li> </ul>		
5. <i>Minimize the visual impacts of parking facilities from adjacent rights-of-ways.</i>	<ul style="list-style-type: none"> <li>● Parking areas should not be located between NE 38<sup>th</sup> Place and buildings.</li> <li>● Access driveways to parking areas should be minimized.</li> <li>● Parking lots should be designed to provide for clear vehicular and pedestrian circulation and be well organized.</li> <li>● Screening and landscaping should be used to reduce the visual impact of parking lots and/or parking structures to the surrounding</li> </ul>	<ul style="list-style-type: none"> <li>● Minimum 10' setback for parking structures along NE 38<sup>th</sup> Place</li> <li>● Add regulation to KZC 105.58 – Location of Parking Areas Specific to Design Districts</li> </ul>	<ul style="list-style-type: none"> <li>● KZC 95.44 – Internal Parking Lot Landscaping Requirements</li> <li>● KZC 95.45 – Perimeter Landscape Buffering for Driving and Parking Areas</li> </ul>		

Existing Comprehensive Plan Policies	Proposed Design Guidelines <sup>1</sup> - <i>Design Review Board Authority</i>	Proposed Zoning Regulations	Existing Zoning Regulations	Additional Guidelines Needed?	Specific Regulations Needed?
	<p>neighborhood.</p> <ul style="list-style-type: none"> <li>• Parking structures shall be designed and located to obscure the view of parked cars from adjacent properties. Parking structures should be located to the back of buildings or underground with intervening uses.,</li> <li>• Portions of parking structures visible from the street that cannot be placed behind an intervening use due to site topography, should be constructed with high quality materials and be architecturally compatible with the character of surrounding buildings.</li> </ul> <p>In addition, architectural treatment, artwork, building setbacks, and/or dense landscaping should be used to further reduce the visual impact of parking structures along the street.</p> <p>If adjacent to the required gateway, the exterior of parking structure should reflect the design elements of the gateway. Design should avoid the appearance of a parking structure.</p>				
6. Foster the creation of vibrant and desirable living environment through the use of high quality design, public amenities, and open space.	<p><b><i>High Quality Design</i></b></p> <p>See Policy #1</p> <p><b><i>Public amenities and Open Space</i></b></p>	None Proposed	None		

Existing Comprehensive Plan Policies	Proposed Design Guidelines <sup>1</sup> - <i>Design Review Board Authority</i>	Proposed Zoning Regulations	Existing Zoning Regulations	Additional Guidelines Needed?	Specific Regulations Needed?
	<ul style="list-style-type: none"> <li>• Public open space should be provided on the subject property which can be used by the general public, residents, and transit users.</li> <li>• Public open space should be open to the sky except where overhead weather protection is provided (e.g. canopies and awnings). The space should appear and function as public space rather than private space.</li> <li>• Public open space should be located in close proximity to commercial and retail uses that are required along NE 38<sup>th</sup> Place. The public open space should be well defined and contain amenities such as outdoor dining, seating areas, art, water features, and/or landscaping. Adequate room for pedestrian movement through the space should be maintained. Additional public open space in a location convenient to the site's transit users may also be appropriate.</li> <li>• Careful attention should be paid to the transition between transit operations and the building to create a well defined pedestrian space such as a small plaza with landscaping features.</li> <li>• A combination of lighting, access to sunlight, paving, landscaping, and seating should be used to enhance the pedestrian experience with the public open space.</li> </ul>				
7. Promote sustainable	None Proposed	<ul style="list-style-type: none"> <li>• New regulation calls for</li> </ul>	None		

Existing Comprehensive Plan Policies	Proposed Design Guidelines <sup>1</sup> - <i>Design Review Board Authority</i>	Proposed Zoning Regulations	Existing Zoning Regulations	Additional Guidelines Needed?	Specific Regulations Needed?
development through support of green building practices at the Park and Ride.		combination of Evergreen and LEED standards.			